

MEMO TO THE HISTORIC PRESERVATION COMMISSION

HEARING DATE: JULY 7, 2021

Case Number:	2021-004690CRV
Project Address:	555 19TH ST
Zoning:	UMU (URBAN MIXED USE) Zoning District
	Life Science and Medical Special Use District
	68-X Height and Bulk District
Block/Lot:	4058/009
Project Sponsor:	Justin A. Zucker
	Reuben, Junius, and Rose LLP
	One Bush Street, Suite 600
Staff Contact:	Monica Giacomucci – 628-652-7414
	Monica.Giacomucci@sfgov.org

Recommendation: Adopt Resolution to Recommend Approval

Background

The Planning Department seeks the advice of the Historic Preservation Commission on the proposed project at 555 19th Street. As described in detail below, the proposed project entails a change in use from PDR (Production, Distribution and Repair) to office on the first, second, and third floors (approximately 24,848 gross square feet), pursuant to Planning Code Section 803.9(c), which states:

(c) **Preservation of Historic Buildings within and UMU Districts**. The following rules are intended to support the economic viability of buildings of historic importance within the UMU District.

(1) This subsection applies only to buildings that are a designated landmark building, or a building listed on or determined eligible for the California Register of Historical Resources by the State Office of Historic Preservation.

(2) All uses are permitted as of right, provided that:

(A) The project does not contain nighttime entertainment.

(B) Prior to the issuance of any necessary permits, the Zoning Administrator, with the advice of the Landmarks Preservation Advisory Board, determines that allowing the use will enhance the feasibility of preserving the building.

(C) Residential uses meet the affordability requirements of the Residential Inclusionary Affordable Housing Program set forth in Section 315.1 through 315.9.

(3) The Landmarks Preservation Advisory Board shall review the proposed project for compliance with the Secretary of the Interior's Standards, (36 C.F.R. § 67.7 (2001)) and any applicable provisions of the Planning Code.

The proposed project qualifies for this section of the Planning Code, since the subject building at 555 19th Street is located within an Urban Mixed Use (UMU) Zoning District, and has been identified as a contributing building within the Central Waterfront: Third Street Industrial Historic District which is eligible for listing in the California Register of Historical Resources.

The Project Sponsor has also notified the Department that the property owner is pursuing individual designation of the subject property on the California Register of Historical Resources and National Register of Historic Places. As of the publication of this memo, the California State Historical Resources Commission (SHRC) informed the applicant in writing that the SHRC intends to take action on the nomination at its next meeting, on Friday, July 30, 2021.

Property Description

Constructed by Emil A. Neumarkel in 1919, 555 19th Street was commissioned by M. Levin and Sons, a scrap metal and salvage business founded by Russian immigrant Max C. Levin. Used as a warehouse, store, and office building by M. Levin and Sons, 555 19th Street is a three-story-over-basement, heavy timber frame, brick loft building designed in the American Commercial style with three visible facades along 3rd, 19th, and Illinois Streets. Distinctive features of the building include its extruded corner pavilions and stepped parapet, corbelled brick ornament, and symmetrical punched fenestration pattern with divided lite metal windows. The existing building currently contains approximately 33,998 square feet, with 24,848 square feet of PDR use on the first through third stories, and 9,150 square feet at the basement level dedicated to storage and building systems.

Current Proposal

The proposal is to convert approximately 24,848 gsf of existing PDR use to office use on the first, second, and third floors. An existing basement storage area will not be converted or altered. No exterior work is proposed as part of the project.

To further support the preservation of the subject building, the Project Sponsor has submitted a Historic Building Maintenance Plan prepared by VerPlanck Historic Preservation Consulting, which outlines a program for regular maintenance, inspection, and repair of the building. Due to a significant repair program executed in the 1980s following a fire and a seismic retrofit undertaken in the 2000s, the building is in good condition and



requires little intervention beyond routine maintenance. Please see the attached Historic Building Maintenance and Repair Plan for more information.

Staff Analysis

The Department asks the HPC to consider the following information:

Based upon a review of the proposed change of use per the Secretary of the Interior's Standards for Rehabilitation (Secretary's Standards), the change in use from PDR to office would be considered a compatible use within the former industrial mixed-use building.

According to Standard 1, "A property shall be used for its historic purpose or be placed in a new use that requires minimal change to the defining characteristics of the building and its site and environment." The proposed office use requires minimal, if any, change to the defining characteristics of 555 19th Street, especially considering that the building historically contained the offices of M. Levin and Sons. The project would not impact the building's status as a contributor within the Central Waterfront: Third Street Industrial District, which is eligible for listing in the California Register of Historical Resources.

As noted in Standard 5, "Distinctive materials, features, finishes, and construction techniques or examples of craftsmanship that characterize a property will be preserved." Likewise, Standard 6 states that "Deteriorated historic features will be repaired rather than replaced. Where the severity of deterioration requires replacement of a distinctive feature, the new feature will match the old in design, color, texture, and, where possible, materials. Replacement of missing features will be substantiated by documentary and physical evidence." The Historic Building Maintenance Plan proposed by the Project Sponsor does not include any recommendations for major alterations, and appropriately recommends a cyclical maintenance program for 555 19th Street which would proactively correct any material deficiencies with the existing windows, brickwork, and roof.

Recommendations

The Department finds the proposed change of use to be in compliance with the Secretary of the Interior's Standards for Rehabilitation, since no exterior work is proposed that would impact character-defining features of the historic resource. The proposed office use is compliant, and restores a component of the original building owner's use of the building. Further, the Department finds that the proposed project would enhance the feasibility of preserving the building by providing for a cyclical maintenance and restoration program and compatible new use. When executed, scopes described in the maintenance plan would improve the viability of preserving the subject property without impacting the building's integrity or historic resource status.

Required Commission Action

In order for the project to proceed, the Commission must adopt a Resolution to Recommend Approval for subsequent review and approval by the Zoning Administrator, pursuant to Planning Code Section 803.9(c).



Recommendation: Adopt Resolution to Recommend Approval

Attachments:

- Draft Resolution
- Maps and Exhibits
- Project Plans (dated March 22, 2021)
- Historic Building Maintenance and Repair Plan prepared by VerPlanck Historic Preservation Consulting (dated March 12, 2021)
- DPR Survey Form, 555 19th Street
- DPR District Form, Central Waterfront: Third Street Industrial Historic District
- Project Sponsor Brief







HISTORIC PRESERVATION COMMISSION DRAFT RESOLUTION NO. XXX

HEARING DATE: JULY 7, 2021

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ADOPTING FINDINGS FOR THE PROPOSED PROJECT AT 555 19th STREET (ASSESSOR'S BLOCK 4058, LOT 009), WHICH IS A CONTRIBUTING BUILDING WITHIN THE CENTRAL WATERFRONT: THIRD STREET INDUSTRIAL DISTRICT ELIGIBLE FOR LISTING IN THE CALIFORNIA REGISTER OF HISTORICAL RESOURCES AND LOCATED WITHIN A UMU (URBAN MIXED USE) ZONING DISTRICT AND A 68-X HEIGHT AND BULK DISTRICT.

Preamble

WHEREAS, On April 5, 2021, the Project Sponsor (Justin Zucker of Reuben, Junius, and Rose LLP), on behalf of the Property Owner (Martin C. Levin), filed Building Permit Application (BPA) No. 202104057866 with the San Francisco Planning Department for 555 19th Street (Block 4058, Lot 009).

WHEREAS, the proposed project intends to utilize Planning Code Section 803.9(c) to allow office use on the first through third floors of 555 19th Street. Pursuant to Planning Code Section 803.9(c), the following provision is intended to support the economic viability of buildings of historic importance within the UMU District:

(c) **Preservation of Historic Buildings within and UMU Districts**. The following rules are intended to support the economic viability of buildings of historic importance within the UMU District.

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(C) Residential uses meet the affordability requirements of the Residential Inclusionary Affordable Housing Program set forth in Section 315.1 through 315.9.

(3) The Landmarks Preservation Advisory Board shall review the proposed project for compliance with the Secretary of the Interior's Standards, (36 C.F.R. § 67.7 (2001)) and any applicable provisions of the Planning Code.

WHEREAS, on July 7, 2021, Department staff presented the proposed project to the Historic Preservation Commission. The Commission's comments on the compliance of the proposed project with the Secretary of the Interior's Standards for Rehabilitation and the ability of the proposed project to enhance the feasibility of the historic resource would be forwarded to the Zoning Administrator for consideration under Planning Code Section 803.9(c).

THEREFORE BE IT RESOLVED that the Historic Preservation Commission has reviewed the proposed project at 555 19th Street, on Lot 009 in Assessor's Block 4580, and this Commission has provided the following comments:

BE IT FURTHER RESOLVED that the Historic Preservation Commission hereby directs its Recording Secretary to transmit this Resolution, and other pertinent materials in the Case File No. 2021-004690CRV to the Zoning Administrator.

I hereby certify that the Historic Preservation Commission ADOPTED the foregoing Resolution on July 7, 2021.

Jonas P. Ionin Commission Secretary

AYES:

NAYS:

ABSENT:



RECUSE:

ADOPTED: July 7, 2021







Sanborn Map*



*The Sanborn Maps in San Francisco have not been updated since 1998, and this map may not accurately reflect existing conditions.



Zoning Map





Aerial Photo



SUBJECT PROPERTY



Site Photo



ABBREVIATIONS

A.C.—A/C	
	Asphalt concrete
ACOUS.	Acoustical
ADJ.	Adjacent
ALUM.	Aluminum
AGG.	Aggregate
APPROX. ARCH.	Approximate Architectural
AKCH.	/ Architect
B.C.	Bottom of conc.
D.C.	/ curb
BLDG.	, Building
B.O.	Bottom of
BLK.	Blocking/ block
BM.	Beam
BTWN.	Between
B.U.R.	Built-up roof
B.W.	Bottom of wall
CAB.	Cabinet
C.B.	Catch basin
CEM.	Cement
C.L.	Centerline
CLG.	Ceiling
CMU.	Concrete masonry
	unit
CNTL. JT.	Control joint
C.O.	Cleanout
COL.	Column
CONC.	Concrete
CONT.	Continuous
CTR.	Center
DEMO.	Demolition
D.F.	Drinking fountain
DIA.	Diameter
DIM.	Dimensions
DISP.	Dispenser
DN.	Down
DWGS.	Drawings
(E)	Existing
EA.	Each
ELEC.	Electrical
ELEV.	Elevation
EQ.	Equal
	Equal
EQUIP.	Equipment
EX. EXP.	Existing
EXT.	Expansion Exterior
F.D.	Floor drain
FDN.	Foundation
F.F.	Finished Floor
FIN.	Finish
FL.	
FLR.	Floor Floor
	Foundation
	roundation
FNDN. F O C	Face of concrete
F.O.C.	Face of concrete Face of framina
F.O.C. F.O.FRM'G	Face of framing
F.O.C. F.O.FRM'G F.O.F	Face of framing Face of finish
F.O.C. F.O.FRM'G F.O.F F.O.P	Face of framing Face of finish Face of plywood
F.O.C. F.O.FRM'G F.O.F	Face of framing Face of finish
F.O.C. F.O.FRM'G F.O.F F.O.P F.O.SHT'G.	Face of framing Face of finish Face of plywood Face of sheathing
F.O.C. F.O.FRM'G F.O.F F.O.P F.O.SHT'G. GA. GALV. G.B.	Face of framing Face of finish Face of plywood Face of sheathing Gauge
F.O.C. F.O.FRM'G F.O.F F.O.P F.O.SHT'G. GA. GALV. G.B. GL.	Face of framing Face of finish Face of plywood Face of sheathing Gauge Galvanized Grab bar Glass
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(N)N.I.C. NOM. N.T.S. O.C. O.D. OPNG. OPP. OPP.HD. OZ. PERF. PL. P/L PLAS PLYWD PR. PT. PTN. P.V.C. R. RCPT. REC'D. REF. REINF. REQ'D R.D. RM. R.O. RWD. SCHED. SECT. S.E.D. SHT. SIM. S.M.D. S.P.D. SPEC. SQ. S.S. S.S.D. STD. STL. STOR. SURF. SUSP. SYM. TEL. T&G. THK. Т.О. T.O.C. T.W. TYP. U.O.N. UNF. VAR. VERT. V.I.F. W/ W.C. WD. W/O W.W.M. WWF

North New Not in contract Nominal Not to scale On center Outside diameter opening opposite Opposite hand Ounce Peforated Plastic Property line Plaster Plywood Pair Point Partition Polyvinyl Chloride Pipe Radius Receptacle Recommended Reference Reinforcing/ment Required Roof drain Room Rough opening Redwood Schedule Section See electrical drawings Sheet Similar See mechanical drawing See plumbing drawing Specifications/ specified Square Stainless steel See structural drawings Standard Steel Storage Surface Suspended Symmetrical Telephone Tongue and groove Thick Top of Top of concrete Top of wall Typical Unless otherwise noted Unfinished Varies Vertical Verify in field With Water closet Wood Without Woven wire mesh Welded wire fabric And At Centerline Number



DIRECTORY							
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ATTORNEY	REUBEN JUNIUS & ROSE LLP 1 BUSH STREET, SUITE 600 SAN FRANCISO, CA 94104 415-567-9000 TTUNNY@REUBENLAW.COM	THOMAS TUNNY					
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Wall, floor and roof type

Door number (for door schedule)

Window number (for window schedule)

Window number — obscured glass

Interior elevation number

Sheet note designation and number

Room number (for finish schedule)

Existing Sprinkler: Pendent Concealed

Existing Sprinkler: Pendent Semi-recessed

Existing Sprinkler: Sidewall

Existing Sprinkler: Upright with Riser

Existing Sprinkler: Pendent

Existing Sprinkler: Pendent Online

EXISTING BUILDING

Existing building at 555 19th St.; 3 stories; F-1 Moderate Hazard Factory Industrial occupancy over a basement of S-1 Storage per CBC (California Building Code); 16-Manufacturing Use per SFPC (SF Planning Code); Type III-B Construction; Automatic Sprinklers, and Fully accessible. 1 hour shafts include: 3 stair shafts and 1 HC elevator. There is 1 HC entry, 3 fire exit doors, and multiple HC WC's. The façade is 2 HR brick masonry with black metal framed windows. This was most recently approved as F-1-Industrial Occupancy (CBC) and 16-Manufacturing Use (SFPC) per building permit 2018-0207-0675.

PROPOSAL PER THIS PERMIT

Change F-1-Industrial (CBC) Occupancy and 16-Manufacturing (SFPC) Use, to B-Business Office (CBC) Occupancy and 10-Office (SFPC) Use. The Basement remains S-1 Storage.

There will be NO PHYSICAL CHANGE to the building and the following will remain the same: Construction Type III-B; Height; Area; Story count; Envelope; WC Fixture count; Accessible Path; Exit/Egress Path; Fire Rating; Sprinklers; Mechanical; Fire Alarm; and Lighting. This building meets or exceeds code minimum in terms of egress, fire life safety, accessibility, and plumbing fixture count as F-1-Industrial OR B-Business Office, and was designed to serve both occupancies and uses at different times. Note the proposed B occupant load is less than the existing F-1 load and less than the previous B occupant load, because the 2019 CBC has relaxed the occupant laod factor from 100 to 150. This equates to a lower demand with egress components and plumbing fixtures. See load calculation on A0.2. In terms of DBI, this is simply a return to the previously approved B-Business Office Occupancy as documented on sheet A0.8.

In terms of Planning, UMU zoning allows office uses on a limited basis based on the number of floors at the property, but is not limited by the number of floors if the building is designated as Historic per SFPC 843.65 & 803.9(C). A Historic consultant will submit an application package to display that the building is eligible for the California Register of Historical Resources by the State Office of Historic Preservation. The Historic Preservation Commission shall review the proposed project for compliance with the Secretary of the Interior's Standards, and any applicable provisions of the Planning Code.

RELEVANT SFPC CODE SECTIONS FOR CHANGE OF USE

SFPC 843.65

Office Uses in Landmark Buildings permitted per 890.70, 803.9(c)

SFPC 803.9(C)

Preservation of Historic Buildings within and UMU Districts. The following rules are intended to support the economic viability of buildings of historic importance within the UMU District.

1) This subsection applies only to buildings that are a designated landmark building, or a building listed on or determined eligible for the California Register of Historical Resources by the State Office of Historic Preservation.

2) All uses are permitted as of right, provided that:

A) The project does not contain nighttime entertainment.

B) Prior to the issuance of any necessary permits, the Zoning Administrator, with the advice of the Landmarks Preservation Advisory Board, determines that allowing the use will enhance the feasibility of preserving the building.

C) Residential uses meet the affordability requirements of the Residential Inclusionary Affordable Housing Program set forth in Section 415et seq.

3) The Landmarks Preservation Advisory Board shall review the proposed project for compliance with the Secretary of the Interior's Standards, (36 C.F.R. § 67.7 (2001)) and any applicable provisions of the Planning Code.



PROJECT DATA

PROJECT SITE	555 19TH ST. San Francisco, california, 94107						
CROSS STREETS	3RD STREET AND ILLINOIS ST.						
ASSESSOR'S PARCEL #	BLOCK 4058, LOT 009						
PARCEL SIZE	13,000 SF = 65' X 200, EXISTING & PROPOSED						
FOOTPRINT	9,780 SF = 60' X 163', EXISTING & PROPOSED						
ZONING	UMU, EXISTING & PROPOSED						
CONSTRUCTION TYPE	III-B, EXISTING & PROPOSED 2 HR MASONRY ENVELOPE AND EXPOSED INTERIOR WOOD FRAME INSIDE						
OCCUPANCY GROUP PER CBC/DBI	EXISTING: F-1/MODERATE HAZARD FACTORY INDUSTRIAL OVER S-1/STORAGE PROPOSED: B/BUSINESS OFFICE OVER S-1/STORAGE						
BUILDING USE PER SFPC/PLANNING							
BUILDING HEIGHT	40'—10", EXISTING & PROPOSED (68'—X ALLOWABLE)						
NO. OF STORIES	3 OVER BASEMENT, EXISITNG & PROPOSED						
GROSS AREA	24,848 SF LEVELS 1–3, EXISITNG & PROPOSED 9,150 SF BASEMENT, EXISITNG & PROPOSED						
FIRE SPRINKLER	FULLY SPINKLERED, EXISITNG & PROPOSED						
CODES USED:	2019 CBC 2019 SFBC						

SHEET INDEX

A 0.1	COVER SHEET	NO SCALE
A 0.2	AREA, LOAD, FIXTURE, CALCULATIONS	1/16"=1'-0"
A 0.3	EXITING EGRESS CODE DIAGRAM	1/16"=1'-0"
A 0.4	FIRE RATING CODE DIAGRAM	1/16"=1'-0"
A 0.5	ACCESSIBILITY CODE DIAGRAM	1/16"=1'-0"
A 0.6	CBC 11B ACCESSIBLE DETAILS	NA
A 0.7	CBC 11B ACCESSIBLE DETAILS	NA
A 0.8	DBI B OCCUPANCY PERMIT PRECEDENT	NA
A 1.1	EXISTING & PROPOSED PLAN 1ST FLOOR & SITE	1/8"=1'-0"
A 1.2	EXISTING & PROPOSED PLAN 2ND FLOOR	1/8"=1'-0"
A 1.3	EXISTING & PROPOSED PLAN 3RD FLOOR	1/8"=1'-0"
A 1.4	EXISTING & PROPOSED PLAN BASEMENT	1/8"=1'-0"
A 2.1	EXISTING & PROPOSED EXTERIOR ELEVATION	1/8"=1'-0"
A 2.2	EXISTING & PROPOSED EXTERIOR ELEVATION	1/8"=1'-0"
A 2.3	EXISTING & PROPOSED EXTERIOR ELEVATION	1/8"=1'-0"
A 2.4	EXISTING & PROPOSED EXTERIOR ELEVATION	1/8"=1'-0"
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PLUMBING FIXTURE LOAD CALCULATIONS

FIXTURE) B PROPOSED REQUIRED 111 LOAD (56 PER M/F)	PROVIDED EXISTING	F—1 PROVIDE OVERAGE
W WC	1 = 1 - 50	4 = 51 - 100	6	5
M WC	1 = 1 - 50	2 = 51 - 100	6	5
M URINAL	N/A	1 = 1 - 100	0 (CREDIT 1 M WC)	N/A
W LAV	1 = 1 - 50	2 = 51 - 100	6	5
M LAV	1 = 1 - 50	1 = 1 - 75	4	3
FOUNTAIN	1 = 1 - 250	1 = 1 - 150	1	0
CONCLUSION:	EXISTING PLUMBING FIXTU	JRE COUNT MEETS OR EXCEED	S REQUIRED COUNT FOR	F—1 AND B OC









AREA & OCCUPANCY LOAD CALCULATION FOR EGRESS & PLUMBING

LEVEL	GROSS AREA	OCCUPIED AREA	EXISTING F—1 OCCUPANT LOAD CBC 1004.5 EGRESS 1:100 FACTOR	PROPOSED B OCCUPANT LOAD CBC 1004.5 EGRESS 1:150 FACTOR	EXISTING & PROPOSED S-1 OCCUPANT LOAD CBC 1004.5 EGRESS 1:300 FACTOR	EXISTING F—1 OCCUPANT LOAD UPC 422.1 TBL A PLUMBING FIXTURE 1:200 FACTOR	PROPOSED B OCCUPANT LOAD UPC 422.1 TBL A PLUMBING FIXTURE 1:200 FACTOR
FIRST LEVEL	8,971 GSF	7,267 SF	73	49	_	4	37
SECOND LEVEL	8,404 GSF	7,647 SF	77	51	_	4	39
THIRD LEVEL	7,473 GSF	6,849 SF	69	46	_	4	35
OCCUPIED TOTAL	24,848 GSF	21,763 SF	219	146	_	12	111
NON OCCUPIED BASEMENT	9,150 GSF	7,264 SF	_	_	25	_	_

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03-22-21

SCALE 1/16" = 1'- 0'

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EXITING EGRESS CALCULATION AND NOTES

A)	EXIT PATH	H: GREATES	ST TRAVEL DISTANCE TO STAIR	FOR E	3 < 300' F	PER
	LEVEL 1	102D	19+11+16+40+6+2+37+15=	146'	< 300', 0	ЭK
	LEVEL 2	202H	48+12+38+5=	103'	< 300', 0)K
	LEVEL 3	302SOUTH	+ 55+27+27+11+6=	126'	< 300', 0)K

B)	LONGEST	COMMON	PATH OF	TRAVEL	OR	ONE	EXIT	<	100'	FOR	B PE
	LEVEL 1	102D	19+11+	16=				46	6' <	100',	OK
	LEVEL 2	201B	14+15+	-6=				35	5' <	100',	OK
	LEVEL 3	301B	28+33=	=				61	' <	100',	OK

C) EXIT PATH LENGTHS BETWEEN EXIT DOORS ARE GREATER THAN 1/3 OF THE DIAGONAL LENGTH OF HABITABLE SPACES PER CBC 1015.2.1 (EX 2) IN THIS FULLY SPINKLERED BUILDING. LEVEL 1 153'-0" > 57' (= 170' / 3), OK LEVEL 2 152'-4" > 57' (= 170' / 3), OK LEVEL 3 132'-2" > 57' (= 170' / 3), OK

C) STAIR WIDTH DESIGN LOAD 51 LEVEL 2 PER A0.2 EXISTING MIN STAIR WIDTH (EAST EXIT STAIR 013) $4'-8^{1''}_{2} > 5.1''$ [(17 PEOPLE PER EXIT=51 OL PEOPLE/3 EXITS) X 0.3], OK PER CBC 1005.3.1 EXISTING MIN STAIR WIDTH (EAST EXIT STAIR 013) $4'-8^{1''}_{2}$ > 44" (DEFAULT MINIMUM), OK PER CBC 1011.1

- D) EGRESS WIDTH DESIGN LOAD 51 LEVEL 2 PER A0.2 EXISTING MIN EGRESS WIDTH 4'-3" > 5.2" [26 PEOPLE PER EXIT=51 PEOPLE/2 EXITS) X 0.2], OK PER CBC 1005.3.2 EXISTING MIN EGRESS WIDTH 4'-3'' > 44'' (DEFAULT MINIMUM), OK, PER CBC 1020.2
- E) PER CBC 1010.1.2.1, A SINGLE DOOR MAY SWING OPPOSITE THE PATH OF TRAVEL IF THE OCCUPANT LOAD IS LESS THAN 50 PEOPLE.
- F) PER CBC 1016.2, AN EXIT PATH MAY PASS THROUGH ONE INTERVENING ROOM OF THE SAME B OCCUPANCY OF THE SAME TENANT. SPACE. SEE PATH FROM 202H TO 202 TO STAIR 013.

ER CBC TABLE 1017.2

PER CBC TABLE 1006.2.1

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FIRE RATING NOTES

A) THIS IS TYPE III-B CONSTRUCTION PER CBC TABLES 601 AND 602.3 WITH 2 HR NON COMBUSTIBLE ENVELOPE MASONRY BEARING WALLS AND NON RATED (EXCEPT AT SHAFTS) AND EXPOSED WOOD FRAMING AT THE INTERIOR. THE 3 STORIES ARE PROPOSED B OCCUPANCY AND THE BASEMENT REMAINS S-1 OCCUPANCY.

B) 3 STORY EXISTING = 4 STORY ALLOWABLE PER CBC 504.4, OK

C) 40'-10" EXISTING HEIGHT <75' ALLOWABLE PER 504.3, OK

D) 24,848 SF < 114,00 SG (57,000 SF X 2) PER 506.2, OK

E) ALL EXISTING WINDOWS & DOORS NON RATED BECAUSE > 20' TO C.L.(FSD) OF 3RD ST, 19TH ST, ILLINOIS ST, AND <25% WINDOWS ON FACADE PER FLOOR ON SOUTH ELEVATION WITH >5' SETBACK, PER CBC TABLE 705.8.

F) 3 STORY SHAFTS AT ELEVATOR AND STAIRS MAY BE 1 HR PER CBC 713.4 AND 1023.2

G) 2 STORY FLOOR OPENING LEVELS 2-3 ARE NOT CONSIDERED ATRIUMS AND DO NOT REQUIRE A SHAFT ENCLOSURE OR SPECIAL MECHNICAL EXHAUST PER 202, 404.5 EX, 712.1.9

H) NO CHANGE TO ENVELOPE WALLS (2 HR), DOORS(NR), AND WINDOWS(NR)

I) NO CHANGE TO SHAFT WALLS(1) AND DOORS(60) AT ELEVATOR AND STAIRS

FIRE RATING LEGEND & CODE

- EXISTING NON RATED INTERIOR PARTITION WALL OR FURRED WALL (CBC TABLE 601)
- EXISTING I HR, 3 STORY, SHAFT WALL AT STAIR OR ELEVATOR (CBC TABLES 713.4, 1023.2)
- EXISTING 2 HR RATED EXTERIOR BEARING WALL (CBC TABLE 601)
- NR EXISTING NON RATED EXTERIOR WINDOWS AND DOORS (CBC TABLES 705.8, 602)
- NR EXISTING NON RATED INTERIOR DOORS IN NON RATED PARTITIONS (CBC TABLES 601, 716.1(2))
- 60 EXISTING 60 MIN. RATED INTERIOR DOORS IN 3 STORY, SHAFT WALLS AT STAIR OR ELEVATOR (CBC TABLES 713.4, 716.1(2), 1023.2)

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A# ACCESSIBLE SYMBOL	ACCESSIBLE FERATURE	2019 CBC 11B SECTION
م م	ACCESSIBLE ROUTE	11B-206 11B-402
$\begin{bmatrix} & & & & & & \\ & & & & & & \\ & & & & & \\ & & & & & & $	TURNING CLEARANCE: 60" CIRCLE OR T. SOOR DWING & KNEE TOE MAY ENCROACH	11B-304.3.1 11B-304.3.2 11B-304.4
トー ア 入気意 レ <u>36"</u> 述 PUSH INTERIOR FRONT APPROACH	DOOR CLEARANCES. PUSH FRONT APPROACH: 12" SIDE X 48"	11B-303.3 TBL 11B- 404.2.4.1 TBL 11B- 404.2.4.2
N T T A A S S I V 36" LI18 N PULL INTERIOR	PULL FRONT APPROACH: 18" SIDE X 60" (INTERIOR) 24" SIDE X 60" (EXTERIOR)	11B-404.2.5 11B-404.2.9 11B-404.3.1 1010.1.1
FRONT APPROACH	1:2 CANT MAX, 1/4" H RISE MAX Threshold	11B-404.2.7 11B-309.4
UZ IN UZ UZ <u>36"</u> ⊥24 [*] N PULL EXTERIOR FRONT APPROACH	5LB OPENING FORCE INTERIOR, 8.5 LB OPENING FORCE EXTERIOR	
	32" CLEAR WIDTH	
	LEVER HARDWARE @ 34"-44" AFF	11B-305.5,
	30"X48" PARALLEL APPROACH & SIDE REACH CLEARANCES OVER COUNTER/CAB. 34"H MAX COUNTER. PARALLEL APPROACH AT COMPANION SEATS AT FIXED FURNITURE.	11B-303.3, 11B-308.3.2, 11B-802.3, 11B-804.3.1 11B-804.4, 11B-606 11B-804.6.4, 11B-804.6.6, 11B-904.1-11B -904.6
	KITCHENETTE 30" x 24" SURFACE PARALLEL APPROACH	11B-804.3
	30X48 FRONT APPROACH CLEAR FLOOR SPACE AT LAV, URINAL, FOUNTAIN; KNEE TOE CLEARANCE, INSULATION, 34" MAX LAV HEIGHT, 27" HEIGHT MIN LAV CLEARANCE, 36" MAX FOUNTAIN SPOUT HEIGHT, 40"H MAX MIRROR	11B-305.7, 11B-306, 11B-602 11B-605, 11B-606, 11B-902
	SINGLE WATER CLOSET: 60"X48" CLEAR IN FRONT OF TOILET. COMPARTMENT IN MULTISTALL: 60"X60" CLEAR IN FRONT OF TOILET. 60"X56" CLEAR UNOBSTRUCTED @ TOILET. GENERAL:	11B-603, 11B-604.2 TO 604.9
MULTI STALL	WC SHALL BE 17"-18" FROM SIDE WALL TO C.L. 17"-19"H SEATS. 36" REAR GRAB BAR AND 54" SIDE GRAB BAR	
	36"X60" TYPICAL NON ACCESSIBLE WC STALL	11B-604.8.2
	CENTERED DOOR ELEVATOR	11B-407
	SLIDING DOOR SIDE APPROACH. 22"X42" POCKET SIDE APPROACH. 24" X 42" STOP OR LATCH SIDE APPROACH	11B-404.2.4.2
A11	STAIR TREADS, WARNING STRIPE, HANDRAILS	11B-210, 11B-504 TO 11B-505
ACCES	SIBILITY LEGEND & C	ODE

B) EXISTING ACCESSIBLE PUBLIC ENTRY OFF OF 19TH STREET PER CBC 11B-206.4

C) EXISTING BUILDING ENTRY DOOR, EXISTING SUITE ENTRY DOORS, NEW WC DOORS, AND NEW WORK SPACE DOORS INCLUDE A THRESHOLD NO HIGHER THAN $\frac{1}{2}$ " ABOVE OR BELOW THE FLOOR, WITH A MAX 2:1 CANT PER CBC 11B-303.2, 11B-303.3

D) EXISTING ACCESSIBLE ELEVATOR SERVES ALL STORIES PER CBC 11B-206.6, 11B-407

E) EXISTING HC SINGLE STALL AND MULTI STALL WC'S PER FLOOR PER CBC FIG 11B-213 & 11B-603 TO 11B-606

F) EXISTING HC ROOM AND WC SIGNAGE WITH RAISED LETTERS AND WITH BRAILLE PER 11B-703

G) EXISTING DOORS INCLUDE: MANEUVERING CLEARANCES PER CBC 11B-404.2.4.1; LEVER HARDWARE AT 34"-38" AFF PER CBC 1010.1.9.1, 1010.1.9.2, 11B-505.10; < 5LB CLOSING FORCE PER CBC 11B-404.2.9; 32" MIN CLEAR PASSAGE PER CBC 11B-404.2.3

H) EXISTING STAIR HANDRAILS INCLUDE A 12" EXTENSION AT THE TOP OF THE FLIGHT AND 23" EXTENSION AT THE BOTTOM OF THE FLIGHT PER CBC 1014.6, 11B-505.10

I) EXISTING 2" WARNING STRIPES PROVIDED AT THE TOP AND BOTTOM TREADS AT INTERIOR FLIGHTS AND AT EVERY TREAD AT EXTERIOR FLIGHTS PER CBC 11B-504.4.1

J) EXISTING BREAK AREA COUNTERS INCLUDE: 34" HIGH
COUNTER CBC FIG 11B-804.3.2; 30"W REMOVABLE BASE
CABINETS UNDER SINK PER CBC 11B-804.3.1; 30"
COUNTER WORK SPACE PER CBC 11B-804.3; 48" THROUGH
CLEARANCE PER CBC 11B-804.2.1.

ACCESSIBILITY NOTES

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STAIRWAYS

















Control Button	Raised Symbol	Braille Message
Emergency Stop		"ST"OP Three Cells
Alarm	+	AL"AR"M Four Cells
Door Open		OP"EN" Three Cells
Door Close	M	CLOSE Five Cells
Main Entry Floor	*	MA''IN' Three Cells
Phone	~	PH"ONE" Four Cells





THIS BUILDING WAS PERMITTED AND APPROVED BY DBI AS B-BUSINESS OFFICE OCCUPANCY FROM 2013-2015 PER PERMITS: 2013-0220-0511; 2013-1205-3404; 2013-1216-4254; 2015-0310-0371. IN 2018, PERMIT 2018-0207-0675 WAS APPROVED BY DBI TO CHANGE THE OCCUPANCY FROM B-BUSINESS OFFICE TO F-1-INDUSTRIAL TO SUIT THE NEEDS OF THE TENANT. THERE WERE MINOR CHANGES TO NON RATED PARITIONS AND DOORS, AND A CHANGE FROM A URINAL TO A TOILET, BUT NO CHANGE TO THE ENVELOPE, AREA, STORIES, HEIGHT, FIRE RATING, CONSTRUCTION TYPE, SPRINKLERS, FIRE ALARMS, EXITING/EGRESS, PLUMBING FIXTURE COUNT, AND ACCESSIBILITY FEATURES.

THIS CURRENT PERMIT PROPOSES TO REVERT BACK TO A B-BUSINESS OFFICE OCCUPANCY WITHOUT PHYSICAL CHANGE TO THE BUILDING. THE PREVIOUS PERMITS MET OR EXCEEDED CODE REQUIREMENTS FOR BOTH B AND F-1 OCCUPANCIES. ADDITIONALLY WITH THE 2019 CBC RELAXING OF OCCUPANT LOAD FACTOR FOR B-OFFICE OCCUPANCY FROM 100 OLF TO A MORE LENIENT 150 OLF, THE PROPOSED B LOAD IS LESS THAN THE CURRENT F-1 LOAD AND LESS THAN THE PREVIOUS B OFFICE LOAD. SEE LOAD CALCULATION ON SHEET A0.2.

WE UNDERSTAND DBI REQUIRES ACCESSIBILITY REVIEW (DA CHECKLIST, 20% EXCEPTION), T-24 MECHANICAL M-04, ELECTRICAL PLANS, MECHNICAL PLANS, ETC. FOR TYPICAL CHANGE OF OCCUPANCY PERMITS. HOWEVER BECAUSE WE ARE REVERTING BACK TO THE PREVIOUSLY APPROVED B-OFFICE OCCUPANCY, WITHOUT CHANGE TO THE ENVELOPE/AREA/STORIES/FIRE RATING/CONSTRUCTION TYPE/EXITING/SPRINKLERS/FIRE ALARM/MECHANICAL/PLUMBING FIXTURES/LIGHTS, THE OCCUPANT LOAD HAS DECREASED SINCE 2015, AND THE BUILDING IS FULLY ACCESSIBLE, WE REQUEST APPROVAL OF THIS OCCUPANCY CHANGE WITHOUT PHYSICAL CHANGE TO THE BUILDING AS DOCUMENTED PER THIS PERMIT. WE PROVIDE THE FOLLOWING TO CONFIRM COMPLIANCE OF THE EXISTING/PROPOSED BUILDING: EXITING/EGRESS, FIRE RATING, ACCESSIBILITY DIAGRAM; AREA CALCS; LOAD CALCS; FIXTURE CALCS; EXISTING/PROPOSED PLANS(NO CHANGE), & EXISTING/PROPOSED ELEVATIONS(NO CHANGE).

Form Number: 8 Address(es): 4058 / 009 / 0 555 19TH ST Address(es): 4058 / 009 / 0 555 19TH ST Address(es): 4058 / 009 / 0 555 19TH ST Description: AREAIN UNITS 100, 202, 202, 202, 202. MEP under SEPARATE PERMIT. ON VEW BREAK Address(es): 4058 / 009 / 0 555 100 Cock \$100,000 00 S100,000 00 Occupancy Code: E.S-1 Building Use: 10 - OFFICE Disposition / Stage:	vve	Icome	to ou	r Pern	nit / C	ompla	aint Tr	acking Systen	n! Permit Details Report		Welcome	e to our	Permit /	Co	
Form Number: 8 Address(es): 455 / 405 / 40 5 55 19TH ST Address(es): 10.000 RATED, NON SPRUCTURAL PARTITIONS, NEW CONVENIENCE WC, NEW BREAK Address(es): 4665 / 406 / 40 406 / 40666 / 4066 / 4066 / 4066 / 4066 / 4066 / 4066 / 4066 / 4066 / 4066	Rep	ort Date:			1/7/20)21 5:29:(03 PM				Report Date	1. 	1/7/	2021	1
Address(e): 4959 /009 /0 555 1911 5T Address(e): 1059 /009 /0 555 1911 5T Description: T. NeW YON RATES, NON STOCUTURA, PARTITIONS, NEW CONVENIENCE WC, NEW EREAK AREAN UNITS 102, 202, 302. MEP under SEPARATE PERMIT. Address(e): 4093 /009 /0 Cock S100 000 00 65.51 Building Use 10 - OFFICE Disposition / Stage: 10 - OFFICE Disposition / Stage: Disposition / Stage: Action Date Stage Comments 2/2/2/013 TRIAGE Disposition / Stage: 2/2/2/013 TRIAGE 2/2/2/013 TRIAGE Disposition / Stage: Disposition / Stage: Contact Details: 2/2/2/013 FILING 2/2/2/013 FILING 2/2/2/013 11/1/2/013 2/2/2/013 12/1/2/013 12/1/2/013 12/1/2/013 12/1/2/013 12/1/2/013 2/2/2/013 12/1/2/013 2/2/2/013 12/1/2/013 2/2/2/013 12/1/2/013 2/2/2/013 2/2/2/013 2/2/2/013 2/2/2/013 2/2/2/013 2/2/2/013 2/2/2/013 2/2/2/013 2/2/2/013 2/2/2/013 2/2/2/013 2/2/2/013 2/2/2/2/013 2/2/2/2/2/2/2/013 2/2/2/2/2/2/2/2/2/2/2/2/2/2/2/2/2/2/2/	_					2200511								3120	1
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DBI NOTE: REVERTING BACK TO PREVIOUSLY APPROVED B OFFICE OCCUPANCY 3

	Report Date:	1	/7/2021 5:23:28 PM		Permit Details Report
		20			
	Application Number: Form Number:	2	01312164254		
ST	Address(es):			55 19TH	ST
REAK AREA OF PA# 201302200511.	Description:	T. A	I. INCLUDES:NEW REAS, AND VOLUN	NON-RATED, NON-ST	RUCTURAL PARTITIONS, WORK SPACES, BREAK ODATION TOILET ROOMS. MEP, SPRINKLER UNDE
	Cost		EPARATE PERMIT. 241,945.00		
	Cost: Occupancy Code:	100	241,945.00 ,S-1		
	Building Use:		0 - OFFICE		
Comments	Disposition / Stage:				
	Action Date	Sta	age		Comments
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L.	1/7/2014 4/8/2014	ISSUED COMPLET	E 2077500	Final Inspection/Appro	wed
	4/0/2014	COMPLET	2017300	i na nispection Appr	ALCO
	Contact Details:				
	Contractor Details:				
	License Number:		829808		
IGAME CA 94010-0000	Name:		JOHN-MICHAI	EL JOHNSON	
500 CO 24010-0000	Company Name:		PRINCIPAL BU		
	Address:			* SAN FRANCISCO	CA 94103-0000
	Phone:		4154341500		
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ed By Hold Description	Addenda Details: Description:				
	Step Station Arrive	e Start In I	Hold Out Hold Finis		Hold Description
	1 INTAKE 12/16/1	3 12/16/13	12/16	13 SHAWL HAREGGEWAIN	
52-3450.		3 12/16/13	and the second s	13 LIN EMILY	Approved, OTC
		3 12/16/13		13 SHAIKH MOHSIN	Approved, orc
		3 12/16/13		13 ENG AMY	N/A, MEP, SPRINKLERS UNDER SEPARATE
		1 12/16/13		01 TOM BILL	PERMIT. NOT APPLICABLE - Not enough changes for
	6 CPB 1/7/14	1/7/14	10/12/27	PANGELINAN MARIANNE	capacity charges. Return to OTC applicant 12/16/13.

