

# Bringing Factory Built Housing Into Compliance with San Francisco's Building Code

San Francisco Building & Construction Trades Council

The City and County of San Francisco must ensure that any Factory Built Housing complies with the same minimum building safety standards required for traditional site-built construction for all the reasons that San Francisco has amended the state's code in the first place.

San Francisco is unique among California communities with respect to local climatic, geological, topographical, and other conditions. San Francisco faces extreme risk of earthquakes in a highly concentrated urban area, creating unique risks of earthquake damage, fires and loss of life. For these and other reasons, San Francisco has enacted local amendments to the California Building Standards Code. Buildings must conform to the code to obtain development approval from the Planning Commission and building permits from the Department of Building Inspection.

Notwithstanding assertions by proponents of exclusive state regulation, who cite the *Factory Built Housing Law of 1969*, (HSC §19960), multi-family factory-built housing (FBH) is subject to local building standards regulation pursuant to the *State Building Standards Law of 1979*, (HSC §18901, et seq.). Proponents' assertions are based on an inaccurate and incomplete analysis of the relevant statutory framework governing building construction in California.

## **THE STATE BUILDING STANDARDS LAW OF 1979 GOVERNS ALL RESIDENTIAL BUILDING STANDARDS**

The *State Building Standards Law of 1979*, (SB 331 Robbins) vested in the Building Standards Commission with the responsibility to approve and publish the building standards related to all occupancies in the State of California, including FBH. Furthermore, the *State Building Standards Law* vests in local governments the authority to promulgate building standards to protect public health, safety and general welfare as they relate to the construction and occupancy of buildings and structures.

Prior to the enactment of the *State Building Standards Law* California had empowered multiple agencies to promulgate building standards; at the time the Housing and Community Development Department (HCD) governed standards for factory-built housing. (HSC §19969, §19990). Per the Legislature's *Summary Digest of Statutes Enacted and Resolution (Including Proposed Constitutional Amendments Adopted in 1979 and 1979 Statutory Record*<sup>1</sup> With the enactment of the *State Building Standards Law*,

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This bill would vest in the commission the responsibility for approving and publishing, as specified in the bill, all building standards, as defined in the bill, adopted by state agencies, as defined, into the State Building Standards Code, which code would supersede all building standards, except those relating to mobilehomes, which are adopted by state agencies on or before January 1, 1985, as provided in the bill. Excepting regulations relating to mobilehomes, the bill would designate the commission as the sole state agency with authority to approve and publish building standards, as defined, after July 1, 1980.

The State Building Standards Law expressly gives the Building Standards Commission jurisdiction over all buildings, except, *"except any mobilehome as defined in Section 18008, manufactured home, as defined in Section 18007, special purpose commercial coach, as defined in Section 18012.5, and recreational vehicle, as defined in Section 18010."* (HSC §18908(a). The legislature defines Manufactured Homes in HSC §18007 as "built on a permanent chassis and designed to be used as a single-family dwelling with or without a foundation when connected to the required utilities."

Factory Built Housing is conspicuously absent from this list, evidencing the legislature's intent to place FBH under the jurisdiction of the Building Standards Commission, under the doctrine of *expressio unius est exclusion alterius*. (See, e.g., *Mountain Lion v. Fish and Game Comm'n* (1997) 16 Cal.4th 105, 116 ("where exceptions to a general rule are specified by statute, other exceptions are not to be presumed unless a contrary legislative intent can be discerned").) The summary is unambiguous about legislative intent.

The bill would conform other provisions of the codes to the requirements of approval by the commission of all building standards except those relating to mobilehomes and occupational safety and health standards, which are adopted by state agencies and publication of such building standards only in the State Building Standards Code.

That authority includes *adopting and approving standards amended, added to, or deleted by a city, county, or city and county to establish more restrictive building standards reasonably necessary because of local climactic, geological, or topographical conditions*. (HSC 17958. 5 and 18941. 5). San Francisco has enacted amendments to the California Building Code and those have been accepted by the California Building Standards Commission.

The building standards for the *Factory Built Housing Law* were now defined the same as the State Building Standards Law.

HSC §18909. (a) "Building standard" means any statute, rule, regulation, order, or other requirement promulgated by a state agency, including any amendment or repeal of such requirement, which affects, regulates, requires, forbids, or pertains to the method of use, properties, performance, or types of materials used in construction, alteration, improvement, repair, or rehabilitation of a building, structure, factory-built housing, or other improvement to real property, including fixtures therein, and as determined by the commission."



Furthermore, HSC §18919 provides additional clarity of what comprises a "building standard" by defining "Regulation" as *"any rule, regulation, ordinance, or order promulgated by a state or local agency, including rules, regulations, or orders relating to occupancy or the use of land. "Regulation" includes building standards."*

In short, *State Building Standards Law of 1979*, (SB 331 Robbins) made wholesale revisions to the Health and Safety Code that allowed for the uniform application of local building code amendments to FBH.<sup>2</sup>

#### **THE FACTORY BUILT HOUSING LAW OF 1969 HAS LIMITED PREEMPTION AUTHORITY**

Ignoring the effect of the *State Building Standards Law of 1979*, (SB 331 Robbins) proponents of state code supersession of local building code amendments cite the *Factory Built Housing Law of 1969*. The State of California enacted the *Factory Built Housing Law* "in an effort to meet the housing needs within the State of California" and in coordination with the Nixon Administration's Operation Breakthrough. Then Assemblyman Pete Wilson was among its major proponents.

The *Factory-Built Housing Law* was a broad piece of legislation that hoped to produce an efficient system of home production by encouraging mass production techniques, restricting cities and counties from discriminating against the use of FBH, and centralized regulatory oversight within a state agency. The Legislature scaled back the reach of the FBH when it reworked the codes and regulations in the *State Building Standards Law of 1979*.

The *Factory Built Housing Law* separated FBH from the regulatory regime established by the *State Housing Law of 1961*.<sup>3</sup> In HSC §19961, the Legislature declared that:

*"the private housing and construction industry has developed mass production techniques which can substantially reduce housing construction costs, and that the mass production of housing...presents unique problems with respect to the establishment of uniform health and safety standards and inspection procedures. The Legislature further finds and declares that by minimizing the problem of standards and inspection procedures, it is demonstrating its intention to encourage the reduction of housing construction costs and to make housing and home ownership more feasible for all residents of the state."*

The *Factory Built Housing Law* contained three key provisions related to its regulatory regime. First was an amendment to HSC §17911, which exempted Factory-Built Housing from the State Housing Law and was last amended in 1971. Second was the inclusion of HSC §19990-19991 placing the authority to establish building standards and enforcement those standards under the umbrella of the Department of Housing and Community Development (HCD). Lastly, local enforcement agencies were limited to inspection of the on-site installation FBH units.

Additionally, legislative history leading up to the passage of the law indicates the Legislature in 1969 did not anticipate that Factory-Built Housing would be "stacked" and used for multi-story

developments containing four or more dwelling units as is increasingly being proposed in San Francisco. The legislative intent was to preempt local building codes only for small single-story residential developments. Testimony before the Assembly Urban Affairs and Housing Committee on April 12, 1969 indicated that the law was intended to apply to "single-story" housing.

In passing the Building Standards Law the Legislature circumscribed the *Factory Built Housing Law's* pre-emption of local building standards. The Buildings Standards law was amended in 1979 to state, "Building standards in individual titles of the California Code of Regulations other than the California Building Standards Code shall have no force or effect after January 1, 1985." (HSC §18943). In revisiting the issue in 1991, the Legislature transferred HCD's responsibilities to adopt regulations relating to buildings standards to the State Building Standards Commission. HSC §18949.5,<sup>4</sup> now states, "Any responsibilities of the Department of Housing and Community Development to adopt regulations relating to building standards are hereby transferred to the [State Building Standards Commission]."<sup>5</sup> The stated purpose for this amendment was to consolidate building codes and avoid inconsistency and duplication. (Legislative History of AB47 (1991)).<sup>6</sup>

## **CONCLUSION**

The *State Building Standards Law* has provided the Building Standards Commission with the authority to approve and publish the building standards related to all occupancies in the State of California.

The primary purpose of the San Francisco Building Code is to protect public health, safety and general welfare as they relate to the construction and occupancy of buildings and structures. The City and County of San Francisco is unique among California communities with respect to local climatic, geological, topographical, and other conditions. San Francisco faces extreme risk of earthquakes in a highly concentrated urban area, creating unique risks of earthquake damage, fires and loss of life. For these and other reasons, San Francisco has enacted local amendments to the California Building Standards Code, which have been duly approved by the California Building Standards Commission. Buildings must conform to the code to obtain development approval from the Planning Commission and building permits from the Department of Building Inspection.

The *Factory Built Housing Law* has limited pre-emption under the HSC. While its standards are regulated by the State Building Standards law, the *Factory Built Housing Law* puts it on equal footing with traditional site-built construction. While local governments are forbidden from banning FBH or from establishing special discriminatory rules that interfere with its installation, they are empowered to apply the same building standards to FBH as they do other forms of construction.

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<sup>1</sup> Available at <https://books.google.com/books?id=BRxv5ZylJdQC&pg=PA377#v=onepage&q&f=false>

<sup>2</sup> HCD retained the authority to adopt or propose building standards for FBH, provided those standards were submitted to, and approved or adopted by, the California Building Standards Commission prior to codification.

<sup>3</sup> In 1961 the State of California revamped the California Housing Law, enacting rules related to the construction of housing in the state. Housing Law was enforced by the Chief of the Division of Housing in the Department of Industrial Relations. DIR had been empowered to:

*"adopt, amend, repeal, and, except as hereinafter provided, enforce rules and regulations for the protection of public health, safety, and general welfare of the occupant and the public governing the erection, construction, enlargement, conversion, alteration, repair, moving, removal, demolition, occupancy, use, height, court, area, sanitation, ventilation and maintenance of all hotels, apartment houses, and dwellings."*

Prior to 1961 the construction of housing was tied to the Labor Code; the Department of Industrial Relations had jurisdiction for apartment houses and hotels outside of city limits and inside city limits if the city failed to act on violations of the code. The California Housing Law was codified as **Part 1.5 of Division 13 of the Health & Safety Code**. **HSC §17950** established that *the "provisions of this part and the rules and regulations promulgated pursuant thereto which relate to apartment houses, hotels, and dwellings apply in all parts of the State."* **HSC §17951** empowered the *"governing body of any city or county [to] enact ordinances or regulations imposing restrictions equal to or greater than those imposed by this part[.]"*

<sup>4</sup> HCD may still propose building standards for FBH for consideration by the CBSC, but CBSC has ultimate authority on whether to accept or reject those standards. (HSC 19990). Furthermore, when developing standards, HCD must consider any amendments to the model codes referred to in this section." (HSC §19990(c)).

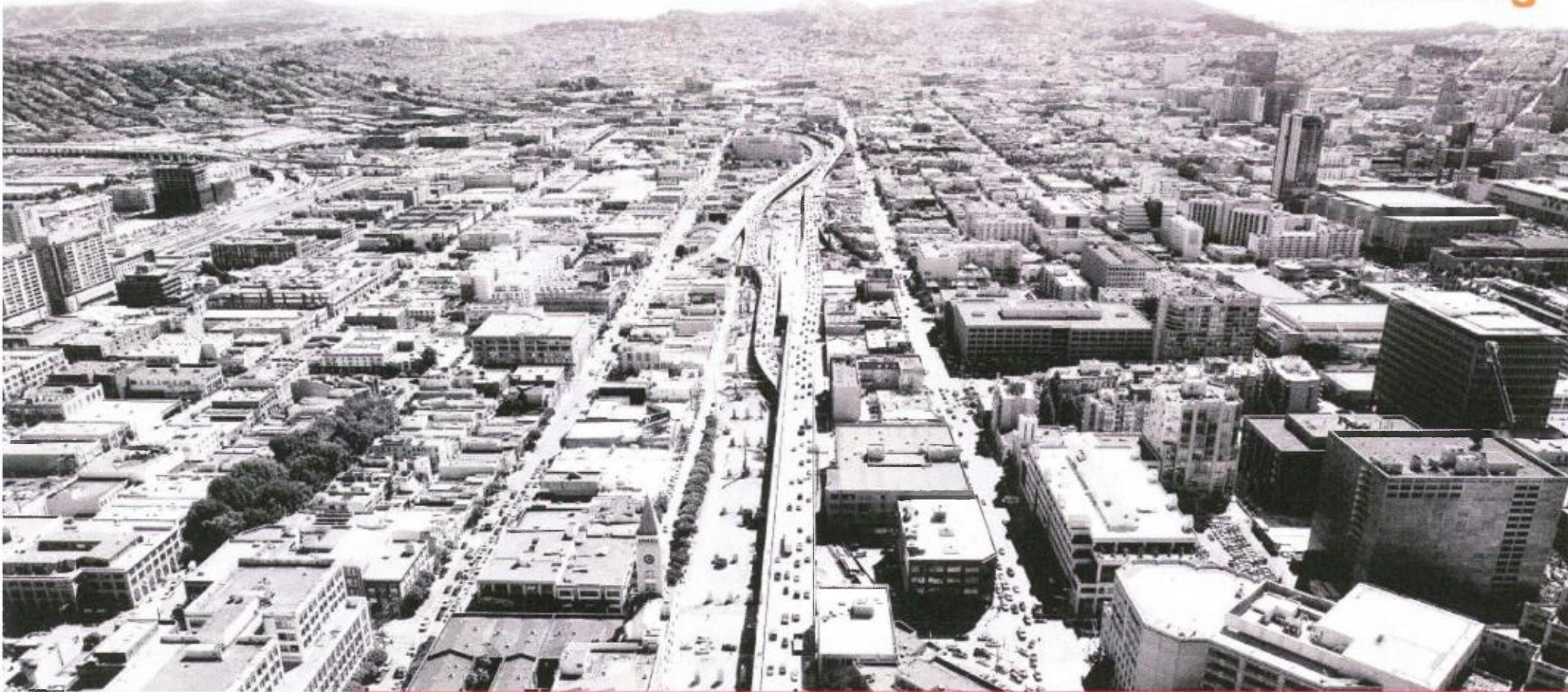
<sup>5</sup> Subsequent amendments to the State Building Standards Law have only strengthened and clarified the authority of a city or county to establish more restrictive building standards reasonably necessary

<sup>6</sup> Legal interpretations have generally held that local authorities may impose requirements on Factory-Built Housing, such as minimum size requirements, so long as they are the same as those imposed in other housing. 55 Ops. Cal. Atty. Gen. 133 (1972).



<http://centralsoma.sfplanning.org>

Received at CPC Hearing 3/22/18  
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# CENTRAL SOMA

## HOUSING

Planning Commission Informational | March 22, 2018

# **TODAY: CENTRAL SOMA & HOUSING**

- 1) Central SoMa Delivers Housing
- 2) Provides Jobs for the City that Cannot Be Accommodated Elsewhere
- 3) Adding More Housing in Central SoMa Can Be Achieved, With Certain Costs
- 4) Housing Capacity Is Increasing Across the City
- 5) More Plans to Increase Housing Capacity are in Development
- 6) Even More Ideas for Increasing Housing Capacity



# HOW MUCH HOUSING IS PROPOSED FOR CENTRAL SOMA?

**7,100** HOUSING UNITS

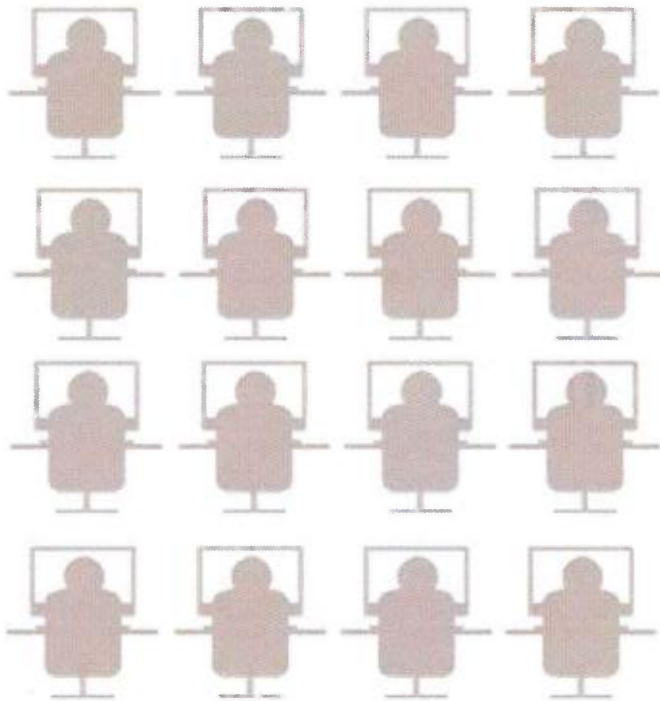
CENTRAL SOMA DELIVERS HOUSING



# HOW MUCH HOUSING IS PROPOSED FOR CENTRAL SOMA?

**8M<sub>sf</sub>**

**OFFICE**



**=**

**8M<sub>sf</sub>**

**HOUSING**



**CENTRAL SOMA DELIVERS HOUSING**

# CENTRAL SOMA DELIVERS HOUSING

2

=

1

**HIGH DENSITY HOUSING PLANS  
RINCON HILL AND TRANSBAY**

**CENTRAL SOMA**





# CENTRAL SOMA DELIVERS HOUSING

CANDLESTICK  
POINT



CENTRAL  
SOMA



TREASURE  
ISLAND





**CENTRAL SOMA WILL BE...**

**50%**

**MORE DENSE  
THAN PARIS**



**2X**

**MORE DENSE  
THAN BARCELONA**



# CENTRAL SOMA DELIVERS HOUSING

**CENTRAL  
SOMA**

**MORE HOUSING**



**MARKET  
OCTAVIA**

**60% LARGER AREA**





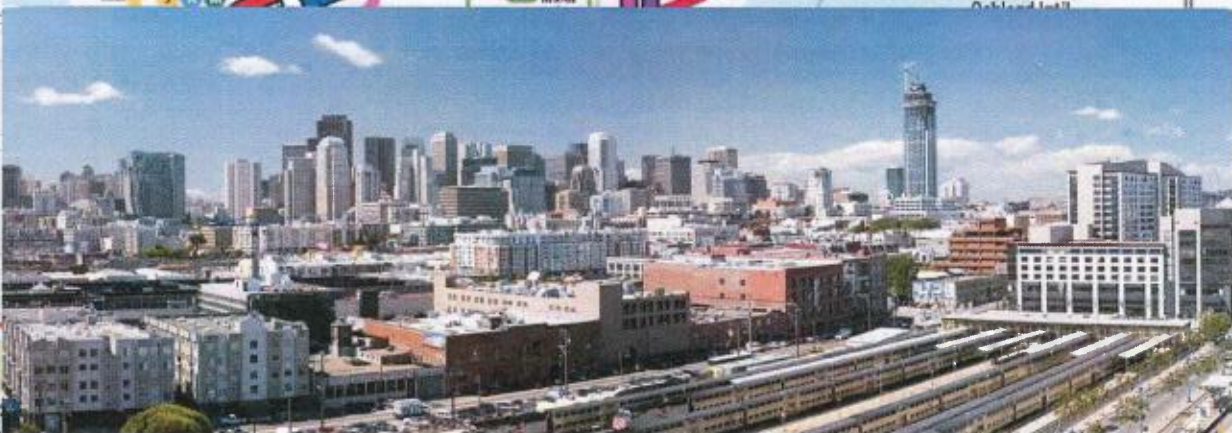
# CENTRAL SOMA DELIVERS HOUSING AND MORE

- Most sites will be for housing
- Seven sites will be available for large office
- There will be a mix of other uses: hotels, institutional, ground floor retail, PDR and community facilities





# CENTRAL SOMA ALSO PROVIDES FOR JOBS - BETTER THAN ANY OTHER LOCATION



# HOUSING IS ALLOWED IN MOST OF SF, WHILE OFFICE SPACE IS CONSTRAINED



**ZONING FOR HOUSING**



**ZONING FOR OFFICE**



# **JOBS/TRANSIT ACCESSIBILITY = ECONOMIC EQUITY**

- Lower income households own fewer or no cars
- Job centers away from transit hubs limits access to lower income households
- Jobs centers away from transit can cause unreasonable financial, social and health toll from long car commutes
- Reduce displacement pressure by 25k jobs



# ADDING MORE HOUSING TO CENTRAL SOMA PLAN

- **Alternate 1:**  
Within the EIR &  
Existing Studies
- **Alternate 2:**  
Beyond the EIR &  
With Additional Study



ADDITIONAL STUDY?

EIR MAX 8,320

EIR NOW 7,060

# CENTRAL SOMA, COSTLY TO DELAY OR HALT

1) Convert Large Office Sites to Housing

2) Increasing Heights

MAKING  
THIS CHANGE

=



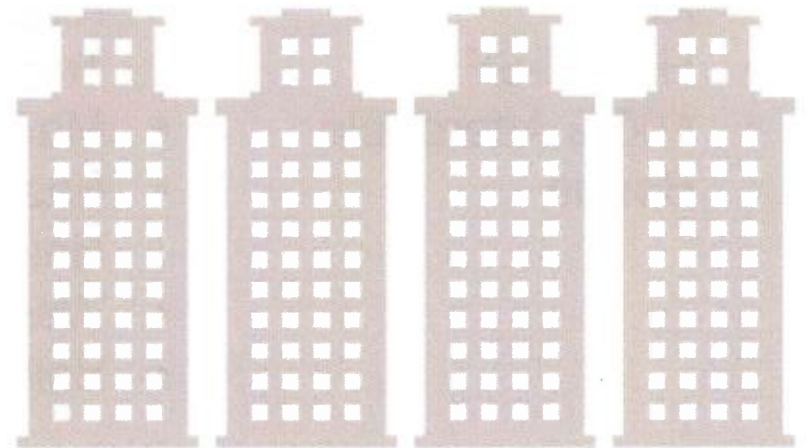
## COSTS

- **Delay.** More delay for housing, than for office.
- **Jeopardize Affordable Housing.** Planned in concert with office.
- **Lost Community Benefits.** Planned in concert with office.
- **Re-evaluate Priorities.** Historic South Park, Existing Housing Preservation in alleys, goals underpinning the plan.



# ADDING HOUSING ACROSS THE CITY, NOW

|                                |                      |
|--------------------------------|----------------------|
| Existing Housing               | <b>392,000 units</b> |
| Under Construction             | <b>6,275 units</b>   |
| Entitled, pending construction | <b>40,975 units</b>  |
| Under Review                   | <b>18,000 units</b>  |
| Latent Capacity                | <b>68,000 units</b>  |



# ADDING HOUSING ACROSS THE CITY, NOW

## INITIATIVES

**23,000 units**

- ADUs
- HOME-SF
- 100% Affordable Bonus

## AREA PLANS

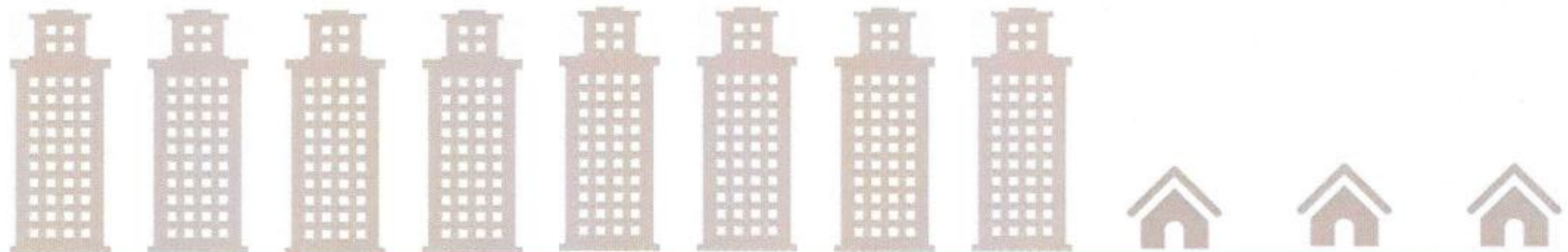
**23,800 units**

- Rincon Hill
- Transbay
- Market & Octavia
- Eastern Neighborhoods
- Western SoMa
- Balboa Park

## MASTER PLANS

**34,900 units**

- Candlestick/  
Hunters Point
- Pier 70
- HOPE SF
- Mission Rock
- Executive Park
- Treasure Island
- Parkmerced
- Schlage Lock
- Trinity





# HOUSING PRODUCTION ACROSS THE CITY

## ANNUAL PRODUCTION

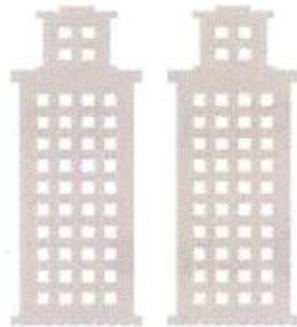
**1990s**

< 1,000 units



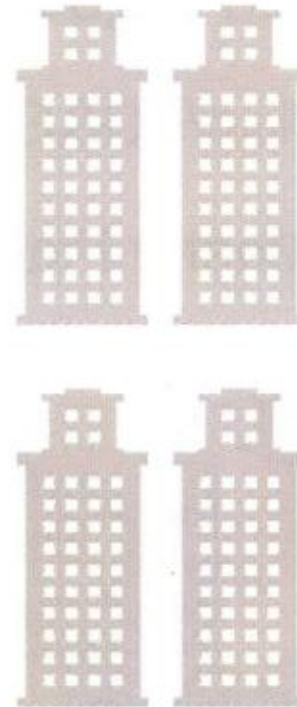
**2000s**

< 2,200 units



**2014-2017**

4,000 units



# HOUSING PRODUCTION, UP NEXT

## In Progress Now

- 700 Innes/India Basin Master Plan | **1,200 units**
- The “HUB” Area Plan | **2,500 units**
- Potrero Power Station/PG&E Master Plan | **2,700 units**
- Balboa Reservoir Master Plan | **1,100 units**





# HOUSING PRODUCTION, UP NEXT

## About to Begin

- Study of 4th & King Railyards + vicinity, including possibly, Western SoMa and parts of Showplace Square & Central SoMa | **# units**
- New density bonus for plan areas | **# units**
- Potential Neighborhood Discussions | **# units**
  - Excelsior
  - District 9 (Mission, Cesar Chavez to Randall)
  - Alemany Corridor
  - The Portola
  - The Bayview (industrial parcels on 3rd Street)

# HOUSING PRODUCTION, WANT MORE?

## Nascent Ideas

- Further expand ADU program
- “Missing Middle” added density in low-scale, mid-scale form
- New neighborhood or corridor-specific planning
- Pushing developers to study more housing during CEQA
- Producing housing faster could improve feasibility

## SUMMARY

- 1) Central SoMa Delivers a Great Deal of Housing
- 2) Provides Jobs for the City that Cannot Be Accommodated Elsewhere
- 3) Adds More Housing in Central SoMa Can Be Achieved With Certain Costs
- 4) Adds Housing Capacity Is Happening Across the City
- 5) More Plans to Increase Housing Capacity Under Development
- 6) Even More Ideas for Increasing Housing Capacity



**MAXIMIZING HOUSING****CONTEXT**

- Proposed Plan: 7,060 units
- EIR-analyzed maximum: 8,320 units
- Difference: 1,260 units



**MAXIMIZING HOUSING****PROPOSAL**

- Raise “commercial-orientation” site threshold from 30k to 40k = 640 net units
- Rezone WMUO to CMUO = 600 net units

**RESULT**

- Plan potential for 8,300 units
- Reduction in potential for jobs to 33,000





## PUBLIC BENEFITS

### Proposal for \$70M of “TBD” benefits

| BENEFIT                                      | \$/YR       | \$/25 YRS      |
|--|-------------|----------------|
| Social and cultural programming              | \$1,000,000 | \$25 million   |
| Park and greenery maintenance and activation | \$600,000   | \$15 million   |
| Capital for cultural amenities (e.g., YBG)   | N/A         | \$15 million   |
| Neighborhood cleaning                        | \$350,000   | \$8.75 million |
| Bessie Carmichael supplemental services      | \$250,000   | \$6.25 million |



**PUBLIC BENEFITS****Rental housing**

- Issue: Projects have become economically infeasible
- Proposal: Removal of CFD on rental projects (up to \$1.75/sqft)
- Results:
  - » Return to economic feasibility
  - » Loss of \$20M-\$30M in public benefits revenue
  - » Need to make commensurate reductions in the public benefits package



**PUBLIC BENEFITS****Timing for delivery of public benefits****Delivered before/with new development:**

- Affordable housing on-site or off-site
- Production, distribution, and repair space
- Some parks, rec centers, and all POPOS
- On-site environmental benefits (e.g., living roofs, stormwater retention)
- On-site childcare
- On-site community facilities
- Transferable Development Rights

**PUBLIC BENEFITS****Timing for delivery of public benefits****Delivered after new development:**

- In-lieu fee-funded affordable housing
- Transit and complete streets
- Other new parks and rehabilitation of existing parks
- Environmental benefits on public land
- School improvements and fee-funded childcare
- Capital funding for Old Mint, cultural facilities, and community facilities



## PUBLIC BENEFITS

### Timing for delivery of public benefits

#### Delivered ongoingly:

- Park and greenery maintenance
- School services (Bessie Carmichael)
- Cultural and social programming
- Neighborhood cleaning



## ADDITIONAL ISSUES

- Input from HPC
- Old Mint
- Affordable Housing
- Community Oversight
- Flower Mart





**THANKS**

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SAN FRANCISCO  
**PLANNING DEPARTMENT**

Received at CPC Hearing 3/22/18  
S. Wertheim

**Historic Preservation Commission  
Resolution No. 0943**

HEARING DATE MARCH 21, 2018

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*Project Name:* Central SoMa Plan  
*Date:* March 14, 2018  
*Record No.:* **2011.1356MTZU [Board File. No 170961]**  
*Staff Contact:* Steve Wertheim, Principal Planner, Citywide Planning  
(415) 558-6612; [steve.wertheim@sfgov.org](mailto:steve.wertheim@sfgov.org)

**ADOPTING A RESOLUTION SUBMITTING A REPORT OF COMMENTS ON THE  
CENTRAL SOMA PLAN TO THE PLANNING COMMISSION AND BOARD OF  
SUPERVISORS**

WHEREAS, Section 4.105 of the Charter of the City and County of San Francisco mandates that the Planning Commission shall periodically recommend to the Board of Supervisors for approval or rejection proposed amendments to the General Plan in response to changing physical, social, economic, environmental or legislative conditions; and

WHEREAS, Section 4.135 of the Charter of the City and County of San Francisco mandates the referral of such matters, prior to passage by the Board of Supervisors, be submitted for written report by the Historic Preservation Commission regarding effects upon historic or cultural resources: ordinances and resolutions concerning historic preservation issues and historic resources; redevelopment project plans; waterfront land use and project plans; and such other matters as may be prescribed by ordinance. If the Planning Commission is required to take action on the matter, the Historic Preservation Commission shall submit any report to the Planning Commission as well as to the Board of Supervisors; otherwise, the Historic Preservation Commission shall submit any report to the Board of Supervisors; and

WHEREAS, The desire for a Central SoMa Plan began during the Eastern Neighborhoods planning process. In 2008 the City adopted the Eastern Neighborhoods Plan, including new land use controls and proposed community improvements for the eastern part of the South of Market neighborhood (SoMa), as well as the Central Waterfront, Mission, and Showplace Square/Potrero Hill neighborhoods. At that time, the City determined that the development potential of the industrially zoned part of East SoMa, coupled with the improved transit to be provided by the Central Subway, necessitated a subsequent, focused planning process that took into account the city's growth needs and City and regional environmental goals. The Central SoMa Plan is the result of that subsequent process; and

WHEREAS, Similarly, the Western SoMa Area Plan, adopted in 2013, explicitly recognized the need to increase development capacity near transit in Objective 1.5, which states that the City should "Support continued evaluation of land uses near major transit infrastructure in recognition of citywide and regional sustainable growth needs." The explanatory text in Objective 1.5 concludes that "The City must continue evaluating how it can best meet citywide and regional objectives to direct growth to transit-oriented locations and whether current controls are meeting identified needs." The Objective's implementing Policy 1.5.1 states that the City should "Continue to explore and re-examine land use controls east of 6th Street, including as part of any future evaluation along the 4th Street corridor." The Central SoMa Plan is intended to fulfill the Western SoMa Plan's Objective 1.5 and Policy 1.5.1; and

WHEREAS, The process of creating the Central SoMa Plan began in 2011. Since that time, the Planning Department released a draft Plan and commenced the California Environmental Quality Act (CEQA) process in April 2013, released an Initial Study in February of 2014, released a revised Draft Plan and Implementation Strategy in August of 2016, and released the Draft Environmental Impact Report in December of 2016; and

WHEREAS, Throughout the process, the Central SoMa Plan has been developed based on robust public input, including ten public open houses; ten public hearings at the Planning Commission; two public hearings at the Board of Supervisor's Land Use & Transportation Committee; additional hearings at the Historic Preservation Commission, Arts Commission, and Youth Commission; a "technical advisory committee" consisting of multiple City and regional agencies; a "storefront charrette" (during which the Planning Department set up shop in a retail space in the neighborhood to solicit community input on the formulation of the plan); two walking tours, led by community members; two community surveys; an online discussion board; meetings with over 30 of the neighborhoods groups and other community stakeholders; and thousands of individual meetings, phone calls, and emails with stakeholders; and

WHEREAS, The Central SoMa Plan Area runs from 2nd Street to 6th Street, Market Street to Townsend Street, exclusive of those areas that are part of the Downtown Plan that comprise much of the area north of Folsom Street. The vision of the Central SoMa Plan is to create a sustainable neighborhood by 2040, where the needs of the present are met without compromising the ability of future generations to meet their own needs. The Central SoMa Plan seeks to achieve sustainability in each of its aspects – social, economic, and environmental. The Plan's philosophy is to keep what is already successful about the neighborhood, and improve what is not. Utilizing the Plan's philosophy to achieve the Plan's vision will require implementing the following three strategies:

- Accommodate growth;
- Provide public benefits; and
- Respect and enhance neighborhood character; and

WHEREAS, Implementing the Plan's strategies will require addressing all the facets of a sustainable neighborhood. To do so, the Plan seeks to achieve eight Goals:

1. Accommodate a Substantial Amount of Jobs and Housing

2. Maintain the Diversity of Residents
3. Facilitate an Economically Diversified and Lively Jobs Center
4. Provide Safe and Convenient Transportation that Prioritizes Walking, Bicycling, and Transit
5. Offer an Abundance of Parks and Recreational Opportunities
6. Create an Environmentally Sustainable and Resilient Neighborhood
7. Preserve and Celebrate the Neighborhood's Cultural Heritage
8. Ensure that New Buildings Enhance the Character of the Neighborhood and the City; and

WHEREAS, The Plan would implement its vision, philosophy, and goals by:

- Accommodating development capacity for up to 40,000 jobs and 7,000 housing units by removing much of the area's industrially-protective zoning and increasing height limits on many of the area's parcels;
- Maintaining the diversity of residents by requiring that over 33% of new housing units are affordable to low- and moderate-income households and requiring that these new units are built in SoMa;
- Facilitating an economically diversified and lively jobs center by requiring most large sites to be jobs-oriented, by requiring production, distribution, and repair uses in many projects, and by allowing retail, hotels, and entertainment uses in much of the Plan Area;
- Providing safe and convenient transportation by funding capital projects that would improve conditions for people walking, bicycling, and taking transit;
- Offering an abundance of parks and recreational opportunities by funding the rehabilitation and construction of parks and recreation centers in the area and requiring large non-residential projects to provide publicly-accessible open space;
- Creating an environmentally sustainable and resilient neighborhood by requiring green roofs and use of non-greenhouse gas energy sources, while funding projects to improve air quality, provide biodiversity, and help manage stormwater;
- Preserving and celebrating the neighborhood's cultural heritage by helping fund the rehabilitation and maintenance of historic buildings and funding social programs for the neighborhood's existing residents and organizations; and
- Ensuring that new buildings enhance the character of the neighborhood and the city by implementing design controls that would generally help protect the neighborhood's mid-rise character and street fabric, create a strong street wall, and facilitate innovative yet contextual architecture.

WHEREAS, The San Francisco Planning Department is seeking to adopt and implement the Central SoMa Plan. The core policies and supporting discussion in the Plan have been incorporated into an Area Plan proposed to be added to the General Plan. The General Plan, Planning Code, Zoning Map Amendments, and Implementation Document provide a comprehensive set of policies and implementation programming to realize the vision of the Plan. The Implementation Document describes how the Plan's policies will be implemented, outlines public improvements, funding mechanisms and interagency coordination that the City must pursue to implement the Plan, as well as controls for key development sites and key streets, and design guidance for new development; and



WHEREAS, Mayor Farrell and Supervisor Kim introduced the Central SoMa Plan's proposed Planning Code and Administrative Amendments and Zoning Map Amendments on February 27, 2018; and


WHEREAS, The Planning Commission voted to Initiate the Central SoMa Plan's proposed General Plan amendments at a duly noticed hearing on March 1, 2018; and

NOW, THEREFORE BE IT RESOLVED that the Historic Preservation Commission has reviewed the Draft Central SoMa Area Plan and other pertinent materials related to Case File No. 2011.1356MTUZ, and this Commission has provided the following comments:

1. The HPC provides its overall support for all policies in the Plan in its efforts to create a better neighborhood, including the creation of open space, the prohibition on small lot consolidation, and the sensitive treatment of the alleyways.
2. The HPC recommends providing increased financial support for the Old Mint under the Central SoMa Plan of at least 50 million in order to rehabilitate and stabilize the structure, including seismic and life safety upgrades.
3. The HPC recommends providing increased financial support for the Arts under the Central SoMa Plan of at least 50 million, so that the preservation, promotion, and programming of tangible and intangible Cultural Heritage activities in the community are well represented.

BE IT FURTHER RESOLVED that the Historic Preservation Commission hereby directs its Recording Secretary to transmit this Resolution, and other pertinent materials in the Case File No. 2011.1356MTUZ to the Planning Commission and the Board of Supervisors.

I hereby certify that the foregoing Resolution was adopted by the Commission at its meeting on March 21, 2018.



Jonas P. Ionin  
Commission Secretary

AYES: K. BLACK, A. HYLAND, A. WOLFRAM, D. MATSUDA, J. PEARLMAN,  
E. JOHNCK, R. JOHNS

NOES: NONE

ABSENT: NONE

ADOPTED: MARCH 21, 2018



# SAN FRANCISCO PLANNING DEPARTMENT

Received at CPC Hearing 3/20/18  
S. Wertheim

MEMO

To: Planning Commission

From: AnMarie Rodgers, Joshua Switzky, and Steve Wertheim,  
Citywide Planning Division

Re: Housing Strategies in San Francisco and Contextualizing the  
Central SoMa Plan

Date: March 21, 2018

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Over the next few weeks you will be considering the Central SoMa Plan. During this time it is important to ask: are we doing enough for housing? The purpose of this memo is to convey the Plan's proposal for housing, and to put this plan within the context of all of the housing strategies taking place in the City. Central SoMa is based upon solid concepts that are central to the City's and the region's future: putting jobs in the right location and producing as much housing as is optimal in this location in the context of all other land use and transportation decisions citywide. Above all, every plan must be considered in the broader context of planning for the broader City over time, as no parcel, site, block, or neighborhood is a self-contained eco-system or isolated decision.

### Central SoMa Delivers Significant Housing Benefit & Capacity

The Central SoMa Plan legislation as currently proposed and reflected in the legislation introduced by Mayor Farrell and Supervisor Kim would likely produce approximately 7,100 new housing units. While Central SoMa is often discussed as primarily a "jobs" plan, the plan provides for a substantial amount of new housing capacity. These 7,100 units would represent over 5% of the City's zoned capacity for housing in a 17-block area that contains only 0.8% of the City's land. In fact, the baseline plan proposal would result in equal square footage for housing and jobs (roughly 8 million square feet each). The Central SoMa Plan, if approved, would deliver as much housing as the Rincon Hill and Transbay Redevelopment Plans combined, both housing-focused plans creating the densest neighborhood in the City. And, Central SoMa provides housing primarily in mid-rise building types that fit with the character of SoMa and the well-considered intentional evolution of the skyline. The Central SoMa Plan, if approved, would deliver more new housing than did the Market & Octavia Plan (6,000 units) — another housing-centric plan that spans 376 acres, over 60% more area than the 230 acres of Central SoMa. Notably, the Central SoMa Plan also provides as much housing as each of the Candlestick Point and Treasure Island projects will achieve (7,200 and 7,800 respectively) -- both of which were complete redevelopment of massive publicly-owned sites each substantially larger in area than Central SoMa -- while Central SoMa still achieves the additional long-term citywide jobs objectives in a much higher-density mixed

use environment than is being delivered in these other areas. Combined with the existing 5,500 housing units currently in the neighborhood, Central SoMa will have a residential population density of about 80,000 people per square mile<sup>1</sup> (not including workers and visitors), which notably denser than both Paris and Barcelona.

Most of the area's hundreds of developable sites are expected to be housing, with a modest percentage of these sites expected to develop with hotel, institutional, and small office uses, and most buildings having ground floor retail, PDR, or community facilities.

However, testimony from the Planning Commission and Board of Supervisors has made it clear that there is interest in the Plan maximizing the amount of housing possible under its Environmental Impact Report: approximately 8,300 new housing units.

### **Central SoMa Provides Jobs for the City that Cannot Be Accommodated Elsewhere**

The Central SoMa Plan area is the best location for anticipated job growth in the City. It is even more important for jobs to be located near transit than housing. This is true whether you are considering the issue from either a vehicular emissions or environmental perspective or from a walkability or human point of view. Numerous studies have shown that people's travel behavior is far more sensitive to distance from mass transit on the destination (i.e. work, shopping, school) end of the trip than on the home end. Compounding this truth is that job space is occupied more densely than housing, so it is much more effective in terms of concentrating more people, activity and major destinations near transit. This means that transit is viable and attractive for more people living in more places to access more activities and opportunities. Forgoing the finite opportunity here and thus locating jobs elsewhere would exacerbate traffic and air quality issues, keep the region from being able to meet its State-mandated targets for greenhouse gas reduction, and increase the transportation cost-burdens for lower-income workers. Outside of Central SoMa, there are very few places in San Francisco with the capacity for new jobs, and none that have the transit infrastructure of this neighborhood. San Francisco has a long-standing policy of centralizing and locating high-density job growth in a compact area near the highest quality transit and limiting spread of jobs in residential neighborhoods around the City. This is both a practical and appropriate policy. If the jobs were to move outside of San Francisco, the environmental and livability outcomes would be even worse, as we could expect the jobs to be located in areas like suburban San Mateo and Santa Clara counties.

### **Jobs-Housing Balancing is an Important Citywide and Regional Issue, But is Not Relevant at the Neighborhood Scale**

The jobs-housing balance (in sheer numbers) or jobs-housing fit (in terms of incomes) of any specific parcel, block, or neighborhood plan is not reasonable to exam outside the context of the city as a whole (and arguably an even larger geography of the commute-shed). No

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<sup>1</sup> Population density is gross density including streets, parks, freeway, and non-residential property. Assuming average of 2.3 residents per unit, 12,600 units total (5,500+7,100). Plan area is 230 acres, or 0.36 square miles. Paris is 55,000 residents/sq mi, Barcelona is 41,000 residents/sq mi.



project or plan sits outside of the context of the city as a whole; the housing built in Central SoMa has no more direct relation to the jobs within the boundaries of the Central SoMa plan area than does the housing planned and built in Hayes Valley or Potrero Hill or Treasure Island or Parkmerced. The workforce is housed throughout the entire city, though jobs are appropriately concentrated in key areas to enhance accessibility. The Central SoMa Plan was developed in the context of all the plans that have been considered in the City over the past 15 years along with those in the works, and in consideration of these citywide overall capacity figures to ensure that the local jobs-housing balance is stabilized and improved over the long term.

The Central SoMa Plan, if adopted, would accommodate approximately 7,100 housing units (for 9,600 workers) and space for approximately 32,000 jobs<sup>2</sup>. With adoption of Central SoMa as currently proposed, the City would have residential buildout capacity of about 136,000 units for approximately 313,000 new residents (of which about 184,000 would be workers<sup>3</sup>) and job buildout capacity for about 167,000 additional jobs. These figures would bring us roughly in line with what the adopted 2017 Plan Bay Area expects San Francisco to accommodate through 2040 in order to meet state mandated greenhouse gas emissions targets. While we could and should continue to add capacity for housing to ensure that there is opportunity to build more housing faster to improve housing affordability, the overall capacity picture for the City is one where there is and would continue to be sufficient zoned housing capacity to house the zoned capacity for new workers.

#### **Adding More Housing in Central SoMa Can Be Achieved With Certain Costs**

Can additional housing be added within Central SoMa? The answer is yes, but there are ramifications to that choice. Under the plan Environmental Impact Report, an additional 1,200 units could be added to the current Plan, totaling approximately 8,300 new housing units, without the uncertainty of additional time, study, and delay.

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<sup>2</sup> The job figures of 50,000 and 40,000 are frequently cited by members of the public as attributable to the Plan or the Plan area. These figures are not correct and are the result of misunderstandings. The 50,000 figure comes from an early environmental review document that was looking at a much broader area than the current plan area, including buildout of portions of the downtown and Transbay under existing zoning. The total maximum non-residential buildout within the current Plan area itself under the proposed Plan is 8 million square feet, of which approximately 7 million would likely be office and the remainder PDR, ground floor retail, hotel, institutional and community uses. The maximum job capacity of this 8 million square feet is approximately 32,000 jobs. The EIR very conservatively estimated the job density for office space at 200 gross square feet per worker versus the long-standing city metric of 276. This aggressive metric was used to be conservative because no updated studies had yet been conducted on the subject. In 2016, the City engaged a consultant to conduct a study of current office worker densities downtown, SoMa and citywide using a variety of methods in order to inform such analyses. The conclusion was that the actual density calculation that should be used is 240 square feet per worker. This metric is now being used in analyses going forward and should be considered as the actual likely capacity. The upshot is that the Central SoMa EIR notably overestimated the potential number of workers that could be accommodated in the Plan area.

<sup>3</sup> Average household is 2.3 persons and 1.35 workers.

Following are potential modifications to the proposed Plan we have identified that could be pursued to gain the 1,200 unit capacity under the EIR:

1. Raise the lot size threshold for requiring some non-residential uses from 30K to 40K sf, which would allow two sites (Wells Fargo and 330 Townsend) to go completely residential, adding approximately 650 units. This reduces the number of potential large office sites from 7 to 5, reducing the likely office development by about 800,000 square feet, and reducing the expected jobs in the area by about 3,500.
2. Rezone lots along Bryant and 6<sup>th</sup> Streets, primarily adjacent to the Flower Mart, to CMUO instead of WMUO, thus allowing housing there, which would allow up to 600 additional units.

These strategies would increase the likely housing count to 8,300 units and reduce the likely job potential to 28,500 jobs.

To go beyond 8,300 units at this juncture would require revisiting the Plan, including some of the Plan's core principles. It would also add significant delay and costs to plan adoption. Within the Plan area, the vast majority (75%) of the projected 28,500<sup>4</sup> jobs would be expected to occur in the area's five remaining large office developments: the Flower Mart, Tennis Club/88 Bluxome, 598 Brannan, 725 Harrison, and One Vassar (2<sup>nd</sup> and Harrison). To yield more housing units while keeping the neighborhood's proposed height limits would require shifting one or more of the five remaining major office sites to be housing. It is important to note that each of these five sites has an active application filed with the Department. Notably, four of these five projects include housing on their sites in addition to office space, three of which would include 100% affordable housing sites on their properties. As well, a key principle of the Plan has been to achieve a balance of significant density while ensuring a quality of place characteristic of SoMa.

To increase the amount of foreseeable housing in Central SoMa beyond 8,300 units would require revisiting the Central SoMa Plan's Environmental Impact Report – a process which is likely to take a substantial amount of time and effort. Delay is important because immediate housing production is part of the complete Central SoMa package. As has been discussed, legislation could streamline and expedite approval of the 7,100 units of housing envisioned by the plan by invoking Assembly Bill 73 adopted last year (sponsored by Assemblymember David Chiu). If the plan is adopted with use of AB73, housing in Central SoMa may come to fruition faster here than anywhere else in the City not covered by a Development Agreement. At present, we have applications on file for 1,800 housing units in the Plan area which are waiting for and reliant on Plan adoption. Notably, delay would imperil the federal financing for a 200-unit affordable housing project at 5<sup>th</sup> and Howard, as well as postpone at least 1,600 other units that would be ready to start construction in the next couple of years.

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<sup>4</sup> Assuming the lot size threshold for requiring non-residential uses is increased from 30,000 to 40,000 square feet as described above.

However, it is possible that some additional capacity for housing could be determined feasible and desirable in Central SoMa, and even more capacity in a broader area. To examine this possibility without endangering immediate housing production, Planning staff could undertake a high-level study to assess potential options for increasing housing capacity in the broader SoMa area. Based on that assessment of options, at the direction of the Commission (and provided with necessary resources) staff could pursue more in-depth planning efforts to flesh these out and undertake the necessary environmental review to enable consideration of such plans or measures. Note that, as described below, the Citywide division has already proposed in its work program for the coming years (FY18/19 and 19/20) a planning process to examine the Fourth & King Railyards and immediately adjacent areas, which we anticipate will consider rezoning along with necessary environmental review.

### **Adding Housing Capacity Is Happening Across the City**

The City of San Francisco currently has approximately 392,000 units. We have an entitled pipeline of 47,250 units<sup>5</sup>, of which 6,275 are currently under construction. An additional 18,000 units are currently proposed and under review<sup>6</sup>. Under today's zoning, another 68,000 units could be entitled now. Most of these pipeline and potential units have been enabled by legislation passed by the City in the past 13 years, as the City has dramatically expanded its housing potential with a rapid succession of housing-oriented plans and policies. Of the City's current housing capacity, over 70% was created within the past decade and a half. During the same time period, many of these same planning efforts significantly reduced job capacity in broad areas of the City<sup>7</sup> while others increased it in focused areas, resulting in an overall shift of the geography of job capacity citywide rather than a net increase overall. The following is a partial list of these recently adopted efforts to increase housing capacity:

- **Citywide Initiatives: Total 23,500**
  - Accessory Dwelling Units (2014/2016): 14,000 units
  - Density Bonus Programs, including 100% Affordable Housing Bonus and Home SF (2016/2017): 9,500 units
- **Area Plans: Total 24,600**
  - Rincon Hill+Transbay (2005): 7,000 units
  - Market & Octavia (2008): 6,000 units
  - Balboa Park Station (2009): 1,800<sup>8</sup> units
  - Eastern Neighborhoods/Western SoMa (2008/2013): 9,800 units
- **Master Plan/Development Agreements: Total 35,600**
  - Candlestick Point/Hunters Point Shipyard (2010): 10,500 units combined (7,200 and 3,300 respectively)

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<sup>5</sup> Including all of the approved master plan development agreements (approx. 30,000 units).

<sup>6</sup> Including proposed individual specific housing projects filed in Central SoMa and other pending rezonings described in this memo (eg Hub, India Basin, etc).

<sup>7</sup> Particularly Market & Octavia, which rezoned a significant chunk of the C-3 to disallow office and require housing, and Eastern Neighborhoods, which eliminated the ability to build office space in large swaths of the former M districts where high-density office space had been a principally permitted use.

<sup>8</sup> Excluding Balboa Reservoir



- Pier 70 (2017): 2,000 units
- HOPE SF (2017): 1,900 units
- Mission Rock (2018): 1,400 units
- Executive Park (2012): 2,800 units
- Treasure Island (2011): 7,800 units
- Parkmerced (2011): 5,600 units
- Schlage Lock (2009): 1,700 units
- Trinity (2006): 1,900 units

It is important to note that the adoption of these plans and policies, along with the more streamlined review processes (eg Community Plan Exemption) that accompanied them are substantially responsible for the notable increase in annual housing production that has taken place in recent years in San Francisco. Since their passage, the City has seen a substantial increase in housing production annually, with an average of 4,000 units per year from 2014 through 2017, compared to an average of less than 1,000 units per year during the 1990s and 2,200 units per year during the 2000s. Given that almost none of the major master plan development agreement projects have yet to begin production, the current pace of housing construction has the potential to increase further over the coming years.

