

Giacomucci, Monica (CPC)

From: Katherine Doumani <kdoumani@gmail.com>
Sent: Wednesday, October 02, 2019 11:57 AM
To: Giacomucci, Monica (CPC)
Cc: Craig Peters; Maureen Deboer
Subject: 804 22nd street support

This message is from outside the City email system. Do not open links or attachments from untrusted sources.

Hello,

My name is Katherine Doumani and I am a neighbor to Maureen and Craig Peters at 804 22nd street. I am writing in support of their remodel project.

For a family raising children in San Francisco today, and in their case also housing their parents, the ability to expand a home is critical. The scope of their project is in keeping with the neighborhood of mixed-use commercial, PDR, and residential that makes our community so dynamic.

The construction will not disrupt the community an more than the never-ending work being done all around us, but the end result will offer their family the opportunity to stay here, in San Francisco, rather than losing yet another family to the areas with larger homes. Keeping families in SF is critical and I urge you to allow this remodel to go forward.

Thank you,

Katherine Doumani
1006 Tennessee St
SF CA

Received at HPC Hearing 10/2/19
M. Giacomacci

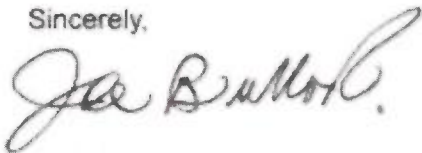
St. Stephen's Baptist Church
800 22nd Street
San Francisco, CA 94107

Oct. 1, 2019

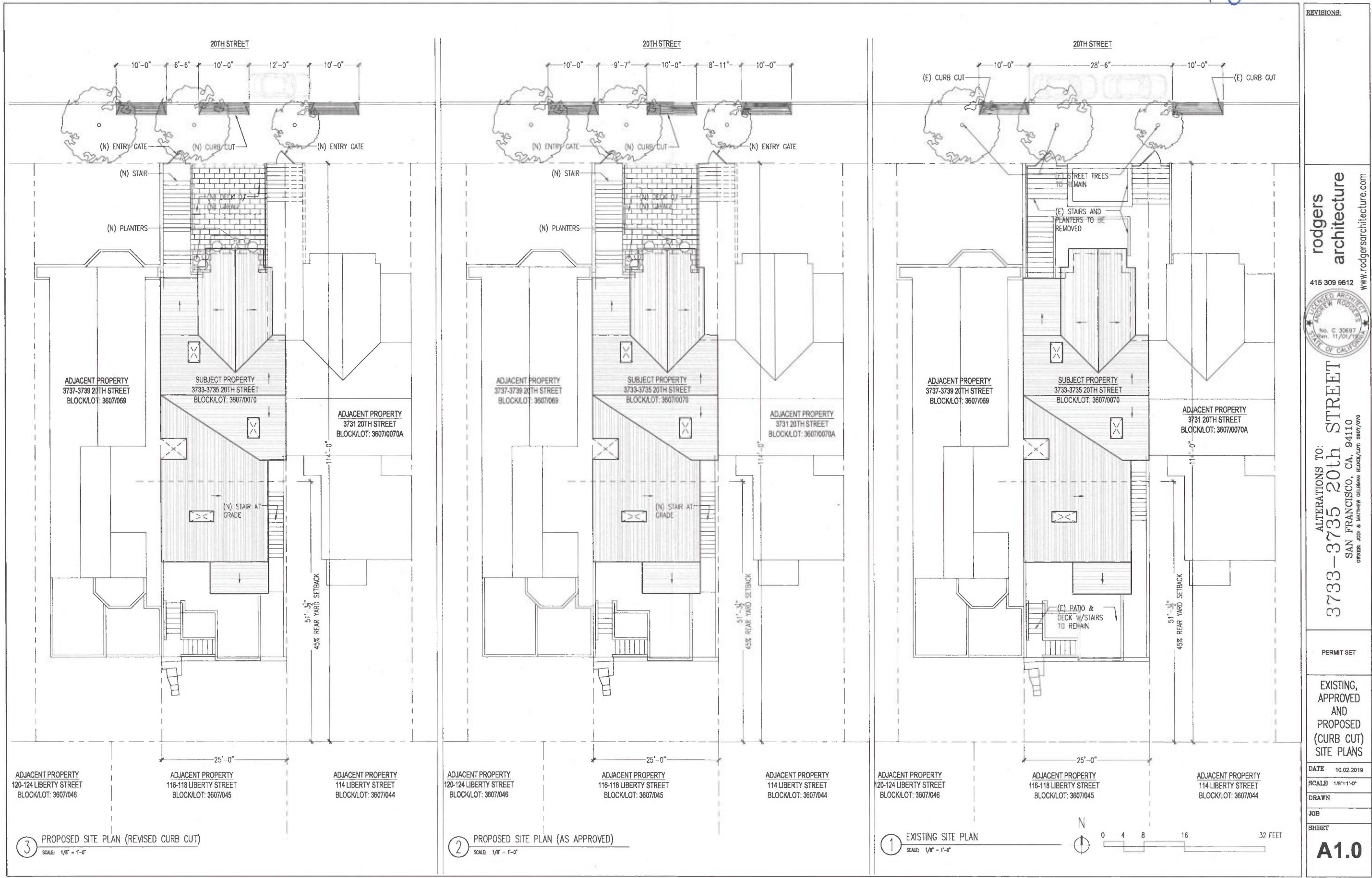
To whom it may concern:

The Pastor Ricky L. Hannah, Joe Bullock, Chairman of the Deacon Board and Alice Burn, Church Clerk, and members of St. Stephen's Baptist Church are in full support of the plans to renovate the building located next door to the church at 804-806-806A 22nd Street. We feel the project will improve the overall look and feel of the Dogpatch neighborhood and will not disturb the congregation in any way.