/IOUS 2013-2015 PERMITS: B OCCUPANCY PRECEDENT

Rep	port Date:			1/7/20	21 5:22:1	I1 PM		Permit Details Report
Ann	lication Nu	mber		20180	2070675	Ъ		
	m Number	0.2022/02/28		8	2010010	J		
Add	iress(es):			4058	/009 /0	555	19TH	ST
Des	scription:			WOR	KSHOP S	PACES	v/ doors/partitions, A	HAL SPACE TO PROVIDE SMALL ENCLOSED REMODELED BREAK AREA, and conversion of urinals t AHER ORD 155-13 NA
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	Action	Date		Stage				Comments
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Step	p Station	Arrive	Start	In Hold	Out Hold		Checked By	Hold Description
1	INTAKE	2/7/18	2/7/18			2/7/18	SHAWL HAREGGEWAIN	
2	CP-ZOC	2/20/18	2/20/18			2/20/18		LINDA HOAGLAND
3	CP-ZOC	3/30/18	3/30/18			3/30/18	OROPEZA EDGAR	n/a, not a change of use
						6		Openings shown between 2nd and 3rd Floor, draft

1	INTAKE	2/7/18	2/7/18	2/7/18	HAREGGEWAIN	
2	CP-ZOC	2/20/18	2/20/18	2/20/18		LINDA HOAGLAND
3	CP-ZOC	3/30/18	3/30/18	3/30/18	OROPEZA EDGAR	n/a, not a change of use
4	SFFD	3/20/18	3/20/18	3/20/18	CHEUNG JANICE	Openings shown between 2nd and 3rd Floor, draft curtains for sprinkler protection? Provide site plan for orientation, show and label street names, front entrance
5	BLDG	2/23/18	2/23/18	2/23/18	HUANG VIVIAN	comments noted on dwg
6	BLDG	3/20/18	3/20/18	3/20/18	WALLS MARK	
7	INTAKE	3/20/18	3/20/18	3/20/18	BUFKA SUSAN	NEW PLANS SET BACK TO PLANNING
8	SFFD	3/28/18	3/28/18	3/28/18	CHEUNG JANICE	ОТС
			1		n.	OTC - Capacity Charge not applicable. Not enough

2018 PERMIT: F-1	()()()

Welcome to our Permit / Complaint Tracking System!

Stage

399163

4158822170

GARY J. FILIZETTI

DEVCON CONSTRUCTION

Permit Details Report

2

Report Date:	1/7/2021 5:25:28 PM		
Application Number:	201503100371		
Form Number:	8		
Address(es):	4058 / 009 / 0 555	19TH	ST
Description:	NON-RATED, NON STRU DOORS.	JCTURAL, ALUMINUM FI	RAMED, TEMPERED, GLASS PARTITIONS AN
Cost:	\$31,465.00		
Occupancy Code:	B,S-1		
Building Use:	10 - OFFICE		

Comment

Disposition / Stage	
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ISSUED		
COMPLETE	2558089 Final Inspection/Approved	
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Contact Details:

Contractor Details: License Number: Name: Company Name:

Address: Phone:

Addenda Details:

Step	Station	Arrive	Start	In Hold Out I	fold Finish	Checked By	Hold Description
1	INTAKE	3/10/15	3/10/15		3/10/15	SHAWL HAREGGEWAIN	
2	BLDG	3/10/15	3/10/15	1	3/10/15	LIN EMILY	Approvd. OTC
2	PAD-STR	3/13/15	3/13/15		3/13/15	PANG DAVID	
3	MECH	3/10/15	3/10/15	0	3/10/15	NAGATA TIMOTHY	OTC Approval
4	SFFD	3/13/15	3/13/15	1	3/13/15	FIELDS MELISSA	
5	CPB	3/13/15	3/13/15		3/13/15	SHAWL HAREGGEWAIN	N/A FOR SFFD

690 GIBRALTAR DR * MILPITAS CA 95035

REVISION	
OWNER	
MARTIN C. LEVIN INVESTMENT CO. LLC	
P.O. Box 148 Burlingame, CA 94011 martinclevinllc@yahoo.com	
ARCHITECT STANLEY SAITOWITZ	
NATOMA ARCHITECTS Inc. 1022 Natoma Street, No. 4 San Francisco, CA 94103	
415-626-8977 ulim@saitowitz.com	
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* No. C 32704	
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		REVISION
		OWNER
		MARTIN C. LEVIN INVESTMENT CO. LLC P.O. Box 148
		Burlingame, CA 94011 martinclevinllc@yahoo.com
		ARCHITECT
		STANLEY SAITOWITZ NATOMA ARCHITECTS Inc. 1022 Natoma Street, No. 4
3'		San Francisco, CA 94103 415-626-8977 ulim@saitowitz.com
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BASEMENT PLAN - PROPOSED (NO CHANGE)

(11)		REVISION
	_	
		OWNER
	1	MARTIN C. LEVIN INVESTMENT CO. LLC
		P.O. Box 148 Burlingame, CA 94011 martinclevinllc@yahoo.com
		ARCHITECT
		STANLEY SAITOWITZ NATOMA ARCHITECTS Inc.
	I	1022 Natoma Street, No. 4 San Francisco, CA 94103 415-626-8977 ulim@saitowitz.com
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REVISION	
OWNER	
MARTIN C. LEVIN INVESTMENT CO. LLC P.O. Box 148 Burlingame, CA 94011 martinclevinllc@yahoo.c	om
ARCHITECT	
STANLEY SAITOWITZ NATOMA ARCHITECTS 1022 Natoma Street, No. San Francisco, CA 9410 415-626-8977 ulim@saitowitz.com	4
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555 19TH STREET, SAN FRANCISCO CHANGE OF USE & OCCUPANCY	
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A2.	







WEST ELEVATION - EXISTING AND PROPOSED



REVISION	
OWNER	
MARTIN C. LEVIN	
INVESTMENT CO. LLC P.O. Box 148	
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SOUTH ELEVATION - EXISTING AND PROPOSED

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		OWNER
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HISTORIC BUILDING MAINTENANCE PLAN



555 19th Street

San Francisco, California

March 12, 2021

Prepared by



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I. Introduction



Figure 1. Aerial photograph showing the location of 555 19th Street. Source: Google Maps

VerPlanck Historic Preservation Consulting prepared this Historic Building Maintenance Plan (HBMP) for the Martin C. Levin Investment Company, owner of a three-story-over-basement, heavy timber-frame and brick commercial building at 555 19th Street in the Central Waterfront district (Figure 1). The property, also known as the Alberta Candy Factory, contains a building that occupies approximately 80 percent of the property, with a surface gravel lot occupying the remainder. Designed by the German immigrant architect Emil A. Neumarkel and built in 1919, the commercial loft building was commissioned by M. Levin & Sons, a scrap metal and salvage business founded by a Russian immigrant named Max C. Levin. In 1925, the building was converted into a candy factory, a use it retained until 1969. Since 1970, the building has been used for a variety of industrial and commercial purposes. In 2000, the San Francisco Planning Department surveyed the property as part of its Central Waterfront Survey and 555 19th Street was determined to be a contributor to the California Register-eligible Third Street Industrial Historic District. As such, the property is a "historical resource" per the California Environmental Quality Act (CEQA). The property owner is applying to take advantage of Section 803.9 of the San Francisco Planning Code, which allows a variety of uses, "by right," within the building, which is within an Urban Mixed-Use (UMU) zoning district. In order to do this, the Planning Department requires the applicant to prepare an HBMP to guide future work to the building.



II. Methods

Christopher VerPlanck, the author of this report, has 24 years of experience evaluating historic buildings in San Francisco. VerPlanck visited 555 19th Street on June 3, 2020, and again on February 25, 2021, to photograph and survey the building, including the four visible exterior façades, the interior, and the roof. VerPlanck recorded fieldnotes documenting the building's construction materials and methods, the physical condition of the building, as well as any apparent alterations. VerPlanck obtained copies of all building and alteration permits on file for the building at the San Francisco Department of Building Inspection. The maintenance recommendations contained in this HBMP are derived from the National Park Service's *Preservation Briefs*, including *Preservation Brief Nos. 1, 2, 4, 6, 15, 18,* and *41*. The property description and history are summarized from the National Register of Historic Places nomination prepared by the author for 555 19th Street in 2020.



III. Concise Property Description

Figure 2. 555 19th Street; view toward southeast from intersection of 3rd and 19th streets.



555 19th Street is a three-story-over-basement, heavy timber frame, brick commercial loft building with three publicly visible elevations facing 3rd, 19th, and Illinois streets **(Figures 2-3)**. Designed in the American Commercial style, the building's principal character-defining characteristics include its simple box-like massing with extruded corner pavilions and stepped parapets, its modest amount of corbelled brick ornament, and its symmetrical fenestration pattern consisted of divided-lite metal windows housed within deeply punched openings that increase in size from the basement to the third-floor level. Completed in 1919, the building makes use of what was already an anachronistic structural system consisting of load-bearing brick walls with internal timber framing that supports the internal floors and ceiling only. The only ornament consists of the corbelled trim along the parapets. The shallow-pitch gable roof, which is concealed behind high brick parapets, is gently sloped to drain rainwater into the gutters and down-spouts. Although not original, the divided-lite, anodized aluminum windows are very similar to the original steel industrial window sashes. Most of the doors are not original, although there is one original freight door on the 19th Street façade. 555 19th Street is in very good condition with no signs of deterioration or deferred maintenance.



Figure 3. 555 19th Street; view toward southwest from intersection of 19th and Illinois streets.



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IV. Concise Property History

What is now 555 19th Street was historically submerged beneath the shallow waters of San Francisco Bay. The property was filled in the late nineteenth century by the Southern Pacific Railroad as the Central Waterfront was evolving into one of San Francisco's foremost zones of heavy industry. On this land-fill went up dozens of brick, corrugated metal, and concrete warehouses, factories, and machine shops interconnected by a network of street-level rail spurs.

In September 1917, Max Levin, a Russian immigrant scrap metal dealer, purchased the northernmost part of Block 4058 (Potrero Nuevo Block 411) – a lot measuring 200' x 200' – from the Pacific Improvement Company, the real estate arm of the Southern Pacific Railroad. According to the 1915 Sanborn Map, when Mr. Levin bought it, the parcel contained a nickelodeon, an asphalt batching plan, a saloon, and several mixed-use buildings with commercial spaces on the ground floor and flats above (Figure 4).



Figure 4. 1913 Sanborn Map showing the future site of 555 19th Street in red. Note, the original lot was somewhat larger. Source: San Francisco Public Library

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In early 1918, Max Levin hired German immigrant architect Emil A. Neumarkel to design a new warehouse and office building for his business, M. Levin & Sons, a scrap/salvage business that had previously been located in the South of Market area. On August 6, 1918, Levin submitted an application to the Department of Public Works to build a two-story-over-basement, heavy-timber, brick loft building measuring 60' x 163' in plan and 35' high.¹ Neumarkel's design included a double-height second-floor level that would allow the \$22,000 building to be expanded to three floors in the future without having to enlarge its exterior envelope. The easternmost section of the lot was left free of construction to accommodate a dedicated rail siding accessing the Southern Pacific's industrial spur on Illinois Street.

Completed in 1919, M. Levin & Sons moved into the building that same year. The company used the building for offices and storing more valuable scrap metal (less-valuable materials were stored in an open-air yard south of the building). The company dealt in all sorts of metal scrap, including junked machinery, rails, boilers, pipes, shafts, vessels, and corrugated iron sheds. In addition to metal scrap, the company dealt in rubber, rags, rope, bones, and other "condemned materials."² The building also contained a small retail store where the Levins sold World War I surplus, including tents, blankets, vehicle covers, and rucksacks.

In 1924, the Levin family built a smaller brick building next-door at 2225 3rd Street, moved their offices into the new building, and leased 555 19th Street (originally 2201 3rd Street) to the Alberta Candy Company, a subsidiary of the Bunte Candy Company of Chicago. Alberta Candy Company applied for a pair of permits in November and December 1925 to convert the building into a candy factory. Work included demolishing the interior office and retail store partitions, painting the interior, and building a freestand-ing boiler house and chimney at the southeast corner of the property (no longer extant).³ Alberta Candy Factory remained in the building for nine years, moving out in 1933.⁴

Between 1934 and 1969, two other candy companies leased 555 19th Street, including the Sierra Candy Company (1934 to 1949) and the White Candy Company (1953 to 1969).⁵ These companies made only minor changes to the building – mostly reconfiguring interior partitions. All building permit applications for the property are attached in **Appendix Item A** of this report.

555 19th Street appears in a 1940 photograph that was looking north along 3rd Street midway between 19th and 20th streets. Although the building is not central to the image, it looks very much like it does today, albeit with signage painted directly on the brick exterior (**Figure 5**). Recorded a decade later, the 1950 Sanborn Maps depict a three-story, heavy timber-frame brick factory building with a pair of one-story additions at the southeast corner (no longer extant), including the 1925 boiler house and an uni-dentified shed (**Figure 6**). The Sanborn Maps also indicate that the building originally had a monitor roof.



¹ San Francisco Department of Building Inspection, Permits and plans on file for 555 19th Street.

² Advertisement in the 1920 San Francisco City Directory.

³ San Francisco Department of Building Inspection, Permits and plans on file for 555 19th Street.

⁴ San Francisco City Directories.

⁵ San Francisco City Directories.

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Figure 5. View of 2200 Block of 3rd Street with 555 19th Street indicated by red arrow. Source: OpenSFHistory wnp14.10711



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Between 1970 and 1982, 555 19th Street was leased by two different businesses, including S. Rieke & Sons, a glassware manufacturer (1970 to 1972); and Kosman Lighting Equipment Company (1973 to 1982), a wholesale dealer of industrial lighting equipment.⁶ In 1982, a fire swept through the building, destroying the roof, windows, and heavily damaging the framing at the third-floor level.

After the fire, the Levin family hired the engineering firm of Toft & DeNevers to design and oversee the \$50,000 repairs, which included rebuilding the roof and third-floor level, and adding vertical bracing to the insides of portions of the brick walls. The work was completed in April 1984.⁷ Under a separate permit, the family applied to replace the fire-damaged windows with custom divided-lite aluminum windows similar to the original steel windows.⁸ Although not mentioned on the permits, the 1925 boiler room addition was apparently demolished around this time.

In the spring of 1984, the Martin C. Levin Investment Company began upgrading the interior to accommodate a variety of commercial uses. The work, which took place over the course of 1984 and 1985, included installation of an elevator; construction of pairs of toilet rooms on the first, second, and thirdfloor levels; construction of two enclosed emergency egress stairs; new electrical, plumbing, and lighting; refinished wood floors; and removal of paint from the exterior and interior surfaces. Architect Michael Sands designed the remodel and Lowenthal Construction completed the work.⁹

In 1987, 555 19th Street was placed back into use. Tenants during the 1980s and 1990s included a television production studio, a computer graphics company called Chromaset, and a financial magazine publisher called Research Holdings, Inc. In the late 1990s, a "dotcom" company called Gazoontite occupied the building. These companies completed minor tenant improvement projects, such as moving partitions and installing mechanical equipment and utilities. Meanwhile, the Martin C. Investment Company completed multiple life-safety upgrades, including bracing the parapets in 1993.¹⁰

The implosion of the dotcom boom in the early 2000s led to widespread commercial vacancies throughout San Francisco, including 555 19th Street, which emptied out in 2001-02. The owner took advantage of this time to complete a full seismic retrofit and several disabled access upgrades. The seismic work consisted of constructing a pair of moment frames inside the building and bracing the north wall. The accessibility work included creating a new grade-level entrance at 555 19th Street, installing self-closing doors, and completing assorted upgrades to the toilet rooms. Stanley Saitowitz's Natoma Architects designed the remodel. Additional work completed in 2010-12 included the installation of a new fire sprinkler system, demolition of the rear shed addition, installation of a new HVAC system, and re-roofing.¹¹ In 2013, the Martin C. Levin Investment Company completed another \$250,000 of interior upgrades, also designed by Stanley Saitowitz, including various improvements to the toilet rooms, break rooms, and lobbies. This work was completed in 2014 for the building's last tenant, Beats Electronics and Music. In recent years, the current tenant, ThirdLove, has made several minor changes to the interior.

⁸ San Francisco Department of Building Inspection, Plans and building permit applications on file for 555 19th Street.

¹¹ San Francisco Department of Building Inspection, Plans and building permit applications on file for 555 19th Street.



⁶ San Francisco City Directories.

⁷ San Francisco Department of Building Inspection, Plans and building permit applications on file for 555 19th Street.

⁹ San Francisco Department of Building Inspection, Plans and building permit applications on file for 555 19th Street.

¹⁰ San Francisco Department of Building Inspection, Plans and building permit applications on file for 555 19th Street.

V. Maintenance Plan

Apart from several portions of the interior reconstructed after the 1982 fire, 555 19th Street has undergone few permanent physical alterations since it was built 102 years ago. As a historical resource eligible for listing in the California Register of Historical Resources (California Register), the Planning Department maintains that all future modifications to the building should comply with the Secretary of the Interior's Standards (Standards).¹² Initially devised to review certified federal rehabilitation tax projects, the Secretary of the Interior provides four sets of standards to guide the treatment of historic properties: Preservation, Rehabilitation, Restoration, and Reconstruction. The four approaches are summarized below:

- **Preservation:** Preservation is defined as "the act or process of applying measures necessary to sustain the existing form, integrity, and materials of an historic property." This approach focuses on ongoing repair of existing materials and features rather than extensive replacement and/or new construction.¹³
- **Rehabilitation:** Rehabilitation is defined as "the act or process of making possible a compatible use for a historic property through repair, alterations, and additions while preserving those portions or features which convey its historical, cultural, or architectural values."¹⁴
- Restoration: Restoration is defined as "the act of accurately depicting the form, features, and character of a property as it appeared at a particular period of time by means of the removal of features from other periods in its history and reconstruction of missing features from the restoration period.¹⁵
- **Reconstruction:** Reconstruction is defined as "the act or process of depicting, by means of new construction, the form, features, and detailing of a non-surviving site, landscape, building, structure, or object for the purpose of replicating its appearance at a specific period of time and in its historic location."¹⁶

Typically, one set of Standards is chosen given the significance of the property, its integrity, and the types of uses envisioned for the building. The Standards have been adopted by hundreds of cities and other governmental bodies across the country to review changes to historic properties. In San Francisco, the Rehabilitation Standards are most commonly used for light industrial and commercial properties, because these types of buildings tend to undergo periodic changes in use in response to the needs of new tenants and/or industries. As an industrial and commercial property, is our recommendation that the Rehabilitation Standards be used when reviewing any changes to 555 19th Street. There are 10 individual Rehabilitation Standards that are used to evaluate the effects of a development proposal:

¹⁴ Ibid., 61.



¹² The author of this maintenance plan has also submitted a National Register nomination to the Office of Historic Preservation for 555 19th Street.

¹³ National Park Service, Secretary of the Interior, *The Secretary of the Interior's Standards for the Treatment of Historic Properties* (Washington, D.C.: National Park Service, rev. ed. 1995), 17.

¹⁵ lbid., 117. ¹⁶ lbid., 1<u>65.</u>

Rehabilitation Standard 1: A property will be used as it was historically or be given a new use that requires minimal change to its distinctive materials, features, spaces and spatial relationships.

Rehabilitation Standard 2: The historic character of a property will be retained and preserved. The removal of distinctive materials or alteration of features, spaces and spatial relationships that characterize the property will be avoided.

Rehabilitation Standard 3: Each property will be recognized as a physical record of its time, place and use. Changes that create a false sense of historical development, such as adding conjectural features or elements from other historic properties, will not be undertaken.

Rehabilitation Standard 4: Changes to a property that have acquired historic significance in their own right will be retained and preserved.

Rehabilitation Standard 5: Distinctive materials, features, finishes and construction techniques or examples of craftsmanship that characterize a property will be preserved.

Rehabilitation Standard 6: Deteriorated historic features will be repaired rather than replaced. Where the severity of deterioration requires replacement of a distinctive feature, the new feature will match the old in design, color, texture, and, where possible, materials. Replacement of missing features will be substantiated by documentary and physical evidence.

Rehabilitation Standard 7: Chemical or physical treatments, if appropriate, will be undertaken using the gentlest means possible. Treatments that cause damage to historic materials will not be used.

Rehabilitation Standard 8: Archeological resources will be protected and preserved in place. If such resources must be disturbed, mitigation measures will be undertaken.

Rehabilitation Standard 9: New additions, exterior alterations, or related new construction will not destroy historic materials, features, and spatial relationships that characterize the property. The new work shall be differentiated from the old and will be compatible with the historic materials, features, size, scale and proportion, and massing to protect the integrity of the property and its environment.

Rehabilitation Standard 10: New additions and adjacent or related new construction will be undertaken in such a manner that, if removed in the future, the essential form and integrity of the historic property and its environment would be unimpaired.

Character-defining Features

The primary character-defining features of the exterior of the Alberta Candy Factory include its threestory height and rectangular massing, as well as its regular fenestration pattern of punched window openings that increase in size from the basement to the third-floor level. Other exterior characterdefining features include the narrow reveals creating the corner pavilions, the corbeled frieze encircling the parapet, as well as the parapet itself, which consists of alternating segmental-arch and pedimented sections. Although the existing window sashes are not original and therefore not character-defining, the window openings are entirely original, with jack arches in the corner bays and segmental arches in the interior bays. The remaining historic freight door on 19th Street is also character-defining. Within the interior, the exposed brick walls and heavy timber framing are the primary character-defining features, as well as the expansive interior volume apparent throughout much of the interior. The following sections analyze each major feature of 555 19th Street, working from the outside to the inside, and from the basement to the third-floor level.



EXTERIOR

Building Feature: Masonry Walls



Figure 7. North (left) and west (right) façades.

Existing Conditions: All four exterior façades of 555 19th Street are visible from public rights-of-way, including the north, west, and east façades, which face 19th, 3rd, and Illinois streets, respectively. The south (property line) façade, which faces a narrow courtyard between the building and the adjoining apartment complex, is partially visible from both 3rd and Illinois streets (Figures 7-9). All four elevations are made of red brick laid in American bond with a lightcolored, cementitious mortar. The only decorative features include the extruded corner pavilions that extend outward about two inches (Figure 10), narrow brick lug sills beneath each window, segmental and jack-arched window openings, and a corbeled brick parapet composed - in sequence - of a rowlock course, a soldier course, and a rowlock coping (Figure 11). The segmental-arch windows are located within the interior bays and the jack-arch windows are located in the corner pavilions. In addition to the parapet detailing described above, there is a simple frieze on the two long walls composed of small square panels alternating with wider sections of plain brickwork.



Figure 8. South façade from 3rd Street.





Figure 9. North (right) and east (left) façades.



Figure 10. Corner pavilion facing 19th Street.



Figure 11. Detail of parapet and window opening facing Illinois Street.



The exterior walls of 555 19th Street are in quite good condition for a building that is over a century old. In the mid-1980s, the exterior was cleaned of paint and other residue using an abrasive technique – most likely sandblasting – because the bricks have a slightly abraded appearance **(Figure 12)**. A common paint removal technique widely used in the 1970s and 1980s, sandblasting is no longer an approved method for use with brick because it can remove the hard outer skin of the fired brick, exposing the softer inside to water absorption and spalling. Fortunately, the brick used to build 555 19th Street is of high quality and, although somewhat abraded, the brick does not show any signs of spalling or disintegration which can happen years after sandblasting. Given that the exterior of the building was sandblasted nearly 40 years ago, it is holding up extremely well. The only other issues observed include the mortar, which is missing and cracked in localized areas, and unpatched openings where anchors for fire escapes and roof framing for additions once joined the building. There is also some graffiti residue on the parapet facing 3rd Street. Otherwise, the brick walls are clean and very intact.



Figure 12. detail of brick facing 19th Street. Note old paint residue and cracked and missing mortar.

There are some concrete elements on the exterior of 555 19th Street as well, including the foundation, which is exposed along 19th Street, as well as the loading docks facing Illinois Street **(Figures 13-14)**. The concrete elements are painted dark gray and the original board form marks are still visible. The only issues include some minor surface cracks, divots from trucks backing into the building, and minor penetrations made for utilities. None of these imperfections affect the building envelope's performance, and they provide an interesting historical patina common to industrial buildings of this era.





Figure 13. Detail of concrete foundation and basement window facing 19th Street.



Figure 14. Concrete foundation and loading docks facing Illinois Street.



Maintenance Plan: Unfortunately, it is impossible to restore brick that has been damaged by sandblasting. Fortunately, the brick of 555 19th Street is still in very good condition, suggesting that the brick used to construct the building was well-fired and of good-quality. If water ever starts to become absorbed into the brick in the future, it may be necessary to apply a breathable water repellent, such as Silane or Siloxane, but for now this does not appear to be a problem. Broken or missing bricks should be replaced where necessary, taking care to match the existing brick color as closely as possible. Fortunately, no missing bricks were observed during the field work conducted for this report. As for minor imperfections, as long as water is not getting into the building, they only add patina to the building.

Repointing is the most important task that needs to occur on limited areas of the exterior. The building owner is clearly aware of this issue, as several areas have already been repointed in recent years, including the southeast corner and along the parapet on the south side of the building. However, the mortar used in these areas is a brighter white than the original mortar, causing it to stand out. For future repointing projects, a qualified architectural conservator should be hired to complete a chemical analysis of the original mortar composition so that it can be replicated to match.

Building Feature: Windows

Existing Conditions: The exterior of 555 19th Street has approximately 80 windows, depending on how you count them (Figure 15).¹⁷ The majority contain divided-lite, anodized-aluminum, custom-fabricated window sashes installed in 1983-84. The original steel industrial windows were destroyed in the 1982 fire, requiring total replacement. As mentioned, most windows either have jack-arch or segmental-arch headers. Typically, the windows in the corner bays have jack-arch headers (Figure 16), whereas those in the interior bays have segmental-arch headers (Figure 17). The windows with jack arches are rectangular, whereas the windows with segmental arches have an arched header profile. The windows increase in size from the basement to the third-floor level, with the windows at the third-floor level being three times larger than their counterparts at the basement and first-floor levels. The purpose of this design feature is both aesthetic and functional. In addition to making the otherwise long and low building look taller, the higher windows at the third-floor level bathe the interior in natural light. Most of the openings at the basement level are infilled with original metal grates perforated by tiny holes to allow some natural light into the basement (Figure 18). Nearly all window sashes at the first-floor level have three lites whereas those at the second and third-floor levels have six. Several of the windows at the first-floor levels infill what were once pedestrian doors or freight doors; these openings contain contemporary aluminum storefronts installed Ca. 2010. Several of the first-floor windows along 3rd and the west half of 19th Street are protected behind non-historic steel security bars (Figure 19). In contrast, nearly all of the windows at the second and third-floor levels contain 1980s-era aluminum windows with operable pivot sashes at the center. Although not made of the original steel, these replacement windows were carefully designed and fabricated to resemble the originals.¹⁸

¹⁸ Conversation with the Building Manager, Mr. Bruce Powelson, June 12, 2020.



¹⁷ Several windows at the first-floor level are better-categorized as storefronts and there are several openings at the basement level that are better-described as vents.



Figure 15. East (Illinois Street) façade showing the variety of window types present.



Figure 16. Typical jack-arch window.

Figure 17. Typical segmental-arch window.





Figure 18. Typical basement window.

Figure 19. Typical first-floor window.

Maintenance Plan: In general, the windows of 555 19th Street are in good condition. The anodized coating appears to be holding up for the most part, although some surface oxidation and ultraviolet damage are present. In addition, all of the glass is intact and the weatherproofing appears to be holding up. That said, even with regular maintenance, the maximum lifespan of anodized aluminum windows is about 40 years, and having been installed in 1983-84, these windows are nearing the end of their usable life. When the windows are replaced, careful thought should be given to finding an appropriate window type. Although the existing divided-lite windows closely resemble the originals, they do not exactly match a traditional divided-lite, steel industrial window. In addition to the muntin profiles being slimmer and less substantial, the lites are slightly larger than a traditional steel window. As far as replacements go, there are manufacturers that still fabricate steel industrial windows that would match what was originally used at 555 19th Street. In addition, there are powder-coated aluminum windows that look very similar and yet perform better in regard to both sound insulation and energy conservation than a traditional single-pane steel window. As long as the aluminum counterpart matches the originals in terms of lite pattern, rail and stile profile, and muntin thickness, it would be fine to install aluminum windows again.

Building Feature: Doors

Existing Conditions: 555 19th Street has three pedestrian entrances – one each on 19th, 3rd, and Illinois streets. None are historic. The entrances facing 3rd and Illinois streets both contain hollow-core metal doors installed in 1983-84 when the building was repaired after the fire. The 3rd Street entrance at 2201 3rd Street, which was originally the main entrance to the building, is deeply recessed within the original jack-arched opening (**Figure 20**). In contrast, the entrance at 601 Illinois Street is contained within what was originally one of the four loading docks at the back of the building (**Figure 21**). What is now the main entrance at 555 19th Street was added in 2003 as an accessible entrance within an existing freight door. This entrance consists of an all-glass door flanked by a pair of matching all-glass storefronts (**Figure 22**). The 19th Street façade contains three other openings that were originally freight doors. Two now contain contemporary aluminum-frame storefronts installed Ca. 2010 and one is entirely original (**Figure 23**). There are no entrances on the south, property line-facing, façade.





Figure 20. Entrance at 2201 3rd Street.





Figure 22. Main entrance at 555 19th Street.

Figure 23. Freight door on 19th Street.

Maintenance Plan: The doors of 555 19th Street all appear to be in very good condition. All work as they are supposed to and have no apparent corrosion, denting, delaminated paint, or other issues. The metal doors are occasionally marked with graffiti, but the building owner has it painted over right away. Our only recommendation regarding the doors involves the 3rd Street entrance. Although now just an emergency exit, historically it was the main entrance to the building facing busy 3rd Street. If or when the existing hollow-core metal door needs replacement, we recommend that the replacement door be more compatible with the building's original design. Although the appearance of the original door is not known, it was probably a two-panel wood door with a single lite in the upper panel. In regard to the other doors, the main entrance is very modern but it is unobtrusive and elegant. The rear entrance on



Illinois Street is part of what was originally a loading dock, so its utilitarian design is appropriate. Regarding the original freight door; it is in good condition and should be retained and preserved as it is.

Building Feature: Roof Membrane and Parapets

Existing Conditions: 555 19th Street has a shallow-pitch gable roof built in 1983-84 (Figure 24). The original roof was a shallow-pitched gable roof with a monitor at the ridge that was destroyed in 1982. According to the permit record, the roof surface was last replaced in 2012, when it was taken down to the plywood substrate and covered in a "Class A" rolled-membrane, modified bitumen roof. The roof is not visible from any surrounding streets because it is concealed behind the high brick parapets. The tops of the parapets are covered in the same materials as the roof, with the roofing extending up the inside of the parapets and onto the coping in one continuous sheet to prevent water from entering the building at the intersection of the walls and the roof plane. The parapets are tied back to the roof by metal flanges installed in 1993 (Figure 25). The roof is punctuated by 13 roof-mounted, fiberglass skylights installed in 1983-84, as well as eight HVAC units, several vents, and a roof hatch (Figure 26). The roof appears to be in good condition, with no evidence of failing materials or water intrusion. In addition, the skylights are well-maintained without any cracks or signs of water intrusion.



Figure 24. Roof of 555 19th Street. Source: Google Maps

Maintenance Plan: As mentioned, the roof of 555 19th Street was last replaced in 2012, making it nine years old. The average lifespan of a rolled-membrane, modified bitumen roof is about 20 years, leaving plenty of life in the existing roof. Since the roof itself is not historic, and the existing roof is not visible from any public right-of-way, we recommend keeping the existing roof and replacing it in-kind when it begins to fail. The property owner has a roofing company inspect the roof annually to check its condition and to make sure that the waterproofing is intact and that rainwater and condensation are properly draining into the gutters and downspouts. The building owner has also had the parapet and the braces checked in recent years to ensure that the parapet is sound. We recommend that the owner keep following the same protocols into the future.





Figure 25. Detail of roof showing parapet braces and rolled roofing materials; view toward west.



Figure 26. Detail of roof showing roof-mounted vents and mechanical equipment; view toward northwest.



INTERIOR: General Description

Including the basement, 555 19th Street contains four floor levels within the 33,998-square-foot building. Originally built as a combined office building and warehouse, the building is currently occupied by a women's undergarments manufacturing company which uses the first, second, and third-floor levels for a combination of uses, including design, mock-up fabrication, and administrative functions. The basement is used for storage as well as for mechanical equipment used in the operation and maintenance of the building. It is not used by the tenant. All levels are connected via an elevator and three stairs. While historic fabric is abundant in the lower parts of the building, the third-floor level features mainly newer materials installed after a major fire swept the interior in 1982. Nevertheless, the original brick walls and heavy timber framing survive throughout the building, as well as some original flooring. The remaining interior partitions mostly post-date the 1982 fire, including the stairs, toilet rooms, and work room enclosures. However, the interior still retains the feel of an early twentieth-century commercial loft building, by virtue of the exposed brick walls and heavy timber framing, the relative lack of interior partitions, and the open-air light court at the center of the building. Because the character-defining materials are essentially the same for each floor level, to avoid repetition each floor level is described first, with each distinctive building feature discussed afterward. Maintenance recommendations come at the end.

Basement

Existing Conditions: The basement of 555 19th Street occupies the entire footprint of the building. It is utilized for storage and mechanical equipment used in the operation and the maintenance of the building. Its plan consists of a central double-loaded corridor lined on either side by large storage rooms. Toward the center of the basement is the utility core, consisting of the elevator room, the elevator itself, and a mechanical room. Stairs at either end of the corridor lead up to the first-floor level. The basement has a concrete floor, concrete perimeter foundation walls, some exposed heavy timber framing, and gypsum board partition walls. It is a utilitarian space without any significant features. The building's seismic moment frames – installed 2003 – are visible at either end of the building. There are also some exposed mechanical/HVAC ducts, water pipes, and electrical conduit (Figures 27-28).



Figure 27. Basement storage room; note moment frame.



Figure 28. Basement storage room.



General Recommendations: The basement of 555 19th Street is in good condition but it does not contain any significant spaces or materials worthy of preservation. We recommend that it continue to be used for storage and other back-of-house purposes so the upper floors can remain largely unencumbered by partitions and mechanical equipment.

First-floor Level

The first-floor level of 555 19th Street is arguably the best-preserved interior floor, due in large part to the fact that it escaped the worst of the damage in the 1982 fire. Indeed, this floor retains its original brick perimeter walls and heavy timber framing **(Figures 29-32)**. Although the wood flooring is not original, it is characteristic of historical conditions. The first-floor level is also largely free of interior partitions apart from the two emergency egress stairs at either end of the floorplate, the toilet rooms/utility core at the center, and the main lobby facing 19th Street.



Figure 29. First-floor level, looking northwest toward 3rd Street with emergency egress stair in background.





Figure 30. First-floor level, looking west toward 3rd Street.



Figure 31. First-floor level; northeast corner.

Figure 32. First-floor level; lobby, looking east.



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General Recommendations: The first-floor level of 555 19th Street is in good condition with no visible deterioration or decay. We recommend that the building owner continue maintaining the interior of the building has they have done over the last few decades. Furthermore, we recommend that the brick perimeter walls, heavy timber framing, flooring, and floor joists for the floor above remain exposed as much as possible in order to convey the historically industrial uses of the building. We recommend that neither the brick nor the wood be painted over and that new partitions be installed so that they are easily removable without damaging historic materials or features.

Second-floor Level

The second-floor level of 555 19th Street is very similar to the first-floor level (Figures 33-36). Indeed, the only differences include the following: the second-floor level has carpeted flooring; it has several enclosed work rooms arrayed along the perimeter of the building; and it has two large openings in the ceiling, opening up views to the third-floor level and the underside of the roof. Throughout most of the space, the historic brick walls and heavy timber framing remain exposed to view. However, the framing for the third-floor level uses contemporary materials such as plywood and smaller dimensioned lumber joists because the third-floor was reconstructed in 1983-84 after the fire.



Figure 33. Second-floor level; view from the center of the building toward the northeast.



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Figure 34. Second-floor level; view toward east along the south side of building.



Figure 35. Second-floor level; view toward west from the center of building.



Figure 36. View of exposed brick walls and moment frame at second-floor level.



General Recommendations: The second-floor level of 555 19th Street is in good condition with no visible deterioration or decay. Similar to the rest of the building, we recommend that the building owner continue maintaining it as has they have along with the rest of the building. Furthermore, we recommend that the brick perimeter walls, heavy timber framing, flooring, and floor joists for the floor above remain exposed. We recommend that neither the brick nor the wood be painted and that new partitions be installed so that they are easily removable without damaging historic materials or features.

Third-floor Level

Unlike the other floor levels, the third-floor level of 555 19th Street is largely new construction, as it was rebuilt following the 1982 fire **(Figures 37-40)**. In addition, although the heavy timber framing is fully exposed at the third-floor level, smaller sections of the brick perimeter walls are exposed – in particular between the windows on the south side. As mentioned, apart from the heavy timber posts and beams, the framing for the third-floor level uses contemporary materials such as plywood and smaller dimensioned lumber joists. The roof structure also dates to the 1980s, and it is concealed behind a gently curved gypsum board ceiling punctuated by 13 skylights. The third-floor level has a similar plan to the first-floor level, with very few interior partitions. However, it has a smaller footprint due to the fact that a pair of openings are cut into the floor to bring natural light into the heart of the building.



Figure 37. Third-floor level; southwest corner of the building.



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Figure 38. Third-floor level; view toward east from the center of the building.



Figure 39. Northwest corner of third-floor level.

Figure 40. Southeast corner of third-floor level.



General Recommendations: The third-floor level of 555 19th Street is in good condition with no visible deterioration or decay. Similar to the rest of the building, we recommend that the building owner continue maintaining it as has they have along with the rest of the building. Furthermore, we recommend that the brick perimeter walls, heavy timber framing, flooring, and floor joists for the floor above remain exposed. We recommend that neither the brick nor the wood be painted and that new partitions be installed so that they are easily removable without damaging historic materials or features.

Building Feature: Brick Walls

Existing Conditions: As mentioned previously, the perimeter walls of 555 19th Street are red brick laid in American bond, and they remain visible throughout the interior of the building from the first to the third-floor levels (Figures 41-42). Similar to the exterior, the interior surface of the brick appears to have been sandblasted in the 1980s, although they remain in good condition. Indeed, the interior surfaces of the brick walls show no evidence of spalling, water intrusion from roof leaks, or moisture wicking up from the ground (rising damp). No efflorescence was observed anywhere within the interior. The only issues observed include a few areas where holes in the brick have been inexpertly patched with different-colored mortar, as well as residue from paint and stucco long since removed. There are also many other "imperfections," including remnants of old metal hardware and wood furring and nailers that remain embedded within the walls. But again, these "issues" pose no danger to the building, instead providing historical patina.



Figure 41. Exposed brick walls at the northwest corner of the first-floor level. Note the wood nailer between the windows.





Figure 42. Interior brick at the second-floor level above the lobby on 19th Street. Note the filled joist pockets.

Maintenance Plan: Essentially, there is nothing wrong with the brickwork within the interior of 555 19th Street. As stated previously, we recommend not obscuring the brick walls with permanent new construction or cluttering up the surface with exposed metal conduit, pipes, and other building systems. In regard to avoiding problems in the future, the roof and parapet flashing should be regularly inspected – as they have been – to make sure that moisture is not getting into the brick walls through the parapet. Similarly, gutters and downspouts should be inspected every year to clear out debris and to make sure that there are no breaks that could result in water getting into the walls. In addition, the lower part of the brick walls should be regularly inspected to make sure that moisture is not wicking upward from the surrounding soil. This phenomenon – called "rising damp" – is especially prevalent in areas with a high water table, such as the subject property. If repairs are necessary, the mortar should match the original mortar in regard to color and texture. Prior to any repairs, a qualified conservationist should be hired to chemically analyze the mortar to determine its composition. Similarly, if any brick repairs are necessary, the replacement brick should match the original as closely as possible in terms of size, color, and texture.