#### **More Plans to Increase Housing Capacity are in Development Now**

In addition to the current capacity plus Central SoMa, in the next couple of years there will be multiple additional opportunities to increase the City's housing potential based on projects and plans currently underway. These include:

- 700 Innes/India Basin master plan (expected 2018): 1,200 units
- The Market/Van Ness "Hub" plan (expected 2019): 2,500 units (beyond the existing Market & Octavia Plan)
- Potrero Power Station/PG&E Switchyard master plan (expected 2019): 2,700 units
- Balboa Reservoir master plan (expected 2019): 1,100 units

In addition to these efforts already underway, which would add 7,500 units to the City's capacity, the Planning Department is intending to begin over the coming year the following efforts on our work program<sup>9</sup>:

- Study of the 4<sup>th</sup> and King Railyards and immediately adjacent areas<sup>10</sup>
- Creation of a local density bonus program (like HOME-SF) for density decontrolled zoning districts like those in the Eastern Neighborhoods and Market & Octavia Plan areas.
- Neighborhood discussions in the Excelsior, D9 (including Mission between Cesar Chavez and Randall, the Alemany Corridor, and the Portola), and the Bayview (including rezoning the remaining M parcels along 3<sup>rd</sup> Street).

<sup>9</sup> Reviewed and approved by the Planning Commission on February 8, 2018, pending release by the Mayor's Office in June.

<sup>10</sup> While the specific study area boundaries have not been determined, this scope was initially conceived of including blocks of Showplace Square and Western SoMa proximate to the railyards. This planning effort could also include portions of Central SoMa.

While these efforts have not yet begun or quantified the additional housing potential that could arise, it would be safe to say the cumulative net addition would be several thousand units.

#### **Additional Ideas for Increasing Housing Capacity Beyond Central SoMa**

While these underway and soon-to-begin plans are likely to yield over 10,000 more units in a few specific areas, the time is right for the City to develop even more strategies for housing, especially strategies to look both beyond the eastern half of the City where plans over the past 15 years have focused and to add a broader range of housing types throughout the City. Given the City's broader housing needs and the extent to which South of Market has been the ongoing focus of successive planning efforts over the past 15 years, a more comprehensive consideration of the City seems warranted. Such ideas could include:

- Expansion of ADU program to allow more flexibility
- Development of a "Missing Middle" program to permit new low-scale and mid-scale multi-family housing that fit within the context of lower density neighborhoods
- Consideration of further neighborhood or corridor-specific planning
- Further use of AB73 to declare additional sustainability districts to speed up housing entitlements





Received at CPC Hearing 3/22/18  
S. Wentheim

Main Concerns

- 1) Customer Parking
- 2) Concern for vendor stall placement in the Pier
- 3) Safety



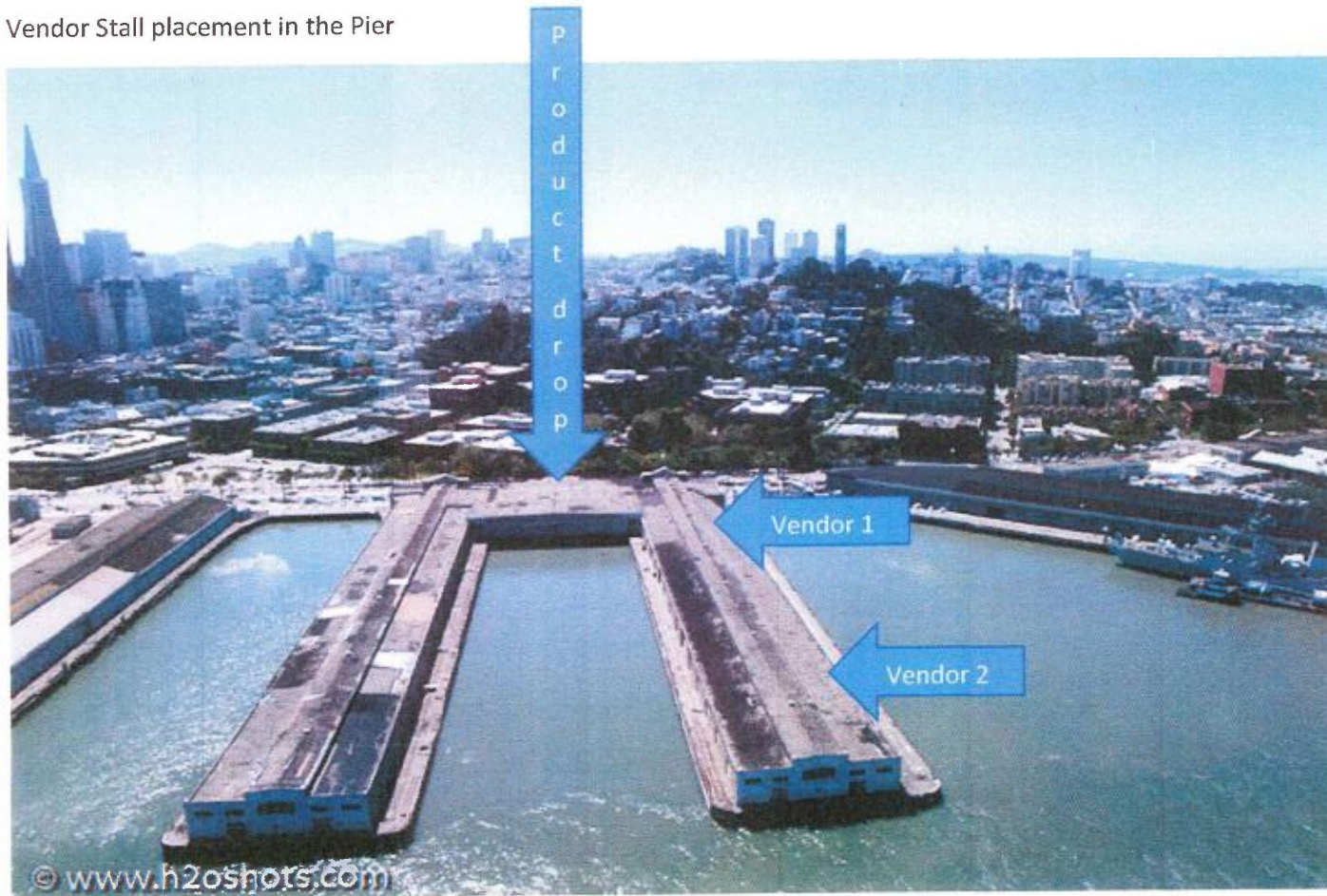
Piers are Approx 600 ft long-( Football field is 360 ft long)

Park at Parking A-Get your flowers at Vendor B

You are walking 2,400 ft- to get product to your van or truck.

That over 6 football fields

Vendor Stall placement in the Pier



Because there is only one access point Vendor 1, who is closest to the door -is going to get more business that Vendor 2 who is at the end of the Pier

Also Vendor 2 is going to have a much harder time getting their product to their stall-





Our truck parked at the curb and we do not clear the bike lane

6AM- Our truck pulling  
in off the Embarcadero

Note the bike lane

And pedestrians



Pulling out  
of Pier 19  
1/2







|               |  |                  |
|---------------|--|------------------|
| Mon, 06/25/18 | San Francisco, California<br><a href="#">View general port information</a>                 | – 4:00pm         |
| Tue, 06/26/18 | At Sea   |                  |
| Wed, 06/27/18 | At Sea   |                  |
| Thu, 06/28/18 | <a href="#">Juneau, Alaska</a>   | 1:30pm – 10:00pm |
| Fri, 06/29/18 | <a href="#">Skagway, Alaska</a>  | 6:00am – 8:30pm  |
| Sat, 06/30/18 | Glacier Bay National Park, Alaska (scenic Cruising)<br>Scenic Cruising                     | 6:15am – 3:15pm  |
| Sun, 07/01/18 | <a href="#">Ketchikan, Alaska</a>  | 8:00am – 6:00pm  |
| Mon, 07/02/18 | At Sea   |                  |
| Tue, 07/03/18 | <a href="#">Victoria, British Columbia</a>   | 7:00am – 2:00pm  |
| Wed, 07/04/18 | At Sea   |                  |
| Thu, 07/05/18 | <a href="#">San Francisco, California</a><br><a href="#">View general port information</a> | 7:00am           |

Princess Cruise Line-  
Alaska Cruise

Thursday July 05, 7AM



Pedestrian, Bikes and traffic on the Embarcadero






**COUNCIL OF COMMUNITY  
HOUSING ORGANIZATIONS**
*The voice of San Francisco's  
affordable housing movement*

February 28, 2018

**RE: Central SOMA Plan Housing Allocations and Jobs-Housing Fit**

Dear Commissioners:

As the Planning Commission begins the process of considering the proposal for Central SOMA, we believe it is very important to evaluate the Jobs-Housing "Fit" – that is, the extent to which the proposed housing, both the total amount and the affordability levels, will match the jobs that the proposed commercial development will create. And, to the extent that this housing need is not met within the Plan, the pressure that this demand will place on existing residents and communities within the South of Market neighborhood.

The Planning Department projects that 7,060 housing units will be built in the Central SOMA Plan Area by 2040 (staff letter to Commission dated Dec 7, 2017). This estimate is based on available soft sites plus state density bonus plus some land dedication from commercial parcels. Some of the 100% affordable housing sites would be located not in the Plan Area, but in the broader South of Market neighborhood, in sites that MOHCD would have to acquire. A breakdown of the Dec 7, 2017 letter estimates:

4,360 market-rate units  
1,040 inclusionary units (19% of 5,430 units built by private developers)  
1,630 in 100% affordable buildings (approx. 15-20 sites with 80-110 units avg.)  
7,060 total units

Planning staff assumes that most of the development in the Plan Area will be commercial development (from email communication with Steve Wertheim 2-23-2018):

6 million sq. ft. office and tech space  
1 million sq. ft. PDR/light industrial and arts activities  
1.5 million sq. ft. retail, restaurant, and hotel/visitor services  
8.5 million square feet of new commercial space

This amount of commercial space would create close to 35,000 new jobs, depending on the future density of office and the split of office and non-office jobs. Using an assumption of 1.27 workers per household, that job creation is equivalent to almost 30,000 total new households.

Putting these two data points side by side – 30,000 new workforce households and 7,060 housing units – reveals a troubling mismatch. The Planning Department's housing proposal assumes that only about a quarter of the overall housing need created by the Plan will be accommodated by the Plan. The remainder of new workers will be left to find housing either elsewhere in the city or

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The Council of Community Housing Organizations (CCHO) is a coalition of 24 community-based housing developers, service providers and tenant advocates. We fight for funding and policies that shape urban development and empower low-income and working-class communities. The work of our member organizations has resulted in nearly 30,000 units of affordable housing, as well as thousands of construction and permanent jobs for city residents.

elsewhere in the Bay Area region. Planning staff's response has been that the city already has a pipeline of 22,000 entitled units throughout the city (or 38,000 if you count the major long-term master plans), that may someday accommodate this need throughout the City.

Even if the Commission agrees that relying on prospective housing development elsewhere in San Francisco and the region to accommodate three-quarters of the workforce households created by the Central SOMA Plan is good policy or that it is realistic that the timing of such housing will be built in relation to the Central SOMA buildout, the realistic likely outcome is that a substantial number of the 30,000 new households will be looking for housing within the existing community of the South of Market area, in proximity to their jobs. With such a dramatic imbalance of housing compared to jobs in the Central SOMA Plan, this will greatly exacerbate SOMA's displacement crisis and evictions epidemic, as new workers who cannot find new housing will push out existing lower income households.

**We, along with the We Are SOMA Coalition, recommend that the plan include strategies and funding priorities to protect existing tenants and acquire vulnerable rent-controlled and SRO buildings for preservation as permanent affordable housing.** This preservation strategy should be aimed at SOMA households earning up to 90% of median income (\$72,000-\$104,000/yr). This is not a strategy for accommodating growth but rather to provide stability to the existing community and mitigate the impacts of the tremendous expected job growth.

The situation for low and moderate-wage workers in these new jobs created by the Central SOMA Plan commercial development is especially dire. We can estimate the percentage of these households that are low-income, moderate-income, and higher income with a Jobs-Housing Fit analysis, by comparing the jobs categories to wage data published by the California EDD.

**Attached is our Jobs-Housing Fit Analysis for Central SOMA.**

Generally, even with an office-heavy mix, up to 55% of the new household growth, or close to 15,000 households, will be in the low to moderate-income categories. Even more will not be able to afford market-rate housing, given today's hyper-expensive SOMA housing market. Planning staff proposes only about 17% of those needed 15,000 affordable units within the Plan Area and in the broader SOMA neighborhood, leaving the Central SOMA Plan short approximately 13,000 affordable units of the need created by the Plan's buildout. *The current pipeline of entitled affordable units FOR THE ENTIRE CITY is only 3,092 affordable units from very-low to moderate-income.*

**We recommend that the Plan commit to at least 50% of all housing be affordable for low and moderate-income households up to 120% of median income, in order to achieve a truer "fit" between new worker household incomes and the housing provided through the Plan.**

Finding sites to build the Plan's affordable housing is a big challenge, given the incredible rise in land prices. In its research for the City's recent Inclusionary Housing policy update, the Office of the Controller identified a 350% increase in the price of unentitled land over the last five years. This kind of land inflation is likely to be exacerbated by the upzoning of the Central SOMA Plan. Even with its low numbers of committed affordable housing, the Planning Department's estimate of 1,630 affordable units would require the acquisition of 15-20 sites for development.

**We recommend a land acquisition and banking strategy in order to get ahead of the land speculation that will be spurred by the upzonings.** This can be linked to land

dedications as part of any development being able to take advantage of greater heights, density or FAR.

Creating moderate-income housing is also a big challenge, and can be best accommodated through inclusionary policies, as was done when the city's inclusionary housing policy was expanded to include middle-income households. The Plan assumes that the Citywide baseline inclusionary housing percentage will be the only one that will be applicable for the Plan Area, whether or not the site has received an upzoning. The Central SOMA Plan in fact envisions major upzonings, which confer significant value on those sites that can be recaptured for a higher affordable housing requirement. The recently adopted "HomeSF" local density bonus program is a fresh example of how such upzoning/value capture works – the Inclusionary was increased to 30% in exchange for additional height and increased density and other development incentives. Only requiring the standard baseline Inclusionary is otherwise akin to a giveaway of the value being conferred by the City through the Central SOMA Plan. Moreover, while the Plan does not prohibit a fee-out option, the Planning staff's numbers seem to assume that almost all developers will provide onsite units, which is very unrealistic.

**We, along with the We Are SOMA Coalition, recommend raising the inclusionary percentage for those residential developments taking advantage of increased heights and densities. This should be based on financial feasibility and a value capture analysis of the upzoning.**

We believe the City's Planning Department should commit to actually analyze the housing impacts, by wage level, by performing a transparent and replicable Jobs-Housing Fit analysis for all new multi-acre projects (and cumulatively for the sum of all projects in the City). We believe this kind of Jobs-Housing-Fit analysis should be a prerequisite for Planning Commission discussions on development and Area Plan approvals. Failing to link commercial development to housing need will continue to exacerbate our jobs-housing imbalance, worsen our housing crisis, and increase pressure on our existing communities.

We look forward to continued dialogue, and would be happy to meet with Commissioners and Planning staff regarding the development of an appropriate Jobs-Housing Fit methodology.

Sincerely,

Peter Cohen and Fernando Martí  
Co-directors, Council of Community Housing Organizations



# Central SOMA Jobs-Housing Fit Analysis

| CENTRAL SOMA JOBS-HOUSING FIT           |           |               |
|---|-----------|---------------|
| Total commercial s.f. of the project    | 8,500,000 | s.f.          |
| Worker Density - Office                 | 200       | s.f./worker   |
| Worker Density - Restaurant & Retail    | 368       | s.f./worker   |
| Worker Density - PDR/Arts               | 597       | s.f./worker   |
| Total jobs created:                     | 35,751    | workers       |
| Workers/Household Assumption:           | 1.27      | workers/HH    |
| Total demand for units:                 | 28,150    | households    |
| Very Low-Income Households              | 5,523     | VLI units     |
|   | 20%       |               |
| Low-Income Households                   | 4,321     | LI Units      |
|   | 15%       |               |
| Moderate-Income                         | 5,712     | Mod Units     |
|   | 20%       |               |
| Total demand for affordable units:      | 15,548    | Afford. units |
| Affordable housing balance:             | 55%       |               |
| Actual units proposed:                  | 7,060     | units         |
| Proposal compared to total demand:      | 25%       | of need       |
| Actual affordable units proposed (38%): | 2,670     | affordable    |
| Proposal compared to affordable demand: | 17%       | of need       |

Council of Community Housing Organizations - DRAFT: February 28, 2018

[illegible]

Notes

This analysis assumes a 247 s.f. per office worker ratio.

The methodology was based on the 1997 Jobs-Housing Fee Nexus Study by Keyser Marston Associates, with updated worker/s.f. densities and accounting for one- and two-income households rather than citywide household sizes.

(1) Proposed s.f., from December 7, 2017 Planning Department letter, and email communication with Planning staff Steve Wertheim, 2-23-2018. Note staff did not differentiate retail from hotel sq. ft.

(2) s.f./workers, from the Transportation Sustainability Fee Nexus Study, Urban Economics, May 2015, Tables A-3 and A-4, p. 50-51.

(3) Workers/Household, derived from Pier 70 EIR assumptions. This mirrors Jobs-Housing Linkage Nexus Study, but is lower than 2015 TSF Nexus (Table A-1, p. 48), which assumes a higher worker density (1.65 workers/household) and greater need for family units.

The EIR assumption seems a more reasonable approach reflective of new household trends toward smaller households, rather than the existing jobs/household ratio for the broader MSA.

(4) The lower the jobs density assumption, the more households are assumed, but greater percent of single-income units (studios and 1-BR) needed to meet demand. The analysis does not consider households with three-plus incomes.

(5) 2016 HUD AMI from SF Mayor's Office of Housing

(6) Household size derived from ACS data. Rather than percent breakdown for MSA, we looked at one-income and two-income households, using assumed workers/household ratio from EIR

(7) SOC Codes and Occupations from 2016 Occupational Employment Statistics 2016 Q1 for MSA. The major occupation categories were assigned to the closest land use category. Occupations with relatively small percentages were not considered.

(8) Assumed distribution based on existing MSA distribution.

(9) Wage data from 2016 Q1 OES.

(10) Quartiles were assigned HUD AMI categories based on wages. If the quartile break point was close to the HUD category break point, the number of workers in that quartile were assigned to that HUD category.

Very Low Income (0-30% AMI)

Low-Income (30-50% AMI)

Moderate Income (50-100% AMI)

Upper Middle (120-150% AMI)

Market-rate (>150% AMI)

(11) One-income households were generally compared to 1-2 person household AMI levels, and two-income households compared to 2-3 person household AMI levels.

(12) Median rents from 2016 Housing Inventory p. 33, from Zumper and Pricedonomics data

(13) HUD assumes that no households under 120% of median income can afford market-rates without excessive rent burden or overcrowding. This is the total need for affordable housing.

(14) We only assumed direct jobs creation from commercial land uses. Indirect jobs created to support residential uses, open space, and parking were not included in the analysis.

(15) Project affordable housing need differs from RHNA percentages because RHNA looks at citywide job growth estimates beyond the project site.

and RHNA estimates account for citywide population growth, including non-worker households such as seniors, which are not accounted for in the project-based Jobs-Housing Fit.

(16) OES does not provide average wage data for the top quartile. We assumed the next AMI category above the third quartile, and assigned it to the top quartile.

(17) "Upper Middle" is not formally a HUD income category, but is added here because of San Francisco's unique affordability issues for upper middle income earners.





## South of Market Community Action Network

1110 Howard Street | SF, CA 94103 | phone (415) 255-7693 | [www.somcan.org](http://www.somcan.org)

March 22, 2018

SF Planning Commission  
San Francisco Planning Department  
1650 Mission Street, Suite 400  
San Francisco, CA 94103

Dear Commissioners,

The proposed rezoning under the Central SoMa Plan represents a plan created by and for developers, not the existing community. As it stands, the plan is a recipe for gentrification and displacement. By upzoning and allowing office and housing uses where they were previously banned, the city is effectively raising the value of land in the South of Market and inviting rampant speculation. These changes will also mean increased rents for both residential and commercial tenants as new developments create a new and higher comparable value for the area.

The creation of new technology jobs and majority new market-rate housing, as envisioned in the plan, explicitly shows how the city is catering towards a wealthier and more highly educated group of people who do not currently live in San Francisco or the South of Market. The plan proposes adding roughly 32,000 new jobs and 7,000 new housing units (with only 30% affordable). This is at the expense of existing low-income and working-class communities and communities of color in the South of Market. The all too familiar result of the changes proposed under the Central SoMa Plan will include evictions, displacement, and the further homogenization of San Francisco in terms of race and class.

The South of Market Community Action Network (SOMCAN) was born out of the struggle against gentrification and displacement brought with the first technology boom in San Francisco and the South of Market. As a leader in inclusive, community-based planning, SOMCAN has continued this fight over the last 20 years and was recently chosen by the design firm Raising Places as one of six organizations across the country to participate in a year long community design process focused on children, youth, and families here in the South of Market. SOMCAN has witnessed the results of successive waves of gentrification in the South of Market as low-income residents, people of color, and the working class continue to get displaced. Based on the Urban Displacement Project, a research and action initiative of UC

Berkeley in collaboration with researchers at UCLA, community-based organizations, regional planning agencies and the State of California Air Resources Board, the South of Market is in advanced stages of displacement and gentrification.

With the passage of the Central SoMa Plan, this pattern of displacement will only intensify. Instead of looking to market-based solutions to address the gentrification and displacement crisis, the city needs to start prioritizing interventions and regulations that can actually keep people in place. In order to combat the destructive effects of the Central SoMa Plan on the existing community, emergency controls need to be put in place and serious changes must be made to the plan.

SOMCAN outlines the following emergency steps for the Central SoMa Plan in order to address gentrification and displacement:

A) Establish Interim Controls in the South of Market **BEFORE** the Central SoMa Plan is Implemented to Prevent Gentrification and Displacement:

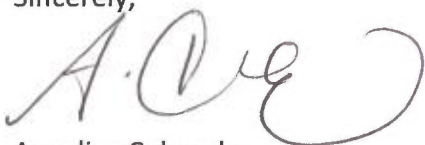
1. Aggressive acquisition of rent-controlled buildings
2. Aggressive site acquisition for new 100% affordable housing
3. Right of First Refusal for residential and commercial renters
4. Moratorium on the sale of existing rent-controlled buildings, the sale of public land for private or for-profit development, and on new market-rate housing construction for projects not included in the existing Central SoMa Plan

B) Make the Following Changes to the Central SoMa Plan **BEFORE** the Plan is Implemented:

1. 50% affordable housing for any new market rate housing development, with an AMI range of 30%-90% for new affordable units
  - a. This percentage of affordable housing is consistent with San Francisco's Housing Balance Policy passed in 2015
2. Mandatory land dedication of sites for affordable housing for any development that is 1 acre or larger

The city must take proactive steps towards stabilizing and protecting the existing community in the South of Market, especially those who are most vulnerable to displacement. Without implementing the solutions outlined above, SoMa and the entire City's shocking levels of inequality will only worsen.

Sincerely,

A handwritten signature in dark ink, appearing to read 'A. Cabande', with a large, sweeping loop at the end.

Angelica Cabande

## TODCO GROUP CENTRAL SOMA COMMUNITY PLAN - ESSENTIAL PROVISION: ASSURING AFFORDABLE SPACES TO BUILD A REAL NEIGHBORHOOD

Expand the uses allowed in the required new Central SOMA office project PDR/Arts spaces (with an off-site option) to also include: Neighborhood Retail (define carefully, no restaurants), nonprofit community services, city-owned public facilities (like a new firehouse, rec center), and Legacy Businesses - These are things real neighborhoods need!

**Affordable BMR PDR/Neighborhood Spaces: Give Prop M Priority** to office projects that commit to (a) rent their new PDR space @ 50% of market and (b) give priority for it to displaced businesses, PDR/arts, neighborhood retail, nonprofit community services including childcare centers, public facilities, and Legacy Businesses.

### FINAL RESULTS: San Francisco Voter Survey Citywide Sample: N=500, Margin of Error: $\pm 4.5\%$ Interview Dates: March 14-19, 2018

25. In some years, there are more office developments proposed than can be approved due to the limits in Proposition M. When this happens, the City Planning Department can set priorities for which office developments to approve first. Would you support or oppose giving the highest priority to office developments that include significant amounts of affordable space at substantially below-market rents, with this space restricted to small PDR businesses, neighborhood-serving retail shops, arts studios and workshops, and nonprofit community services?  
**IF SUPPORT/OPPOSE:** Is that strongly or somewhat?

|                   |    |      |
|-------------------|----|------|
| SUPPORT STRONGLY  | 42 | →73% |
| SUPPORT SOMEWHAT  | 31 |      |
| OPPOSE SOMEWHAT   | 8  | →19% |
| OPPOSE STRONGLY   | 11 |      |
| DON'T KNOW        | 7  |      |
| PREFER NOT TO SAY | 1  |      |

Please tell me if you would support or oppose giving the highest priority to office developments that include permanently affordable space for each of the following.

**IF SUPPORT/OPPOSE:** Is that a strongly SUPPORT/OPPOSE, or only somewhat?)

|     |   | SUPPORT  |           | OPPOSE    |          | Don't Know | SUMMARY |        |
|-----|---|----------|-----------|-----------|----------|------------|---------|--------|
|     |   | Strongly | Some-what | Some-what | Strongly |            | Support | Oppose |
| 26. | Small Production, Distribution, and Repair businesses and arts activities | 42       | 36        | 9         | 8        | 5          | 78      | 17     |
| 27. | Small Legacy Businesses   | 46       | 35        | 5         | 10       | 4          | 81      | 15     |
| 28. | Neighborhood retail, such as a supermarket                                | 40       | 39        | 8         | 9        | 4          | 79      | 17     |
| 29. | Public recreation facilities, such as a community swimming pool           | 50       | 34        | 7         | 7        | 2          | 84      | 14     |
| 30. | Childcare centers   | 61       | 25        | 4         | 7        | 3          | 86      | 11     |
| 31. | Nonprofit community service centers                                       | 52       | 30        | 6         | 8        | 4          | 82      | 14     |



## ZONING

# GIVE PROP M OFFICE DEVELOPMENT PRIORITY TO PROJECTS THAT INCLUDE 0.4 FAR AFFORDABLE SPACE

For PDR/Arts, Heritage Businesses, Neighborhood Retail, Childcare Centers, Community Services and Public Recreation Facilities



*PDR Small Business*



*Legacy Business*

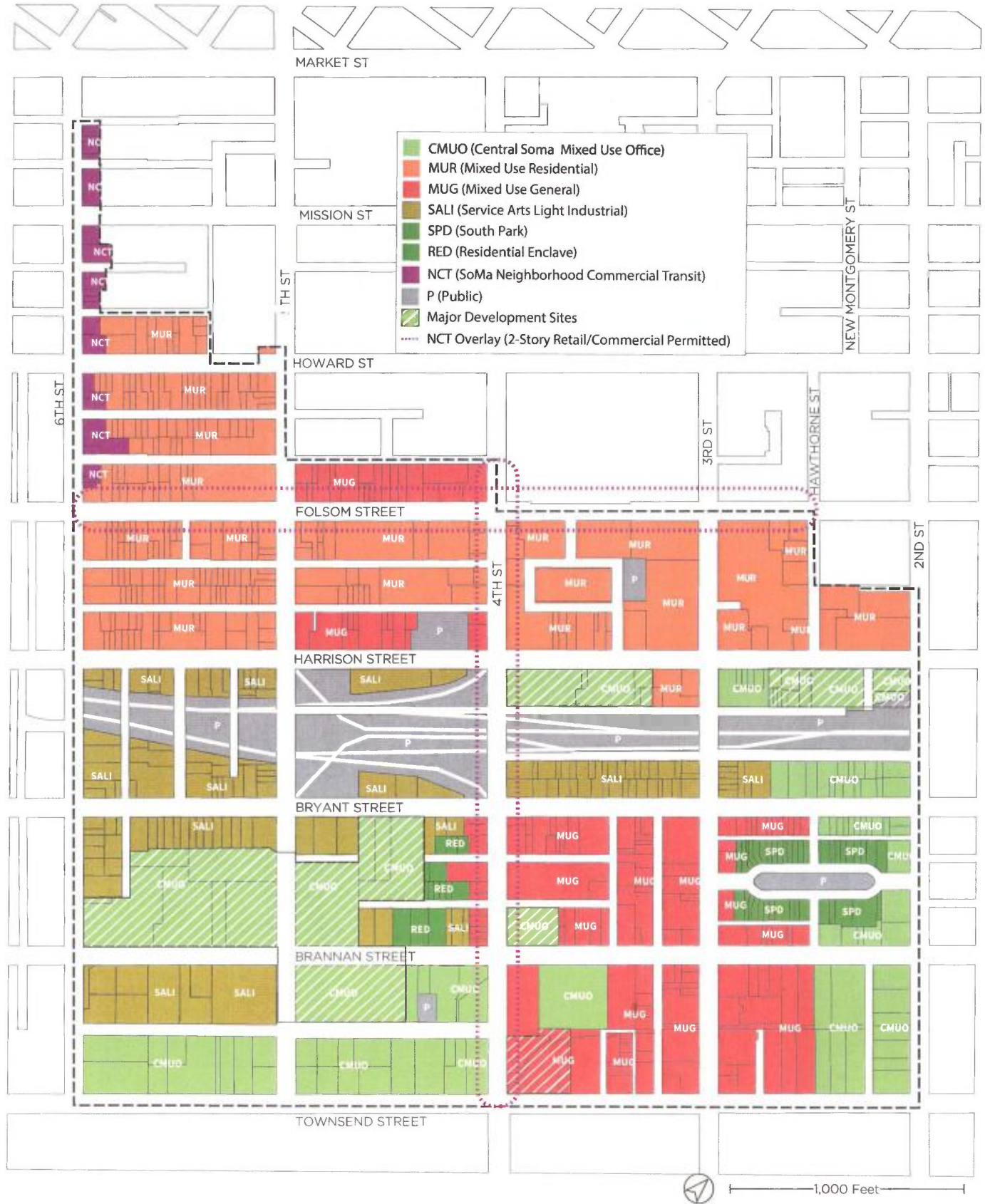


*Neighborhood Retail*



*Community Arts*

**TODCO PROPOSED ZONING (Adapted from SF DCP)**





# ZONING

## COMPARING LAND USE PRIORITIES

SF Department of City Planning's Central SOMA Plan  
Versus TODCO's Central SOMA Community Plan

| PRIORITY USE                     | CURRENT 1990 AND 2010 ZONING | DCP PROPOSED ZONING    | TODCO PROPOSED ZONING |
|----------------------------------|------------------------------|------------------------|-----------------------|
| OFFICE                           | 33.8 acres<br>(21.1%)        | 106.8 acres<br>(66.5%) | 41.9 acres<br>(26.1%) |
| RESIDENTIAL                      | 45.5 acres<br>(28.3%)        | 27.0 acres<br>(16.8%)  | 77.6 acres<br>(48.3%) |
| SERVICE ARTS<br>LIGHT INDUSTRIAL | 60.2 acres<br>(37.5%)        | 7.1 acres<br>(4.4%)    | 21.3 acres<br>(13.3%) |
| PUBLIC                           | 21.0 acres<br>(13.1%)        | 19.6 acres<br>(12.2%)  | 19.6 acres<br>(12.2%) |
| <b>TOTAL</b>                     | 160.5 acres                  | 160.5 acres            | 160.5 acres           |

Source: TODCO, March 2018.







Received at CPC Hearing 3/22/18  
 S. Warheim  
 I am submitting  
 these documents  
 as public comment  
 #126  
 A.S.



THE GAME HAS CHANGED

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## Is the Shipyard safe? Dr. Sumchai writes EPA opposing transfer of more Hunters Point Shipyard land to San Francisco and Lennar, as NBC questions radiation testing

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May 13, 2015



This report by [Vicky Nguyen](#), [Liz Wagner](#) and [Felipe Escamilla](#) of NBC Bay Area's Investigative Unit aired May 12, 2015. Read the transcript [here](#).

by *Ahimsa Porter Sumchai, M.D.*

To: Lily Lee, Cleanup Project Manager, Superfund Division, U.S. Environmental Protection Agency Region 9, 75 Hawthorne St., San Francisco, California 94105

Re: Public Comment – Proposed Transfer of Hunters Point Naval Shipyard Parcels D2, UC1 and UC2

Dear Ms. Lee,

I wish to submit the following comments regarding human health and safety concerns stemming from the proposed transfer of HPNS (Hunters Point Naval Shipyard) Parcels D2, UCI, UC2 and associated buildings 813, 819, 823 and IR 50 storm drains and sanitary sewer lines.

In August 2001, I founded the Radiological Subcommittee of the HPNS Restoration Advisory Board and submitted comments to the HRA (Historical Radiological Assessment) iterations beginning in 2002 until publication of the Draft [Final HRA](#) in 2004. Additionally, I served as the attending physician for the Palo Alto Veterans Administration Hospital Persian Gulf, Agent Orange, Ionizing Radiation Registry in 1997 and as the health and environmental science editor of the San Francisco Bay View newspaper beginning in 2000.

The Radiological Subcommittee of the Hunters Point Naval Shipyard Restoration Advisory Board played an instrumental role in advocating for Navy, public and regulatory response to radiation impacted structures on former Parcel A, slated for transfer to the City and County of San Francisco in 2004, including laboratories of the NRDL (National Radiological Defense Laboratory) and radiation contaminated storm drains and sanitary sewer lines designated IR 50.

My principle concerns center on the presence of lead based paint (LBP) and asbestos containing material (ACM) in buildings on these parcels that may ultimately be used for residential development. The U.S. Navy states in Section 3.6 of the UCI FOST (Finding of Suitability to Transfer), "In the event Buildings 819 and 823 will be reused as residential property, the Transferee will be required to renovate them consistent with regulatory requirements for abatement of LBP hazards."

Ahimsa Porter Sumchai, M.D.



Additionally, I am concerned about potential radiation contamination from storm drains and sewer lines emanating from Building 813 on Parcel D2 that did not undergo excavation and disposal by the Navy as documented in the Parcel D2 FOST dated Aug. 9, 2010.

The Naval Radiological Defense Laboratories were located along Crisp Avenue and included Buildings 816 and 821 on Parcel A and Building 322 on Parcel D. The HRA documents that scientists of the NRDL poured effluents of radioactive waste down laboratory drains of the main laboratories located along Crisp Avenue, communicating with the sanitary and storm sewer system, constructed in the 1940s to drain via conveyance piping and 40 separate discharge outfalls into San Francisco Bay.

The Parcel D2 FOST documents sewer lines were not excavated on the north side of Building 813. The Department of Toxic



Research by Dr. Sumchai and by NBC Bay Area's Investigative Unit finds that several building on parcels proposed to be transferred from the Navy to the City and then to Lennar for development have not been cleaned of radiation and other toxins that would threaten the health of anyone who may work or live on the Hunters Point Shipyard. This photo was taken in 2011. – Photo: Crystal Carter



Substances Control memo dated Oct. 28, 2009, clearly states, "This memo does not issue radiological-free release of the subsurface sewer and utility lines emanating from Building 813." Parcel D2 was created in March 2004 when the Navy revised the southeastern boundary of Parcel A to exclude structures identified by the HRA as being radiologically impacted, including Buildings 813 and Buildings 819. Thus, adjacency issues are evident that challenge the U.S. Navy covenant warranting "all remedial action necessary has been taken" and its decision not to utilize covers or institutional controls at Parcel D2 (7.0 Covenants Parcel UCI FOST).



Hunters Point Shipyard Building 813 is a large, 262-by-262-foot, four-story reinforced concrete, flat-roofed building.

contain LBP based on the date of construction." This conclusion is stated in stark contrast to the U.S. Navy's admission that "demolition of non-residential buildings and structures constructed prior to 1978 creates the possibility of lead being found in soil." Buildings 819 and 823 on UCI were not surveyed for LBP, as they were not residential structures; however, "they are assumed to

The Navy's statement in Section 3.6 that it is "not aware of any LBP that has been released into the environment and poses a threat to human health on the property" is not true and should be deleted from the Parcel UCI FOST, given this parcel was created by redefining the southeast boundary of Parcel A. The U.S. Navy should correct this statement to reflect the findings of the Parcel A FOST dated Oct. 14, 2004, Section 3.0 Regulatory Coordination:

"In November 1996 SFDPH (San Francisco Department of Public Health) sent a letter stating the Navy did not adequately address SFDPH's concerns about lead based paint in soil. Soil at former residential structures on Parcel A were sampled during a 1993 LBP survey (TetraTech 1993a). Elevated concentrations of lead were detected in soil samples collected from former housing unit R-105.

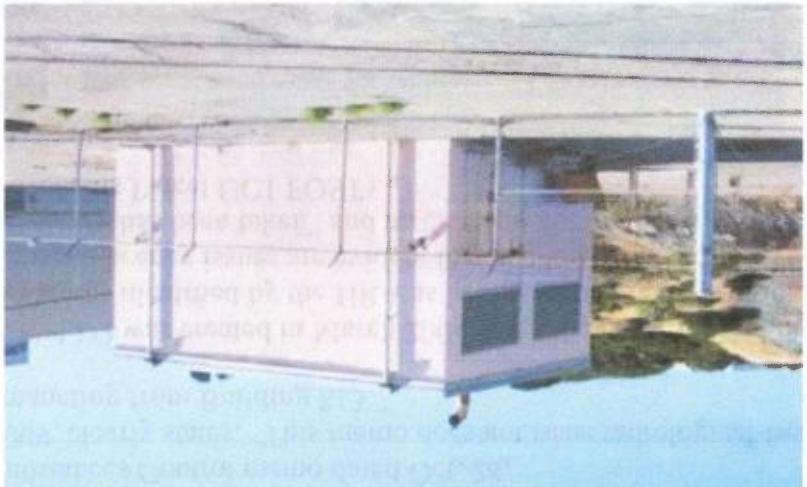
"It was resampled in 1997 and when the supplemental sampling was complete, the BRAC (Base Realignment and Closure) Cleanup Team reviewed all data on lead for Parcel A (1993-1997) with respect to the 221 mg/kg health based cleanup standard set by DTSC's (California Department of Toxic Substances Control) blood level computer model (DTSC 1994). The average concentration of lead in soils across Parcel A derived from the 1993 and 1997 sampling events was 215 mg/kg (EPA 1998).

"The transferee acknowledges that the transferor assumes no liability for costs of any kind or for damages for personal injury, illness, disability or death to the transferee, or to any other person, including members of the general public, arising from or incident to the purchase, transportation, removal, handling, use, disposition, or activity causing or leading to contact of any kind whatsoever with ACM in the improvements including, but not limited to, the buildings, structures, facilities, and utilities on the property."



Obvious concerns are raised by the known presence of asbestos containing material in Buildings 819 and 823. Remediation of ACM by the Navy is not required on buildings, structures, facilities scheduled for demolition by the transferee. Transfer documents prohibit occupation of buildings until ACM is abated or the building demolished and the transferee is responsible for ACM.

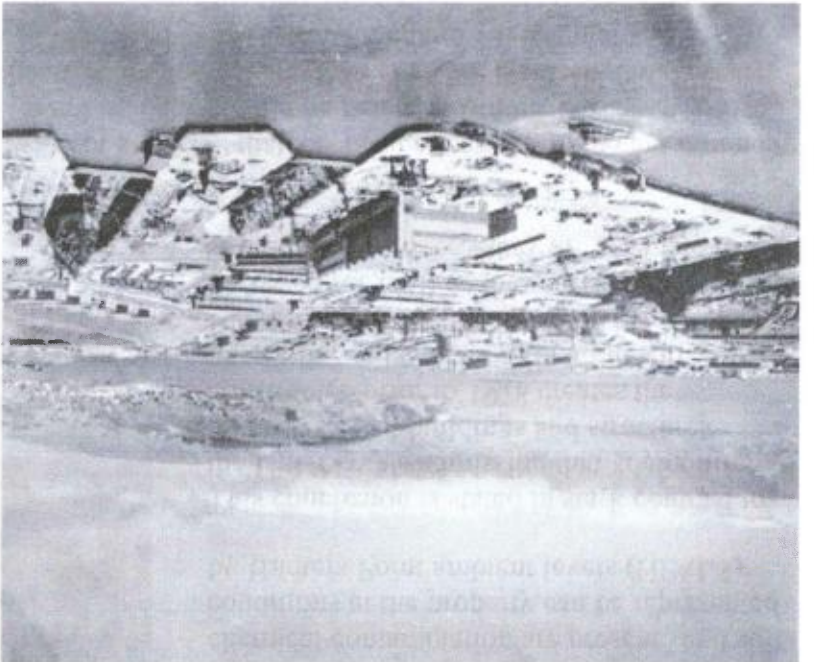
Finally, it should be brought to public awareness that significant adjacency issues are evident from Parcel B benzene vapor intrusion. "2010 soil gas samples collected from portions of southeast Parcel B indicated concentrations that could pose an unacceptable risk to potential future



Hunters Point Shipyard Building 819 is a sewage lift station containing dry and wet wells, both approximately 20 feet deep.

residential receptors via vapor intrusion (Sealaska 2010)." Section 6.0 identifies that "risk to human health may exist from potential intrusion of VOC (volatile organic compounds) vapors into structures built at the property in certain areas as designated in Figure 5."

CERCLA institutional controls will be implemented to prevent exposure to chemicals of concern in soil and groundwater on the property. It should be restated the Navy conducted no soil sampling on Parcel D2 for chemicals other than radiological contaminants despite the potential for lead and asbestos from demolished buildings being present on the base and known elevations in average lead samples approaching remediation standards.



In 1945, when this aerial photo was taken, the Hunters Point Shipyard was the nation's center for radiological research. Some 20,000 people worked there, the majority Black people recruited from Texas and Louisiana and living in barracks on Hunters Point Hill that rises to the right, just out of view in this photo.

4165.72 that "all remedial action necessary to protect human health and the environment with respect to any hazardous substance remaining on the property has been taken before the date of transfer." } DoD (Department of Defense) Instructions the provisions of CERCLA and as set forth in 7.0) by the United States, made pursuant to that CCSF not accept the covenant (Section City and County of San Francisco and ask transfer of Parcels UC-1, UC-2 and D2 to the

In conclusion, I do not support the proposed remediation standards. cc: Mara Rosales, chairperson, Commission on Community Investment and Infrastructure; Tiffany Bohee, executive director, Office of Community Investment and Infrastructure; Willie Ratcliff, publisher, SF Bay View newspaper; Veronica Hunnicutt, chairperson, Hunters Point Shipyard Citizens Advisory Committee Executive Committee; Mayor Edwin Lee; San Francisco Board of Supervisors; Elizabeth Wagner, NBC News.

Ahimsa Porter Sumchai, M.D., founded and chaired the Radiological Subcommittee of the Hunters Point Shipyard Restoration Advisory Board in August 2001 and was an elected member of the Hunters Point Shipyard

3/19/2018 San Francisco Bay View » Is the Shipyard safe? Dr. Sumchai writes EPA opposing transfer of more Hunters Point Shipyard land to San Francisco and L... Restoration Advisory Board from November 2000 to June 2005. She can be reached at 415-859-5471. This letter was transmitted on April 30, 2015.

### To learn more

Dr. Sumchai recommends that for an excellent, comprehensive account of the cleanup and redevelopment of the Hunters Point Naval Shipyard, you read "[Hunters Point Shipyard: A Shifting Landscape](#)," a report of the Civil Grand Jury of the City and County of San Francisco 2010-2011.

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## Comments (4)

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Ahimsa Porter Sumchai · 147 weeks ago

At the time of presentation to Commission on Community Investment and Infrastructure Building 823 does not appear on maps identifying radiation impacted sites at HPNS that appear in the February 16,2005 Basewide Radiological Work Plan published by the Navy Base Realignment and Closure Program.

Report

golars · 110 weeks ago

Golars personnel have extensive experience in providing consultation with federal, state and local regulators, voluntary remediation programs, Remediation strategies and Brownfield programs, and provide site investigations, health risk assessments, risk prioritization and analysis of Brownfield cleanup alternatives to maximize public-private investment dollars.

[Remediation strategies](#)  
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Reply

Report

Alvaro souza · 37 weeks ago

It is safe for me to work at hunters point construction site

Reply

Report



Ahimsa Sumchai MD · 5 hours ago

Please note that according to an article appearing in SF Curb today San Francisco accepted transfer of this property with three levels of federal regulators who were aware of the radiological fraud. <https://sf.curbed.com/2018/3/13/17081188/san-fran...>, Francisco DaCosta and I were the only two people who showed up at the Planning Commission Hearing to argue against this transfer!

Reply

Report

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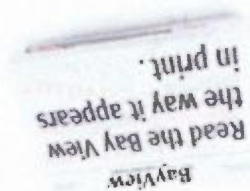
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September 13, 2016

Lawrence Lansdale, Environmental Director  
Naval Facilities Engineering Command (NAVFAC)  
Base Realignment and Closure (BRAC) Program Management Office West  
Department of the Navy  
33000 Nixie Way, Building 50  
San Diego, CA 92147

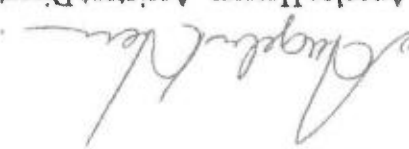
Dear Mr. Lansdale:

Thank you for the meeting on July 14, 2016, at the Region 9 office of the U.S. Environmental Protection Agency (EPA) regarding Navy's cleanup of radioactive material at the Hunters Point Naval Shipyard (HPNS) in San Francisco, California. As we stated then, integrity of the data from the Navy's contractor Tetra Tech ECL, Incorporated, (Tetra Tech) is of the utmost importance in ensuring the cleanup decisions are made in a manner that protects public health and the environment and complies with requirements of the Comprehensive Environmental Response, Compensation, and Liability Act (CERCLA).

We understand that several agencies are currently engaged in ongoing investigations regarding the nature and extent of Tetra Tech's misrepresentation of data delivered to the Navy. I am confirming that we agreed in the July 14, 2016, discussion, that the Navy will not propose any further transfers of Navy property at HPNS without results of these investigations and/or any other Navy action necessary to clarify the actual potential public exposure to radioactive material at and near the HPNS.

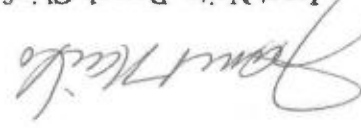
If you have any questions or comments about the Hunters Point Naval Shipyard cleanup, please contact either of the undersigned if you would like to discuss this matter further.

Sincerely,

  
Angeles Herrera, Assistant Director

Federal Facilities Branch, Superfund Division  
U.S. Environmental Protection Agency

Region IX  
(415) 972-3144

  
Janet Naito, Branch Chief

Department of Toxic Substances Control  
State of California  
Cleanup Program - Berkeley Office  
(510) 540-3833

Cc: Thomas Machiarella and Derek Robinson, Navy  
Amy Brownell, City of San Francisco Department of Public Health  
Alec Naugle and Tina Low, Regional Water Quality Control Board

Steve Castleman, SBN 97564  
Collin McCarthy, SBN 305489  
Jordan Davis, PTL # 41751  
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1717 Redwood Ln  
Davis, CA 95616  
Telephone: (530) 759-8421  
Facsimile: (530) 759-8426

Attorneys for Petitioners  
GREENACTION FOR HEALTH  
AND ENVIRONMENTAL JUSTICE

**UNITED STATES NUCLEAR REGULATORY COMMISSION**  
**Before the Executive Director for Operations**

GREENACTION FOR HEALTH AND ENVIRONMENTAL JUSTICE,

Petitioner,

V.

TETRA TECH EC, Inc.

Licensee.

**10 CFR §2.206 PETITION  
TO REVOKE MATERIALS  
LICENSE NO. 29-31396-01**



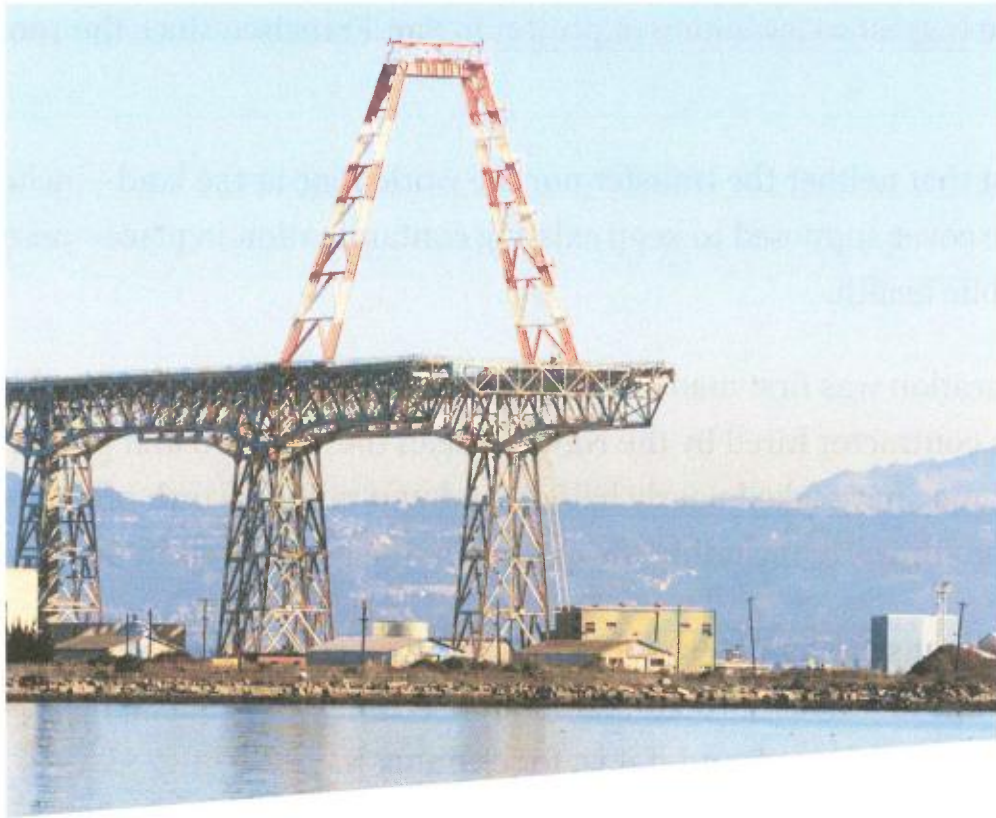


Photo by Associated Press/Eric Risberg

# San Francisco accepted Hunters Point shipyard land that may still be radioactive

*EPA, state health regulators approved transfer in 2015 despite awareness of fraud allegations*

By **Chris Roberts** | @cbloggy | Mar 13, 2018, 3:45pm EDT

**S**an Francisco officials accepted land for development at the former Hunters Point naval shipyard that may still be contaminated with radioactive pollution, documents and interviews show.

The transfer occurred despite three layers of review from federal, state, and local environmental and public-health regulators.

All of those agencies were aware at that time of a widening falsification scandal that, one year later, halted all land transfers at the shipyard, an EPA Superfund site that's the

location of the biggest redevelopment project in San Francisco since the 1906 earthquake.

Officials insist that neither the transfer nor the work done at the land—including repairs to the durable cover supposed to keep existing contamination in place—pose any risk to worker or public health.

But that declaration was first made in 2016, before a later review revealed that Tetra Tech, a major contractor hired by the Navy to clean the shipyard and prepare it for development, may have faked nearly half of the \$250 million worth of work done throughout the shipyard, including the cleanup at the two parcels in question.

The Navy is responsible for determining if the land—a strip of mostly paved roadway in front of buildings that house artists' studios and a commercial kitchen used by food trucks—is still contaminated, and if it is, for cleaning it.

There's currently no timeline for when that may happen, a Navy spokesman said in an emailed statement.

Spokespeople for the Navy, EPA, and the city's Office of Community Investment and Infrastructure did not directly address questions as to how the city received potentially dirty land. For environmental watchdogs, the transfer reveals what they say are deep flaws in the process at the shipyard—the planned anchor of a new neighborhood that's supposed to have 12,000 badly needed housing units—that they say prioritize redevelopment over concerns for public health and safety.

**“Why did government agencies keep saying that everything was fine even after they knew that fraud had occurred?” – Bradley Angel, executive director of Greenaction**

Tetra Tech was able to present findings to the Navy showing the areas were clean. These claims were made based on data that a later review found to be obviously flawed—but also presented in a context where other Tetra Tech data was known to be questionable.