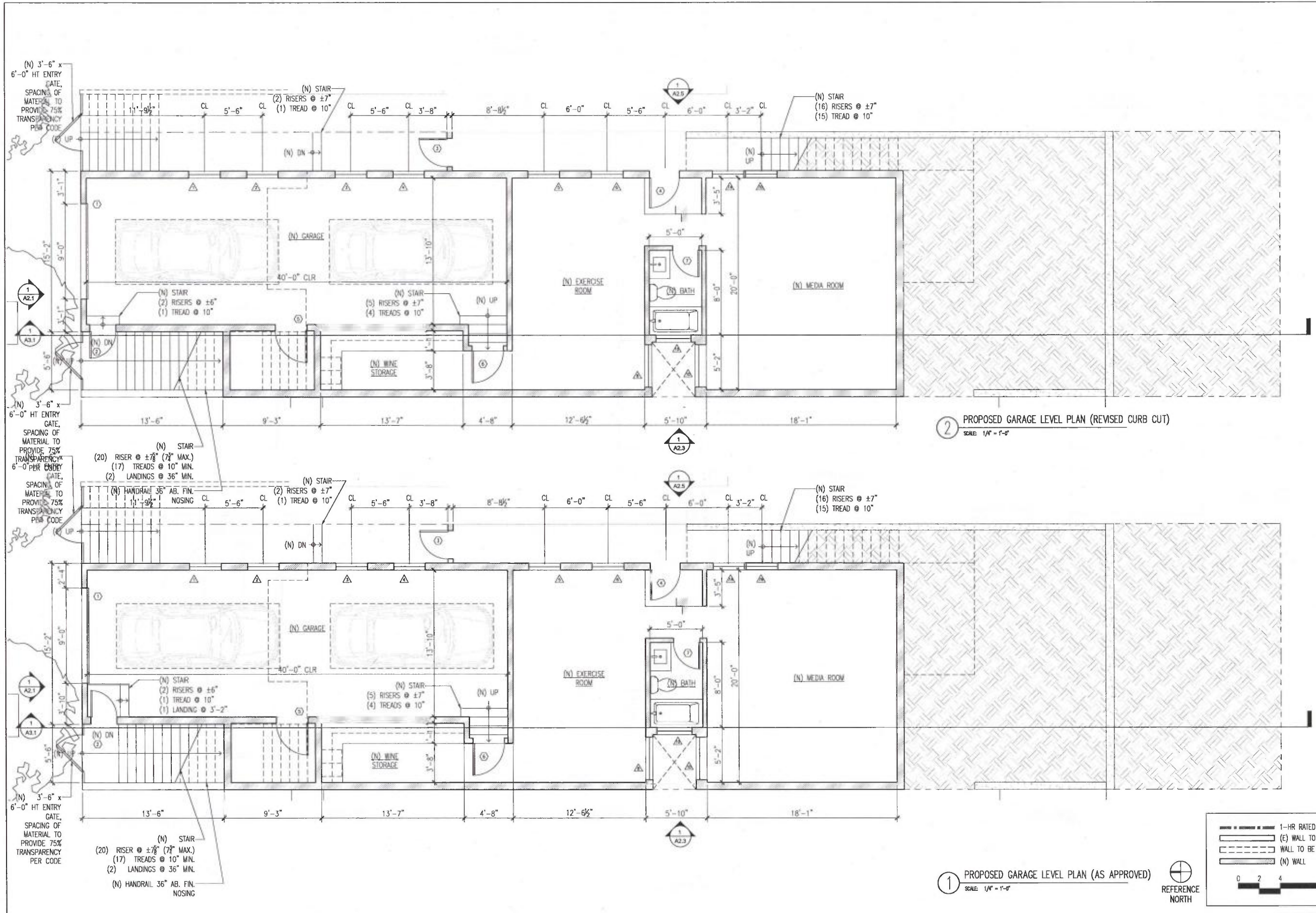
Sincerely,



Joe Bullock, Chairman of the Deacon Board



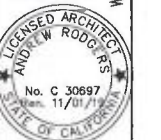
REVISIONS:	
rogers architecture 415 309 9612 www.rogersarchitecture.com	
ALTERATIONS TO: 3733-3735 20th STREET SAN FRANCISCO, CA, 94110 OWNER: JODI & MATTHEW BELMAN BLOCK/LOT: 3607/070	
PERMIT SET	
EXISTING, APPROVED AND PROPOSED (CURB CUT) SITE PLANS	
DATE	10.02.2019
SCALE	1/8"=1'-0"
DRAWN	
JOB	
SHEET	A1.0