Building Feature: Heavy Timber Frame

Existing Conditions: As mentioned, 555 19th Street has a heavy timber frame – most of which appears to be original to the building **(Figures 43-44)**. In addition to the posts and beams, the joists and other framing elements of the first and second-floor levels appear to be original. They are made of old-growth Douglas fir and are substantial in size in order to support heavy dead and live loads. They are also designed to burn very slowly in a fire, allowing firefighting crews to extinguish the blaze before structural collapse occurs. Apart from the posts and beams, the framing for the third-floor level is not original, dating to the 1983-84 post-fire repairs. The heavy timber framing consists of 12 x 12 Douglas fir posts throughout the interior, as well as beams measuring 12 x 18 (basement), 10 x 16, (first floor), and 10 x 12 (second floor). Similar to the brickwork, the framing appears to have been lightly sandblasted in the early 1980s because the grain is slightly raised and there is some paint residue. In recent years, the posts and beams have been reinforced with steel connectors, T-straps, and other hardware to improve the building's resistance to earthquakes. In addition, there are two moment frames – one at either end of the building – that bolster the building's timber frame. This work was completed very sensitively although it remains visible. However, the heavy timber frame is in very good condition and it remains an attractive feature of the building's interior.



Figure 43. Heavy timber frame at first-floor level; view toward northwest.





Figure 44. Heavy timber framing on the third floor; view toward northwest. Note moment frame at right.

Maintenance Plan: Although the sandblasting has affected the appearance of the wood framing, it has not caused any long-term structural damage that would require repair. Otherwise, the framing is in perfect condition and requires no specific treatment. In terms of maintenance, the building owner has the roof regularly inspected and repaired if necessary, to ensure that no water gets into the building through the roof or the parapets. Furthermore, the building has already undergone a full seismic retro-fit. Our only recommendation is that the timber frame not be painted and that care is taken when installing metal conduit, pipes, and other building infrastructure, so the framing is not unnecessarily obscured.



VI. Conclusion

Designed by architect Emil A. Neumarkel, and built in 1918-19 for M. Levin & Sons as a warehouse, store, and office building, 555 19th Street was converted into a candy factory in 1925. Between 1925 and 1969, the building housed three different candy companies. During this time, the interior was periodically remodeled in response to changes in production methods, as well as changing health and safety requirements. During this time, two additions were added onto the building, including a boiler house/smokestack at the southeast corner in 1925, and a shed built over the property's dedicated rail siding at an unknown time. Between 1969 and 1982, 555 19th Street was leased by two different companies, including a glassware manufacturer (1970-72), and a wholesale lighting distributor (1973-82). A fire swept through the building in 1982, destroying the windows, the roof, and much of the third-floor level framing. The longtime building owner decided to repair the building. The scope of work included, among other things, bracing the brick walls, replacing the windows, and rebuilding the roof and the third-floor level. In the early 2000s, following the dotcom crash, the owners completed a full seismic retrofit, accessibility upgrades, roof replacement, and an interior remodel for commercial use by a single tenant. As a result, 555 19th Street is in exceptionally good condition, requiring little attention beyond continued routine maintenance. Our only substantial recommendations are that more compatible metal windows be used when the existing aluminum window sashes require replacement and that a chemical analysis of the historic mortar be completed prior to repointing the exterior brick.



VII. Appendix

A. Building permits on file for 555 19th Street.







ICISCO SAN FR Phillips & Van Orden WRITE IN INK-FILE TWO COPIES Applipant must indicate in ink correctly and distinctly on the back of this sheet, a diagram of the bt with street, alleys, location of existing buildings on the lot, if any, and location and dimen-T. O. F. of proposed buildings. Plans and Specifications must be fastened together. DEPART EW BUILDING NOTOBACH APPLICATION FOR BUILDING PERMIT. BRICK BUILDING Application is hereby made to the Buard of Public Works of the City and County of San Francisco for permission to build a fratery x breast class "C" Laft Sincling & Conner the lot situated santh Fact Corner . Tind & Mineteenth streets in accordance with the plans and specifications submitted herewith. Size of Lot 200 leet front 200 feet rear 200 feet deen Size of proposed building. 125 x 60. and 36 x 55-0 Extreme height of building 45-0 If party walls are to be used, give thickness and height of stories ... Are foundations to be on solid or filled ground ? Top is filled - See Not on once Footings will be of l'ancrete !! Foundation walls will be of _____ Canceret Parlland Concrete will be made of cement _____parts. Sand _____parts. Broken Stone 2/2____parts } Stone work laid in . mortar Granel 21/2 Brick work laid in Coment line more Face brick work laid in ... e. Face brick work. How bonded ? e de trans e PIERS OR COLUMNS WALLS and an arrive and in the second JOISTS GIRDERS Thickness Height Material Material Size Material Longest Size Side Frant Material Size Longest Span Span Footings 15 40 40 norto 5%5 +0'r . . Foundations 21. 21 310 <u>P</u>____ ----8'0 Basemant 2/ 2/ 12/2 1 2×14 13-0 12×18" 16-0 1st story 17 17% 12/27 2×12 13-0 12:214 1) 2nd story 943 DXD 17 17 2 X/2 13:0 10°x/6 12 9:3 **3rd story** 1/2 8×8' 2×12 13-0 10×16" 4th story 13 15 646 6'x 14 12% 5th story-**Sth** story 7th story 8th story 9th story 10th story State size of bearing partitions on each floor. MATERIAL SIZES

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Architect 6. a. Menmarkel) Oremer M. Levin Jacramenta ST. Address 2682 Address 1062 Falson ST. Builder B. E.C. Hermarkel. Address 110 Jessie ST.

NOTE-The owner's name must be signed by himself, or by his Architect or authorized Agent.)



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	Estimated cost of work, \$ 1900,00
	Building to be used as Candy Factory.
	Building to be used as <u>Candy Factory</u> . I hereby agree to save, indemnify and keep harmless the City and County of San Francisco and its officials against all liabilities, judgments, costs and expenses which may in anywise accrue against said city and county in consequence of the granting of this permit, and all costs and damages which may accrue from the use or occupancy of any sidewalk, street or sub-sidewalk place by virtue thereof and will
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CISC AH ſ Building Inspe **ALTERATION BLANKS** DEPAR A E N T . G T BUILDINC INSPECTION WRITE IN INK-FILE TWO COPIES TO THE HONORABLE THE BOARD OF PUBLIC WORKS 3 OF THE CITY AND COUNTY OF SAN FRANCISCO Gentlemen: The undersigned respectfully petition your Honorable Board for permission to do the following work at corner side of street of **BE DONE** 000 Estimated cost of oon Building to be used 88. I hereby agree to save, indemnify and keep harmless the City and County of San Francisco and its officials against all liabilities, judgments, costs and expenses which may in anywise accrue against said city and county in consequence of the granting of this permit, and all costs and damages which may accrue from the use or occupancy of any sidewalk, street or sub-sidewalk place by virtue thereof and will in all things strictly comply with the conditions of this permit. alberta Can Name of Architect. Owner Address .. Address Name of Build Address Report favorably Inspector. ... 192

OFFICIAL COP
OFFICIAL COPY	ANCISCO MENTO INSPECTION NO	APPLICATION OF STERPA CANDY COMPANY OWNER	FOR PERMIT TO MAKE ADDITIONS, ALTERATIONS OR REPAIRS TO BUILDING Location 2201 - Third. St., San	STERRA CANDY CONDANY	Cost & Approx. \$850.00 // ///	APPROVEDE.C	Bureau of Building Inspection Permit No.	19	
	Treport Jaunaby	R.J. aun	-	•		Workmen's Compensation Insurance Policy or Certificate filed with Central Permit Bureau	No Worknen's Compensation Insur- ance Policy or Certificate on file for reason of exclusion checked: (a) No one to be employed		 (c) Services or labor to be performed in return for aid or sustenance only, received from any religious, charitable or relief organization
	APPROVED:	Rureau of Building Inspection	APPROVED: Pec 3-4	APPROVED:	Director of Public Health APPROVED:	Department of Blactricity	APPROVED: Bureau of Engineering	APPROVED:	Art Commission
	BUREAU OF FIRE PREVENTION AND INVESTIGATION	Construct and Install on Building to Satisfac- tion of Bureau of Fire Prevention the Follow- hg Fire Protection Equipment and Appliances	F. D. (Dry) Standpipes Wet Standpipes	Hose Reels	Downpipes	Groundfloor Fipe Cashings		APPROVED: FRANK P, KELLY, CHIEF	By R. C. R. C. C.

		CO.	
	l (write in the File Two Copies
DEPAR		DEPARTMI BLDG. FORM	CITY AND COUNTY OF SAN FRANCISCO ENT OF PUBLIC WORKS CENTRAL PERMIT BUREAU
<u>BUILDING</u>	NSPECT		APPLICATION FOR BUILDING PERMIT
		3	ALTERATION
		Application	W N
	•	to the descript	to build in accordance with the plans and specifications submitted herewith and according tion and for the purpose hereinafter set forth:
		(1) Location	2201 - Third Street, San Francisco, California
		(2) For what]	purpose is present building now used? Food Manufacture
		(3) For what p	purpose will building be used hereafter 7
		(4) Total Cost	\$ Approx. \$850.00
	فنتعن	(5) Description	n of work to be doneInstall acoustical treatment aniling - General Offices
	5 25		2nd Floor.
		-	
	е 1. - с. с.		
		(6) APPLICAN	NT MUST FILL OUT COMPENSATION TO
		(7) Summer :	NT MUST FILL OUT COMPENSATION INSURANCE DATA ON REVERSE SIDE.
		(1) Supervision	n of construction by WESTERN ASBESTOS CO.
		I hereby co	675 Townsend Street, San Francisco, California ertify and agree, if a permit is issued, that all the provisions of the BUILDING LAW, THE DNE ORDINANCES, SET BACK LINE REQUIREMENTS AND THE DURY OF
		demnify and ke costs and expension	DNE ORDINANCES, SET BACK LINE REQUIREMENTS AND THE FIRE ORDI- THE CITY AND COUNTY OF SAN FRANCISCO and the STATE HOUSING ACT OF will be complied with, whether herein specified or not; and I hereby agree to save, in- seep harmless the City and County of San Francisco against all liabilities, judgments, nit, or from the use or occupancy of any sidewalk street or sub sidered.
		chereor, and wi	It in all things strictly comply with the conditions of this permit.
		(8) Architect.	It in all things strictly comply with the conditions of this permit.
		(8) Architect.	None License No
		(8) Architect Certificate State of C	None None None License No City and County of San Francisco
		 (8) Architect Certificate State of C. Address 	None None License No California City and County of San Francisco
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OFFICIAL	<u>sco</u>
	CENTRAL PERMIT BUREAU F435 Write in Ink-File Two Copies
	CITY AND COUNTY OF SAN FRANCISCO DEPT. OF PUBLIC WORKS
O DEPAR MENT	PLDG FORM
BOILDING INSPEC	APPLICATION FOR BUILDING PERMIT 1550 NOT 20 PR
	Application is hereby made to the Department of Public Works of San Francisco for permission to
	build in accordance with the plans and specifications submitted herewith and according to the description and for the purpose hereinafter set forth:
	(1) Location
	(2) Total Cost \$9.,
	(5) Present use of building
	(7) Proposed use of building
	(9) Type of construction
	(11) Any other building on lot
	(12) Does this alteration create an additional floor of occupancy
	(13) Does this alteration create an additional story to the building
	(14) Electrical work to be performed
	(15) Ground floor area of building
	(17) Detailed description of work to be done
	metal-frames-from-basement-to-first-floer-in-both-stateways;
	plaster both stairways, basement to first floor; install plaster-
	board-partitions-for-offices-on-first-floor-se-per-sketch;
	new magnasite floors throughout building except for office area;
	new boiler and steam lines; paint building as per instructions
	of the Board of Health.
	(18) No portion of building or structure or scaffolding used during construction, to be closer than 6'0" to
	any wire containing more than 750 volts. See Sec. 385, California Penal Code. (19) Supervision of construction by Charles Address. 49. Zoe. Street.
	(20) General contractor Gharles O. Jones Co. California License NG 346.3
	Address. 49 Zoe Stmet San Francisco
	(21) Architect
	Address
	(22) Engineer
	Address
	(23) I hereby certify and agree that if a permit is issued for the construction described in this applica- tion, all the provisions of the permit and all laws and ordinances applicable thereto will be complied with. I further agree to save San Francisco and its officials and employees harmless from all costs and damages which may accrue from use or occupancy of the sidewalk, street or subsidewalk space or from anything else in connection with the work included in the permit. The foregoing covenant shall be bind- ing income to save applicable the applicant their heirs, successors and assignees.
	(Phone 1971 #1447)
	(24) Owner
	By Owner's Authorized Aren't to be Owner's Authorized Architect, Engineer or General Contractor. PERMIT OF OCCUPANCY MUST BE OBTAINED ON COMPLETION OF HOTEL OR PERMIT OF OCCUPANCY MUST BE OBTAINED ON COMPLETION OF HOTEL OR
	APARTMENT HOUSE PURSUANT TO SEC. 808 SAN FRANCISCO BUILDING CODE.



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OFFICIAL COP SANF ANCISCO CITY AND COUNTY OF SAN FRANCISCO FORM BUDG Bepartment of Public Works Central Permit Bureau $T M \in NT \cap F$ Application for a permit for a sign subject to Section 604(b) of the DEPA City Planning Code (no Building Code approval required). BUILDI G INSPECTION Application to be filed in duplicate. For further information contact the Department of City Planning at 558-3301 or 558-3056. 7120 _____ 19<u>73</u> Permit fee \$5.00 (non-refundable) Date 2 ml Location of sign (street address) 🎘 🎝 🔿 / S.F. 4158 Assessor's Block ommercial (LigHTING CO) Present use of building Type of sign (check one): Painted Wall X; Door or Window / /; Bulletin Board / /; Awning, Marquee or Canopy (separate permit required for construction) / /; Professional Occupation / /; Other ______ Purpose of sign application (check one): New Sign /_/; Replacement); Reconstruction /_/; Relocation /_/; Expansion or Intensification /_/; Change of copy /_/; Other FROM WHITE CANDY CO. TO KOSMON Lighting Illumination (check one): Direct / /; Indirect / /; Nonilluminated 🔀; Flashing / / (separate permit required for any illumination) square feet 46 sq. 1512 sq. -3) 12 + 8 ' + 3 68 sq. ' Total area of signssurface 926 Size) 64 X 81 * 81 46 PLOT PLAN AND ELEVATION: Indicate in scaled drawings exactly the location of the sign horizontally and vertically on the building and on the lot, as well as the sign copy. If more space is required, use other side of this sheet. PAINT ONLY, CHARCOAL BACK 9 Round W/ WHITE LETTERS Kosmon Lighting EQUIPMENT CO. FLOUR ESCENTAND INCANDESCENTFATURE PLOT! PLASTIC LAMP GLOPES 8' Letters (Rightside) Kosman-Lighting 457024) 46' 28' KOSMAN LIGHTING (LEFT Side) BLOCKEDOJTONLY W/ CHARCOOL PAINT. (REAR) NOSIGN 64'28' MR.KOSMAN Address 22013 ST. S.F. Leased's NAME Property winer's name 864-2686 Applicant's name Bob CORKISH Address 60 SHARON ST Telephone 864-2641 I agree that if a permit is issued for the sign described in this application, all the provisions of the permit and all the laws and ordinances applicable thereto will be complied with. kiel USOB C. Applicant's signature DEPARTMENT USE ONLY BELOW THIS LINE Actions Approved: Approved: Approved: by other Departments (where required): Approved: Action by M No. 78026 Department of City Planning: C.V JE 84 1973 Department of City Planning Date of Permit Issuance THE APPROVED COPY OF THIS APPLICATION RETURNED TO APPLICANT CONSTITUES THE PERMIT



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OWNER OR LESSEE DE		JC	OB LOCATION		APP. NO	
CONTRAD		2201 3rd st.			424793	
	104522			BLOCK - LOT	HOUSE N	
ESTIMATED COST				4058		
				DATE		
ENT SSUED		PERMIT			7/24/	73
7/24/73		-	_	INSPECTOR		
1/23/13		7802	0			
ERECT/ALTER	SLDG. TYPE			PLANS	NUMBER OF	
PAINTED	1172	CODE	DESCRIPTION	T LANS	STORIES	FAN
SIGN			Cosa.			
CN RACTOR			ADERESS		<u></u>	1
RC-7807	· <u>····</u>		ADDRESS			
NO NER			ADDRESS			
				DEPARIMEN CITY AND COUR	DING RECORD T OF PUBLIC V	NORES
INNER OR LESSEE		2201	OS LOCATION		APE NO	93



WORK COMPLETED. FINAL CERTIFICATE POSTED.

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FIRE ESCAPE INSTALLED PER APPROVED PLAN.	11
ALL SPECIAL INSPECTION REPORTS RECEIVED.	1 1
EXTERIOR OR STRUCTURAL PLASTERING OK	11
ELUES BY	1 1
LATHING PERMISSION TAG POSTED	11
FOUNDATION FORMS INSPECTED. O.K. TO POUR	
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BUILDING INSPECTORS JOB RECORD	

<u></u>	المالة المستحد المستحق	AL USE ONLY	CITY AND COUNTY OF SAN FRANCISCO	្វិទី
SAN FR	ORISSUANCE.	APR 0 6 1983 ()	DEPARTMENT OF PUBLIC WORKS	RA C
			F PPLICATION FOR BUILDING PERMIT	CON.
			SALADITIONS ALTEDATIONS OF PEPAIPS	L.
O E P A R T BUILDING	(RENTOT) ANSPE CTON	Dept. Public Wo		G H
P 	TITLE 24 Y	□ N 1 PR 2 5 198	BAPPLICATION HEPEBY MADE TO THE DEPARTMENT OF PUBLIC WORKS	021
	HANDICAPPED_A	PESS EUPERMITENDENT	JE PLANS AND SPECIFICATIONS SUBMITTED HEREWITH AND ACCORDING	37
	72	MAN BUREAU RUILLING INSPECTION	TO THE DESCRIPTION AND FOR THE PURPOSE HEREINAFTER SET FORTH:	Notubly S.S 5.3
	-)-	83	(1) STREET ADDRESS OF JOB:	
		FILING FEE RECEIPT NO	2201 - 3rd St. (Dop)	
•	J-10-5) PERMITNO.	15999 ISSUED	(3) ESTIMATED COST OF JOB	[i
	500160	APR 25 1983	\$50,000 At	1-
	(4A) TYPE OF CONSTR. 1 HT I N	LISAL NUMBER	TION OF EXISTING BUILDING	ND OF DWELLING
	1020324050	OF OCCUPANCY: - AND CELL		
		(5) NUMBER OF STORIES OF OCCUPANCY 2 (6) NUMBER O BASEMEN AND CELL	SF 107 PROPOSED USE 15 AAS Warchouse 107 CLASSE 91 OCCUP CLASSE	NO. OF DWELLING UNITS
	(10A) DOESTHIS ALTERATION CREATE ADDITIONAL STORY TO BUILDING?	YES (10) IF YES, STATE	(11A) DOES THIS ALTERATION YES (11) IF YES, STATE CREATE DECK OR HORIZ FIL EXTENSION TO BUILDING? NOT FILORAREA	SQ FI
	(14) WILL SIDEWALK OVER SUB-SIDEWALK SPACE BE REPARED OR ALTERED?	NON CENTERLINE OF FRONT	YESO (16) ISAUTORUNWAY NO BE CONSTRUCTED VESO (17) WILLSTREET SPACE BE USED DURING CRALTERED? NO CONSTRUCTION?	YES D
	(19) ANY OTHER EXISTING BLDG ON LOT? (IF YES, SHOW ON PLOT PLAN)	YES (20) DOES THIS ALTERATION CONSTITUTE A CHANGE NOX OF OCCUPANCY?	YES (21) ELECTRICAL YES (22) PUDMBING WORK TO BE WORK TO BE NOT PERFORMED? NOT PERFORMED?	NO X YEL
	(23) GENERAL CONTRACTOR SAN FRI	INCISCO RENOVATIO		3927 B
	(24) ARCHITECT OR ENGINEER (DESIC	DENEVERS 251	ADDRESS CALIF CERTIFICAT	2
	(23) CONSTRUCTION LENDER (ENTER IF THERE IS NO KNOWN CONST UNKNO	NAME AND BRANCH DESIGNATION IF ANY. RUCTION LENDER, ENTER "UNKNOWN").	ADDRESS	$\left \right $
	(26) OWNER - HARRE (CROSS OUT O		ADDRESS ETALS 2225-3rd St. (86104	BY BUREAU)
		ORK TO BE PERFORMED UNDER THIS APPLICATI	ION (REFERENCE TO PLANS IS NOT SUFFICIENT)	
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		RTANT NOTICES	APPLICANT'S CERTIFICATION I HEREBY CERTIFY AND AGREE THAT IF A PERMIT IS ISSUED FOR THE CO DESCRIBED IN THIS APPLICATION, ALL THE PROVISIONS OF THE PERMIT A	NSTRUCTION ND ALL LAWS
	a Building Permit authorizing such a 502.1, San Francisca Building Code	acter of the occupancy or use without first ob hange. See Sec. 103, 104.8, 104.8.1, 104. and Sec. 104, San Francisco Housing Coc	C, 502, AND ORDINANCES THERETO WILL BE COMPLIED WITH.	
	No portion of building or structure of	r scaffalding used during construction, to be more than 750 volts. See Sec. 385, Ca	doser HOLD HARMLESS CLAUSE: The Permittee(s) by acceptance of this permit informia indemnify and hold hamless the City and County of San Francisco from any and all claims, demands and actions for damaces resulting from open	and against ations under
	Pursuant to Sec. 302 A.8, San Fran posted on the job. The owner is resp	icisco Building Code, the building permit s ionsible for approved plans and application	hall be this permit, regardless of negligence of the City and County of San France	sisco, and to
	correct. If actual grade lines are r	accompanying this application are assume tot the same as shown revised drawings s	d to be In conformity with the provisions of Section 3800 of the Labor Code of California, the applicant shall have on file, or file with the Central Permit B Certificate (I) or (III) designated below or shall indicate Item (IV) of	ureau, eilher or (V) or (VI)
	wall footings required must be sub ANY STIPULATION REQUIRED HERE	IN OR BY CODE MAY BE APPEALED.	below, whichever is applicable. If however, item (VI) is checked then item checked as well. Mark the appropriate method of compliance below	(V) must be
	POSTED ON THE BUILDING OR PER/ APPROVAL OF THIS APPLICATION	UNTIL CERTIFICATE OF FINAL COMPLET NIT OF OCCUPANCY GRANTED, WHEN REG DOES NOT CONSTITUTE AN APPROVAL F	DIRED. UIRED. OR THE II. Certilicate of Workman's Compensation Insurance is admitted insurer	
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	THIS IS NOT A BUILDING PERMIT. PERMIT IS ISSUED.	NO WORK SHALL BE STARTED UNTIL A BU	IIIDING () V I certify that in the performance of the work for which issued, I shall not employ any person in any manner	so as to be-
	In dwellings all insulating materials from all electrical wires or equipm	must have a clearance of not less than two ent. £	 inches come subject to the workman's compensation laws further acknowledge that I understand, in the event become subject to the workman's compensation prov laber Conte of California and Ital to compensity forthwith w 	that I should isions of the

- Certificate of Consent to Self-Insure issued by the Director of Indus-trial Relations. Certificate of Workman's Compensation Insurance issued by an admitted insurer. 1. ()
- 1 u.
- An exact copy or duplicate of (1) certified by the Director or (11) certified by the insurer. ţŧt. ()

- III. An exact copy or duplicate of (i) certified by the Director or (ii) certified by the insurer.
 IV. The cost of the work to be performed is \$100 or less.
 V. The cost of the work to be performed is \$100 or less.
 V. I certify that in the performance of the work for which this Permit is issued, I shall not employ any person in any manner so as to become subject to the workman's compensation laws of California. If further acknowledge that I understand, in the event that I should become subject to the workman's compensation provisions of the Labor Code of California and full to comply forthwith with the provisions of Section 3800 of the Labor Code, that the Permit herein applied for shall be deemed freewoked.
 VI. I certify as the owner (or the agent of the commencement of any work will file, or prior to the commencement of any work will file, with the Cathal Section laws of California and the workman's compensation laws and the deemed for the detend entral Bereau evidence that workman's compensation insurance is carried.
 Applicant's Synature A Advance A Advance is carried.

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T ENGINEER OWNER ARCHITECT

AGENT WITH POWER OF ATTORNEY 🗋 LESSEE

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	OWNER NAME	<u></u>		TEL	EPHONE
STANLEY LEVIN					861-0418
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PERMIT INSPECTION RECORD DEPARTMENT OF PUBLIC WORKS CITY AND COUNTY OF SAN FRANCISCO BUILDING INSPECTION JOB CARD

OFFICIAL COPY DATE **BUILDING INSPECTORS JOB RECORD** in all reant On 12/15 B3 / / / / / WORK COMPLETED. FINAL CERTHICATE POSTED. 84 APP. NO. BUILDING INSPECTOR

SANFRANCISCUM JUL 201983	CITY AND COUNTY OF SAN FRANCISCO
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BUILDING STPECTION Dept. Public Works	A PUCATION CHERER TO THE OFPOSIE OF PUBLIC WORKS
SUPERINTENDENT SUPERINTENDENT MA JIREAU RULIDING INSPECTION	TO THE DESCRIPTION AND FOR THE PURPOSE HEREINAFTER SET FORTH
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SUB SIDE WAIK SPACE BE REFAILED GRALERED? (19) ANY OTHER EXISTING BLDG ON LOT? IF YES, SHOW NOA (20) DESTING ALTERATION CONSTITUTE A CHANGE OF OCCUPANCY (21) GENERAL CONTRACTOR	NO CONSTRUCTED NO SI CONSTRUCTED NO SI CONSTRUCTED CONSTRUCTED CONSTRUCTED CONSTRUCTED NO SI CONSTRUCTION? NO SI VEST VEST VEST VEST VEST VEST VEST VEST
(24) ARCHITECT OR ENGINEER (DESIGN CONSTRUCTION), NON C (23) CONSTRUCTION LENDER (ENTER NAME AND BRANCH DESIGNATION IF ANT IF THERE IS NO KNOWN CONSTRUCTION LENDER, ENTER "UNKNOWN"), NO	ADDRESS CAUF CERTIFICATE NO ADDRESS
120) OWNER - LESSEE (BODS OUT ONE) JOSCPA LEVIN & SON 127) WRITE IN DESCRIPTION OF ALL WORK TO BE PERFORMED UNDER THIS APPLICA	ADDRESS 2225 312 St 861-0418
INSTRUC 53 to replace dance	Alum. Windows -
IMPORTANT NOTICES No change shall be made in the character of the occupancy or use without first of a Building Permit authorizing such change. See Sec. 103, 104,8, 104 Building Code and Sec. 104, San Francisco Housing Code	A.C. 502, AND ORDINANCES THERETO WILL BE COMPLED WITH.
No portion of building or structure or scalfolding used during construction, to b than 60° to any wire containing more than 750 volts. See Sec. 385, Cr Penal Code. Pursuant to Sec. 302 A B, San Francisca Building Code, the building permit posted on the job The owner is responsible for approved plans and application kept at building site Grade lines as shown on drawings accompanying this application are assume	HOLD HARMLESS CLAUSE. The Permitter(s) by acceptance of this permit, agree(s) to alifornia indemnity and hold harmless the City and County of San Francisco from and against any and all claims, demands and actions for damages resulting from operations under this permit, regardless of negligence of the City and County of San Francisco, and to assume the defense of the City and County of San Francisco against all such claims, demands and actions of the Labor Code of the State of in conformity with the provisions of Section 3800 of the Labor Code of the State of in conformity with the provisions of the clabor Code of the State of in conformity with the provisions of the clabor Code of the State of in conformity with the provisions of the clabor Code of the State of in conformity with the provisions of the clabor Code of the state of in conformity with the provisions of the clabor Code of the state of in conformity with the provisions of the clabor Code of the state of in conformity with the provisions of the clabor Code of the state of in conformity with the provisions of the clabor code of the state of in conformity with the provisions of the clabor code of the state of in conformity with the provisions of the clabor code of the state of in conformity with the provisions of the clabor code of the state of in conformity with the provisions of the clabor code of the state of in conformity with the provisions of the clabor code of the state of in conformity with the provisions of the clabor code
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- Christicate of Workman's Compensation Insurance issued by an admitted insurer \sim ii.
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BUILDING NOT TO BE OCCUPIED UNTIL CERTIFICATE OF FINAL COMPLETION IS POSTED ON THE BUILDING OR PERMIT OF OCCUPANCY GRANTED, WHEN REQUIRED APPROVAL OF THIS APPLICATION OCES NOT CONSTITUTE AN APPROVAL FOR THE ELECTRICAL WIRING OR PLUMBING INSTALLATIONS A SEPARATE PERMIT FOR THE WIRING AND PLUMBING ONST BLO BEATHED SEPARATE PERMITS ARE REQUIRED ANSWER IS 'YES'' TO ANY OF ABOVE OUESTIONS '15/ 116/ 117/ 120, 172, 10 THIS IS NOT A BUILDING PERMIT NO WORK SHALL BE STARTED UNTIL A BUILDING PERMIT IS ISSUED.

In dwellings all insulating inatgrials must have a clearance of not less than two inches from all electrical wires or equipment

CHECK APPROPRIATE BOX

OWNER	ARCHITECT ENGINEER
LESSEE	AGENT WITH POWER OF ATTORNEY
	ATTORNEY IN FACT

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- Circlebate of Workman's Compensation Insurance --ssued by an aumited insurar An exact copy or displicate of the certified by the Director or ally certified by the insurer. The cust of the work to be performed is \$100 or (cus) I certify that in the performance of the work for which this Permit is sound. I shall not employ any person in any mounter so as to be come subject to the working's come multiple laws of Californ I outbe acknowledge that I understand as the event that I she use before subject to the working's compensation provisions of the laster acknowledge that I understand as the event that I she use before subject to the working's compensation provisions of the laster Code of California and fait to complet furthwith with the provision arms of Section 1800 and the Lober Code that the Permit hierer applied for shall be deemed revoked. Furthy as the owner for the agent of the owner filled as the perfor-mance of the work for which this Permit is issued, I will employ contractor who Complete with the Workman's compression laws of California and who has on file, or prior to the commencement of any workman. Compensation insurance is corried Outer and the out of the control Permit Bergia. 1V. V Applicants Signation





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**		OWNER NAME			TELEPHONE
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obtaining a Building Permit authorizing such change. See San Francisco Building Cade and Son Francisco Housing Cade. No partion of building or structure or scaffolding used during construction, to be closer	indemnity and hold harmiest the Lity and Lounty of San Francisco from ana against any and all daims, demands and actions for damages resulting from operations under this permit, regardless of negligence of the City and County of San Francisco, and to assume the defense of the City and County of San Francisco against all such daims,
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job. The owner is responsible for approved plans and application being kept at building site.	Certificate (I) or (II) or (III) designated below or shati indicate item (IV) or (V) or (V) below, whichever is applicable. If however, item (VI) is checked then item (V) must be checked as well. Mark the appropriate method of compliance below:
Grade lines as shown on drawings accompanying this application are assumed to be correct. If actual grade lines are not the same as shown revised drawings showing correct grade lines, cut cand fills together with complete details of retaining walls and	 Certificate of Consent to Self-Insure issued by the Director of Industrial Relations.
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THIS IS NOT A BUILDING PERMIT, NO WORK SHALL BE STARTED UNTIL A BUILDING PERMIT IS ISSUED.	further acknowledge that I understand, in the event that I should became subject to the workman's compensition provisions of the Labor, Cade of California and fail to camply indrivith with the
In dwellings all insulating materials must have a clearance of not less than two inches from all electrical wires or equipment.	provisions of Section 3800 of the Labor Code, that the Permit herein applied for shall be deemed revoked. (XVI) Lectify as the owner (or the agent of the owner) that in the perfor-
CHECK AFPROPRIATE BOX	mance of the work for which this Permit is issued, I will employ a contractor who complies with the workman's compensation laws of Colifornia and who has on this, or prior to the commencement of any
	work will file, with the Central Permit Bureau evidence that workman's compensation insurance is carried.
APPLICANT'S CERTIFICATION	MALONE 1.11.
I HEREBY CERTIFY AND AGREE THAT IF A PERMIT IS ISSUED FOR THE CONSTRUCTION DESCRIBED IN THIS APPLICATION, ALL THE PROVISIONS OF THE PERMIT AND ALL LAWS AND ORDINANCES THERETO WILL BE COMPUED WITH.	Wilda X Janes 2/23/04

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PERMIT INSPECTION RECORD DEPARTMENT OF PUBLIC WORKS CITY AND COUNTY OF SAN FRANCISCO * BUILDING INSPECTION JOB CARD

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IMPORTANT NOTICES

Na change shall be made in the character of the occupancy or use without first obtaining a Building Permit authorizing such change. See San Francisco Building Cade and San Francisco Housing Cade. Na portica of building or structure or scatfolding used during construction, to be closer than 60° to any wire containing more than 750 volts. See Sec. 385, California Perol Cade

Penal Code

Pursuart to San Francisco Building Code, the building permit shall be posted on the jab. The owner is responsible for approved plans and application being kept at building site.

Grade lines as shown on drawings accompanying this application being kepf at building site. Grade lines as shown on drawings accompanying this application are assumed to be correct, if actual grade lines are not the some as shown revised drawings showing correct grade lines, cuts and fills together with complete details of retaining walls and wall footings required must be submitted to this bureau for approval. ANY STIPULATION REQUIRED HEREIN OR BY CODE MAY BE APPEALED. BUILDING NOT TO BE OCCUPIED LINTIL CERTIFICATE OF FINAL COMPLETION IS POSTED ON THE BUILDING OR PERMIT OF OCCUPANCY GRANTED, WHEN REQUIRED. APPROVAL OF THIS APPLICATION DOES NOT CONSTITUTE AN APPROVAL FOR THE ELECTRICAL WIRING OR PLUMBING INSTALLATONS. A SEPARATE PERMIT FOR THE WIRING AND FULMBING MUST BE OBTAINED. SEPARATE PERMITS ARE REQUIRED THE ANSWER IS "YES" TO ANY OF ABOVE QUESTIONS (10) (11) (12) (13) (22) or (24). THIS IS NOT A BUILDING PERMIT, NO WORK SHALL BE STARTED UNTIL A BUILDING PERMIT. IS ISSUED. In dwellings all insulating materials must have a clearance of not less than two inches from all electrical wires or equipment.

CHECK APPROPRIATE BOX

- AGENT WITH POWER OF ATTORNEY
- 🗌 LESSEE

APPLICANT'S CERTIFICATION I HEREBY CERTIFY AND AGREE THAT IF A PERMIT IS SUED FOR THE CONSTRUCTION DESCRIBED IN THIS APPLICATION, ALL THE PROVISIONS OF THE PERMIT AND ALL LAWS AND ORDINANCES THERETO WILL BE COMPUED WITH.

NOTICE TO APPLICANT HOLD HARMLESS CLAUSE: The Permittee(s) by acceptance of the permit, agree(s) to indemnify and hold harmless the City and County of San Francisco from and against any and all deams, demands and actions for damages resulting from operations under this permit, regardless of negligence of the City and County of San Francisco, and to assume the defense of the City and County of San Francisco against all such claims, demands and actions. In conformity with the provisions of Saction 3800 of the Labor Code of the California, the applicant shall have an file, or file with the Cental Permit Burees. Cartificate (I) or (III) or (III) designated below or shall indicate item (IY) or (V) a below, whichever is applicable. If however, them (V) is checked than item (V) mut, checked as well. Mark the appropriate method of compliance below:

- Certificate of Consent to Self-Insure issued by the Director of Industrial Relations. ()L
- Certificate of Workman's Compensation Insurance issued by an admitted insurer. \otimes ŧi.
- () П.
- Certificate of Workman's Compensation Insurance issued by an admitted insurer. An exact copy or duplicate of (I) certified by the Director or (II) certified by the insurer. The cast of the work to be performed is \$100 or less. I certify that in the performance of the work for which this Permit is issued, ishell not employ any person in any manners as a became subject to the workman's compensation laws of California. I further acknowledge that I understand, in the eyent that I should become subject to the workman's compensation provisions of the labor Cade of California and fail to comply torthwith with the provisions of Section 3800 of the labor Cade, that the Permit therein applied for shall be deemed revoked. I will employ or contractor who complies with the workman's compensation laws of California on dia via the achieven to the comencement of any work will file, with the Central Permit Bureau evidence that workman's compensation layroau evidence that any set of the compensation layroau evidence that any set of the compensation layroau evidence that any set of the set of carried. the set of the set of the compensation layroau evidence that any set of the set of the compensation layroau evidence that any set of the set o $\begin{pmatrix} \\ \\ \end{pmatrix}$ ΙV. γ.

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BUILDING INSPECTORS JOB RECORD DATE aming n ۲ 5-185 PI ju purgrass. 74 Hurton 04) 6 119 185 0/24/35 Requist 112 porti 61 1 / / ired. Z485 cture (4 WORK COMPLETED FINAL CERTIFICATE POSTED APP NO. 8410080 DING INSPECTOR

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Provisions or section serve or the taber Lader, that the Permit Retein applied for shall be deemed revoked.
 VI. I certify as the owner (or the agent of the owner) that in the performance of the work for which this Permit is issued, I will employ a contractor who complies with the workman's compensation laws of California and wha has an file, or prior to the commencement of any work will file, with the Central Permit Byreau evidence that workman's compensation insurance that workman's compensation insurance is carried.

<u>9-26-5.U;</u> Date

Applicant's Signature

CONTRACTOR ATTORNEY IN FACT

CHECK APPROPRIATE BOX

C) OWNER

APPLICANT'S CERTIFICATION I HEREBY CERTIFICATION AND AGREE THAT IF A PERMIT IS ISSUED FOR THE CONSTRUCTION DESCRIBED IN THIS APPLICATION, ALL THE PROVISIONS OF THE PERMIT AND ARM / *** LAWS AND ORDINANCES THERETO WILL BE COMPUED WITH.

ARCHITECT

CONDITIONS AND STIPULATIONS OFFICIAL SANFR ANCI ()REFER APPRO DATE 10: REASON: COP DEPARTMENT 01 BUILDING INSPECTION ĆO NOTIFIED APPROVED: DATE Per attached latter for Se-10 . REASON: DO For o ven CATEGORICALLY EXEMPT FROM ENVIRONMENTALOREVIEW TO STOFFANNING NOTIFIED MR. INOV 11-3-1984 APPROVED: FOR WORK DATE: JOLD SECTION REASON: PLEASE NOTIFY SFFD AT START OF WORK CONTACT FIRE INSPECTOR NOTIFIED MR (415) 861-8000 EXT. 315 NORE BUREAU OF FIRE PREVENT DATES APPROVED: DATE: REASON: AND NAMES OF ALL NOTIFIED MR. CIVIL ENGINEER, BUR. OF BLDG. INSPECTION PERSONS NOTIFIED APPROVED: DATE: REASON: PURING PROCESSING NOTIFIED MR. BUREAU OF ENGINEERING DATE: APPROVED: REASON: NOTIFIED MR. DEPARTMENT OF PUBLIC HEALTH APPROVED: DATE: _ REASON: NOTIFIED MR. REDEVELOPMENT AGENCY DATE: _ APPROVED: REASON: NOTIFIED MR. RESIDENTIAL-ENV. INSPECTOR, DIV. OF APT. & HOTEL INSP., B.B.I. DATE: _ APPROVED: REASON: NOTIFIED MR. ents noted on this application, and attache with all conditions or stipulations of the various bure ditions or stipulations, which are hereby made a pa l agree to co statements o auth Number of attachments NER'S AUTHORIZED AGENT



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PERMIT INSPECTION RECORD DEPARTMENT OF PUBLIC WORKS CITY AND COUNTY OF SAN FRANCISCO BUILDING INSPECTION JOB CARD

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	APPLICATION FOR BUILDING PERMIT CITY AND COUNTY OF SAN FRANCISCO
	ADDITIONS, ALTERATIONS OR REPAIRS
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CHECK APPROPRIATE BOX

-] OWNER ENGINEER ARCHITECT
- AGENT WITH POWER OF ATTORNEY ... LESSEE

APPLICANT'S CERTIFICATION

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- $\left\{ \right\}$ IV.