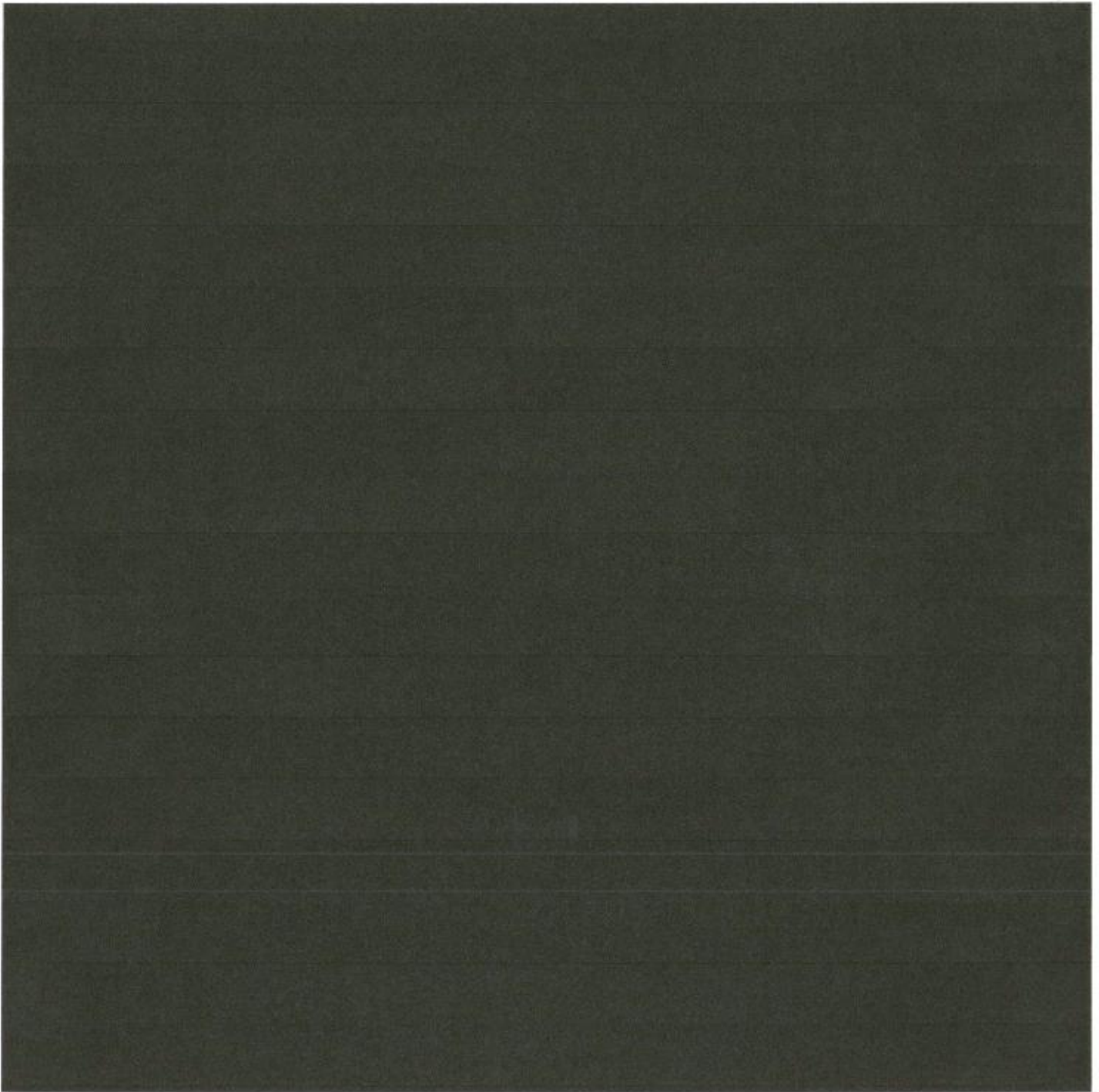
The Navy then presented those findings to the federal EPA, state [Department of Toxic Substances Control](#), and local Department of Public Health. No regulators raised concerns, according to a review of documents filed prior to the transfer. These documents show no mention of even the possibility of problems on these parcels, despite knowledge of the widening scandal with Tetra Tech's work.

Instead, while the land transfers were halted, the city's Office of Community and Investment and Infrastructure hired an engineering firm to make an argument to relax existing land-use restrictions in order to place more housing at the shipyard, documents show.

Watchdogs say this series of events raises serious questions about the effectiveness of federal and local oversight at the contentious project—oversight that may be even weaker in the future, with an understaffed Trump administration-era EPA—and whether that oversight ever amounted to more than a rubber stamp at best.

“As far as I can tell” that’s what it was, said David Anton, an environmental lawyer representing several former Tetra Tech workers and contractors at the shipyard, whose whistleblower complaints broke the scandal open. “I have not seen them do anything on their own to confirm health and safety aspects at all.”





Hunter's Point Naval Shipyard. | Photo by [vhines200](#)

“Everyone should be alarmed, and outraged, that the apparent fraud was so widespread and included areas already transferred from the Superfund Site to the city,” said Bradley Angel, executive director of [Greenaction](#), an environmental nonprofit that’s closely monitored the shipyard cleanup.

“We should be even further outraged that city, state, and federal government agencies said for years that they had verified the adequacy of the cleanup work at the Shipyard

when we now know massive fraud took place,” he added. “Why did government agencies keep saying that everything was fine even after they knew that fraud had occurred?”

Four presidents and five mayors have come and gone since the redevelopment process began at the shipyard, a fist-shaped peninsula in the city’s southeastern corner. From World War II until its closure in 1974, [Hunters Point Naval Shipyard](#) was a key Cold War-era military installation and an irreplaceable source of jobs for the surrounding neighborhood, which is heavily African-American.

Through the changing administrations and even into the widening alleged fraud scandal, local and federal elected officials and authorities have stayed on message. No land at the shipyard—where the Navy ran a nuclear warfare research lab and dumped radioactive material into landfills, the bay, and down storm drains—would be transferred unless it was guaranteed to be clean, they vowed.

## **“Exposure to these radioactive elements can lead to serious health complications, including cancer.”**

“San Francisco will not accept the transfer of any land until federal and state regulators are satisfied that the land is clean and safe, and our own Department of Public Health validates that decision,” wrote then-Mayor [Ed Lee](#) and Supervisor Malia Cohen in a September 2016 letter to then-EPA Administrator Gina McCarthy.

The shipyard is divided into alphanumeric parcels. Lee and Cohen sent the letter almost exactly one year after San Francisco accepted two parcels called UC-1 and UC-2, for “utility corridor.”

About seven acres in size total, the parcels are down the hill from the area where developer FivePoint has built and sold about 300 occupied housing units.

According to Navy documents, toxic threats there stemmed from storm and sewer lines, down which the Navy would routinely flush waste from tests. Potential contaminants included cesium, strontium, thorium, cobalt, plutonium, radium, and uranium.

Exposure to these radioactive elements can lead to serious health complications, including cancer.

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In 2011, Tetra Tech claimed to have removed 876 cubic yards of soil contaminated with “low-level radioactive waste” that was later shipped off-site. The company then say they installed a “hard cap” consisting of soil and asphalt to keep in place other existing contaminants, including potentially toxic vapors from the soil.

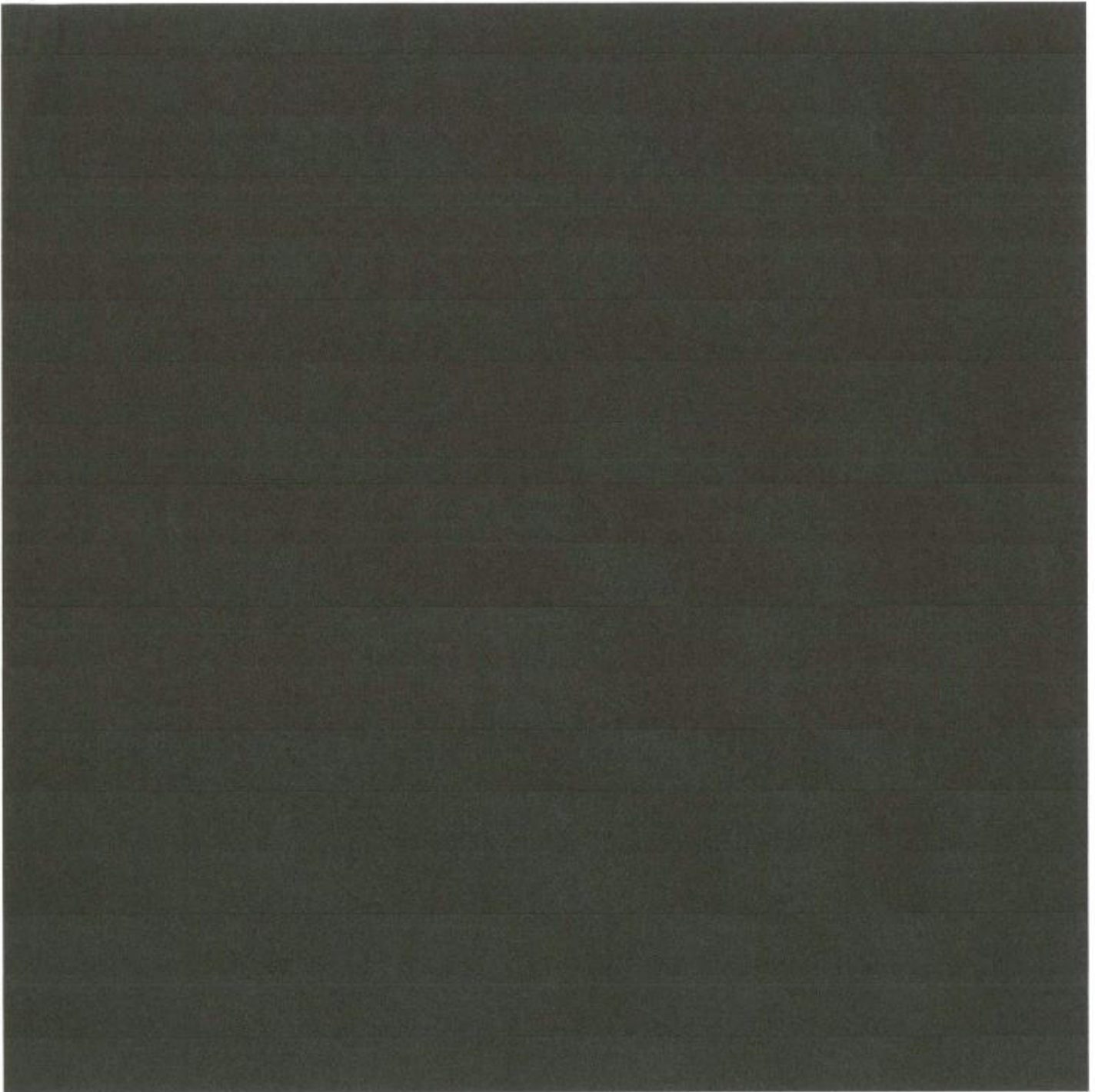
Based on these claims, in 2015, the Navy offered the land for transfer to the city. After the transfer, a local contractor, [Albion Partners](#), was hired to perform minor repair work at the sites, including pothole repair and some fixes to the “hard cap,” which was cracking in places and had been disturbed by “burrowing animals,” according to a work plan filed with regulators.

Beginning in 2012 and through 2014, former workers and contractors made multiple allegations of fraud at the shipyard, allegations made publicly in television news reports. Despite these allegations, the land transfer continued—and Tetra Tech kept winning contracts.

In 2014, the Navy awarded the company a pair of contracts “totaling \$7.5 million” for more shipyard work, according to [NBC Bay Area](#).

At that time, environmental regulators—including the EPA and state Department of Toxic Substances Control—were queried about the fraud allegations by NBC but declined to comment.





Construction of The Shipyard, 2014. | Photo by [Todd Lappin](#)

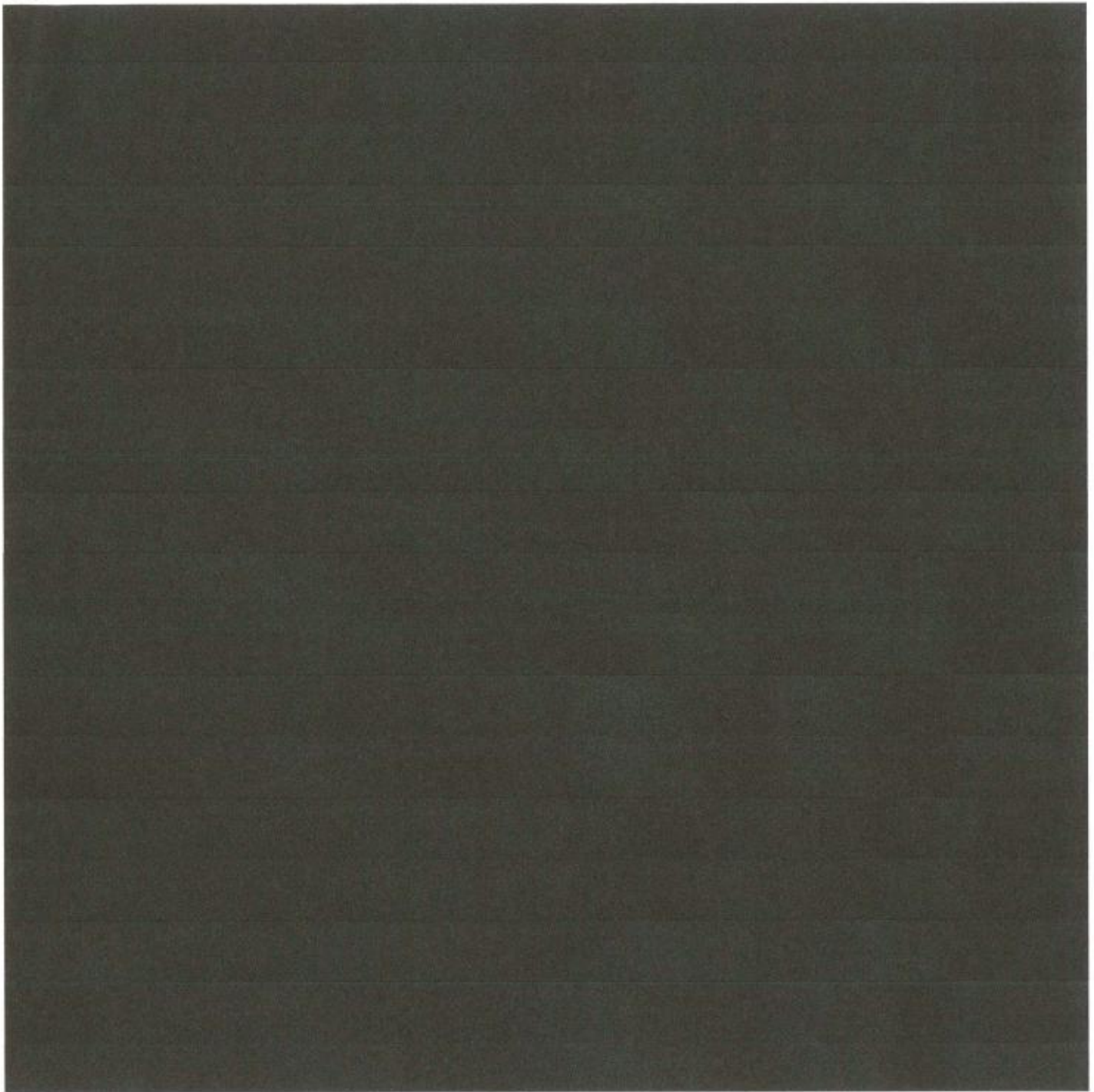
Last summer and fall, third-party contractors hired by the Navy to review Tetra Tech's data found widespread evidence of possible "falsification and data manipulation" throughout the shipyard, according to a draft report for their findings, including at the two UC parcels. At one—UC-2—potential fraud was found with 75 percent of Tetra Tech's work.

“[L]ocations with potentially elevated radionuclide concentrations are likely still present” at both sites, according to the Navy’s data review.

In an emailed statement provided to Curbed SF, the Navy did not offer an explanation for the apparent breakdown in its process.

“The Navy will continue to work with City of San Francisco and regulatory agencies to validate any potentially falsified radiological data and take appropriate action, if necessary, to ensure the property is ready for redevelopment,” the statement said. “The investigation will gather new soil samples and building survey data to ensure parcels are ready for transfer, and or development by the City of San Francisco.”

The EPA would not say directly what risks may be posed by any potential contamination remaining on-site. Nor did it directly account for how potentially contaminated land evaded its oversight.



The Hunters Point Naval dry dock in 1945. | Photo by Associated Press

In an e-mailed statement, Michele Huitric, a spokeswoman for the EPA, said that the agency “is still investigating the impacts of Tetra Tech EC Inc.’s failure to follow the cleanup work plan at Hunters Point Naval Shipyard,” but believes that the cleanup poses no threat, despite the questionable work.

“Our focus is on ensuring both that no current workers or residents are exposed to hazardous materials and that future residents and workers are protected,” she added.



“We believe that current procedures and protocols will protect current workers and residents, and we are working with the Navy and the state of California on plans to ensure that any radiological contamination that may remain on-site is cleaned up to the standards set in the cleanup decision documents.”

In an e-mailed statement, a spokesman for the city’s Office of Community Investment and Infrastructure, which is overseeing the shipyard project, steered responsibility towards the Navy.

“The city has not and will not accept property until it is determined to be suitable for its intended uses,” wrote Maximilian Barnes, an OCII project associate. That’s a small but significant pivot from the language used in 2016 by Lee, who declared the city would not accept land that wasn’t guaranteed “clean and safe.”

#### RELATED

**Almost half of toxic cleanup at Hunters Point Shipyard is questionable or faked, according to initial review**

**Navy: Do-over of \$250 million cleanup at Hunters Point necessary**

Barnes noted that the EPA and Navy declared the land safe to be used as a road, parking area, and storage, he noted, adding “[t]he issue of the questionable data was raised after transfer.” In response to further questions regarding the process, Barnes advised Curbed SF to “kindly direct your questions” to the EPA and Navy.

For environmental watchdogs, regulatory oversight at the shipyard is an exercise in doublespeak, evasion, and—ultimately—concerted negligence.

“They [the city] say they will not accept land that is not clean, but then say they have land they now suspect is not clean,” said David Anton, the environmental attorney representing the whistleblowers. “They should have the Navy take it back until it is clean.”

“And what happens if they can never get it clean?” he asked. “That’s possible.” ■

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## Office of Community Investment and Infrastructure (OCII) Hunters Point Shipyard and Candlestick Point Project



The Office of Community Investment and Infrastructure (OCII) oversees the implementation of development at Hunters Point Shipyard and Candlestick Point, which comprises nearly 780 acres of underutilized land that is being transformed into productive areas for jobs, parks and housing, including affordable housing. A public-private partnership between OCII and FivePoint, formerly Lennar Urban, the project is being constructed in phases over the next 15 to 20 years.

### Specifically, the Project will generate long sought after public benefits by:

- Improving and creating hundreds of acres of public parks and open space, particularly along the waterfront
- Significantly increasing the quality and quantity of affordable housing in southeastern San Francisco, including the complete rebuilding of the Alice Griffith Housing Development, also known as Double Rock
- Providing thousands of commercial and construction job opportunities for San Francisco residents and businesses, especially in the Bayview Hunters Point community
- Supporting the creation of permanent space on the Shipyard for artists
- Providing transportation improvements that will benefit all of southeastern San Francisco
- Attracting and sustaining neighborhood-serving retail and cultural amenities and services



Office of  
Community Investment  
and Infrastructure

One S. Van Ness Ave., 5th Floor, San Francisco, CA 94103

415 749 2400

[www.sfooci.org](http://www.sfooci.org)

2/7/2017

## Office of Community Investment and Infrastructure (OCII) Hunters Point Shipyard and Candlestick Point Project

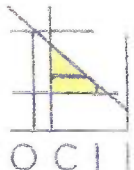
Over the expected 15-20 year phased build out, the Project will include:

- 12,100 residential units, approximately 30% of which will be offered at below-market rates
- More than 350 acres of new and improved public parks, recreational fields, open spaces and waterfront trails and plazas
- 885,000 square feet of regional and neighborhood-serving retail space
- 255,000 square feet of new and renovated replacement space for the Shipyard artists
- More than 3 million square feet of commercial, research and development, and office space
- New public and community facilities on the Shipyard and Candlestick Point



**Meeting Information:** Project information and updates are regularly shared with the Commission on Community Investment and Infrastructure and the Mayor's Hunters Point Shipyard Citizens Advisory Committee (CAC) and its subcommittees. The Legacy Foundation for Bayview Hunters Point also meets regularly to discuss the programming and use of community benefit funds.

| MEETING                        | DAY & TIME   | LOCATION                         |
|--------------------------------|--|----------------------------------|
| OCII Commission                | 1 <sup>st</sup> & 3 <sup>rd</sup> Tuesdays at 1 pm | City Hall, Room 416              |
| Legacy Foundation              | 2 <sup>nd</sup> Mondays at 5 pm                    | OCII Site Office: 451 Galvez Ave |
| Full CAC (subcommittees below) | 2 <sup>nd</sup> Mondays at 6 pm                    |                                  |
| Planning Development & Finance | 2 <sup>nd</sup> Thursdays at 6 pm                  |                                  |
| Business & Employment          | 3 <sup>rd</sup> Thursdays at 6 pm                  |                                  |
| Housing                        | 3 <sup>rd</sup> Thursdays at 7 pm                  |                                  |
| Environmental & Reuse          | 4 <sup>th</sup> Mondays at 6pm                     |                                  |
| Executive                      | 4 <sup>th</sup> Mondays at 7pm                     |                                  |



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Community Investment and Infrastructure

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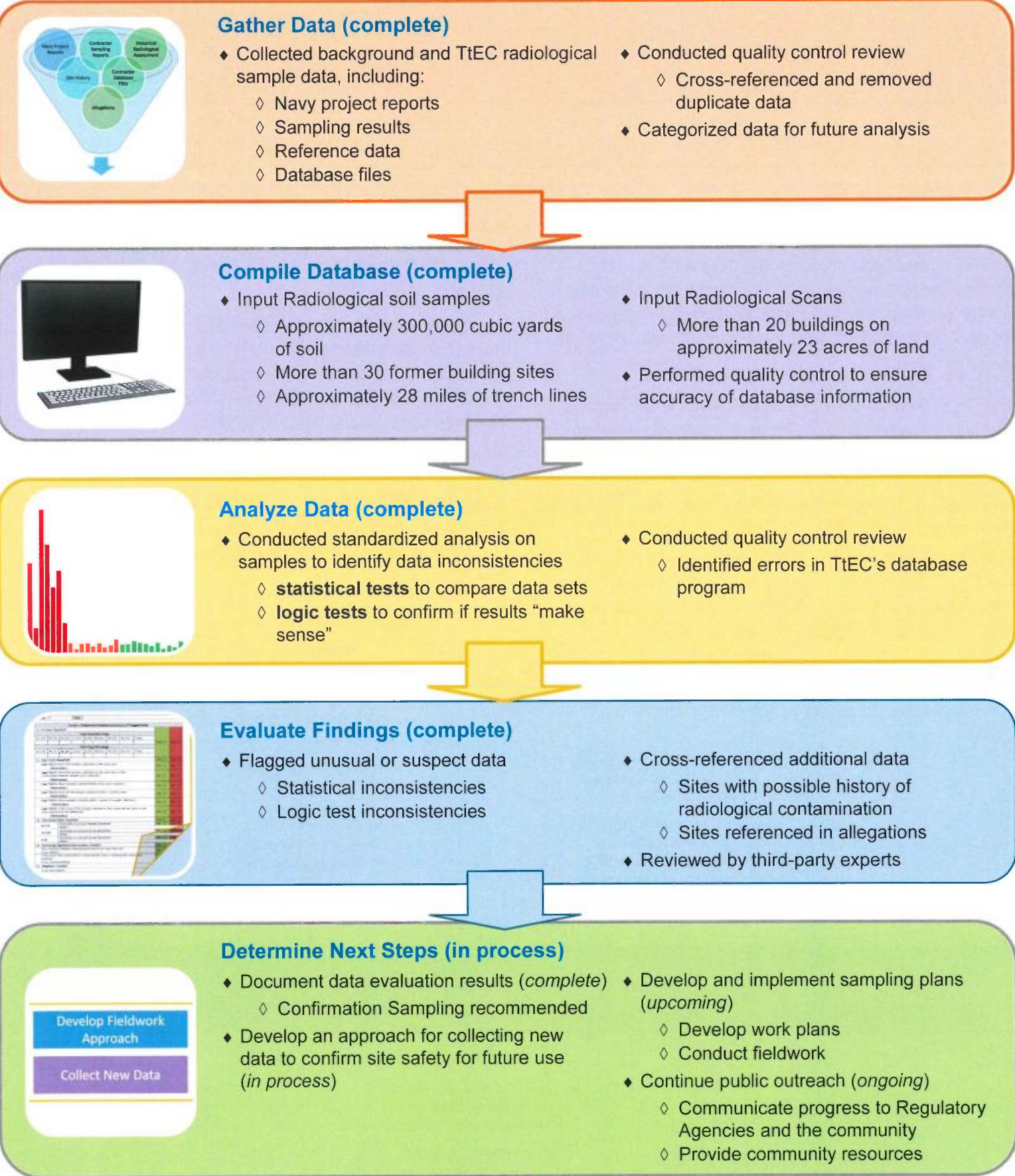
[www.sfoicii.org](http://www.sfoicii.org)

2/7/2017



Radiological Data Review: Process

A thorough and comprehensive data review is underway on the TtEC radiological data samples, as described below:



Para más información sobre el programa de limpieza de la Marina en Hunters Point Naval Shipyard, favor de dejar un mensaje en (833) 202-5888.

有关海军在猎人角海军造船厂的清理活动方案的更多信息 请拨打 (833) 350-6222 并留言。

Radiological Data Review: Results

After reviewing more than 900,000 analytical results, evidence of data falsification was found in additional locations not previously identified. Past laboratory data quality and sample procedures complicate information already in question.

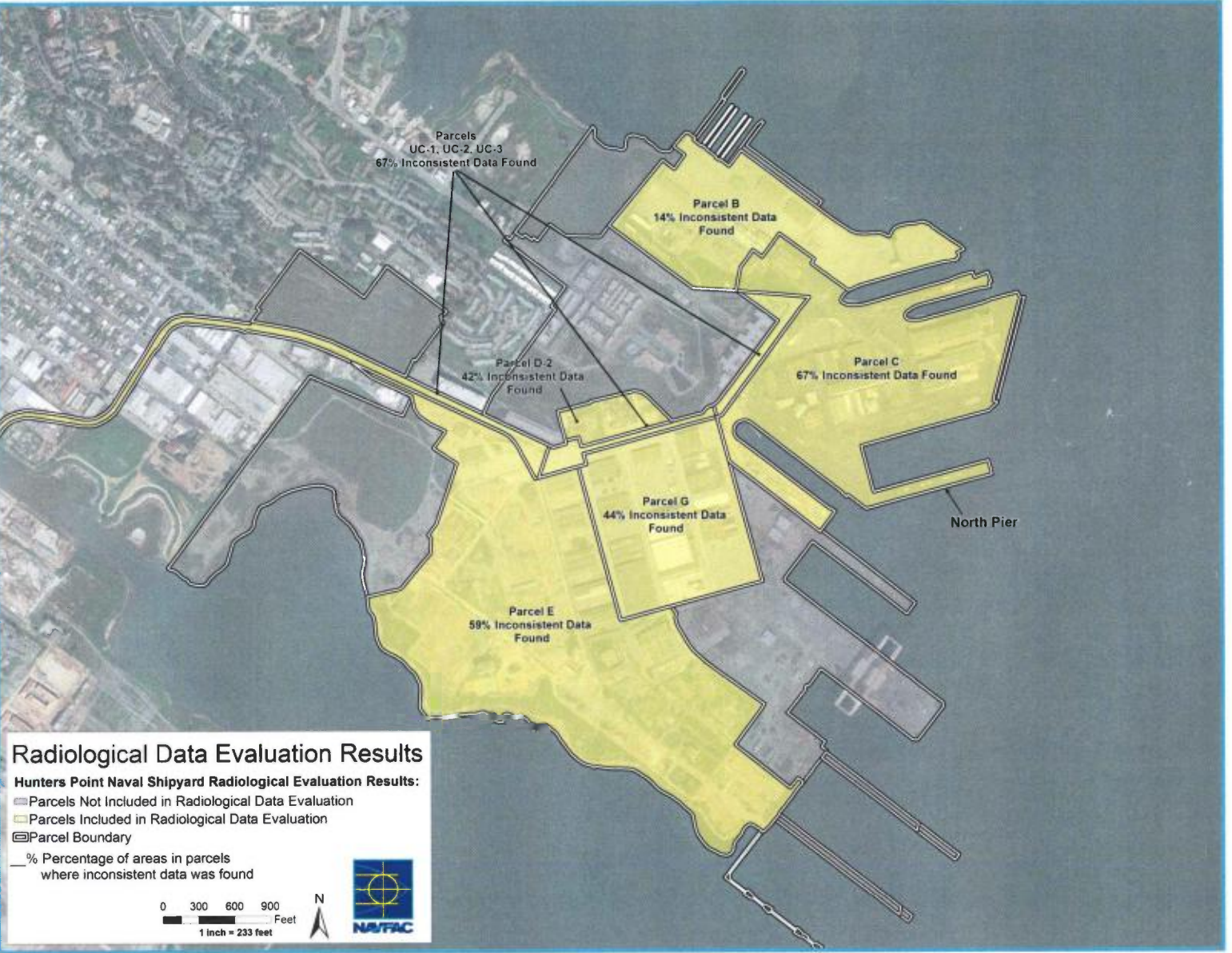
Potentially Falsified Data Identified

The percentages in the map below represent inconsistent data that was potentially falsified. Other data issues were identified and have created uncertainty for all radiological

data collected by TtEC. Due to the lack of confidence in these results, the Navy will collect new data to ensure the site is safe.

Next Steps

Next steps are currently under development and include additional sampling to confirm that the parcels are safe for planned reuse before transferring the property to the City of San Francisco.



Note: For visual purposes, the utility corridors (UC) have been combined.  
Individual UC results for areas where inconsistent data was found include: UC-1: 75% UC-2: 75% UC-3: 60%

**Fact Check:**  
The Navy has evaluated approximately 70,000 samples and more than 900,000 analytical results.



Public Health and Safety

The Navy's top priority is its commitment to public health and safety. The Navy has taken several actions since the falsification of data was reported. The ongoing evaluation of TtEC radiological data samples is one way that the Navy is ensuring public safety.

The Navy will take action on the recommendation to re-evaluate sample areas that have been flagged as inconsistent or questionable.

Independent Verification and Oversight

The U.S. EPA is conducting independent review of the HPNS radiological findings to validate the Navy's results. In addition, Oak Ridge Associated Universities (ORAU) and Argonne National Lab are working with Oregon State University's (OSU) Radioecology Research Group and provide independent third party review of data.

For more information, visit the HPNS web pages at [www.bracpmo.navy.mil/hpnsrc](http://www.bracpmo.navy.mil/hpnsrc).

Protecting the Public

Throughout the environmental cleanup process, the Navy follows an established set of procedures to protect the public. If a threat to the public exists, immediate action is taken.

The Navy and regulatory agencies have sufficient data for HPNS to determine that there is no immediate threat to public safety, allowing the Navy the time to conduct this comprehensive data review.

The Navy's priority is community safety during all investigation and cleanup activities at HPNS. Comprehensive safety procedures, including dust control, air monitoring, and management of soil samples, ensure the safety of cleanup workers, shipyard tenants, and nearby residents.

The Navy's goal for this process is to verify that the parcels are safe for planned reuse before transferring the property to the City of San Francisco.



FACT SHEET  
Hunters Point Naval Shipyard  
Radiological Data Review Update #3



January 2018

This is the third in a series of fact sheets and other ongoing communications about the radiological data review being conducted at Hunters Point Naval Shipyard (HPNS). Previous fact sheets and information on the topic may be found on the Navy's website at [www.bracpmo.navy.mil/hpnsrc](http://www.bracpmo.navy.mil/hpnsrc) or by using the resources listed on page 4 of this fact sheet.

What is Radiation?

Radiation is energy given off by atoms as rays, waves, or particles. It can be in the form of light, sound, or heat. Humans are exposed to some radiation from manmade sources (such as medical X-rays and smoke detectors), as well as from natural sources (such as rocks and the sun).

Why is There Radiation at HPNS?

From 1939 through 1974, the Navy used HPNS for ship repair and maintenance, including the decontamination of ships involved in atomic testing. These activities, along with luminescent deck markers, dials, gauges, and signs which were in common use during this timeframe, resulted in low levels of radioactive contamination at HPNS.

Additionally, from 1948 to 1969, HPNS was home to the Naval Radiological Defense Laboratory (NRDL). NRDL studied the potential hazards of radiation and developed ways to prevent or minimize its harmful effects.

The Navy standards for health and safety during radiological cleanup are more stringent than those from the Nuclear Regulatory Commission and the

United States Environmental Protection Agency (U.S. EPA), both for members of the public and contaminated sites.

Why is the Navy Reviewing Radiological Data at HPNS?

As described in the Navy's September 2017 Radiological Data Review Update #2 Fact Sheet, a thorough data evaluation is underway for radiological samples collected by Tetra Tech EC (TtEC) as a result of the Navy's identification of sampling issues by TtEC in 2012.

The timeline below shows the actions and activities leading up to the discovery of the falsified data, and the Navy's actions since the falsification was reported. The Navy's goal for this process is to confirm that the parcels are safe for planned reuse before transferring the property to the City of San Francisco.

Fact Check:

The Navy's data evaluation includes samples of approximately 22,000 truckloads of soil and 28 miles of sanitary sewer and storm drain lines.

How to Get More Information on HPNS Radiological Data Review



The Navy will continue to update the community on radiological data review results and achievements in upcoming program updates, fact sheets, website updates, and community meetings.

Previously published reports and documentation may be found on the U.S. EPA's website at [www.epa.gov](http://www.epa.gov), on DTSC's website at [www.envirostor.dtsc.ca.gov](http://www.envirostor.dtsc.ca.gov), and on the HPNS Radiological Cleanup Program pages of the Navy's website at [www.bracpmo.navy.mil/hpnsrc](http://www.bracpmo.navy.mil/hpnsrc).

Contact HPNS Program Management

**Derek Robinson**, BRAC Environmental Coordinator  
Dept. of the Navy, BRAC Program Management Office West  
33000 Nixie Way, Bldg. 50, 2nd Deck, San Diego CA 92147  
(619) 524-6026  
[derek.j.robinson1@navy.mil](mailto:derek.j.robinson1@navy.mil)

To be added to the HPNS mailing list or for additional information, email [info@sfhpnsc.com](mailto:info@sfhpnsc.com) or call (415) 295-4742.

Contact the HPNS Community Liaison for Program Information and Resources

**James Bryant**  
1333 Evans Avenue  
San Francisco, CA 94124  
(415) 970-9051  
[community@sfhpnsc.com](mailto:community@sfhpnsc.com)

Visit Mr. Bryant during open office hours on the first Tuesday of each month from 2:00-4:00 p.m., or by appointment.

Review HPNS Reports

**City of San Francisco Main Library**  
100 Larkin Street, 5th Floor, Gov't Information Center  
San Francisco, CA 94102  
(415) 557-4400

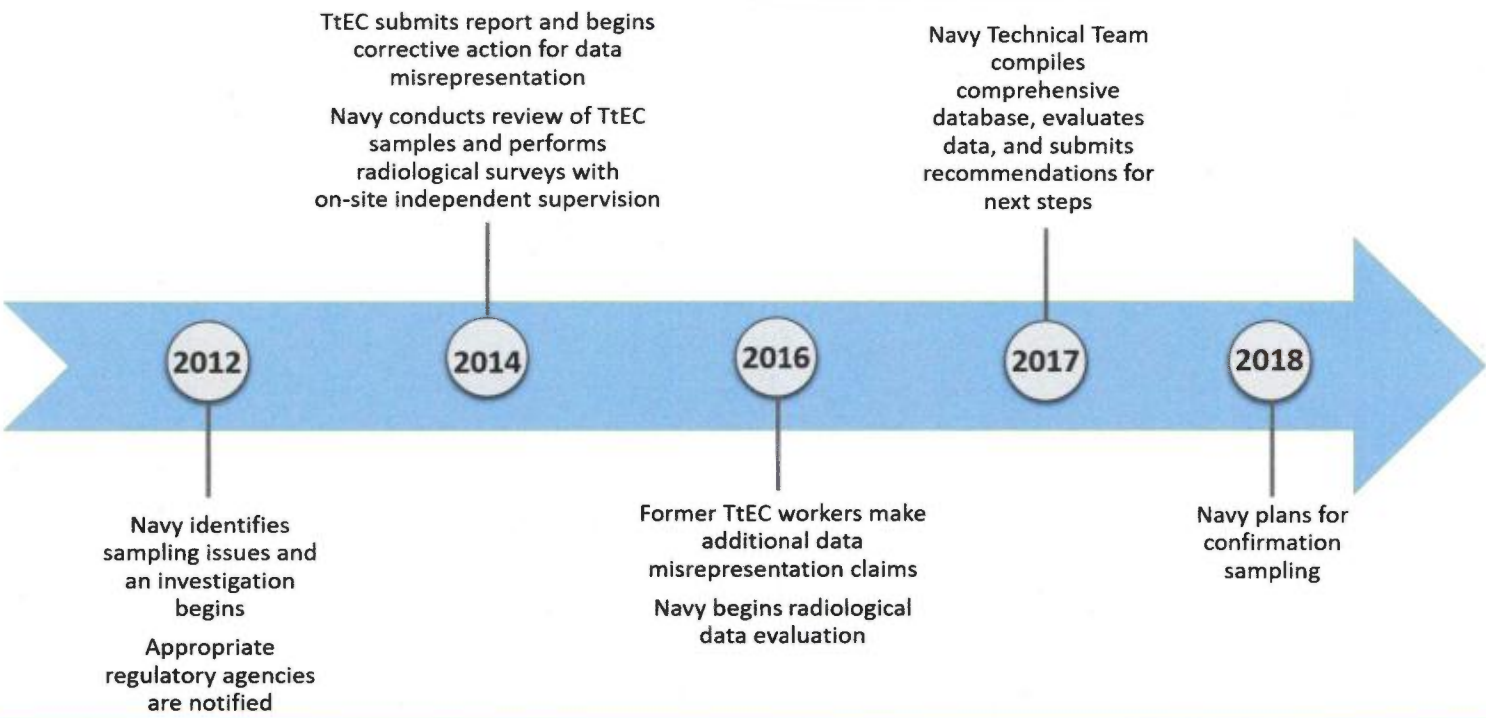
The Shipyard Site Trailer

690 Hudson Avenue, San Francisco, CA 94124

Contact the Radiological Health and Safety Community Technical Advisor with Questions

**Dr. Kathryn Higley**  
Oregon State University  
School of Nuclear Science and Engineering  
(541) 737-7063  
[kathryn.higley@oregonstate.edu](mailto:kathryn.higley@oregonstate.edu)

Call or email Dr. Higley with your questions. Look for announcements for local opportunities to visit with her.







# FACT SHEET Hunters Point Naval Shipyard

## Frequently Asked Questions About the Shipyard Landfill at Parcel E-2

August 2017

### History of Parcel E-2

Parcel E-2 consists of 47 acres in the southwest portion of Hunters Point Naval Shipyard (HPNS) that was created between the 1940's and the 1960's by filling the area along the edges of



Location of Parcel E-2 at HPNS

### What do we know about what's in the landfill?

The Navy has studied the landfill at Parcel E-2 extensively, including the review of historical records and collection of hundreds of samples. Many investigations have been conducted by the Navy, including digging test pits, drilling boreholes to take samples from below the ground, using radiation detectors over the entire surface, and sampling the water from under the landfill. Based on this work, the Navy knows that municipal trash, construction debris, soil, and shipyard industrial waste were buried in the landfill. The Navy took more than 300 samples of soil within the landfill from soil borings, excavation holes where polychlorinated biphenyls (PCBs) were being removed, groundwater monitoring wells, and test pits. The sample results showed low levels of contamination, of which most were within United States Environmental Protection Agency's (USEPA's) acceptable risk range. Lead, PCBs and chemicals related to asphalt were the most common contaminants found. The areas with the highest levels of contamination were excavated and removed from the site.

### Who is making decisions about the landfill?

The Comprehensive Environmental Response Liability Compensation Act (CERCLA) of 1980 is a federal law that established a process for environmental cleanup at contaminated sites, including HPNS. In accordance with CERCLA requirements, the landfill project at HPNS involves the Navy, USEPA, California Department of Toxic Substances Control (DTSC), the San Francisco Regional Water Quality Control Board, and other regulatory agencies. The Navy worked closely with the environmental regulators and the City of San Francisco during the development of the proposed cleanup solution for the landfill, which was outlined in the Navy's 2011 Proposed Plan (PP). After a public comment period and regulatory review,

the San Francisco Bay with artificial fill. Parcel E-2 includes a 22-acre landfill for the historic disposal of construction debris, municipal-type trash, and a variety of industrial wastes.



Detail of areas that make up Parcel E-2

this remedy was documented in the November 2012 Final Record of Decision (ROD), a public document that describes the selected remedy for the cleanup of a site that has been agreed upon by the Navy and the regulators. The community was engaged throughout the PP and ROD process, and their concerns and feedback were taken into account when choosing the selected remedy.

### Is there radioactive waste in the landfill?

The Navy has found glow-in-the-dark dials and markers during several excavations. These devices were painted with radium, which is a radioactive material that is no longer used. The Navy has excavated the two areas most likely to have such devices; there may be more buried throughout the landfill.

The radiation levels from these devices are low and do not pose a risk to human health or the environment if they remain underground.

### What about dust and risk?

As soil is moved, there is a risk of releasing dust that has chemicals and asbestos (which occur naturally in the Hunters Point environment). The Navy follows an approved dust control plan that prevents public exposure to dust during earth-moving activities. Measures include containing soil to prevent contaminated dust from getting into the air; covering the beds of all truck carrying soil on or off HPNS; washing and/or brushing off truck

wheels before leaving HPNS; continuous watering down of any areas where soil is being moved to prevent dust from blowing; and regularly monitoring the air around all of the active cleanup areas. To date, air monitoring test results show no risk to the members of the surrounding community, tenants, or the workers at HPNS. Air monitoring results can be found on the Navy's website at [www.bracpmo.navy.mil](http://www.bracpmo.navy.mil) and on the DTSC website at [www.envirostor.ca.dtsc.gov](http://www.envirostor.ca.dtsc.gov).

### What cleanup actions have been completed at the landfill and in Parcel E-2?

Cleanup actions completed at the landfill and within Parcel E-2 include:

- Installed a sheet-pile wall (below-ground barrier) and a groundwater extraction system in the southeast portion of Parcel E-2 in 1998 to keep PCBs from moving towards the Bay—the system operated until 2005 when the Navy excavated (44,500 cubic yards) and removed the source of contamination
- Installed a cap, made up of a multi-layer protective liner system covered by two feet of clean soil, in order to manage the penetration of water and the release of gases, over 14.5 acres of the landfill in 2000
- Installed a landfill gas control and extraction system in 2002 (see below for more information on landfill gases)
- Removed debris from the shoreline in 2003, including 81 tons of metal sent to a recycler, 52 dump trucks of non-metal debris, 344 tires and 10 cubic yards of material containing asbestos

*continued on page 2*



- Removed 8,200 cubic yards of contaminated soil and sediment from the Metal Slag Area in 2005-2006
- From 2010-2012, removed another 40,000 cubic yards of contaminated soil that was remaining after the initial PCB removal action in 2005
- In 2012, removed 3,800 cubic yards of soil and screened it for radiological contamination
- An additional 39,000 cubic yards of impacted soil was removed from hot spot excavations and an underground barrier made up of wet clay mixed with soil and cement in large trenches (known as a slurry wall) was installed along the shoreline in 2016-2017 to limit the flow of groundwater between areas

### What about gases from the landfill?

The primary gases from landfills (including Hunters' Point landfill) are methane and carbon dioxide as a result of rotting material. Neither of these gases are toxic, however methane must be controlled because it is flammable. In addition, there are small amounts of other gases present, called non-methane organic compounds.

The Navy installed an engineered cap over the landfill in 2000 to trap the gases, which are then sent through a carbon filter that removes the non-methane organic compounds before venting them to the atmosphere. Navy tests have shown the gases do not pose a risk to the community. The Navy is planning for the installation of a new multi-layer cap and an upgraded methane collection system in late 2018.

### What about earthquakes and liquefaction?

The Navy has done geotechnical testing of the landfill area and found a low likelihood for major soil movement, called liquefaction. Liquefaction and earthquake-related effects are well understood in California. CERCLA, the federal law regulating cleanup at HPNS, requires an evaluation of nine criteria, including short and long-term protectiveness for any remedy proposed. The remedy chosen has been designed to be protective during and after an earthquake. Technologies used

to implement the landfill remedy (currently under construction) were designed and will be constructed with this in mind.

### Can contaminants move into the San Francisco Bay?

The Navy has completed a large amount of sampling in the San Francisco Bay and did find PCBs at low levels in sediment near the landfill and the mouth of Yosemite Slough, a result of historical use by private companies located or operated near Yosemite Slough and the PCB Hotspot Area along the shoreline of the landfill. The Navy has removed the PCB Hotspot Area and is currently evaluating methods to dredge or clean contaminated sediments near Yosemite Slough and the landfill.

The Navy has thoroughly sampled groundwater flowing underneath the landfill and has not found any groundwater plumes with contamination migrating towards the San Francisco Bay. Installation of slurry and sheet-pile walls, as well as the construction of rock walls (revetments) built along Parcel E-2's shoreline will prevent human exposure to contaminated soil or sediment and prevent erosion of the soil cover, protective liner, and underground barriers into the San Francisco Bay.

### What if there is a rise in sea level?

All Navy remedies at HPNS, including those proposed for the landfill, are designed to withstand potential sea level rise. The landfill remedy revetments and elevations will account for significant sea level rise.

### Will the landfill be safe for future use?

The remedy at the landfill includes an engineered cap, soil cover, and a protective rock wall (revetment) along the shoreline. The remedy, as summarized in the Navy's Proposed Plan (available on the Navy's website at [www.bracpmo.navy.mil](http://www.bracpmo.navy.mil)), has removed access to any possible contamination left beneath the ground. This action protects humans and the environment for future alternative use of the landfill site. The current projected future use of the landfill and immediately surrounding area is open space, including a park and Bay Trail.

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## Where can I get more information about the landfill and Parcel E-2 cleanup at HPNS?

There are several ways to learn more about the Navy's cleanup at HPNS.

### Review an HPNS Report

#### City of San Francisco Main Library

100 Larkin Street, 5th Floor, Gov't Information Center  
San Francisco, CA 94102 (415) 557-4400

#### Hunters Point Naval Shipyard Site Trailer

690 Hudson Avenue, San Francisco, CA 94124

**Navy Website:** [www.bracpmo.navy.mil](http://www.bracpmo.navy.mil)

*There is a link to the online HPNS Administrative Record on the Documents Page of the Navy's HPNS web pages*

### Contact HPNS Program Management

**Derek Robinson**, BRAC Environmental Coordinator  
Dept of the Navy, BRAC Program Management Office West  
33000 Nixie Way, Bldg. 50, 2nd Deck, San Diego CA 92147  
(619) 524-6026 [derek.j.robinson1@navy.mil](mailto:derek.j.robinson1@navy.mil)

*To be added to the HPNS mailing list or for additional information, email [info@sfhpn.com](mailto:info@sfhpn.com) or call (415) 295-4742*

### Contact the Radiological Health and Safety Community Technical Advisor with Questions

**Dr. Kathryn Higley**  
(541) 737-7063

[kathryn.higley@oregonstate.edu](mailto:kathryn.higley@oregonstate.edu)  
[www.ne.oregonstate.edu/kathryn-higley](http://www.ne.oregonstate.edu/kathryn-higley)

Dr. Higley is the Head of the School of Nuclear Science and Engineering at Oregon State University and is a Certified Health Physicist with a Ph.D. and M.S. in Radiological Health Sciences. She is available to answer community member questions by phone or email.

有关海军在猎人角海军造船厂的清理活动方案的更多信息，  
请拨打 (415) 295-4742 并留言。

Para más información sobre el programa de limpieza de la Marina en Hunters Point Naval Shipyard, favor de dejar un mensaje en (415) 295-4742.

Para sa higit pang impormasyon sa programa sa paglilinis ng Navy sa Hunters Point Naval Shipyard, mangyaring mag-iwan ng mensahe sa (415) 295-4742.

Mo nisi faamatalaga e uiga I le polokalame faamama a le Navy I Hunter's Point, faamolemole tuu mai se feau I le telefoni (415) 295-4742.



March 21, 2018

Dear President Hillis and Members of the Planning Commission,

I am a first-generation Mexican-American flower farmer and owner of Brothers Floral in the San Francisco Flower Mart. I began my career working at a Sakai Brothers rose farm in Richmond, CA when I was 18 years old and worked my way up through the company, from delivering roses to the flower market to working on the sales floor. In 1997, I purchased the business from Sakai and opened my own store in the San Francisco Flower Mart as Brothers Floral along with my two brothers. We began by specializing in Ecuadorian roses and have since expanded into a variety of locally grown cut flowers and greens.

Owning my business in the Flower Mart for the past 21 years, I have seen the current buildings at 6<sup>th</sup> and Brannan get more and more outdated. Right now, our buildings are not nearly strong enough to survive the next earthquake and can barely even survive a rainstorm without being completely flooded. As the largest wholesale flower market in the USA that serves the Bay Area and beyond, we need a modern facility that will last us into the future. It is important that we are able to serve our customers with more efficient systems and a better experience overall so that this remains the best wholesale flower market in the USA. I am in full support of Kilroy building us a new state-of-the-art flower market at 6<sup>th</sup> and Brannan, as we really need it to remain relevant for many years to come.

I also understand that in order for us to get a brand new flower market at the current location, we will need to move temporarily during construction. I am in full support of a temporary move to Piers 19, 19 ½, and 23. It is very important for a temporary market to have a large enough space for all tenants to stay together, loading areas, and adjacent parking for our customers. It also needs to be centrally located to serve all of our customers from all over the Bay Area. The Piers satisfies all of those requirements, and even has more parking and loading space than we have today. I also think the Piers location will help get our customers excited about the new market that will be built at 6<sup>th</sup> and Brannan. Even though we are a wholesale operation, I think it will be an advantage to the whole floral industry if the Flower Mart is at the Piers, because the visibility can make more people excited about flowers.

Please do not delay in showing your support for both the plan to build a new Flower Mart at 6<sup>th</sup> and Brannan Streets, as well as the plan for a temporary Flower Mart at Piers 19, 19 ½, and 23. Both projects are critical for the success of my business and the future success of the San Francisco Flower Mart.

Thank you,

A handwritten signature in black ink, appearing to read 'Rigoberto Gonzalez', with a stylized flourish extending from the end.

Rigoberto Gonzalez

Brothers Floral  
San Francisco Flower Mart, Stall #72  
San Francisco, CA, 94107  
(415) 896-5532

March 14, 2018

Dear President Hillis and Members of the Planning Commission,

As someone who has been in the industry for 30 years and watched the landscape change I can't help but want to be prepared for the future. The Flower Mart has been in San Francisco for over 100 years and has been in the same buildings for many decades, but it is now severely outdated and is not a place that our businesses will be able to survive into the future. Like other world-class cities, San Francisco deserves a modern, state-of-the-art wholesale flower market that can continue to serve the entire Bay Area for the next 100 years. For that reason, I am fully in support of Kilroy Realty building a brand new market for us at our current site at 6th and Brannan Streets.

I also understand that in order for us to get the new market that we need, the Flower Mart needs to move to a temporary location during construction. I believe that in order for our businesses to thrive, the most important aspect of finding an acceptable temporary location is that it accommodates everyone currently at the Flower Mart. There is a cross pollination of our customers so it is crucial that we stay together. On that note, I also know how important it is that any temporary site be accessible to my customers. Based on the plans, studies, and presentations that I have seen, I believe the location at Piers 19, 19 ½ and 23 satisfies these needs. The proposed layout of the Piers has enough square footage for all Flower Mart tenants to remain in business, provides more parking for our customers than our existing location, and is centrally located for our customers that come to us from all over the Bay Area.

As we move into the future with new Flower Mart and the possibly even the creation of a "Flower District" in SoMa, I see an opportunity become more visible and mainstream. I think having the temporary Flower Mart at the Piers could add excitement to the opening of the new, permanent Flower Mart at 6<sup>th</sup> and Brannan, which is currently not well-known to people outside of the floral industry.

Please do not delay in showing your support for both the plan to build a new Flower Mart at 6th and Brannan Streets, as well as the plan for a temporary Flower Mart at Piers 19, 19 1/2 and 23 during construction. Both projects are critical for the success of my business and the future success of the San Francisco Flower Mart.