REVISIONS:

rodgers
architecture

415 309 9612



ALTERATIONS TO:
3733-3735 20th STREET
SAN FRANCISCO, CA, 94110
OWNER: LOU & MATTHEW GELMAN BLOCK/JOT: 5807/070

PERMIT SET

PROPOSED
GARAGE LEVEL
PLAN
(AS APPROVED
+ REVISED
CURB CUT)

DATE 10.02.2019

SCALE 1/8"=1'-0"

DRAWN

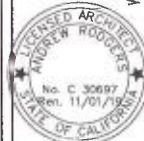
JOB

SHEET

A1.1

REVISIONS:

rodgers
architecture
415 309 9612
www.rodgersarchitecture.com



ALTERATIONS TO:
3733-3735 20th STREET
SAN FRANCISCO, CA 94110
OWNER: JOY & MATTHEW BELMAN BLOCK/LOT: 3607/070

PERMIT SET

EXISTING
NORTH
ELEVATION

DATE 10.02.2019

SCALE 1/4"=1'-0"

DRAWN

JOB

SHEET

A2.0





1 PROPOSED NORTH ELEVATION (AS APPROVED)
SCALE: 1/4" = 1'-0"

REVISIONS:

rodgers
architecture

415 309 9612
www.rodgersarchitecture.com

ALTERATIONS TO:
3733-3735 20th STREET
SAN FRANCISCO, CA, 94110
OWNER: JOE & MATTHEW GELMAN BLOCK/LOT: 3607/070

PERMIT SET

PROPOSED NORTH ELEVATION (AS APPROVED)

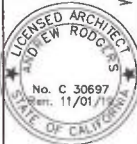
DATE: 10.02.2019
SCALE: 1/4"=1'-0"
DRAWN:
JOB:
SHEET

A2.1

REVISIONS:

rodgers
architecture

415 309 9612



ALTERATIONS TO:
3733-3735 20th STREET
SAN FRANCISCO, CA, 94110
OWNER: JODI & MATTHEW GELMAN BLOCK/LOT: 3607/070

PERMIT SET

PROPOSED
NORTH
ELEVATION
(REVISED
CURB CUT)

DATE 10.02.2019

SCALE 1/4"=1'-0"

DRAWN

JOB

SHEET

A2.1

ADJACENT PROPERTY
3737-3739 20TH STREET
BLOCK/LOT: 3607/069

SUBJECT PROPERTY
3733-3735 20TH STREET
BLOCK/LOT: 3607/070

ADJACENT PROPERTY
3731 20TH STREET
BLOCK/LOT: 3607/070A

ATTIC LEVEL
35'-7"

2nd LEVEL
23'-6"

1st LEVEL
13'-6"

LOWER LEVEL
3'-5 1/2"

GARAGE LEVEL
0'-0"

(E) ADJACENT
PROPERTY CURB CUT
DASHED LINE OF (E)
ADJACENT PROPERTY
STREET TREE

(N) PTD WD
ROLL-UP SECTIONAL
GARAGE DOOR
DASHED LINE OF (N)
3'-6" x 6'-0" HT
ENTRY GATE,
SPACING OF MATERIAL
TO PROVIDE 75%
TRANSPARENCY PER
CODE

(N) CURB CUT
(N) SMOOTH CEMENT
PLASTER W/
INTEGRAL COLOR
DASHED LINE OF (E)
ADJACENT PROPERTY
STREET TREE

(E) ADJACENT
PROPERTY CURB CUT
(N) ENTRY STAIR
DASHED LINE OF (N)
3'-6" x 6'-0" HT
ENTRY GATE,
SPACING OF MATERIAL
TO PROVIDE 75%
TRANSPARENCY PER
CODE

DASHED LINE OF (E)
ADJACENT PROPERTY
STREET TREE

1 PROPOSED NORTH ELEVATION (REVISED CURB CUT)

SCALE: 1/4" = 1'-0"

Received at HPC Hearing 10/2/19
M. Giacomucci

To: Historic Preservation Commission
From: Andrea Yvette Huginnie
3738 20th Street, San Francisco, CA 94110
Re: 3733-3735 20th Street, San Francisco, CA 94110
Lot 3607/070
Liberty-Hill Landmark District
Record No: 2018-008528COA
Date: September 30, 2019

Dear Sir or Madam:

I have owned and lived on 20th Street since 1996. I have seen this neighborhood change many times. I am writing to lend my support for the Certificate of Appropriateness and Variance for the project at 3733-3755 20th Street. The project maintains the historical and architectural integrity of the property while allowing for increased utilitarian usage.

If you have any questions, please free feel to contact me.

Sincerely,

Andrea Yvette Huginnie
3738 20th Street
San Francisco, CA 94110
Immosley333@gmail.com
415-900-8377 (cell)

Received at HPC Hearing 10/2/19
M. Giacomucci

Historic Preservation Commission (HPC), October 2, 2019
Regular Calendar Items 7a. and 7b.