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Date

O.t-2.84

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BLOCKILOT APPLICATION NO ADDRESS OF JOB 2201 3RD 8410639 ST TELEPHONE OWNER NAME MARTIN . LEVIN INVESTMENT CO. (415)397-0373 PERMIT NO EXPIRE DATE DISPOSITION DATE ESTIMATED COST FILE DATE DISPOSITION \$4,000 10/02/84 ISSUED 10/29/84 523062 04/29/85 DISTRICT PLANS STORIES OCCUPANCY CODES UNITS FORM CONST TYPE BID-INSP 11 35 **B-2** 2 2 TELEPHONE CONTACT NAME (415)861-0419 LOEWNTHAL CONST. OTHER DESCRIPTION STANDARD DESCRIPTION/BLDG USE ADDITIONAL PARTITION WALLS OFFICE NO SPECIAL INSPECTIONS? FIRE ZONE NO SPECIAL USE DISTRICT TIDF NO COMPLIANCE WITH REPORTS PENALTY NOTES

PERMIT INSPECTION RECORD DEPARTMENT OF PUBLIC WORKS CITY AND COUNTY OF SAN FRANCISCO BUILDING INSPECTION JOB CARD

FRANCISCO BUILDING INSPECTORS JOB RECORD DATE 1 the start Ļ 115/34 OT Usited Site, WKin progress 3/12/85 6/19/85 Routide 1/85 Regat respective told samers deed Final mipertion to dere File / 1 / / WK to be done 14-29-85 18. et was Noted to have final NU DESPACE Expirel ETED FINAL CERTIFICATE POSTED APP NO 84106 G INSPECTOR
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<u>b.</u>....

OFFICIAL COPY

- CHECK PPROPRIATE BOX
- ARCHITECT OWNER
- AGENT WITH POWER OF ATTORNEY 🗌 LESSEE

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NOTICE TO APPLICANT

HOLD HARMLESS CLAUSE: The Permittee() by acceptance of the permit, agree(s) to indemnify and hold harmless the City and County of San Francisco from and against any and all claims, demands and actions for demager resulting from operations under this permit, regardless of negligence of the City and County of San Francisco, and to assume the defense of the City and County of San Francisco against all such claims, demands and actions.

demands and actions. In conformity with the provisions of Section 3800 of the tabor Code of the State of California, the applicant shall have on file, at file with the Cental Permit Buyeau, either Certificate (!) or (!!) actignated below or shall indicate item (!V) or (V) or (V) below, whichever is applicable. If however, item (V) is checked then item (V) must be checked os well. Mark the appropriate method of compliance below:

- ()I. Cartificate of Consent to Self-Insure issued by the Director of Industrial Relations.
- Certificate of Workman's Compensation Insurance issued by an admitted insurer. () И.
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Certificate of Workman's Compensation Insurance issued by an admitted insurer. An exact copy or duplicate of (I) certified by the Director or (II) cartified by the insurer. The cost of the work to be performed is \$100 or less. Jearlish that in the performance of the work for which this Permit is issued, I shall not employ any person in any manner so as to became subject to the workman's compensation laws of California. I further acknowledge that I understand, in the event that I should become subject to the workman's compensation provisions of the labor Code of California and fail to comply forthwith with the pravisions of Section 3800 of the labor Code, that the Permit herein applied for shall be deemed revoked. I tertify as the owner (or the agent of the awer) that in the perfor-mance of the work for which this Permit is issued, I will employ a Contractor who complex with the Central to the commencement of any work will file, with the Central Permit Bureou evidence that workman's compensation insurance is carried. (V VI.

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PERMIT INSPECTION RECORD DEPARTMENT OF PUBLIC WORKS CITY AND COUNTY OF SAN FRANCISCO BUILDING INSPECTION JOB CARD

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	APPLICATION FOR BUILDING PERMIT	CITY AND COUNTY OF SAN FRANCISCO
10.000 A.	ADDITIONS, ALTERATIONS OR REPAIRS	
	FORM 3 RE-APPROVAL SITE INSPECTION REQUIRED	PUBLIC WORK O SIN RUICISC TOR FERMISSICI TO BUILD ACCOMPANY THE PLANS AND SPECIFICATIONS
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	LANDVOIGT - BAYNE 555 19" ST	ADDRESS PHONE (FOR CONTACT BY BUREAU) SAN FRANCISCA CA 567.4141
	(16) WHITE IN DESCRIPTION OF ALL WORK TO BE PERFORMED UNDER THIS APPLICATION (REFERENCE) ALTERATIONS 70 T.V. PRODUC	
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	(23) ARCHITECT OR ENGINEER (DESIGN CONSTRUCTION)	NO SE ON PLOT PLANT, NO SA OF OCCUPANCY , NO SA ADDRESS
	[24] CONSTRUCTION LENDER (ENTER NAME AND BRANCH DESIGNATION IF ANY, IF THERE IS NO KNOWN CONSTRUCTION LENDER, ENTER "UNKNOWN").	ADDRESS
2 - -	IMPORTANT NOTICES	
	No change shall be made in the character of the occupancy or use with obtaining a Building Permit authorizing such change. See Son Francisco E Code and San Francisco Housing Code.	Building any and all claims, demands and active any and county of son transitions from operations under this permit reported so of perdiagnee of the City and County of San Francisco, and to this permit reported so of perdiagnee of the City and County of San Francisco, and to the city and county of San Francisco, and the city and count
G	No portion of building or structure or scaffolding used during construction, to b than 60° to any wire containing more than 750 volts. See Sec. 385, Co Penol Code. Pursuant to San Francisco Building Code, the building permit shall be posted	liternia demands and actions. In conformity with the provisions of Section 3800 of the Labor Code of the State of
5	 job. The owner is responsible for approved plans and application being building site. 	kept at Certificate (I) or (II) or (III) designated below ar shall indicate item (IV) or (V) or (VI) below, whichever is applicable. If however, item (VI) is checked then item (V) must be
1	Grade lines as shown on drawings accompanying this opplication ore assume correct. If actual grade lines are not the same as shown revised drawings a correct grade lines, cuts and fills together with complete details of retaining w	howing () 1. Certificate of Consent to Self-Insure issued by the Director of
	wall faotings required must be submitted to this bureau for approval. Any STIPULATION REQUIRED HEREIN OR BY CODE MAY BE APPEALED. BUILDING NOT TO BE OCCUPIED UNTIL CERTIFICATE OF FINAL COMPLET	rION IS () III. An exact copy or duplicate of (1) certified by the Director or (11)
ģ	POSTED ON THE BUILDING OR PERMIT OF OCCUPANCY GRANTED, WHEN REC APPROVAL OF THIS APPLICATION DOES NOT CONSTITUTE AN APPROVAL F ELECTRICAL WIRING OR PLUMBING INSTALLATONS, A SEPARATE PERMIT F	OR THE () IV. The cost of the work to be performed is \$100 or less. OR THE () V. I certify that in the performance of the work for which this Permit is unrous issued. I shall not employ any person in any manner so as to become
	WIRING AND PLUMBING MUST BE OBTAINED, SEPARATE PERMITS ARE REQU NSWER IS "YES" TO ANY OF ABOVE QUESTIONS (10) (11) (12) (13) (22) HIS IS NOT A BUILDING PERMIT. NO WORK SHALL BE STARTED UNTIL A BU	or (24). Subject to the workmon's compensation lows of California, I further acknowledge that Lugderstand in the event that I should
	ERMIT IS ISSUED. 	o inches provisions of Section 3800 of the Labor Code, that the Permit herein opplied for shall be deemed revoked. () YL certify as the avener for the agent of the owner) that in the perfor-
		mance of the work for which this Permit is issued, I will employ a contractor who complies with the workman's compensation laws of California and who has an life, or prior to the commencement of any
		work will file, with the Central Permit Bureau evidence that workman's compensation insurance is carried.
	APPLICANT'S CERTIFICATION HEREBY CERTIFY AND AGREE THAT IF A PERMIT IS ISSUED FOR THE CONSTR DESCRIBED IN THIS APPLICATION, ALL THE PROVISIONS OF THE PERMIT A	UCTION
;	LAWS AND ORDINANCES THERETO WILL BE COMPLED WITH.	Applicant's Signature Date
	na 1997 - Andrea Maria, and an	tente de la contrata d

CONDITIONS AND STIPULATIONS OFFICIAL NCPSC SANFR DATE: _ APPROVED; Notity Building Inspector, 4 2 REASON: 51 13 at start of job. . COP DEPARTMENINOF BUILDING INSPECTION WITH Betrical or plumbing ¥5.... riseparate permitte. d ليم معر معرج ru Ø. r NOTIFIED MR. BUILDING INSPECTOR, BUR. OF BLDG. O. DATE: . APPROVED: REASON: YGC 100 8 MSG4 -117 IFTERE STATES - 1. \$1 Xe ENT POINT ANNING tan - US of the _ 138.g NOTIFIED MR. . 0 DEPAREMENT G T ALUS HOLD SECTA DATE: APPROVED: 5-161 REASON 50 Ţ 1 NOTIFIED MR. BUREAU OF FINE PREXENTICH & PUBLIC SAFE 5 DATE: APPROVED: REASON: CARLINES CONTRACT 4 4 <u>ي</u> ۲۰ 25 -• in the contract Saturday Start . . NOTIFIED MR. CIVIL ENGINEER, BUR. OF BLDG. INSPECTION ------DATE: 21.1 11 APPROVED: Territe Jack ----REASON: 生产的制造工具 网络美国山 1 4264 (N 19459) 19652 南の NOTIFIED MR BUREAU OF ENGINEERING DATE: APPROVED: AN PERIOD REASON: TOTAL STRUCTURE NOTIFIED MR. á DEPARTMENT OF PUBLIC HEALTH uninte 2. 1. 1. 2. 2 in installes. DATE: APPROVED: REASON: 1 NOTIFIED MR. Man all idaj ta galelard ÷. REDEVELOPMENT AGENCY 20 1: 200; DATE: " CITALE TANK APPROVED: . · 🕹 NOTIFIED MR INV DESTIDA 1.4 RESIDENTIALENV. INSPECTOR, DIV. OF APT. & HOTEL INSP., B.B.I. 7 DATE: Conducts 1 1914 202 APPROVED: REASON: SHE WALL TO BE TO THE 0 THE BULK CONTRACT AND NOTIHED MR. ور المراجع 51 m 122222 NORASTO T yeaus or departments noted on part of this application. this application, and attached i agree to comply with all conditions or stoulations of the statements of conditions or stoulations which are hereby varieus bu s'añode é LI ANETOLISE ALEXANDESE ALEXANDESE भाषात्रकः सम्बद्धाः भाषाः भाषाः स्वयुक्तित्व भाषाः भाषाः स्वयुक्तित्व Number of attachment: OUSDERS AUTHORIZED AGENT 225



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	(17) DOES THIS ALTERATION CREATE ADDITIONAL STORY TO BUILDING? (21) WILL SIDEWALK OVER	NOX	18) IF (17) IS YES, STATE NEW HEIGHT AT CENTER UNE OF FRONT	FT.	9) DOES THIS ALTERAT CREATE DECK OR EXTENSION TO BUIL (3) ANY OTHER EXISTIN	HORIZ. DING?	YES 7 (20) IF (19) IS YES, S NEW GROUND FLOOR AREA YES 1 (24) DOES THIS ALTE	1274 6	E CP
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	Penal Code, Pursuant to San Franc	isca Building Code, the b	750 volts. See Sec. 385, building permit shall be pos	ted on the	California, th	with the provision e applicant shall h	is of Section 3800 of the ave on file, or file with the	t Cental Permit 8	ureau, either 💷 🤕
	building site. Grade lines as shown	on drawings accompany	plans and application beining this application are assu	umed to be	below, which checked as w	ever is applicable. well. Mark the app	gnated below or shall in . If however, item (VI) is c propriate method of com	hecked then iter pliance below:	n (V) must be
	correct grade lines, cu wall footings required	ts and fills together with a I must be submitted to th	e as shown revised drawing complete details of retaining his bureau for approval.		() 1. () 11.	Industrial Rel Certificate or	f Workmon's Compensa	,	
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	DESCRIBED IN THIS A	APPLICANT'S CERT AGREE THAT IF A PERMI PPLICATION, ALL THE PS CES THERETO WILL BE C	T IS ISSUED FOR THE CONS ROVISIONS OF THE PERMI		Applicant's	ignatuje	Ment	n OCT. Date	15,1992

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FORM PPROVE JAM FRANCISCO 90 NDC APPROVED/FOR ISSUANCE Dept. of Building Insp. - 1 ω ş NE JUN 2 3 2003 4 DEPARTMENT OF 2003 87 BUILDING INSPECTION ん DIRECTOR DEPT, OF BUILDING INSPECTION S 2 2 2 ()MR ICATION NUMBER >575Mic RETROFT CITY AND COUNTY OF SAN FRANCISCO PAQ APPLICATION FOR BUILDING PERMIT DEPARTMENT OF BUILDING INSPECTION ADDITIONS, ALTERATIONS OR REPAIRS Ľ, APPLICATION IS HEREBY MADE TO THE DEPARTMENT OF BUILDING INSPECTION OF SAN ERANCISCO FOR FORM 3 X OTHER AGENCIES REVIEW RECOIRED d PERMISSION TO DUILD IN ACCORDANCE MUTH HE PLANS AND SPICE CATIONS SUBMITTED HERBUTH INND ACCORDING TO THE DESCRIPTION AND FOR THE PURPOSE PFRMI OSHA APPROVAL REQ'D APPROVAL NUMBER: FORM 8 🗋 OVER-THE COUNTER ISSUAN CALC + SOIL + BRICK NUMBER OF PLAN SETS DO NOT WRITE ABOVE THIS UNE ▼ 2+ 3 5 BLOCH & LOT FILING FEE RECEIPT NO (1) STREET ADDRESS OF JOB AKA 223 DATE FILED -03 336065 555 ST 1974 Q4 PERMIT NO (2A) ESTIMATED COST OF (2B) REVISED ISSUED ζς 000 C 6 い 23-03 C Ø 2 č ODO INFORMATION TO BE FURNISHED BY ALL APPLICANTS LEGAL DESCRIPTION OF EXISTING BUILDING (9A) NO. 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YES 📋 at -NO NO NO Ð EXPIRATION DATE ADDRESS PHONE CALE LIC NO 14) GENERAL CONTRAC SELETTEE 110 PHONE (FOR CONTACT BY DEPT.) 415 347.0313 ADCRESS 716 BIBC# ULLIESS APPLICATION (REFERENCE TO PLANS IS NOT SLAFFICENT) <u>IARTIN</u> Elliv ACANT RETROF SEISIVIC UESS DONF ĿD BE ω_{1} TENA IMPROVOMENT \sim PH P =Y2 ADDITIONAL INFORMATION (20) IF (19) IS YES, STATE NEW GROUND FLOOR AREA (18) IF (17 IS YES, STATE NEW HEIGHT AT CENTER LINE OF FROM (19) DOES THIS ALTERATION CREATE DECK OR HORIZ EXTENSION TO BUILDING (17) DOES THIS ALTERATION CREATE ADDITIONAL HEIGHT OR STORY TO BUILDING? YES 🗋 YES so ri NO Ηİ NO (23) ANY OTHER EXISTING BLDG ON LOT? (IF YES, SHOW ON PLOT PLAN) (22) WILL BUILDING EXTEND BEYOND PROPERTY LINE (24) DOES THIS ALTERATION (21) WILL SIDEWALK OVER YES YES YES • SUB-SIDEWALK SPACE BE REPAIRED OR ALTERED? YES 1 CONSTITUTE A CHANGE OF OCCUPANCY? 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The permitted by acceptance of the permit, agree(s) to indemnify and hold harmless the City and County of San Francisco from and against any and all claim, demand and accors for damages resulting from operations under this permit, regardless of negligence of the light and County of San Francisco, and to assume his defense of the City an County of San Francisco against all County of San Francisco and to assume his defense of the City an County of San Francisco against all County of San Francisco and to assume his defense of the City and County of San Francisco against all county of San Francisco against all so that the City and County of San Francisco against all county of San Francisco and to assume his defense of the City and County of San Francisco against all county of San Francisco against all so that the City and County of San Francisco against all county of San Francisco against all so that the City and County of San Francisco against all county of San Francisco against all so that the City and County of San Francisco against all county of San Francisco against all so that the City and County of San Francisco against all county of San Fra inge shall be made in the character of the occupancy or use without first obtaining a Bu authonoing such change. See San Francisco Building Code and San Francisco Housir No portion of building or structure or scatfolding used during construction, to be closer than 60° to any wire containing more than 750 volts See Sec 385, California Penal Code In conformity with the provisions of Section 3800 of the Labor Code of the State of Ca (formul, the applicant shall have coverage under ((), or ((i)) designated below or shall indicate item (\mathbf{N}), or (\mathbf{V}), or (\mathbf{V}), whichever is approximate the other share \mathbf{N} where \mathbf{N} is a specific data well. Mark the appropriate method of compliance below. Pursuant to San Francisco Building Code, the building permit shall be posted on the job. The owner is responsible for approved plans and application baing kept at building site. Grade lines as shown on drawings accompanying this application are assumed to be correct. If actual grade lines are not the same as shown revised drawings showing correct grade lines, cuts and hits together with complete details of retaining walls and wall footings required must be submitted to this department for approval by affirm under penalty of perjury one of the following declarations I have and will maintain a centificate of consent to self-insure for worker's compensation, as provided by Section 3700 of the Labor Code, for the performance the work for which this permit is used () ANY STIPULATION REQUIRED HEREIN OR BY CODE MAY BE APPEALED ANT STIPLATION REQUIRED HERIN OF BY COMMANDE MAT BE APPRACE BUILDING ON TO BE OCCUPRED UNTIL CERTIFICATE OF FINAL COMPLETION IS POSTED ON THE BUILDING OR PERMIT OF OCCUPANCY GRANTED, WHEN REQUIRED ANTIOVAL OF THIS APPLICATION DOES NOT CONSTITUTE AN APPROVAL FOR THE ELECTRICAL WIGHING OR PLUMBING INSTALLATIONS A SEPARATE PERMIT FOR THE WIRING AND PLUMBING MUST BE OBTAINED SEPARATE PERMITS ARE REQUIRED IF ANSWER IS YES' TO ANY OF APOVE QUESTIONS (10) (11) (12) (13) (22) OF (24) I have and will maintain workins' compensation insurance, as required by Section 3700 of the Labor Code, for the performance of the work for which this permit is issued. My workers' compensation insurance carrier and policy number are Carner Policy Number THIS IS NOT A BUILDING PERMIT. NO WORK SHALL BE STARTED UNTIL A BUILDING PERMIT IS 111 The cost of the work to be done is \$100 or less. ISSUED Leetly that in this performance of the work for which this permit is issued, I shall not employ any person in any manner so as to become subject to the workers compensation laws of California. I whither acknewledge that I understand that in the event that I should become subject to the workers' compensation provisions of the Labor Code of California and tait to comply torthwin which the provisions of Section 3800 of the Labor Code, bit the permit that is howing work with the provisions of Section 3800 of the Labor Code, bit the permit the permit subject to the workers' compensation provisions and the control as the complex of the permit the permit subject to the subject to the section of the sec N In dwellings all insulating materials must have a clearance of not less than two inches from all electrical wires or equipment. CHECK APPROPRIATE BOX DOWNER LARCHITECT LESSEE LAGENT LONTRACTOR LENGINEER herein applied for shall be deemed revoked. I certify as the owner (or the opent for the owner) that in the performance of the work for which this permit a issued. I will employ a contractor who complets with the workers' complexiting lines of California and who, prive to the commencement of any work, will fie a complete goog of this term with the Central Permit Buroau. APPLICANT'S CERTIFICATION LHEREBY CERTIFY AND AGREE THAT IF A PERMIT IS ISSUED FOR THE CONSTRUCTION DESCRIBED IN THIS APPLICATION, ALL THE PROVISIONS OF THE PERMIT AND ALL LAWS AND ORDINANCES THERETO WILL BE COMPLED WITH and Agent 1,17.03 9003-03 (REV 1/02) DAVID B. MYERS

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	I agree to comply with all conditions or stipulations of the various dureaux of department noted on this application, and attached statements of conditions or stipulations, which are hereby made a part of this application.	1
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OFFICIAL COPY	APPROVED DEPARTMENT OF BUILDING INSPECTION ISPECTION FEB 1 1 2004 FRANK Y. CHIU, DIRECTOR APPLICATION FOR BUILDING PERMIT ADDITIONS, ALTERATIONS OR REPAIRS FORM 3 DOTHER AGENCIES REVIEW REQUIRED FORM 8 DOVER-THE COUNTER ISSUANCE ZHMANUMBER OF PLAN SETS DATE FIFD FUNG HE RECEPT IND I 20/04 SSUED I 20/04 JU/7043 Z(1/04)	H ST 4508/00 LOUP 100 LOUP 10
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	IMPORTANT NOTICES No change shell be mede in the character of the occupancy or use without hist obtaining a Building Permit authorizing such change. See San Francisco Building Code and San Francisco Housing Code No portion of building or structure or scatfolding used curing construction. Is be closer than 60° to any write containing more than 750 vidts See See 325 Cationia Planal Code	NOTICE TO APPLICANT HOLD HARMLESS CLAUSE. The permitting(s) by acceptance of the permit, agren(s) to indemnivly and hold harmless the City and County of San Francosco thom and against any and all claim, demands and accors for damages resulting from operations under this permit, regardless of negligence of the City and County of San Francosco, and to assume the defense of the City an County of San Francosco against all such claims, demands or actions
	Pursuant to San Francisco Building Codo, the building permit shall be posted on the job. The owner is responsible for approved plans and application being kept all building ste	In conformity with the provisions of Section 3600 of the Labor Code of the State of California, the appricant shall have coverage under (1), or (II) desugnated below or shall indicate stem (III), or (M), or (M), which ever is applicable. If however item (1) is checked item (IV) must be checked as well. Mark the
	Grade lines as shown on drawings accompanying this application are assumed to be correct. If actual grade lines are not the same as shown revised drawings showing correct grade lines, outs and this together with complete oealas of relativity will and will footings required must be	appropriete metricol of compliance below. I hereby aftirm under penalty of perjury one of the following declarabons.
•	Eubmitted to the department for approved Any Stipulation required herein or by code may be appealed	() I have and will maintain a cartificate of consent to self-insure for workers compensation, as provided by Section 3700 of the Labor Code, for the performance the work for which this operant is assed.
	BUILDING NOT TO BE OCCUPED UNTIL CERTIFICATE OF FINAL COMPLETION IS POSTED ON THE BUILDING OR PERMIT OF OCCUPANCY GRANTED, WHEN REQUIRED	E I have and will maintain workers' compensation insurance, as required by Section 3700 of the Labor Code, for the performance of the work for which this permit is issued. My workers'
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	issoed In drefings all insulating materials must have a clearance of not less than two inches from all electrical write or squapment.	IV I certify that in the performance of the work for which this permit is issued, I shall not employ any person in any marine so as to become subject to the workers compensation laws of Certiforms. If until acknowledge that I understand that in the event that I should become
		subject to the workers' compensation provisions of the Labor Code of Caltorina and last to comply forthwith with the provisions of Saction 3800 of the Labor Code, that the permit here in applied to shall the determined revolved.
~		(V I certify as the owner (or the agent for the owner) that in the performance of the work for which this permit is issued, I will employ a contractor who complies with the workers'
	APPLICANTS CERTIFICATION IHEREBY CERTIFY AND AGREE PATIFA PERMIT IS ISSUED FOR THE CONSTRUCTION DESCRIBED IN THIS APPLICATION, ALL THE PROVISIONS OF THE PERMIT AND ALL LAWS AND ORDINANCES THERETO WILL BE COMPLIED WITH	Conception layer Graftions and who, pion to the commencement of any work, will be a commencement of any work, will be a contrained commencement of any work, with the contrained commencement of any work, will be a contrained commencement of any work, with the a contrained commencement of any work, with the contrained commence
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- 	APPLICATION FOR BUILDING PERMITSPECTION CITY AND COUNTY OF SAM FRANCISCO ADDITIONS, ALTERATIONS OR REPAIRS FORM 3 SI OTHER AGENCIES REVIEW REQUIRED PERMISSION TO BUILDIN ACCORDANCE WITH THE PLANS
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F	Code. No portion of building or structure or scatfolding used during construction, to be closer than 60' to any write containing more than 750 volts See Sec 385, California Penal Code. Pursuant to Same Section 3800 of the Labor Code of the State of California, the applicant bala have coverage under (i), or (ii) designated below or shall indicate item (III), or (IV), or (IV) must be checked as well, Mark the applicant bala have coverage under (ii), or (ii) designated below or shall indicate item (III), or (IV), or (IV), must be checked as well, Mark the applicant bala have coverage under (ii), or (iii) designated below or shall indicate item (III), or (IV), or (IV) must be checked as well, Mark the applicant below or shall indicate item (III), or (IV), or (IV) must be checked as well, Mark the applicant below or indicate. It how were item (V) as checked as well, Mark the applicant below well have the mark the applicant below were item (V) as checked as well, Mark the applicant below well have the mark the applicant below well have (IV) and the state of the
	Grade lines as shown on drawings accompanying this application are assumed to be correct. If actual grade targs are not the same as shown revised drawings showing correct grade lines, cuts and fills together the associate as shown in the same as shown revised drawings and wall footings required must be submitted to the department for approval. Any STIPULATION RECURED HEREIN OR BY CODE MAY BE APPEALED. BUILDING NOT TO BENCCUPIED UNTIL CERTIFICATE OF FINAL COMPLETION IS POSTED ON THE BUILDING OF PERMIT OF OCCUPANCY GRANTED, WHEN REQUIRED. It have and will maintain a certificate of consent to self-insure for worker's compensation insurance, as required by Section 3700 of the Labor Code, for the performance the work for which this permit is issued. It is the are and will maintain workers' compensation insurance, as required by Section 3700 of the Labor Code, for the performance of the work for which this permit is issued. It is the and will maintain workers' compensation insurance, as required by Section 3700 of the Labor Code, for the performance of the work for which this permit is issued. My workers' compensation insurance carrier and policy number are:
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	CONTRACTOR DENGINEER APPLICANT'S CERTIFICATION HEREBY CERTIFY AND AGREE THAT IF A PERMIT ENSUED FOR THE CONSTRUCTION DESCRIBED IN THIS APPLICATION, ALL THE PROVISIONS OF THE PERMIT AND ALL LAWS AND ORDINANCES THERETO WILL BE COMPLED WITH. 9003-03 (REV. 1/02) CRIGGINIAL

OFFICIAL COPY SAM FRANCISCO CONDITIONS AND STIPULATIONS REFER APPROVED: DATE: -.TO: REASON: DEPARTMENTOF BUILDING INSPECTION BUILDING INSPECTOR, DEPT. OF BLDG. INSP NOTIFIED MR. APPROVED: per plane and work stated. No change of use authorized under this appeared. DATE: REASON CATEGORICALLY EXEMPT FROM ENVIRONMENTAL REVIEW 8.17.0 NOTIFIED MR. APPROVED: DATE: والإ - المسرمة الأر ملاققة REASON: 414 282 BUREAU OF FIRE PREVENTION & PUBLIC SAFETY HOLD NOTIFIED MR DATE: APPROVED: 7-1-4-SECTION REASON: ζ_{2}^{-1} / KACK STOR TO BAC Щ MECHANICAL ENGINEER, DEPT OF BLDG. INSPECTION NOTIFIED MR PA APPROVED: DATE: rr C REASON: AND NAMES OF 11 2 1 FI-1- A. CIVIL ENGINEER DEPT OF BUDG INSPEC NOTIFIED APPROVED PRE-CONSTRUCTION SITE 7 MEETING REQUIRED BY PERSONS REQUIRED PRIOR TO DEI FINAL -DPW/BSM STREET INSPECTION 6. 60.1 GALL 554-7149 ROBEREDUL LON 3. 5 Call (415) 554-7149 To Schedule E. White Mar TIFIED NOTIFIED MR. APPROVED: DATE DURING PROCE REASON: 62412 1.17-DEPARTMENT OF PUBLIC HEALTH NOTIFIE APPROVED: 制水因 DATE: REASON: REDEVELOPMENT AGENCY NOTIFIED MR. APPROVED: DATE: REASON: HOUSING INSPECTION DIVISION NOTIFIED MR. I agree to comply with all conditions or stipulations of the statements of conditions or stipulations, which are berefore noted on this application, and attached Number of attachments

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	i agree to comply with all conditions or stipulations of the various bureaus or department noted on this application and a	
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		I agree to comply with all conditions or allputations of the various bureaus or department noted on this application, and attack statements of conditions or allputations, which are hereby made a part of this application Number of attachments	Ned

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		I agree to comply with all canditorie or interletations of the verticus bureaux of department noise on this application, and attached attalaments of conditions or algorithment, which are trendy made a part of the application.	0.
		Number of attachmania	



•		ADDRESS	OFJOB			BLOCK	LOT •	APPLIC	ATION NO.	
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••		OWNER	AME						TELEPHONE	
MART	ÍŃĊ.	LEVIN	INVESTME	NT CC).			(415	5)397-0	373
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9003-15

PERMIT INSPECTION RECORD DEPARTMENT OF BUILDING INSPECTION CITY AND COUNTY OF SAN FRANCISCO BUILDING INSPECTION JOB CARD





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	APPLICATION IS HEREBY MADE TO THE DEPARTMENT OF BUILDING INSPECTION OF SAN FRANCISCO FOR PERMISSION TO BUILD IN ACCORDANCE WITH THE PLANS AND SPECIFICATIONS SUBMITTED HEREWITH AND ACCORDING TO THE DESCRIPTION AND FOR THE PURPOSE HEREINAFTER SET FORTH.
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PERMIT INSPECTION RECORD DEPARTMENT OF BUILDING INSPECTION CITY AND COUNTY OF SAN FRANCISCO BUILDING INSPECTION JOB CARD

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OWNERS AUTHORIZED AGENT



555	19TH	ADDRESS OF JOB			вьоск 4058			ITION NO. 03162208
) MYER	OWNER NAME						TELEPHONE
ESTIMAT	ED COST	FILE DATE	DISPOSITION	DISPOSITION	DATE	PERM	AT NO.	EXPIRATION DAT
\$100.0	000	03/16/11	ISSUED	03/22/	11 20	11031	62208	03/16/12
FORM CC	NST. TYPE	OCCUP	ANCY CODES	PLANS	STORIES	UNITS		DISTRICT
3	3	B.S-1		2	З	0	8	BID-INSF
	C	ONTACT NAME					Π	ELEPHONE
7UM ¹ S	MECHA	NICAL CO	INC				415	6421818
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SPECIAL	INSPECTIONS	NO	FIRE ZONE NO	0				
SPECIAL USE DISTRICT				5				
			PENALTY NO	o — c		COMPLIAN	CE WITH REPOR	T8
NOTES:								

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9003-15

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PERMIT INSPECTION RECORD DEPARTMENT OF BUILDING INSPECTION CITY AND COUNTY OF SAN FRANCISCO BUILDING INSPECTION JOB CARD

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DATE FLED FLING FRE RECEPT INO. 11) STREET ADDRES 5555 PERMIT NO 1235465 FLING FRE RECEPT INO. 11) STREET ADDRES 123555 12355 13555 14737 1507	APPLICATION IS HEREBY MADE TO THE DEPARTMENT OF BUILDING INSPECTION OF SAN FRANCISCO FOR PERMISSION TO BUILD IN ACCORDANCE WITH THE PLANS AND SPECIFICATIONS SUBMITTED HEREWITH AND ACCORDING TO THE DESCRIPTION AND FOR THE PURPOSE HEREINAFTER SET FORTH. HEREINAFTER
DESCRIPTION OF B	
TILA STORIES OF Z LASSINGHTS OF COLUMNARY INTO STORIES AND GELLARS:	fice Storage B/5-1 UNITS OF
TO BE CONSTRUCTED YES C BE USED DURING OR ALTEREDY NO C CONTRACTOR (14) GENERAL CONTRACTOR ADDRESS	YES WORK TO BE TES III WORK TO BE TES NO YES PERFORMED7 NO PERFORMED7 NO D D PERFORMED7 NO D
(15) OWNER LESSEE (CROSS OUT ONE) LA AUDITES	1/18, CL 94030 (6) Utt 38/8 8/701 3 /21/2013 10
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(17) DOES THIS ALTERATION CREATE ADOPTIONAL HEIGHT YES ON STORY TO BUILDING? NO CENTER LINE OF FRONT	FT (19) DOES THES ALTERATION (20) IF (16) IS YES, STATE CREATE DECK OR HORIZ YES NEW GROUND FT EXTENSION TO BULDWIG? NO
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(25) ANCHITECT OR ENGINEER (DESIGN C CONSTRUCTION C)	ADDRESS CALIF CERTIFICATE NO.
(28) CONSTRUCTION LENDER (ENTER NAME AND BRANCH DESKANTON F ANY, F THERE IS NO KNOWN CONSTRUCTION LENDER, ENTER "UNKNOWN")	ADDRESS
IMPORTANT NOTICES No change shall be made in the character of the occupancy or use without first obtaining Permit authoriting such change. See Sen Fendesco Building Code and Sen Francisco H Code. No portion of building or structure or acaficiding used during construction, to be closer it any wire constaining more than 750 volta See Sec 385, California Penal Code. Pursuent to Sen Francisco Building Code, the building parmit shall Code. Pursuent to Sen Francisco Building Code, the building parmit shall be posted on the job. comer to maponeble for approved plane and application being text at building shall. Cardia these as shown on deserting a accompanying this application ere assumed to be co	Outers The splatter Incomposition County of the Francisco from and against any and air classification of the City and classification of the City and classification of the City and County of San Francisco, and to assume the defense of the City and County of San Francisco, and to assume the defense of the City and County of San Francisco, and to assume the defense of the City and County of San Francisco, and to assume the defense of the City and County of San Francisco, and to assume the defense of the City and County of San Francisco, and to assume the defense of the City and County of San Francisco against all such claims, demand or and county of San Francisco against all such claims, demand or an advect the splatence. It however them (Vi) is charked there (Vi) and the classification (R), or (V), or (V).
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ABOVE QUESTIONS (10) (11) (12) (13) (22) OR (24). THS IB NOTA BUILDING PERMIT, NO WORK SHALL BE STATTED UNTIL A BUILDING. ISSUED.	PORcy Number
In developings all insulating metarials must have a clearance of not less than two inches ho electrical when or equipment. CHECK APPROPENTE BOX	
	herein applied for what be deemed revolved. () V. I certify as the owner (or the agent for the owner) that in the performance of the work for
APPLICANT'S CERTIFICATION I HEREBY CERTIFY AND AGREE THAT IF A PERMIT IS ISSUED FOR THE CONSTRUCT DESCRIBED IN THIS APPLICATION, ALL THE PROVISIONS OF THE PERMIT AND ALL L AND ORDINANCES THERETO WILL BE COMPLIED WITH	115 124holl
9003-03 (H€V/ 1/02)	Stand of Account of Account Operation of Account

	REFER	APPRIOVED:	
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		BUILDING INSPECTOR, DEPT, OF BLDG. IMSP.	
		APPROVED:	DATE:
			REASON:
		APPROVED:	DATE:
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		AT THE START DE WORK (#15) C58 3300	
		BUREAU OF FIRE PREVENTION & PUBLIC BAFETY APPROVED:	NOTIFIED MR.
			DATE:
		MECHANICAL ENGINEER, DEPT OF BLDG. INSPECTION	NOTIFIED MR.
		APPROVED:	DATE:
			REASON:
		CIVIL ENGINEER, DEPT. OF BLDG INSPECTION APPROVED:	NOTIFIED MR.
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		BUREAU OF ENGINEERING	NOTIFIED MR.
		APPROVED:	DATE:
			REASON:
	<u> </u>	DEPARTMENT OF PUBLIC HEALTH	NOTIFIED MR.
	[]		REASON:
			NOTIFIED MR.
		APPROVED:	DATE:
		HOUSING INSPECTION DAVISION	
		I scree to comply with all conditions or stipulations of the various bureaus or department noted on this application, and stac	NOTIFIED MR.
		statements of conditions or allputations, which are hereby made a part of this application.	
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City and County of San Francisco 1660 Mission Street San Francisco, CA 94103-2414

Date: 04/20/20 16:12:07

Permit details report

Application Number: 201105236573

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Form Number: 8 Application Installation of fire sprinkler monitoring and elevator recall system **Description:**

Address: 4058/009/0 555 19TH ST

Cost: \$3,000		Occupancy code:	B,S-1	Buildir Use:	ng 10	OFFICE
Disposition/Sta	ige:					
Action Date	Stage		Comments	· ··· ·		
23-MAY-2011	TRIAGE					
23-MAY-2011	FILING					
23-MAY-2011	FILED					
23-MAY-2011	APPROVED	•				
23-MAY-2011	ISSUED					
23-JUL-2012	COMPLETE	JOB FINA	LED BY FIRE			

Contact Details:

Contractor	Details
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License No. :	893093
Name:	KUNG KAY CHIU
Company name:	K K ELECTRIC & ALARM COMPANY
Address:	423 BROADWAY MILLBRAE CA 94030-0000

Phone:

Addenda Details:

Description:

Step # Station	Arrive Date Start Date	In Hold Out Hold	Finish Date Plan Checked by	Hold Description
1 INTAKE	23-MAY-2011 23-MAY-2011		23-MAY-2011 VALLE JAIME	· · · · · · · · · · · · · · · · · · ·
2 SFFD	23-MAY-2011 23-MAY-2011		23-MAY-2011 CURD JOHN	
3 CPB	23-MAY-2011 23-MAY-2011		23-MAY-2011 SHAWL HAREGGEV	

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555	19 T I	ADDRESS (DF JOB			в.оск/ 4058,			3236781
LE	VIN MAI	OWNERN		LLC					TELEPHONE 3397-0373
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FORM	CONST. TYPE		OCCUPANCY COD	EŜ	PLANS	STORIES	UNITS		DISTRICT
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OFFI	CE			01-20-	-4340	FOR F	INAL	COMPLE	TION.
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	BUILDING POPPACTOR, CAPT, OF ILDG, HAP	NOTIFIED MR
	APPROVED /	
		DATE REASON [®]
+	DEPARTMENT OF CITY PLANNING /	NOTIFIED MR.
		DATE REASON
	BURBAU OF PARE PREVENTION & PUBLIC BAPETY	NOTIFIED MR
	APPROVED	DATE
		REASON
	MECHANICAL ENGINEER, DEPT OF BLDG. INSPECTION	NOTIFIED MR
	APPROVED	DATE
		REASON
	CIVIL ENGINEER, DEPT OF ILDG INSPECTION	NOTIFIED MR
	Call 415-558-657) to schedul- inspections for building, elac.rich and / or plumbing This photocolor is approved without, the inspection	DATE REASON ,
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	t agree to comply with all conditions or stipulations of the vertous burnous or department noted on this application, and stad statements of conditions or stipulations, which are hordby made a part of the application	



_ ADDRESS OF	JOB		BLOCKLOT	. APPLICA	TION NO.
555 19TH ST			4058/0	09 20120	6142644
OWNER NAM	Ĕ				TELEPHONE [
	NV CO LI				317-4670
ESTIMATED COST FILE DATE	DISPOSIT	ION DISPOSITIO	NDATE	PERMIT NO.	EXPIRATION DATE
\$60.000 06/14/1				06142644	06/09/13
FORM CONST. TYPE OC	CUPANCY CODES	PLAY	IS STORIES (INITS	DISTRICT
8_ <u>3</u> B		Q	3 (Q 8	BID-INSP
CONTACT NAME	-			וז	ELEPHONE
WESTCO ROOFING CO.	INC.			510	5335270
DESCRIPTION/BLDG. USE	REF	ROOFING	OTHE	R DESCRIPTION	
OFFICE					
SPECIAL INSPECTIONS? NO	FIRE ZONE	NO			
SPECIAL USE DISTRICT	TIDF	NO			
	PENALTY	NO -	00	MPLIANCE WITH REPOR	TS
NOTES:					
			c	PERMIT INSPECTIO	NRECORD
9003-15				RTMENT OF BUILD	

PERMIT INSPECTION RECORD DEPARTMENT OF BUILDING INSPECTION CITY AND COUNTY OF SAN FRANCISCO BUILDING INSPECTION JOB CARD

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	WORK COMPLETED. FINAL CERTIFICATE ISSUED
APP. NO.	diana Non.Th
2012	00142844
	BUILDING INSPECTOR

COPERATINENT OF BUILDING INSPECTION WWANL DAY DIRECTOR/CHIEF BUILDING INSPECT	FIRE SFFD INSP. ONLY REC.
ВОЛО ВНИ 1 2012 ПОЛИ ПЕ ПЕСИТИО. (1) БТИЕТ АСОЛЕВИ 6/16/172 РЕМИТ КО. 555 РЕМИТ КО. 1269073 INFORMATION TO	CITY AND COUNTY OF SAN FRANCISCO DEPARTMENT OF BUILDING INSPECTION APPLICATION IS HEREBY MADE TO THE DEPARTMENT OF BUILDING INSPECTION OF SAN FRANCISCO FOR PERMISSION TO BUILD IN ACCORDANCE WITH THE PLANS AND SPECIFICATIONS SUBMITTED HEREWITH AND ACCORDING TO THE DESCRIPTION AND FOR THE PURPOSE HEREINAFTER SET FORTH. MITE ABOVE THIS LINE V SIT 28 STOLE TO JUN STREET ADDITION AND FOR THE PURPOSE HEREINAFTER SET FORTH. MITE ABOVE THIS LINE V SIT 28 STOLE TO JUN STREET ADDITION AND FOR THE PURPOSE HEREINAFTER SET FORTH. MITE ABOVE THIS LINE V SIT 28 STOLE
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City and County of San Francisco 1660 Mission Street San Francisco, CA 94103-2414

Date: 04/20/20 16:10:51

Permit details report

Application Number: 201206182832

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Application Modify permit issued under #2011-0523-6513 1. Eliminated smoke & heat detector& heat detector Form Number: 8 **Description:** unside elevator maching room. 2. Relocate FACP to 3rd street side.