Thank you for your time,

Charlie Cheng



Pin Nursery  
San Francisco Flower Mart, Stall #79  
San Francisco, CA, 94107  
(408) 710-9338



March 14, 2018

Dear President Hillis and Members of the Planning Commission,

As a South American fresh cut flowers distributor & wholesaler, we're a small team of two women that have been servicing the San Francisco Flower Mart since the late 1980's and are excited to be part of the re-development for a new market at the same spot in SoMa.

San Francisco is rapidly changing its urban scene and building high rises to continue to grow in this great city, yet still juxtaposes against its founding, iconic architecture, ranging from Beaux-Arts, Victorian and Art Deco to contemporary. As this city evolves, as individuals we also adapt to its growth, while maintaining our sole work purpose, to service the city with cut flowers and at the legendary site.

We're optimistic to face the changes and work at a temporary place during the construction phase and before moving back to the original location. The ideal set up is that all the flower vendors stay together in same place, and united we help one another and hence all of our customers continue to visit and support all of the great vendors. It may be challenging at first, as it is with any move, yet we're confident that the temporary site will be a positive transition.

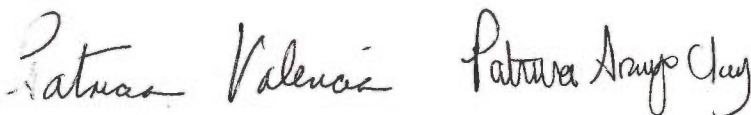
Pier 19, 19 ½ and 23 shall be a great temporary venue, centrally located on the Embarcadero- a few blocks away from the Ferry Building and away from the opposite busy touristy side. It is situated at the historic piers - the "port city" where big boats were once built and worked on, and it will be a similar warehouse environment to the wholesale flower industry. We feel it will be a good fit for working with cut flowers and servicing local vendors, customers, and visitors alike.

Kilroy will do their very best to accommodate each vendor's needs during each phase of the move and while in its physical space. They are eager to always help the Flower Mart and enhance its potential until we get to its ultimate place, a new modern market in the current location for the city and its people.

Please do not delay in showing your support for both the plans to build a permanent new Flower Mart at 6th and Brannan Street, as well as the plans for a temporary Flower Mart at Piers 19, 19 ½ and 23 during construction. Both projects are critical for the success of my business and the future success of the San Francisco Flower Mart.

Many thanks,

Patricia Valencia & Patricia Araujo Clay

The block contains two handwritten signatures in black ink. The first signature, on the left, is 'Patricia Valencia' and the second, on the right, is 'Patricia Araujo Clay'. Both are written in a cursive, flowing style.

Sunshine International  
San Francisco Flower Mart, 644 Brannan Street, Stall # 39  
San Francisco, CA, 94107  
(415) 513-5614

March 14, 2018

Dear President Hillis and Members of the Planning Commission,

My name is Carlos Ortega. I was born in Mexico and came to the US when I was 12 years old. I started to work part time at a rose growing nursery, while at same time going to school. When I went to college, I continued working at the nursery. I also spent 8 years in the National Guard.

Flowers are my life. I started my own business producing roses at the age of 21. In 2000, I closed the business and came to work in the San Francisco Flower Mart. After working in the market for 11 years, I was able to start my own business within the Flower Mart, and for the past 6 years, I have had my own stall – Agave Flowers.

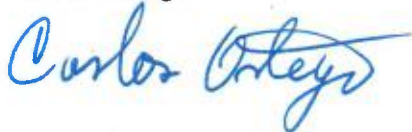
It is very exciting that the Flower Mart will be rebuilt. The current buildings are very old and worn down and will not be able to house the Flower Mart much longer, especially if there is an earthquake. While moving to a temporary location is not the best situation, I understand that it is necessary in order to get a new market at the same location.

Looking for a temporary place has been a challenge, but I feel that Piers 19, 19 ½ and 23 are the best choice and an exciting one. I have seen the plans and visited the site and think the Piers will work very well for my business and my customers as a temporary flower market. I feel that the transition will not be easy for all of us, but I am in favor of this choice, because it is important that we get a new market at 6<sup>th</sup> and Brannan. I plan to stay in business for a long time in the Flower Mart – for all my life.

Please show your support for the plan to build a permanent new Flower Mart at 6th and Brannan Streets, as well as the plan for a temporary Flower Mart at Piers 19, 19 ½ and 23 during construction. Both projects are essential for the success of my business and the future success of the San Francisco Flower Mart.

Best Wishes,

Carlos Ortega



Agave Flowers  
San Francisco Flower Mart, 644 Brannan Street, Stall #27  
San Francisco, CA, 94107  
(415) 957-0214



March 13, 2018

Dear President Hillis and Members of the Planning Commission,

I am a broker-grower of roses in Ecuador and have been in business at the San Francisco Flower Mart for twenty-five years as owner of Americana Flower Brokers. The current buildings at 6<sup>th</sup> and Brannan Streets have housed the Flower Mart for many decades, but they are severely lacking in the services needed to sustain a successful Flower Mart for decades to come. Today, my employees and I suffer through cold, dark, and outdated working conditions at the market. The San Francisco Flower Mart is the largest wholesale flower market in the USA and serves the needs of the entire Bay Area. We need to be more innovative and we deserve a world-class Flower Mart that will allow us to thrive into the future. I want my company to stay in business at the Flower Mart for generations and, therefore, we are in complete support of Kilroy Realty building a brand new facility for us at our current site at 6<sup>th</sup> and Brannan Street.

I am also in full support of Kilroy's proposal to temporarily move the Flower Mart to Piers 19, 19 ½, and 23 during the construction of our permanent new home. While moving to any temporary location isn't ideal, I understand that it is necessary for the long term success of the Flower Mart and I think that a temporary move to the Piers is a good thing. As a business owner, I am very aware of how important it is that any temporary site be in a good location that is accessible for my customers. I believe the location will work very well for my customers and my fellow Flower Mart tenants. From taking a tour of the Piers and studying the proposed layout of the temporary Flower Mart there, I can see that has enough space for all Flower Mart tenants to remain in business, and will provide better parking and loading for our customers than we have at the current location.

Please do not delay in showing your support for both the plan to build a new Flower Mart at 6<sup>th</sup> and Brannan Streets, as well as the plan for a temporary Flower Mart at Piers 19, 19 ½, and 23. Both projects are critical for the success of my business and the future success of the San Francisco Flower Mart.

Cordially,  
Jon Gomez

Americana Flower Brokers  
San Francisco Flower Mart, Stall #76  
San Francisco, CA, 94107  
(415) 543-2004

March 14, 2018

Dear President Hillis and Members of the Planning Commission,

The San Francisco Flower Mart has a long-standing history. My husband, Randy, and I are passionate about growing flowers and plants and had the concept to start our business in 1996. Finally, a few years ago, I opened my store, Grace Nursery, at the San Francisco Flower Mart in SoMa. Over the last 3 years, I have been expanding and growing and I hope to continue to grow with a new and improved Flower Market. The buildings we have now are old and we need a modern facility in order for our businesses to succeed in this fast-paced environment with all of the changing technologies. Therefore, I am happy to show my support the New Flower Mart Project because I can see the nice future that the San Francisco Flower Mart tenants and customers need.

Since we need a new market and the best location is here at 6<sup>th</sup> and Brannan Streets, I understand that we will need to move to a temporary site during the construction. It is important that all the tenants move together and come back to the new market together when it is complete. I know that any change is difficult and for some people even scary, but I think Piers 19, 19 ½ and 23 will be a great location to have the temporary market and I support it. I have visited the building on the Piers and have reviewed the proposed plan and I can see that it will work well for the tenants and our customers. There is enough space for all of the tenants in the warehouse and the customer parking is even better than the current location. Kilroy and the architects have been working with the tenants to make sure we have what we need at the Piers – for example I have special needs, because most of my flowers are tropical and require warmer temperatures.

I think the location of the Piers will also be really beneficial to promote the market and get people excited about the new, state-of-the-art Flower Mart that Kilroy is building for us. The visibility of the location will also help promote the floral industry in general, so more and younger people can see the market and get interested in having a career as a floral designer, event planner, or other floral jobs.

We really appreciate that Kilroy will build a brand new market for us and really thank them for the hard work. We also thank the City for working so hard on the Central SoMa Plan, so that we can have a modern home to thrive into the future.

Please do not delay in showing your support for the plan to build a new Flower Mart at 6<sup>th</sup> and Brannan Streets, as well as the plan for a temporary market at Piers 19, 19 ½ and 23. Both projects are extremely important for the future success of the San Francisco Flower Mart. We're excited and we're ready for the project to start as soon as possible!

Thank you,

Grace Su

A handwritten signature in dark ink, appearing to read 'Grace Su', with a stylized, flowing script.

Owner, Grace Nursery  
San Francisco Flower Mart, 676 Brannan Street  
San Francisco, CA, 94107  
(415) 371-1228



**From:** Jason C. Braatz  
**To:** [Stoelzle, Alexandra](#)  
**Cc:** [Grisso, Mike](#)  
**Subject:** From Tenant European Wholesale: Very positive outlook to move to temporary site  
**Date:** Thursday, June 8, 2017 3:06:57 AM

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Good morning Alexandra,

I hope this finds you well! I just wanted to reach out to share with you how excited we are, as longtime tenants of the San Francisco Flower Market, to be able to move into such an incredible space you've been able to uncover at Pier's 19, 19.5 and 23!

Our family floral business has been in San Francisco since 1871, and my wife and I have lived in San Francisco as residents with our young daughter. Including our employees, we're a family, so safety and security is tantamount to us with our business. Additionally, we are lucky to have great customers, and like any tenant, we wouldn't want to lose them with a transition. But the Piers would actually enhance both of these factors for us: the Pier areas are more than safe enough for our younger ones but we also hear from our customers directly that this would be an incredible step in the right direction!

We aren't the only tenant who feels this way; we look forward to moving to the temporary location as quick as we are able to do so.

I'd like to elaborate on this point if I may. I am the primary resource and San Francisco & California floristry historian for the renowned California Historical Society & Museum. I've been tasked as the industry's expert by the Executive Director, Anthea M. Hartig, Ph.D., to analyze historical assets pertaining to flowers and floristry in California and San Francisco, as well as to offer suggestions of preservation, value and explain and research the background on our state and city's rich history of farming and marketing flowers. I've been cited in museums, on television, in books, in documentaries and in publications on how floristry has changed in our state and city over the last two centuries.

The San Francisco Flower Market, as an idea, is truly historic. But the buildings we currently occupy are not. The main market was moved from Market Street to its present location during a decade in the 20<sup>th</sup> century when construction wasn't safe, secure or resistant to inclement weather. Instead, our vibrant city is really in need of a new building or set of buildings which meet even the simplest of modern-day safety ideals. Historically, our buildings were part of the warehousing boom in San Francisco; but the goals that the men and women had when building them are far different than their use today. In order for modern floristry and its heritage to survive in San Francisco, one thing we absolutely need – for our business and many other tenants of the flower market – is a modern facility. The move to temporary space on the Piers amplifies our ability to demonstrate this to our customers that we are – within the future – able to offer a modern facility for them to visit us in.

This will keep California and San Francisco's rich history in floristry alive, and the move to the Piers will nicely project that we are all working towards that goal. We're so excited to move to the Piers temporarily, as many customers (florists, caterers, wedding specialists, and event coordinators) have

already said that they will come back to our market at the temporary space; some who haven't been to the current market location in over a decade or more. This makes a big positive economic impact for our business, for the other tenants, and for the City of San Francisco. It also is a big economic win for the state of California and it's growers.

But I do understand that other tenants may be scared of change. I suppose change by itself could be scary, but we've tested this notion (of moving to the temporary site/the Piers) and in my thorough analysis of it, growers' and wholesale floristry won't be around for decades to come without this move to the Piers. We need it.

Thank you so much for understanding our point of view!

Gratefully yours,

Jason Braatz  
Family Owner  
Podesta Baldocchi Flowers  
Rossi & Rovetti Flowers  
European Flower Wholesale, Inc.  
"The Longest Continually Operated Family Flower Business in America, Since 1871"  
415-200-5500



Received at CPC Hearing 3/22/18  
*L. Hoagland*

# Church of God "Rosa De Saron"

3155 Cesar Chavez St.  
San Francisco CA 94110  
(415) 285-3371

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## HISTORY OF THE CHURCH OF GOD "ROSA DE SARON"

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## **OUR IDENTITY**

### **BIRTH OF OUR LOCAL CHURCH**

The Church of God "ROSA DE SARON" was founded in the City of San Francisco, California in year 1962 on Castro Street, in the Noe Valley District. The Church stayed in that location for about ten years until it moved to its current location at: 3155 Cesar Chavez St., San Francisco, CA. 94110 back in year 1972.

For over 40 years the Church of God "Rosa de Saron", San Francisco has been an active partner in the community participating in different community activities

### **AFILIATION**

We are affiliated to the Church of God International based in Cleveland, Tennessee at 2490 NW Keith Street, Cleveland, TN 37320-2430. Through our international affiliation we serve our Lord and help communities of all races in over 186 countries worldwide.

### **LOCAL CONGREAGATION IN SAN FRANCISCO**

We are a congregation of over 100 members. We are a well balanced congregation; we have children, teenagers, young adults, seniors, married couples, single mothers and fathers, etc.

### **DOCTRINE**

Our doctrine is based in the teaching of our Lord Jesus Christ, in which we most Love God above all things and our neighbor just like ourselves. **The fruits of our beliefs are: love, joy, peace, longsuffering, gentleness, goodness, faith, Meekness, temperance, hope, and forgiveness.**

## **OUR GOAL AND MISSION**

**Our goal is NOT to provide families with new religion but instead with a better life style,** a life full of love, peace, forgiveness and hope, free of violence and substance abuse where children, teenagers, young adults, grown up adults, the elderly and the whole congregation can focus in education and thus bring up more professionals and less criminals. When we talk about education we mean secular education and spiritual education, both go hand to hand. In a few words, our goal is to make responsible citizens.

## **LIFE EXPERIENCES AT OUR CURRENT LOCATION**

The Church is located in an upcoming area. We have been a part of the renovation and growth of our neighborhood but there is still more to do in this regard. Through the years we have experienced and continue to experience losses resulting from: theft, violence, vandalism.

In several occasions' we have been in danger of being harmed in cross fire during night and early morning shootings. In one occasion on a Sunday morning some of our members had to lie on the floor to avoid bullets.

One evening of 1993 during a service and when our Pastor was giving his sermon, one of his daughters was hit by a strayed bullet, her right shoulder was injured and she had to be taken to the hospital emergency room.

**(Copy of the Examiner Newspaper for 4/22/93 showing the news of the incident is attached)**

### **Why has the Church chosen to stay at the current location?**

This was the question asked by all the TV channels that covered the news about the Pastor Daughter's shooting and our answer was: because God needs us in this location where there is so much need.



### **PARKING CONDITIONS NEAR OUR HOUSE OF PRAYER**

Our congregation for the most part lives near our House of Prayer and don't need to drive; the members that live far away use public transportation such as Bart or bus. The Bart station and bus stops are very close to our location. Few of our members drive and park in the neighborhood.

There is a Public School near our Church (Lenard R Flynn School) located on Harrison Street and Cesar Chavez Street and since our services are in the evenings our members park in front of the school that is closed at those hours.

SEE STREET VIEW PICTURE (Harrison and Cesar Chavez)

## **OUR CHURCH ACTIVITIES**

For over 40 years the Church of God "Rosa de Saron", San Francisco has been an active partner in the community participating in different community activities.

### **SOME OF OUR COMMUNITY RELATED ACTIVITIES ARE AS FOLLOWS:**

- a) Street and sidewalk cleaning
- b) Neighborhood walls' cleaning and painting, removing graphite
- c) Donating food, clothing and shoes to the needy
- d) Visiting inmates in the SF jail.

### **SOME OF OUR OUTREACH ACTIVITIES ARE AS FOLLOWS:**

- a) Every two months a secular lecture for Women only at the Patio Espanol Restaurant.
- b) Every two months a secular lecture for Men only at the Patio Espanol Restaurant

The themes for these lectures range from: Family help, Marriage, children, education, etc. We focus on inviting Men and Women that have no knowledge of the new life Offered by our Lord Jesus Christ.

- c) Capellan service visiting teenagers at the Juvenile jail.
- d) Sports. Sport tournaments in which the different Churches in our area participate.
- f) Regular Evening Services at our Church on Tuesdays, Wednesdays, Fridays and Sundays.
- g) Sunday school for children.

The most important activity for our congregation is to help to improve the condition of dysfunctional families in the area of physical abuse, domestic violence, alcoholism and addiction to drugs, depression, low self steam, etc.

We achieve the above by preaching the message from of our Lord Jesus Christ. The message is about Peace, Love, Hope, forgiveness, helping the needy. This is what believing in Jesus Christ is all about.

## **SOME OF OUR ACHIEVEMENTS**

Our greatest satisfaction so far is that we have over 200 leaders; among them are Pastors, teachers, musicians, preachers, counselors and professionals in many areas. Men and women that left behind drug abuse, alcoholism, prostitution and gang affiliation, families that were transformed by the mercy of our **Lord Jesus Christ**

**Some of examples of these achievements are:**

Mr. Efrain Benavides: he was an alcoholic that used to live under the SF bridges for over 10 years and now he works for the Federal Government (Caltrans) and has special assignment.

Mr. Alvaro Quijano: a former Drug dealer on Mission Street, now a Pastor in Los Angeles, CA. with a congregation of over 1,300 members.

Mr. Fernando Millan: Former Chief of a gang organization, now back in Mexico as a Pastor helping children and teenagers with drug addiction problems.

Mr Rodolfo Murga: dealer and user of Crack and marihuana, now an honest worker with his own janitorial business. Also helping others with his testimony about how Jesus changed and saved his life.

The above are only a few of so many testimonies of what Jesus has done in so many lives. There are many other brothers that met Jesus in our Church that are now serving GOD in their original countries such as: Mexico, El Salvador, Guatemala, Nicaragua, Peru, Colombia, Honduras, Dominican Republic, Puerto Rico and other countries.



## OUR FINANCES

### HOW IS OUR CHURCH FINANCED

Our Church is supported by its members or congregation through: tithes, offerings, one time contributions and other activities such as: kermeses, luncheons, etc. We do not receive any public or private assistance

With the above sources of income we cover all of our local expenses and we contribute monthly through our International affiliation to our World Missions Organization, to Orphan houses, centers of rehabilitation, houses of prayer, disaster relief such as earthquakes, tsunami, flooding, fires, etc.

### SUBDIVISION COSTS LOST DUE TO THE PROCESS DELAY

Back in 1991 we began the building permit process to expand our facilities (our house of prayer) but for reasons out of our control the subdivision process took an extraordinary long time and a large portion of the costs associated with it are now a loss because the services rendered no longer serve a purpose, can no longer be used or became obsolete. **These costs total about \$154,961** and the breakdown is as follows:

- 1- An architect and contractor named Earl took \$45,000.00 (This Architect disappeared)
- 2- An Engineer from Transamerica \$ 3,500.00 (Also no longer can be found)
- 3- Robert T. Roddick for the sub-division \$ 12,271.00 (Unsuccessful effort)
- 4- City of San Francisco \$ 9,000.00 (Fees that no longer serve a purpose)
- 5- Law offices of Carr McLellan \$43,600.00 (Lawyer for the sub-division)
- 6- Carolyn Liu \$19,500.00 for plan drawings (Plans became obsolete & can not be used)
- 7- DBI \$15,698.80 (same as above)
- 8- SF City planning \$6,392.00 (same as above)

We have incurred in the above and other expenses with the hope to be able to expand our facilities. These are moneys that we need so bad to continue our support to the community.

## CLOSING COMMENTS


We are in great need to upgrade our facilities to be able to serve our growing attendance. In addition, lots of our original member families now have children and grand children in need to receive the guidance for a better life offered by our Lord Jesus Christ. We anticipated this need over 20 years ago and decided then that it was time to expand the capacity of our building. Our congregation has paid a huge price tag in unforeseen costs for the project.

A property that in year 1991 had a cost of about \$115,000.00 ended up costing us about \$375,000.00 due to the issues related to the sub-division.

Still, even with all of the adversity that we have encountered, we are very enthusiastic and confident about our project. We believe that now is the time to complete this endeavor. We value the cooperation that we are receiving from the different City of San Francisco Departments involved in the project approval process.

On behalf of our congregation, we thank you in advance for your support to make this dream of twenty one years a reality.

## **GOD BLESS YOU ALL**

  
Bishop Juan José Segura (Church Pastor)  
América Guzmán (Secretary)

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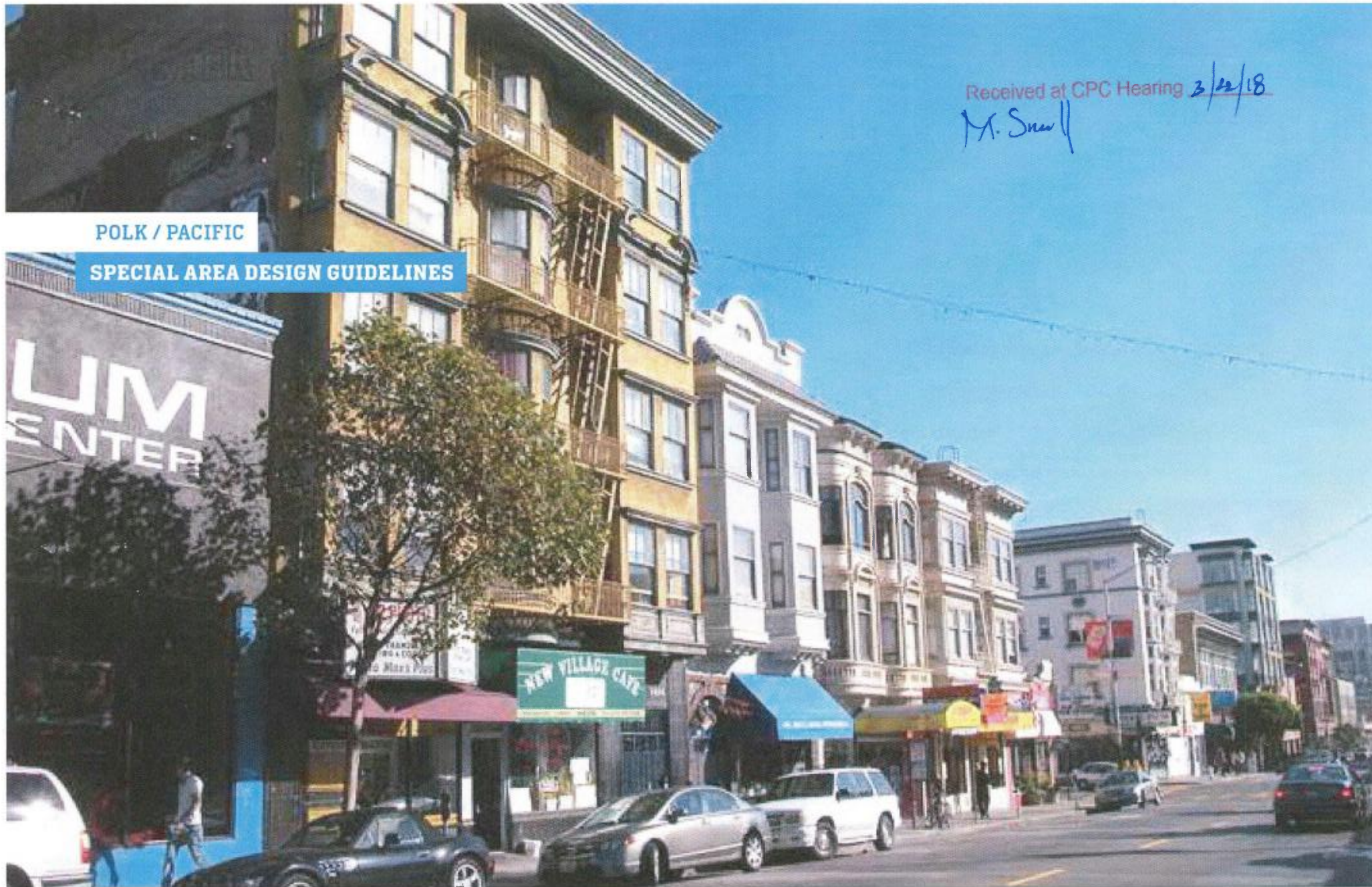
Juan Roman



Received at CPC Hearing 3/22/18  
M. Snewell

POLK / PACIFIC

SPECIAL AREA DESIGN GUIDELINES



REVIEW DRAFT  
3.22.2018

San Francisco  
**Planning**





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# Polk / Pacific Special Area Design Guidelines

## SITE DESIGN



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- S2.2 Respect Mid-Block Open Space and Retain Access to Light and Air of Adjacent Buildings and Residential Open Space 9
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## PUBLIC REALM

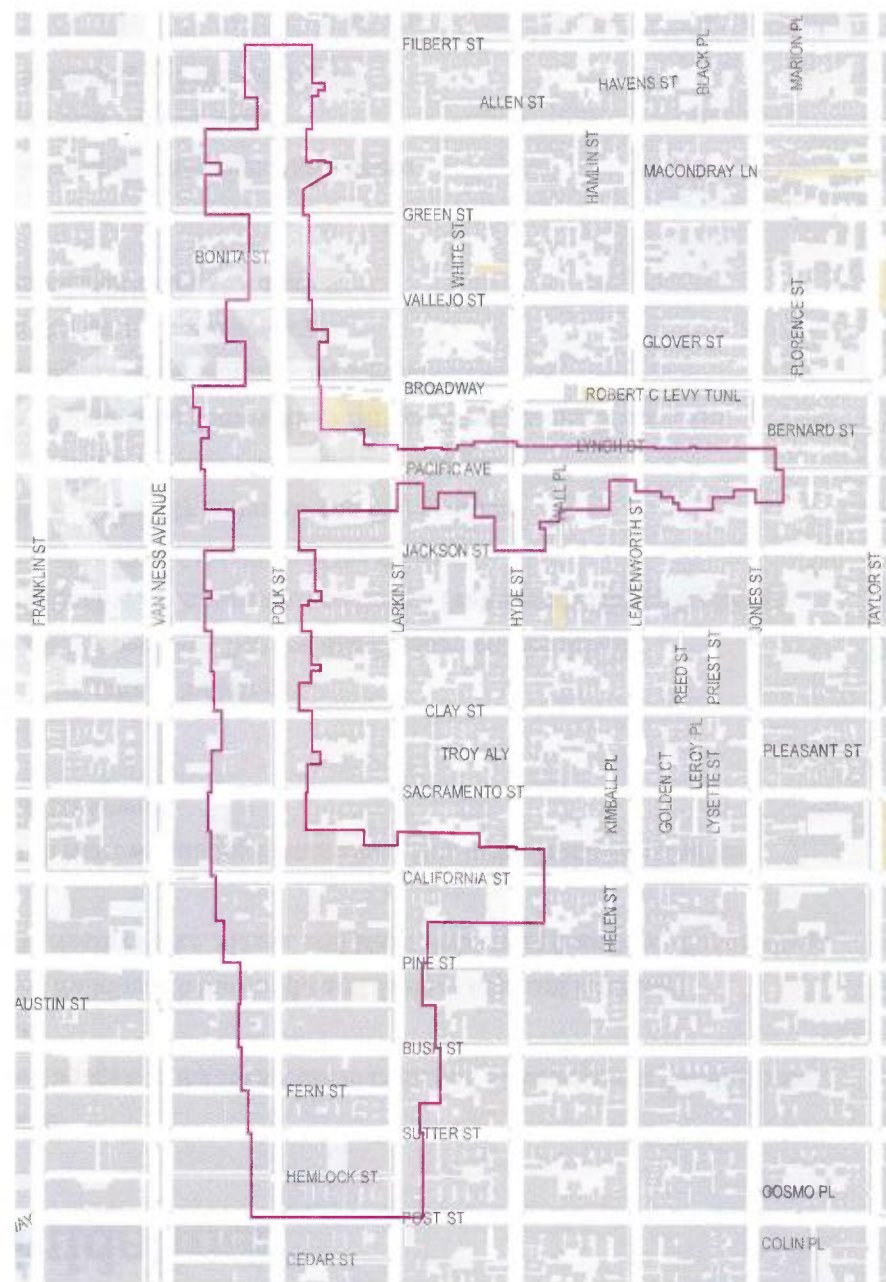


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Sitting in the gulch between Nob and Russian Hills and Pacific Heights, Polk Street and Pacific Avenue are among the oldest neighborhood commercial districts in San Francisco. The adjoining Neighborhood Commercial Districts extend a mile north-south along Polk Street and include a portion of Larkin Street between Post and California Streets, and a five block portion of Pacific Avenue. Polk Street's dense mixed-use character consists of buildings with ground-story commercial uses that extend continuous commercial activity for almost its entire length and typically have residential units above. The cross streets in the district have a greater proportion of residences than Polk Street itself. The district provide convenience goods and services to the residential communities in the Polk Gulch neighborhood and to the residents on the west slopes of Nob and Russian Hills. It has many apparel and specialty stores, as well as some automobile uses, which serve a broader trade area. Commercial uses also include offices, as well as restaurants and bars which keep the district active into the evening.

A common feature of this district is the prevalence of small-scale development based on the small lot pattern of blocks which mainly were intended for residential development. During the first half of the century, in cases where several lots were merged for larger commercial development, builders tended to articulate the facades to complement the scale of the smaller development.

The Polk Street NC District and Pacific Avenue NC District controls are designed to promote development which is compatible with the surrounding neighborhood. The building standards monitor large-scale development and protect rear yards at residential levels. Consistent with Polk Street's existing mixed-use character, new buildings may contain most commercial uses at the first two stories. The controls encourage small neighborhood-serving businesses, but limit new eating, drinking, other entertainment, and financial service uses, which can produce parking congestion, noise and other nuisances or displace other types of local-serving convenience goods and services. They also prohibit new adult entertainment uses. Restrictions on drive-up and most automobile uses protect the district's continuous retail frontage and prevent further traffic congestion.





# Guideline Origin

The Polk / Pacific Special Area Design Guidelines are based on existing guidelines established in the Commerce and Industry Element of the San Francisco General Plan and the Polk Street NC District, and Pacific Avenue NC District of the Planning Code (Sections 723 and 726 respectively). The Guidelines illustrate means by which new development should contribute to the existing environment.

In an effort to help preserve and promote the livability and attractiveness of the Polk and Pacific Neighborhood Commercial Districts, the design and siting of new buildings, additions, and alterations should be compatible with the character of surrounding buildings and the existing pattern of development. In designing or evaluating a development proposal, consider the following criteria:

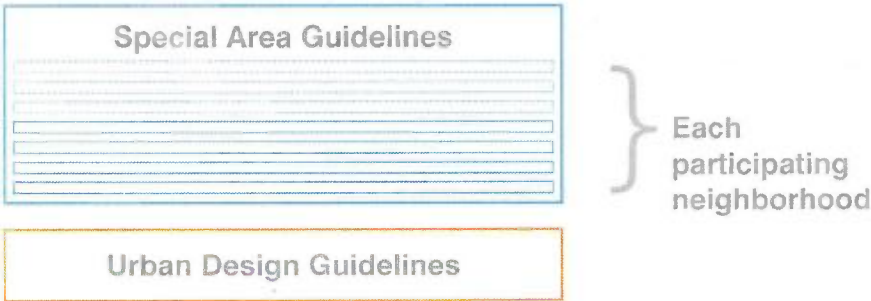
- Overall district scale;
- Individual street character and form;
- Lot development patterns;
- Adjacent property usage, especially buildings of historical, cultural or architectural importance; and
- Site development and building design

| GUIDELINE  | RATIONALE   | RANGE OF MEANS   | EXAMPLE   |
|--|---|--|---|
| <p><b>S.1</b> REFLECT THE ARCHITECTURAL QUALITY, COMPOSITION, AND DESIGN FEATURES OF EXISTING BUILDINGS THAT CONTRIBUTE TO THE POSITIVE VISUAL QUALITIES OF THE DISTRICT</p>  | <p>Existing buildings in the district are characterized by their architectural quality, composition, and design features. These buildings contribute to the positive visual qualities of the district and should be preserved or replicated in new development.</p> | <p>The range of means describes important parameters and methods by which a project can meet the guideline, but is not a prescriptive list. Projects may satisfy the guideline by applying one or all of the means or by suggesting something unique to the project that meets the intent.</p> |  |

# Application of the Guidelines

The Special Area Design Guidelines establish a localized set of goals, values, and qualities by which projects are evaluated in design review: projects must demonstrate compliance with applicable guidelines to be successfully entitled. The Polk / Pacific Special Area Design Guidelines incorporate neighborhood-specific context statements and guidelines.

The Polk / Pacific Special Area Design Guidelines work in concert with the Urban Design Guidelines (UDGs). Consistency with both sets of guidelines is mandatory in the approval process. Should application of the respective guidelines conflict, the Special Area Guidelines supercede the Urban Design Guidelines (UDGs).



## Guideline Structure

Each guideline is described at the top of the page, followed by a sidebar that explains the rationale for the guideline, a range of means by which one might achieve that guideline, and illustrations that further describe its application. The range of means describes important parameters and methods by which a project can meet the guideline, but is not a prescriptive list. Projects may satisfy the guideline by applying one or all of the means or by suggesting something unique to the project that meets the intent. The guidelines are organized to relate and elaborate with more specificity to the relevant guideline in the Urban Design Guidelines. For example, S1.1 of the Polk / Pacific Special Area Design Guidelines is related to S1 of the UDGs. The illustrations are existing examples in the Polk and Pacific Commercial Districts that exemplify the means for the guideline indicated but are not necessarily exemplary of every guideline.





# S SITE DESIGN

The setting and built form of the Polk and Pacific Neighborhood Commercial Districts give them a unique neighborhood identity. The guidelines in this section guide the height, form, massing, and scale of development to maintain the balance between consistency and variety found in the district.

Site design is concerned about the massing of buildings and their relationship to topography, open space and the overall city fabric. Each building plays a role in the block and street environment and should support the existing patterns of open space, circulation, uses, access to sunlight, and pedestrian experience.

Three key patterns appear in this section's guidelines:

- Respecting mid-block open space;
- Defining the streetwall to fit the existing fabric; and
- Shaping buildings to fit the scale of existing buildings.

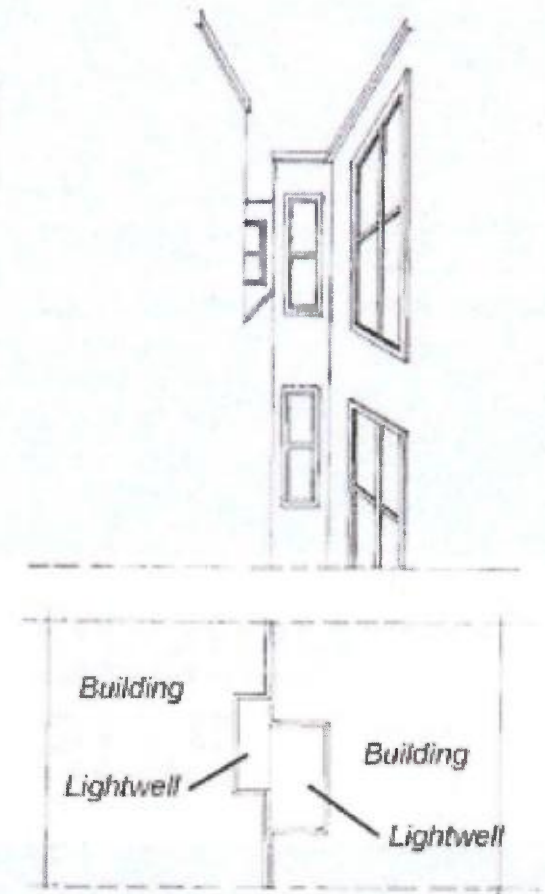
- S2.1 Reflect Setback Patterns
- S2.2 Respect Rear Yard Mid-Block Open Space and Retain Access to Light and Air of Adjacent Buildings and Open Space
- S2.3 Relate the Height of New Buildings to the Height and Scale of Adjacent Buildings
- S2.4 Maximize Sun Access to Nearby Parks, Plazas and Major Pedestrian Corridors – Especially in Alleys
- S2.5 Maintain Small Lots with Narrow Building Fronts where this is the Traditional Pattern
- S5.1 Maintain the Prevailing Streetwall



## S2.1 REFLECT SETBACK PATTERNS

Side setbacks and light-wells are design features that provide light and air to narrow and deep buildings. These qualities should be protected to ensure the future livability and adaptability of these buildings.

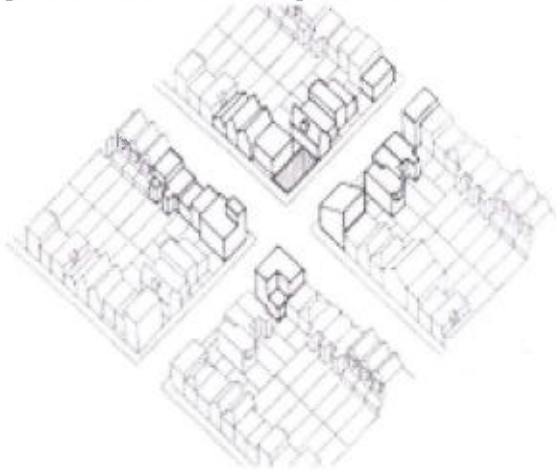
- » Use front set-backs sparingly to provide space for outdoor activities such as sidewalk seating.
- » Protect existing lightwells and side setbacks by providing light wells and side setbacks that match existing adjacent conditions.



Provide shared light wells to maximize light to both properties.

## S2.2 RESPECT REAR YARD MID-BLOCK OPEN SPACE AND RETAIN ACCESS OF LIGHT AND AIR TO ADJACENT BUILDINGS AND RESIDENTIAL OPEN SPACE

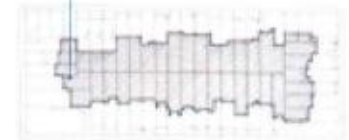
The aggregation of rear yards creates a mid-block open space that is visually accessible to residents and provides access to light and air for the back portions of buildings. It is important that access to the mid-block open space is not blocked. The same care must be taken to rear yards where development occurs adjacent to these districts. New development has the responsibility to create and contribute to mid-block open space -- even if no clear pattern exists.



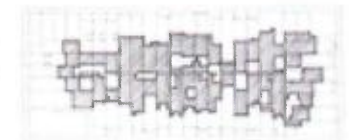
Locate buildings to provide continuity with the mid-block open space.

- » Locate, orient, and shape open space to establish, respect, or enhance the existing mid-block open space and minimize impacts to privacy.
- » On irregularly shaped lots, through-lots or those adjacent to fully-built lots, locate open space where it improves the access of light and air to residential units.
- » Site new buildings to respect mid-block open space and retain access of light and air to adjacent buildings.
- » New development over existing structures that encroach into rear yards should re-establish a code complying rear yard.
- » Rear yard open space should be at-grade or as close as possible to adjoining properties grade, unless unusual or exceptional circumstances are present.
- » In some cases it may be necessary to create side yards to augment existing rear yards.

Block with a strong mid-block open space pattern.



Block with an irregular mid-block open space pattern. The rear yards of many of the parcels are developed with structures.



SHAPE BUILDINGS TO CONTRIBUTE TO MID-BLOCK OPEN SPACE EVEN WHEN THE PATTERN IS BROKEN

Shape and locate new open spaces to support and enhance existing open space.

## S2.3 RELATE THE HEIGHT OF NEW BUILDINGS TO THE HEIGHT AND SCALE OF ADJACENT BUILDINGS

Polk Street and Pacific Avenue have a range of buildings with varied heights and widths. Building design should avoid abrupt transitions of and promote a continuity of scale.

- » Irrespective of height and bulk limits, provide transitions between high and low buildings on a street of varied building heights. While three- and four-story buildings are appropriate in many locations, two-story buildings are more appropriate in some areas with lower-scale development.
- » Setback upper stories to be subordinate to the prevailing height of the street wall.
- » Sculpt buildings to avoid abrupt transitions in scale between existing buildings.
- » Provide setbacks to reduce impacts of light air and privacy to adjacent buildings.
- » provide setbacks that relate to setbacks of existing buildings.



Sloping the roof helps the massing of this building transition to the scale of the adjacent lower buildings.



Setting back the upper floor moderates the height and sense of scale between neighboring buildings.



Setting back the portion adjacent to the neighboring building helps ease the change of scale



## S2.4 MAXIMIZE SUN ACCESS TO NEARBY PLAZAS AND PEDESTRIAN CORRIDORS - ESPECIALLY IN ALLEYS

Streets and alleys comprise most of the public open space in the Polk Street and Pacific Avenue corridors. Preserving sun light to these public spaces helps encourage their use and activation of a commercial neighborhood.

- » Locate and shape buildings to reduce shadows on public open space.
- » Setback upper floors to minimize shadows
- » In some cases it may be necessary to reduce the height of proposed development to preserve sun access.



Low scale development on alleys preserves light to the street space.



New building fronting alleys should retain solar access

## **S2.5 MAINTAIN SMALL LOTS WITH NARROW BUILDING FRONTS WHERE THIS IS THE TRADITIONAL PATTERN**

Pacific Avenue has a fairly consistent range of traditional building widths based on 25'-30' wide lots, where Polk has buildings that range from a full block to single lot buildings. The effect larger development can have in altering the traditional sense of neighborhood scale should be recognized and tempered.

- » Reflect neighborhood-prevailing lot widths and proportion and size of architectural elements in the modulation of the proposed building.
- » Design new buildings on large lots as a series of elements which are compatible with the existing scale of the district.



Building widths and expression should be consistent with frontages in the neighborhood even on larger lots.



A typical pattern of Polk Street.



## S5.1 MAINTAIN THE PREVAILING STREET WALL

There are almost no setbacks, other than recessed entrances, along Polk Street or Pacific Avenue and where they do occur it weakens the definition of public space. Streetwalls promote a continuity of urban fabric along with pedestrian experience. The scale and design of building fronts at the street are important in contributing to an active, engaging, and pedestrian-oriented street-scape.

- » Design new buildings to help define and maintain the street wall.
- » Stepping buildings with topography maintains continuity of the street wall height.
- » Design building frontages with active and direct engagement to the street to support pedestrian-oriented activity.
- » Consider the width of the sidewalk in establishing the articulation of the streetwall.
- » Absolute consistency with streetwalls is not always necessary. In some settings, it may be appropriate for a frontage to employ a forecourt, setback, or recess that acts as a

lively counterpoint to a street wall, but not to such an extent that it erodes the overall sense of urban enclosure.

- » Design inviting transitional entrance spaces between the building and the street.
- » Use front set-backs for special circumstances that provide space for outdoor activities such as sidewalk cafes and walk-up windows, or publicly accessible open space. Public open space is appropriate where the retail activity of the street is not adversely affected; there is a shortage of nearby open space to serve people; the site is appropriate in terms of its topography and sun and wind conditions;



Strong streetwalls provide a backdrop for the public realm.

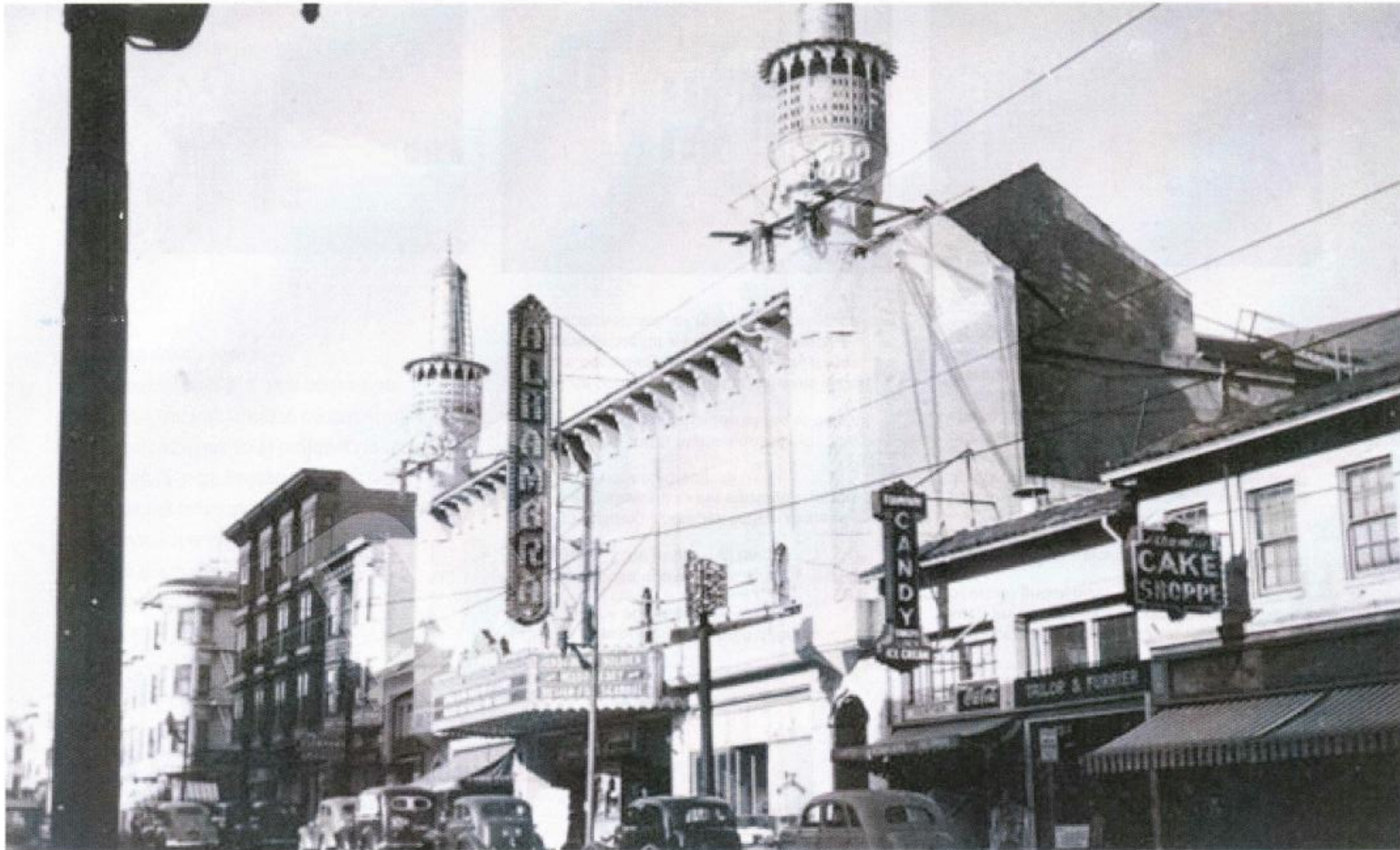


This building reinforces the streetwall by stepping with topography which also modulates its scale.



Continuous streetwall present at the lot edge creates a defined sidewalk space.







# A ARCHITECTURE

The buildings in the Polk and Pacific districts have a variety of visual character spanning various eras of development which all work together to form a vibrant urban fabric. In order to fit into this fabric without dramatic disruption, new buildings have an obligation to respectfully add to the richness of character, texture, and human-scale of their neighbors.

The guidelines in this section guide the architectural features and composition of new buildings and additions to reinforce or enhance the physical patterns and features of surrounding buildings through the use of:

- Compatible scale,
- Proportions,
- Facade composition,
- Materials, and
- Roof forms.

Application of these guidelines is intended to add new variations within the consistency and familiarity of Polk and Pacific NCDs.

- A1.1 Preserve Architecturally Important Buildings
- A3.1 Harmonize with the Scale, Proportions, Texture, and Character of the District
- A3.2 Reflect the Architectural Quality, Composition and Design Features of Existing Buildings that Contribute to the Positive Visual Qualities of the District
- A4.1 Design Blank Walls with High Quality Materials and Composition
- A4.2 Design Roofs and Balconies to Minimize Visual and Noise Impacts
- A7.1 Relate Size and Design of Signs to be Compatible with the Character and Scale of the Building as well as the Neighborhood Commercial District
- A8.1 Maximize Commercial Storefront Transparency
- A8.2 Design Storefronts with Human-Scaled Features



## **A1.1 PRESERVE ARCHITECTURALLY IMPORTANT BUILDINGS**

Renovation and restoration of older, well-designed buildings helps preserve neighborhood character and visual interest of the streetscape if the original building design is respected in use of materials and details. Characteristic of the Polk / Pacific district are auto service buildings. Retaining their architecture while allowing new development is desired.

Reuse or rehabilitate existing structures in sound condition and of worthwhile architectural character where feasible to retain the unique character of a given neighborhood commercial district.

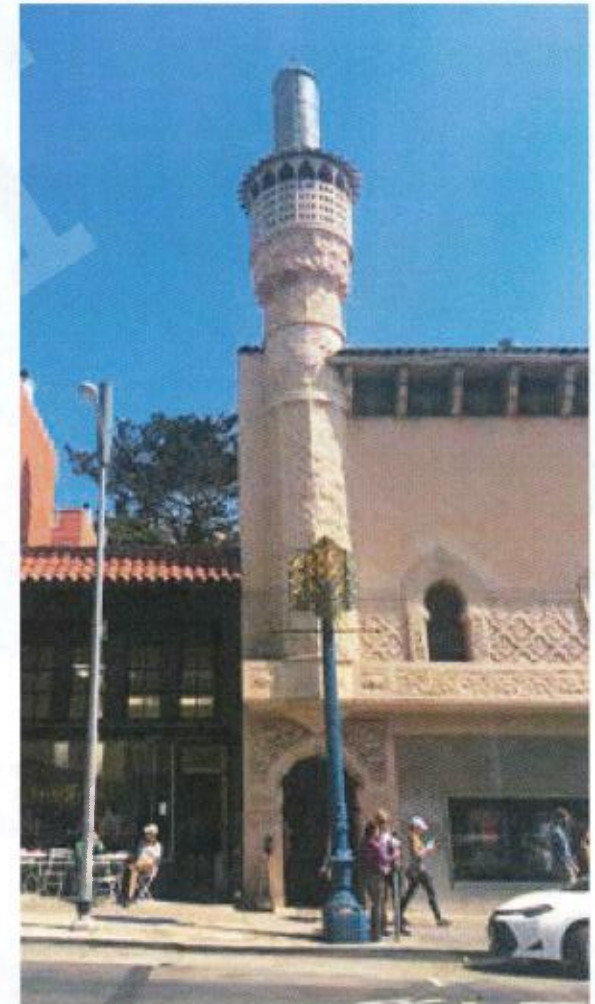
- » Development of auto service garage sites should retain significant portions of the existing structure while accommodating new additions.



Historic fabric provides fine grain and patterns that represent different eras.



Look for ways to preserve old buildings for new uses in meaningful ways.



Historic buildings function as focal points for the neighborhood.



### A3.1 HARMONIZE WITH THE SCALE, PROPORTIONS, TEXTURE AND CHARACTER OF THE NEIGHBORHOOD COMMERCIAL DISTRICT

Many of the buildings that define Polk Street were developed in the Victorian era with vertically proportioned bays and windows, detailed with wood trim and clad in wood and stucco.

Respecting scale and proportion in the design of buildings help maintain continuity with the existing context.

- » Design alterations and new buildings to be compatible with existing buildings' architectural quality to contribute to and preserve the scale and character of the neighborhood commercial district.
- » The details, material, and color of existing architecturally distinctive buildings should be complemented by new development.
- » Buildings designed to follow a prescribed formula by businesses with multiple locations is discouraged if such design would be

incompatible with the scale and character of the district in which the building is located.

- » Proportion the scale and overall amount of glazing to the existing building patterns.
- » Reflect the positive aspects of the existing scale and design features of the area in the design of new buildings, additions and alterations, and facade renovations. Building forms should complement and improve the overall neighborhood environment.



Projecting head and sill trim adds shadow and detail. Vertically proportioned windows with divided lites reflect the prevailing scale of the neighborhood.



New buildings maintain the fabric of existing residential qualities above the ground level commercial uses.

### **A3.2 REFLECT THE ARCHITECTURAL QUALITY, COMPOSITION, AND DESIGN FEATURES OF EXISTING BUILDINGS THAT CONTRIBUTE TO THE POSITIVE VISUAL QUALITIES OF THE DISTRICT**

Many buildings along Polk Street developed as large facades composed with simple, regular repeating features. In most cases buildings are composed of strongly defined and differentiated bases, bodies, and tops.

New buildings that recognize and respond to existing features, details and materials strengthen and maintain continuity with the existing neighborhood.



Relate a new or remodeled building to its surrounding area by using compatible proportions, textures, and details. Nearby buildings of architectural distinction can serve as primary references. Existing street rhythms should also be continued on the facade of a new building, linking it to the rest of the district.