Supplemental Review Materials from Neighboring Resident for my 3-minute presentation during public comment

Hi Commissioners,

I plan to attend tomorrow's hearing to present reasons why I believe the proposed new two-car garage should not be approved by the HPC nor a variance granted for it to be located in the front yard setback.

The focus of my presentation is on why existing City policies, Codes, and Mission Streetscape Plan should compel the Commissioners to disapprove the proposal. So, I have attached excerpts of these documents to allow adequate time for the Commissioners to peruse the materials. Also, I have provided an email I sent as a general inquiry to the City's Residential Design Advisory Team (RDAT) as to guidance on adding a garage and removing on-street parking though it was not project-specific and was just general guidance on the topic.

I have tried to work out an amicable solution that would minimize the disruption of the neighborhood by limiting their removal of on-street parking to only 1 of the total 12 parking spaces on this entire blockface. But the project sponsor is not willing to make any meaningful concession to their plan as they want their 2-car garage, gym, and media room and favored two street trees. Also, locating their garage in the middle of the lot is different from most of the other "new" garages on this blockface which typically have the garages on one side and stairs on the other.

Based on the City's Transit First policies (and in the 2-block corridor linking Dolores Street and Valencia Street), it would seem that the City should uphold its own policies and deny the garage. Thereby, no on-street parking for the neighborhood would be eliminated.

Additionally, I wanted to also point out that 10 years ago, the same property owner was granted a rear yard variance to construct structures to within approx.. 16 feet of the rear lot line. In that decision letter it noted that the variance was being granted to the large front yard setback. So now, they would like to build out the front yard as well.

Please don't support this overbuilding campaign as it would have a materially injurious impact on the neighborhood by the removal of two on-street parking spaces and the exacerbation of a pedestrian safety issue with the proliferation of garages on 20th Street.

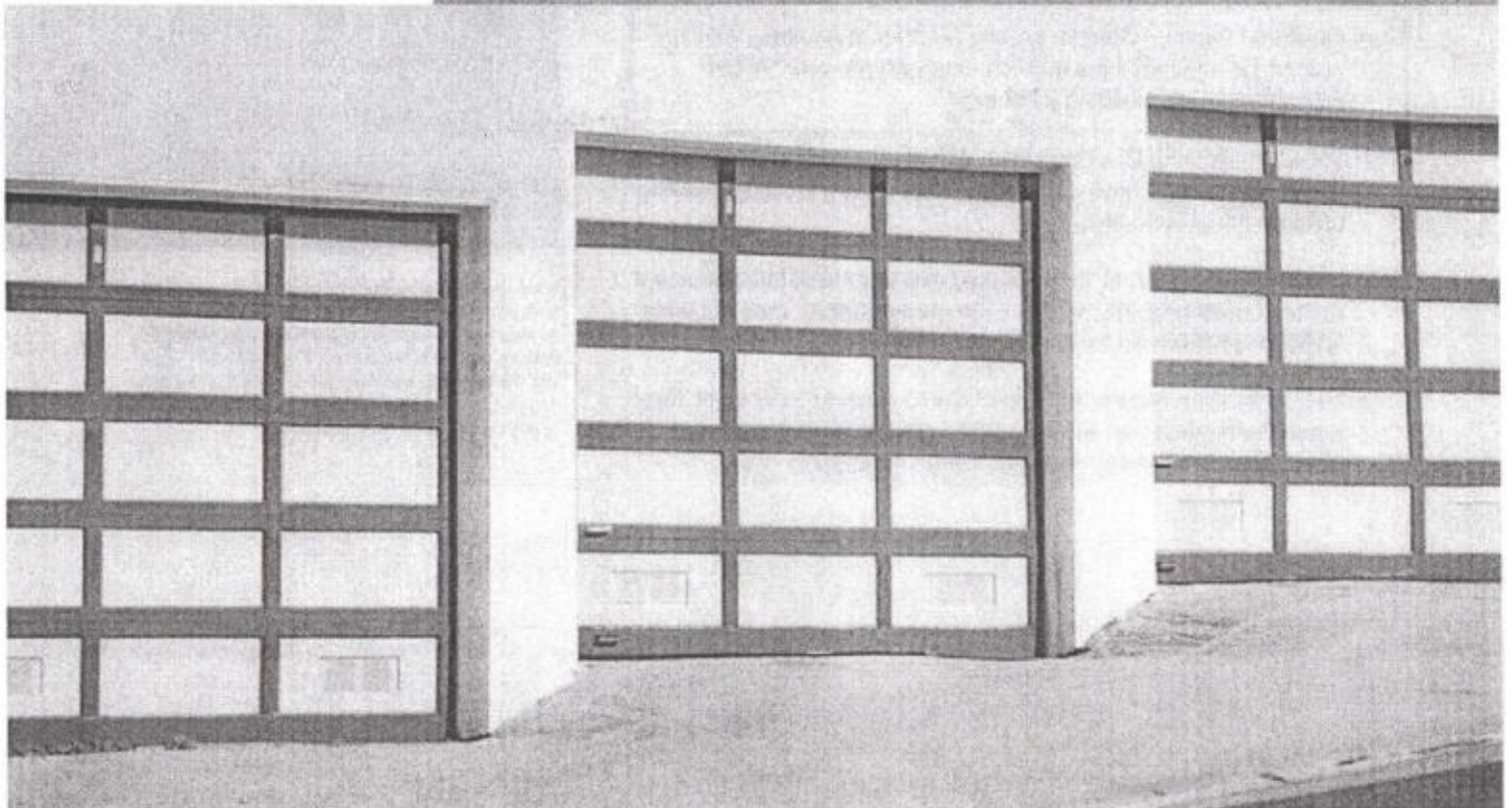
Thank you,
Heidi



Guidelines for Adding Garages and Curb Cuts

CRITERIA FOR ADDING GARAGES
AND CURB CUTS TO EXISTING STRUCTURES
(INCLUDING HISTORIC RESOURCES)