555 19TH ST Address: 4058/009/0

Cost: \$1		Occupancy code:	B,S-1	Building Use: ¹⁰ OFFICE
Disposition/Sta	ige:			
Action Date	Stage		Comments	
18-JUN-2012	TRIAGE			
18-JUN-2012	FILING			
18-JUN-2012	FILED			
11-JUL-2012	APPROVED	- · · · · ·		
11-JUL-2012	ISSUED			· · ·
27-JUL-2012	COMPLETE	JOB FINAI	LED BY FIRE	
Contact Details	3:			
Contractor Det	ails			

License No. :	893093
Name:	KUNG KAY CHIU
Company name:	K K ELECTRIC & ALARM COMPANY
Address:	423 BROADWAY MILLBRAE CA 94030-0000

Phone:

Addenda Details:

Description:

Step	# Station	Arrive Date	Start Date	In Hold	Out Hold	Finish Date	Plan Checked by	Hold Description
1	INTAKE	18-JUN-2012	18-JUN-2012				YU JOSEPH	
2	SFFD	18-JUN-2012	18-JUN-2012			18-JUN-2012	CURD JOHN	
3	СРВ	11-JUL-2012	11-JUL-2012			11-JUL-2012	YU ZHANG REN	
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OWNERS AUTHORIZED AGENT



ADDREBS OF JOB	· .	BLOCKLOT	APPLICATION NO.
555 19TH ST		4058/009	201302200511
OWNER NAME	00 110		TELEPHONE (415)397-0373
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9003-15	UPILOT	DEPARTMEN CITY AND C	TINSPECTION RECORD IT OF BUILDING INSPECTION OUNTY OF SAN FRANCISCO 3 INSPECTION JOB CARD

OFFICIAL COPY SAM FRANCISCO **BUILDING INSPECTORS JOB RECORD** DATE <u>de</u>pārt where ch On to carr 10 () <u>4 ||8||3</u> Hoor nail 713 Eu ch WORK COMPLETED PINAL CERTIFICATE ISSUED APP. NO. 201302200511 BUILDING INSPECTOR

OFFICIAL OF DEPARTMENT OF BUILDING INSPECTION		DEC 1 1 2013	DEC 1 1 2013 Approved for issumce	HUDA. 3/8 JUDI3-
APPLICATION FO		OF BUILDING INSPECTION		13-12-05-340
ADDITIONS, ALTER	ATIONS OR REPAIRS	DEPARTMENT OF BUI APPLICATION IS HEREBY MADE	TO THE DEPARTMENT OF	-340
	FPLAN SETS SB	BUILDING INSPECTION OF SAN PERMISSION TO BUILD IN ACCO AND SPECIFICATIONS SUBMITT ACCORDING TO THE DESCRIPTI HEREINAFTER SET FORTH.	RDANCE WITH THE PLANS ED HEREWITH AND ON AND FOR THE PURPOSE	APPROVAL MANBER:
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MAY THE OF CONSTR. SAY IND. OF TTL - B STORES OF OCCUPANTY.	(BA) INC. OF BASEMENTS AND GELLARS: / (7A) PRESENT USE BASEMENTS	S-1 BUSINESS	B,S-/ (M)	
	DESCRIPTION OF BUI (1) 10.0F MEDIATRI MOCELURS: (7) PROPOSED LSR (7) PROPOSED L		ATTION (1) CLASS (1) CT	
(14) 13 AUTO RESERVAY TO BE CORESTRUCTED OR ALTERED?	YES CI AF USED DURING NO 301 CONSTRUCTION?	YES CI WORK TO BE NO 25 PERFORMENT	YEB UM (13) FULMATING WORK TO BE NO V PERCHANEZ?	YE
(14) BENERAL CONTRACTON KILEYAYO (16) OWNER-LESSER CROSS OLF OWD	SO 97 MBVD AV	E. BALY UTY, CA 415-	CAUFLIEDO. ERPRATEM 606-1406 90015 RC# PROVE (FOR CONTACT BY DEP1.)	6 4/32/14
PAVID MYER.	S (58 HUWAL	1 57 - 14105	415-397-03	73
BREAK AF	EA PENSION -	DERMIT 2013	02200511	
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and the support site and states at a second s	perging this application are assumed to be come real-minut descinge abusing correct grade train againing walks and out fundings required must be over the state of a source of the sourc	Cuts I hundry allow under panalty of page () 1 I have and will maintain		compensation, ee It for which this
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WEING OR PLUMMING INTALLATEDIS	i Noti Constituite an Apprional for the El A Befarate penalt for the Wrinks and P Is are received by Answer B "Yes" to any 2) or (ng.	LINGING	t carrier and policy number and	
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AND ORDNWACHS THERETO WALL BE 9008-03 (REV. 1,00)		Bigmans of Applicate or Apprent		

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		,	REASON:
		NA	
		DEPARTMENT OF OTY PLANNING	NOTIFIED MR.
	APPROVED;		
			DATE:
·*·····		BUREAU OF FIRE PREVENTION & PUBLIC BAFETY	NOTIFIED MR.
	APPROVED:		DATE:
			REASON:
·	APPROVED:	MECHANICAL ENGINEER, DEPT OF BLDG. INSPECTION	NOTIFIED MR.
			DATE:
			nehourt.
		CIMIL IENGINEER, DEPT OF BLDG INSPECTION	NOTIFIED MR.
	APPROVED:		DATE:
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			nexact.
		DEPARTMENT OF PUBLIC HEALTH	NOTIFIED MR.
	APPROVED:		DATE:
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	APPROVED:		NOTIFIED-MR.
			DATE: REASON:
·		100 12/5/13	
	· · ·	HOUSING INSPECTION DAYSICH	NOTIFIED MR.
) agree to comply with all conditions (statements of conditions or allocation	or algulations of the various bureaus or department noted on this application, and atlact na, which are handly made a part of this application.	ed .
	Number of electronicity	OWNERS AUTHORIZED AGENT	_



ADDRESS OF JOB			BLOCKA	മ് _	APPLICAT	ion no.	
555 19TH ST			4058	/009		2053404	
ÓW MÉR NAME					•	ELEPHONE	
DAVID MYERS				PERM		<u>) 397-03</u> Experimentation	
ESTIMATED COST FLE DATE	DISPOSITION	DISPOSITION D/				CALINALINA	IDAIC
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8 <u>3 B.S-1</u>		2	3	0	8	BID-IN	SP
CONTACT NAME					151	EPHONE	
SIMPLY BUILDING INC						6061406	
DESCRIPTION/BLDG. USE	REVIS	ION TO	COUN	LER A	ND' CAE	INETRY	OF
OFFICE	BREAK	AREA (DF PA	# 201	302200	511.	
SPECIAL INSPECTIONS? NO	FIREZONE NO						
SPECIAL USE DISTRICT	TIDF NO		•				
	PENALTY NO			COMPLIANC	E WITH REPORT	9	
NOTES:							-
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9009-15	-					IG INSPECTION	
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ISPECTION DEC ST 2015	SFFD INSP. FEES REQ. ONLY	PORN 3 B
TOM C. HUI. S.E. DIRECTOR		NHUCE
DEPT. OF BUILDING INSP		
APPLICATION FOR BUILDING PERM ADDITIONS, ALTERATIONS OR REPA		
	PERMISSION TO BUILD IN ACCORDANCE WIT	THE PLANS
FORM 8 OVER-THE COUNTER ISSUANCE	AND SPECIFICATIONS SUBMITTED HEREWITH ACCORDING TO THE DESCRIPTION AND FOR	THE PURPOSE
NUMBER OF PLAN SETS	HEREINAFTER SET FORTH. DT WRITE ABOVE THIS LINE ▼	1 AND THE PURPOSE 9/ 9/
DATE FILED FILMA FEE RECEIPT NO. (1) STREET A		NUM
12-7/13 55	5 19749T. (200 \$384FL) 4058/00	
PERMITING. 24 ESTIMAT 3/3382. 2/31/13 # 9	252 ee (925) Surl	
	TO BE FURNISHED BY ALL APPLICANTS	
LEGAL D	ESCRIPTION OF EXISTING BUILDING	
(44) TYPE OF CONSTR. (34) INC. OF TILL B STORES OF 3 INACOF MASCHENTS / (7A) PRE	OPFACE E	P. CLASE (RA) NO. OF DWELLING
DESCRIPTION O		10)
TIL B OCCUPANCY 2 AND CELLARS:	OTTIE	VIELING 7 DWELING 9 UNITS:
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(14) CENERAL CONTINACTOR ADDRESS	PHONE CALIFICINO	EXPERATION DATE
(15) OWNER - LABORE (JEDGS OUT COR) ADDRESS	ZP BTRC# PHO	9-30-14 NE (FOR CONTACT BY DEPT.)
TAG WATTE W GESCHETTEN OF ALL WORK TO BE PERFORMED LADOR THE APPLICATION OF	ST HOWARD SI-SF 94114 (415) ERENCE TO PLANS IS NOT SUFFICIENT)	781-0603
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	2560475) 25LOUATE)	
3 ²⁰ Floer= (4 + 00) (3 1		2
	ADDITIONAL INFORMATION	3-702-20-0511
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(21) WILL SIDEWALK OVER SUB-BIDEWALK SPACE BE YES C (22) WILL BUILDING EXTEND BEYOID	(23) ANY OTHER EXISTING BLDG.	LOOR AREA SO, FT. Desitive a change yes
REPARED OR ALTERED? NO CASE PROPERTY LINE? (25) ARCHTECT OR ENGINEER (DESUBIC) CONSTRUCTION ())	ADDRESS	FOCCUPANCY? NO CR
STANLES SATTO WITZI 10221 200 CONSTRUCTION LENGER POTTER NAME AND BRANCH DESIGNATION F ANY,	ATTOMA ST. #4 SF- G.9410.	3~
# THERE IS NO KNOWN CONSTRUCTION LENDER, ENTER "UNKNOWN")	SNOW /	
IMPORTANT NOTICES No change wheth he made in the changes of the occupancy or use without first obta	Ing a Building HOLD HARMLESS CLAUSE. The permittee (a) by acceptar	OF OF THE DENTHE, ECHERALISE IN Inclements, and
Parmit authoriding auch change. Bas San Francisco Building Code and San Francis Code. No portion of building or structure or sostibilding used during construction, to be clo	o nousing noto namines the city and county of isan Francisco from a actions for damages resulting from operations under this p of then attrian. County of San Francisco, and to assume the defense of the	nd against any and all claim, demands and armit, recerdings of nacionance of the City and
Bitly whe containing more than 780 volts See Sec 385, California Panal Code. Pursuant to Ben Francisco Building Code, the building permit shall be posted on the	Juch Carms, Cernence of Ecolory.	or Code of the State of California, the
commer la responsible for approved plans and application baing laspt at building etc. Grade lines as shown on drawings accompanying this application are assumed to b addual grade lines are not the same as shown revised classings showing correct gra	whichever is applicable. If however learn (V) is chacked iter correct. If appropriate method of compliance below, a linear or de	n (IV) must be shecked as web. Mark the
and file together with complete details of retaining wells and well footings required n submitted to this department for approvel.	ant be I hereby stilling under penalty of perjury one of the tollowing (} L. I have and will maintain a certificate of consent provided by Section 3700 of the Labor Code, it	to bell-installs for tetricaria companyation, as
ANY STIPLIATION RECURRED HEREIN OR BY CODE MAY BE APPEALED. BUILDING NOT TO BE OCCUPIED UNTL CERTIFICATE OF FINAL COMPLETION I ON THE BUILDING OR PERMIT OF COCUPANCY GWANTED, WHEN FICURED.	POSTED (). I have and will maintain workant compensation	Philosophies and packaged by Revenue works of the
APPROVAL OF THIS APPLICATION DOES NOT CONSTITUTE AN APPROVAL FOR WIRING OR PLUMENCI HISTALLATIONS. A BEPARATE PERMIT FOR THE WIRING	HE ELECTRICAL Code, for the performance of the work for Ompervision insurgoe carrier and pelloy nam IND PLUMGING	f which this narrow is been and the second
MUST BE OSTAMED, BEPARATE PERMITS ARE REQUIRED 3" ANSWER 18 "YEB" 1 ABOVE GUESTIONS (10) (11) (12) (13) (22) OH (24).	Party OF Dency Number A09290	455
THER IS NOT A SULLDING PERMIT, NO WORK SHALL BE STARTED UNTIL A BUILD ISSUED. In develops all insulating materials must have a clearance of not lass than two inche	 IV. I certify that in the performance of the work for: 	which this permit is leaved, I shall not employ
Nichigi what or equipment. CHECK APPROPRIATE BOX	California. I further animowiedge that I underst subject to the workard companyation provision	and that in the event that I should become is of the Labor Code of California and laters
Dimien Dianchitect Diebree Diagent Matchitractor Diekreen	comply for thinkin with the provisions of Section hursen applied for shall be dearned revolued. () V. Loarity as the center for the agent for the owne	n that in the conformance of the work de-
APPLICANT'S CERTIFICATION	which this parries is second. I will employ a cont comparation issue of California and the prior	solor who complies with the workers' to the commencement of any work, will be a cost-Revenue.
I HEREBY CERTERY AND AGREE THAT IF A PERMIT IS BISLED FOR THE CONSTR DEBCRIBED BY THIS APPLICATION, ALL THE PROVIDENCE OF THE PERMIT AND A AND ORDINANCES THERETO WILL BE COMPLIED WITH.	LAWS COMPANY	12-31-13
8003-03 (782Y, 1702)	Stefanur of Applicant of Appre	Deta

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	d County of San Franc Ission Street San Fran		Date: 04/20/20 16:05:44
EPARTMENT UILDING INSPECT		Permit details repor	t
Application Num	ber: 201312315325	5	
Form Number:	Description: 2ND 3RD	D & RELOCATE FIRE SPRINK D FL: (8 ADD), (3 RELOCATE). D FL: (4 ADD), (3 RELOCATE) D PA# 201302200511	LER HEADS PENDANT IMPROVEMENT
4058	/009/0 555 19TH ST	ſ	
Cost:		2	D ucit dia a
\$9,252		Occupancy code: ^B	Building Use: ¹⁰ OFFICE
Disposition/Sta	ige:		
Action Date	Stage	Comments	S
31-DEC-2013	TRIAGE	• • • • • • • • • • • • • • • • • • • •	
31-DEC-2013	FILING		
31-DEC-2013	FILED		
31-DEC-2013	APPROVED		
	ISSUED	··· ·	
31-DEC-2013	1000000		

Contractor Details

License No. :	392967
Name:	PETER MCHUGH
Company name:	: AYOOB & PEERY PLUMBING
Address:	975 INDIANA ST SAN FRANCISCO CA 94107

Phone: 4155500975

Addenda Details:

Description:

Step # Station	Arrive Date Start Date	In Hold Out Hold	Finish Date Plan Checked by	Hold Description
1 INTAKE	31-DEC-2013 31-DEC-2013	··· •	31-DEC-2013 YU ZHANG REN	





Date: 04/20/20 16:05:44

Permit details report

201312315325

Step #	Station	Arrive Date	Start Date	In Hold	Out Hold	Finish Date	Plan Checked by	Hold Description

2	SFFD	31-DEC-2013	31-DEC-2013		31-DEC-2013	WEISS-ISHAI SAGI
3	СРВ	31-DEC-2013	31-DEC-2013	•	31-DEC-2013	KARCS EVELYN

Page 2

OFFICIAL COPY	APPLICA ADDITION FORM 3 0 FORM 3 0 FORM 8 0 FORM 9 FOR FORM 7 FORM	IS, ALTEI	(MA) NO. OF AND CELLARS: DESCE (6) NO. DF BASEMENTS AND CELLARS: YES D (11) WILLS YES D (11) WILLS ADDRE LUSERS 6/	A REQUIRED JANCE 10 - 20 - 14 ▼ DO NO ▼ DO NO ■ DO	C DI APPLIC BUILDU PERMIS ACCOF HEREIN ACCOF HEREIN TWRITE AI F JOB TH ST G-JOB CF JOB CF JOB		Here: S.E.	THE DEPA ANCESCO F DANCE WITH HEREWITH N AND FOR NO D FOR ANTS (a) (ARTMENT OF OR H THE PLANS H AND THE PURPOS THE PURPOS THE PURPOS THE PURPOS COUP. CLASS $B_{1}S - 1$ COUP. CLASS $S_{2}S - 1$ COUP. CLASS COUP. CLA		YES DC NO CL DATE - 3 /-/5
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	(17) DOES THIS ALTER CREATE ADDITION	AL HEIGHT	YES [(15) IF (17) I	S YES, STATE GHT AT		(19) DOES THIS CREATE DECK O	ALTERATION R HORIZ		20) (F (19) (S YES, S NEW GROUND	TATE	
	(21) WILL SIDEWALK O SUB-SIDEWALK SI	WER	NO CENTER YES C (22) WILL BU EXTEND		YES CI	EXTENSION TO	BUILDING? 1 EXISTING BLDG.	NO DA YES DU	FLOOR AREA (24) DOES THIS ALTI		SQ. FT. YES D
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	(26) CONSTRUCTION LE	NOER (ENTER N	AME AND BRANCH DESIS	NATION IF ANY.	<u></u>		ADDRESS		6327	14	
	enthorizing such change, 8 No portion of building er st containing more than 750 v Pursuant to San Prancisco i	the cheracter of the see San Francisco I nucleure or acasticidi colta. San Sac 386, Building Code, the I	RTANT NOTICES he occupency or use without building cade and iten Freed hig used during occutraction California Penal Code. building permit shail be peet in being hept at building sha.	first obtaining a Building P soo Housing Code. Is to be closer than 610° to at on the job. The evener is	any wire	the City and County resulting from open matches the defause in conductably with 1 here worker's come	AUSE. The permittee(e) (of Sen Pranoisco from ar sticms under this permit, (of the City and County of the provisions of Section : constition powerses under	is against any and reparchess of rangi I San Francisco ag 1800 of the Laber (18 or (1) designed	the permit, serve(s) to (i all cialize, demends a ignice of the City and (short all each clubra, o Code of the State of Ca. but brites, or shall being the city of the bard brites, or shall being the city of the bard brites.	nd actions to county of Ban immands or a Mornis, the s sets Rem (11)	r danseges i Prancisco, asif ta ations. ppčioust shull ////
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	PERMIT OF OCCUPANCY GR. APPROVAL OF THIS APPLICA PLUMBING INSTALLATIONS.	ANTED, WHEN RECK ATION DOES NOT CO . A REPARATE PIER	CATE OF FINAL COMPLETION Ared Dastetute an Approval for NIT for the Wiring and Pli N is "Yeb" to any of Above	THE ELECTRICAL WIRLING (BRINNIS MOUT BE CONTAINED	DR.	A Hunve LT Code, for Insurance Carrier	Havat	compensation ins	urance, as required by	Section 1700	of the Lubor
	in dwalings, sil insulating i whoe ar squipment. CHECK APPROPRIATE BOX	matariala mual kav	GALL BE STARTED UNTIL A BU		ical	6/1Y 24/3		become aubiect t	a the worker's comme	antine invest	of California
	 D OWNER LESSEE CONTRACTOR 		Architect Reent Engineer			compens provision () V. Lourstly (ation provinions of the La is of Section 3800 of the 1 is the owner (or the agen	ibor Code of Califo abor Code, that 12 1 for the center) th	rnia and fall to comply to permit herein applie at in the performance :	forshwith wi d for shall be of the work fr	th the descript revolution
		REE THAT IP A PER	NT'S CERTIFICAT INIT IS LINUED FOR THE CON BRINT AND ALL LAWS AND C	STRUCTION DESCRIBED IN		Califor	It is incured, I will employ the and white, prior to the Control Permit Statement		composes with the wer f any work, will file a c		nazioni lavva ny of this form
ţ	REV 06/13				OFFICE	COPY	Cor Agent	mal	JAN	07	2014



I agree to comply with all conditions or stipulations of the various bureaus or departments noted on this application, and attached statements of conditions or stipulations, which are hereby made a part of this application.

Number of attachments



	ADDRESS OF JOB	BLOCKA	-	APPLICATI	on no.
555 19TH	ST	4058.	/009		2164254
	OWNER NAME				ELEPHONE
ESTIMATED COST	N C INV CO LLC	DISPOSITION DATE	PERMIT		1397-0373 EXPENSION DATE
\$241,945 12	/16/13 ISSUED		131216		12/22/16
FORM CONST. TYPE	OCCUPANCY CODES	, PLANS STORIES	UNITS	D	STRICT
8 3 B	I.S-1	2 3	0	<u>8</u>	BID-INSP
PRINCIPAL BUI					4341500
DESCRIPTION/BLDG. USE	T.I.	INCLUDES: N		RATE	D. NON-
OFFICE	STRU	TURAL PARTI	CIONS.	WORK	
	EAK	AREAS. AND VO	DLUNTA	RY MU	LTI ACCOM
SPECIAL INSPECTIONS?	NO FREZONE NO)			
SPECIAL USE DISTRICT	TIDF NO				
	PENALTY NO		COMPLIANCE	MITH REPORTS	
NOTES:					
9003-15	(D(PARTMENT TY AND COL		G INSPECTION FRANCISCO

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OFFICIAL SAN FRANCISCO	BUILDING INSPECTORS JOB RECORD
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APP. NO.	$Q \parallel I$
2013 12-161	1254 BUILDING INSPECTOR

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OFFICIAL COPY	Dept. of Building Insp. D JAN 15 2014
W	TOM C. HUI, S.E.
APPLICATION FOR BUILDING PERMIT ADDITIONS, ALTERATIONS OR REPAIRS	DEPT. OF BUILDING INSPECTION CITY AND COUNTY OF SAN FRANCISCO DEPARTMENT OF BUILDING INSPECTION APPLICATION IS HEREBY MADE TO THE DEPARTMENT OF
ADDITIONS, ALIENATIONS ON NETAINS	APPLICATION IS HEREBY MADE TO THE DEPARTMENT OF
FORM 3 🗅 OTHER AGENCIES REVIEW REQUIRED	PERMISSION TO BUILD IN ACCORDANCE WITH THE PLANS AND SPECIFICATIONS SUBMITTED HEREWITH AND
2 NUMBER OF PLAN SETS 10/14	ACCORDING TO THE DESCRIPTION AND FOR THE PURPOSE 33 3 HEREINAFTER SET FORTH. 92 2 OT WRITE ABOVE THIS LINE ▼
DATE FILED / FILING FEE RECEPT NO. (1) STREET ADDRESS	
PERSON NO. PERSON NO. CAN BENHAUTED CORE	TOF-JOB (29) REVISED COST:
13/4444 / 1/3/14 529	3,950 # # 30,00 118/14
LEGAL DESC	RIPTION OF EXISTING BUILDING
TT B STORIES OF 3 BASEMENTS OF	ILDING AFTER PROPOSED ALTERATION
(4) EXPERIENT CONSTR. (5) NO. OF (7) NO. OF (7) PROPOSED	ILDIANG AFTER PROPOSED ALTERNATION
TO BE AUTO RUNWAY TO BE CONSTRUCTED TO BE CONSTRUCTED TO BE CONSTRUCTED TO BE CONSTRUCTED TO BE CONSTRUCTION?	YES (12) ELECTRICAL YES (13) PULWBING YES YES YES YES YES YES NO YES YES <thyes< th=""> <thyes< th=""> YES</thyes<></thyes<>
Hingi Dal Powlders Cille	
THE OWNER - LESSEE COROSS OUT ONE, ADDRESS MARTIN S. LAVIN MULDITM	entry CO. LLC USY HOWARD SF 94105
(16) WRITE IN DESCRIPTION OF ALL WORK TO BE PERFORMED UNDER THIS APPLICATION	THE 24
Mechanical an Junant	mprovement for floors 1-3 to rk and exception heating and cooling
installation of new trans	for dir fm. Flage retarto PAH
2013 12 16 4254 * Complian	nce with ordinance Nr. 155-13 mit minut
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IMPORTANT NOTICES No change shall be made in the character of the socupancy or use offbords that hold hing a Building suffacting such change. See San Francisco Building Code and San Prancisco Housing Code.	the City and County of San Prancisco from and against any and all shalms, domands and actions for damages
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Persuant is Seo Principles Building Code, the kulturing persuit shull be posted on the job. The conner responsible for approved plana and application being large it shulling atta. Orned lines as shown on drawings accompanying this application are assumed to be correct. If act	have earther's economentation conversage under () or (10 declaration belows, or stat2 indicate lown (10), (14), or (17), which were in applicable. If however flam, (17) is checked, films (17) must be obsched as well. Mark the appropriate host i grade
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wine or equipment. CHECK APPRIPHIATE 80X □ OWNER □ ARCHITECT	() N. I certify that in the performance of the work for which this permit is issued, i shall not employ any person in any measure as its in become subject in the worker's comparestric number of California. I before softwareledge that i understand that in the event that i should become subject to the worker's comparestion providents of the Labor Code of California and fail to comply tarthweth with the
	provisions of Section 3800 of the Labor Code, that the permit hweak applied for shall be descend revolued. () Y. I centrify as the owner (or the spent for the owner) that is the performance of the work for which
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OWNER'S AUTHORIZED AGENT



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9003-15

PERMIT INSPECTION RECORD DEPARTMENT OF BUILDING INSPECTION CITY AND COUNTY OF SAN FRANCISCO BUILDING INSPECTION JOB CARD

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PERMIT INSPECTION RECORD DEPARTMENT OF BUILDING INSPECTION CITY AND COUNTY OF SAN FRANCISCO BUILDING INSPECTION JOB CARD

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	BUILDING INSPECTORS JOB RECORD
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Grade These as adverse on drawings accompanying this applicative lines are not the same as adverse, revised drawings adverting con- draints of rethining wells and wall foothigs must be submitted to ANY STIPULATION REQUIRED KEREIN OR BY CODE MAY BE APPEN BUILDING NOT TO BE OCCUPIED UNTIL CENTRICATO FIRML CO PRIMIT OF DOCUPIENCY DRAWINE, MARKING REQUIRED APPROVAL OF THIS APPLICATION DOES NOT CONSTITUTE AN APP PULMARIAL INTO S. A SEPARATE PRIMIT NO THE THE THAT SEPARATE PRIMITS ARE REQUIRED FAMILY BE STATUTE OF THAT ON CA. THIS IS NOT A BUILDING PERMIT. NO WORK SHALL BE STARTED L In drawillings, all insulating maturitab must have a clearatico of no writes or equipment. CHECK APPROPRIATE BOX DUMINER	Next grade lines, cute and fills, and complete the department for approval. LED. MPLETION IS POSTED ON THE BUILDING OR NOVAL FOR THE ELECTRICAL WIGUNS OR AS AND PLINEBOG MUST BE OBTAINED. OF ABOVE QUESTIONS (10) (11) (12) (12) (22) UNTIL A BUILDING PERMIT IS ISSUED.	 I have and will maintain worker's con Code, for the performance of the work buschance carrier and wolfs munthen Carrier Policy Number () III. The cost of the work to be done is \$10 () N. I certify that in the performance of the any person in any memory as is to be is buffer accessible to the labor provisions of Section 3000 of the Labor () K. I certify us the owner (or the agent (of the section 3000 of the Labor () K. I certify us the owner (or the agent (of the section 3000 of the Labor () K. I certify us the owner (or the agent (of the section 3000 of the Labor () K. I certify us the owner (or the agent (of the section 3000 of the Labor () K. I certify us the owner (or the agent (of the section 3000 of the Labor () K. I certify us the owner (or the agent (of the section 3000 of the Labor () K. I certify us the owner (or the agent (of the section 3000 of the Labor () K. I certify us the owner (or the agent (of the section 3000 of the Labor () K. I certify us the owner (or the agent (of the section 3000 of the Labor () K. I certify us the owner (or the agent (of the section 3000 of the Labor () K. I certify us the owner (or the agent (of the section 3000 of the Labor () K. I certify us the owner (or the agent (of the section 3000 of the Labor () K. I certify us the owner (or the agent (of the section 3000 of the Labor () K. I certify us the owner (or the agent (of the section 3000 of the section 30000 of the section 3000 of the section 3000 of the section 30	the following declarations: of consent to anti-insum for worker's comp reperiormance of the work for which th imperation insurance, as negatived by Section to which this permit is issued. My works of the section of the work of the section of the section of the section of the section of the section of the section of the section of the section of the section of the section of code, that the section of the section of the of the origination of the section of the section of the origination of the section of the section of the section of the section of the section of the section of the origination of the section of the	pensation, as provided in permit is issued. and 3700 of the Labor are compares tion of the compares tion of the compares tion of the compares to the compares of the worker's with with the shall not employ that the decreed revoked.
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OWNER'S AUTHORIZED AGENT



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PERMIT INSPECTION RECORD DEPARTMENT OF BUILDING INSPECTION CITY AND COUNTY OF SAN FRANCISCO BUILDING INSPECTION JOB CARD

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No change that be made in the character of the occupancy or use withing Code. Building Code und San Franchisco Nousing Code. No portion of building or shutcher or scalability used during construction to be check than 8% to any with contributing more than 750 mile. See Sec 333, Califiest construction to be check than 8% to any with contributing more than 750 mile. See Sec 333, Califiest construction to be check than 8% to any with contributing more than 750 mile. See Sec 333, Califiest construction to be check than 8% to any with contributing more than 750 mile. See Sec 333, Califiest construction to be check than 8% to any with contributing more than 750 mile. See Sec 333, Califiest construction of the laber of the check of the San Franchisco and the status of the stat	details of relaking walls and wall footings must be submitted to this department for approval. I hereby effine under penalty of perfury one of the tohowing declarations: Art STRULATION REQUERED MEEN OR BY CODE WAY BE APPEALED. () I. I have and will maintain a certificate of consent to self-instants for works to dependent on the tohowing declaration, as provided to Section 3700 of the Labor Code, for the performance of the work for which this permit to issued. APPROVAL OF TUG APPLICATION DOES NOT CONSTITUTE AN APPROVAL FOR THE EXAMPLE OF THE BUILDING OR PLUMBERG MUST EE COSTLUCED. I have and will maintain more the work for which this permit to issues it. SUPARTY ALLATIONS, A SEPARATE PERMIT FOR THE WIRKING AN OPPONENT CONSTITUTE AN APPROVAL FOR THE EXAMPLE AND THE ELECTRICAL WIRKING OR PLUMBERG MUST EE COSTLUCED. I have and will maintain more there with the performance of the work to which this permit to issues it. SUPARTY FERMIT STATE COMPLETION IS POSTED OUT THE BUILDING OR PLUMBERG MUST EE COSTLUCED. I have and will maintain more there are built to the performance of the work to which this permit to issues it. APPROVAL OF TUG APPLICATION DOES NOT CONSTITUTE AN APPROVAL FOR THE EXECUTION (1) (1) (1) (1) (1) (1) (1) (1) (1) (1)	OR (24). THIS IS NOT A BUILDING PERMIT. NO WORK SHALL BE STARTED LYTTL A BUILDING PERMIT IS ISSUED. In dwellings, all insulating materials must have a clearance of not less than two inches from all electrical writes are clearance of the work to be done is \$100 or less. () III. The ossi of the work to be done is \$100 or less. () III. The ossi of the work to which this permit is issued, I shall not enginement or permit is a to become subject to the work are which this permit is issued, I shall not enginement or permit is an of the control that is the event that it doupdoenne subject is the compensation permits of the Line of the the event that if doupdoenne subject is the compensation permits of the line of the line of the interview of the inter	Çalitarnlə. Hə worker's Də emed revoked,
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	CONDITIONS AND STIPULATIONS	
DEPARTMEN 3UILDING INSP	APPROVED: TOF C[IO]	DATE: REASON:
	MAR 2 0 2018 BUILDING INSPECTOR, DEPT. OF BLDG. INSP.	NOTIFIED MR.
	APPROVED: N/A DOCUMENT CONFORMING	DATE: 2.20.18
	USE (VACANT TO INDUSTRIAL)	REASON:
	Hot A change of Approved Menning Stat Ling Actio hydriand X	NOTIFIED MR.
	APPROVED:	DATE:
Ķ	PLEASE NOTIFY DISTRICT FIRE INSPECTOR	REASON:
Į, Ci	LEASE NOTIFY DISTRICT FIRE THE STARLOF WORK 415-554-5921	
	AT THE START OF WORK 415-554-BUREAU OF FIRE PREVENTION & PUBLIC SAFETY	NOTIFIED MR.
	APPROVED:	DATE:
	N/A 1	REASON:
	MECHANICAL ENGINEER DEPT. OF BLDG. INSPECTION	
	APPROVED:	DATE:
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	CIVIL ENGINEER, DEPT. OF BLDG. INSPECTION	NOTIFIED MR.
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	BUREAU OF ENGINEERING	NOTIFIED MR.
Ċ	APPROVED:	DATE: RÉASON:
	DEPARTMENT OF PUBLIC HEALTH	NOTIFIED MR.
	APPROVED:	DATE:
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	REDEVELOPMENT AGENCY	NOTIFIED MR.
	APPROVED:	DATE:
5	SFPUC_Laura Antos Laura Antos 03(20,48	REASON:
-		NOTIFIED MR.
	agree to comply with all conditions or stipulations of the various bureaus or departments noted on this applicat	ion, and attached statements

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Department of Building Inspection

City & County of San Francisco 1660 Mission Street San Francisco, CA 94103-2414

Building Inspection History



HEADER MATERIAL APPEARS TO BE 25 GA. PROVIDE APPROVED DETAIL FOR HEADER.

RE-INSPECTION REQUIRED.

DEPARTMENT OF

OFFICIAL COPY

Building Inspection Application Number 201802070675

Block/Lot 4058 / 009 Address 555 19TH ST

Description MINOR REMODEL OF EXISTING INDUSTRIAL SPACE TO PROVIDE SMALL ENCLOSED WORKSHOP SPACES w/ doors/partitions, A REMODELED BREAK AREA, and conversion of urinals to toilet in two bathrooms 1st, 2nd, 3rd floor. MAHER ORD 155-13 NA

Owner Name	CO LLC		Form # 8	Job Cost \$35,000.00	•	osition MPLETE	Disposition 06/05/2019	
Owner Phone (415) 397-0373		# of Units O	# of Stories 3	Occupancy F-1,S-1	Bidg Us 16	-	i ration Date /30/2019	Penalty O
Inspector Name	Activity Date	Statu: Code	-	Status Descriptio	on		Comment	5
Gonzalez, Kenneth	06/04/20	19 106	FINA	L INSPECT/APP	RVD	FID obtain	ed ok for final (0 of 0 S.I.
Gonzalez, Kenneth	05/24/20	19 142	PRE-	FINAL			ns made pre-fir ounter pending	nal OK OK to final g FID sign off
Gonzalez, Kenneth	05/22/20	19 142	PRE-	FINAL	i	REMOVAL restrooms	c. OK for final F of cabinets at that are limiting and resched	accessible
Gonzalez, Kenneth	06/01/20	18 114		RECTION UIRED				revise drawings if offices at 1st
Gonzalez, Kenneth	05/31/20	18 134	OK	IO COVER		InInsulation cover	n and sheet ro	ck Nailing OK to
Gonzalez, Kenneth	04/09/20	18 125	ROL	IGH FRAME, PAF	RTIAL	RF Bathroc	oms OK. OK TO	COVER.
Gonzalez, Kenneth	04/05/20	18 125	ROL	IGH FRAME, PAR		EACH OK 1		BATHROOMS 1 OFFICE 201H NO E SPAN BOX BEAM

OFFICIAL DEPARTMENT OF BUILDING INSPECTION	PROVED ept. of Building Insp. - San Francisco DEC 1 02018)	APPROVED FOR ISSUANCE	bldg. 3/8 form 3/8
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	R BUILDING PERMIT	CITY AND COUNTY OF SU DEPARTMENT OF BUILDI APPLICATION IS HEREBY MADE TO BUILDING INSPECTION OF SAN FR	NG INSPECTION	APPLICATION NUMBER
FORM 8 SOVER-THE-	2110	PERMISSION TO BUILD IN ACCORI AND SPECIFICATIONS SUBMITTED ACCORDING TO THE DESCRIPTION HEREINAFTER SET FORTH.	HEREWITH AND	APPROVAL RE
NUMBER O	FPLAN SETS HU DO NO	T WRITE ABOVE THIS LINE V		
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(44) TYPE DF CONSTR. (5A) NO. OF STORIES OF CCCUPANCY:	AND CELLARS:	Industrial	D_S-1 (4/	VÉLLING ITS: ()
(4) TYPE OF CONSTR. (5) NO. OF	(6) NO. OF (7) PROPOSED U	LDING AFTER PROPOSED ALTERAT		
(10) IS AUTO RUNWAY	YES U (11) WILL STREET SPACE	YES I (12) ELECTRICAL YES I (12) ELECTRICAL	YES I (13) PLUMBING WORK TO BE	
TO BE CONSTRUCTED OR ALTERED? (14) GENERAL CONTRACTOR	ND CONSTRUCTION?	ZIP PHONE	CALIF. LIC. NO.	NO
Modern const	uction 22 Batter	4 St., SF. CA 41585-	+4494 847920	2
(15) OWNER - LESSEE (CROSS OUT ONE) MACKIN C. LOWI	ADDRESS 1 Westment CO.	LLC 38 Keyes av St		PT.) 597-0373
(16) WRITE IN DESCRIPTION OF ALL WO	IK TO BE PERFORMED UNDER THIS APPLICATION			
Yenisian.	40 BPA 2018 08	1070675 - elimin	rate several	
partitioned 1	work spaces; char	rge in equess path;	elimnate Kitch	en
counter and s	liding glass door,		taramp) to	
coordinate Al		<u>nce on First Ploor.</u> TIONAL INFORMATION	(Floor Slope @ 28	Approx.
(17) DOES THIS ALTERATION CREATE ADDITIONAL HEIGHT	YES (18) IF (17) IS YES STATE	(19) DOES THIS ALTERATION CREATE DECK OR HORIZ.	YES (20) IF (19) IS YES, STATE NEW GROUND	
OR STORY TO BUILDING? (21) WILL SIDEWALK OVER SUB-SIDEWALK SPACE BE	NO	YES T (23) ANY OTHER EXISTING BLDG. ON LOT? (IF YES, SHOW	VO T FLOOR AREA YES (24) DOES THIS ALTERATION CONSTITUTE A CHANGE	SQ. FT. YES 🗆
REPAIRED OR ALTERED? (25) ARCHITECT OR ENGINEER (DESIGN	NO PROPERTY LINE?	NO ON PLOT PLAN) ADDRESS	NO	NO-2
NEIL D. KAYE (28) CONSTRUCTION LENDER (ENTER N.	1022 Natoma	NO. 4, SF, CA 941	03 32794	
F THERE IS NO KNOWN CONSTRUCTION		ADDICS	-	
No change shall be made in the character of authorizing such change. See San Francisco I No portion of building or structure or exantifold containing more than 750 volts. See Sec 385, Pursuant to San Francisco Building Code, the responsible for approved plans and applicatio Grade lines as shown on drawings accompany	ng used during construction is to be closer than 6'0" to California Penal Code. willding permit shall be posted on the job. The owner is	termit HOLD HARMLESS CLAUSE. The permittee(s) the City and County of San Francisco from is any wire activiting from operations under this permit, assume the defense of the City and County of In contormity with the provisions of Section have worker's compensation coverage under whichever is applicable. If however item (V) of grade	TICE TO APPLICANT by acceptance of the permit, sprec(s) to indemnify of against any and all claims, demands and action regardless of negligence of the City and County of 6 an Francisco against all such claims, demands, 300 of the Labor Code of the State of California, (f) or (fi) designated below, or shall indicate them is checked, item (IV) must be checked as well. Mo	s for damages San Francisco, and to or actions. He applicant shall (III), (IV), or (V),
	st be submitted to this department for approval.	I hereby affirm under penalty of perjury one	-	-
	CATE OF FINAL COMPLETION IS POSTED ON THE BUILDU	IG DR by Section 3700 of the Labor Code	ate of consent to self-insure for worker's compense, for the performance of the work for which this pr	armit is issued.
PLUMBING INSTALLATIONS. A SEPARATE PERM	NSTITUTE AN APPROVAL FOR THE ELECTRICAL WIRING IIT FOR THE WIRING AND PLUMBING MUST BE OBTAINE I IS "YES" TO ANY OF ABOVE QUESTIONS (10) (11) (12) (08 Code, for the performance of the Insurance carrier and pelify num		a a and a bar
In dweilings, all insulating materials must hav	ALL BE STARTED UNTIL A BUILDING PERMIT IS ISSUED. • a clearance of not less than two inches from all elect	() III. The cost of the work to be done is	\$100 or less. FIA 0084	13
wires or equipment. CHECK APPROPRIATE BOX		 IV. I certify that in the performance of any person in any manner so as t 	I the work for which this permit is issued, I shall n o become subject to the worker's compensation la rstand that in the event that I should become subje	ws of California.
D OWNER D LESSEE	NECHITECT AGENT NECHIEFR	 compensation provisions of the L 	rscene marin the event that i should become subje abor Code of California and fall to comply forthwild Labor Code, that the permit herein applied for shat	with the
	INGINEER	 V. I certify as the owner (or the ages this permit is issued, I will emplo 	It for the owner) that is the period mance of the wor y a contractor who complete with the worker's con commencement many work, will file a completed	x for which pensation laws
I HEREBY CERTIFY AND AGREE THAT IF A PER APPLICATION, ALL THE PROVISIONS OF THE P	IT S GERTIFICATION MIT IS ISSUED FOR THE CONSTRUCTION DESCRIBED IN ERMIT AND ALL LAWS AND ORDINANCES THERETO WI	THIS Awith the Central Peradit Sureau.	commencement and from Avork, will file a completed	copy of this form
COMPLIED WITH.		Signature of Applicant or Agent	12/b	142
		OFFICE COPY	1/10/18	

		CONDITIONS AND STIPULATIONS	
to:			DATE:
IPARTMEN ILDING INSPE	TOF	Mark Walls, DBI	REASON:
	4101	DEC 0 5 2018	
		BUILDING INSPECTOR, DEPT. OF BLDG. INSP.	NOTIFIED MR.
	APPROVED:		DATE:
		NM 1	REASON:
<u> </u>		DEPARTMENT OF CITY PLANNING	NOTIFIED MR.
	APPROVED:	N IF	DATE:
	:	F10)1	
		BUREAU OF FIRE PREVENTION & PUBLIC SAFETY	DEC 0 5 2018 NOTIFIED MR.
	APPROVED:		DATE:
			REASON:
		MECHANICAL ENGINEER, ¹ DEPT. OF BLDG. INSPECTION	NOTIFIED MR.
	APPROVED:		DATE:
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		CIVIL ENGINEER, DEPT. OF BLDG. INSPECTION	NOTIFIED MR.
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		BUREAU OF ENGINEERING	NOTIFIED MR.
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		DEPARTMENT OF PUBLIC HEALTH	NOTIFIED MR.
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·		REDEVELOPMENT AGENCY	NOTIFIED MR.
	APPROVED:	11,	DATE:
	•	\mathcal{W}	REASON:
		HOUSING INSPECTION DIVISION	NOTIFIED MR.