- » Individual buildings in the Polk neighborhood commercial districts are rich in architectural detailing, yet vary considerably from building to building, depending upon the age and style of their construction. Vertical lines of columns or piers, and horizontal lines of belt courses or cornices are common to many buildings as are moldings around windows and doors. These elements add richness to a flat facade wall, emphasizing the contrast of shapes and surfaces.



The spirit of artistic detail of this building give it life and a unique identity, but it also shares common materials and features with its neighbors.

- » Respond to the ornamental scale of adjacent buildings. Historic features may be reinterpreted, but should be identifiable as from their own era. Avoid cursory historicism and facade elements that mimic neighbors.
- » Consider a rhythm of horizontal and vertical elements, such as bay windows, cornices, belt courses, window moldings,
- » Balconies provide an opportunity for an active interface with the street, but should be limited in size and exposure to the street.



Recognize and reflect architectural features that occur in the neighborhood. A building entry is made evident through detailing.



## A4.1 DESIGN BLANK WALLS WITH HIGH QUALITY MATERIALS AND COMPOSITION

When adjacent buildings vary in height property line walls are often exposed. Due to building uses other walls may need to be solid. Blank walls that are visible should exhibit high quality design and materiality.

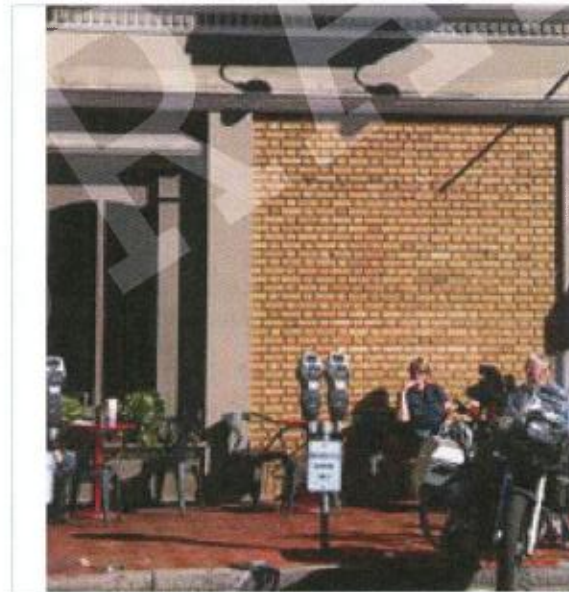
- » Design all visible facades with similar effort and consideration as primary facades
- » Sculpt and articulate sidewalls that are likely to be significantly exposed.
- » Match materials used on the primary facade
- » Match the texture and scale and pattern of the primary facade.



Materials that are found on the primary facade are used on the exposed side walls, providing a unifying element of scale as well.



Walls that cannot be transparent may be augmented with other design features



Although blank, the material quality of this wall make it nice to sit next to.



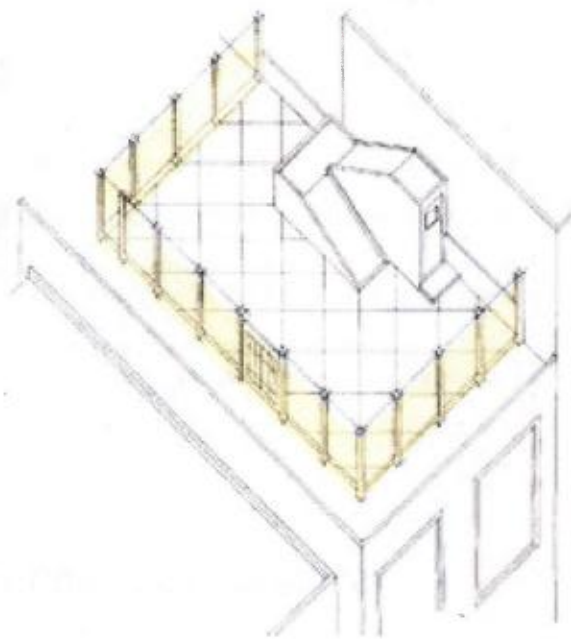
Murals engage the eye, and when significant and extensive can attract visitors and foot traffic.



## A4.2 DESIGN ROOFS AND BALCONIES TO MINIMIZE VISUAL AND NOISE IMPACTS

In a city of hills and tall buildings, roofs can be seen from many vantage points. The extent to which roof top appurtenances and uses affect the visual quality, noise, and access to sunlight needs to be considered.

- » Locate rooftop mechanical equipment away from areas of residential use and screened and integrated with the design of the building..
- » Minimize and locate stair and elevator penthouses to reduce their visual impact
- » Size and locate roof decks to minimize visual and noise impacts.
- » Minimize, combine, and integrate rooftop utilities into the overall building architecture.
- » Decking and green roofs support a more visually compelling roof landscape and reduce solar gain, air pollution, and the amount of water entering the stormwater system.
- » Use translucent or opaque materials at balconies to avoid visual clutter.



To minimize its visibility, this transparent windscreen is set in from the front, rear, and sides of the property.

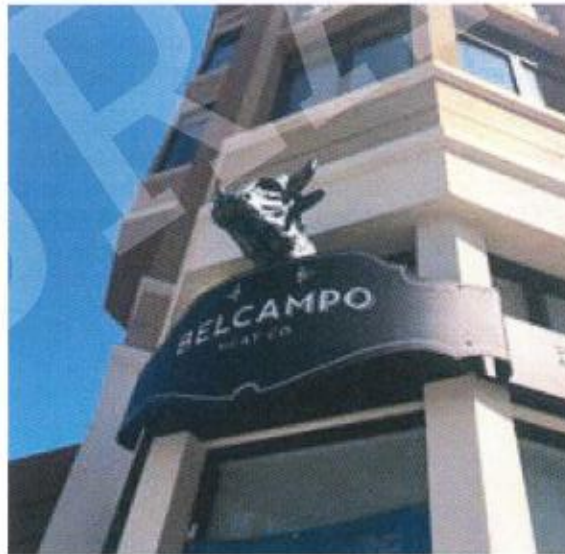


Translucent materials increase privacy for residents and improve the visual experience from the public realm.

## **A7.1 RELATE SIZE AND DESIGN OF SIGNS TO BE COMPATIBLE WITH THE CHARACTER AND SCALE OF THE BUILDING AS WELL AS THE NEIGHBORHOOD COMMERCIAL DISTRICT**

The character of signs and other features attached to or projecting from buildings is an important part of the visual appeal of a street and the general quality and economic stability of the area. Opportunities exist to relate these signs and projections more effectively to street design and building design. As much as signs and other advertising devices are essential to a vital commercial district, they should not interfere with or diminish the livability of residences within the neighborhood commercial district or in adjacent residential districts.

- » Signs should not be attached to facades at residentially- occupied stories nor should sign illumination shine directly into windows of residential units.
- » Use signs to demonstrate craft and uniqueness of the business.
- » Coordinate signs with scale, location and design of other business signs.
- » Control the intensity of building and signage lighting and allow for dimming and color variation.
- » Orient and size signs to the pedestrian scale, and so as to not overwhelm the building facade.
- » Design building signs to reflect the type and sensibility of their use. Consider marquees where programmatically appropriate.
- » Design signs and canopies appropriately to illustrate the hierarchy of entrances and information along facades where there are many elements or uses.



Signage inventively incorporated as a facade element.



Signage works with canopy elements to help define the sidewalk and provide shelter.



## A8.1 MAXIMIZE COMMERCIAL STOREFRONT TRANSPARENCY

Buildings that provide an active and transparent interface between their interior uses and the street support interest, well-being, and safety through natural surveillance. Ground floor retail should create an engaging, human-scale street experience

- » Use clear, un-tinted glass at the street level to allow maximum visual interaction between sidewalk areas and the interior of buildings. Mirrored, highly reflective glass or densely-tinted glass should not be used except as an architectural or decorative accents.
- » Where a substantial length of blank wall is unavoidable, use eye-level display, a contrast in wall treatment, offset wall line, outdoor seating and/or landscaping to enhance visual interest and pedestrian vitality.



Outdoor sidewalk displays may augment the intent of this guideline, so long as sufficient room is retained on the sidewalk for travel.



Coordinate scales of retail space, architectural details, and signage.



Recessed entries and clear display windows engage store interiors with the sidewalk.



Corner entries offer unique opportunities.



## A8.2 DESIGN STOREFRONTS WITH HUMAN-SCALED FEATURES

The ground floors of the district are primarily comprised of commercial storefronts and residential entries—where the public interacts most with buildings. Ground floors that are designed to be active, invitational, and human-scaled can foster a successful neighborhood.

- » Use features such as transom bands and windows, projecting signage, and high bulkheads to provide scale.
- » Use lighting to highlight significant building features but do not over-light buildings nor project light into the sky. Employ sustainable or "dark sky" measures to reduce illumination when not needed.

- » Recess and provide adequate transition space for storefront entries to allow people to step out of the pedestrian flow.
- » design corner entries for corner buildings.
- » Design lighting to reinforce pedestrian comfort at the ground level.
- » Compositionally integrate signage, canopies and other finer-grained architectural elements to impart human-scale, enhance the public realm, and harmonize a project with its context



- BUILDING BASE TERMINATION
- FINE-GRAINED CLERESTORY GLAZING
- ARTICULATION JUST ABOVE EYE LINE
- HUMAN-SCALED GLAZING PROPORTIONS
- RECESSED ENTRY WITH LIGHTING
- FRAME DETAIL
- TEXTURED BULKHEAD



Human-scaled features at the storefront help create comfortable places for people.

Storefront features including bulkheads, transoms, signage, and awnings provide shelter and a human-scale at the street.





# P PUBLIC REALM

- P2.1 Improve the Alleys to Serve as Neighborhood Open Space
- P5.1 Provide Street Trees with New Development
- P5.2 Use Landscaping to Buffer Parking and Unbuilt Lots

DRAFT



## P2.1 IMPROVE THE ALLEYS TO SERVE AS NEIGHBORHOOD OPEN SPACE

Polk gulch and Pacific Avenue are densely built neighborhoods with few large green parks or public open spaces. Its alleys are an opportunity that can be used to provide neighborhood serving open spaces and public amenities. The community has invested much effort into developing a vision plan to design and activate these alleys.

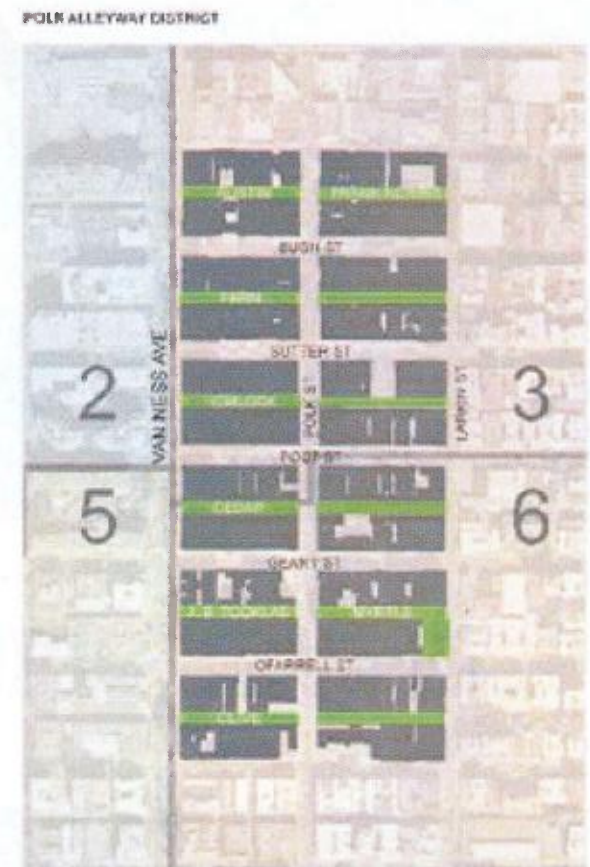
- » Alleyways naturally lend themselves to a walkable, human-scaled network. The community has created a vision to guide a long term transformation and improvement of the alleys into such a district. Refer to the Lower Polk Alleyways District Vision Plan for specific pedestrian priority designs.
- » Larger projects that abut alleys and subject to the Better Streets Plan should focus efforts to make improvements consistent with the Community Vision Plan.



Animating blank walls with murals in alleys helps create a sense of life and care.



Lighting also helps animate alleys.



Lower Polk Alley Master Plan map



## P5.1 PROVIDE STREET TREES WITH NEW DEVELOPMENT

Regularly spaced street trees selected to complement the street contribute much to the sense of overall pedestrian comfort and safety.



Trees can frame and define the sidewalk almost as much as the buildings.

- » If a district tree planting program or streetscape plan exists, new development should be landscaped in conformity with such plans.
- » Trees should be scaled according to their context, including the intensity of activity, building heights, and available light.
- » Protect trees from automobiles by locating and or providing buffers bet
- » Minimize conflicts between pedestrians in the location of trees
- » Align trees and other sidewalk landscape features to provide a direct and continuous path of travel.
- » Size tree wells and planters to support healthy trees and increased foliage.



Street trees help define sidewalk space and use.

- » Consider permeable paving wherever possible to reduce water flow during heavy rain.
- » Integrate pedestrian lighting with tree wells.
- » In places where tree planting is not appropriate due to inadequate sidewalk width, interference with utilities, undesirable shading, or other reasons, other means such as window boxes, planter boxes or trellises may be chosen.



Properly selected and healthy street trees help moderate the scale of buildings to the human scale.



## P5.2 USE LANDSCAPING TO BUFFER PARKING AND UNBUILT LOTS

Gaps that occur in the building streetwall may be opportunities for visually screening and greening undesirable uses.

- » Visually screen parking lots along the street frontage by low walls, earth berms and/or landscaping. Buffer the edges of parking lots bordering residentially-developed properties with trees and shrubs.
- » Consider maintenance and stewardship in development of uses and features.
- » Use planters, ledges, and low walls to provide places for people to view, socialize, and rest.



Landscaping shields the view of cars from the street but also shields car headlights onto the street.



**San Francisco**  
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Received at CPC Hearing 3/22/18  
M. Smith

SAN FRANCISCO

URBAN DESIGN GUIDELINES



**Planning Commission** Adoption Hearing 22 March 2018

**San Francisco**  
**Planning**



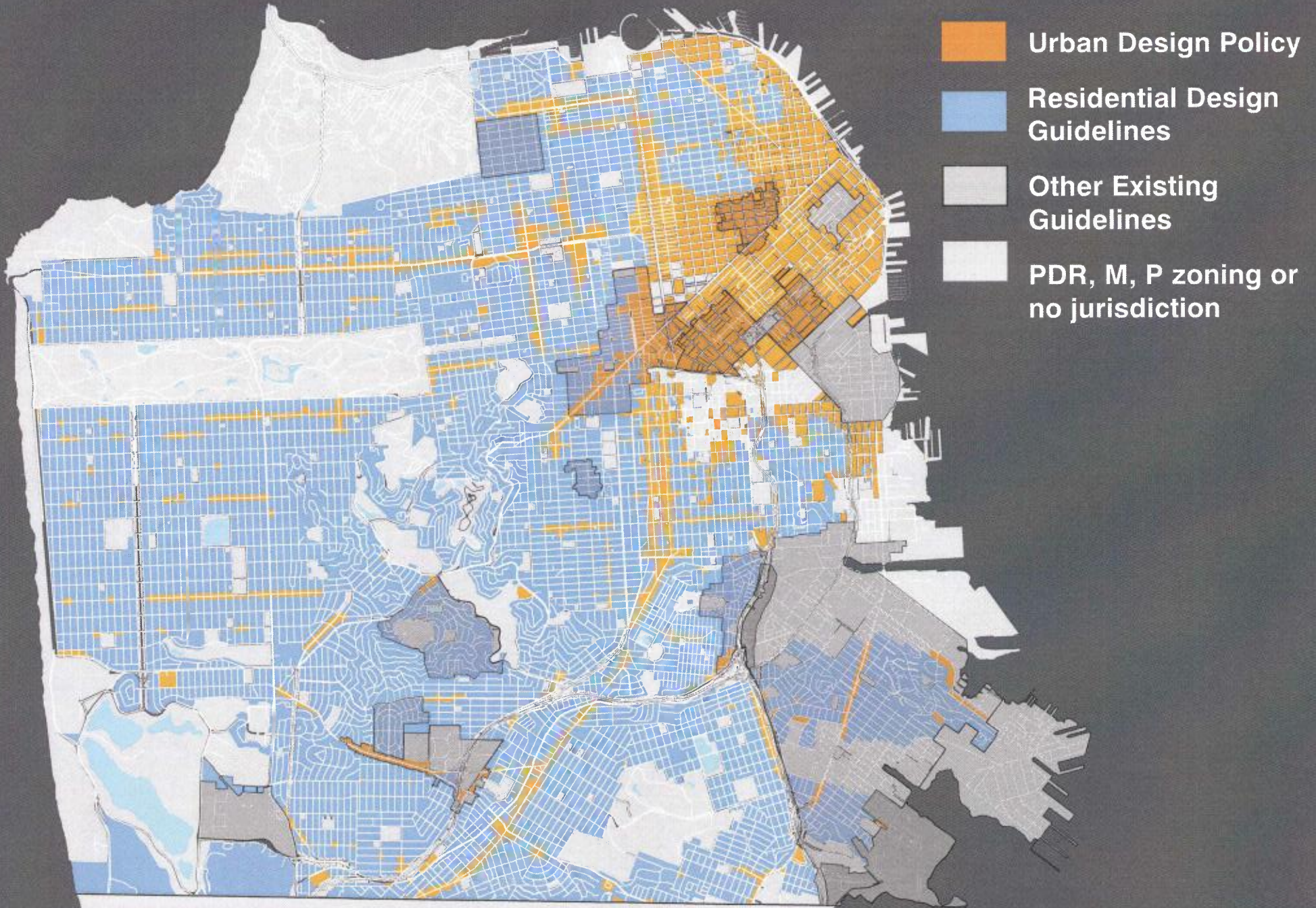


**How does design review work now?**





# Existing Design Guidelines





## CITY PATTERN

### Human Needs



The agreeable pattern of San Francisco's appearance is, perhaps above all, what makes this a city with feeling. The pattern is a visual framework composed of the natural base upon which the city rests, together with man's development. In some ways the pattern is seen in two dimensions as though it were a map; in other ways it has a sculptural or three-dimensional form.

To describe the pattern is not to describe a rigid order, for rigidity will not produce a city meant for human needs. Rather than rigidity, the sense is one of balance and compatibility, with diverse and even random features fitting together to form the whole. The pattern is made up of:

**WATER**, the Bay and Ocean, which are boundaries for the city and a part of its climate and way of life. The water is open space, a focus of major views and a place of human activity.

**HILLS AND RIDGES**, which allow the city to be seen, define districts, and more than any other feature produce the variety that is characteristic of San Francisco. The central mass of Twin Peaks separates the city into quadrants, for example, while Telegraph Hill, Sunset Heights and Potrero Hill are neighborhoods. In the topographic form of the city, the valleys and plains are as important as the hills, for they define their own districts and give the hills their visual meaning.

**OPEN SPACES AND LANDSCAPED AREAS**, whose dark green patterns enrich the color of the city and define and identify hills, districts and places for recreation. These areas may be large, as at the Presidio, Lake Merced and Golden Gate Park, smaller but still prominent as at Bayview Hill and Alta Plaza, or mixed with buildings as on the slopes of Russian Hill and Buena Vista.

**STREETS AND ROADWAYS**, which unify the pattern, emphasize the hills and valleys, provide vistas and open space and determine the character of development. Streets and roadways are of many types, each with its own functions and characteristics, and together they make up a system that accommodates man's movements and joins the districts of the city.

**BUILDINGS AND STRUCTURES** and clusters of them, which reflect the character of districts and centers for activity, provide reference points for human orientation, and may add to (but can detract from) topography and views. Some buildings and structures, such as the Golden Gate and Bay Bridges, Coit Tower, the Palace of Fine Arts and City College, stand out as single features of community importance, while elsewhere the dominant pattern of man's development is a light-toned texture of separate shapes blended and articulated over the landscape.



Design guidance  
is primarily  
based on staff  
interpretations of  
the General Plan.



# The Residential Design Guidelines (RDGs) are effective in the Residential Districts:

- » Designers start with them
- » Planners know expectations
- » Neighbors use them to support concerns



*This addition has been scaled back to two stories and is set in from the side property lines to minimize its impact.*



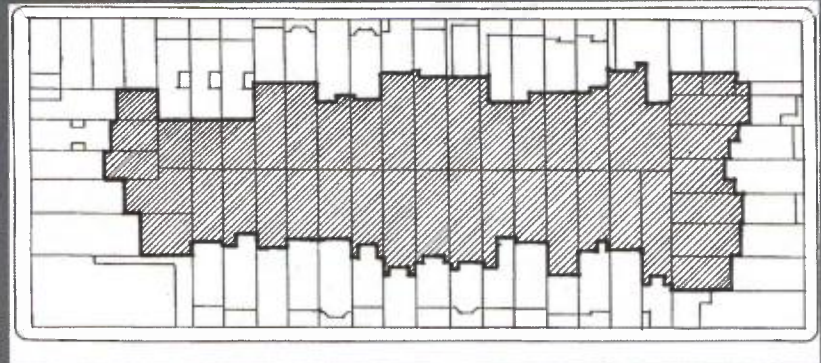
*This addition extends the full width of the lot but is set back at the second floor so the building steps down to the rear yard.*



*A fourth story setback and facade articulations make the building more compatible with the scale of surrounding buildings.*



*Although features such as bays and chimneys project into the side yards, the overall side yard pattern is consistent, creating a defining characteristic of the block face.*





We do not have  
this clarity for  
most of the  
**Neighborhood  
Commercial,  
Mixed-Use, or  
Commercial** areas  
in the city.





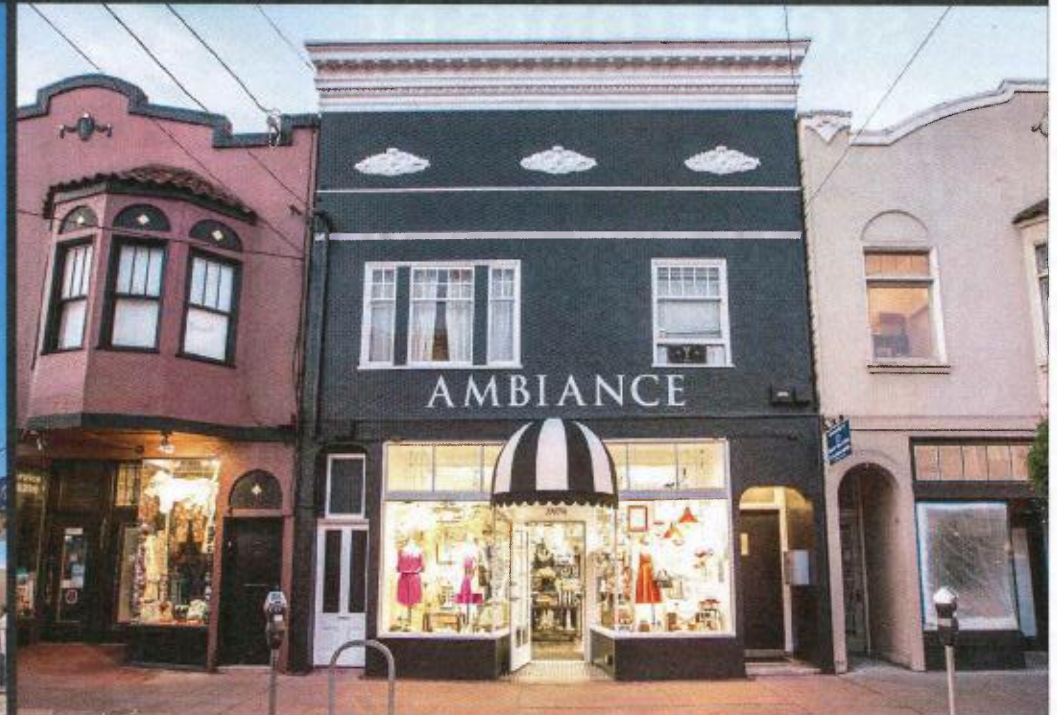
The Residential  
Design Guidelines  
do not cover  
storefronts or  
block patterns.







## What about Historic Districts?





# Guidelines Comparison

| GUIDELINE DOCUMENTS                  | Urban Design Guidelines | Residential Design Guidelines | Special Area Guidelines | Industrial Area Design Guidelines | Historic Design Guidelines |
|--------------------------------------|-------------------------|-------------------------------|-------------------------|-----------------------------------|----------------------------|
| <b>TOPICS</b>                        |                         |                               |                         |                                   |                            |
| <b>General Principles</b>            |                         |                               |                         |                                   |                            |
| Contextual compatibility             |                         |                               |                         |                                   |                            |
| Sustainability                       |                         |                               |                         |                                   |                            |
| Human needs                          |                         |                               |                         |                                   |                            |
| <b>Site Design</b>                   |                         |                               |                         |                                   |                            |
| Front and side setbacks              | S2                      |                               |                         |                                   |                            |
| Rear yard open space                 | S2                      |                               |                         |                                   |                            |
| Public views                         | S4                      |                               |                         |                                   |                            |
| Special locations                    | S3                      |                               |                         |                                   |                            |
| Extend street patterns               | S1                      |                               |                         |                                   |                            |
| Maintain street wall                 | S6                      |                               |                         |                                   |                            |
| <b>Building Scale and Form</b>       |                         |                               |                         |                                   |                            |
| Buildings conform to topography      | S8                      |                               |                         |                                   |                            |
| Scale, form, and massing at street   | A3                      |                               |                         |                                   |                            |
| Scale and massing at rear            | S2                      |                               |                         |                                   |                            |
| Facade width                         | A2                      |                               |                         |                                   |                            |
| Proportions                          | A3                      |                               |                         |                                   |                            |
| Facade composition                   | A2:A3                   |                               |                         |                                   |                            |
| Rooflines                            | A5                      |                               |                         |                                   |                            |
| <b>Architectural Features</b>        |                         |                               |                         |                                   |                            |
| Entrances                            | A6                      |                               |                         |                                   |                            |
| Porches                              |                         |                               |                         |                                   |                            |
| Utilities                            | A7                      |                               |                         |                                   |                            |
| Bays projections and balconies       | A6                      |                               |                         |                                   |                            |
| Garage parking and access            | S6                      |                               |                         |                                   |                            |
| Roof top architectural features      | A4                      |                               |                         |                                   |                            |
| Active building front                | A6                      |                               |                         |                                   |                            |
| Ground floor commercial transparency | A8                      |                               |                         |                                   |                            |
| <b>Details</b>                       |                         |                               |                         |                                   |                            |
| Architectural details                | A6:A7                   |                               |                         |                                   |                            |
| Windows, scale and proportion        | A3                      |                               |                         |                                   |                            |
| Exposed building walls               | A4                      |                               |                         |                                   |                            |
| Materials and detailing              | A3                      |                               |                         |                                   |                            |
| <b>Public Realm and Open Space</b>   |                         |                               |                         |                                   |                            |
| Connected and accessible             | P1                      |                               |                         |                                   |                            |
| Visibility and comfort               | P2                      |                               |                         |                                   |                            |
| Neighborhood identity                | P3                      |                               |                         |                                   |                            |
| Support pedestrians and bicyclists   | P4:P5                   |                               |                         |                                   |                            |
| Social activity, play and rest       | P6                      |                               |                         |                                   |                            |
| street scape                         | P8                      |                               |                         |                                   |                            |
| <b>Sustainability</b>                |                         |                               |                         |                                   |                            |
| Respect natural systems and features | S8                      |                               |                         |                                   |                            |
| Employ sustainable building          | A9                      |                               |                         |                                   |                            |
| <b>Historic Buildings</b>            |                         |                               |                         |                                   |                            |
| Preserve historic character          |                         |                               |                         |                                   |                            |

Matrix Comparing Content of Urban Design Guidelines, Residential Design Guidelines, Special Area Guidelines, Industrial Area Design Guidelines and Historic Design Guidelines

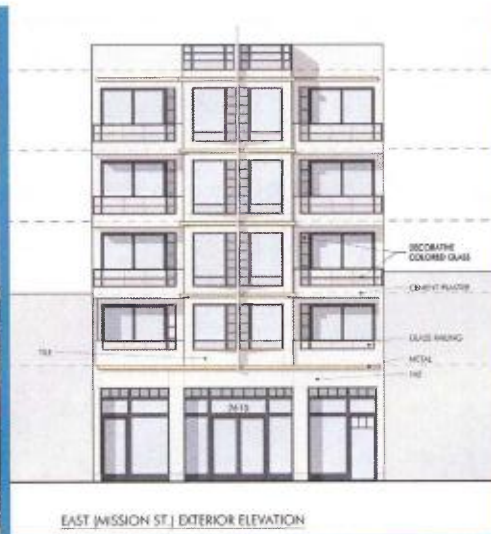




During the recent wave of applications, the Commission asked the Planning Department to develop guidelines for design.





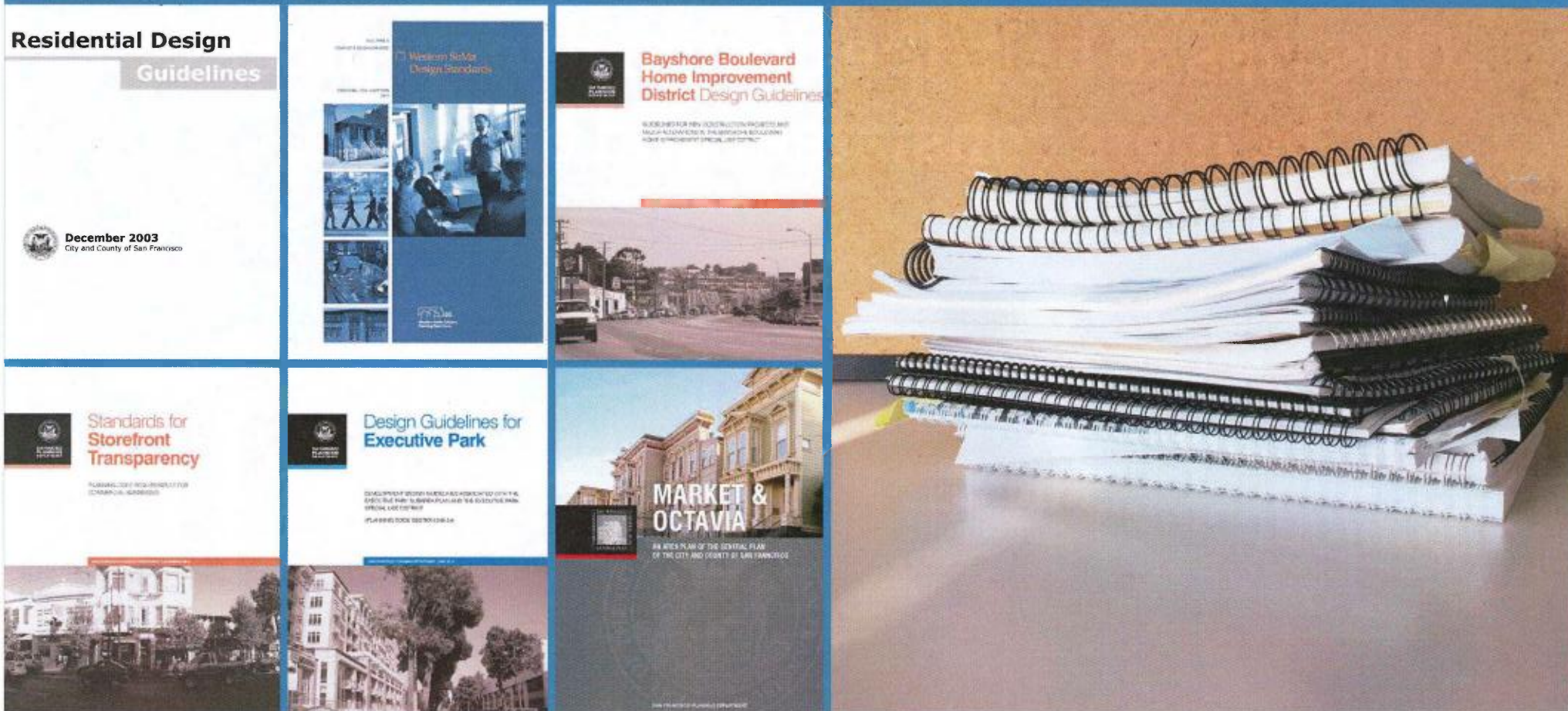


This was to help the design review process be more consistent and effective.

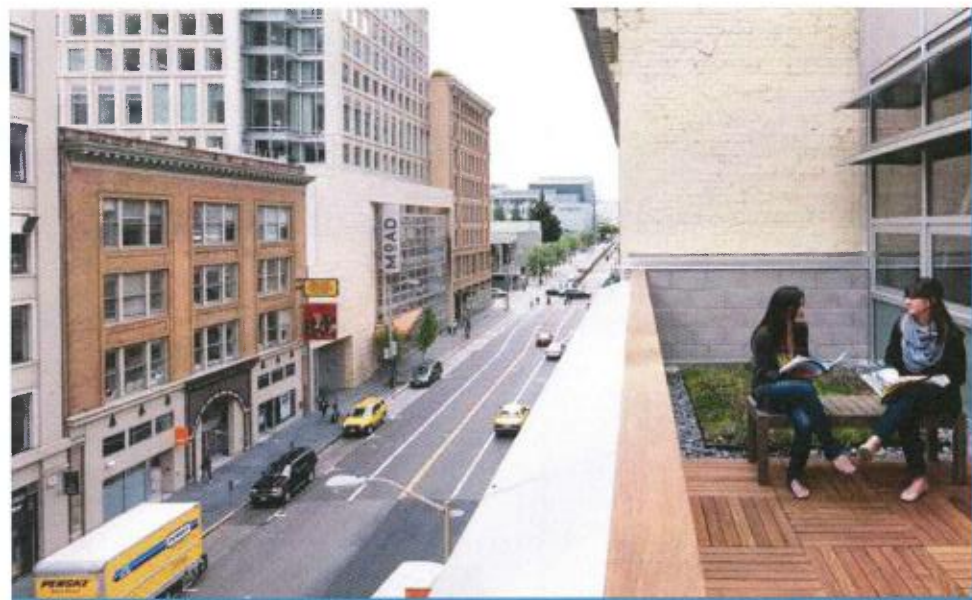




The Department studied  
the General Plan,  
the Urban Design Element, and  
the 30+ sets of existing guidelines.







Key values from this assessment:  
Neighborhood compatibility and  
Human-centered design





# Guidelines from General Plan

Attachment E page 1 of 8  
Urban Design Guidelines

Matrix of Guideline Origin from  
General Plan

| Guideline<br>Applicable Document                              | Policy Number, if applicable   | Supporting Text  |
|---|--|--|
| <b>5.1</b>  | <b>Recognize and Respond to Urban Patterns</b>                             |  |
| Urban Design Element, City Pattern                            | POLICY 1.2   | Recognize, protect and reinforce the existing street pattern, especially as it is related to topography.   |
| Urban Design Element, City Pattern                            | POLICY 1.3   | Recognize that buildings, when seen together, produce a total effect that characterizes the city and its districts.  |
| Urban Design Element, City Pattern                            | POLICY 1.7   | Recognize the natural boundaries of districts, and promote connections between districts.  |
| PRINCIPLES FOR CITY PATTERN, URBAN DESIGN ELEMENT             |  | Tall, slender buildings at the tops of hills and low buildings on the slopes in valleys accentuate the form of the hills.  |
| PRINCIPLES FOR CITY PATTERN, URBAN DESIGN ELEMENT             |  | Street spaces impart a unifying rhythm to the pattern and image of the city.   |
| PRINCIPLES FOR CITY PATTERN, URBAN DESIGN ELEMENT             |  | Certain streets, because of unusual width or direction, are important form elements in themselves, giving identity to districts and order to the city structure.   |
| PRINCIPLES FOR CITY PATTERN, URBAN DESIGN ELEMENT             |  | Uninterrupted grid streets in flat areas often result in monotonous vistas.  |
| PRINCIPLES FOR NEIGHBORHOOD ENVIRONMENT, URBAN DESIGN ELEMENT |  | Alleys and small streets which are usable as part of the general network of pedestrian and service ways are potential areas of activity and interest.  |
| Site Layout, COMMERCE AND INDUSTRY ELEMENT                    | URBAN DESIGN GUIDELINES  | The site plan of a new building should reflect the arrangement of most other buildings on its block, whether set back from, or built out to its front property lines.  |
| Transportation Element  | OBJECTIVE 25   | DEVELOP A CITYWIDE PEDESTRIAN NETWORK  |
| Transportation Element  | OBJECTIVE 26   | CONSIDER THE SIDEWALK AREA AS AN IMPORTANT ELEMENT IN THE CITYWIDE OPEN SPACE SYSTEM.  |
| Transportation Element  | POLICY 26.1  | Retain streets and alleys not required for traffic, or portions thereof, for through pedestrian circulation and open space use.  |
| Recreation and Open Space Element                             | Objective 3  | IMPROVE ACCESS AND CONNECTIVITY TO OPEN SPACE  |
| Downtown Area Plan  | POLICY 10.2  | Encourage the creation of new open spaces that become a part of an interconnected pedestrian network.  |
| Rincon Hill   | OBJECTIVE 3.7  | Reduce the present industrial scale of the streets by creating a circulation network through the interior blocks, creating a street scale comparable to those in existing residential areas elsewhere in the city.   |
| Transit Center District Plan                                  | OBJECTIVE 2.2  | Create an elegant downtown skyline, building on existing policy to craft a distinct downtown "hill" form, with its apex at the transit center, and tapering in all directions.   |
| Transit Center District Plan                                  | OBJECTIVE 2.3  | Form the downtown skyline to emphasize the transit center as the center of downtown, reinforcing the primacy of public transit in organizing the city's development pattern, and recognizing the location's importance in local and regional accessibility, activity, and density. |
| Northeastern Waterfront                                       | POLICY 3.1.1   | Adopt heights that are appropriate for the Central Waterfront's location in the city, the prevailing street and block pattern, and the anticipated land uses, while producing buildings compatible with the neighborhood's character.  |
| Northeastern Waterfront                                       | POLICY 3.1.3   | Relate the prevailing heights of buildings to street and alley width throughout the plan area.   |
| Northeastern Waterfront                                       | POLICY 3.1.4   | Heights should reflect the importance of key streets in the city's overall urban pattern, while respecting the lower scale development of Dogpatch.  |
| Executive Park Sub Area                                       | OBJECTIVE 3  | CREATE A CITY STREET PATTERN SUPPORTIVE OF AN URBAN RESIDENTIAL NEIGHBORHOOD   |
| East SoMA   | POLICY 3.1.4   | Heights should reflect the importance of key streets in the city's overall urban pattern, while respecting the lower scale development that surrounds South Park and the residential enclaves throughout the plan area.  |
| East SoMA   | POLICY 3.1.8   | New development should respect existing patterns of rear yard open space. Where an existing pattern of rear yard open space does not exist, new development on mixed-use-zoned parcels should have greater flexibility as to where open space can be located.                      |
| Showplace Square / Potrero                                    | POLICY 3.1.3   | Relate the prevailing heights of buildings to street and alley width throughout the plan area.   |
| Showplace Square / Potrero                                    | POLICY 3.1.4   | Heights should reflect the importance of key streets in the city's overall urban pattern, while respecting the lower scale development on Potrero Hill.  |
| Balboa Park Station   | POLICY 5.3.3   | Pedestrian routes, especially in commercial areas, should not be interrupted or disrupted by auto access and garage doors.   |
| Balboa Park Station   | Objective 6.4  | respect and build from the successful established patterns and traditions of building massing, articulation, and architectural character of the area and the city.   |
| Hunters Point Shipyard  | POLICY 1.5   | Acknowledge history as part of the land use and urban design plan  |
| Hunters Point Shipyard  | POLICY 3.2   | Ensure a block pattern and street network that relates to adjacent neighborhood, is coherent, and provides the development with organization and orientation   |
| <b>5.2</b>  | <b>Harmonize Relationships between Buildings, Streets, and Open Spaces</b> |  |
| Urban Design Element, City Pattern                            | POLICY 2.6   | Respect the character of older development nearby in the design of new buildings.  |
| PRINCIPLES FOR CITY PATTERN, URBAN DESIGN ELEMENT             |  | Strong and organized development adjacent to parks creates an effective contrast and makes the street space between the two a pleasing space to be in.   |
| PRINCIPLES FOR CITY PATTERN, URBAN DESIGN ELEMENT             |  | Wide streets with low and/or scattered buildings are poorly defined and do not contribute to an orderly city pattern and image.  |
| PRINCIPLES FOR CITY PATTERN, URBAN DESIGN ELEMENT             |  | Green space closing a street provides an accent on an upper slope or top of hill.  |
| Site Layout, COMMERCE AND INDUSTRY ELEMENT                    | URBAN DESIGN GUIDELINES  | New development should respect open space corridors in the interior of blocks and not significantly impede access of light and air nor block views of adjacent buildings.  |
| Site Layout, COMMERCE AND INDUSTRY ELEMENT                    | URBAN DESIGN GUIDELINES  | On irregularly shaped lots, through-lots or those adjacent to fully-built lots, open space located elsewhere than at the rear of a property may improve the access of light and air to residential units.  |
| Scale, Height and Bulk, Commerce & Industry Element           | URBAN DESIGN GUIDELINES  | The height and bulk of new development should be designed to maximize sun access to nearby residential open space, parks, plazas, and major pedestrian corridors.  |
| Downtown Area Plan  | OBJECTIVE 13   | CREATE AN URBAN FORM FOR DOWNTOWN THAT ENHANCES SAN FRANCISCO'S STATURE AS ONE OF THE WORLD'S MOST VISUALLY ATTRACTIVE CITIES.   |
| Downtown Area Plan  | POLICY 13.1  | Relate the height of buildings to important attributes of the city pattern and to the height and character of existing and proposed development.   |



# Guidelines from Existing Guidelines

| Guideline   | Supporting Text  |
|---|--|
| Applicable Document   |  |
| <b>S1</b>   | <b>Recognize and Respond to Urban Patterns</b>   |
| Affordable Housing Bonus Program Design Guidelines                    | Design a site plan that is harmonious with the characteristics found with the district. Avoid unnecessary contrast with historic fabric in form or building articulation, to maintain the integrity and character of the site and its context.   |
| Cow Hollow Neighborhood Design Guidelines                             | Side spacing: Respect spacing pattern  |
| Design Guidelines for Executive Park                                  | Reflect fine-grained block pattern typical of San Francisco; Generally, new blocks should be no larger than a typical San Francisco 200-foot by 600-foot block. Smaller blocks are encouraged. Larger blocks should provide publicly accessible pedestrian paths through the block.  |
| Design Guidelines for Executive Park                                  | Open spaces should be part of a larger network of pedestrian connections that help lead residents and visitors through the neighborhood and connect to larger City and regional open space resources such as Bayview Hill Open Space and Candlestick Point State Recreation Area.  |
| Industrial Area Design Guidelines                                     | New buildings must maintain a mid-block open space pattern where such a pattern exists   |
| Residential Design Guidelines   | Respect the existing pattern of building entrances.  |
| Western SoMa Design Standards   | Reinforce existing patterns and encourage designs that create future opportunities for at grade mid-block landscaped open space by strict adherence to rear yard requirements.   |
| Western SoMa Design Standards   | Buildings and building frontages should provide variety along a block, but remain consistent with the overall urban design.  |
| Westwood Park Association Specific Area Residential Design Guidelines | Site: The topography and location of the project lot and the position of the building on that site guide the most basic decisions about design. The location, front setbacks, rear yards, side spacings will be particularly important to the adjacent neighbors and for maintaining or creating rhythm along the block-face, and maintaining a sense of common open space in the interior of the block. |
| Affordable Housing Bonus Program Design Guidelines                    | The facades of new buildings should extend patterns.   |
| <b>S2</b>   | <b>Harmonize Relationships between Buildings, Streets, and Open Spaces</b>   |
| Affordable Housing Bonus Program Design Guidelines                    | Buildings on sloping sites should follow the slope to reinforce and accentuate the city's natural topography and maintain a strong relationship to the street.   |
| Affordable Housing Bonus Program Design Guidelines                    | For buildings on slopes, the ground floor and building entries should step-up in proportion to the slope between façade segments.  |
| Cow Hollow Neighborhood Design Guidelines                             | Rear yards: Respect rear yard and adjacent buildings   |
| Design Guidelines for Executive Park                                  | Streets should be connected to publicly accessible rights-of-way at both ends (there should be no dead-ends or cul-de-sacs), including connections to streets, alleys, pathways or open spaces.  |
| Design Guidelines for Executive Park                                  | Where provided, alleys should not only be used for service functions, but should also be designed for all uses and to be pedestrian-friendly, attractive, and safe.  |
| Design Guidelines for Executive Park                                  | Relationship between built form and public realm   |
| Design Guidelines for Executive Park                                  | Building size should be proportional to the scale of streets, alleys and pathways to allow a well-defined streetwall while still allowing adequate sun access and sky to the ground.   |
| Design Guidelines for Executive Park                                  | On residential neighborhood streets, building streetwalls should generally be no taller than the width of the right-of-way, or where there are consistent setbacks, the width between setback lines across the street from each other  |
| Industrial Area Design Guidelines                                     | create an urban building scale and relationship of development to streets  |
| Residential Design Guidelines   | Design building facades to enhance and complement adjacent public spaces.  |
| <b>S3</b>   | <b>Recognize and Enhance Local Variations</b>  |
| Bayshore Boulevard Home Improvement District                          | Building form should celebrate corner locations. Special design elements and architectural features are encouraged, and special entries should be used strategically at street intersections and near important transit nodes.   |
| Cow Hollow Neighborhood Design Guidelines                             | Topography & Views: Emphasize Corner Buildings   |
| Cow Hollow Neighborhood Design Guidelines                             | Setbacks: Acknowledge Significant Neighboring Buildings  |
| Design Guidelines for Executive Park                                  | Buildings should define and highlight corners, important public spaces, and public vistas such as street terminations.   |
| Design Standards for Storefronts in the KMMMS Conservation District   | Emphasis of Corner Lot: Corner entrances, storefront windows, and displays that extend along both street façades are examples of elements that emphasize corner lot locations and are encouraged.  |
| Industrial Area Design Guidelines                                     | preserve the Dogpatch Neighborhood's existing character (roughly bounded by Mariposa Street on the north, 25th Street on the South, Pennsylvania on the west, and 3rd Street on the east)  |
| Industrial Area Design Guidelines                                     | Identify cultural resources and develop policies to protect them   |
| Industrial Area Design Guidelines                                     | Improve the visual quality, and strengthen the pedestrian orientation, of the Third Street core area   |
| Industrial Area Design Guidelines                                     | recognize and enhance the distinctive features of South Bayshore as an interlocking system of diverse neighborhoods  |
| Industrial Area Design Guidelines                                     | achieve a visually attractive design which reflects the character of a distinct urban neighborhood oriented toward education, arts, and industry   |
| Industrial Area Design Guidelines                                     | provide continuity with the community's history and culture by conserving and enhancing historic resources   |
| Market & Octavia Area Plan: Fundamental Design Principles             | Special building elements and architectural features such as towers and special entries should be used strategically at street intersections and near important public spaces.   |
| Market & Octavia Area Plan: Fundamental Design Principles             | Building entries and shop fronts should add to the character of the street by being clearly identifiable and inviting.   |
| Residential Design Guidelines   | In areas with a defined visual character, design buildings to be compatible with the patterns and architectural features of surrounding buildings.   |
| Residential Design Guidelines   | In areas with a mixed visual character, design buildings to help define, unify and contribute positively to the existing visual context.   |
| Residential Design Guidelines   | Provide greater visual emphasis to corner buildings.   |
| Western SoMa Design Standards   | Architectural detail should reflect the "warehouse" character of the neighborhood regardless of the proposed uses, but use typical residential architectural vocabulary at residential levels is allowed.  |
| <b>S4</b>   | <b>Create, Protect, and Support View Corridors</b>   |
| Cow Hollow Neighborhood Design Guidelines                             | Tree selection and placement for views   |
| Design Guidelines for Executive Park                                  | Street should be designed for multi-modal use with the street design physically reinforcing slower auto traffic speeds.  |
| Design Guidelines for Executive Park                                  | Buildings over 85 feet in height should be slender and adequately spaced in order to allow sunlight and sky access to streets and public spaces, to preserve views through the district to San Francisco Bay and to Bayview Hill.  |
| Industrial Area Design Guidelines                                     | respect public view corridors  |
| Industrial Area Design Guidelines                                     | maximize the opportunity for views within the neighborhood and promote the preservation and enhancement of views from adjacent neighborhoods   |
| Residential Design Guidelines   | Protect major public views from public spaces.   |

Attachment F page 1 of 9  
Urban Design Guidelines

Matrix of Guideline Origin from  
Existing Guideline Documents

# Outreach and Working Group

Staff worked with identified neighborhood groups, design and development professionals, agency staff, and Commissioners.

| Urban Design Advisory Group Comments - 15 August 2016                      |  |
|--|--|
| <p>Item Built<br/>Comments<br/>Notes for the City<br/>of San Francisco</p> | <p>I suggest that you use an image that provides a resolution of the "relationship" that the guidelines seek to achieve, and that image should be a photograph, not a 3D rendering. I suggest that you use a photograph of a building that is a good example of the relationship that the guidelines seek to achieve. I suggest that you use a photograph of a building that is a good example of the relationship that the guidelines seek to achieve. I suggest that you use a photograph of a building that is a good example of the relationship that the guidelines seek to achieve.</p> <p>The three goals are excellent but read as different rules - for instance the 3 goals referenced in being a Good Neighbor could become better, much like the components of "Supporting Human Needs" are called out - although those seem too long for the introduction. Building sustainably doesn't seem "heavy" enough compared to the other two. TM</p> <p>2nd para: "Buildings that strongly distinguish..." This phrase and the 3rd para tends to overemphasize the concept and can lead to keeping everything the same as before. The 3rd para should emphasize the context of new buildings being of their time in history. But buildings which strongly distinguish themselves can become buildings of greater public significance such as the Transamerica pyramid and Mission Drive ABSP.</p> <p>A sentence might be added that encourages creative and innovative design through the flexibility of the DDO's. Since the DDO's are subjective artworks and designs should be encouraged to raise the design bar and create new and innovative ideas. This could take place during Stage One. Another item that may be covered in other policy or law, however, there are a number of institutions, campuses and multi-story developments, yet there is no mention regarding how the UDO's facilitate campus and master plans. These larger developments occur over long periods of time and can influence neighborhood context and form of the city. LC</p> <p>Suggest balancing importance of consultation with understanding that new construction is inherently different and essential components need to be reached. Specifically, and "buildings" or some other modifier to "buildings that strongly or unduly distinguish themselves..." Suggest using "buildings" instead of "buildings" in fourth paragraph of "Being a Good Neighbor". Overall, some sentences are too generic (first sentence-what kind of construction? enough? there is nothing in the value about quality of time, attention, beauty, shouldn't this be important alongside being a good neighbor? "Culture &amp; Social Well-being" first three sentences seem a bit broad of statements and actually phrased, suggest beginning from "Good urban design..." LC</p> <p>In the Exterior section, this sounds too much like it's only about the wealthy and developers who have budget options. In the Culture Section: A lot of people come here because of its unique and beautiful physical setting, proximity to nature and open space, and climate. Preservation and enjoyment of open space is an integral part of San Francisco life. ABSP</p> <p>Annotation: "small" and "smaller" is used. All Annotation: what do you mean by small? RM</p> <p>Comment: "and 'proportion' before 'but not...' " JS</p> <p>Comment: "aggressive in scale and type" RM</p> <p>Context: Do you mean the site in its setting and historic context? RM</p> <p>Context: "use" or how a proposed building may be described when in its surroundings. The design context of a building may evaluate, represent or contrast with its surrounding environment. JS</p> <p>ADD Character: Preserving existing architectural elements, including building mass, scale and form they were built. JS</p> <p>POROS: and separate definitions for "Public Open Space" and "Private Open Space" RM</p> <p>Private Open Space: Outdoor space dedicated to a particular user or occupant of a building. JS</p> <p>Paraphrase add a definition for human scale which relates to POROS, Building Context, Orientation. LC</p> <p>Streetscape: "urban roof" as spaces between or . 6' and "urban" provides, "rather than helps encourage" RM</p> <p>Low the questions throughout, except this Christopher Alexander question. LC</p> <p>ABSP: Add definition of "TYPE" Concept mentioned in "complement" definition.</p> <p>ARTICULATION: this applies to only a particular kind of expression "textures" and "textures" are also types of expression PAGE 8, 1st paragraph I think it's important here that articulation is not understood as just "façade" on the building. The paragraphs should conclude with "...that support the overall design intent." "Changes" could be changed to "definition of Articulation" Consider the use of "variation" in lieu of "changes" when appropriate. The definitions might be expanded to use more additional considerations such as those found in the DDO's of Candlestick Point, Mission Bay and other similar design guidelines. Examples could include Delicate, Built-to-last, Proportion, Day forms.</p> <p>GLAZING: and glass walls, glazed doors?</p> <p>ADD "PROPORTION" This is a key concept used in the documents.</p> <p>HARMONIZE: In music, painting and other arts forms, relationships can be as important as harmony. Is there a way to leave an opening for the value of difference? Perhaps say "make meaningful relationship" instead of "harmonize"?</p> |
|  | <p>Change photo, use 3D photo, three images more closely capture the intent of the guidelines</p> <p>Agree selected, and incorporated in Application section</p> <p>Images made, in order to round (balanced) and as one note. Sustainable section looked up</p> <p>edit (buildings of their time and place. Substantive update for clarity)</p> <p>Incorporated: See "Applicability" Section</p> <p>Agree in principle. Also paragraph rewritten. More discussion if necessary.</p> <p>This is about long-term quality and value, not real estate. Rewritten to respond to comment. Agree and have incorporated.</p> <p>revised definition of "neighborhood"</p> <p>revised definition of "context"</p> <p>revised definition of "context" to include this</p> <p>revised definition of "character"</p> <p>revised definition of human scale</p> <p>as the E</p> <p>revised definition of "articulation"</p> <p>revised definition of "articulation" to address comment</p> <p>included and incorporated</p> <p>edit direct "make meaningful relationship"</p>  |