Formerly known as: Zoning Administrator Bulletin Nos. 2006.1a and 2006.1b



General Standards and Criteria for Existing Buildings

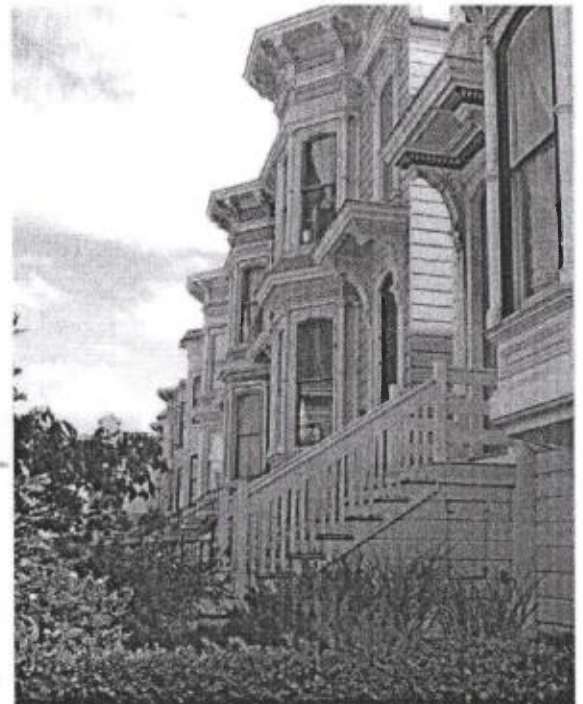
GARAGE DOOR APPEARANCE

Garage door design and materials should be compatible with the existing building and surrounding neighborhood character.

PLACEMENT OF THE GARAGE AND CURB CUT

The location of the curb cut, garage, and garage door should ensure maximum compatibility with existing on-street parking, existing dwelling units, and the structure's context. Greater numbers of entryways and units along a building activate more of the street frontage by increasing the points where residents come and go as well as the number of opportunities for personalization.

- On-Street Parking. Could a greater number of on-street parking spaces be retained if the curb cut and garage were shifted elsewhere on the building's frontage?
- Impact to Existing Dwelling Units. Would the proposed placement of the new garage have a negative effect on any existing dwelling units on the ground level?
- Loss of Existing Street Trees.² Could existing street trees adjacent to the subject property remain if the garage and/or curb cut were shifted elsewhere on the building's frontage?
- Loss of existing Significant Trees.³ Could existing Significant Trees within the subject parcel remain if the garage and/or curb cut were shifted elsewhere on the building's frontage?⁴



Front yard setbacks that not only enliven the public realm but also represent the historic pattern of development should be maintained and protected. When a garage is necessary, it should be inserted into the building, avoiding impacts on the character-defining features of the building and the displacement of any ground floor residential units.

San Francisco Planning Department

Residential Design

Guidelines



December 2003

City and County of San Francisco

\$3⁰⁰

For information about obtaining a minor sidewalk encroachment permit for a new curb cut, contact the Department of Public works at 554-5810 or www.sfdpw.org.

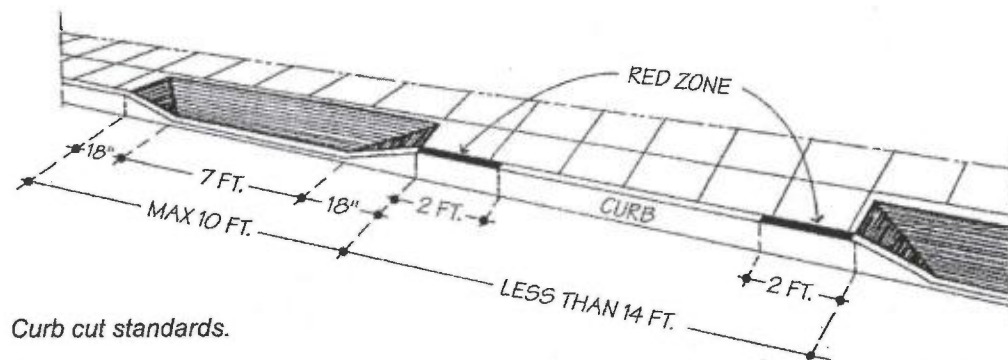
For information about requesting a "red zone" for a curb cut, contact the Department of Parking and Traffic Engineering Division at 554-2300 or www.sfgov.org.