OWNER'S AUTHORIZED AGENT



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	PERMIT NO. 1490641	STEB 2 2 2019	(2A) ESTIMATED COST	OF JOB	(2B) REVISED COST:	CH	DATE 2/19/18	,	
		INFORM	ATION TO BE	E FURNISH	вт. 1000, - ED BY ALL AF	PLICANTS	DATE: 2/19/18		<u> </u>
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	(15) OWNER - LESSEE (CROSS		stment Co. L	-LC 38	Keyes Ave.	BTRC#	PHONE (FOR CONTACT SF, CA 9412		15)3917-
	(16) WRITE IN DESCRIPTION C	OF ALL WORK TO BE PERFORMED UNI					0.701 11		0323
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		LENTER NAME AND BRANCH DESIGN				ADDRESS	<u> </u>		
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		IMPORTANT NOTICES haracter of the occupancy or use without f in Francisco Building Code and San Francis		the (ity and County of San Franci	emittee(s) by acceptant sco from and against ar	APPLICANT ce of the permit, agree(s) to inc ny and all claims, demands and	actions for	damages
	No portion of building or structure containing more than 750 volts. S	e or scattolding used during construction is See Sec 385, California Penal Code.	s to be closer than 6'0" to	any wire essu	tting from operations under t me the defense of the City ar	his permit, regardless o Id County of San Francis	f negligence of the City and Co sco against all such claims, de	unty of San i mands or act	Francisco, and to tions.
	responsible for approved plans ar	g Code, the building permit shall be poster id application being kept at building site.		have whice	worker's compensation cov	erage under (I) or (II) de	abor Code of the State of Califi signated below, or shall indica item (IV) must be checked as y	ke itera (III), j	(N/), or (N),
	lines are not the same as shown,	s accompanying this application are assur revised drawings showing correct grade II I footings must be submitted to this depart	nes, cuts and fills, and co	mplete	od of compliance below. eby affirm under penalty of p	erjury one of the fallow	ing declarations:		
		EIN OR BY CODE MAY BE APPEALED. NTIL CERTIFICATE OF FINAL COMPLETION II	S POSTED ON THE BUILDIN	()	I. I have and will maintai	in a certificate of conse	nt to self-insure for worker's of formance of the work for which	ompensation h this permit	a, as provided is issued.
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	CWNER	G AGENT G AGENT C ENGINEER			compensation provision	ans of the Labor Code of	California and fall to comply & that the permit herein applied	orthwith with	h the
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OWNER'S AUTHORIZED AGENT

State of California — The Resources Agency DEPARTMENT OF PARKS AND RECREATION	Primary # HRI #	
PRIMARY RECORD	Trinomial NRHP Status Code	
Other Listings		
Review Code	Reviewer	Date
Page _1_ of _4_ Resource name(s) or number(assignment)	igned by recorder) 2201-2203 3 rd Street	
 P1. Other Identifier: 78; 2201 3rd Street *P2. Location: □Not for Publication ⊠Unrestricted *b. USGS 7.5' Quad San Francisco North, CA Date 199 *c. Address 2203 3rd Street 	* a. County San Francisco 95 City San Francisco	Zip 94107
*e. Other Locational Data: Assessor's Parcel Number	Block: 4058 Lot: 9	

*P3a. Description: (Describe resource and its major elements. Include design, materials, condition, alterations, size, setting, and boundaries.)

This three-story, freestanding sand blasted brick building is situated along 19th Street on a large lot that extends the entire width of the block from 3rd Street to Illinois Street. It is rectangular in plan with longer façades of ten bays, shorter facades of four bays and articulated corners. Aluminum replacement windows with brick sills and lintels are set in openings that have segmental-arched heads in the center portions of the building and square heads at the corners. The window heights increase with each story. The exterior brickwork is common bond with a soldier course cornice defining the edge of the parapet. A shallow pitched gable roof hides behind a parapet with scalloped corners on all façades and a gable on the eastern and western façades.

A train shed is attached to the rear of the building along the Illinois Street façade.

*P3b. Resource Attributes: (list attributes and codes) HP 8 Industrial Building; HP 45 Unreinforced Masonry Building

*P4. Resources Present: I Building I Structure I Object I Site I District I Element of District I Other



P5b. Photo: (view and date)
View from 3rd and 19th Streets looking southeast
May 14, 2001
*P6. Date Constructed/Age and Sources: ⊠historic
1919 – Assessor's Record

***P7. Owner and Address:** Martin C. Levin Inv. Co. LLC 658 Howard Street San Francisco, CA 94105

***P8. Recorded by:** Planning Department City & County of San Francisco 1660 Mission Street, 5th Floor San Francisco, CA 94103

*P9. Date Recorded: 05-21-2001

*P10. Survey Type: Intensive

*P11. Report Citation: (Cite survey

report and other sources, or enter "none")

Southern Waterfront Survey, Carey & Co., 1994.

*Attachments: DNone DLocation Map DSketch Map Continuation Sheet Building, Structure, and Object Record Archaeological Record District Record Linear Feature Record Milling Station Record Rock Art Record Artifact Record Dhotograph Record Other (list)

State of California — The Resources Agency	Primary #
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 Resource Name or # (Assigned by recorder) 2201-2203 3rd Street

 *Recorded by Planning Department – City and County of San Francisco
 *Date 06-11-2001
 Image: Continuation

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Photo 2. View of train shed from Illinois and 19th Streets looking southwest. May 14, 2001.



Photo 3. View from Illinois Street looking northwest. May 14, 2001. Note: Steel fence demolished July 2001.

State of California — The Resources Agency	Primary #							
DEPARTMENT OF PARKS AND RECREATION	HRI#							
BUILDING, STRUCTURE, AND OBJECT RECORD								
Page <u>3</u> of <u>4</u>	*NRHP Status Code_4D2							
*Resource Name or # 2201-2203	3 3 rd Street							
 B1. Historic name: Alberta Candy Company B2. Common name: White Candy Company B3. Original Use: Candy factory B4. Present use *B5. Architectural Style: Renaissance Revival *B6. Construction History: (Construction date, alterations, and date of alter Built in 1919. Windows replaced with aluminum windows in 1983. 								
*B7. Moved? ⊠No □Yes □Unknown Date: <u>n/a</u> *B8. Related Features: None	Original Location: <u>n/a</u>							

 B9a. Architect: Emil. A. Neumarket
 b. Builder: William Martin

 *B10. Significance: Theme Industrial Development and Settlement
 Area_San Francisco's Central Waterfront

 Period of Significance 1854-1948
 Property Type Industrial
 Applicable Criteria_A_

 (Discuss importance in terms of historical or architectural context as defined by theme, period, and geographic scope. Also address integrity)

The Alberta Candy Company may have constructed the building at this site due to its proximity to the Spreckles Sugar plant.

German-born architect Emil A. Neumarket designed a number of residential buildings in San Francisco. Before immigrating to California, Neumarket served as the city architect in Auerbach, Germany. He died in 1955 at the age of 79.

This property is a contributor to a fully documented historic district that may become eligible for listing in the National Register when more historical or architectural research is performed. This resource is significant under Criterion A: Resources that are associated with events that have made a significant contribution to the broad patterns of our history.

This resource possesses integrity of location, design, setting, materials, workmanship, feeling, and association.

B11. Additional Resource Attributes: (List attributes and codes)

*B12. References:

Heritage Architect Files, Sanborn Maps 1886, 1900, 1915, 1920. WPA Land Use Map, 1940. Block Books 1935, 1946, 1965, current.

B13. Remarks:

*B14. Evaluator:

Tim Kelley, historian, Central Waterfront Survey Advisory Committee *Date of Evaluation: July 20, 2001

(This space reserved for official comments.)



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*Date

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Photo 4. View of 2201-2203 3rd Street looking east c. 1925-1929. Photo courtesy of the Potrero Hill Archives. The building is occupied by the Alberta Candy Factory.



Photo 5. View of 3rd Street at 20th Street September 1944. Photo San Francisco Public Library accession number AAB 5827.

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Page <u>1</u> of <u>2</u> *Resou	ce Name or # (Assigned by recorder)	2201-2203 3 rd Street

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Update

2201-2203 3rd Street (APN 4058/009) was surveyed in 2001 by the City of San Francisco as part of the Central Waterfront Cultural Resources Survey and was assigned a National Register Status Code of "4D2." In 2003, the State of California converted all National Register Status Codes (NRSC) into California Historical Resource Status Codes (CHRSC). All properties listed with a NRSC of "4D2" were converted into CHRSC of "7N1," thus identifying these properties as "Needs to Be Reevaluated (Formerly NR SC4) – may become eligible for NR w/restoration or when meets other specific conditions."

11/8/2012

*Date

With the exception of a steel chain-link and scrap-iron fence that ran north along Illinois Street at the building's rear façade that was demolished in 2001, the building appears to be unchanged since the last survey. Constructed in 1919 as the Alberta Candy Company factory, 2201-2203 3rd Street is a three-story, free-standing brick building. The building is an excellent example of the industrialization of Third Street that occurred in the period from the 1920s to the 1950s. 2201-2203 3rd Street is significantly associated with industrial development in the Central Waterfront area of San Francisco. Although the brick facades have been sandblasted and the original windows were replaced with aluminum sashes in 1983, the building played a significant individual role in this trend, and appears individually eligible for local, state and national designation. 2201-2203 3rd Street also appears eligible as a contributor to the Central Waterfront/Third Street Industrial District. This district is eligible for local designation under Criterion A (Events) as a resource "associated with events that have made a significant contribution to the broad patterns of our history."

2201-2203 3rd Street has been assigned a new California Historical Resource Status Code of "3S," thus identifying the property as "Appears eligible for NR as an individual property through survey evaluation." This property was not assessed for its potential to yield information important in prehistory or history, per National Register Criterion D (Information Potential).



2201-2203 3rd Street, primary façade (6 February 2007)

*Recorded by: Page & Turnbull

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Illinois Street (east) façade with train shed (6 February 2007)



19th Street (north) façade (6 February 2007)

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DISTRICT RI	ECORD	Trinomial		
Page <u>1</u> of <u>32</u>		*NRHP Status Code		
	*Resource Name or # (Assigned by recorder)	Potrero Point Historic District		
D1. Historic Name	Potrero Point/Lower Potrero	D2. Common Name: Central Waterfront		

***D3.** Detailed Description (Discuss coherence of the district, its setting, visual characteristics, and minor features. List all elements of district.): The Potrero Point Historic District (also referred to as the Central Waterfront) is located in the Potrero Hill district of San Francisco on the western side of San Francisco Bay in the City of San Francisco between Mission Creek on the north and Islais Creek to the south. The approximately 500-acre area is more precisely described as a roughly rectangular district bounded by Sixteenth Street to the north, San Francisco Bay to the east, Islais Creek to the south, and U.S. Interstate 280 to the west. The area measures approximately 1.3 miles from north to south, and approximately 0.6 miles wide from east to west. (See Continuation Sheet, Pg. 2)

*D4. Boundary Description (Describe limits of district and attach map showing boundary and district elements.):

The Potrero Point (Central Waterfront) area is enclosed within a rectangle formed by the following streets and natural features: Beginning at the northwest corner of Pennsylvania and Sixteenth streets, the northern boundary of the area extends east along Sixteenth Street into San Francisco Bay. The boundary turns ninety degrees and heads south through the bay encompassing the entirety of Piers 70 and 80. At Islais Creek Channel, the boundary makes a ninety degree turn and heads west along the southern shore of the channel. At the western end of Islais Creek Channel, the boundary shifts north and proceeds along Pennsylvania Street until it reaches the point of beginning. **(See Continuation Sheet, Pg. 9)**

*D5. Boundary Justification:

The boundaries of the Central Waterfront area were selected on the basis of common historical patterns and events, as well as physical continuity. The western boundary—Pennsylvania Street—marks the western end of the area historically known as "Lower Potrero." West of Pennsylvania Street the land becomes much steeper, and industrial uses give way to residential. I-280 and the former Southern Pacific commuter railroad tracks (now Caltrain), which are both located immediately east of Pennsylvania Street, create another formidable boundary. Sixteenth Street—the northern boundary—marks the approximate location of the southern shore of Mission Bay, a natural water feature that historically divided the Potrero District from the South of Market area and downtown San Francisco. Mission Bay was filled in the 1890s and early 1910s, and until recently was the location of massive rail yards. Its historical use, street pattern, and development patterns were quite different from the Central Waterfront. **(See Continuation Sheet, Pg. 11)**

D6.	Significance: Theme	Industrial Developme	ent and Settlement	Area	San Francisco Central Waterfront
	Period of Significance	1872-1958	Applicable Crit	eria 1, 3	
	(Discuss district's importance in	n terms of its historical con	itext as defined by theme	e, period of signifi	cance, and geographic scope. Also address
	the integrity of the district as a v	whole.)			

The Central Waterfront, inclusive of Pier 70, the proposed Third Street Industrial District, and the Dogpatch Historic District appears significant under Criterion 1 (Events) for association with the industrial development of the City of San Francisco from 1872 to 1958. The Central Waterfront Historic District contains a significant concentration of mixed-use industrial properties, associated residential and commercial properties, and civic infrastructure oriented to water, railroad, and road transportation. The district was the epicenter of major industrial production beginning in the late 1850s, and continuing through the end of World War II. During the World Wars, the Central Waterfront was a centerpiece of the single-largest shipbuilding region in the Western United States (and briefly, the world), employing up to 18,500 workers at the height of World War II. The district also includes one of the only surviving grouping of workers' housing located adjacent to industrial buildings and structures in the West, primarily the historic industrial buildings and structures at Pier 70 associated with Bethlehem Steel and the earlier Union Iron Works. Elements of other important industrial facilities, including PG & E's Station A, the Western Sugar Refinery and the American Can Company, remain substantially intact. (See Continuation Sheet, Pg. 12)

*D7. References (Give full citations including the names and addresses of any informants, where possible.) [See Continuation Sheet, Pg. 31]

*D8.	Evaluator:	Christopher VerPlanck, Rebecca Fogel, and Rich Sucré	Date: March 2008		
Affiliation and Address:		Kelley & VerPlanck and Page & Turnbull, Inc., 724 Pine Street,	San Franc	isco, CA 94108	

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D3. Detailed Description (continued)

The existing topography of the Central Waterfront area is low and relatively flats—largely the product of extensive blasting, cutting, and filling operations. The land rises gently toward the west from San Francisco Bay, ranging from 5 feet above sea level at the Bay to a maximum of 25 feet near Interstate 280. One notable exception to the generally level topography is "Irish Hill," an 80 foot, rocky promontory located east of Illinois Street, between Twentieth and Humboldt streets. A remnant of the eastern spur of Potrero Hill that once occupied the area, the small serpentine hillock looms over a paved parking lot between Pier 70 and the PG & E Potrero Station. Most of the modern shoreline of the Central Waterfront is the result of cutting and filling operations that took place during the late nineteenth and early twentieth centuries. Of the total land area, approximately one-third consists of solid bedrock, with two-thirds representing filled land.¹ Water-related landscape features include the abandoned remains of piers and shipways at Pier 70 and Warm Water Cove, the latter named for the heated industrial discharge of the PG&E Potrero Station. Islais Creek Channel, a more recent manmade intervention in the landscape of the Central Waterfront, forms the southern boundary of the study area. The waterfront has an irregular shoreline created by fill and pier construction. Major piers include the irregularly-shaped Pier 70 at the north end of the shoreline, lined with ship ways, wharves, and drydocks; and the filled expanse of the container shipping facility at Pier 80, located at the southern end of the Central Waterfront, opened in 1967. Other manmade features include the trench and railroad tunnel network built by the Southern Pacific Railroad in 1904, and the I-280 right-of-way that transects the western part of the neighborhood.²

Development and vehicular and pedestrian circulation in the Central Waterfront area follows a standard American grid street pattern with named streets running north-south and numbered streets running east-west. According to the Wackenruder Map, the street network of the Potrero district was laid out as early as 1861, although many remained as "paper streets," meaning that they were ungraded and unpaved well into the early twentieth century. Most of the numbered east-west streets dead-end east of Illinois Street, where major industrial properties occupy the waterfront. Kentucky (Third) Street, laid out in 1867 and widened in 1938, forms the main north-south artery through the area. The rest of the north-south streets are named for states, like rest of the Potrero district. The east-west streets of the Potrero district, originally named for California counties, were changed to numbered streets in the early 20th century to match the naming convention of streets in the adjoining Mission and South of Market districts.³

With the exception of the residential enclave of Dogpatch, production, distribution, and repair uses dominate the Central Waterfront area. The most significant industries were historically concentrated along the waterfront, including the former Union Iron Works/Bethlehem Steel San Francisco Yard facility at Pier 70; the Pacific Gas & Electric facility at the foot of Humboldt Street; and the remnants of the Western Sugar Refinery at the foot of Twenty-Third Street. Smaller industrial and commercial properties that support waterfront industrial activity fill the street grid west of the waterfront, particularly within a linear section between Illinois and Third streets and in the filled area south of Twenty-Third Street. Commercial corridors are clustered at the intersections of Twenty-Second and Third streets and Twentieth and Third streets. A compact residential neighborhood of approximately eighty properties, commonly known as Dogpatch, is located near the center of the Central Waterfront, running along Minnesota and Tennessee streets between Mariposa and Twenty-Third streets.

Although residential and commercial uses are present, the Central Waterfront is unique in San Francisco for its concentration of heavy industrial buildings and associated waterfront infrastructure, including piers, pilings, seawalls, slips, drydocks, and cranes. Most of the Central Waterfront area was built well before the introduction of municipal zoning regulations in 1921, leading in part to the heterogeneous character of the area. Building types range from large multi-story brick, concrete, and steel-frame industrial buildings along the waterfront, to smaller pre-World War II brick and concrete light industrial structures along Illinois and Third Streets, to lighter corrugated steel and concrete warehouses south of Twenty-Third Street. The residential enclave of Dogpatch is mostly characterized by frame single-family and multiple-family housing, most of which was built between 1880 and 1920.

¹ Moses Corrette Central Waterfront Cultural Resources Survey Summary Report and Draft Context Statement (San Francisco: San Francisco Planning Department, 2001), 15.

² Ibid., 16.

³ Ibid.
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DESCRIPTION BY BUILDING TYPE AND AREA

This section provides information on the building types and features found within the Potrero Point Historic District/Central Waterfront Area, and discusses each type within one of three sub-areas, where possible. The three sub-areas are: Pier 70 Waterfront, Dogpatch, and the Third Street Industrial District. The Pier 70 Waterfront is currently being evaluated by the Port of San Francisco, as part of the planning work anticipated for this area. Dogpatch is a locally-listed historic district. The Third Street Industrial District is detailed as part of this district record.

Industrial Buildings

The earliest industrial buildings in the Central Waterfront area were constructed in the 1850s for powder magazines, rope manufacturing and the early iron industry. Most of these buildings, few of which survive, were constructed of brick. Wood-frame and wood-clad buildings were prevalent during this period, but over time have typically been refaced in corrugated steel siding or stucco. The following list discusses some of the most important concentrations of historic industrial buildings and structures in the Central Waterfront area.

Pier 70 Waterfront

The waterfront was historically the most desirable location for heavy industry, particularly industries dependent on either deep water access for launching ships or shipping. In the Central Waterfront area, the waterfront area, a swath of land located between Illinois Street and the Bay, attracted the largest and most well-financed industries, several of which still survive. Pier 70, a large irregularly shaped tract of fill, dry docks, wharves and finger piers was historically the location of Pacific Rolling Mills, Risdon Locomotive & Iron Works, Union Iron Works and Bethlehem Steel's San Francisco Yard. The site is occupied by largely vacant offices, machine shops, foundries, warehouses, dry docks, platforms, and ancillary buildings constructed in several major campaigns, beginning in 1883 and in subsequent periods including 1896-1900, 1912, 1917, and 1937-45. There are six extant buildings and structures from the earliest Union Iron Works period (circa 1883-1902). These include: Buildings 113, 114, and 104; Pier 68, Slip #4, and parts of the rail spur system. Additionally, later buildings incorporated parts of buildings from this period, including Buildings 118, 105, and possibly 109. One building survives from the operations of Risdon Locomotive & Iron Works at Pier 70: Buildings 21 (1900). There are four buildings and structures at Pier 70 that were constructed between 1905 and 1915 for Bethlehem Steel: Buildings 38, 102, 108, and 109. Approximately twenty-five buildings at Pier 70 were constructed for the Bethlehem Steel Company between 1937 and 1945.

Pier 70 is not a historic district, although it is informally treated as a potential National Register historic district by the City and County of San Francisco. Most of the historic buildings at Pier 70 have California Register Status Codes of '3D,' meaning that they "appear eligible for the National Register as a contributor(s) to a National Register eligible district through survey evaluation." Currently, the Port of San Francisco is completing historic documentation of the Pier 70 site.

Dogpatch

The most important industrial buildings in Dogpatch are two large timber-framed brick warehouses constructed after 1900 on land belonging to the Santa Fe Railroad. These include the San Francisco Building & Manufacturing Building at 800-50 Tennessee (1900), and the Schilling Wine Warehouse at 900 Minnesota (1906). During the 1920s and 1930s, industrial encroachment on Dogpatch increased and several additional industrial buildings were erected, including:

- 904 Twenty-Second Street, the concrete George Scharetg & Sons drayage warehouse (1919),
- 900 Tennessee Street, the steel-frame and corrugated metal-clad American Meter Company warehouse (1926), and
- 970 Tennessee Street, the Streamline Moderne style Taylor Machine Shop (1935).

Dogpatch is a local landmark district in San Francisco and as such most contributors have a status code of 5D1, meaning that they are contributors to a local district.

Third Street Industrial District

A long swath of land between Dogpatch and Pier 70 contains a large number of manufacturing, repair, and processing plants

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constructed mostly during the first half of the twentieth century. Most of these industries did not require access to deep water but they were dependent on freight-hauling service provided by Santa Fe Railroad trains that ran along Illinois Street. Initially discussed in the 2001 Central Waterfront survey as a potential historic district comprised of "industrial-type buildings," this district is fully documented in this district record and is comprised of the following buildings:

- 2121 Third Street, Seaside Oil Company Plant (1930),
- 2203 Third Street, the Alberta Candy Company Building (1919),
- 2225 Third Street, M. Levin & Sons Warehouse (1924),
- 2250 Third Street (1919),
- 2255 Third Street, Jos. Levin & Sons Warehouse (1922),
- 2289-95 Third Street, a mixed-use commercial building and boarding house (1900),
- 2290-98 Third Street, Anglo America Trust Co. (1917),
- 2301 Third Street, the American Can Company Building (1915-29),
- 2350 Third Street, the Castellino Machine Shop (1927),
- 2364 Third Street, the Pelligrini Bros. Winery (1939),
- 2400 Third Street, the Goodyear Rubber Co. Building (1937),
- 2440 Third Street, Bertsch Machine Works (1937),
- 2501 Third Street, the American Can Extension Building (1955), and
- 825 Tennessee Street, Bowie Switch Company Building (1926),
- 728 Twentieth Street, Dr. Frank M. Close Medical Center (1948),
- 1300 Illinois Street, the Pacific Telephone & Telegraph Co. Building (1947).

Most of these buildings have been assigned a status code of '5D2,' signifying their inclusion in a historic district eligible for local designation. For those properties that were previously assigned a NRSC of "4," Page & Turnbull completed an update of the evaluation of all of these properties through DPR 523L forms.

In addition to the aforementioned buildings, the Third Street Industrial District includes several properties just south of Pier 70 that once constituted PG & E's Potrero Station A and the remnants of the Western Sugar Refinery. PG & E's property includes five historic buildings: Station A, the Pump Station, the Gate House (all 1901), the Meter House (1914), and the Compressor House (1924). Most of these buildings are constructed of brick in the American Commercial style. The former Western Sugar Refinery plant stands south of the PG & E property at the foot of Twenty-Third Street. The plant was originally constructed in 1881 by Claus Spreckels but only two warehouses remain from a later expansion of the plant that occurred between 1923 and 1929. The rest of the plant has been demolished. These properties have been previously evaluated by qualified architectural historians. Although the Meter House and Compressor House on PG & E property were both found eligible for listing in the California Register have status codes of "7," along with the rest of the facility. The Western Sugar Refinery buildings were determined eligible for listing in the National Register. These buildings have a similar history and significance as the other properties found within the Third Street Industrial District and are considered contributors to the sub-district.

Minnesota and Indiana Streets

Several historic industrial and food processing buildings continue to stand in isolated pockets of the Central Waterfront area, and are located outside of the three sub-areas: Pier 70 Waterfront, Dogpatch, and Third Street Industrial District. Constructed along Santa Fe spur lines along Indiana and parts of Minnesota Street, these buildings are extremely varied in regard to construction materials and methods and industries housed within them. Some of the more notable examples include:

- 590 Minnesota Street, the California Bag Company warehouse (ca. 1929),
- 600 Minnesota Street, the California Canneries Company (ca. 1900),
- 580 Indiana Street, the E.J. Bartells Co. Building (1946), and
- 1275 Minnesota Street, the Crown Products Corp. Plant.

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Retail and Mixed-use Buildings

Restaurants, bars, groceries and other neighborhood services have operated within the Central Waterfront since its earliest days, serving the working-class residents of the area. Many of these building types were constructed as two-or-three-story buildings with commercial uses on the ground floor and residences above. The existing retail and mixed-use buildings are generally located along Third Street at the intersections of Twentieth and Twenty-Second streets within the Dogpatch neighborhood. They are characteristically small-scale buildings constructed of wood, brick or concrete, and many have been altered over time.

There are seven mixed-use residential and commercial buildings, mostly within the Dogpatch Historic District. Most of these were built around 1900, and are typically two-or-three-story structures located on corner lots. Examples of this type include 900-02 Twenty-Second Street (built 1899), and 700 Twenty-Second Street (built 1912). Others are larger residential hotels or lodging houses with commercial space on the ground floor, including the Kentucky Hotel at 2500-03 Third Street (1902), and the March Hotel at 726-32 Twenty-Second Street (1917). There are a handful of other residential hotels located along Third Street outside Dogpatch.

Residential Buildings

Outside of the Dogpatch neighborhood, there are only seven residential buildings within the Central Waterfront area. These woodframe houses were largely constructed as housing for workers employed at nearby factories. The majority of housing that continues to exist outside Dogpatch is typically part of mixed-use structures (ground floor commercial with residential above), or dwellings that remain from compact residential areas that have been largely demolished. Notable examples of this type of housing include:

- 2636-2638 Third Street, the oldest residential building outside of Dogpatch, but within the study area (circa 1875),
- 670-674 Tennessee Street, a two-story frame house that appears heavily altered (circa 1884),
- 2476-2478 Third Street, another two-story frame building that appears heavily altered (1900), and
- 1270 Indiana Street, a single-family dwelling that is isolated within an industrial zone in the southern part of the study area (1893).

Dogpatch is the only significant concentration of residential structures in the Central Waterfront; of the 100 buildings built during the period of significance, 83 are wood-frame residential structures built before 1930. Of these, 32 are single-family dwellings; 41 are multi-family dwellings; eight are commercial buildings with residential above; and two are residential hotels. Multi-family dwellings were constructed in Dogpatch throughout most of the period of significance, with the greatest number erected between 1900 and 1917. 920-22 and 924-26 Minnesota, built as a pair in 1900, as well as most buildings on the west side of Minnesota Street between Twentieth and Twenty-Second Streets, or the north side of Twenty-Second Street between Minnesota and Tennessee Streets, are home to many good examples of this type. The single-family dwelling is the second-most common residential building type in Dogpatch, with thirty surviving within the district boundaries. Single-family dwellings were generally divided into two main subcategories: spec-built workers' cottages that were rented to laborers, such as the "Pelton cottages" located at 1002-1014 Tennessee (constructed 1890-91); or larger single-family dwellings built by more affluent skilled laborers, such as 700-02 and 704 Tennessee (constructed 1883 and 1891, respectively). Single-family dwellings were constructed throughout the period of significance, although relatively few were built after 1900. Most contributing buildings within Dogpatch have CHRSC of '5D2,' thus designating them as "Contributor to a district that is eligible for local listing or designation."

Civic Buildings

There are four surviving public and civic buildings in Dogpatch:

- 1060 Tennessee Street, the Irving M. Scott School (1895);
- 2300 Third Street, the Potrero Police Station (1912);
- 2310 Third Street, Potrero Emergency Hospital (1915); and
- SFFD Engine House #16.

Typically more architecturally significant than residential or commercial buildings, several of the civic buildings have higher status codes, including the Irving M. Scott School, which has a status code of '1S,' meaning that it is an individual property listed in the

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National Register. In addition, the Potrero Police Station, Potrero Emergency Hospital, and SFFD Engine House #16 all have a status code of '5B1,' meaning that it is a contributor to a locally designated district (Dogpatch) and are individually significant.

Transportation-Related Resources

The railroad tunnels under Iowa Street and many of the railroad tracks seen today in the Central Waterfront area were constructed in the first decade of the 20th century. Between 1904 and 1907, the Southern Pacific cut tunnels for trains under Iowa Street to create the alignment for what became known as the "Bayshore Cut Off." The tracks laid through these tunnels continued south over a bridge at Islais Creek. Spurs to individual warehouses were also built to allow rail cars to deliver goods from ships to warehouses. Buildings were often constructed around the locations of the rails and set back 10 feet to accommodate the width of a rail car.

This network of tunnels and associated bridges over Twenty-Second and Twenty-Third streets were recognized by the San Francisco Planning Department as a National Register-eligible district and as a result have a status code of '3D.'

Landscape Features

Irish Hill, historically a small residential enclave, was located between Illinois, Maryland, Twentieth, and Humboldt Streets. Over the past 100 years, the hill has been reduced in size to the extent that only a "T" shaped portion remains. The remaining portion is located within three areas: the unopened portion of Michigan Street, and portions of Assessor's blocks 4111 and 4120. Irish Hill represents two major significant themes. First, it represents the extent to which industry has made the land in the study area conform to its needs through a system of grading and filling of San Francisco Bay—primarily in the Pier 70 area. Gravel and soil taken from Irish Hill over the years was also used as fill material for the reclamation of land from the Bay at Islais Creek Basin and Mission Bay. Second, Irish Hill represents the last vestige of an entire residential neighborhood Irish Hill a historically working-class residential enclave of workers' housing demolished in various stages of shipyard expansion.

Contributors to the Central Waterfront: Pier 70 Waterfront District

The list of contributors to the Pier 70 Waterfront District will be further refined by documentation to be published by the Port of San Francisco. However, the 2001 Central Waterfront Cultural Resources Survey provides an evaluation and examination of all Pier 70 properties.

Contributors to the Central Waterfront: Dogpatch Historic District

The list of contributors to the Dogpatch Historic District is defined in Appendix L, Article 10, San Francisco Planning Code.

Contributors to the Central Waterfront: Third Street Industrial District

The following properties are contributors to the Third Street Industrial District:

APN	Address	Resource Name
	20th Street	20th and Illinois streets paving
3994 002	2085 3rd Street	Gilmore Oil Co. Office Bldg
4045 002	2121 3rd Street	Seaside Oil Co. Plant
4058 005	2289-2295 3rd Street	
4058 009	2201-2203 3rd Street	Alberta Candy Company
4058 010	2225 3rd Street	M. Levin and Sons Warehouse
4058 010	2255 3rd Street	Jos. Levin and Sons Warehouse
4059 001A-001B	815-825 Tennessee Street	Bowie Switch Co.
4059 008	2250 3rd Street	
4059 009	2290-2298 3rd Street	Anglo California Trust Co.

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APN (cont'd)	Address (cont'd)	Resource Name (cont'd)
4059 011	724-728 20th Street	Dr. Frank M. Close Medical Clinic
4108 003	2350 3rd Street	
4108 003J	2440 3rd Street	Bertsch Machine Works
4108 003R	2360-2364 3rd Street	Pellegrini Bros. Winery
4108 030	2400 3rd Street	Goodyear Rubber Co.
4109 001	2301 3rd Street	American Can Co. Building
4172 005	2530 3rd Street	(1516-1510 Kentucky Street)
4172 007	2542-2544 3rd Street	(1522 Kentucky Street)
4173 001	2501 3rd Street	American Can Co. Southern Ext.
<mark>4175 006</mark>	1201 Illinois Street	PG&E, Station A Power Plant
<mark>4175 006</mark>	1201 Illinois Street	PG&E, Pump House, Station A
<mark>4175 006</mark>	1201 Illinois Street	PG&E, Meter House, Station A
<mark>4175 006</mark>	1201 Illinois Street	PG&E, Compressor House, Station A
<mark>4175 006</mark>	1201 Illinois Street	PG&E, Gate House, Station A
4232 010	435 23 rd Street	Western Sugary Refinery Warehouses
4232 010	435 23 rd Street	Western Sugary Refinery Warehouses
4231 002	1300 Illinois Street	

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Central Waterfront/Third Street Industrial District



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D4. Boundary Description (continued)

The eastern boundary—San Francisco Bay—would seem an obvious demarcation line. Historically speaking, however, the bay was not much of an impediment to eastward expansion, as streets were platted far out into the bay and filling operations added several acres of new industrial sites in the area. Therefore, the eastern boundary extends out into the Bay in order to encompass all of the filled lands that encompass Piers 70 and 80. The southern boundary—Islais Creek—marks what was once the center of a vast area of tidal marshes that was not filled until the 1920s, ending only with the construction of Pier 80 in 1967. South of Islais Creek is the Bayview Hunters Point District.

Pier 70 Waterfront

Refer to 2001 Central Waterfront Cultural Resources Survey completed by San Francisco Planning Department for a description of the boundaries of Pier 70.

Dogpatch Historic District

The boundaries of the Dogpatch Historic District are defined in Appendix L, Article 10, San Francisco Planning Code.

Third Street Industrial District

The proposed Third Street Industrial District posited by this district record is a narrow linear district bounded by Eighteenth Street to the north (inclusive of the Carpenters' Union Hall at 2069 Third Street), Illinois Street to the east, Twenty-fourth Street to the south, Third Street to the west, and those parcels that encompass PG&E Station A (APN 4175 006) and the remnants of the Western Sugar Refinery (APN 42320010). The proposed district also includes several properties on the west side of Third Street between Twentieth and Twenty-Second streets and the contiguous block bounded by Nineteenth, Third, Twentieth, and Tennessee streets.

The illustrated map on the next page provides an accurate visual account of the boundaries of the Potrero Point Historic District and the three sub-areas.

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D5. Boundary Justification (continued)

Pier 70

Pier 70 is treated as a local landmark district and it has been determined eligible for listing in the National Register as a nationally significant historic district.

Dogpatch

The Dogpatch Historic District is already listed as a local landmark district. The boundaries of this district are listed in Appendix L of Article 10 of the San Francisco Planning Code.

Third Street Industrial District

The boundaries of the proposed Third Street Industrial Historic District encompass the highest concentration of significant light industrial and processing properties remaining in the Central Waterfront district. The linear character of the district boundaries is dictated by the separation of heavy maritime industrial uses along the waterfront from the residential enclave of Dogpatch. The intermediate zone between the two areas gradually developed with light industrial, repair, warehousing and food processing businesses, as well as some wholesale businesses, such as oil distribution companies, that needed to have proximity to rail lines along Third Street as well as a local labor force of blue collar workers. Historically, the blocks between Third and Illinois have been occupied by manufacturing operations and warehouses, most notable of which is the vast American Can Company plant.

The proposed Third Street Industrial Historic District links Pier 70 and Dogpatch and provides a sense of historical and geographical continuity between the two areas. Potentially, these three districts could be conceived as a single entity, San Francisco's only historic district that recognizes the remaining infrastructure of a mixed-use industrial and residential community, once the most important industrial zone on the West Coast.

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D6. Significance (continued)

Many are good examples of late-19th and early 20th-century American industrial design, justifying the district's eligibility for listing in the California Register under Criterion 3 (Design/Construction).

PREFACE

The Central Waterfront is presently the focus of millions of dollars of private and public investment. Increasing residential development, in particular the arrival of dozens of newly constructed "live-work lofts" placed an increasing amount of pressure on the older residential and industrial resources of the study area. In response to these development pressures, the San Francisco Planning Department surveyed the Central Waterfront at the same time that Page & Turnbull surveyed the Dogpatch neighborhood. Both surveys were completed in 2001 and both surveys were adopted by the San Francisco Landmarks Board and Planning Commission prior to being submitted to the Office of Historic Preservation (OHP) and listed in the Historic Resources Inventory (HRI). The Dogpatch Cultural Resources Survey resulted in the listing of the "Dogpatch Historic District," a local historic district (See Appendix L, Article 10, San Francisco Planning Department provided funding to Kelley & VerPlanck Historic Resource Consulting and Page & Turnbull, to complete an update of the 2001 Central Waterfront Survey through the completion of a DPR 523D (District Record) form and DPR 523L (Continuation/Update Sheets).