Attachment I page 1 of 14  
Urban Design Guidelines

Matrix of Urban Design Advisory  
Group Comments on 2016 Draft



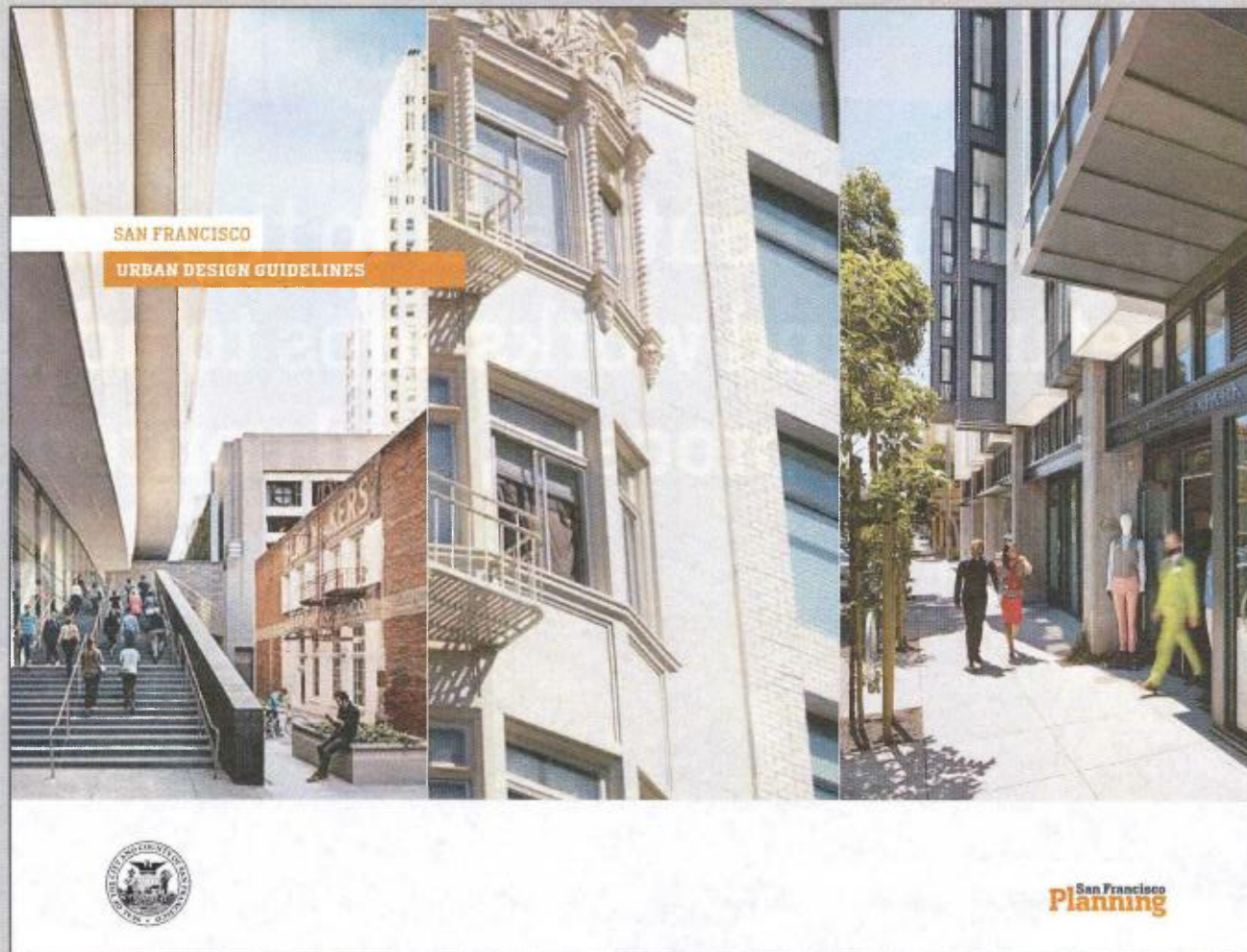


**Staff broadened outreach to the public with meetings and workshops to address applicability, process, and content.**





# Presenting the Urban Design Guidelines for adoption





# Content



**Site Design**



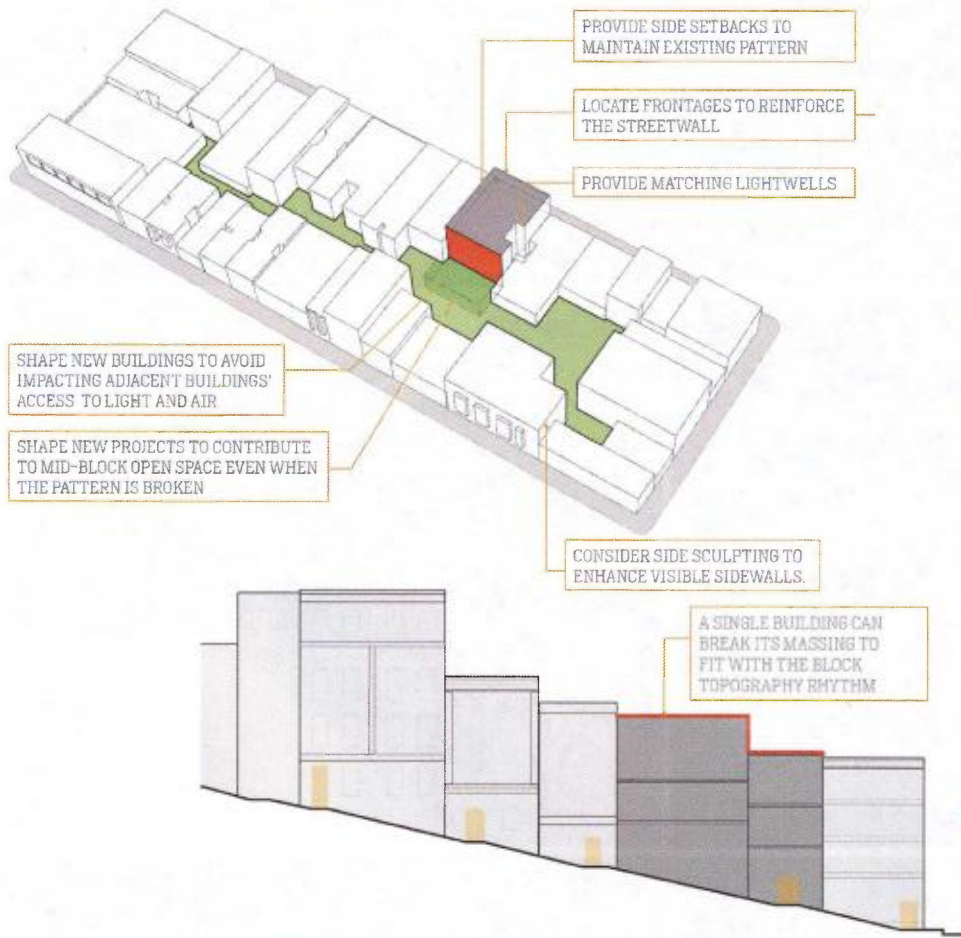
**Architecture**



**Public Realm**



# Site Design



Match streets,  
open space, and  
massing

Highlight unique  
patterns

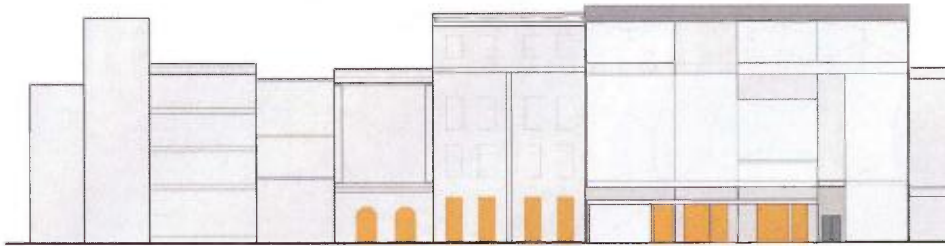
Keep natural  
features, like  
topography

Maintain a  
consistent street  
wall





# Architecture



Have a design intention

Respond to neighborhood form & materials

Relate to nearby heights & widths

Use similar window sizes and shapes



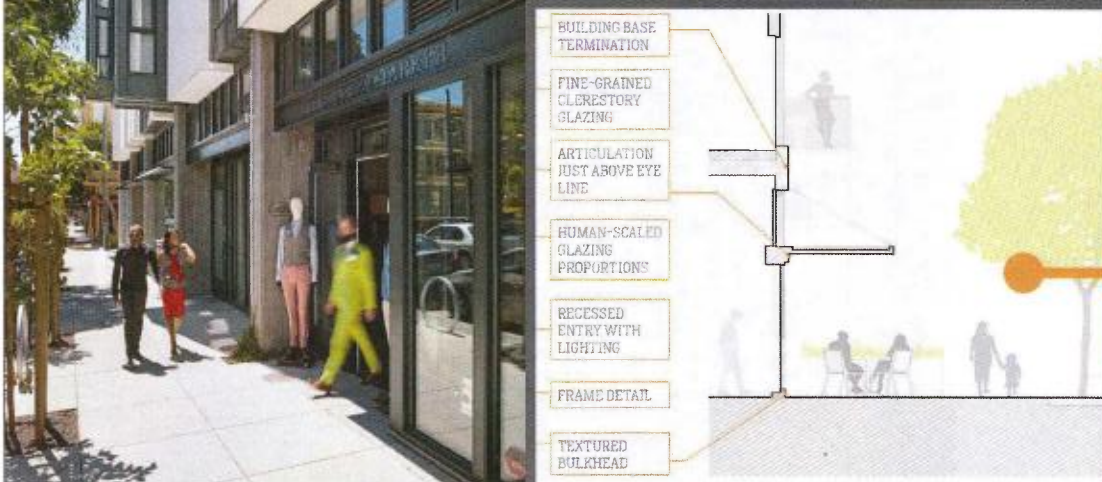
# Architecture



Provide a facade with texture and human-scale



Design the top to stop thoughtfully at the sky



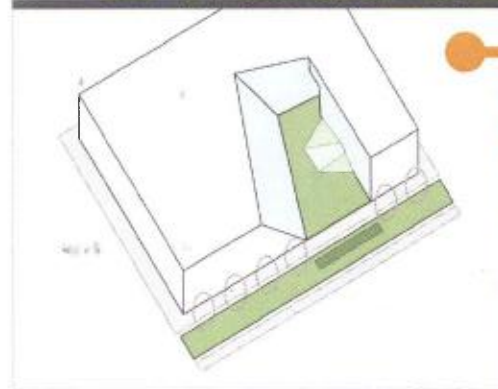
Design the base to connect to people at the ground



# Public Realm

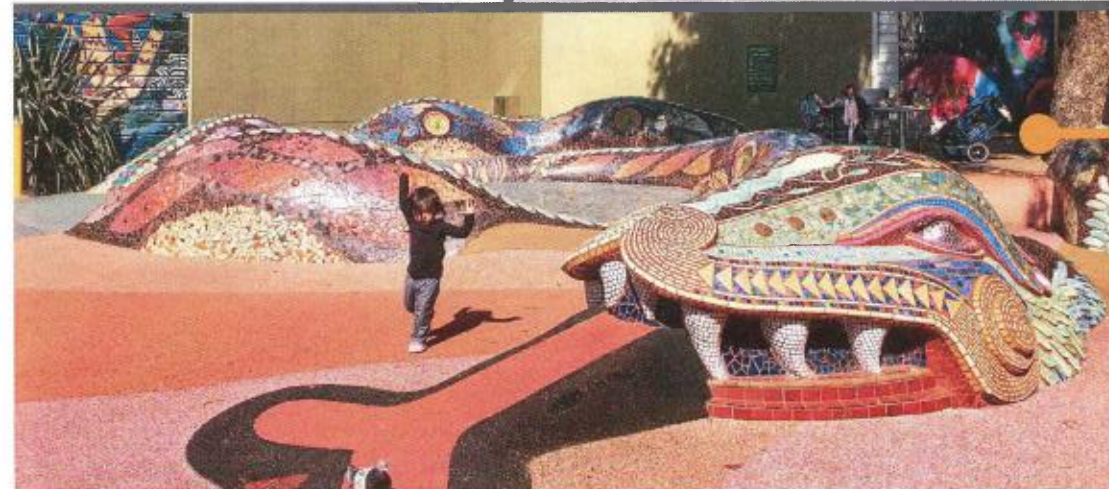


Connect to  
existing public  
spaces



Support public  
transportation

Foster walking,  
play, and rest



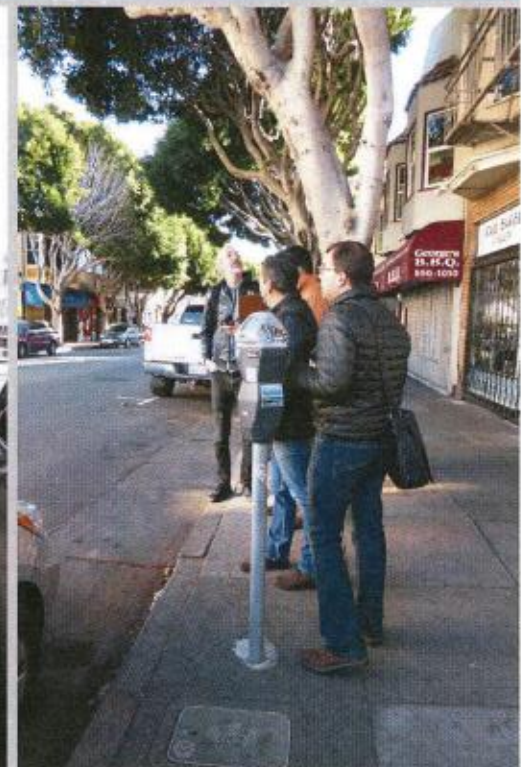
Express  
neighborhood  
uniqueness



# Special Area Guidelines

Underway for North Beach, Polk / Pacific  
Avenues, Calle 24, and Japantown

The resolution confirms the commitment to  
this program of guidelines.

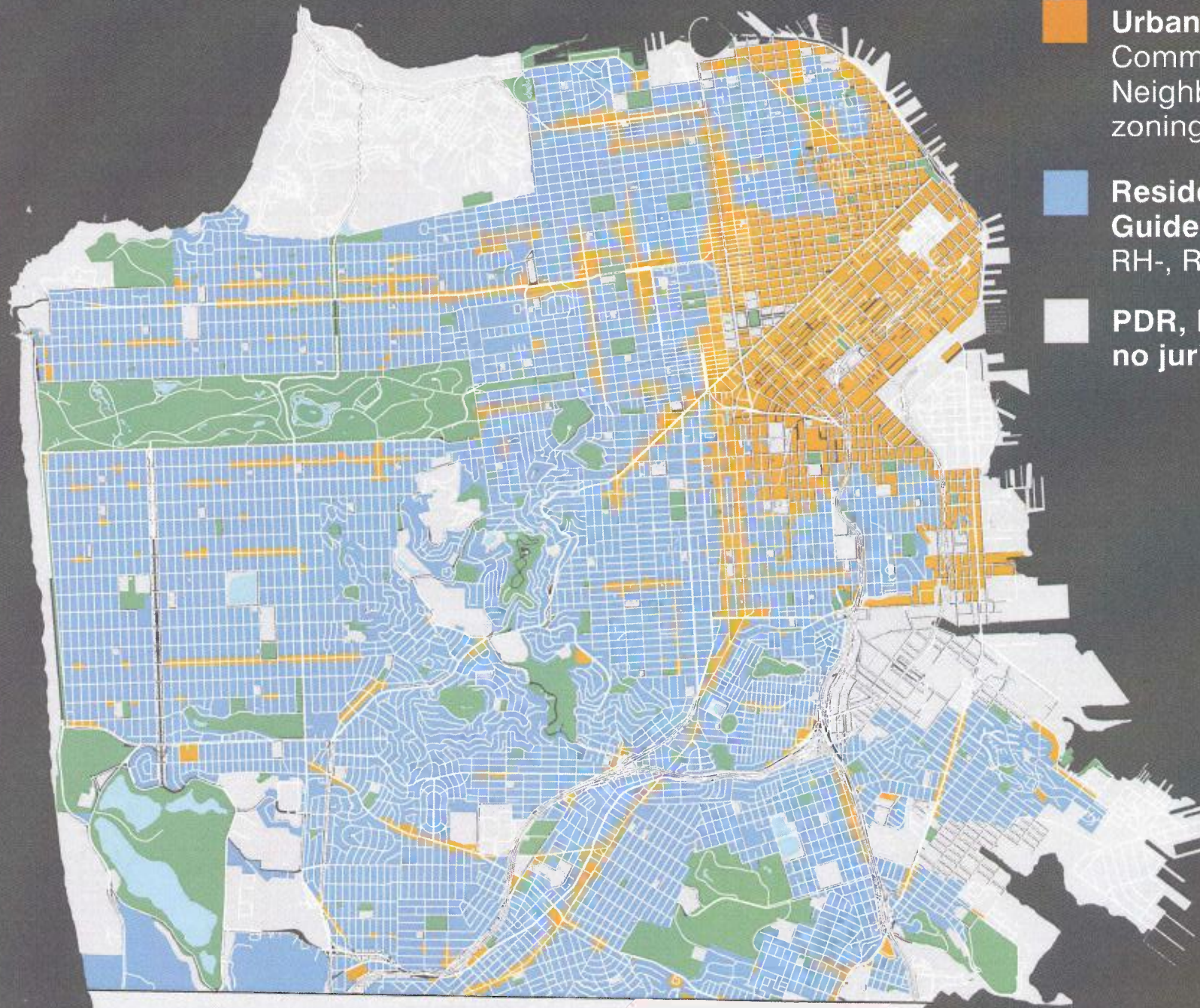





REVIEW DRAFT  
10.27.2017

San Francisco  
Planning



# Applicability

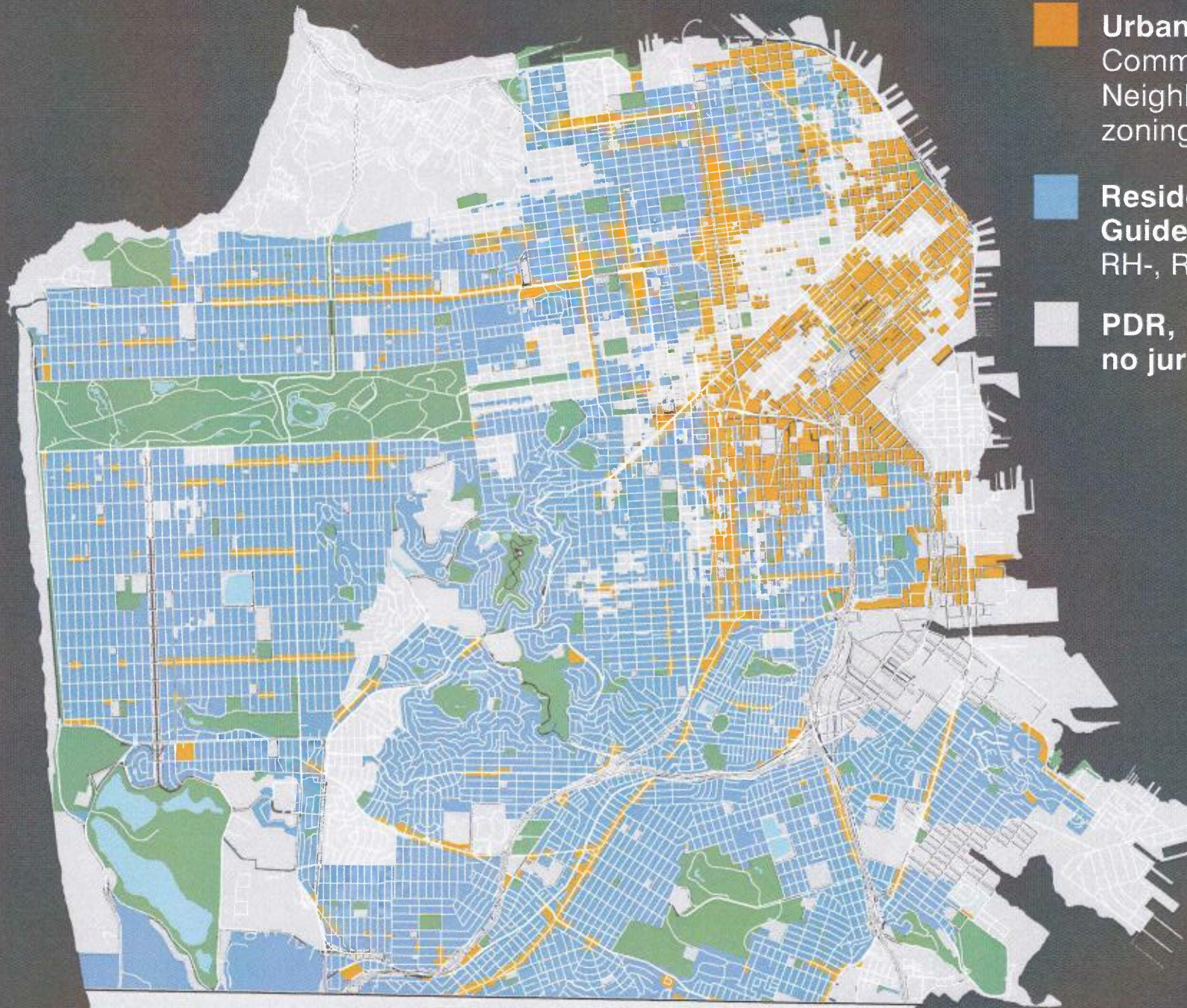


-  **Urban Design Guidelines**  
Commercial, Mixed Use,  
Neighborhood Commercial  
zoning
-  **Residential Design  
Guidelines**  
RH-, RM-, RTO zoning
-  **PDR, M, P zoning or  
no jurisdiction**



# Applicability

Not including Historic Districts



**Urban Design Guidelines**  
Commercial, Mixed Use,  
Neighborhood Commercial  
zoning

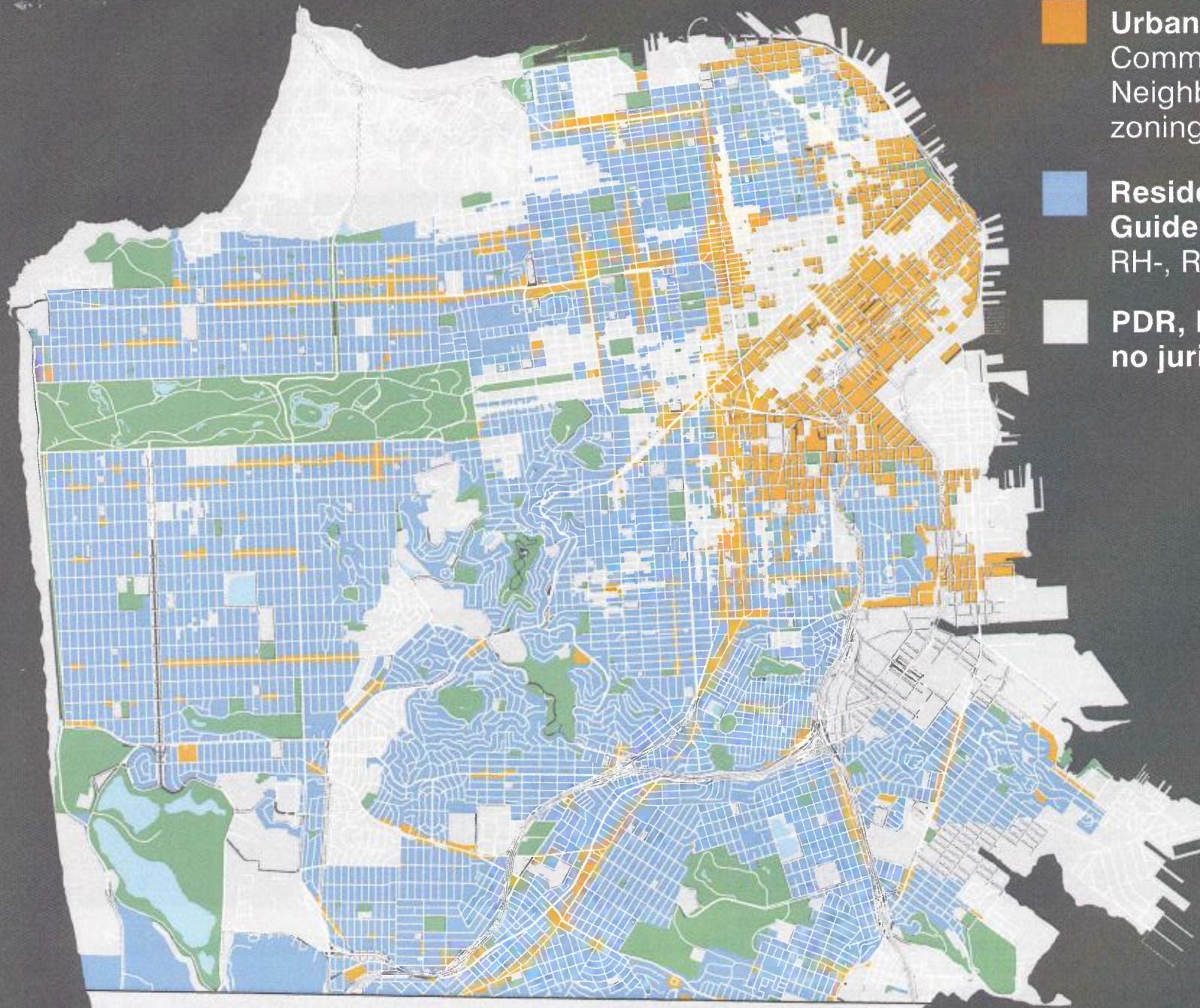
**Residential Design  
Guidelines**  
RH-, RM-, RTO zoning




**PDR, M, P zoning or  
no jurisdiction**



# Applicability

Not including Historic Districts and delayed applicability in  
Columbus, Broadway, Polk and Pacific NC Districts



-  **Urban Design Guidelines**  
Commercial, Mixed Use,  
Neighborhood Commercial  
zoning
-  **Residential Design  
Guidelines**  
RH-, RM-, RTO zoning
-  **PDR, M, P zoning or  
no jurisdiction**



# Applicability

|  | Outside of<br>RH-, RM-,<br>RTO-, M-, and<br>PDR- Districts | Areas with<br>Special<br>Area Design<br>Guidelines | Areas with<br>Other<br>Existing<br>Guidelines <sup>1</sup> | R-District<br>Larger<br>Projects <sup>2</sup> | Historic<br>Districts |
|--|--|--|--|---|-----------------------|
| <b>Historic<br/>Guidelines</b>                           | IF IN A HISTORIC<br>DISTRICT                               | IF IN A HISTORIC<br>DISTRICT                       | IF IN A HISTORIC<br>DISTRICT                               | IF IN A HISTORIC<br>DISTRICT                  |                       |
| <b>Residential<br/>Design<br/>Guidelines</b>             |  |  |  |   |                       |
| <b>Other<br/>Neighborhood<br/>Guidelines<sup>1</sup></b> |  |  | AS APPLICABLE  |   |                       |
| <b>Special<br/>Area Design<br/>Guidelines</b>            |  |  |  | IF DEVELOPED FOR<br>THAT AREA                 |                       |
| <b>Urban<br/>Design<br/>Guidelines</b>                   |  |  | IF UDGS APPLY  | IF UDGS APPLY                                 | IF UDGS APPLY         |

1 - Applicable Guidelines are those formally adopted, identified for specific areas

2 - Applies to non-residential projects, or to projects that have either twenty-five units or more or a frontage longer than 150' feet. This provision will sunset once a revision to the Residential Design Guidelines is adopted.



# Applicability

Not including Historic Districts

|  | Outside of<br>RH-, RM-,<br>RTO-, M-, and<br>PDR- Districts | Areas with<br>Special<br>Area Design<br>Guidelines | Areas with<br>Other<br>Existing<br>Guidelines <sup>1</sup> | R-District<br>Larger<br>Projects <sup>2</sup> |
|--|--|--|--|---|
| <b>Residential<br/>Design<br/>Guidelines</b>             |  |  |  |   |
| <b>Other<br/>Neighborhood<br/>Guidelines<sup>1</sup></b> |  |  | AS APPLICABLE  |   |
| <b>Special<br/>Area Design<br/>Guidelines</b>            |  |  |  | IF DEVELOPED FOR<br>THAT AREA                 |
| <b>Urban<br/>Design<br/>Guidelines</b>                   |  |  | IF UDGS APPLY  | IF UDGS APPLY                                 |

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# Public Comment

**Conversations have clarified that:**

- » **The UDGs do not ask for projects to be designed in the same way across the city.**
- » **They do not change zoning, height, the General Plan nor any of the existing guidelines.**



# Public Comment

Conversations have resulted in:

- » adding matching lightwells, side setbacks
- » adding reduction in rear glazing and lighting
- » adding more NC District examples
- » modified applicability of UDGs in R zoning
- » removing the waiver provision
- » Special Area Guidelines



# Public Comment Documentation

## Urban Design Guidelines Outreach Consolidation 14 March 2018

| Submitted by                           | Topic / Subtopic       | Comments   | Response   |
|--|------------------------|--|--|
| Middle Polk                            | Overview               | In 10 words, what is design review?  | Design review is an iterative process to regulate design so that projects respond to neighborhood scales and patterns in the most effective and highest quality possible.  |
| Middle Polk                            | Overview               | What is Planning's responsibility and what is the design review process?   | The Planning Department doesn't design buildings, rather regulates design. Typically the city process is to review and refine projects to get them to a place that is compatible and consistent with citywide goals and aspirations. Those goals and aspirations include compatibility with the neighborhood scale, defining general qualities of a place, and providing feedback that is timeless in a set of core principles.  |
| Middle Polk                            | Overview               | What are the guidelines?   | Compilation of design principles and standards from 30+ different documents including area plan guidelines, general plan principles, and urban design element guidelines. They are the core truths that apply to large scale buildings across the city.  |
| Middle Polk                            | Overview               | What do other cities do for design review?   | Portland has 12 staff dedicated. Seattle design review happens in the field with physical walks to sites with neighborhood groups. NYC and Chicago's design review is not substantial.   |
| Jim Billings                           | Overview               | Will this make the whole city look the same?   | The intent of the guidelines is not to create a homogenous character throughout San Francisco, but instead to allow for flexibility while complying with the overall urban design principles for the city. Several of the guidelines ask that the proposed project responds to the unique neighborhood character of its site. First and foremost the urban design guidelines ask a project to respect the context of the neighborhood. For example, guideline S7 (Recognize and Enhance Local Variations), A4 (Harmonize Building Designs with Neighboring Scale and Materials), and P3 (Express Neighborhood Character in Open Space Designs), share the same goal for the project to reflect the individuality and interesting architectural aspects of adjacent buildings and neighborhood character. |
| Jim Billings                           | Overview               | How does this affect height, bulk, and density?  | The Urban Design Guidelines do not change height, bulk, or density requirements. These regulations are a separate matter from the guidelines. Guideline S2 "Harmonize Relationships between Buildings, Streets, and Open Spaces" is an example of a guideline that asks a project to consider its adjacent buildings scale, massing, and proportions.  |
| Anastasia Yovanovoulos, Niss Valley    | Overview               | Clarify introduction. Explain more about why the guidelines have been devised, what they are, and how to use them, who will be served, where they apply, what benefit they are to users, what purpose they potentially serve to communities, neighborhoods, districts. Maps to direct.   | The introduction contains: a background on the document and why it is needed, brief text on our overall policy goals (sustainability, quality of life, culture of San Francisco), a background on the origin of the guidelines, applicability, procedures and how they are related to the Planning Code, who will be using them and the design review process, and the structure of the guidelines. Maps for applicability are also available on our webpage.  |
| Telegraph Hill Dwellers                | Overview               | Is the intent that the UDGs are prescriptive or aspirational?  | Compliance with the UDGs will be mandatory, but the means of compliance will not be mandatory. Each of the 23 guidelines includes a rationale and a series of potential means for achieving the guideline, but the document cannot be exhaustive to cover every potential way of meeting the guideline.  |
| Eureka Valley                          | Overview               | The handout does not address the "living spirit of the city." Does the document envision the city of the future without regard to the city of the past? "Humans" start from how it feels while "professionals" start with the buildings.   | The opening of the document describes the Built Environment Values for the city, which include valuing the specific context and variations that distinguish one place from another. A number of guidelines address these ideas including S7: Recognize and enhance local variations; A4: Harmonize building designs with neighboring scale and materials; and P3: Express neighborhood character in open space designs.  |
| Eureka Valley                          | Overview               | In the world, there are no standards for aesthetics.   | Agreed, but the intent of the UDGs, by starting with values and carefully defining terms, is to remove as much subjectivity as possible.   |
| Ocean Avenue                           | Overview               | Overall supportive   | Follow-up on specific ocean avenue street life design guidelines.  |
| Victorian Alliance                     | Overview               | Overall supportive   | Overall in HDGs.   |
| CCN                                    | Overview               | Introduction text to add: Designing Urban Landscapes with Children, Youth and Families in Mind: The size of a child's unofficial realm (ex. nature, backyards, and marginal landscapes) has diminished over the years due to many factors. Currently the majority of places to connect and play in a natural setting are official areas (ex. parks and schoolyards) which are often primarily constructed of human made materials. There is now a global movement in cities to reframe childhood and nature, to create new types of places where children can enjoy nature play and connection. Viewed as a genetically driven process of learning about self and surroundings across the millennia of human history, such experiences can be considered a childhood right. Natural settings for children, youth and families that previous generations took for granted must now be deliberately created in urban environments. | While we appreciate the intent, in response to other outreach concerns, we have reduced the overall built environment values description in the preface and therefore unfortunately are not addressing more specific considerations. Our Family Friendly team at the Planning Department is currently working on a draft document that is a Design Resource Guide for Housing for Families with Children that will include many of these ideas and comments.   |
| CCN                                    | Overview               | Additions to the glossary including: Nature play and learning places; Ecosystem thinking; Beneficial risk.   | The glossary is constrained to terms used multiple times in the guidelines.  |
| Chinatown Community Development Center | Overview               | Too prescriptive/one-size-fits-all. The idea of "visual richness" or "compositional clarity" should be open to broader interpretation.   | Understood. The Team will be working to integrate neighborhood specific call-outs.   |
| Russian Hill Community Association     | Overview               | Primacy of context and neighborhood character. With the pressure of in-fill housing and expansion of post-earthquake residences, the impact of these proposed projects requires consideration of the surrounding residences. When a residence is raised a story or two, the impact on an adjacent residence's light well must be considered. And although neighboring residences may be non-complying/non-conforming – because they were built in 1908 or 1909 or 1914 – the impact of balconies of a proposed project on the residences to the rear must be considered. In our neighborhood, 80 Russell Street is the poster-child for lack of consideration of context.  | HDGs consider this aspect during seismic retrofits. The UDGs don't call out specific scopes of work, instead ask for projects to demonstrate how they will comply with guidelines.   |
| Russian Hill Community Association     | Overview               | Primacy of transparency and integrity. We appreciate that you have listened to various neighbors and neighborhood organizations over the review process of the UDG. What is critical is knowing that you have heard us. The proof that you have heard the concerns of the community will be revealed with the next iteration of the UDG.   | Noted.   |
| North Beach Business Association       | Overview               | Dictating universal design guidelines to diverse neighborhoods is not desirable.   | Noted.   |
| Rose Hilson/Jordan Park                | Overview               | When will GFRDGs be worked on? Will they apply to all residential districts rather than to where the documents says they apply to today?   | Staff will create a larger framework to explain design review and the design guidelines process  |
| Victorian Alliance                     | Overview               | Interested in historic design guidelines document  | Follow-up with specifics on HDGs process. Involve early on.  |
| Japantown                              | Specific Nhood Context | Coordinate creation of Japantown guidelines and the UDGs so there is no redundancy   | Japantown will wait for UDGs to come out, then will create focused guidelines  |
| Japantown                              | Specific Nhood Context | Coordinate creation of context statements  | Japantown may craft a context statement similar to ones that exist in General Plan already   |
| Japantown                              | Specific Nhood Context | Group will test projects in their neighborhoods on the guidelines to see how they work and will provide feedback   | UDG Team can follow-up   |
| Miraloma Park                          | Specific Nhood Context | Certain materials not traditionally used in Miraloma Park do not weather well in the damp marine climate (such as glossy stained wood or perforated metal).  | It is helpful to staff to have such concerns noted by community members.   |
| Ocean Avenue                           | Specific Nhood Context | We want to encourage small storefronts, etc  | UDGs cannot regulate interior space sizes, but can ask for vertically modulated facades and active storefronts. By using historic storefronts as a model, the guidelines can ask for that level of detail without replicating historic architecture  |

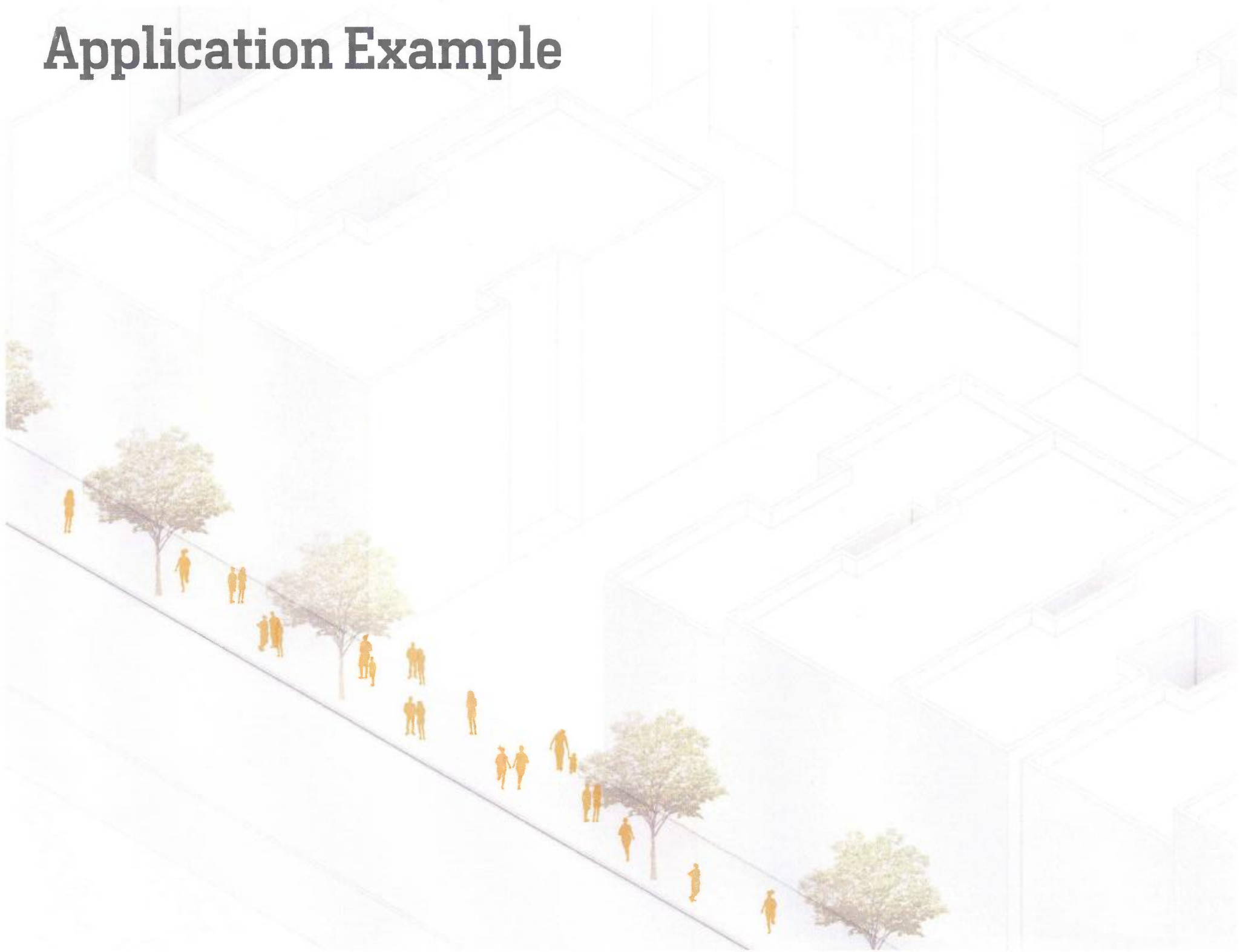
Attachment H page 1 of 12

Urban Design Guidelines

Matrix of All Public Comment and Department Responses

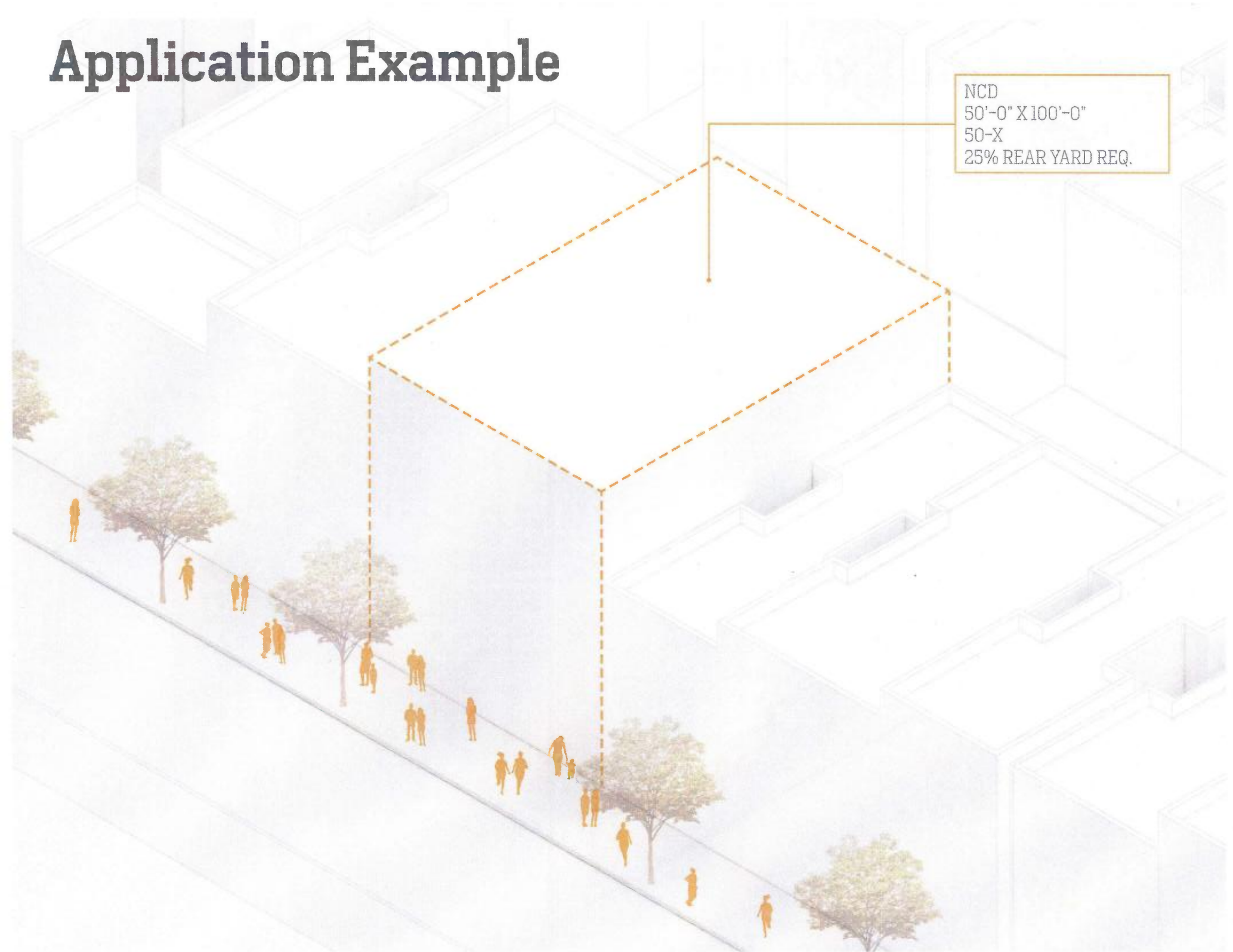


# Application Example



# Application Example

NCD  
50'-0" X 100'-0"  
50-X  
25% REAR YARD REQ.