Curb Cuts

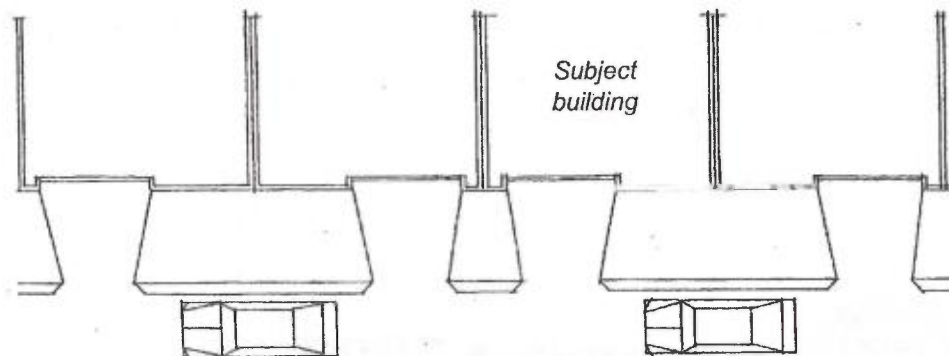
GUIDELINE: Coordinate the placement of curb cuts.

Pursuant to Planning Code Section 155(l), curb cuts must be designed to minimize the width and frequency of curb cuts, and to maximize the number and size of on-street parking spaces available to the public. The Zoning Administrator's "Curb Cut Policy" adopts a standard curb cut width of 10 feet for all new residential development and alterations to existing buildings. Where the curbside length is less than 14 feet, a two-foot "red zone" included on either side of a curb cut may result in a loss of on-street parking because the remaining space is too small to be used for parking.

Pursuant to these regulations, new curb cuts must be placed in locations that will minimize the loss of on-street parking. When developing multi-family projects and contiguous lots, cluster parking areas and coordinate the placement of curb cuts with others on the block. Additionally, locate curb cuts to maximize the potential for tree planting.



Curb cut standards.



By pairing the new curb cut with the existing curb cut to the left, a full-size on-street parking space is preserved.

San Francisco Planning

Report for: **3733 20TH ST**

Zoning Information

Planning Department Zoning and other regulations.

Zoning Districts

RH-2 - Residential- House, Two Family [↗](#)

Height & Bulk Districts

40-X [↗](#)

Special Use Districts

Within 1/4 Mile of the Fringe Financial Services RUD

[↗](#)

Special Sign Districts

None

Legislative Setbacks

None

Limited and Nonconforming Uses

None

Planning Areas

None

Public Realm and Streetscape Plans

Mission District Streetscape Plan [↗](#)

Notice of Special Restrictions

None

Business Zoning Check ^{BETA}

Find out what businesses are permitted at this property [↗](#)

Coastal Zone

Not in the Coastal Zone

Port

Not under Port Jurisdiction

Redevelopment Areas

None

Neighborhood-Specific Impact Fees

In addition to those impact fees that apply throughout the City, the following neighborhood-specific impact fees apply to this particular property:

None

An overview of Development Impact Fees can be found on the Impact Fees Website [↗](#)

Mayor's Invest in Neighborhoods Initiative Areas

None

Community Benefit Districts

None

Other Notices and Regulations

Accessory Dwelling Units

May be eligible for adding new accessory dwelling unit(s).

Read more about this regulation [↗](#)

Added:

Staff Contact: Kimia Haddadan

Fringe Financial Service RUD 1/4-mile buffer

No new fringe financial services shall be permitted as a principal or accessory use in the Fringe Financial Service RUD. The controls of this Section 249.35 shall also apply within a 1/4-mile of the Fringe Financial Service RUD

Read more about this regulation [↗](#)

Added: 8/20/2012

Staff Contact: Dan Sider

Project Review

3735 - 20th Street. Relocate second-floor unit to basement.

Added: 8/17/2004

Planner: JULIAN BANALES

Applicant/Requestor:

Design Guidelines

Area Specific Design Guidelines

Urban Design Guidelines

The Urban Design Guidelines are an implementation document for Urban Design Policy in the General Plan. Sites in National Register, California Register, Article 10 and Article 11 Historic Districts are exempt. They apply in Residential districts only for projects with non-residential uses or residential projects with twenty-five units or more or with a frontage longer than 150'.

Residential Design Guidelines

The Residential Design Guidelines articulate expectations regarding the character of the built environment and are intended to promote design that will protect neighborhood character, enhancing the attractiveness and quality of life in the city. These guidelines are integral to the Department's design review process for residential districts.

Citywide Design Guidelines

Architectural Design Guide for Exterior Treatments of Unreinforced Masonry Buildings during Seismic Retrofit

This design guide should provide guidance on how to maintain the historical character of a building when conducting seismic retrofit.

General information only. Use of this information for specific applications should be determined in each instance by the user and only upon the professional advice of competent experts.

Better Streets Plan

The Better Streets Plan contains guidelines that focus on pedestrian comfort, safety, and the usability of streets as public spaces. They contain pedestrian-oriented guidelines for curb lines, crosswalks, and other street design features to enable generous, usable public spaces.