In the Central Waterfront Cultural Resources Survey, each property was assigned a National Register Status Code (NRSC) ranging from 1 to 7. The Planning Department's *Central Waterfront Cultural Resources Survey Summary Report and Context Statement* includes a spreadsheet that lists each property and its status code. In August 2003, the State adopted the new California Historic Resource Status Codes (CHRSC). The adoption of the CHRSC resulted in the need to re-evaluate properties assigned an NRSC of 4, including many properties in the Central Waterfront.

The Central Waterfront also includes several properties that have been evaluated separately in environmental review documents initiated by other undertakings. These include five surviving historic structures at the PG & E Potrero Station at 1201 Illinois Street. Evaluated by historian Ward Hill for Dames & Moore in 1999, Mr. Hill found the historic Meter House (1914) and the Compressor House (1924) eligible for listing in the California Register under Criterion 1 (events) based on their association with PG & E's gas manufacturing facility. The other three buildings, recorded by Mr. Hill on California Department of Parks and Recreation (DPR) 523A forms: the Gate House (1901), Station A (1901 & 1930), and the Pump House (1930), were determined to be ineligible based on lack of integrity due to the demolition of a large portion of Station A in 1983. Other buildings in the Central Waterfront evaluated separately include two surviving buildings of the Western Sugar Refinery facility at 435 Twenty-Third Street. The warehouses, built in 1923 and 1929, were recorded by architectural historian Michael Corbett on DPR 523A and B forms in 2001. Mr. Corbett found the two warehouses to be eligible for listing in the National Register under Criterion A (events).

The residential enclave of Dogpatch aside, the Central Waterfront area includes 114 industrial and commercial buildings; ten retail, mixed-use buildings; seven residential buildings; and eight structures and objects, which include railroad tracks, railroad tunnels, historic basalt paving, and cranes and other marine structures. Dogpatch contains 122 parcels, of which 100 contain buildings constructed during the period of significance. The period of significance selected for the Central Waterfront study area in 2001 was 1854 to 1948, with the earlier date recognizing the earliest industrial development in the study area and the later date selected to mark the end of World War II and the immediate post war boom.

This updated context statement and district record includes an updated period of significance spanning the years 1872 to 1958. The year 1872 indicates the date of construction of the earliest known building or structure in the area (the Thompson House at 718 Twenty-Second Street) and 1958 marks fifty years before the present year (2008), a standard threshold used by the National Park Service in evaluating historic properties. Furthermore, moving the period of significance forward to 1958 encompasses many of the postwar light industrial and food processing buildings that were erected contemporaneously with the newly filled tidal lands south of Twenty-Third Street.

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HISTORIC CONTEXT: PREHISTORIC ERA – 1776

*Recorded by: Kelley & VerPlanck and Page & Turnbull

Prior to European contact, California was home to what author Malcolm Margolin has called "the densest Indian population anywhere north of Mexico."⁴ It has been estimated that between 7,000 and 10,000 Native Americans inhabited the Bay Region. The Spanish called the indigenous inhabitants *costeños*, or "coastal peoples." Today, the term Ohlone is preferred by their descendents. The Ohlone who lived within what is now San Francisco spoke a dialect called Ramaytush, which was probably intelligible to other Ohlone bands living as far away as the Santa Clara Valley and the East Bay.⁵

The Ohlone were semi-nomadic people who inhabited small seasonal villages near streams and tidal flats, where they had ready access to fresh water and food sources such as waterfowl, fish, and various kinds of shellfish. Hunting small terrestrial and marine mammals, and gathering seeds, nuts, roots, shoots and berries, were also important sources within the Ohlone diet. One of the most important sources of nutrients, as suggested by the presence of grinding rocks and *manos* and *metates* near most Ohlone settlements, were acorns provided by Oak trees.⁶

The Ohlone had a rich material culture that made use of both the materials at hand as well as goods traded with inland tribes. Tules harvested from coastal marshes were used to build houses and to make baskets. Balsa logs were utilized to make seafaring canoes used for trade, fishing and hunting. The Ohlone manipulated stone and bone fragments to make arrowheads, scrapers, knives, spears, hooks, sewing needles, and other tools. Furs were used to create cold weather clothing and bedding. The Ohlone were particularly adept at decorative basketwork and making personal ornaments, such as necklaces and earrings, from feathers, shells, bones and other materials.⁷

It is uncertain when the first humans settled in the San Francisco area. Colder and less hospitable than the Santa Clara Valley or the East Bay, what is now San Francisco was probably settled at a later date than surrounding areas. The early history of Ohlone is difficult to ascertain as many of their prehistoric sites have been built over or destroyed to make way for development. The earliest known occupation sites in San Francisco have been radio-carbon dated to about 5,000 to 5,500 years ago, and prehistoric middens containing both burials and artifacts have been dated to 2,000 years ago.⁸

According to several sources, the northern part of the San Francisco Peninsula was located within the Yelamu tribal territory of the Ohlone. The closest permanent Ohlone village to the Central Waterfront was called *Chutchui*, located on Mission Creek. The group of people who lived at *Chutchui* would move seasonally to harvest shellfish at another village on San Francisco Bay called *Sitlintac*, located along the tidal flats of what is now the Mission Bay area.⁹

HISTORIC CONTEXT: EUROPEAN SETTLEMENT AND THE SPANISH/MEXICAN PERIOD (1776-1846)

The earliest recorded history of the Central Waterfront area dates from the late 18th century in conjunction with the simultaneous establishment of Mission San Francisco de Asís (Mission Dolores) and the Presídio de San Francisco in 1776. At that time, the natural landscape of the Central Waterfront was vastly different than it is today. The majority of the area formed an irregular, broad, hilly peninsula surrounded by Islais Creek inlet to the south, San Francisco Bay to the east, and a broad expanse of tidal mud flats called Mission Bay to the north. A steep extension of Potrero Hill, called Potrero Point, extended east into San Francisco Bay, boasting elevations of as much as 150' above sea level. The geographic bounds afforded by the peninsula made the area a natural grazing place for the Mission's cattle herds, giving the area the name *Potrero*, or "pasture" in Spanish. After secularization of the missions in 1833, the Mexican government granted the Potrero Hill area to the sons of Francisco de Haro, the first alcalde of San Francisco. The land became

⁴ Malcolm Margolin, *The Ohlone Way* (San Francisco: Heyday Books, 1978), 1.

⁵ Allen G. Pastron, Ph.D. and L. Dale Beevers, From Bullfights to Baseball: Archaeological Research Design and Treatment Plan for the Valencia Gardens Hope VI Project (Oakland: December 2002), 16.

⁶ Ibid., 17.

⁷ Ibid., 18.

⁸National Park Service, Southeast Archaeological Center, "An Unvanished Story: 5,500 Years of History in the Vicinity of Seventh & Mission Streets, San Francisco," Website: <u>http://www.cr.nps.gov/seac/sfprehis.htm</u>

⁹ Allen G. Pastron, Ph.D. and L. Dale Beevers, From Bullfights to Baseball: Archaeological Research Design and Treatment Plan for the Valencia Gardens Hope VI Project (Oakland: unpublished report, December 2002), 18.

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part of the vast Rancho Potrero de San Francisco, and continued in use as grazing land well into the American period.¹⁰

HISTORIC CONTEXT: EARLY AMERICAN PERIOD - FIRST INDUSTRIAL AND RESIDENTIAL DEVELOPMENT, TRANSPORTATION INFRASTRUCTURE, LAND DIVISION (1846-1886)

The Central Waterfront area remained largely undeveloped following the American acquisition of California in 1848. The tidal mud flats of Mission Bay physically isolated the area from the burgeoning mixed-use industrial and residential district of the South of Market Area. Similarly, the high stone rise of Potrero Point limited useable land in the area to pasturage and light agriculture. A brief episode in the city's early squatter wars occurred in the Central Waterfront area. In 1849, John Townsend and Cornelius de Boom attempted to establish a squatters' settlement on de Haro's land at Potrero Point, but the effort soon disintegrated.¹¹

Gunpowder Manufacturing (1854-1881)

The physical isolation of the Central Waterfront was pivotal in the development of the area as an early industrial center in San Francisco. A city ordinance in the early 1850s forbidding dangerous industries from operating near settled areas—combined with intensifying development in the South of Market district—led to the removal of certain industries from the existing city limits. The remote location and access to deep water anchorage at Potrero Point attracted gunpowder manufacturer, E.I. du Pont de Nemours Company, to the Central Waterfront in 1854. The E.I. du Pont de Nemours Company, one of the largest manufacturers of black powder in the United States, constructed the first powder magazine on the West Coast on the south shore of Potrero Point, near what is now the corner of Maryland and Humboldt streets (no longer extant). This facility first appears on the 1857 Coast Survey and Geodetic Map. Gunpowder was in demand for use in mining and local street grading in San Francisco. By 1855, a second gunpowder manufacturer, the Hazard Powder Company, constructed a facility on Twenty-Third Street between Maryland and Louisiana streets (no longer extant). Both facilities built wharves for shipping and receiving along the shoreline. Gunpowder manufacturers remained at Potrero Point until 1881, when encroaching industrial and residential development forced them to sell their plants to sugar baron, Claus Spreckels. The manufacturers subsequently moved to rural Contra Costa County.

Rope Manufacturing (1857-1886)

The same need for large tracts of vacant land and access to deep-water anchorage brought maritime industries to Potrero Point. The San Francisco Cordage Manufactory; a pioneer rope-making facility, moved to Potrero Point in 1857. Brothers Alfred and Hiram Tubbs constructed a 1,000 foot ropewalk extending into the Bay from the present-day intersection of Iowa and Twenty-Second streets. The complex was comprised of a large brick structure and several smaller offices, warehouses and sheds. The company sold rope to ship riggers and mining companies throughout the western United States, Mexico, Peru, China and Japan. Later renamed Tubbs Cordage Company, the business was one of the largest employers in the area during the 1870s. The 1886 Sanborn Map shows the facility as being comprised of several large brick buildings, including a building containing offices and spinning jennies, several sheds, and a 1,500 foot rope walk which extended into San Francisco Bay on a plank pier.

Shipbuilding (1862-1872)

Following the establishment of San Francisco Cordage Manufactory, the industrialization of Potrero Point began to intensify. Boat and ship builders in search of deep-water access made the move from the older and more congested South of Market district to Potrero Point. In 1862, John North, San Francisco's most prominent shipbuilder, relocated his shipyard from Steamboat Point to a large site near the foot of Sierra Street (now Twenty-Second Street) on Potrero Point. Other shipbuilders such as Henry Owens, William E. Collyer, and Patrick Tiernan soon followed suit.¹² The construction of shipyards began to change the landscape of the area—an important theme in the physical evolution of the Central Waterfront. The 1869 Coast Survey map shows five wharves and shipways along the rugged coastline. The early shipyards illustrated the potential of the district as a major ship building center, a realization not lost on the owners of Union Iron Works and other major San Francisco manufacturers. The shipyards also began to attract a significant residential labor force to the area. According to the 1869 Coast Survey Map, many of the earliest residents lived either on the steep hillsides of Potrero Point (later called Irish Hill), or west of Kentucky Street in the flats presently known as Dogpatch. The oldest surviving dwelling in the Central Waterfront is a gable-roofed frame dwelling located at 718 Twenty-Second Street in Dogpatch. Built in 1872, the dwelling originally

¹⁰ "Genesis of Our Hill," Potrero View (September 1976), 1.

¹¹ Hubert Howe Bancroft, History of California, Volume 6 (San Francisco: A.L. Bancroft and Co., 1888), 194.

¹² Roger and Nancy Olmsted, San Francisco Bayside Historical Cultural Resource Study (San Francisco: 1982), 191.

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housed a boat builder named William J. Thompson, who worked in the employ of Locke & Montague.

Long Bridge (1865-68)

Continued industrial development in the Central Waterfront area depended on improved communication with downtown and the Northern Waterfront. The creation of such infrastructure was in large part carried out by speculators anticipating the completion of the transcontinental railroad in 1869. Flush with dividends from the Comstock Lode mines, speculators invested heavily in unimproved lands on Potrero Point. Before large-scale industrial development could occur, though, Mission Bay—a large tidal flat separating Steamboat and Potrero Points—needed to be bridged. Investors were eventually rounded up to finance what would be called Long Bridge. The first pilings were driven off Steamboat Point in February 1865, and two years later the Potrero bridgehead was completed near Mariposa Street.¹³

Kentucky (Third) Street (1868-1893)

By 1868, the route established by Long Bridge continued south from the bridgehead through a trench blasted out of serpentine rock. Originally called Kentucky Street (now Third Street), this was the first major vehicular route to traverse the Central Waterfront. It crossed Islais Creek and eventually terminated at the Bayview District, then known as South San Francisco. In this first major alteration of the topography of Potrero Hill, over 100,000 cubic yards of rock were removed and dumped in Mission Bay.¹⁴ Within a few months of the opening of Long Bridge, horse car lines operated by the Potrero & Bayview Railroad were taking day-trippers out to the Bayview Race Track.¹⁵ Land values rose accordingly. Originally, the cost of maintaining Kentucky Street was the responsibility of its private owners, but in 1893 the Board of Supervisors accepted Kentucky Street as a municipally-maintained street and paved it in basalt blocks.¹⁶

Land Transformation (1869-1886)

Perhaps no other district in San Francisco has been physically transformed to such an extent as the Potrero District. Massive blasting and earthmoving projects undertaken by industrialists, railroads and street builders gradually removed the eastern arm of Potrero Hill, using the rubble to fill adjoining marshlands and "water lots." The latter were tracts of submerged "land" granted by the State of California to various industries. The filling of Mission Bay and the leveling of the eastern arm of Potrero Hill began as early as the 1860s with the completion of Long Bridge and the acquisition of the Alvord Grant by Pacific Rolling Mills. The *San Francisco Examiner* ran a feature story on Potrero Point in August 1889, which described the tremendous physical changes that had occurred during the previous twenty years:

There is no portion of San Francisco where the work of 'the mighty hand of man' is shown so plainly, perhaps, as in that district known as the Potrero. Elsewhere within the city's boundaries vast changes have been brought about...work was strictly that of development. But here it is far different. The pioneers of progress at the Potrero, have had first actually to create the very ground upon which have been erected those vast establishments that have given to the district its name and fame as the very foremost center of mechanical industry and wealth-producing enterprises upon the Coast.

Where the massive factories now stand solidly along the level shore, which bristles with its rows of piers, was once but a choice between precipitous hillsides, along which a goat could scarcely make his way, and oozy foul-smelling marshes, a mere glance at which would seem sufficient to have utterly discouraged the most progressive combination of energy and capital.

Great stretches of craggy bluffs have disappeared. Vast masses of rock have been blasted away from the hillsides and thrown upon the marshes. Thousands and thousands sunk into the depths and left no trace, but a time came at last when the vast dumping process had its effect, and the solid earth appeared above the surface. The mountain had perished! That portion it was necessary to remove so that the

¹³ Henry Langley, The Pacific Coast Business Directory (San Francisco: 1867), 14.

¹⁴ Henry Langley, The Pacific Coast Business Directory (San Francisco: 1867), 15.

¹⁵ Moses Corrette Central Waterfront Cultural Resources Survey Summary Report and Draft Context Statement (San Francisco: San Francisco Planning Department, 2001), 18.

¹⁶ Board of Supervisors of the City and County of San Francisco, Tables showing status of streets in San Francisco (San Francisco: The Hinton Printing Co., 1895).

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great manufactories could take root – and with the mountain had gone the marshes.¹⁷

Early Transportation Infrastructure (1869-1886)

Railroads, in particular the mighty Southern Pacific, were the most powerful force in California politics during the 19th century. Through adept manipulation of the political process, the railroads successfully acquired extensive tracts of land in the Central Waterfront area. Potrero Point remained an important base of operations for the Southern Pacific, and later the Western Pacific and Atchison Topeka & Santa Fe, well into the 20th century. In 1869, the Southern Pacific and then-rival, Western Pacific (not to be confused with the post-1903 railroad with the same name), acquired most of Mission Bay from the State of California. Both railroads made out quite well, each receiving 150 acres and extensive rights-of-way throughout the Central Waterfront.¹⁸ In the absence of a direct connection between San Francisco and the Transcontinental Railroad railhead in Oakland, the Southern Pacific (and eventually the Atchison, Topeka & Santa Fe and Western Pacific), established car ferry slips in the Central Waterfront to allow westbound trains to access San Francisco's main depot at Third and Townsend streets.

Early Iron Industry (1868-1886)

In anticipation of the completion of Long Bridge, several early iron manufacturers began purchasing large tracts of bayside land on Potrero Point. The first was Pacific Rolling Mills. Organized on May 10, 1866 by industrialists William Alvord, John Bensley, and Darius O. Mills, Pacific Rolling Mills received from the State Legislature a grant of submerged land north of Potrero Point known as the Alvord Grant. Alvord, the president of Pacific Rolling Mills, then purchased approximately twenty acres of adjoining dry land on Potrero Point and began building wharves and buildings at the foot of Napa Street (now Twentieth Street). By July 1868, Pacific Rolling Mills was producing rolled steel, a first for the West Coast.¹⁹ From 1868 onward, Pacific Rolling Mills turned out approximately 30,000 tons of iron and 10,000 tons of steel annually, most of which came from locally obtained scrap metal. The company also manufactured items such as rails, locomotive parts, marine and engine forgings, bolts, nuts, railroad spikes, track nails, washers and coil chains.²⁰ In 1882, historian J. S. Hittell described the labor conditions for the largely Irish immigrant workforce, many of whom lived in the adjoining community of Irish Hill, as follows:

From 430 to 450 men are employed in the different shifts. The rates of wages are 25 percent higher than the Eastern states. Common laborers receive from \$1.75 to \$2 per day; puddlers, rollers, and the millmen are paid by the ton, and make about \$4 a day; and those in charge of a gang of men receive \$8 per day.²¹

Early Gas Light Companies (1872-1886)

Early industries such as iron manufacturing and shipbuilding were soon joined by manufacturers and distributors of gas for both street lights and household lighting and cooking. The City Gas Company was the first gas works to relocate to Potrero Point. In 1872, the company began building a complex on four blocks of land fronting the bay between Humboldt and Sierra (Twenty-Second) streets. In 1873, City Gas Company merged with Metropolitan Gas Company and San Francisco Gas Company to form the San Francisco Gas Light Company. The 1886-87 Sanborn Map shows the company's facilities on Potrero Point as occupying a tract of land bounded by Georgia Street to the west, Pacific Rolling Mills to the north, San Francisco Bay to the east, and Humboldt Street to the south. The facility included a large plank wharf and adjoining coal shed, a "retort house," a purifying house, several sheds and storage buildings, an office building and two massive gas tanks.

California Sugar Refinery (1881-1886)

The pioneer sugar industry in California owes a huge debt to German immigrant Claus Spreckels. He established his first refinery in the South of Market in 1863. In 1881, he purchased a five-block site on the south shore of Potrero Point from gunpowder manufacturers and commenced construction of the California Sugar Refinery. The massive brick buildings which comprised the plant included a "melt/filter"

^{17 &}quot;The Potrero as It Is," San Francisco Examiner (August 11, 1889), p. 14.

¹⁸ Stuart Daggett, Chapters on the History of the Southern Pacific (New York: The Ronald Press Company, 1922), 99-100.

¹⁹ J.S. Hittell, Commerce and Industry of the Pacific Coast (San Francisco: A.L. Bancroft, 1882), 682.

²⁰ William Issel and Robert W. Cherny, San Francisco: 1865-1932 (Berkeley: University of California Press, 1986), 30.

²¹ J.S. Hittell, Commerce and Industry of the Pacific Coast (San Francisco: A.L. Bancroft, 1882), 683.

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house," a "wash house" and a "char house." All were designed in 1881 by a New York architect named Hepworth.²² By 1884, these huge works were described as "the most complete concern of the kind in the world, and in size ranks with the great refineries of Brooklyn, New York, and St. Louis."23 The 1886 Sanborn Map shows the layout of the facility (no longer extant), including a ten-story brick filter house and refinery, machine shop/blacksmith's shop, a row of one-story frame shops along the western edge of the property, a two-story melting house and three massive timber-frame warehouses—one of which sat atop a large wharf.

Union Iron Works (1882-1886)

From the early-1880s onward, Union Iron Works evolved into the most important industry on Potrero Point. According to census records, it was the largest employer in the area, employing anywhere between a quarter to half of Dogpatch's residents. Union Iron Works was founded in 1849 by the brothers Peter, James, and Michael Donahue. Although initially little more than a blacksmith's shop, Union Iron Works was the first iron works established on the West Coast. In 1862, the company became known as Donahue Iron & Brass Company. A few years later, when H. J. Booth, Irving M. Scott and George W. Prescott joined the firm, it was renamed Union Iron Works. In 1865, the pioneer firm established its position as the most important iron-manufacturing firm in the West when it built the first locomotive on the West Coast for the San Francisco-San Jose Railroad. Within the next decade, Union Iron Works manufactured 90 percent of the heavy machinery used by mining companies working the Comstock Lode. By 1865, Peter Donahue sold his interest in Union Iron Works, and for the next ten years the company was known as H. J. Booth & Co.²⁴ In the early 1880s, H. J. Booth & Co. was reorganized under the management of partner Irving Murray Scott and renamed Prescott, Scott & Co-although it was popularly still known as Union Iron Works. One of Scott's first major accomplishments was to purchase thirty-two acres of land with deep-water frontage on the north side of Potrero Point, adjacent to Pacific Rolling Mills. In 1883, he oversaw the construction of the new Union Iron Works plant at Potrero Point. The total cost of the shipyard came to approximately \$2 million, an extremely large sum for the day. The buildings, which were designed by a civil engineer named Dr. D. E. Melliss, included a boiler shop, a blacksmiths' shop, a riveting and erecting shop (Building 112, which is still extant), a machine shop (Building 113, also still extant), a 120-foot chimney, a brass-plating shop, an iron foundry and a pattern shop. The shipyard was connected to transportation lines via a Southern Pacific spur line. As the operation grew, the physical site was expanded through fill operations that involved removing rock from Irish Hill and dumping it in the Bay. Shipways, cranes, and long wharves were constructed to handle ships of virtually any size. Gradually, Union Iron Works bought out its nearby competitors at Potrero Point, including Atlas Iron Works and Risdon Iron & Locomotive Works.25

California Barrel Company (1884-1955)

Another early industry at Potrero Point was the California Barrel Company. First established at Potrero Point in 1884 on Louisiana Street between Humboldt and Nevada (Twenty-Third) streets, the company was one of the oldest barrel manufacturers in San Francisco. Its original Central Waterfront location adjoined another important maritime chandlery business: Tubbs Cordage Company. According to the 1886 Sanborn maps, the facility consisted of four, large gable-roofed buildings, including a manufacturing building/machine shop and three large warehouses.²⁶ In 1900, California Barrel Company sold part its old facility to adjoining landowner, Claus Spreckels, and consolidated its operations on a block bounded by Twenty-Second, Michigan, Humboldt, and Illinois streets. With the introduction of steel containers in the 1950s, the demand for barrels for shipping and storage declined. By 1955, the facility was torn down and replaced by the American Can Company Annex.

Other Industries (1869-1886)

Contrary to predictions, the completion of the Transcontinental Railroad in 1869 unleashed an economic downturn in San Francisco, as local industries suddenly found it difficult to compete with the influx of inexpensive goods manufactured in Eastern states. As domestic industries collapsed, land values, which had been bid up by speculators over the past decade, likewise declined. The development of Potrero Point and the Central Waterfront temporarily halted. Nonetheless, surviving industries began to recover during the early 1870s,

²² Bancroft Library, University of California-Berkeley, Documents and materials pertaining to the Western Sugar Refinery collected by Dan Gutleben.

²³ Michael Corbett, Historic Architecture Report for 4352 Third Street, City and Count of San Francisco (San Francisco: unpublished report by URS

Corporation, 2001), 5.

²⁴ Robert O'Brien, "Riptides: From Shoves to Ships," San Francisco Chronicle (October 21, 1949).

²⁵ "The New Union Iron Works and the Arctic Oil Works," San Francisco Morning Call (January 24, 1884), 1.

²⁶ Christopher VerPlanck, Context Statement—Dogpatch Cultural Resources Survey (San Francisco: 2001), 7.

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partially as a result of an influx of cheap immigrant labor from eastern cities which helped the industries of Potrero Point be more competitive. In 1871, the State of California auctioned off the remaining water lots surrounding Potrero Point to various industries. Despite another slump that lasted from 1878 to 1884, the industries of Potrero Point continued to expand. The Coast Survey Map of 1883, the first prepared since 1869, show that the existing industries at Potrero Point had been joined by the Arctic Oil Works, the Southern Pacific Cattle Yards, the Atlas Iron Works and the California Barrel Company. These businesses also appear on the 1886 Sanborn Map, the earliest to provide detailed illustrations of development in the Central Waterfront.

Aside from the industries discussed above and the residential enclaves of Irish Hill and Dutchman's Flat (now Dogpatch), there was little other industry or development. Most of the area remained either submerged beneath water or was too steep to develop. The only other industries shown on the 1886 Sanborn Map not discussed above were located near the intersection of Sonoma (Twenty-fourth) and Minnesota streets. Located on the edge of what was still labeled on the map as a "Tide water basin," was a pair of businesses more closely related to nearby Butchertown: the Potrero Compressed Yeast and Vinegar Company and the Ohlandt & Co. Bone Charcoal Works. In addition, the 1886 Sanborn Map depicted a pair of buildings (a car barn and hay barn) belonging to the Potrero & Bay View Railroad near the corner of Nevada (Twenty-Third) and Kentucky streets, as well as a small encampment of Chinese shrimp fishermen located on a block bounded by Nevada, Kentucky, Sonoma, and Minnesota streets.

Residential and Commercial Development (1867-1886)

The arrival of several large industries in the Central Waterfront during the 1860s created a need for a local labor supply. In theory, the completion of Long Bridge in 1867 and the provision of transit along Kentucky Street made the area accessible to the teeming South of Market and its large labor force. But despite improved access, Potrero Point was still difficult to access for those of moderate means. The Potrero & Bay View Railroad charged hefty fares and Long Bridge was a toll bridge, making the commute to Potrero Point too expensive for most laborers. In the absence of reliable public transit, most industrial workers in San Francisco had to live within walking distance of their place of employment. In order to accommodate workers, provisions were made by corporations to allow them to settle on less desirable tracts of land, particularly the steep hillsides behind Pacific Rolling Mills and within a narrow band of land west of Kentucky Street (Dogpatch). The 1869 Coast Survey map shows only a few scattered dwellings near the bridgehead on Kentucky Street, and another cluster of cottages south of Pacific Rolling Mills on Irish Hill.²⁷

Houses constructed in the Central Waterfront were either built on speculation by landlords and rented out to workers, or built by the workers themselves—either on privately held residential lots or on company-owned land. The proximity of many residences to industrial properties and railroad sidings often resulted in houses exposed to noise and smoke and built on irregularly-shaped parcels. Regardless of where they built, house builders faced significant challenges; building sites often had to be terraced from steep, rocky hillsides or built up over marshy, low-lying ground. An article in the August 11, 1889 edition of the *San Francisco Examiner* described the conditions faced by house builders:

Allowing that the factories were built, the mills and docks erected, communication with the city established—this, in itself a Herculean task—there still remained the problem of housing the great armies of workmen, without whom the wheels and the hammers and the forges must forever remain useless and silent. All could not come from a distance and on those bleak and almost inaccessible hillsides there seemed no more chance for human habitations than upon the low-lying, repulsive and tide-swept marsh.

And about the centers of industries which flourish, so to speak, the lifeblood of what has now come to be the vigorous young community, has grown up the town. More and more of the hillsides were leveled down; more and more of the marsh built up into high and solid ground.

Pathways, alleys and finally broad streets and avenues were graded through or up the slopes, and such of the bigger acclivities (sic) as were allowed to remain were terraced and graded and made the sites of numberless cosy (sic) homes. As the industries of the place grew and the necessity for more building ground, both for business purposes and homes, increased, the streets were carried further, though in many instances at least, at almost incredible toil and expense, and more favorable locations were

²⁷ Christopher VerPlanck, Context Statement—Dogpatch Cultural Resources Survey (San Francisco: 2001), 9-11.

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The demand for housing in the area remained acute throughout the history of the Central Waterfront, accounting for the overall high residential densities.²⁹ Areas that were easiest to build on were developed first. The 1886 Sanborn Map indicates that the earliest concentrations of houses were located near the intersection of Illinois and Twentieth streets; on Irish Hill; and along the intersections of Tennessee and Eighteenth and Tennessee and Twenty-Second streets in Dogpatch.³⁰ The residential districts of the Central Waterfront comprised the only significant concentration of urbanization within the entire Potrero District during the 1870s and 1880s. The 1883 Coast Survey Map and the 1886 Sanborn Maps indicate that most of "upper" Potrero Hill remained pasture, and consequently, when San Franciscans referred to the "Potrero" during this time, they generally referred only to the urbanized portion along the Central Waterfront.³¹

Irish Hill, the older of the two residential neighborhoods in the Central Waterfront, was located in an area bounded by Illinois Street to the west, Napa (Twentieth) Street to the north, Maryland Street to the east and Humboldt Street to the south. Irregularly shaped, Irish Hill consisted of two separate areas: a district of approximately 60 cottages huddled on a steep outcropping behind Union Iron Works, and a compact district of approximately 40 lodging houses occupying all four blocks surrounding the intersection of Illinois and Twentieth streets. As suggested by its name, contemporary Census records from 1880 indicate that Irish Hill was a solidly working-class neighborhood inhabited largely by single Irish male laborers.³²

"Dutchman's Flat," now known as Dogpatch, developed a few years later than Irish Hill and consisted of two separate clusters of flats and cottages, saloons, shops and several churches. The 1886 Sanborn Map shows only the southernmost section of the neighborhood, in particular what is now the 1100 block of Tennessee Street south of Sierra (Twentieth) Street. This area consisted for the most part of rows of identical two-story frame flats clustered along Tennessee and Kentucky streets; several tenements along Sierra Street; the original Potrero School; and several small cottages built on the irregularly shaped diagonal lots aligned with the Tubbs Cordage ropewalk. Several remain standing today, including the Thompson Residence at 718 Twenty-Second Street (1872), and several flats on the 1100 block of Tennessee. Although not shown on the map, there was another small cluster of houses centered around the intersection of Tennessee and Solano (Eighteenth) streets, including the Castner Residence at 707 Eighteenth Street (1876). According to the 1880 Census, most of the residents of Dogpatch/Dutchman's Flat were American-born skilled craftsmen employed either in the boatyards or by Tubbs Cordage. Some were also skilled ironworkers employed at Pacific Rolling Mills. Aside from these two clusters of development, Dogpatch remained largely undeveloped, dominated by rocky outcroppings and impassible streets.³³

As a small, self-contained industrial/residential neighborhood removed from the rest of the city, Dogpatch supported several small groceries, butcher shops and vegetable stands within the survey boundaries. The principal commercial districts included Solano (Eighteenth) Street between Iowa and Kentucky streets, and Sierra (Twenty-Second) Street between Minnesota and Kentucky streets. Prior to its demolition during the First World War, the intersection of Illinois and Twenty-Second Streets in Irish Hill was the most dynamic commercial area, accommodating saloons, groceries, cafes and other businesses.³⁴

HISTORIC CONTEXT: BOOM TIMES IN THE CENTRAL WATERFRONT (1886-1900)

Industrial Development (1886-1900)

Union Iron Works, Pier 70 (1886-1900)

Following the collapse of the Comstock Lode boom in 1878, San Francisco—and by extension the entire state of California—entered a recession that lasted through the mid-1880s. By 1886, the regional economy had recovered and the industries of the Central

²⁸ "The Potrero as It Is," San Francisco Examiner (August 11, 1889), 14.

²⁹ Christopher VerPlanck, Context Statement-Dogpatch Cultural Resources Survey (San Francisco: 2001), 9-11.

³⁰ Ibid.

³¹ Ibid.

³² Ibid.

³³ Ibid.

³⁴ Ibid.

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Waterfront were again running full bore. Irving Scott's Union Iron Works led the way with an ambitious campaign to expand its maritime operations. Union Iron Works launched its first ship, the 800-ton collier *Arago*, in April 1885.³⁵ Soon after, the company submitted bids to build new warships for the U.S. Navy, putting it into direct competition with major East Coast shipyards. Union Iron Works won two important commissions in 1888: the battle cruisers *USS Charleston* and *USS San Francisco*. These were followed in 1893 by the legendary cruiser *USS Olympia* (Admiral Dewey's flagship during the siege of Manila in 1898), and the battleship *USS Oregon*. These two ships were especially crucial in solidifying Union Iron Works' growing reputation as one of America's most important shipyards.³⁶

Much to the chagrin of eastern shipyards, Scott took advantage of a preferential bid-price formula designed by the federal government to encourage shipbuilding on the West Coast. As a result, Union Iron Works gained a healthy share of the contracts that led to the creation of the Great White Fleet, the symbol of American Naval might. Thanks to the ongoing Navy contracts, Union Iron Works expanded its labor force to around 1,500 workers by 1893. In 1896, Irving Scott retained the firm of Percy & Hamilton to design a new headquarters/drafting house for the shipyard (Building 104—still extant). In the decades that followed, Union Iron Works launched dozens of battleships, armored cruisers, submarines, destroyers and other craft, making it the rival of major shipyards around the world. One of these ships was the *USS Ohio*, one of the biggest battleships (otherwise known as 'dreadnoughts'), of its era. President McKinley, Irving M. Scott's close friend, presided over the laying of the keel in 1899 after being escorted to the ceremony by a group of schoolchildren from Irving M. Scott School in Dogpatch.³⁷ The 1899-1900 Sanborn maps show a vastly expanded shipyard comprised of the 1883 machine shop, forge/erecting shop and foundry, the 1896 headquarters, a boiler shop, flange shop, pattern shop, plate shop, dozens of other ancillary shops, as well as six large slips and a dry dock. Union Iron Works was located on the northern portion of what is now known as Pier 70, which is generally defined by City Block 4046.

Pacific Rolling Mills/Risdon Iron Works, Pier 70 (1898-1900)

In 1898, Pacific Rolling Mills closed its doors on the southern portion of what is now known as Pier 70 (generally defined by City Block 4052), unable to withstand Eastern competition. Not long after it was acquired by Risdon Iron & Locomotive Works, manufacturers of mining equipment, locomotives and dredges. Risdon, founded in 1855 by blacksmiths John Risdon and Lewis Coffey, originally focused on manufacturing boilers. In 1868, the company expanded its focus, entering the maritime sector by making engines for steamships. During this time the company became known as Risdon Iron & Locomotive Works. The company's plant was located on a full block in the South of Market bounded by Folsom, Howard, Beale, and Main streets. In 1891, the company changed its name to Risdon Iron Works, and in 1899 it acquired the defunct Pacific Rolling Mills and relocated much of its manufacturing operations to Potrero Point. It was there that the company branched out into shipbuilding.³⁸

Western Sugar Refinery, Pier 70 (1891-1900)

Claus Spreckels' California Sugar Refinery remained within the top five employers of residents in the Central Waterfront well into the 1940s. Concerns over political instability in Hawaii in the early 1890s caused Spreckels to diversify into sugar beet cultivation and refining throughout central California. The Potrero plant continued to play a major role as a refiner of imported Hawaiian sugar and was listed in city directories variously as the California, or increasingly, the Western Sugar Refinery. The 1899-1900 Sanborn map labels the facility as the Western Sugar Refinery. Although the plant was doubtlessly constantly being modernized, the footprint of the individual buildings did not change greatly between 1886 and 1900. Raw Hawaiian sugar was delivered by ship to the massive east wharf and then moved into adjoining warehouses. The sugar would then be moved from the warehouses to the melt wash house, and from there through the melt filter house where refining would take place. The refined sugar was then transported via conveyors to a large warehouse located south of Twenty-Third Street where it would be stored in bags prior to shipment via rail or ship.³⁹

³⁵ George R. Adams, National Register of Historic Places Inventory – Nomination Form: "San Francisco Yard" (Nashville, TN: unpublished nomination form prepared by the American Association for State and Local History, 1978), 8-3.

³⁶ Ibid., 8-4.

³⁷ Christopher VerPlanck, Context Statement—Dogpatch Cultural Resources Survey (San Francisco: 2001), 7-8.

³⁸ The Bay of San Francisco: The Metropolis of the Pacific Coast and its Suburban Cities (San Francisco: The Lewis Publishing Co., 1892), 309. Ivan Peterson, The Risdon Iron Works (San Francisco: unpublished manuscript at the Maritime Museum Library, 1948), 1-6.

³⁹ Michael Corbett, *Historic Architecture Report for 4352 Third Street, City and Count of San Francisco* (San Francisco: unpublished report by URS Corporation, 2001), 7.

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San Francisco Gas 🔄 Electric Co, Pier 70. (1897-1905)

In 1897, industrialist Joseph Crockett's San Francisco Gas & Electric took over the San Francisco Gas Light Company plant at Potrero Point.⁴⁰ In 1898, powerful sugar magnate Claus Spreckels complained to Joseph B. Crockett about smoke from Crockett's downtown gas light plant damaging Spreckels' Market Street office building. When Crockett apparently brushed off Spreckels' complaints, the latter hired New York engineer, A.M. Hunt, to design a competing gas light plant to be erected next to Speckles' Western Sugar Refinery on the former site of the California Barrel Company facility at Potrero Point. Completed in late 1901, Spreckels' Independent Electric Light & Power Company expanded the physical presence of the gas light generation industry at Potrero Point. The facility, which consisted of a large powerhouse (Station A), a machine shop, a meter house, and a purifying house on Humboldt Street, was eventually sold to San Francisco Gas & Electric Company in 1903 following the ouster of Crockett.⁴¹

California Barrel Company (1886-1900)

In 1900, California Barrel Company sold part its old facility to adjoining landowner, Claus Spreckels, and consolidated its operations on a block bounded by Twenty-Second, Michigan, Humboldt, and Illinois streets. The 1899-1900 Sanborn map still shows the company as owning two parcels, with the main manufacturing works and offices located at the corner of Twenty-Second and Illinois streets. Storage was accommodated in several warehouses on a large tract located on Humboldt Street. According to the Sanborn map, both facilities had rail access provided by Southern Pacific spurs.

Railroads (1886-1900)

After the demise of the first Western Pacific Railroad in the 1870s, the Southern Pacific enjoyed a monopoly in the Central Waterfront throughout the rest of the 19th century. Although the state gave most of Mission Bay to the railroad in 1869 with the condition that it fill the mud flats, the 1899-1900 Sanborn map indicates most of the area remained unfilled. The Southern Pacific ran an extensive network of spur tracks throughout the entire Central Waterfront area, using Illinois Street as its principal right-of-way. Prior to the completion of the Southern Pacific's Bayshore Cut Off line in 1907, the track network in the Central Waterfront effectively operated as a cul-de-sac, transporting manufactured goods from the Central Waterfront to the main Southern Pacific Depot at Fourth and Townsend streets via tracks that ran along a route following sections of Illinois, Seventeenth, Pennsylvania, and Seventh streets. From there, trains would depart for the main railhead in Oakland from a car ferry slip located at the foot of King Street.⁴² According to the 1899-1900 Sanborn map, the Southern Pacific also maintained a large network of livestock corrals on the west side of Illinois Street, between Twentieth and Twenty-Second streets (now the location of the American Can Company).

In search of a more direct and level route from San Jose to San Francisco, the Southern Pacific began building the so-called "Bayshore Cut Off" in 1904. The Cut Off ran along the eastern shore of the San Francisco Peninsula from San Bruno to the San Francisco Depot. In effect, this project relocated the main north-south trunk line through the western part of the Central Waterfront area. The route was partially trenched and tunneled through the eastern edge of Potrero Hill following the alignments of Pennsylvania and Iowa streets. The project, which cost almost ten million dollars, resulted in the construction of two double-track tunnels through the Central Waterfront: one at the intersection of Twenty-Second and Iowa streets (Tunnel No. 1), and the other at Twenty-Third and Iowa (Tunnel No. 2). Constructed of concrete with Romanesque facades of brick and sandstone, these tunnels still exist, providing a unique gateway to downtown San Francisco. The network of two tunnels and two bridges at Twenty-Second and Twenty-Third streets were determined eligible for listing in the National Register in the Planning Department's Central Waterfront Survey of 2001.⁴³

The Atchison, Topeka & Santa Fe (Santa Fe) Railroad entered the Bay Area following the completion of a competing railhead at Richmond in 1900. The Santa Fe acquired several blocks of land in the Central Waterfront, and established a network of tracks that ran north along Illinois Street, with a parallel line running along Iowa and Indiana streets. The company's real estate arm, the Santa Fe Land

⁴⁰ Ward Hill, *Historic Architecture Report: Station A Potrero Power Plant in the City and County of San Francisco* (San Francisco: Dames & Moore, 1999), 3-4. ⁴¹ Ibid.