# Application Example

S2

**HARMONIZE RELATIONSHIPS BETWEEN BUILDINGS, STREETS, AND OPEN SPACES**

S5

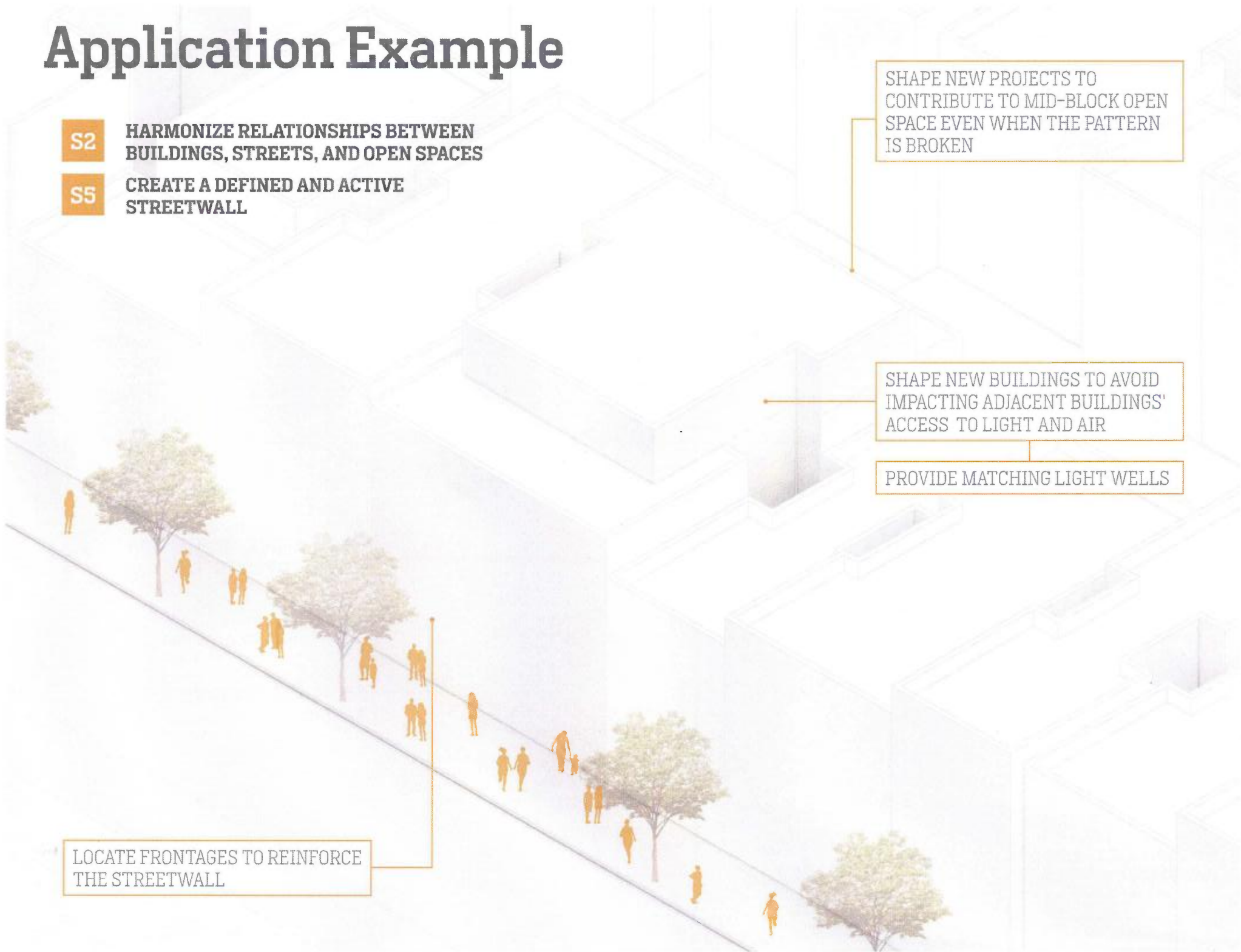
**CREATE A DEFINED AND ACTIVE STREETWALL**

SHAPE NEW PROJECTS TO CONTRIBUTE TO MID-BLOCK OPEN SPACE EVEN WHEN THE PATTERN IS BROKEN

SHAPE NEW BUILDINGS TO AVOID IMPACTING ADJACENT BUILDINGS' ACCESS TO LIGHT AND AIR

PROVIDE MATCHING LIGHT WELLS

LOCATE FRONTAGES TO REINFORCE THE STREETWALL



# Application Example

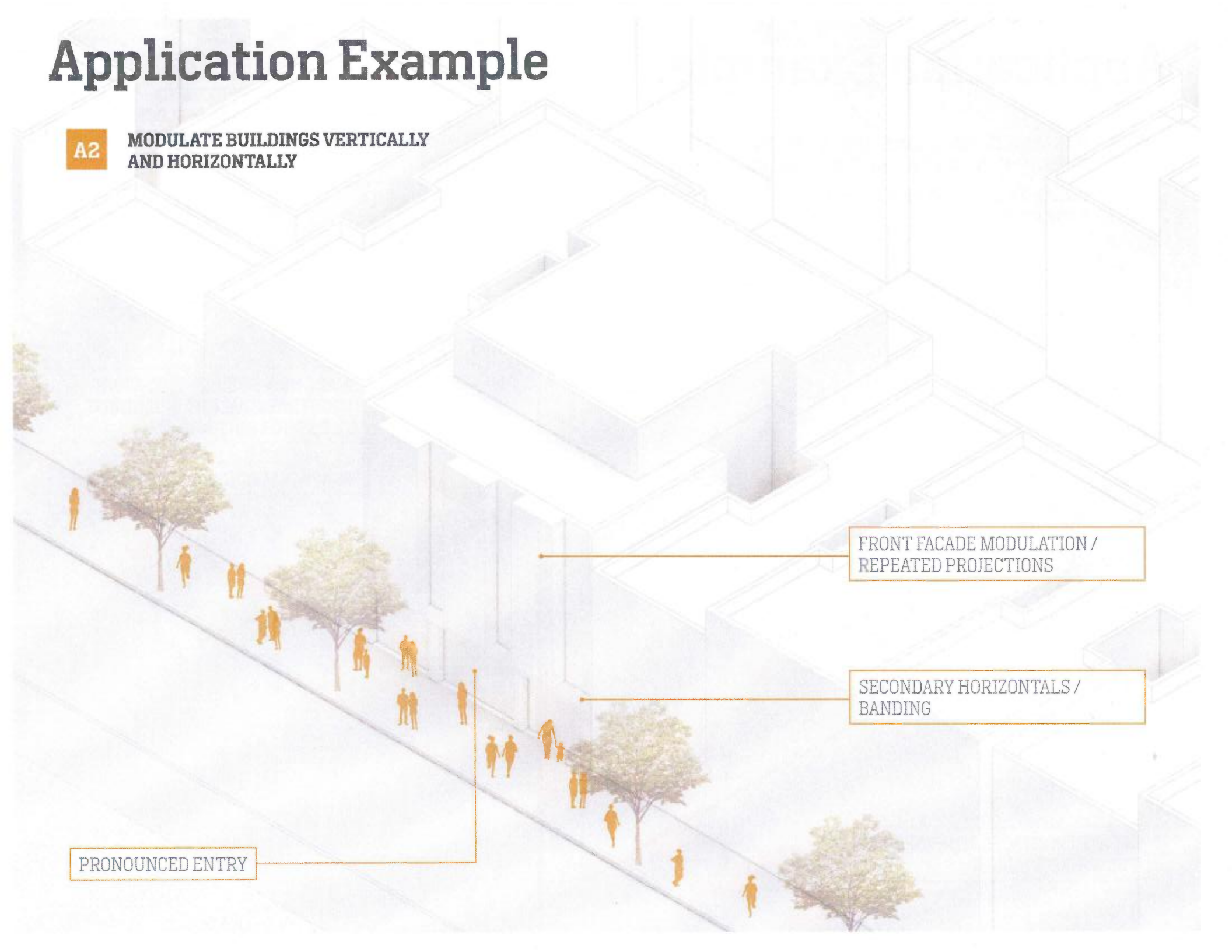
A2

MODULATE BUILDINGS VERTICALLY  
AND HORIZONTALLY

FRONT FACADE MODULATION /  
REPEATED PROJECTIONS

SECONDARY HORIZONTALS /  
BANDING

PRONOUNCED ENTRY





# Application Example

A3

**HARMONIZE BUILDING DESIGNS WITH  
NEIGHBORING SCALE AND MATERIALS**

A4

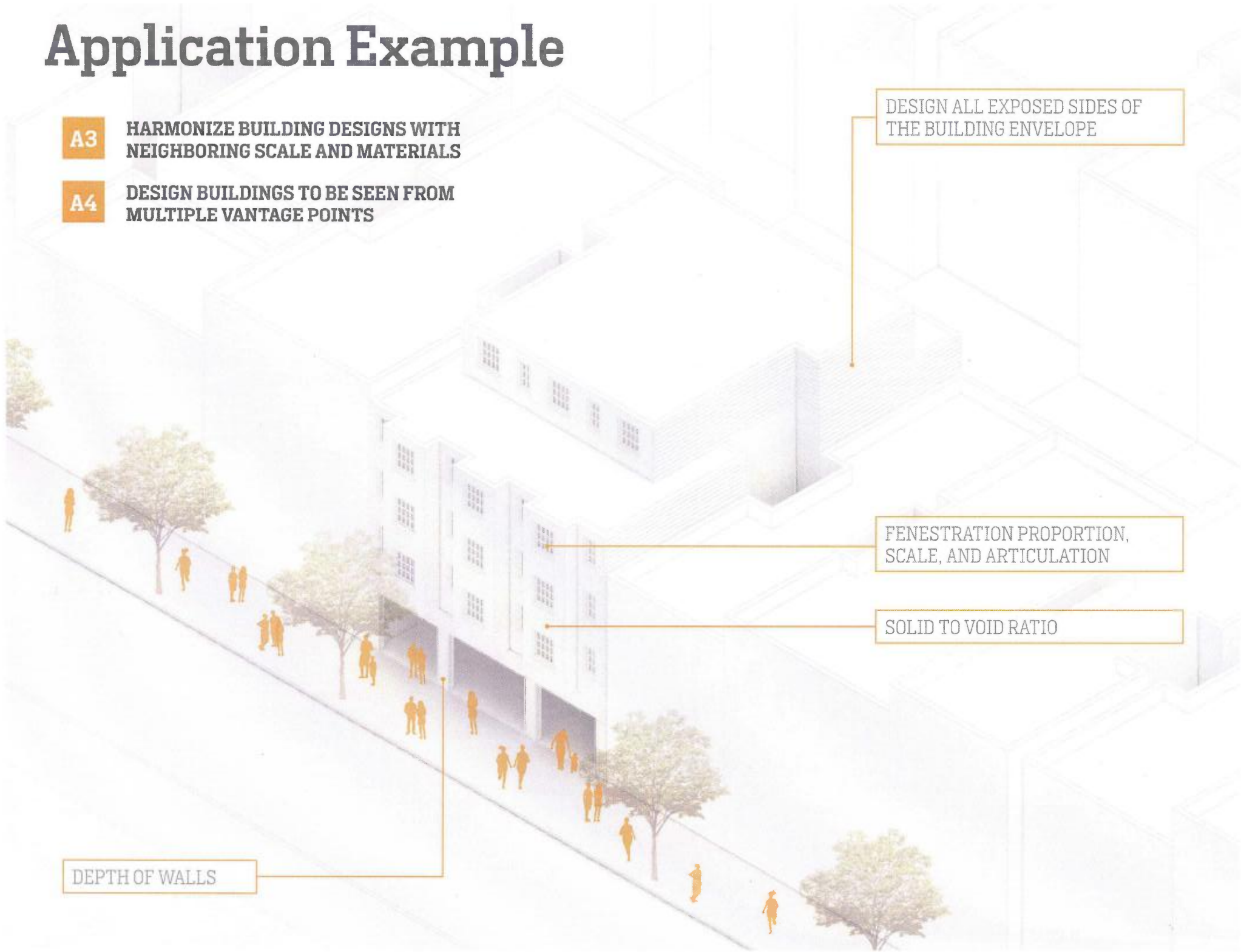
**DESIGN BUILDINGS TO BE SEEN FROM  
MULTIPLE VANTAGE POINTS**

DESIGN ALL EXPOSED SIDES OF  
THE BUILDING ENVELOPE

FENESTRATION PROPORTION,  
SCALE, AND ARTICULATION

SOLID TO VOID RATIO

DEPTH OF WALLS



# Application Example

A5

SHAPE THE ROOFS OF BUILDINGS

A8

DESIGN ACTIVE BUILDING FRONTS

SHAPED ROOF

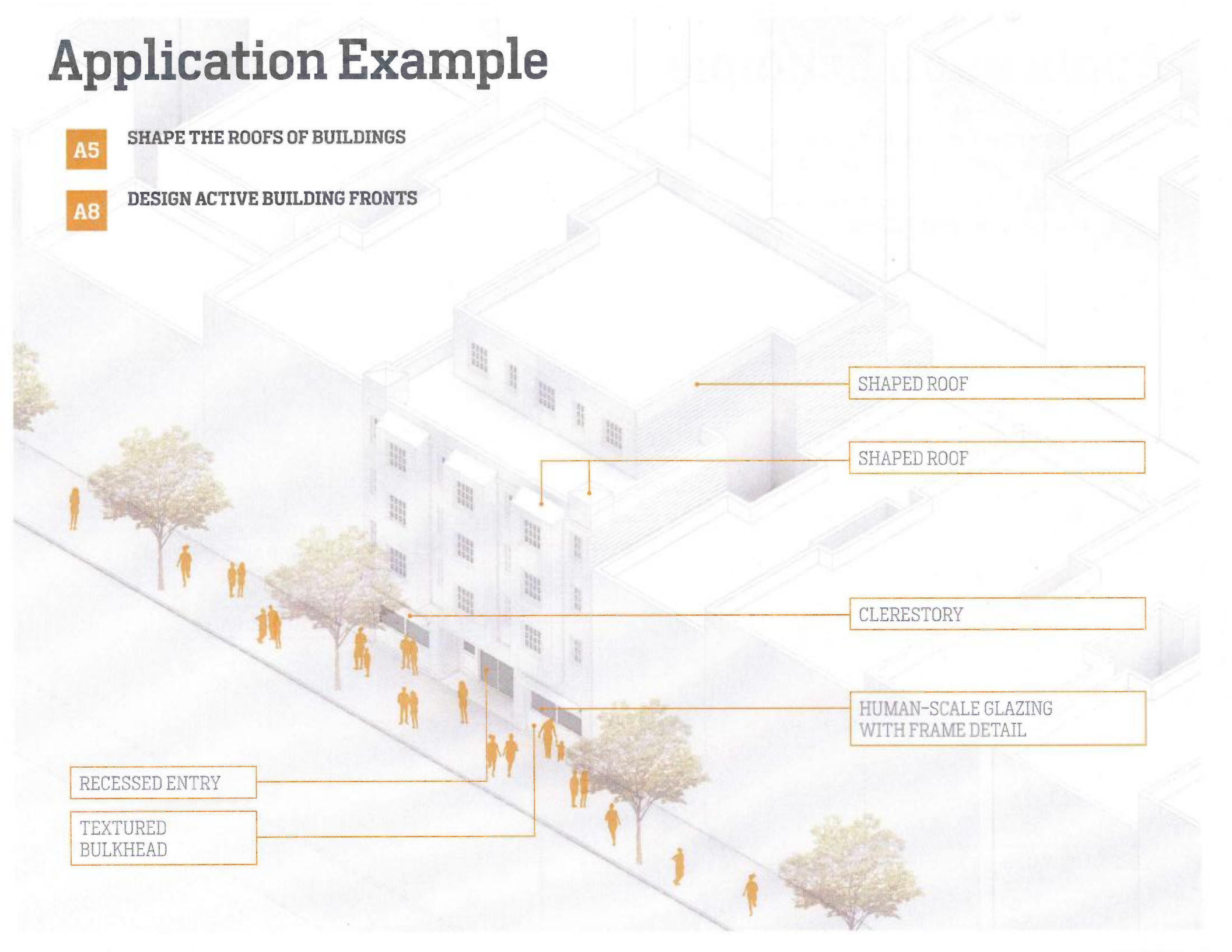
SHAPED ROOF

CLERESTORY

HUMAN-SCALE GLAZING  
WITH FRAME DETAIL

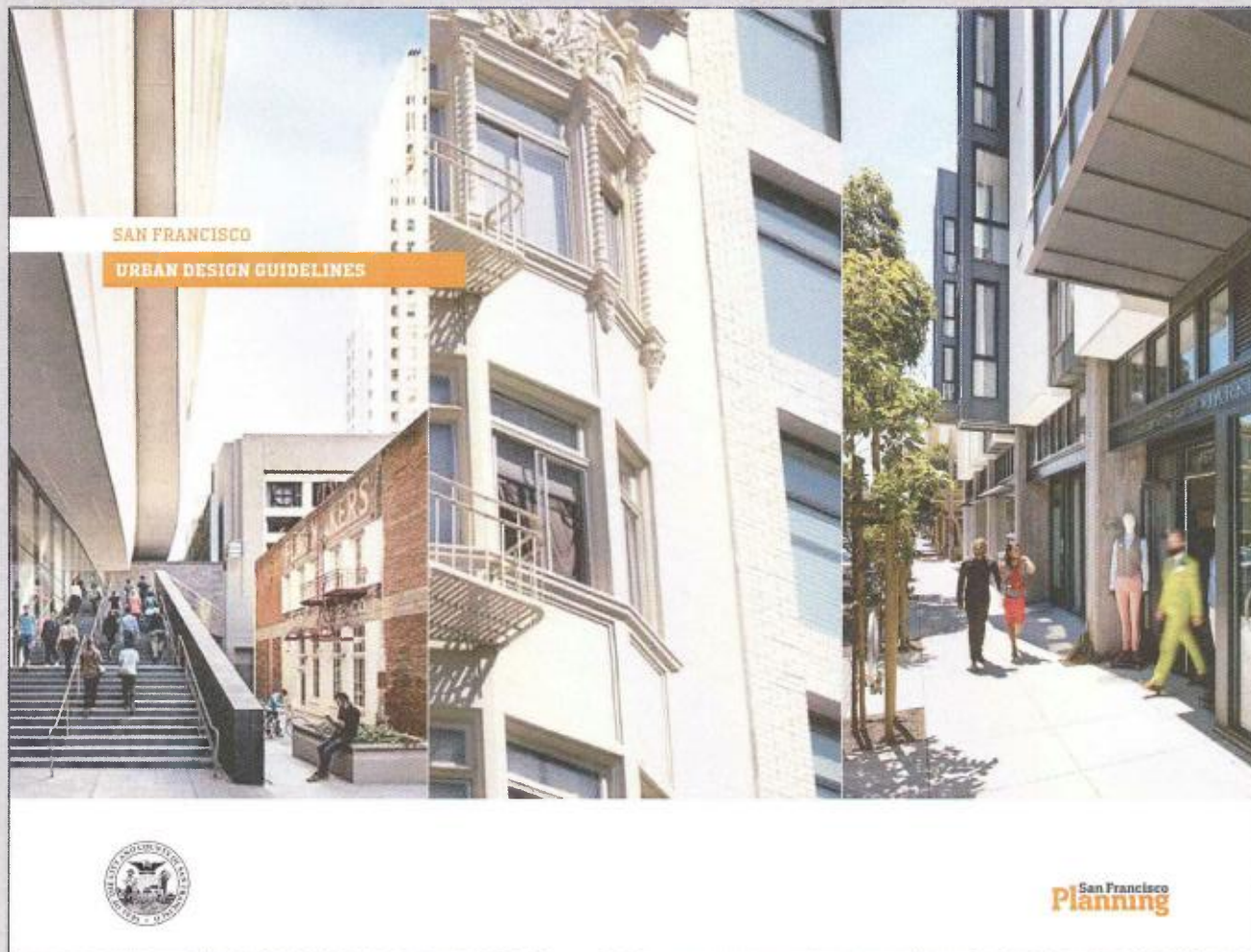
RECESSED ENTRY

TEXTURED  
BULKHEAD





**To support San Francisco's neighborhoods,  
Planning staff recommend the adoption of the  
Urban Design Guidelines.**





The New York Times

# Dorm Living for Professionals Comes to San Francisco

By Nellie Bowles (<https://www.nytimes.com/by/nellie-bowles>) March 4, 2018

Leer en español (<https://www.nytimes.com/es/2018/03/07/vivienda-san-francisco-dormitorios/>)

SAN FRANCISCO — In search of reasonable rent, the middle-class backbone of San Francisco — maitre d's, teachers, bookstore managers, lounge musicians, copywriters and merchandise planners — are engaging in an unusual experiment in communal living: They are moving into dorms.



Starcity is renovating two large buildings in San Francisco's Tenderloin neighborhood to turn into dorm rooms for the middle class. Jason Henry for The New York Times



Shared bathrooms at the end of the hall and having no individual kitchen or living room is becoming less weird for some of the city's workers thanks to Starcity, a new development company that is expressly creating dorms for many of the non-tech population.

Starcity has already opened three properties with 36 units. It has nine more in development and a wait list of 8,000 people. The company is buying a dozen more buildings (including one-star hotels, parking garages, office buildings and old retail stores), has raised \$18.9 million in venture capital and hired a team of 26 people. Starcity said it was on track to have hundreds of units open around the San Francisco Bay Area this year, and thousands by 2019.

These are not micro-units, nor are they like WeWork's WeLive housing developments (<https://www.nytimes.com/2018/02/17/business/my-life-in-weworld.html>), where residents have their own small kitchens, living rooms and bathrooms but share common event space and industrial appliances for parties. These are not single-family homes that are being used as group houses.

Instead, Starcity residents get a bedroom of 130 square feet to 220 square feet. Many of the buildings will feature some units with a private bath for a higher rent. But Jon Dishotsky, Starcity's co-founder and chief executive, said a ratio of one bathroom for every two to three bedrooms makes the most sense for large-scale affordability. The average one-bedroom apartment in San Francisco rents for \$3,300 a month, but Starcity rooms go for \$1,400 to \$2,400 a month fully furnished, with utilities and Wi-Fi included.

"If you think about the most private things that you do, a lot of them are related to the bathroom," said Mr. Dishotsky, 34. "So that's probably the hardest part."



CEO and co-founder Jon Dishotsky, on the roof of a dorm room building.  
Jason Henry for The New York Times

Starcity's target demographic makes \$40,000 to \$90,000 a year. Most of the residents, who range in age from their early 20s to early 50s, have no political philosophy around communes nor any previous experience in them. Moving in was a practical decision they each made. But after they arrive, what they are most surprised by is how much the building changes them.

### **'I Was Looking for More Meaning'**

One recent night, the Mission Street house gathered to celebrate a set of birthdays, and there in a party hat was Carla Shiver, 38.





Carla Shiver with her dog, Stanford, in her unit at the Mission Street house.

Jason Henry for The New York Times

Last year, Verizon eliminated Ms. Shiver's job in Albany, Ga., but offered to transfer her to San Francisco to work at a store. Ms. Shiver, who makes about \$85,000 a year, knew she could never afford a house here but moved anyway. "People talk all the time about what they dream of, and I decided to stop talking about it and just do it," Ms. Shiver said. "I was looking for more meaning."

She divorced her husband, packed her Yorkie Pomeranian, Stanford, in the car and drove west.

The idea of sharing a bathroom was initially alarming, but the pictures of the house looked nice and Ms. Shiver wanted to meet new friends. For \$2,200 a month, she now rents a Starcity room with a queen-size bed, a bedside table and a chair.

She said she could not imagine any other life.

“I’ve run a household; I’ve done the bills; I’ve mowed the yard, and I don’t want to be responsible again,” Ms. Shiver said. “I want to paint and learn how to make ramen noodles. And when we run out of tinfoil, there’s just more tinfoil.”

The Starcity community manager (a.k.a. the building manager) is extremely involved in household affairs, dropping off care packages when someone is sick and organizing birthday parties. If tenants sign up for premium services, Starcity will do their laundry for \$40 a month, clean rooms for \$130 a week and even arrange for dog day care. For many residents, the arrangement does not feel temporary.



The communal kitchen at the Starcity Nottingham House. Jason Henry for The New York Times





Shared bathrooms at the Starcity Nottingham House. Jason Henry for The New York Times

“I never thought I could live like this,” Ms. Shiver said. “But the more I live here, the freer I feel.”

She said she had not locked her bedroom door once since moving in, and most days when she gets home from work, a roommate has taken her dog into the shared living room. She said she hardly thought about the dorm-style bathroom setup, that there had never been a line for a shower, and that the building was like a family.

“This afternoon we’re going to the Exploratorium,” she said, referring to the science museum located at Pier 15.

### **Mr. Dishotsky’s Awakening**

Mr. Dishotsky looked very much the part one morning as he walked into a building site.

Wearing muddy leather boots, black jeans and a hard hat, he examined Mason Street, formerly a residential hotel that served homeless and low-income people in the Tenderloin neighborhood. It will soon be 71 Starcity units.

The Tenderloin, a traditionally working-class and diverse neighborhood with a large arts scene and a sizable homeless population, has been slowly gentrifying, leading to rising tensions. (Most of Starcity's residents are white.) On the sidewalk outside Mr. Dishotsky's construction zone that morning, there were used needles and several tents.

He paced through the first floor's 2,500-square-foot living room. The basement will be a communal kitchen, with a lineup of industrial sized refrigerators.

The only thing people really need to do alone is sleep, he said.

"What are the things you can do with other people? Eat food, drink wine, watch TV," he said. "You don't need to do that in your own unit alone, so why pay for it?"





Mr. Dishotsky in the Tenderloin neighborhood, where Starcity is renovating two large buildings.  
Jason Henry for The New York Times





Painters work on one of Starcity's dorm renovations in the Tenderloin neighborhood.  
Jason Henry for The New York Times

Mr. Dishotsky grew up in Palo Alto, Calif., where housing prices have soared and the median home value is now more than \$3 million (<https://www.zillow.com/palo-alto-ca/home-values/>). His parents were both teachers and left-wing political activists living in an intentional community in the late 1960s before they bought a house for \$50,000.

After Mr. Dishotsky graduated from college, he spent a decade at a commercial real estate firm making deals until one day in 2015, he had a crisis. His friends were leaving town. The arts scene was fading. He saw a political cause and an economic opportunity.

“My mom got shot once protesting for what she believed in,” he said. “And here I am building offices.”

So he quit. He wanted to build something that, at market rate, would be



affordable.

When Mr. Dishotsky first tried to get a bank loan for his new type of pared-down housing, he was turned away by 40 lenders.

“They were like, ‘Who would live this way?’” he said. “We’re like, ‘It’s everybody, it’s normal people you know.’”

A couple blocks away was the Ellis Street building, a former bathhouse turned into medical offices that became a vacant property. Another developer had tried to turn it into 11 luxury condos. Mr. Dishotsky’s pitch was 52 dorm rooms.

The move was both idealistic and practical. Because of arcane permitting rules and neighborhood associations that push against new developments, building new housing in San Francisco is painfully slow. But workers keep flooding the city, so roommates jam tighter into existing housing, already sharing bathrooms and renting living rooms as bedrooms. Mr. Dishotsky said he decided to build for what was already the city’s reality.

At the Ellis Street site, his team is digging down about a level and a half to make a basement lounge. Each floor has a communal kitchen for eight to 15 people. He’s working with his co-founder, Mohammad Sakrani, 30, on new beds that can be hoisted up and suspended from the ceiling during the day. They are also trying to design modular bathrooms and even entire bedrooms that can be “plugged in” to buildings.



Inside the communal kitchen at Starcity's Mission House, where residents gathered for "wine night."  
Jason Henry for The New York Times

### **Ms. Ndrepepaj's New Friends**

In Starcity's South of Market building, known as Gilbert House, which has a reputation for being the party house, tenants call themselves the Gilbertines.

Migerta Ndrepepaj, 25, the headwaiter at the Nob Hill Club at the Intercontinental Mark Hopkins Hotel, said her favorite tradition was Sunday family days when the housemates cook together and go on adventures like renting go-karts.

"That makes us sound like college kids," Ms. Ndrepepaj said. "But we're not."





Migerta Ndrepepaj, right, hangs out with her Starcity housemates in the kitchen of their building.  
Jason Henry for The New York Times

For the annual San Francisco race and parade Bay to Breakers, the housemates rented sets of four-seater tandem bikes and cruised the city. For Halloween, they dressed as characters from “Alice in Wonderland” (Ms. Ndrepepaj was the White Rabbit). Recently, they all went to Lake Tahoe to a house that Starcity supplied.

“You don’t have to think up plans anymore because they kind of do it for you,” she said. “And now, I live with my best friends.”

The units are fundamentally not fancy, but Starcity adds accents that gives the spaces a trendy millennial look. Furniture is a midcentury-modern aesthetic. Plants hang in concrete pots on the walls alongside art that residents make on painting nights.

"I feel like I'm in a relationship with everyone I live with," Ms. Ndrepepaj said. "If their day is bad, your day is bad."

### **A Birthday Party**

One evening back at Starcity's Mission House, Rachel Haltom, 22, an account executive at Yelp, baked a birthday cake with Steph Allen, 24, a fashion boutique merchandise planner, for a housemate.

Ms. Haltom had never made meringue, but Chris Maddox, 27, a writer, had come home and took over the egg-white whipping. One tenant announced a secret crush on another, and there was debate about the merits. They joked about alcoholic seltzer water, a new trend they all agreed was absurd, as Ms. Allen drank one.



Residents and guests in the communal kitchen at the Mission House gathered for a birthday party.  
Jason Henry for The New York Times



Before Starcity, Mr. Maddox paid \$4,100 a month for a one-bedroom apartment and worked near constantly as chief executive of Seneca Systems, a start-up that provides software for local governments.

What he wanted was to be a writer. Now, he pays \$1,900 a month and lives in a cluttered bedroom with a bed, a record player and an overflowing bookshelf.



A glimpse into a Starcity room with a loft bed. Jason Henry for The New York Times



Katherine McKim with her dog, Zoey, in her room at the Mission House.  
Jason Henry for The New York Times

Katherine McKim, 37, came home with her dog, Zoey, who trotted around the kitchen. Ms. McKim had worked for Penguin Random House in New York but always admired the San Francisco-based publisher Chronicle Books, so when she and her husband divorced, she packed up and moved out. (There are quite a few divorcées in Starcity.)

“Everybody told me housing in San Francisco was really expensive, but I was like, ‘I live in New York, how much more expensive can it be?’” she said. “I was a bit cocky.”

Now, for \$2,050 a month, she has space for a dog bed for Zoey, a full-sized bed for herself, a TV, a mini fridge and a sink.



Every other Wednesday is “wine night.” An upcoming Tuesday is “kombucha and yoga night.” On Feb. 14, it was “pal-entines day,” planned and hosted by Starcity.

Nellie Bowles covers tech and internet culture from San Francisco. Before joining The Times, she was a correspondent for “VICE News Tonight.” @nelliebowles (<https://twitter.com/nelliebowles>)

A version of this article appears in print on March 4, 2018, on Page B1 of the New York edition with the headline: Dorm Living for Professionals. Order Reprints (<http://www.nytreprints.com/>) | Today's Paper (<http://www.nytimes.com/pages/todayspaper/index.html>) | Subscribe (<https://www.nytimes.com/subscriptions/Multiproduct/lp8HYKU.html?campaignId=48JQY>)

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app\_id=9869919170&link=https%3A%2F%2Fwww.nytimes.com%2F2018%2F03%2F04%2Fliving-grown-ups-san-francisco.html&smid=fb-share&name=Dorm%20Living%20for%20Professionals%20Comes%20to%20San%20Francisco  
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url=https%3A%2F%2Fwww.nytimes.com%2F2018%2F03%2F04%2Ftechnology%2Fdorm-living-grown-ups-san-francisco.html&text=Dorm%20Living%20for%20Professionals%20Comes%20to%20San%20Francisco  
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Jack Nicas/The New York Times

(<https://www.nytimes.com/2018/03/22/technology/at-mars-jeff-bezos-hosted-roboticists-astronauts-other-brainiacs-and-me.html>)

action=click&module=MoreInSection&pgtype=Article&region=Footer&contentCollection=Technology)

## At Mars, Jeff Bezos Hosted Roboticists, Astronauts, Other Brainiacs and Me

(<https://www.nytimes.com/2018/03/22/technology/at-mars-jeff-bezos-hosted-roboticists-astronauts-other-brainiacs-and-me.html>)?

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22

## YouTube to Ban Videos Promoting Gun Sales

(<https://www.nytimes.com/2018/03/22/business/youtub>



gun-ban.html?

(<https://www.nytimes.com/2018/03/22/business/gun-ban.html>?

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ago

## Kevin's Week in Tech: Zuckerberg's Answers to Privacy Scandal Raise More Questions

(<https://www.nytimes.com/2018/03/22/technology/kevins-aycock.html>)



week-in-tech-zuckerbergs-answers-to-privacy-

(<https://www.nytimes.com/2018/03/22/technology/guy-kevin-schwartz-week-in-tech-zuckerbergs-answers-to-privacy->



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Received at OPC Hearing 3/22/18  
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MARCH 22, 2018  
SAN FRANCISCO PLANNING COMMISSION  
CANDLESTICK POINT & HUNTERS POINT SHIPYARD  
PROJECT UPDATE

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# THE SHIPYARD & CANDLESTICK

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# **INTRODUCTION**

# **PROJECT VISION**

# **DESIGN STANDARDS**

---

# INTRODUCTION

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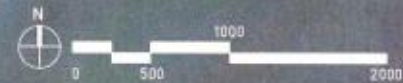


# LOCATIONS

*Shipyards Phase 1*

*Shipyards Phase 2*

*Candlestick*



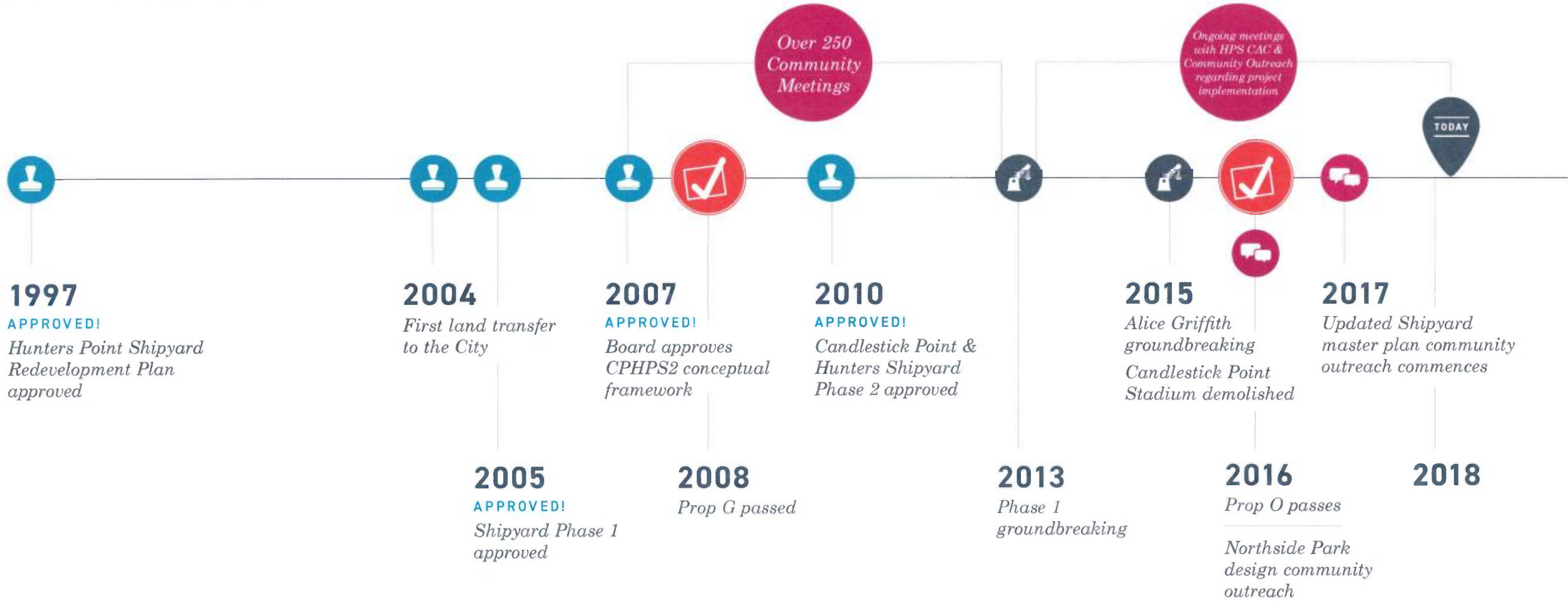
## CONTEXT MAP

Scale: NTS



# PROJECT TIMELINE

## OUR HISTORY





# WHY ARE WE UPDATING THE PLAN?

- 01** Creating a more integrated community by providing a greater mix of uses
- 02** Re-imagining and providing more parks and open space
- 03** Increasing the number of historic buildings that could be retained at the Shipyard
- 04** Incorporating best practice green energy and sustainable infrastructure

# WHAT WE ARE PROPOSING:

- 01** Opportunities for more:
  - Schools
  - Parks & open space
  - Research & development space
  - Retail/makerspace
  - Hotel space
  - Adaptive re-use of existing buildings
  - More robust bicycle network
  - Complete network of transit-only lanes throughout the site
- 02** A more robust and diverse mix of uses that has the potential to create more local jobs and generate significantly more general fund revenues
- 03** To incorporate the new plan, authorizing an additional 2M square feet of commercial uses to align the DDA and the Redevelopment Plan
- 04** Rebalancing the number of homes over the Shipyard and Candlestick



# WHAT'S THE SAME?

- 01** No increase in total square footage approved in the Redevelopment Plan
- 02** Commitment to affordable housing at  $\pm 32\%$
- 03** Backbone infrastructure
- 04** High quality transit service, active transportation options, and robust transportation demand management program

# PROPOSED SHIPYARD LAND USE

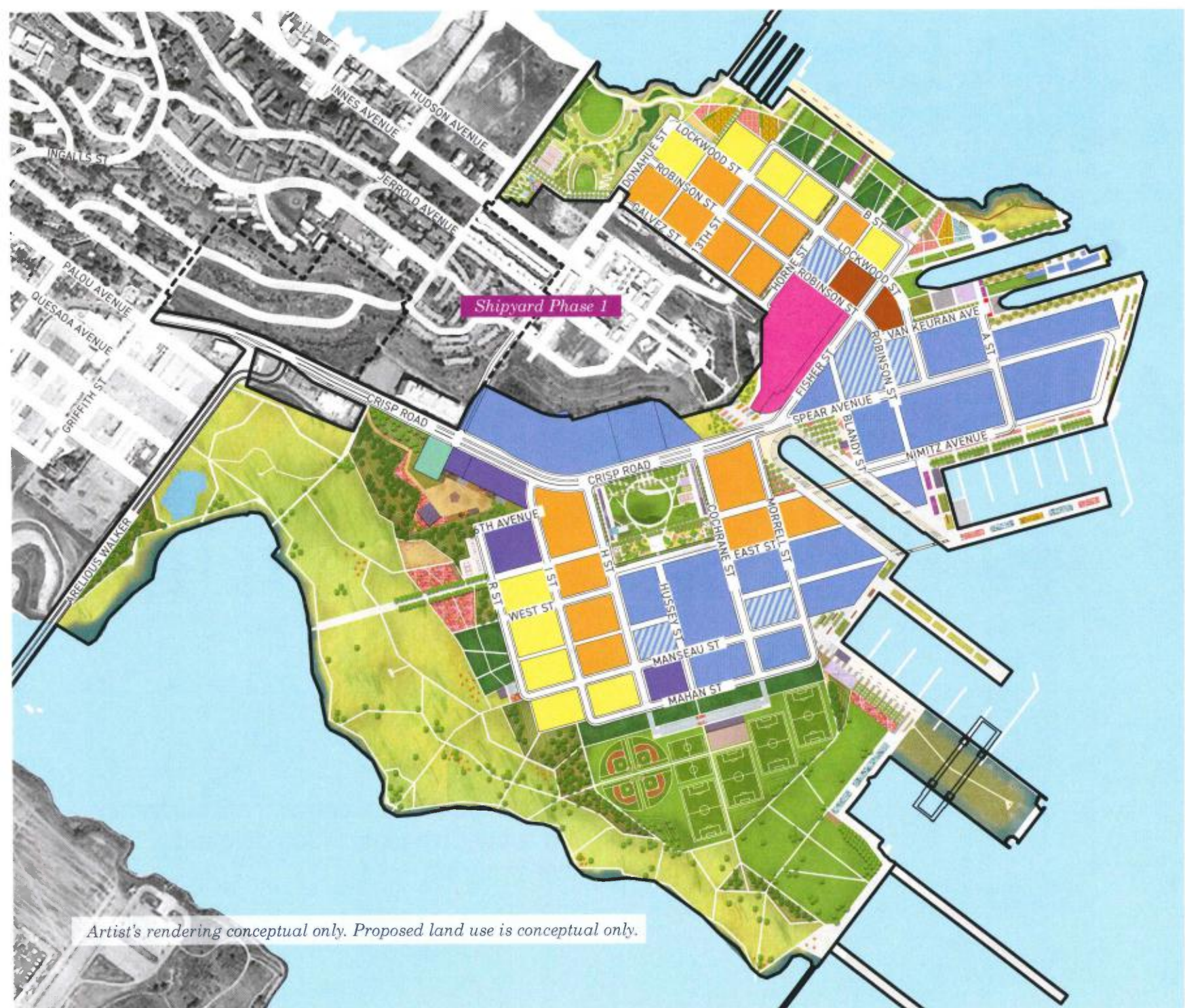
LEGEND

- |  |   |
|--|---|
|  Residential Density I<br><i>Townhomes</i>          |  Commercial<br><i>Includes R&amp;D, Office and Hotel</i> |
|  Residential Density II<br><i>Multi-Story Flats</i> |  Commercial/Parking                                      |
|  Residential Density IV<br><i>High Rise</i>         |  Community Use   |
|  Artist   |  Utilities<br><i>Recycled Water Treatment Plant</i>      |

*\*Ground floor neighborhood retail/makerspace/PDR space is allowed per redevelopment plan. To the extent permitted by the Hunters Point Shipyard Redevelopment Plan and underlying site conditions, institutional uses may be developed on any block within The Shipyard.*

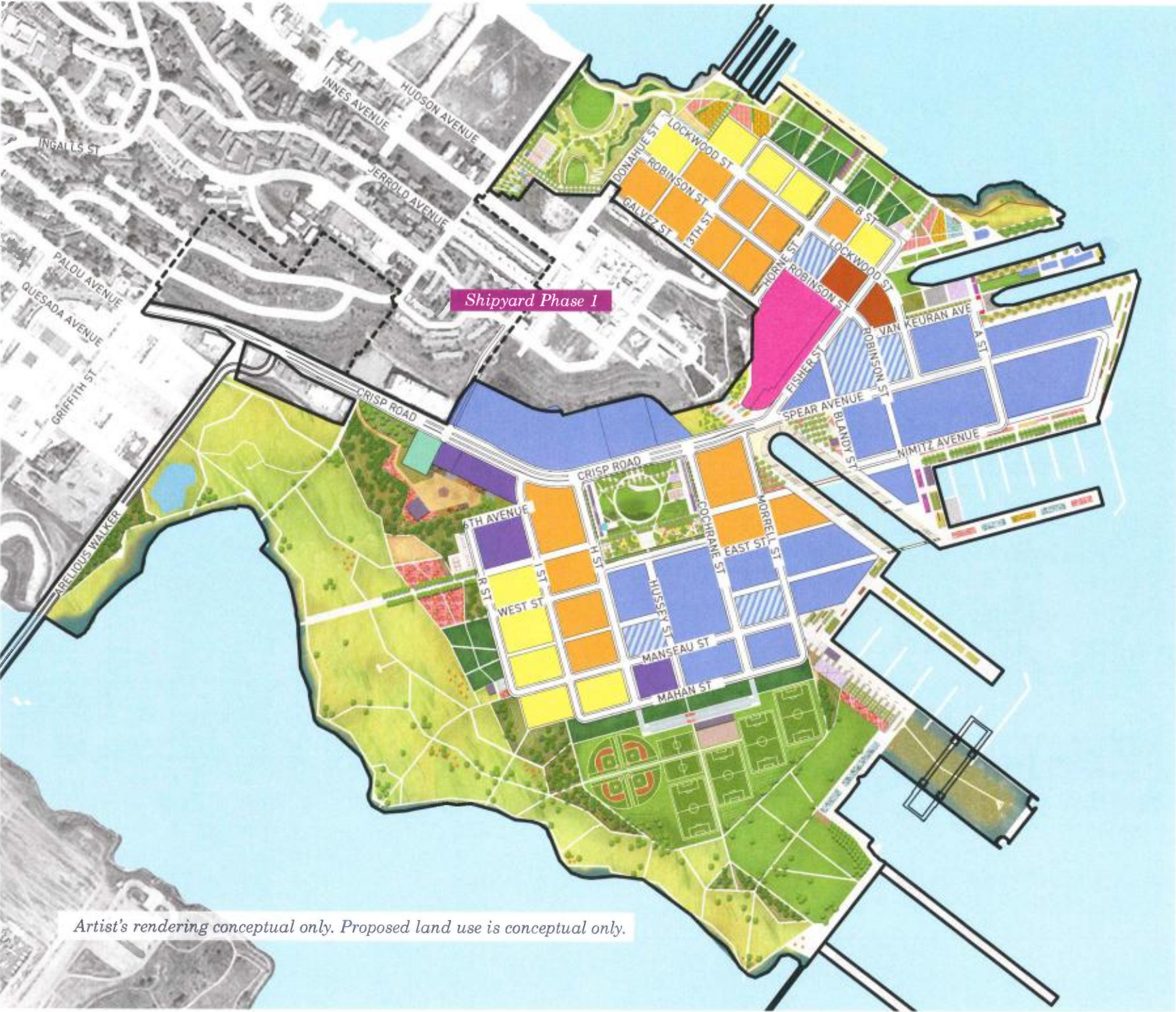


Artist's rendering conceptual only. Proposed land use is conceptual only.





# PROPOSED SHIPYARD LAND USE



Shipyard Phase 1

**3,454**  
planned  
residential units

APPROXIMATELY  
**255,000**  
sq ft planned for artist  
studios & gallery space

APPROXIMATELY  
**4.25 MILLION**  
sq ft of planned office  
& R&D space

APPROXIMATELY  
**50,000**  
sq ft of planned  
community use

APPROXIMATELY  
**401,000**  
sq ft of planned retail  
& makerspace

APPROXIMATELY  
**410,000**  
sq ft of planned  
institutional space

APPROXIMATELY  
**120,000**  
sq ft of planned  
hotel space

APPROXIMATELY  
**240**  
acres of planned total  
parks & open space

Artist's rendering conceptual only. Proposed land use is conceptual only.

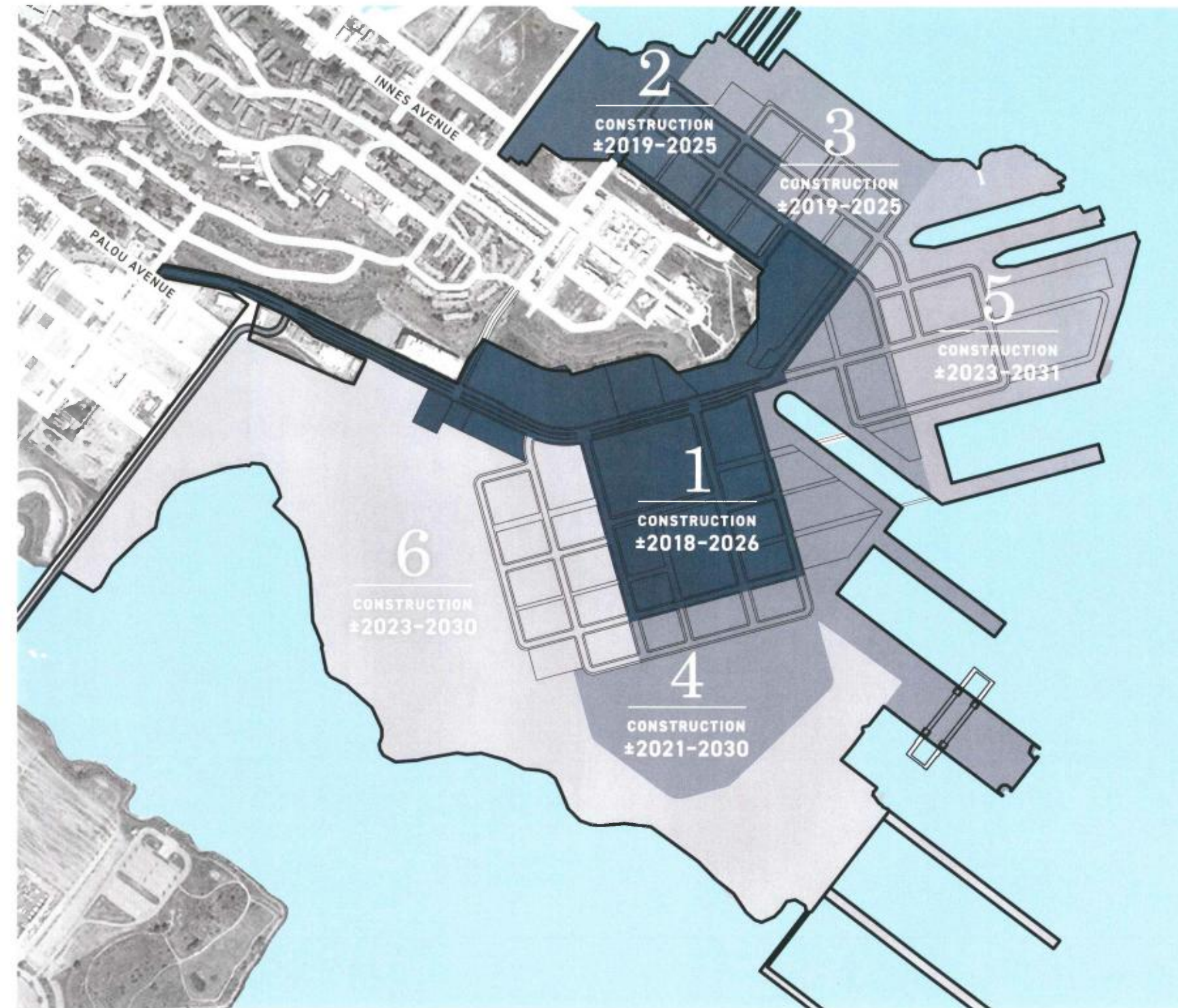


# SHIPYARD PHASE 2 GSF COMPARISON

| LAND USE & HOUSING     | 2010 HPS RDP | 2010 DDA  | 2018      |
|------------------------|--------------|-----------|-----------|
| Residential            | 4,275        | 4,275     | 3,454     |
| Research & Development | 5,000,000    | 3,000,000 | 4,265,000 |
| Retail                 | 125,000      | 125,000   | 401,000   |
| Neighborhood Retail    | 125,000      | 125,000   | 226,000   |
| Regional Retail        | 0            | 0         | 100,000   |
| Makerspace             | 0            | 0         | 75,000    |
| Hotel                  | 0            | 0         | 120,000   |
| Artist                 | 255,000      | 255,000   | 255,000   |
| Community Use          | 50,000       | 50,000    | 50,000    |
| Institutional          | 0            | 0         | 410,000   |
| TOTAL:                 | 5,430,000    | 3,430,000 | 5,501,000 |



## SHIPYARD PROJECTED PHASING



*Note: All dates are projections and subject to Navy conveyance. 2018 dates refer to construction associated with new artist building.*



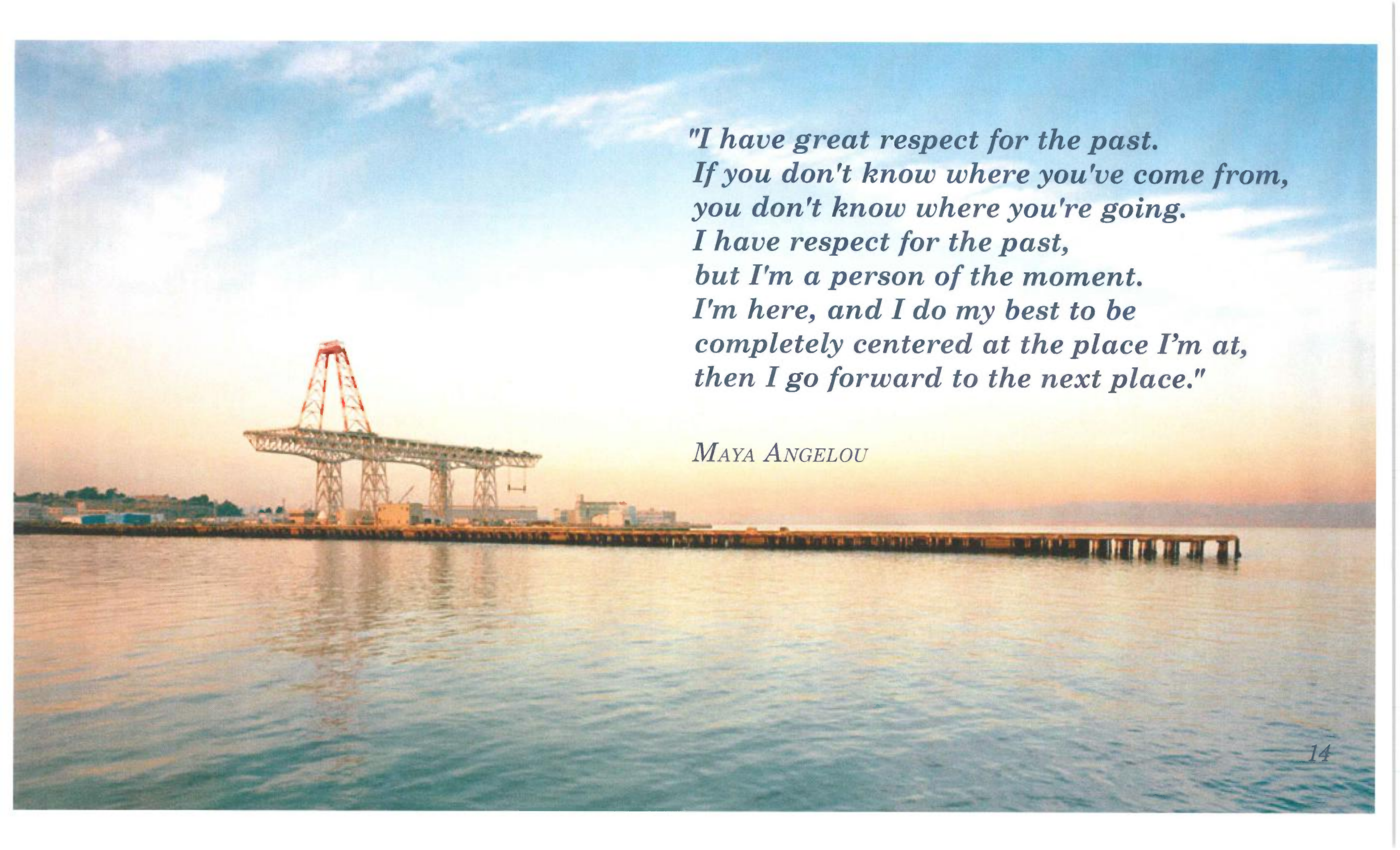


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# PROJECT VISION

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*"I have great respect for the past.  
If you don't know where you've come from,  
you don't know where you're going.  
I have respect for the past,  
but I'm a person of the moment.  
I'm here, and I do my best to be  
completely centered at the place I'm at,  
then I go forward to the next place."*

*MAYA ANGELOU*



## PROJECT VISION

1869



1920s





## PROJECT VISION

### HONORING THIS ICONIC PAST

- **1867 THE DRYDOCK IS COMPLETED**  
At 450 feet long, 24 feet deep, and 100 feet wide at the top, it is the largest stone dock in the world.
- **1875 AN ICONIC AMERICAN BRAND IS BORN**  
Levi Strauss and two colleagues purchase the Mission and Pacific Woolen Mills. They repurpose the company's blanket-weaving facility in Hunters Point to make flannel linings for their riveted dungarees.
- **1947 THE ICONIC SHIPYARD CRANE**  
The American Bridge Company builds a 630-ton gantry crane. It's the largest in the world at the time, capable of lifting battleship gun turrets and other objects weighing up to one million pounds.
- **1941 A SHIPBUILDING BOOM**  
The US Navy acquires the land and expands Drydock No. 4, once again making Hunters Point home to the world's largest graving dock. The Navy officially begins shipbuilding operations to aid in the World War II effort.





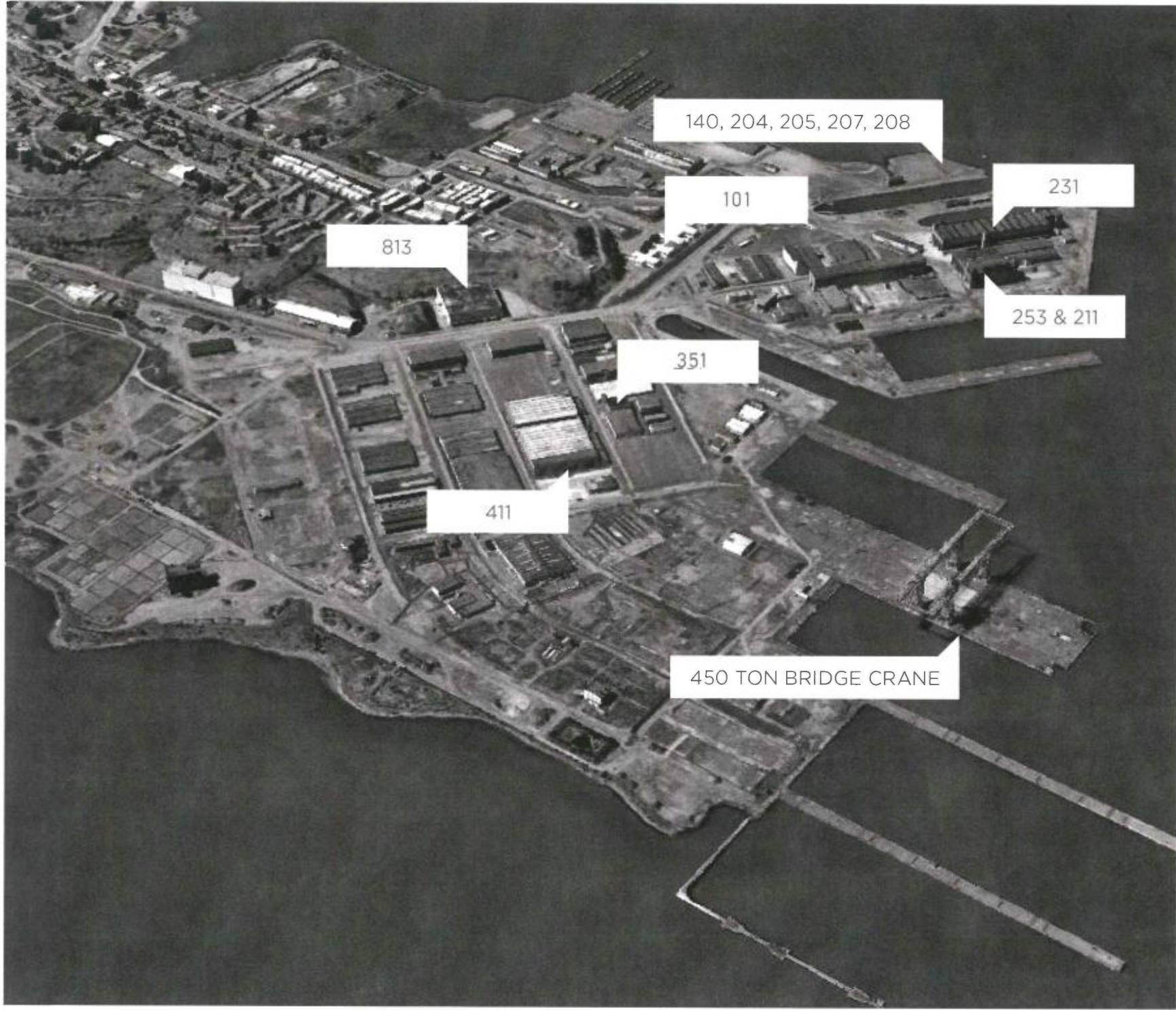
## PROJECT VISION

### SOCIAL HISTORIC FEATS

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- **1938 HUNTERS POINT HOUSES ITS FIRST ARTIST**  
Painter and sculptor Adrien Voisin purchases the old Albion Brewing Company castle. He spends the next 20 years converting it into a private residence and studio.
- **1939 THE UNITED STATES NAVY TAKES OWNERSHIP OF THE SHIPYARD**  
The Hunters Point Improvement Association is formed and is tasked with developing the district and deepening its connections to greater San Francisco. They organize to build more than 12,000 new homes for defense workers.