Commission Guide for Formula Retail

The purpose of this document is to evaluate the appropriateness of each individual formula retail establishment's use, design, and necessity to help preserve the character of the City's neighborhoods. Aligns with Planning Code Sections 303.1, 703.3, 803.6(c), Article 6, Article 11

Design Guide Standards for Bird-Safe Buildings

These guidelines should be applied to new construction and alterations that require treatment options to meet the Bird-Safe Building Standards.

Guide to the San Francisco Green Landscaping Ordinance

The guide describes the Green Landscaping Ordinance and helps san Francisco residents and property owners understand the benefits, requirements, and ways to comply with the ordinance.

Planning Code; Public Works Code

Guidelines for Adding Garages and Curb Cuts

These guidelines explain the criteria in which new garages and curb cuts are reviewed when installing to an existing or an historic building.

Mission District Streetscape Plan (excerpt)

Project Site



	Living Alley		Gateway
	Green Connector Street		Flexible Parking Zone
	Road Diet Street		New Public Space
	Traffic Calmed Street		MDSP Boundary
	Pedestrian Improvements		
	Flexible Mixed Use Streets		

MISSION STREETSCAPE PLAN
San Francisco Planning Department

San Francisco Planning Code

SEC. 155. GENERAL STANDARDS AS TO LOCATION AND ARRANGEMENT OF OFF-STREET PARKING, FREIGHT LOADING, AND SERVICE VEHICLE FACILITIES.

Required off-street parking and freight loading facilities shall meet the following standards as to location and arrangement. Facilities which are not required but are actually provided shall also meet the following standards unless such standards are stated to be applicable solely to required facilities. In application of the standards of this Code for off-street parking and loading, reference may be made to provisions of other portions of the Municipal Code concerning off-street parking and loading facilities, and to standards of the Better Streets Plan and the Bureau of Engineering of the Department of Public Works. Final authority for the application of such standards under this Code, and for adoption of regulations and interpretations in furtherance of the stated provisions of this Code shall, however, rest with the Planning Department.

(a) **Required Parking and Loading on the Same Lot as the Use Served.** Every required off-street parking or loading space shall be located on the same lot as the use served by it, except as provided in Section 161 of this Code.

(b) **Off-Street Parking and Loading on Private Property.** Every off-street parking or loading space shall be located in its entirety within the lot lines of private property. Shared driveways are encouraged.

(c) **Adequate Means of Ingress and Egress.** Every off-street parking or loading space shall have adequate means of ingress from and egress to a Street or Alley. Access to off-street loading spaces shall be from Alleys in preference to Streets, except where otherwise specified in this Code.

Adequate reservoir space shall be provided on private property for entrance of vehicles to off-street parking and loading spaces, except with respect to spaces independently accessible directly from the Street.

For Residential Uses, independently accessible off-street parking spaces shall include spaces accessed by automated garages, or car elevators, lifts or other space-efficient parking as defined in Section 154(a)(4) and Section 154(a)(5) provided that no more than one car needs to be moved under its own power to access any one space.

(d) **Enclosure of Off-Street Loading and Service Vehicle Spaces Required.** All off-street freight loading and service vehicle spaces in the C-3, DTR, MUO, CMUO, WMUO, MUG, WMUG, and MUR shall be completely enclosed, and access from a public Street or Alley shall be provided by means of a private service driveway that is totally contained within the structure. Such a private service driveway shall include adequate space to maneuver trucks and service vehicles into and out of all provided spaces, and shall be designed so as to facilitate access to the subject property while minimizing interference with street and sidewalk circulation. Any such private service driveway shall be of adequate width to accommodate drive-in movement from the adjacent curb or inside traffic lane but shall in no case exceed 30 feet. Notwithstanding the foregoing, if an adjacent Street or Alley is determined by the Zoning Administrator to be primarily used for building service, up to four off-street freight or loading spaces may be allowed to be individually accessible directly from such a Street or Alley, pursuant to the provisions of Section 309 in a C-3 District, the provisions of Section 309.1 in a DTR District, the provisions of Section 329 for projects subject to Section 329 in a MUO, CMUO, WMUO, MUG, WMUG, or MUR District, or by administrative decision of the Zoning Administrator for projects that are not subject to Section 329 in a MUO, CMUO, WMUO, MUG, WMUG, or MUR District.

(e) **Alternate Location of Service Vehicle Spaces.** Where site constraints would make a consolidated freight loading and service vehicle facility impractical, service vehicle spaces required by Sections 153(a)

(6) and 154(b)(3) of this Code may be located in a parking garage for the structure or other location separate from freight loading spaces.

(f) **Freight Elevator Access to Off-Street Freight Loading.** Whenever off-street freight loading spaces are provided, freight elevators immediately accessible from the loading dock shall be provided to all floors which contain uses that are included in the calculation of required number of freight loading spaces. If freight loading facilities are subterranean, the location and operation of freight elevators shall be designed, where feasible, to discourage use of freight elevators for deliveries from the ground floor. Directories of building tenants shall be provided at all freight elevators. A raised loading dock or receiving area shall be provided with sufficient dimensions to provide for short-term storage of goods. All required freight loading and service vehicle spaces shall be made available only to those vehicles at all times, and provision shall be made to minimize interference between freight loading and service operations, and garbage dumpster operations and storage.