⁴² Sanborn Fire Insurance Company, Sanborn Maps for San Francisco, California: Volume 5 (1899-1900).

⁴³ Moses Corrette, State of California Department of Parks and Recreation Primary Record: "Bayshore Cutoff Tunnels No. 1 & 2" (San Francisco: San Francisco Planning Department, 2001).

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Improvement Company, acquired a substantial portion of the rugged Potrero Point peninsula, including much of the acreage that comprises the northern part of Dogpatch—including sections of Blocks 4043, 4060, 4061, 4106, and 4107. The Santa Fe Land Improvement Company gradually graded and developed its holdings in the area, including the construction of a row of cottages along the 900 block of Minnesota Street, and the construction of two large brick warehouses which still stand: the Schilling Wine Cellars warehouse at 900 Minnesota, and the Hulme & Hart Wool Scourers plant at 800-50 Tennessee Street.⁴⁴

Other Industries (1886-1900)

According to the 1899-1900 Sanborn maps, the Central Waterfront area was still largely confined to Potrero Point, although grading operations had expanded development westward toward Potrero Hill and filling operations had created new industrial sites along the margins of San Francisco and Mission bays. A handful of smaller industries were located on the periphery of the established industrial core of the Central Waterfront, including the Potrero Compressed Yeast and Vinegar Works at Twenty-Third and Minnesota streets, and the Ohlandt & Co. Bone Black and Fertilizer Works located immediately south at Twenty-fifth and Minnesota streets. Both of these businesses were on the 1886 Sanborn map. The nearby Chinese shrimpers' village which appears on the 1886 Sanborn map had, by the time of the 1899-1900 map, been partially taken over by a small poultry ranch. Much of the northern part of the Central Waterfront— the area bounded by Iowa, Sixteenth, and Mariposa streets and San Francisco Bay—was still unfilled marshland and tidal lagoon. The only significant industrial development in the area consisted of two oil companies. According to the 1899-1900 Sanborn map, Union Oil Company maintained a small oil distribution facility on the northwest corner of Mariposa and Illinois streets. The much larger Arctic Oil Works/Pacific Steam Whaling Company complex was located on a large tract of filled ground that extended into San Francisco Bay between Sixteenth and Seventeenth streets. Formed in 1877 to hunt Arctic whales, the company established a refinery to process lamp oil on the property. Administered by former California Governor, George C. Perkins, Arctic Oil Works became one of the largest whale oil producers in the world until the popularity of gas and electricity put it out of business in the early 20th century.⁴⁵

Residential and Commercial Development (1886-1900)

Residential Development (1886-1900)

The expansion of Union Iron Works was the most significant factor behind the continued residential development in the Central Waterfront during the 1880s and 1890s—and beyond. Indeed, for the next seventy years the fortunes of the neighborhood ebbed and flowed with the largest shipyard on the West Coast. Observers of the "new Potrero" remarked on the steadily growing residential character of the area as it was transformed from a quasi-rural district into a workingman's suburb inhabited largely by immigrant families employed in the industries of Potrero Point.

The residence portion of the Potrero may be said to be divided like ancient Gaul, into three parts, the "old town" is that first divided, mentioned as crowning the heights above the waterside factories, and the principal means of gaining access to which are long flights of stairs [Irish Hill]. Another section is that which has also been referred to as lying to the southwest in the valley next to the cordage factory [Dogpatch].

The days of the cliff-dwellers is passing. Many and many scores of modest homes still crown the heights which frown above the great waterside factories, and to which the principal means of access is still long flights of wooden stairs, but it is upon the gentler and more pleasing sites that rows of cottages, in later days erected, are located, and for long stretches of level or slightly rising streets, bordered by broad, tree-shaded sidewalks mark the new Potrero...⁴⁶

The density of residential development in Dogpatch remained sparse in comparison with Irish Hill until the early years of the 20th century. Reasons for the relatively uneven level of development include the fact that much of land was occupied by large rock outcroppings or owned by the Atchison, Topeka & Santa Fe railroad. The 1899-1900 Sanborn map indicates that approximately half of the existing enclave of Dogpatch was occupied by clusters of single-family cottages and two or three-story flats. The central section of the neighborhood, a roughly four-block area bounded by Eighteenth, Kentucky, Minnesota and Kentucky Place, remained

⁴⁴ Christopher VerPlanck, Context Statement-Dogpatch Cultural Resources Survey (San Francisco: 2001), 4-5.

⁴⁵ Hubert Howe Bancroft, History of California, Volume 6 (San Francisco: A.L. Bancroft and Co., 1888), 408-11.

⁴⁶ San Francisco Examiner (August 11, 1889).

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undeveloped. Notes on the map indicate that the blocks were occupied by steep banks rising to 50 feet in elevation.

Despite the obstacles, some of the Central Waterfront's most distinctive residential development occurred between 1886 and 1900. One of the most significant developments was a cluster of identical Eastlake cottages erected on the northern end of a block bounded by Twentieth, Tennessee, Twenty-Second, and Minnesota streets. This cluster of thirteen extant (originally sixteen) identical Eastlake style workers' cottages stands near the central part of the area on Tennessee and Minnesota streets, between Twentieth and Twenty-Second streets. The so-called "Pelton cottages" were constructed between 1890 and 1891 by a local carpenter named Rees O. Davis for speculators Jacob and John O. Reis. The Reis brothers owned more land in Dogpatch than any other entity beside the Santa Fe Land Improvement Company, but instead of developing their lands for industrial use, the Reis' constructed small wood-frame cottages which were rented to local workers. The cottages were based on architectural patterns drawn up by San Francisco architect John Cotter Pelton, Jr. A progressive housing reformer and practicing architect, Pelton published plans of inexpensive, single-family cottages in the pages of the *San Francisco Bulletin* between 1880 and 1883. The plans and specifications were geared toward working-class San Franciscans intent on building their own housing, but who could not afford to hire an architect. Ironically, many speculators used the plans to build rental housing, such as the cottages built by the Reis brothers in Dogpatch.⁴⁷

Around 1890, the Santa Fe Land Improvement Company utilized John Cotter Pelton Jr.'s plans to build a row of seven identical double houses on the west side of Minnesota Street between Twentieth and Twenty-Second streets. Between 1890 and 1900, the Santa Fe Land Improvement Company rented these cottages to its railroad workers. In 1900, the company decided to redevelop the large lot with a brick warehouse (the Schilling Cellars Wine Warehouse). But instead of tearing the cottages down, the Santa Fe sold the cottages to John O. Reis, who moved them to a large parcel with frontage on both Tennessee and Kentucky streets. Most of the relocated Kentucky Street cottages were demolished when Third Street was widened in the 1930s; however, one of the cottages survives at 997-99 Tennessee Street.⁴⁸

Meanwhile, Irish Hill continued to grow with large residential hotels going up along Illinois, Michigan, and Humboldt streets. Although there were still cottages and shacks perched on the hill above Union Iron Works, the expansion of the plant before and during the Spanish-American War resulted in the removal of some of the hilltop dwellings as the bank was cut away to build more warehouses and workshops. Ethnically, both Irish Hill and Dogpatch were predominantly Irish by 1900, although Dogpatch was more ethnically diverse, with German, Scandinavian, Japanese, and Italian residents.⁴⁹

Civic Development (1886-1900)

Both Irish Hill and Dogpatch were served by the new Irving Scott School, built in 1895 (extant). Originally founded in 1865 on the corner of Twentieth and Kentucky streets, the school was moved to its current site in 1877. Swamped with the children of workers, the San Francisco School Department hired architect Thomas J. Welsh to design a \$22,834 addition facing Tennessee Street. This building, which still exists and is listed in the National Register of Historic Places, was renamed for Irving Scott, the superintendent of Union Iron Works. Scott, a benefactor of the school, contributed money and equipment and saw to it that the education program for boys centered on vocational training and trades useful for employment at Union Iron Works. The Central Waterfront received its first fire station in the early 1880s when the San Francisco Fire Department erected Station #16, an Eastlake-style, wood-frame firehouse at 1009 Tennessee Street (not extant). ⁵⁰ The 1899-1900 Sanborn map also shows a small police station located at 609 Twentieth Street (not extant).

Religious Institutions (1886-1900)

The religious and cultural life of Central Waterfront residents is reflected in the churches that began appearing in the area during the last quarter of the 19th century. The area acquired its first religious institution in 1869 when a congregation of seventeen Scottish ironworkers built a small Protestant chapel on Tennessee Street. The first regular Catholic services in the Central Waterfront began as early as the 1860s, when priests from St. Peter's began celebrating Mass in the dining room of the Breslin Hotel in Irish Hill. In 1880, Archbishop Patrick Riordan established a new parish in area, calling it St. Teresa, after St. Teresa of Avila. Father John Kenny was appointed the first pastor and a former warehouse was converted into a church. In 1892, Father Patrick O'Connell built a church for

 ⁴⁷ Christopher VerPlanck, *Context Statement—Dogpatch Cultural Resources Survey* (San Francisco: 2001), 14-15.
 ⁴⁸ Ibid

⁴⁸ Ibid

⁴⁹ Christopher VerPlanck, Context Statement—Dogpatch Cultural Resources Survey (San Francisco: 2001), 12-13.

⁵⁰ Christopher VerPlanck, Context Statement—Dogpatch Cultural Resources Survey (San Francisco: 2001), 13.

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the parish on the northeast corner of Nineteenth and Tennessee streets.⁵¹ Despite these developments, the Central Waterfront never appears to have been a hotbed of religious activity. The 1899-1900 Sanborn map shows only two churches within the survey boundaries: St. Teresa's at 600 Nineteenth Street, and Potrero Methodist Episcopal Church at 714-16 Tennessee Street (still extant).

Commercial Development (1886-1900)

As a small, self-contained industrial/residential neighborhood removed from the rest of the city, the Central Waterfront supported several compact commercial districts containing small green grocers, butcher shops and saloons. The principal commercial districts included Eighteenth Street between Iowa and Kentucky Streets, and Twenty-Second Street between Minnesota and Kentucky Streets. Prior to its demolition during the First World War, the intersection of Illinois and Twenty-Second Streets in Irish Hill was the most dynamic commercial area, accommodating saloons, groceries, cafes and other businesses. Most businesses were housed in the first floor of mixed-use commercial/residential buildings, with many merchants living above their business. Census research indicates that many neighborhood merchants worked in one of the local industries before opening their own business. Green grocers and butchers were frequently run by German immigrants, and saloons and general merchandise stores by Irish immigrants.⁵²

HISTORIC CONTEXT: DISASTER AND RECOVERY IN THE CENTRAL WATERFRONT (1900-1929)

1906 Earthquake

The 1906 Earthquake was a major factor behind population growth in the Central Waterfront during the early 20th century. The 1906 Earthquake and Fire left approximately 250,000 San Franciscans homeless. Especially hard-hit was the working-class industrial/residential South of Market district. After the disaster, many earthquake refugees made their way to undestroyed parts of the Mission and Potrero districts where they squatted on parkland and empty lots. The Red Cross Relief Corporation established a large refugee camp on several parcels of unimproved Santa Fe land bounded by Eighteenth, Kentucky, and Indiana streets and Kentucky Place. By autumn 1906, the Army tents that housed the refugees were replaced with more substantial two-and-three-room prefabricated wood dwellings euphemistically called "refugee cottages." During the reconstruction of San Francisco, many earthquake refugees decided to move to outlying parts of the city, including the Central Waterfront.⁵³

Industrial Development (1900-1929)

Union Iron Works/Bethlehem Steel (1900-1929)

Between 1900 and 1929, Union Iron Works (known after 1905 as Bethlehem Steel San Francisco Yard) absorbed an increasingly larger share of the Central Waterfront's employment pool. By the conclusion of the Spanish-American War, the shipyard had become the single-largest employer in the neighborhood, employing approximately 25 percent of Dogpatch residents and an even higher proportion of Irish Hill residents. Other iron-working concerns, such as Risdon and Atlas Iron Works, employed additional neighborhood residents until the companies were absorbed by Bethlehem Steel. The years leading up to American participation in the First World War witnessed significant growth at Bethlehem Steel's San Francisco Yard. Several million dollars were invested in modernizing and expanding the shipyard, including the addition of a state-of-the-art powerhouse (Building 102—extant) in 1912. With orders from the British and American Navy, the shipyard expanded south into what was Irish Hill. A new office building (Building 101—extant) was completed in 1917 at the northeast corner of Twentieth and Illinois streets. Also in 1917, Bethlehem Steel added a concrete foundry (Building 115—extant), an ordnance repair shop/warehouse (Building 116—extant), and various smaller machine shops, watch buildings, gatehouses and washrooms. By 1920, Bethlehem Steel employed 50 percent of the householders in the Central Waterfront.⁵⁴

Western Sugar Refinery (1900-1929)

Western Sugar Refinery remained one of the top five employers in Dogpatch until the early 1950s, employing 1,000 men and between 10 percent and 15 percent of local residents. Throughout this period, Claus Spreckels' sons, J. D. and A. B. Spreckels, ran the company. In

⁵¹ Margaret Henry, "Potrero Hill History," prepared for Potrero Neighborhood Bicentennial Festival, 1976.

⁵² Christopher VerPlanck, Context Statement—Dogpatch Cultural Resources Survey (San Francisco: 2001), 12.

⁵³ Interview with Edward Cicerone, conducted by Cheryl and Clark Taylor, (May 1964).

⁵⁴ United States Census Schedules (1920).

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response to growing competition from C & H Sugar, they continually modernized the Potrero Plant, which remained the company's primary refiner of raw Hawaiian sugar on the mainland. In 1923 and 1929, respectively, the Spreckels brothers demolished several older wood-frame sugar warehouses and replaced them with modern concrete warehouses-two of which still survive at 435 Twenty-Third Street. A comparison of the 1900 and 1913 Sanborn maps illustrates the growth of the Western Sugar Refinery, with several new warehouses located alongside the East Wharf.

Pacific Gas 🗢 Electric Co. (1905-1929)

The merger of the California Gas & Electric Company with San Francisco Gas & Electric in 1905 created Pacific Gas & Electric (or PG&E), the largest private utility company on the West Coast. PG&E went on to run its San Francisco Plant, known as Station A, for the next 70 years. The 1913-15 Sanborn map illustrates the PG&E plant, which at this time wraps around Spreckels' Western Sugar Refinery on three sides. The steam plant housed in the large brick building at the center of the property was the largest of its kind in the West until 1913. Operating as a "peaker" plant, Station A was nonetheless capable of supplying electricity for the entire city. Station A was heavily remodeled and upgraded in 1929-30. A large section of the original building still exists, as well as the historic office building and gatehouse. The San Francisco plant also contained remnant gas manufacturing facilities from the old City Gas Company. Housed in three buildings adjoining two large gas tanks at the western end of the property, the facility featured the 1914 meter house (extant), the 1924 compressor house (extant), and the purifying house (demolished 1960). During the first decades of the 20th century, PG&E employed a relatively small number of Central Waterfront residents. Not as labor-intensive as Bethlehem Steel or other manufacturing operations, PG&E relied on a small, highly trained staff.⁵⁵

California Barrel Company (1900-1929)

The California Barrel Company continued to grow throughout the first quarter of the 20th century after consolidating its operations at Twenty-Second and Illinois streets in 1900. Barrels remained in high demand throughout much of the period for use as wine caskets and maritime shipping containers. The 1913-15 Sanborn map indicates that the California Barrel Company had expanded beyond its cramped property to include the former site of the Pacific Refining and Roofing Company facility on Michigan Street.⁵⁶

Tubbs Cordage Company (1900-1929)

In addition to the California Barrel Company, other maritime-related industries continued to thrive in the Central Waterfrontnotably the Tubbs Cordage Company, which was the oldest surviving industry in the area. Although its offices were located at 200 Bush Street, Tubbs Cordage continued to operate its plant in the Central Waterfront. The 1913-15 Sanborn map indicates that the configuration of the factory underwent relatively little change from 1900, although the 1,500 foot ropewalk had been truncated so that it terminated at Kentucky Street. According to Census records, the company continued to employ a fairly large number of local residents on the ropewalk, and also as overseers and mechanics.57

American Can Company (1900-1929)

American Can Company, the largest manufacturer of cans in the United States, was the last industrial company to construct a major facility in the Central Waterfront. By 1910, there were few suitable vacant parcels remaining in the area. Early in 1915, American Can Company purchased a two-block tract of land bounded by Twentieth, Illinois, Twenty-Second, and Kentucky streets. On the 1913-15 Sanborn map, this tract appears mostly vacant and ungraded. The American Can Company began construction on the first section of its new facility in 1915. Designed by architect N.M. Loney, the \$172,000 factory was completed in 1916. The facility was expanded in 1923 and 1929, and eventually employed upwards of 1,200 workers, becoming one of the largest employers of Central Waterfront residents by 1930. The entire four-building complex remains intact today.⁵⁸

Railroads (1900-1929)

From 1900 onward, the Southern Pacific yielded most of the Central Waterfront to its rival, the Santa Fe Railroad. As a major

⁵⁵ Ward Hill, Historic Architecture Report: Station A Potrero Power Plant in the City and County of San Francisco (San Francisco: Dames & Moore, 1999), 3-4.

⁵⁶ 1913-15 Sanborn Fire Insurance Map.

^{57 1913-15} Sanborn Fire Insurance Map.

⁵⁸ Moses Corrette, State of California Department of Parks and Recreation Primary Record: "American Can Co. Building" (San Francisco: San Francisco Planning Department, 2001).

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landowner in the area, the Santa Fe used its money and influence to construct miles of spur tracks to connect its properties with its main freight tracks on Illinois Street. The Santa Fe also had a secondary line that branched off of Illinois Street at Twenty-fifth Street before heading north along Iowa Street. From there it ran parallel to the Southern Pacific's Bayshore Cut Off to the new Southern Pacific Roundhouse at Mariposa and Minnesota streets. During this period, a third railroad company entered the market: the Western Pacific. Reusing the name of the its long-defunct namesake, the new Western Pacific reached the Central Waterfront via a car ferry slip located on landfill at the foot of Twenty-fifth Street. From there, the line ran west along Twenty-fifth to Iowa Street where it passed beneath Potrero Hill through a tunnel and continued on to the newly developed warehouse district on the western edge of Mission Bay. After acquiring water lots on either side of its car ferry slip, the real estate arm of the Western Pacific—the Standard Realty Development Company—began to fill in the bay south of Twenty-Third Street.⁵⁹ Meanwhile, the 1913-15 Sanborn maps indicate that Mission Bay had been entirely filled by the Southern Pacific, which then built a large roundhouse on the site.

Other Industries (1900-1929)

By the end of the 1920s, the Central Waterfront was increasingly dominated by a handful of major industries as discussed above. Many of the smaller industries that appear on the 1899-1900 Sanborn maps or in city directories had either been absorbed by the larger companies or had gone out of business. Arctic Oil Works was replaced by Hooper Lumber Company at the foot of Mariposa and Seventeenth streets. Other lumber yards, displaced from the South of Market, relocated to the Central Waterfront-most notably Christenson Lumber, located opposite Hooper Lumber Company at Mariposa and Illinois streets. A small area just south of the lumber yards along the Bay remained the province of small boat yards, including John Twigg & Sons and G.W. Kneass Boat Building. Located opposite the shipways of the mighty Bethlehem Steel shipyard, the smaller boat yards took advantage of the deep water of the Central Basin. Between 1900 and 1915, the Santa Fe Land Improvement Company leased much of its landholdings in the Central Waterfront to various businesses, including construction supply, iron working, and alcohol makers and distributors. Early examples of industrial buildings erected on Santa Fe land include: the 1900 San Francisco Building & Manufacturing Company at 800 Tennessee Street (extant), and the 1906 Schilling Wine Cellars building at 900 Minnesota (extant). The 1913-15 Sanborn map also shows several large, wood-frame, steel-clad warehouses and manufacturing facilities, including C. J. Hillard Co. Ornamental Iron Work at 654 Minnesota Street (demolished), George Tay & Co. Plumber Supplies at 700 Minnesota (demolished), and Ralston Iron Works at 830 Indiana Street (extant). At the southern end of the study area, the Central Waterfront remained largely unfilled tidal flats, although the Western Pacific had begun filling along Twenty-Fifth Street. In addition to some dilapidated houses, the only industry in the southern part of the study area was the Potrero Compressed Yeast and Vinegar Company at 1255 Indiana Street.

Residential, Commercial, and Civic Development (1900-1929)

Residential Development (1900-1929)

Before the widespread ownership of automobiles in the early 1920s, most workers employed by Bethlehem Steel's San Francisco Yard and other local industries sought housing within walking distance of their jobs. However, with most of Irish Hill gone and Dogpatch increasingly hemmed in by industry, many of the newcomers took up residence as boarders with local families or in the large residential hotels located along Kentucky and Twenty-Second streets. Between 1910-1920, residential construction declined in the Central Waterfront due to the lack of available land. Rocky outcroppings on residential blocks were expensive to grade and still blocked several streets in the area. In September 1910, the Potrero Improvement Club demanded that the City remove a 30-foot-high mound of serpentine rock that blocked the intersection of Tennessee and Twentieth streets.⁴⁰ Later that year, the Department of Public Works dismantled the hill and dumped the rocks in a large, four-block square lagoon, referred to locally as the "Red Sea."⁶¹ Nevertheless, with ownership of much of the northern part of the neighborhood controlled by the Santa Fe Land Improvement Company, residential development in the Central Waterfront had reached a standstill by the early 1920s. Workers with other options began moving away to neighborhoods where better housing was available and industrial nuisances fewer.⁶² As older Irish-American and German-American residents moved away from the Central Waterfront, they were replaced in large part by Italian and Latin-

⁵⁹ 1913-15 Sanborn Fire Insurance Map. Moses Corrette *Central Waterfront Cultural Resources Survey Summary Report and Draft Context Statement* (San Francisco: San Francisco Planning Department, 2001), 17.

^{60 &}quot;Potrero Demands Improvements," San Francisco Evening Call (September 4, 1910).

^{61 &}quot;One Stone Pile Kills Two Birds," San Francisco Morning Call (November 17, 1910), p. 7.

⁶² Christopher VerPlanck, Context Statement-Dogpatch Cultural Resources Survey (San Francisco: 2001), 19.

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American immigrants.

Institutional Development (1900-1929)

Between 1900 and 1929, the City constructed several institutional buildings in the Central Waterfront in an effort to cope with the expanding population. In 1912, City Architect, John Reid Jr., designed the new Potrero Police Station which was built on a large parcel on the southwest corner of Kentucky and Twentieth streets (extant). Prior to being developed by the City, this lot had featured an ungraded, 60-foot-high outcropping of serpentine. Three years later, John Reid Jr. designed a similarly detailed public hospital for the southern portion of the same lot (extant). The Potrero Emergency Hospital was necessary to cope with injured shipyard workers, who typically had little recourse beyond the company dispensary. Within the next decade these two important public buildings were joined on the site by another John Reid Jr. building: San Francisco Fire Department's Station #16, located at 909 Tennessee Street (extant).

Commercial Development (1900-1929)

Due to the proximity to the nearby residential development, the Central Waterfront continued to see commercial development along established corridors, including the blocks on Eighteenth Street between Iowa and Kentucky Streets, and Twenty-Second Street between Minnesota and Kentucky Streets. Based upon the Sanborn Fire Insurance Maps, commercial developments that occurred in the neighborhood included small drug stores, restaurants (lunch counters), laundries, and a moving picture houses (1201 Kentucky and 1338 Kentucky). In 1917, the first bank, Anglo California Trust Company, was constructed in the neighborhood at 2290-2298 Third Street. This bank was the only financial facility to serve the neighborhood during this time period.

HISTORIC CONTEXT: DEPRESSION AND SECOND WORLD WAR IN THE CENTRAL WATERFRONT (1930-1948)

Industrial Development (1930-1962)

Bethlehem Steel's San Francisco Yard (1930-1948)

After World War I, Bethlehem Steel's San Francisco Yard suffered through a post-war slump, but business recovered by the mid-1920s. By 1938, the shipyard had built 142 vessels, including submarines, oil tankers, freighters, ferries and other passenger ships. Growing orders caused Bethlehem to modernize the plant between 1936 and 1938. The outbreak of war in Europe in 1939 led to more orders from the U.S. Navy. From 1939 to 1944, Bethlehem Steel expanded the shipyard to its greatest extent, employing 18,500 workers in round-the-clock shifts. New cranes, shipways, and equipment were installed to bring the shipyard up to the most modern standards. During World War II, the shipyard built 52 warships and retrofitted another 2,500.⁶³ Expansion of the yard to the south resulted in the elimination of the surviving residential structures on Irish Hill. The concentrated area of residential hotels and businesses that stood on the block bounded by Twentieth, Michigan, Twenty-Second, and Illinois streets was also cleared for a training school and offices. Most of the new buildings were semi-permanent, steel-frame, corrugated steel-clad warehouses and shops—many of which survive today, including Plate Shop No. 2 and Machine Shop No. 2. Additional slips were erected on the eastern part of the property, where Risdon Iron Works once stood. Once the most important privately owned shipyard in the Bay Area, Bethlehem Steel's San Francisco Yard was joined in the war effort by several other major shipyards, including Marinship, the Kaiser yards in Richmond, and Bethlehem Steel's Alameda Yard. Plans of the San Francisco Yard drawn up by Bethlehem Steel in 1944 illustrate a densely built-out facility with nearly every square foot covered with buildings, slips, storage or parking. The only section of Irish Hill that was left was an 80-foot ledge on the alignment of Michigan Street.⁶⁴

Western Sugar Refinery (1930-1951)

By the eve of World War II, the Spreckels family was facing increasing competition from C & H Sugar. In August 1948, the Spreckels brothers decided that sugar refining was no longer profitable and closed the Potrero Plant. In April 1949, C & H Sugar Refining Corporation bought the refinery for \$3,780,000. However, the new owners—facing labor trouble in Hawaii—were overextended and

⁶³ Moses Corrette Central Waterfront Cultural Resources Survey Summary Report and Draft Context Statement (San Francisco: San Francisco Planning Department, 2001), 10.

⁶⁴ 1948-50 Sanborn Fire Insurance Maps. "Plans of San Francisco Yard, San Francisco, California: Bethlehem Steel Company Shipbuilding Division, 1944."

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came to the conclusion that the Potrero Plant was too antiquated to be profitably modernized. In 1951, C & H demolished most of the buildings and sold the machinery for scrap.⁶⁵ The 1948-50 Sanborn map shows the Western Sugar Refinery much as it had appeared in 1915, as demolition had not yet begun. Of those parts of the complex that were spared demolition in the 1950s, the ten-story refinery building survived until the early 1990s, and two 1920s-era warehouses continue to stand today, although they are threatened with demolition.

Pacific Gas 🔗 Electric (1930-1949)

In 1930, PG&E rebuilt Station A as a high pressure steam-electric plant. The work involved installing new machinery and rebuilding the south and west facades of the building in the Renaissance Revival style. A new addition was likewise constructed to house boiler equipment and new turbines, giving the plant its 130,000 kilowatt capacity—the second largest generating capacity of any plant west of the Rockies. Following the dismantling of much of the Western Sugar Refinery, PG&E purchased the land and expanded Station A onto the site with new "open-design" facilities. Station A continued in operation until 1983, when the boiler room was demolished.⁶⁶ According to the 1948-50 Sanborn map, the PG&E plant had not yet expanded into the adjoining Western Sugar Refinery facilities. Its gas plant was still in operation north of Station A, and several large gas tanks had been built. In addition, a new compressor building had been constructed west of Station A in 1926 (extant).

California Barrel Company (1930-1957)

With the introduction of steel containers in the 1950s, the demand for barrels for shipping and storage began to decline. The 1948-50 Sanborn map indicates that the plant was still in operation, but it had not been expanded or clearly modernized since 1915. The California Barrel Company remained in operation until the early 1950s, when the plant was torn down and replaced by the American Can Company Annex in 1955.

Tubbs Cordage Company (1930-1963)

Tubbs Cordage Company continued to manufacture and supply rope at its San Francisco Plant through World War II, and remained the largest rope manufacturer in the western United States into the early 1950s. By 1963, though, the operations had outgrown the outmoded Central Waterfront facility. Seeking cheaper labor overseas, Tubbs Cordage closed the plant in 1962.⁶⁷ Over the next 15 years, the facilities were gradually dismantled or demolished and replaced with a MUNI maintenance yard in the 1970s.⁶⁸

American Can Company (1930-1955)

Following the Second World War—and the concurrent slump in shipbuilding—American Can Company became the largest employer in the Central Waterfront area, employing over 1,200 workers in the manufacture of tin cans and canning fruit. Many of the employees were women, who were reputed to be better workers than men. In 1955, the company expanded its facilities south of Twenty-Second Street, constructing a large concrete annex on the block formerly occupied by the California Barrel Company.

Central Waterfront's Small Oil Companies

Within the survey boundaries of the Central Waterfront, the intersection of Third and Eighteenth streets represents a cluster of small oil companies built circa 1930. Union Oil Company had maintained a small oil distribution business on the corner of Mariposa and Illinois streets since the early 1900s. This facility was eventually joined by several other companies that sold fuel oil to shipping companies. Located on two blocks bounded by Mariposa, Illinois, Nineteenth and Third Streets, the companies included Norwalk Co. at 925 Mariposa; Crescent Pacific Oil Co. at 2065 Third Street; General Petroleum Company at 2085 Third Street; Seaside Oil Company at 2121 Third Street; and Hancock Oil Company at 500 Nineteenth Street. While relatively small in scale, the businesses typically consisted of an office building, warehouse and freestanding tanks. Today, only Seaside Oil Company remains largely intact.

Other Industries

⁶⁵ Michael Corbett, *Historic Architecture Report for 4352 Third Street, City and Count of San Francisco* (San Francisco: unpublished report by URS Corporation, 2001), 7.

⁶⁶ Ward Hill, Historic Architecture Report: Station A Potrero Power Plant in the City and County of San Francisco (San Francisco: Dames & Moore, 1999), 5-6.

⁶⁷ Maria Colyaco, California's Master Ropemakers (Manila: Manila Cordage Company, 1977).

⁶⁸ National Register of Historic Places Inventory - Nomination Form: "Tubbs Cordage Company Office Building," (San Francisco: National Park Service, 1978), 3.

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From the 1920s through the 1950s, the residential enclave of Dogpatch increasingly succumbed to industrial redevelopment. In addition, the Santa Fe Land Improvement Company had leased or sold the rest of its undeveloped holdings in the area for industrial uses. Both factors led to the construction of dozens of one and two-story concrete (and in some cases, brick) industrial buildings housing a variety of business types. Much of this development occurred along Minnesota and Indiana streets between Mariposa and Twentieth streets, and on the newly filled Western Pacific lands south of Twenty-Third streets. Examples include the Bowie Switch Company Building at 815-25 Tennessee Street, which was built in three phases from 1926 to 1942 (extant). They also include the American Meter Company built in 1929 at 900 Tennessee Street (extant), and a row of three identical concrete buildings constructed in 1956 at 760-90 Tennessee Street for Wesco Manufacturing (extant).

Much of Third Street had become industrialized as well, beginning with the east side in the 1920s. Here, small brick light industrial and warehouse buildings replaced residential and commercial uses on the edge of Irish Hill. Some of the best examples include the 1919 Albert Candy Company Building at 2201-03 Third Street (extant), and the 1924 Max Levin & Co. Scrap Warehouse located at 2225 Third Street (extant). A 20-foot widening of Third Street in 1937 (Kentucky Street was renamed Third Street around the same time), resulted in buildings along the west side of the street being moved back on their lots, demolished, or truncated. This period also saw many landowners replace existing residential buildings with light manufacturing, warehousing, or auto repair facilities, further contributing to the expansion of industry westward into Dogpatch. Examples include the 1937 Goodyear Rubber Co. building at 2400 Third Street (extant), and the 1939 Pellegrini Brothers Winery at 2360-64 Third Street (extant).

Transportation Infrastructure

The rise of the trucking industry reduced the Central Waterfront's dependence on water transport and rail beginning in the late 1930s. To accommodate the increase in vehicular traffic, Third Street was widened in 1937, and became a major thoroughfare from downtown to the southeastern section of the city.⁶⁹ As a result, trucking-oriented businesses began locating in the Central Waterfront area, as did truck and auto repair facilities.⁷⁰ This growth is evident in distribution warehouses that have rails along one side and loading docks for trucks on the other. The use of standard containers for trucks accelerated the downfall of rail service.

Land Transformation

Islais Creek and its surrounding wetlands were viewed as a barrier to the southern development of San Francisco. Organized efforts for reclamation were unsuccessful until 1925, when the passage of State legislation enabled the creation of the Islais Creek Reclamation District. The District subsequently filled the tidelands and dredged Islais Creek, constructing a turning basin at its western end to allow room for ship maneuvering. The most recent filling of Islais Creek occurred during the construction of Pier 80, formerly the Army Street Ship Terminal. Financed by a bond issue approved by California voters in 1958, the terminal went into operation in 1967.⁷¹

Industrial development in the blocks immediately north of Islais Creek was delayed by use of the land for temporary military housing during World War II. The housing was demolished after the war and the area was developed as an industrial park with single-story concrete buildings. The development of southern portions of the study area likewise occurred largely after World War II, and included food and oil processing plants constructed south of Army Street (now Cesar Chavez Street). In 1948, Cargill Inc. constructed a plant and refinery for coconut oil, adding an administration building in 1956. The Granex Corporation, a copra processing plant, erected the copra-loading crane in 1965. A report by the Copra Crane Labor Landmark Association states that the crane is significant as it is purportedly the last piece of machinery on the port of San Francisco hand-operated by longshoreman working bulk cargo. The crane also represents the ties of San Francisco's economy with those of the South Pacific Islands. A number of other cranes used in the outfitting of naval and merchant vessels are also still extant, serving as visual icons of the Pier 70 area.⁷²

⁶⁹ Moses Corrette Central Waterfront Cultural Resources Survey Summary Report and Draft Context Statement (San Francisco: San Francisco Planning Department, 2001), 16.

⁷⁰ Ibid., 23.

⁷¹ San Francisco Department of City Planning, "Central Waterfront, An Area Plan of the Master Plan of the City and County of San Francisco," (1990), p. II.8.5.

⁷² Moses Corrette Central Waterfront Cultural Resources Survey Summary Report and Draft Context Statement (San Francisco: San Francisco Planning Department, 2001), 23.

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The buildings constructed for industrial and office uses during this time continued to be built of reinforced concrete or prefabricated steel. These buildings tended to be smaller and almost entirely devoid of ornamentation. Butler Buildings—prefabricated steel buildings that could be ordered from catalogs—were also erected in great numbers in the area.

Residential and Commercial Development (1930-1962)

During the late 1930s, residential construction had all but stopped in Dogpatch—the last remaining residential enclave in the Central Waterfront. As discussed previously, greater mobility among workers meant that they could commute from other neighborhoods via automobile or public transit. Although the influx of war workers into the area during the late 1930s and early 1940s increased the population, most took up residence in converted single-family housing or boarded. There was simply no room for new housing in the area, and industry was actively nibbling away at the edges of the residential enclave. Despite the influx of industry, the existing commercial development in the area resisted and renewed as evidenced by the 1940 face-lift to the only bank in the Central Waterfront neighborhood, the Anglo California Trust Company at 2290-98 Third Street.

Following the end of World War II, the neighborhood began to decline as jobs at the shipyard dried up and other industries, such as the Western Sugar Refinery and Tubbs Cordage Company, began closing shop or moving overseas. Between 1965 and 1980, jobs in the Central Waterfront dropped from 16,304 to 11,004, with most of the losses occurring in manufacturing and ship repair.⁷³ By the late 1960s, Dogpatch had deteriorated to the point where the San Francisco Planning Department considered demolishing it and rezoning it for industrial uses. Arson and industrial encroachment also took their toll, reducing the residential core of Dogpatch to what exists today. The 1980s witnessed a revival of the area, with an influx of artisans in search of inexpensive housing with character.

⁷³ San Francisco Department of City Planning, "Central Waterfront, An Area Plan of the Master Plan of the City and County of San Francisco," (1990), p. II.8.5.

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Thomas Tunny ttunny@reubenlaw.com

June 25, 2021

Delivered Via Email

President Diane Matsuda San Francisco Historic Preservation Commission 49 South Van Ness Avenue, Suite 1400 San Francisco, CA 94103

> Re: 555 19th Street Project Sponsor Submittal Planning Department Case No. 2020-008959CUA Hearing Date: July 8, 2021 Our File No.: 8807.01

Dear President Matsuda and Commissioners:

Our office is working with Martin C. Levin Investment Company, LLC, owner ("Property Owner") of the property located at 555 19th Street (the "Property"), at southeast corner of 19th and 3rd Streets in the Central Waterfront district. The Property is occupied by a historic three-story-over-basement, heavy timber-frame and brick commercial building known as the Alberta Candy Factory, shown below.



555 19th Street; view toward southeast from intersection of 3rd and 19th streets

San Francisco Office One Bush Street, Suite 600, San Francisco, CA 94104 tel: 415-567-9000 | fax: 415-399-9480 **Oakland Office** 492 9th Street, Suite 200, Oakland, CA 94607 tel: 510-527-5589

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Historic Preservation Commission June 25, 2021 Page 2

I. Legal Background

The Property is located in the UMU (Urban Mixed-Use) Zoning District. Under Planning Code Section 803.9(c), all uses are permitted as of right in buildings that are listed on or determined eligible for the California Register of Historical Resources by the State Office of Historic Preservation, and provided that the Historic Preservation Commission ("HPC") advises the Zoning Administrator that "allowing the use will enhance the feasibility of preserving the building." In addition, the HPC is directed to "review the proposed project for compliance with the Secretary of the Interior's Standards (36 C.F.R. § 67.7 (2001)), and any applicable provisions of the Planning Code."

In 2000, the Planning Department surveyed the property as part of its Central Waterfront Survey, and 555 19th Street was determined to be a contributor to the California Register-eligible Third Street Industrial Historic District. On July 30, 2021, the State Historical Resources Commission intends to act on the Property's nomination to the State and National Registers of Historic Places.

II. Office Uses Will Enhance the Feasibility of Preserving the Building

The Property Owner has proposed a change of use at the Property to office uses (the "Project"). The building permit drawings for the change of use have been provided by Staff. Allowing office uses at the Property will enhance the feasibility of preserving the building because it expands the universe of potential tenants, which better ensures the Property Owner will have a reliable rental income to support the maintenance and preservation of the building. This is especially important given the COVID-19 global pandemic and the unprecedented economic challenges faced by commercial landlords and tenants in San Francisco.

The change of use does not include any physical changes at the Property. In the early 2000's, the Property Owner completed a full seismic retrofit and several disabled access upgrades. The seismic work consisted of constructing a pair of moment frames inside the building and bracing the north wall. The accessibility work included creating a new grade-level entrance at 555 19th Street, installing self-closing doors, and completing assorted upgrades to the toilet rooms. Stanley Saitowitz's Natoma Architects designed an interior remodel.

Additional work completed in 2010-12 included the installation of a new fire sprinkler system, demolition of the non-historic rear shed addition, installation of a new HVAC system, and re-roofing. In 2013-2014, the Property Owner completed another \$250,000 of interior upgrades, also designed by Stanley Saitowitz, including various improvements to the toilet rooms, break rooms, and lobbies. Office uses at the Property will help finance all of this prior work to preserve the building.

In connection with the change of use, noted preservation architect Christopher VerPlanck prepared a Historic Building Maintenance Plan ("HBMP"), which Planning Department preservation staff has approved. Mr. VerPlanck has 24 years of experience evaluating historic Historic Preservation Commission June 25, 2021 Page 3

buildings in San Francisco. The maintenance recommendations contained in the HBMP are derived from the National Park Service's Preservation Briefs, including Preservation Brief Nos. 1, 2, 4, 6, 15, 18, and 41. The HBMP ensures that all future modifications to the building will comply with the Secretary of the Interior's Standards for the Treatment of Historic Properties, as required by Planning Code Section 803.9(c). Mr. VerPlanck also is processing the nomination of the Property to be listed on the State and National Registers of Historic Places.

III. Conclusion

For all of the foregoing reasons, we respectfully request that the HPC advise the Zoning Administrator that allowing office uses at the Property will enhance the feasibility of preserving the building, and find that the proposed project complies with the Secretary of the Interior's Standards for the Treatment of Historic Properties.

Very truly yours,

REUBEN, JUNIUS & ROSE, LLP

Thomas Tunny

cc: Monica Giacomucci, Planning Department Martin C. Levin Investment Company, LLC Christopher VerPlanck