(g) **Parking Pricing Requirements.** In order to discourage long-term commuter parking, any off-street parking spaces provided for a structure or use other than Residential or Hotel in a C-3, DTR, SPD, MUG, WMUG, MUR, CMUO, WMUO, or MUO District, whether classified as an accessory or Conditional Use, that are otherwise available for use for long-term parking by downtown workers shall maintain a rate or fee structure for their use such that the rate charge for four hours of parking duration is no more than four times the rate charge for the first hour, and the rate charge for eight or more hours of parking duration is no less than 10 times the rate charge for the first hour. Additionally, no discounted parking rate shall be permitted for weekly, monthly or similar time-specific periods.

(h) **Layout and Markings.** The internal layout of off-street parking and loading spaces, driveways, aisles and maneuvering areas shall be according to acceptable standards, and all spaces shall be clearly marked.

(i) **Parking Spaces for Persons with Disabilities.** For each 25 off-street parking spaces provided, one such space shall be designed and designated for persons with disabilities.

(j) **Bicycle Parking.** Bicycle parking shall be provided, as required by Section 155.2.

(k) **Encroachments.** Off-street parking and loading facilities shall be arranged, designed and operated so as to prevent encroachments upon sidewalk areas, bicycle lanes, transit-only lanes and adjacent properties, in the maneuvering, standing, queuing and storage of vehicles, by means of the layout and operation of facilities and by use of bumper or wheel guards or such other devices as are necessary.

(l) **Driveways.** Driveways crossing sidewalks shall be no wider than necessary for ingress and egress, and shall be arranged, to the extent practical, so as to minimize the width and frequency of curb cuts, to maximize the number and size of on-street parking spaces available to the public, and to minimize conflicts with pedestrian and transit movements.

(m) **Surfacing and Grading.** Every off-street parking or loading facility shall be suitably graded, surfaced, drained and maintained.

(n) **Parking or Loading in Required Open Spaces.** Off-street parking and loading spaces shall not occupy any required open space, except as specified in Section 136 of this Code.

(o) **Accounting of Parking and Loading Spaces.** No area credited as all or part of a required off-street parking space shall also be credited as all or part of a required off-street loading space, or used as all or part of an unrequired off-street loading space. No area credited as all or part of a required off-street loading space shall also be credited as all or part of a required off-street parking space, or used as all or part of an unrequired off-street parking space.

(p) **Freight Loading Adjacent to R Districts.** Any off-street freight loading area located within 50 feet of any R District shall be completely enclosed within a building if such freight loading area is used in regular night operation.

----- Forwarded Message -----

From: "Small, Maia (CPC)" <maia.small@sfgov.org>

To: "Greenan, Trent (CPC)" <trent.greenan@sfgov.org>

Cc: "Heidi" <h*****o@yahoo.com>, "Barata, Luiz (CPC)" <luiz.barata@sfgov.org>, "Albericci, Allison (CPC)" <allison.albericci@sfgov.org>, "Winslow, David (CPC)" <david.winslow@sfgov.org>

Sent: Tue, Oct 1, 2019 at 8:43 AM

Subject: Re: Question for anyone on the Residential Design Advisory Team (RDAT)

One other thing to note is that with the passage of the elimination of minimum parking and our transit first policy the Department generally does not support adding parking in and a curb cut next to housing that currently does not have either. It essentially replaces a public space with a private one and decreases pedestrian safety.

As Trent notes, however, they are reviewed individually and there are some circumstances in which it may be allowable.

Maia Small, Principal Urban Designer + Architect
Citywide and Current Planning Divisions

San Francisco Planning Department
1650 Mission Street, Suite 400 San Francisco, CA 94103
Direct: 415.575.9160 | www.sfplanning.org
[San Francisco Property Information Map](#)

On Oct 1, 2019, at 8:37 AM, Greenan, Trent (CPC) <trent.greenan@sfgov.org> wrote:

Hi Heidi,

Yes, this is something that we review in RDAT and comes up frequently.

Thanks,
Trent

-----Original Message-----

From: Heidi <h*****o@yahoo.com>

Sent: Monday, September 30, 2019 11:54 PM

To: Barata, Luiz (CPC) <luiz.barata@sfgov.org>; Albericci, Allison (CPC) <allison.albericci@sfgov.org>; Small, Maia (CPC) <maia.small@sfgov.org>; Winslow, David (CPC) <david.winslow@sfgov.org>; Greenan, Trent (CPC) <trent.greenan@sfgov.org>

Subject: Question for anyone on the Residential Design Advisory Team (RDAT)

This message is from outside the City email system. Do not open links or attachments from untrusted sources.

Hi,

I have general question and hoping one of you can email me.

Does your RDAT group review the location of a new curbcut, and even the location of the garage, in order to maximize the retention of existing on-street parking when someone proposes to add a new garage to their house?

Page 3 of the City's Guidelines for Adding Garages and Curbcuts and Page 39 of the Residential Design Guidelines seem to require it and I just wanted to find out if it actually gets reviewed and matters.

Thanks!

Heidi