# PROJECT VISION

## KEY FRAMEWORK

- Retention of the original street grid
- Encouraging adaptive reuse of existing character enhancing structures
- Preserving the continuity of history

## CHARACTER ENHANCING STRUCTURES

- |       |         |
|-------|---------|
| • 140 | • 253   |
| • 204 | • 211   |
| • 205 | • 351   |
| • 207 | • 411   |
| • 208 | • 813   |
| • 101 | • CRANE |
| • 231 |         |

## PROJECT VISION

### KEY FRAMEWORK

- Relationship to the shoreline
- Open space
- Integrated use districts
- Green Room
- Water Room





# PROJECT VISION





## PROJECT VISION

**01** Embrace the legacy, authenticity, and unique character of the Shipyard as we look to the future



**02** Create a model for city-making that continues San Francisco's legacy of distinct neighborhoods





## PROJECT VISION





## PROJECT VISION



Draw cues from the scale and craft of the Shipyard's heritage uses to preserve its unique identity





# PROJECT VISION

Embrace the maritime topography of the Shipyard to define the character of the public realm



# PROJECT VISION

BUILDING 140  
PUMPHOUSE



BUILDING 140  
PUMPHOUSE





# PROJECT VISION

INNES AVENUE 1917



INNES AVENUE 2007





2010





2010




STADIUM MASTER PLAN // 2010





## KEY FEATURES

-  Existing Buildings
- ① Green Room
- ② Water Room
- ③ Pedestrian Allée
- ④ Waterfront Open Space
- ⑤ Artists' Studio / Maker-space
- ⑥ Transit Center
- ⑦ Connection to/from Hilltop
- ⑧ Hilltop View to the Bay







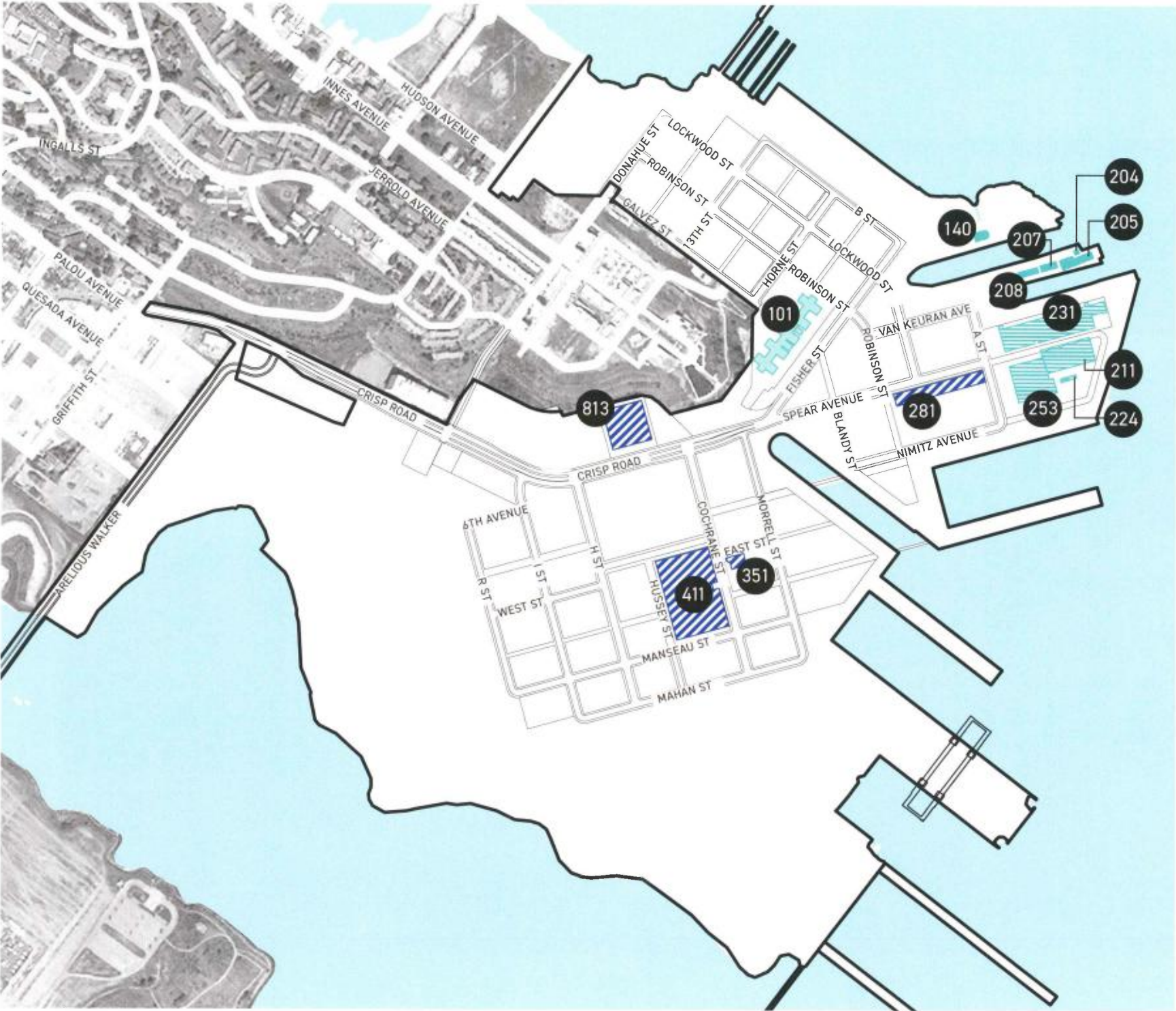
*Artist's renderings are conceptual only. There is no guarantee that the project will be approved, developed or built as shown.*



# SHIPYARD HERITAGE BUILDINGS

LEGEND

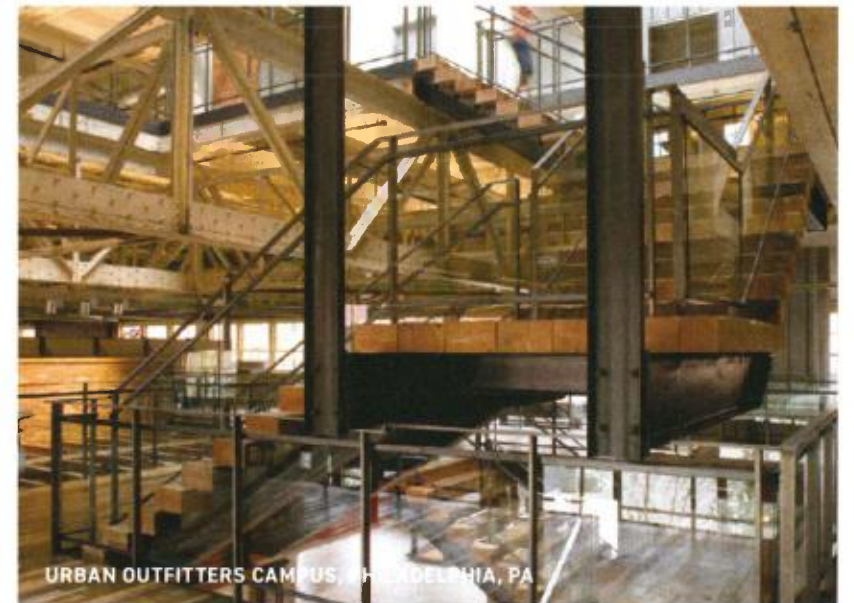
-  National Registry Building
-  To be Studied for Retention, as Required per 2010 Approval
-  To be Considered for Adaptive Reuse
-  Artist Building





# PROJECT VISION

*Precedent images*





# PROJECT VISION



MATOPOLSKA GARDEN OF ARTS  
KRASKOW, POLAND



URBAN OUTFITTERS CAMPUS  
PHILADELPHIA, PA

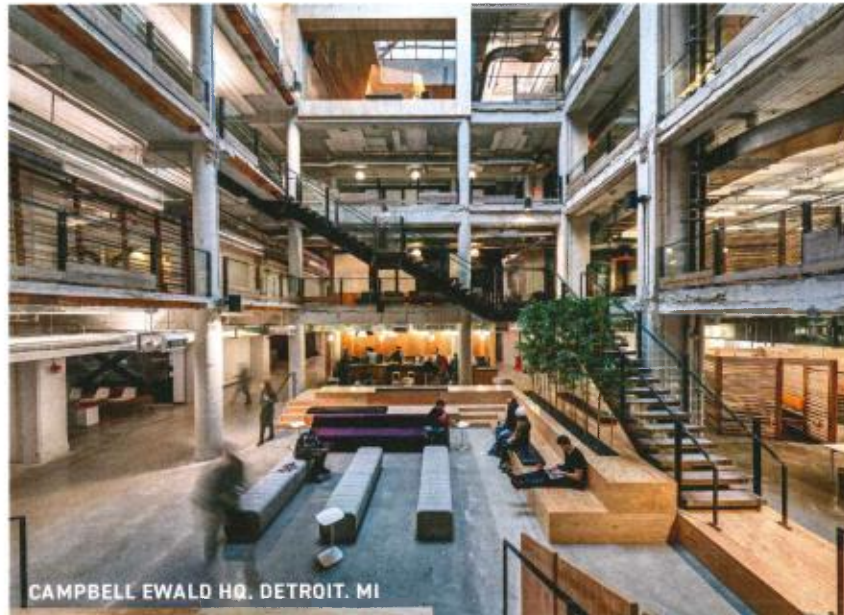


NEW LAB, BROOKLYN, NY

*Precedent images*



# PROJECT VISION



*Precedent images*



# BUILDING 411



*Artist's renderings are conceptual only. There is no guarantee that the project will be approved, developed or built as shown.*

*Retention subject to further study.*



EXISTING BUILDING



**BUILDING 411 LOCATION**  
Scale: NTS



# BUILDING 813



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Retention subject to further study.



EXISTING BUILDING



**BUILDING 813 LOCATION**  
Scale: NTS



# PROJECT VISION

*Precedent images*



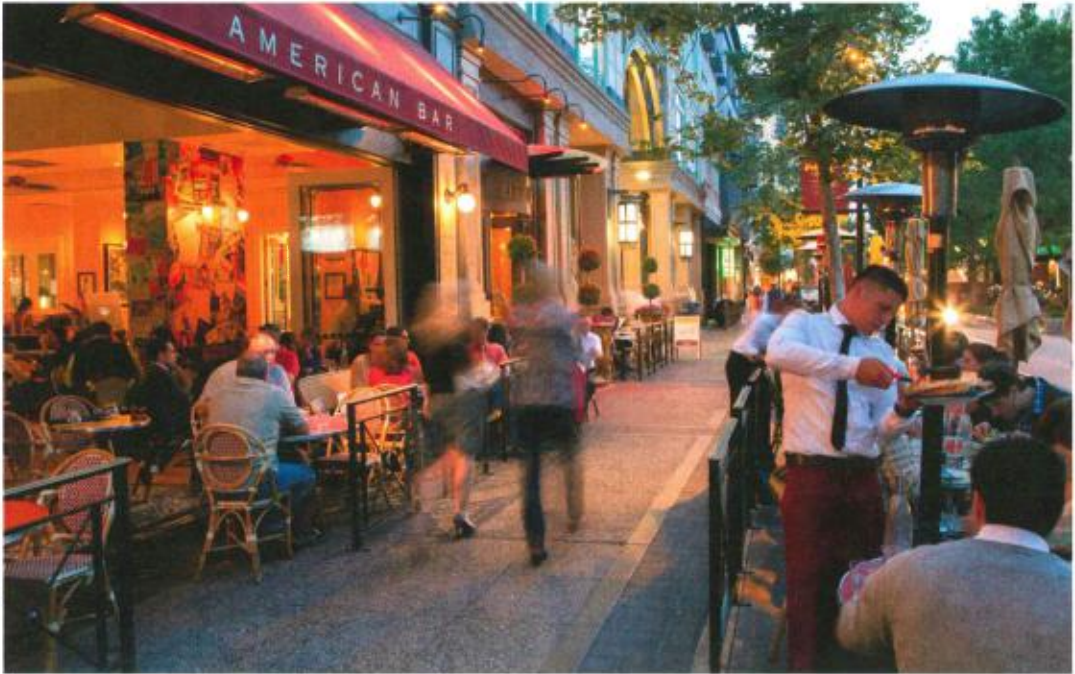
Rebuild the Shipyard as a cultural and economic engine for Bayview Hunters

Establish residential neighborhoods with a variety of housing typologies to create diverse urban life and active streetscapes

Provide retail uses that activate the streetscape and amenities that build community

Create office and R&D workplaces that perpetuate San Francisco's preeminence in the global innovation economy

Invigorate the Artistic Cultural District





# PROJECT VISION





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# DESIGN STANDARDS

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## PROJECT VISION

**01** Embrace the legacy, authenticity, and unique character of the Shipyard as we look to the future



**02** Create a model for city-making that continues San Francisco's legacy of distinct neighborhoods



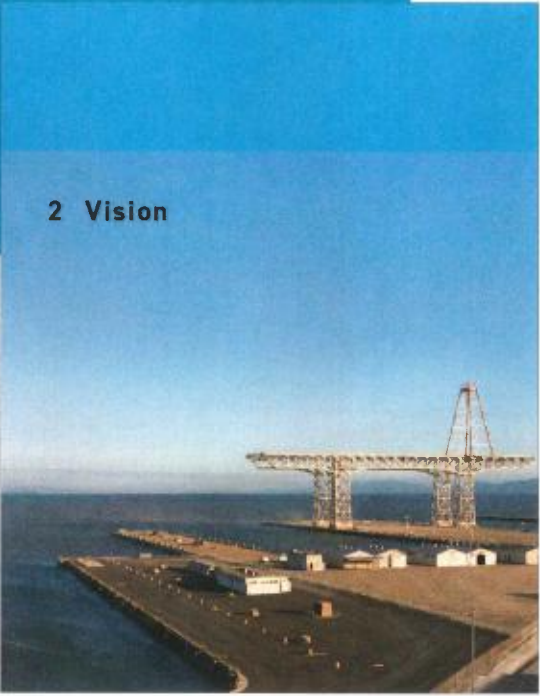


# DOCUMENT ORGANIZATION

*Design for Development*

**1 INTRODUCTION**

- 1.1 Summary of Documents
- 1.2 Companion Documents
- 1.3 Document and Chapter Organization
- 1.4 History



*Design for Development*

**3 DISTRICTS, KEY DESTINATIONS AND FEATURES**

- 3.1 Warehouse District
- 3.2 Village Center
- 3.3 Wharf District
- 3.4 North Shoreline
- 3.5 Green Room
- 3.6 Water Room
- 3.7 Pedestrian
- 3.8 Waterfront

*HUNTERS POINT SHIPYARD DESIGN FOR DEVELOPMENT | 10.12.2016*

**4 BUILDING DESIGN STANDARDS & GUIDELINES**

The Building Design Intent, Standards and Guidelines provide regulatory controls to guide the architectural design of buildings within the site to align with the Hunters Point Shipyard Vision. Each control consists of Intent, Definitions, Standards, and Guidelines. Certain controls include an Application section that outlines additional information including intent, definitions, and guidance on application of Standards.

**Intent:** Describes the principal goals, objectives and rationale of each Standard and/or Guideline, as well as alignment of specific features or provisions to the project vision, principles, design drivers and physical framework.

**Standard:** Mandatory objective and quantifiable specifications or other requirements applicable to the Project. Modifications to Standards require formal approval.

**Guidelines:** Mandatory specifications or requirements that are inherently qualitative and therefore require discretionary interpretation. Guidelines differ from Standards in that variation from them does not require formal modification. Compliance may be evaluated, and conditions amended or waived administratively.

Standards and Guidelines function as a system of controls to shape development consistent with City and community aspirations for an active, vibrant, livable and distinctive waterfront district. The Intent, Standards, and Guidelines are used to describe and delineate each of the four key development categories: Building Design, Private Open Space and Landscape, Building Signage, and Building Lighting.

**CHAPTER 4 BUILDING DESIGN STANDARDS & GUIDELINES**

*Design for Development*

**5 IMPLEMENTATION**

- 5.1 Review and Approval of Design Documents
- 5.2 Deviations and Variances
- 5.3 Process for Amendment of the Design for Development Documents
- 5.4 Interim Uses

*HUNTERS POINT SHIPYARD DESIGN FOR DEVELOPMENT*

**6 APPENDIX**

- 6.1 Term Definitions
- 6.2 List of Figures
- 6.3 Image Credits
- 6.4 Acknowledgments
- 6.5 Project Team
- 6.6 Sitewide Diagrams

# MENU OF OPTIONS

HUNTERS POINT SHIPYARD DESIGN FOR DEVELOPMENT | 03.12.2016

**Building Design**

**4.1.6 Facade Composition**

4.1.6.S1 Facade Composition (FC)

**Intent**

All buildings are composed of a series of composition strategies that give them character, distinction, and access to light. Differentiation in these strategies will create architectural variety in building facades and distinction from block to block.

**Strategy Types**

For HPS2, facade composition strategies are organized into the following four(4) strategy types:

- Facade Modulation (FC1)
- Facade Articulation (FC2)
- Fenestration (FC3)
- Material/Color (FC4)

**Definitions**

"Facade Composition"

The combination of building design strategies that include Facade Modulation (FC1), Facade Articulation (FC2), Fenestration (FC3), and Material/Color (FC4).

**Standards**

**4.1.6.S1 Facade Composition (FC)**

All Buildings shall have a Facade Composition comprising at a minimum two(2) strategy types.

Choose two(2) strategies:

- FC1 Facade Modulation
- FC2 Facade Articulation
- FC3 Fenestration
- FC4 Material/Color

Example: Building A uses the following two(2) strategies. See Figure 4.1v.

**Strategy 1**

FC1 Facade Modulation:

- Angular Shift and Horizontal Shift

**Strategy 2**

FC3 Fenestration:

- Punched Windows

Example: Building B uses the following two(2) strategies. See Figure 4.1w.

**Strategy 1**

FC2 Facade Articulation:

- Sun Shading Devices

**Strategy 2**

FC3 Fenestration:

- Bored Windows and Curtain Wall



Figure 4.1v: BUILDING A

Figure 4.1w: BUILDING B

HUNTERS POINT SHIPYARD DESIGN FOR DEVELOPMENT | 03.12.2016

**Building Design**

**4.1.6 Facade Composition (cont'd)**

4.1.6.S2 Block to Block Variation

**Intent**

To provide architectural variety and visual interest from block to block.

**Standards**

**4.1.6.S2 Block to Block Variation**

Blocks shall demonstrate distinction from one(1) block facade to adjacent block facade by incorporating variations in at least two(2) Facade Composition Strategy Types.

Vary facade with two(2) strategies:

- FC1 Facade Modulation
- FC2 Facade Articulation
- FC3 Fenestration
- FC4 Material/Color

Example:

If Block A and Block B both use the same Facade Modulation Strategy and Fenestration Strategy, then Block A and B shall be made distinct from one another in their Material/Color Strategy and Facade Articulation Strategy.



Figure 4.1x: BLOCK A (HORIZONTAL ARTICULATION AND MATERIAL COLOR STRATEGY 1)

Figure 4.1y: BLOCK A (VERTICAL ARTICULATION AND MATERIAL COLOR STRATEGY 2)



Figure 4.1z: BLOCK TO BLOCK VARIATION (ADJACENT BLOCK FACADES SHALL BE DISTINCT FROM BLOCK A)

HUNTERS POINT SHIPYARD DESIGN FOR DEVELOPMENT | 03.12.2016

**Building Design**

**4.1.6 Facade Composition (cont'd)**

FC1 Facade Modulation Strategies

**Modulation Types**

"Angular Shift"

A series of sloped or faceted surfaces along the facade. See Figure 4.1ac.

"Framing"

Elements of a facade can be identified as modules through the use of a frame or framing element. A frame can be a continuous protrusion which follows some perimeter at the facade scale. See Figure 4.1ad.

"Double Skin"

A facade system created by a second enclosure, typically lighter and slightly translucent or perforated, outboard of the main exterior building envelope. A double skin may have operable components and is meant to add depth and intricacy by way of light and shadows along the facade. See Figure 4.1ae.

"Structural Expression"

Structural elements such as beams, columns, cross-bracing, or fastenings can naturally break up a building's facade if made visible along a building's exterior. See Figure 4.1af.

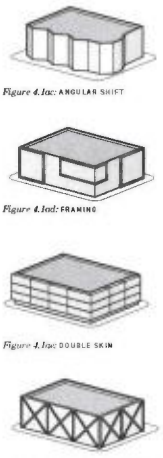


Figure 4.1ac: ANGULAR SHIFT

Figure 4.1ad: FRAMING

Figure 4.1ae: DOUBLE SKIN

Figure 4.1af: STRUCTURAL EXPRESSION

HUNTERS POINT SHIPYARD DESIGN FOR DEVELOPMENT | 03.12.2016

**Building Design**




Figure 4.1ba: DOUBLE SKIN FACADE MODULATION EXAMPLE

Figure 4.1bb: STRUCTURAL FACADE MODULATION EXPRESSION EXAMPLE

Figure 4.1bc: FRAMING FACADE MODULATION EXAMPLE

Figure 4.1bd: ANGULAR FACADE MODULATION SHIFT EXAMPLE



MENU OF OPTIONS

HUNTERS POINT SHIPYARD DESIGN FOR DEVELOPMENT | 03.12.2016

Building Design

4.1.6 Facade Composition (cont'd)  
FC3 Fenestration

Intent

Building Fenestration strategies are facade composition elements that contribute to the character and feel of a building and the urban environment. They control light into buildings, and provide visual access into and out of buildings.

Definitions

"Fenestration"  
The design, construction, or presence of openings in a building. Fenestration includes windows, doors, louvers, vents, wall panels, skylights, storefronts, curtain walls, and sloped glazed systems.

Application

Fenestration strategies include a variety of techniques to bring light into a building and help define the character of a building. Fenestration strategies include shape, size, pattern, rhythm, and location of facade apertures. Successful fenestration strategies stand out as a central component or feature of a building's enclosure. Such strategies strengthen the expression of the building's architectural character.

Strategy Types

- Glass Curtain Wall
- Punched Window
- Window Wall
- Double Skin
- Bayed Window
- Bay Window
- Atria as Facades

Varations in Fenestration strategies may include two[2] distinct strategies or may include a distinctly different application of the same strategy (such as a different pattern, size, and/or shape of punched windows).

Building Design



- 1. Punched Windows Example
- 2. Punched Windows - Curtain Wall Example
- 3. Curtain Wall Example
- 4. Curtain Wall and Atrium Example
- 5. Curtain Wall with Bayed Windows Example
- 6. Window Wall Example
- 7. Double Skin Example

HUNTERS POINT SHIPYARD DESIGN FOR DEVELOPMENT | 03.12.2016

HUNTERS POINT SHIPYARD DESIGN FOR DEVELOPMENT | 03.12.2016

Building Design

4.1.10 Ground Floor Activation (cont'd)

- 4.1.10.51 Zone 1 and 2 Active Entrances
- 4.1.10.52 Zone 3 Active Entrances
- 4.1.10.53 Waterfront Activation
- 4.1.10.54 Active Ground Floor Depth

- 4.1.10.55 Ground Floor Height
- 4.1.10.56 Ground Floor Activation
- 4.1.10.57 Stand-Alone Parking Structures

Standards

**4.1.10.51 Zone 1 and 2 Active Entrances**  
Zones 1 and 2 shall have a minimum average of two[2] active entrances per [150] ft. of facade length.

**4.1.10.52 Zone 3 Active Entrances**  
Zone 3 shall have a minimum average of one[1] active entrances per [100] ft. of facade length.

**4.1.10.53 Waterfront Activation**  
A minimum [3,500] sq. ft. of publicly accessible Type "A" use shall be provided along the waterfront facades of Blocks 26, 27 or 28. The amount of use may be in one or multiple spaces and/or blocks.

**4.1.10.54 Active Ground Floor Depth**  
The minimum depth of ground floor active uses for all non-residential buildings, not including service corridors, is [20] ft.; for residential buildings the minimum is [10] ft.

**4.1.10.55 Ground Floor Height**  
Type A and Type B active uses shall have a minimum ground floor to floor height of [15] ft.

**4.1.10.56 Ground Floor Activation**  
Zone 1  
Facades shall include a minimum 85% of active Type [A] uses.

**Zone 2 and 3**  
Blocks shall include a minimum 75% of active uses on the ground floor. Active uses shall consist of those established in Figure 4.1b.

The percentage is calculated by taking the combined total length of all active use facades around the perimeter of each block and dividing by the overall length of all facades within that same block:

Each single facade shall have a minimum 50% of active uses.

**Zone 4**  
Stand-alone parking structures in Zone 4 shall comply with 4.1.10.57 Stand-Alone Parking Structure Activation. If buildings in Zone 4 are built as primarily commercial or residential uses, ground floor activation rules for Zones 2 and 3 shall apply.



$$\frac{X}{150} \times 120 = \text{Entries Req. for Zone 1 \& 2}$$
$$\frac{X}{100} \times 110 = \text{Entries Req. for Zone 3}$$

Figure 4.1b: ACTIVE ENTRANCES CALCULATION

$$\frac{[A1 + A2 + A3 + A4 + A5 + A6 + A7 + A8]}{[L1 + L2 + L3 + L4]} \times 100 = \% \text{ Active}$$

Figure 4.1b: ACTIVE USE PERCENTAGE CALCULATION

HUNTERS POINT SHIPYARD DESIGN FOR DEVELOPMENT | 03.12.2016



Figure 4.1b: GROUND FLOOR USE REQUIREMENTS

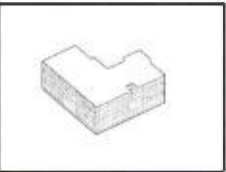
86 CHAPTER 4 BUILDING DESIGN STANDARDS & GUIDELINES

A1 BUILDING DESIGN

CHAPTER 4 BUILDING DESIGN STANDARDS & GUIDELINES 87



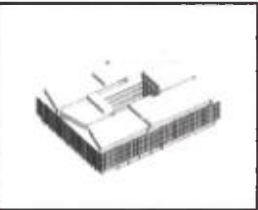
# S, M, L, XL FLOORPLATES



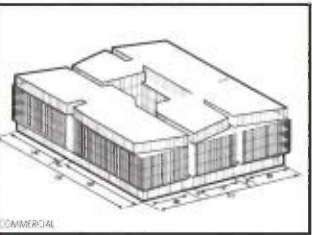
SMALL: PLAN LENGTH <150



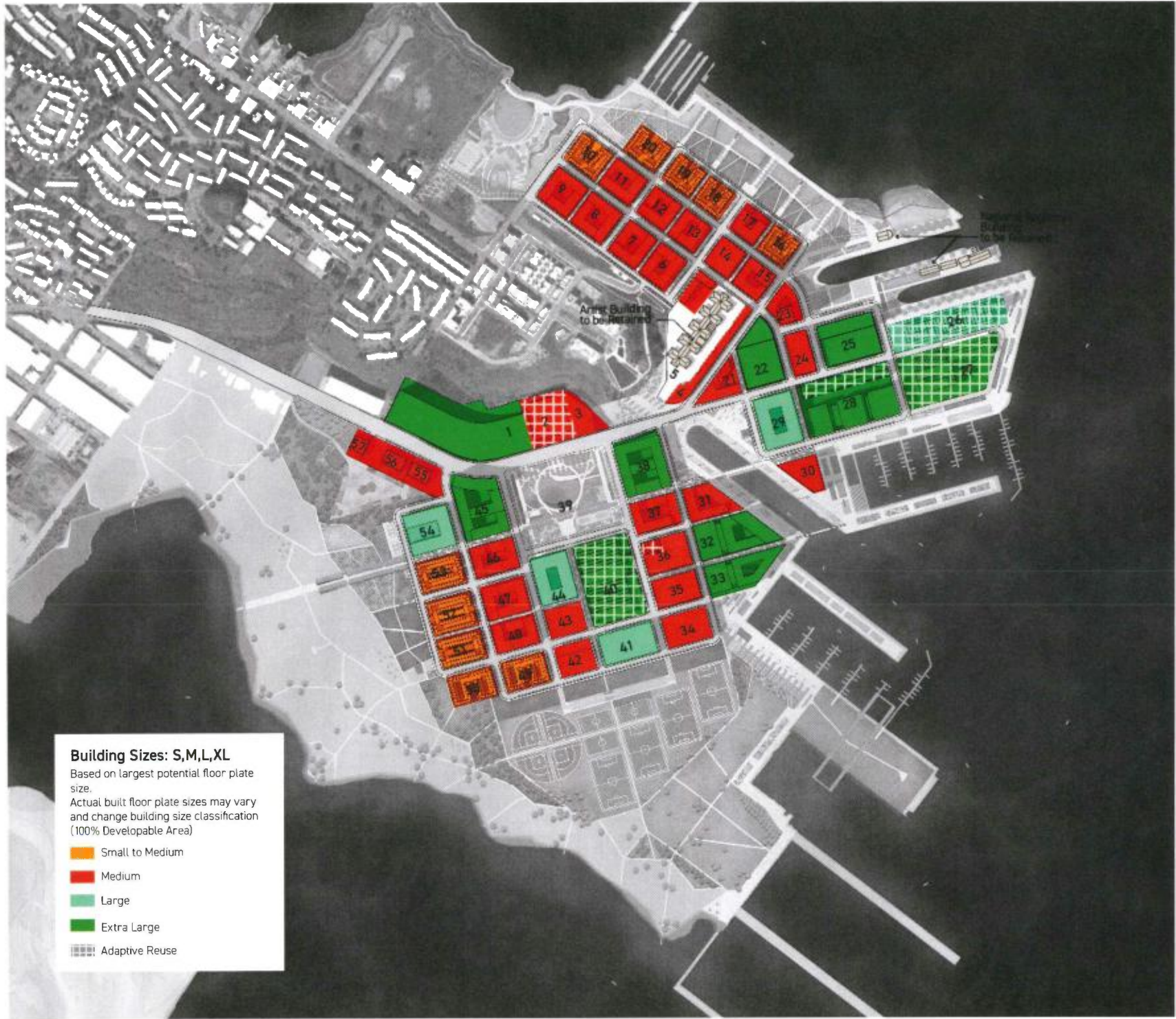
MEDIUM: <70K SF



LARGE: 70-100K SF



X-LARGE: >100K SF



**Building Sizes: S,M,L,XL**  
Based on largest potential floor plate size.  
Actual built floor plate sizes may vary and change building size classification (100% Developable Area)

- Small to Medium
- Medium
- Large
- Extra Large
- Adaptive Reuse

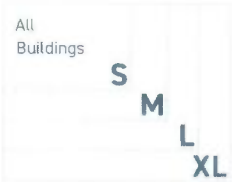


# DESIGN STANDARDS

## 4.0 USER GUIDE TO APPLY BUILDING DESIGN STANDARDS & GUIDELINES BASED ON FLOOR PLATE SIZE

### 01 Apply Standards\*

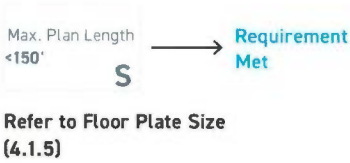
All buildings are required to meet standards as applicable.



**Building Design Standards & Guidelines (4.1 to 4.4.4)**  
Apply standards as applicable

### 02 Determine Further Requirements for Bulk and Massing

Measure maximum building plan length and floor plate size to determine whether further requirements for bulk and massing are required.



**Refer to Floor Plate Size (4.1.5)**



**For M,L, or XL buildings, apply at least one[1] bulk and massing strategy (4.1.7)**

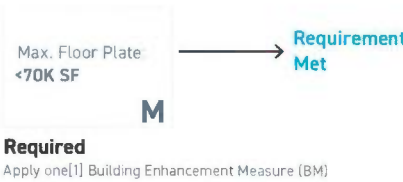
- 4.1.7.S2 Significant Breaks
- 4.1.7.S3 Upper Floor Step Backs
- 4.1.7.S4 Facade Variation

### 03 Determine and Apply Additional Enhancement Measures as Required

M, L, and XL buildings are required to apply additional Building Enhancement Measure (BM) and/or Public Realm Enhancement Measure (PM). For L and XL buildings, select either Option 1 or 2.

| Building Enhancement Measures (BM)     |   |
|--|---|
| BM1                                    | Apply one[1] additional bulk and massing strategy |
| BM2                                    | Face private courtyards/atria onto public ROW     |
| BM3                                    | Distinct corner architecture feature              |
| BM4                                    | Roof expression                                   |
| BM5                                    | Expressive entrances                              |
| BM6                                    | Increased transparency                            |
| BM7                                    | Visual access to interior courtyard/atrium        |
| BM8                                    | 24/7 access to open space                         |
| BM9                                    | Reduction in developable area                     |
| BM10                                   | Additional active entries                         |
| Public Realm Enhancement Measures (PM) |   |
| PM1                                    | Public access through the building                |
| PM2                                    | Public access through open space connections      |

**For M,L, or XL buildings, apply required amount of Enhancement Measures (4.1.7.S5)**



**Required**  
Apply one[1] Building Enhancement Measure (BM)



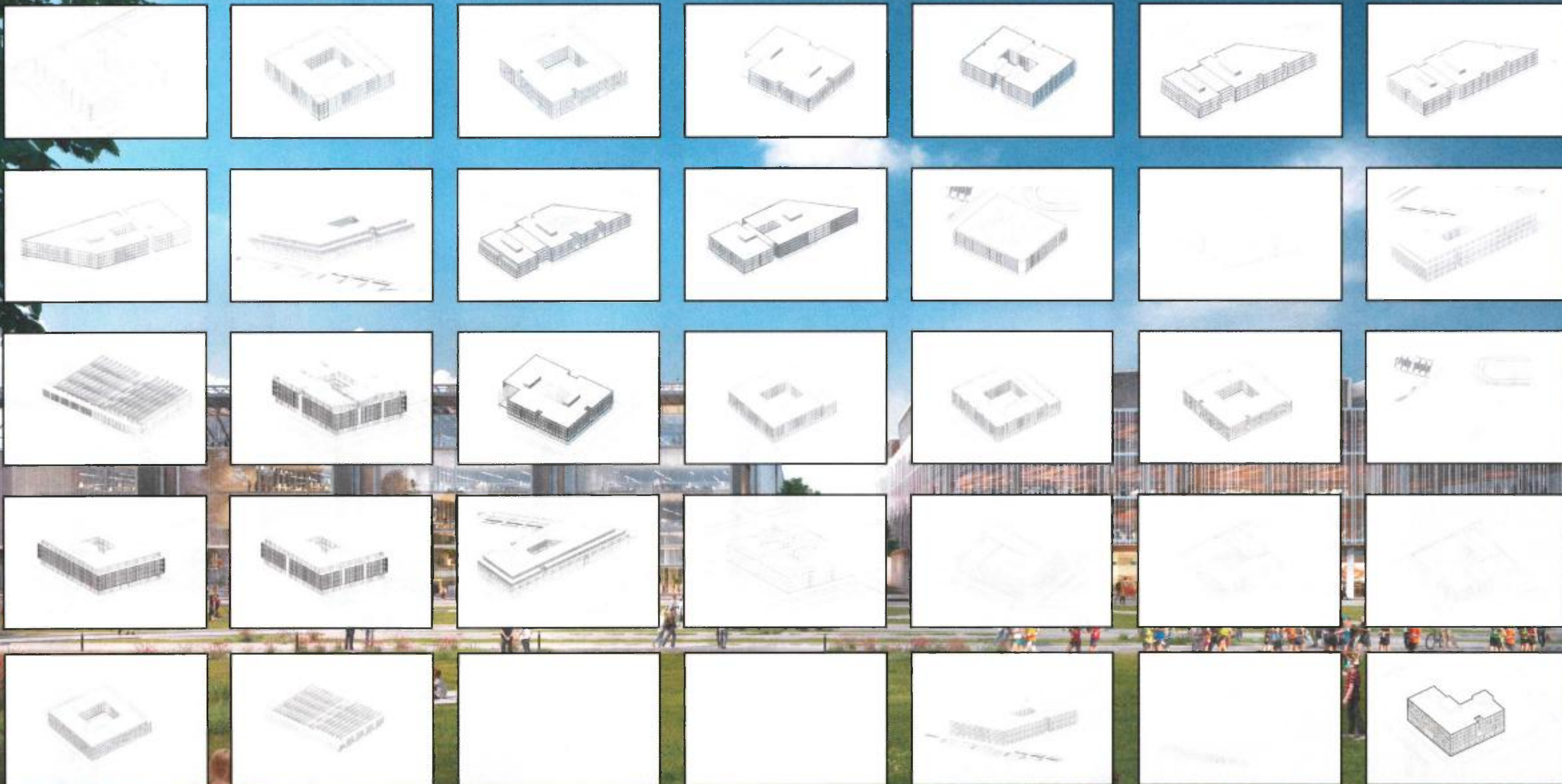
- Option 1**  
Apply three[3] Building Enhancement Measures (BM)
- Option 2**  
Apply one[1] Building Enhancement Measure (BM)  
Apply one[1] Public Realm Enhancement Measure (PM)



- Option 1**  
Apply four[4] Building Enhancement Measures (BM)
- Option 2**  
Apply two[2] Building Enhancement Measures (BM)  
Apply one[1] Public Realm Enhancement Measure (PM)

\* All buildings may apply more Standards & Guidelines at the discretion of the designer, but no less than required.

# TESTING VARIETY





# PRECEDENT ANALYSIS

## RIJINSTRAAT 8

the Hague, the Netherlands

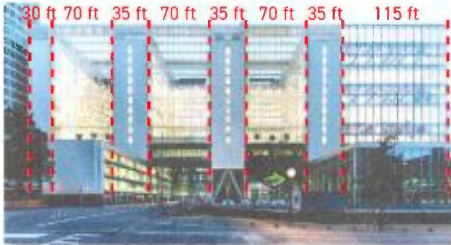
Floorplate - Atrium



Height



Apparent Face - Vertical Change



Horizontal Change



Eye-Level View



## RIJINSTRAAT 8

the Hague, the Netherlands

### STEP 1. APPLY: DEVELOPMENT BLOCK COVERAGE STANDARDS, HEIGHT REGULATIONS, BUILDING SETBACKS, MAXIMUM PLAN LENGTH

Development Block Coverage:

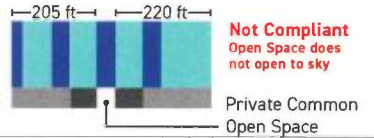
Non-Residential: 65% above 40 ft  
(Required: <90% above 40 ft)



Maximum Plan Length: 460 ft

No significant break for the 500 ft facade  
(Required: >25 ft significant break for facade >400 ft)

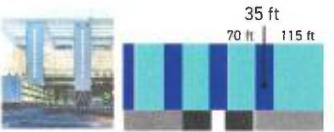
Private Common Open Space with access to opposite side, but not open to sky



### STEP 2. FACADE LENGTH

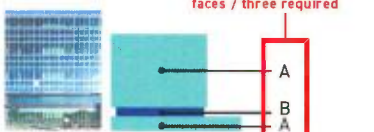
Plan Length > 150 feet  
For all facades in this development, apply one of the Bulk/Massing Strategies

### STEP 3. BULK/MASSING STRATEGIES



Facade 1 (460 ft)

Facade Variation: Vertical  
Variations (Max. 115 ft)  
- Fenestration/Transparency  
- Material/Color



Facade 2 (275 ft)

Facade Variation: Horizontal Variations  
- Fenestration/Transparency  
- Facade Modulation

### STEP 4. FLOORPLATE SIZE

57,500 sf above 40 ft - Medium Building Floorplate

**Thumbs Up Icon**  
If adjusting facade composition on  
Facade 2 to comply with standards  
If extending the private common  
open space all the way to the roof

### ADDITIONAL BULK/MASSING STRATEGIES APPLIED

- ☐ Face private courtyards and atria onto a public ROW or MBB
- ☐ Extend Atria/Courtyards to the Ground Floor
- ☐ Increased Transparency
- ☐ Provide Visual Access to interior courtyard and/or atrium
- ☐ Public Access through the Building



## OPEN SPACE PRECEDENTS



1. Private Common Open Space  
Rooftop Example
2. Private Individual Open Space  
Balcony Example
3. Private Common Open Space  
Internal Courtyard Gardens Example
4. Private Individual Open Space  
Front Yard Example



CANDLESTICK POINT & HUNTERS POINT SHIPYARD PROJECT UPDATE

# PROJECT VISION





# PROJECT VISION





## PROJECT VISION





CANDLESTICK POINT & HUNTERS POINT SHIPYARD PROJECT UPDATE

# PROJECT VISION



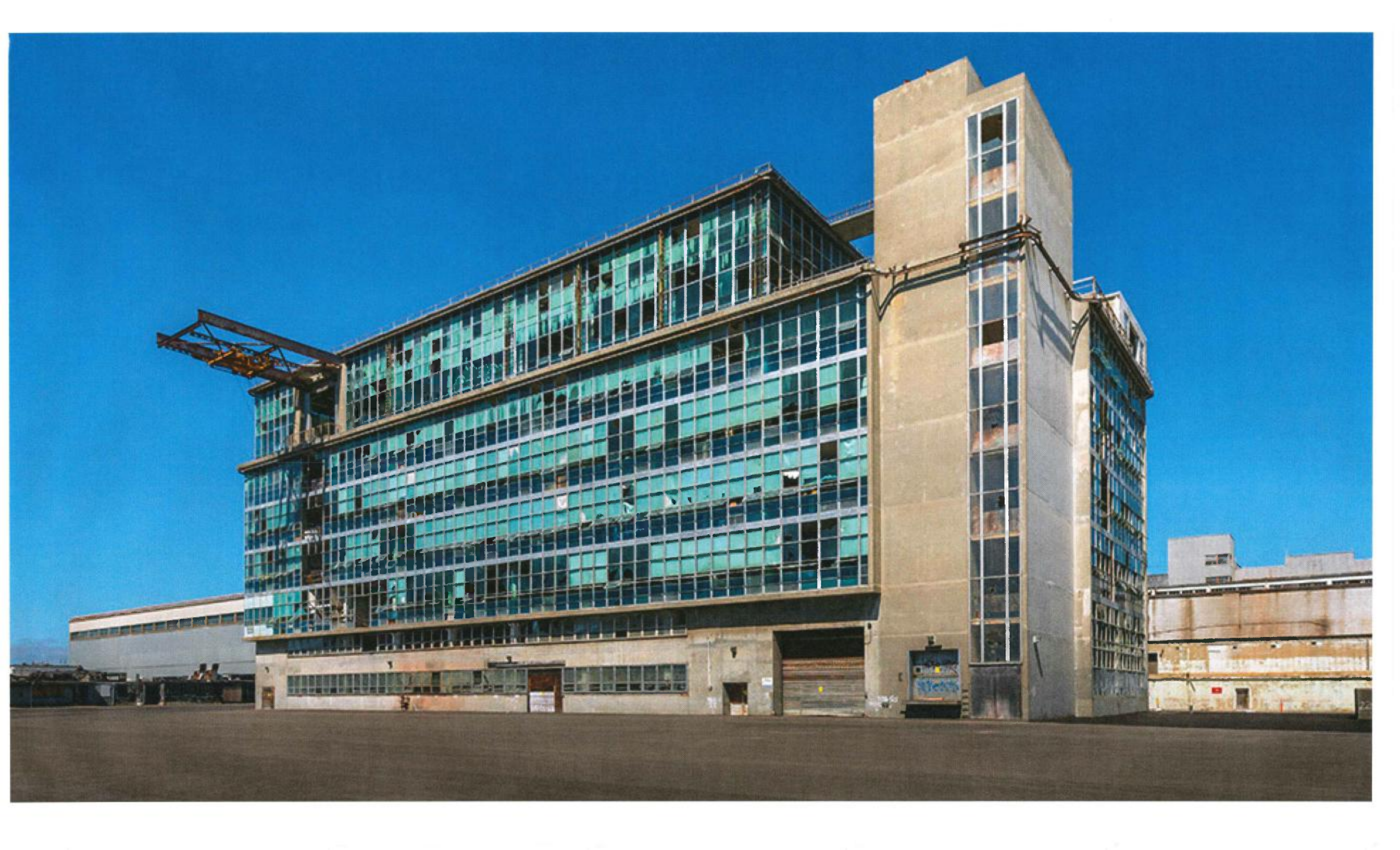


CANDLESTICK POINT & HUNTERS POINT SHIPYARD PROJECT UPDATE

# PROJECT VISION











---

# NEXT STEPS

---



# AGREEMENTS TO BE AMENDED

01

**2010 HPS & BVHP  
REDEVELOPMENT PLAN**

*Establishes legal authority and  
permitted land uses*

*Conforming amendments will also be  
made to the BVHP Redevelopment Plan*

02

**PHASE 1 & 2 DISPOSITION &  
DEVELOPMENT AGREEMENT (DDA)**

*Grants development rights to FivePoint  
and requires certain obligations regarding  
public facilities and community benefits*

03

**PROJECT DOCUMENTS**

*Conforming amendments to the DDA  
Exhibits to reflect the updated master plan*

- *BMR Housing Plan*
- *Community Benefits Plan*
- *D4D*
- *DRDAP*
- *Financing Plan*
- *Infrastructure Plan*
- *Parks & Open Space Plan*
- *Schedule of Performance*
- *Sustainability Plan*
- *Transportation Plan*

# COMMUNITY OUTREACH





# PROJECT TIMELINE

## OUR FUTURE



# ACTION TO BE TAKEN

*Planning Commission, April 26, 2018*

**01**

Approval of amendments to:

- Bayview Hunters Point Area Plan
- Candlestick Point Sub-Area Plan
- Hunters Point Area Plan
- Zoning Maps of the Candlestick Point Activity Node Special Use District and CP Height and Bulk District
- Hunters Point Shipyard Design for Development
- Candlestick Point Design for Development

**02**

Make General Plan Consistency Findings regarding amendments to the Hunters Point Shipyard Redevelopment Plan and Bayview Hunters Point Redevelopment Plan





# SAN FRANCISCO PLANNING DEPARTMENT

Received at CPC Hearing 3/22/18  
M. Small

## Planning Commission Draft Resolution

HEARING DATE MARCH 22, 2018

1650 Mission St.  
Suite 400  
San Francisco,  
CA 94103-2479

Reception:  
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415.558.6409

Planning  
Information:  
415.558.6377

*Project Name:* Urban Design Guidelines  
*Case Number:* 2016-000162CWP  
*Staff Contact:* Maia Small, Principal Urban Designer and Architect,  
Current/Citywide Planning Divisions  
maia.small@sfgov.org, 415-575-9160  
*Reviewed by:* Jeff Joslin, Director of Current Planning  
jeff.joslin@sfgov.org, 415-575-9117

**RESOLUTION ADOPTING THE URBAN DESIGN GUIDELINES FOR ALL PROJECTS IN COMMERCIAL, MIXED-USE AND NEIGHBORHOOD COMMERCIAL DISTRICTS AND PROJECTS WITH TWENTY-FIVE OR MORE RESIDENTIAL UNITS, 150' LONGER FRONTAGE, OR NON-RESIDENTIAL USES IN RESIDENTIAL DISTRICTS; ADOPTING FINDINGS, INCLUDING ENVIRONMENTAL FINDINGS, AND FINDINGS OF CONSISTENCY WITH THE GENERAL PLAN AND PLANNING CODE SECTION 101.1.**

WHEREAS, in 2013 the Planning Commission requested that the Planning Department develop design guidelines that will provide consistently-applied design direction for projects during a period of increased development growth with competing public and private priorities;

WHEREAS, the Planning Department, in response, has developed Urban Design Guidelines derived through an intensive analysis of existing urban design policy found in the General Plan and other adopted area-specific, zoning-based, or use type, guidelines across the city that cover the topics of Site Design, Architecture and Public Realm;

WHEREAS, the Urban Design Guidelines support the built environment goals and values found in the General Plan that include that new projects be contextual and reflect existing neighborhood architecture and urban patterns; contribute high quality design to the city based on best professional standards and practices; enhance neighborhood uniqueness and cultural character; support an active pedestrian environment and human-scaled design; and reinforce sustainability practices.

WHEREAS, the Urban Design Guidelines will not modify or supersede any existing design guidelines or the General Plan;

WHEREAS, the Planning Department worked extensively with design and development professionals, city agencies, neighborhood groups, and members of the public over three years to refine the process, applicability, and content of the Urban Design Guidelines and considerably revised all three by broadening the outreach, changing the applicability, removing the waiver, balancing the content to

## URBAN DESIGN ELEMENT

### OBJECTIVE 1

EMPHASIS OF THE CHARACTERISTIC PATTERN WHICH GIVES TO THE CITY AND ITS NEIGHBORHOODS AN IMAGE, A SENSE OF PURPOSE, AND A MEANS OF ORIENTATION.

#### Policy 1.1

Recognize and protect major views in the city, with particular attention to those of open space and water.

*The proposed Urban Design Guideline S4 requires projects to "Create, Protect, and Support View Corridors" from the public realm. The proposed Urban Design Guideline A4 requires projects to "Design Buildings from Multiple Vantage Points" understanding that San Francisco's unique topography affords view corridors that highlight architecture in the urban fabric.*

#### Policy 1.2

Recognize, protect and reinforce the existing street pattern, especially as it is related to topography.

*The proposed Urban Design Guideline S2 requires projects to "Harmonize Relationships between Buildings, Streets, and Open Spaces." The proposed Urban Design Guideline S5 requires projects to "Create a Defined and Active Streetwall" specifically considering the width and character of the street. The proposed Urban Design Guideline S3 requires projects to "Recognize and Enhance Unique Conditions" including the specific site conditions of street crossings.*

#### Policy 1.4

Protect and promote large-scale landscaping and open space that define districts and topography.

*The proposed Urban Design Guideline S8 requires projects to "Respect and Exhibit Natural Systems and Features" including supporting existing topography and open space.*

#### Policy 1.5

Emphasize the special nature of each district through distinctive landscaping and other features.

*The proposed Urban Design Guideline P3 requires projects to "Express Neighborhood Character in Open Space Designs."*

#### Policy 1.10

Indicate the purposes of streets by adopting and implementing the Better Streets Plan, which identifies a hierarchy of street types and appropriate streetscape elements for each street type.

*The proposed Urban Design Guideline P4 requires projects to "Support Public Transportation and Bicycling" through the design of architecture and public realm amenities that encourage the use of both.*



*The proposed Urban Design Guideline S7 requires projects to "Integrate Common Open Space and Landscape with Architecture" to better organize building massing for the benefit of natural ground and open space.*

#### **OBJECTIVE 4**

**IMPROVEMENT OF THE NEIGHBORHOOD ENVIRONMENT TO INCREASE PERSONAL SAFETY, COMFORT, PRIDE AND OPPORTUNITY**

##### **Policy 4.12**

Install, promote and maintain landscaping in public and private areas.

*The proposed Urban Design Guideline P5 requires projects to "Design Sidewalks to Enhance the Pedestrian Experience" including adding landscaping to sidewalk areas for public enjoyment and stormwater management.*

##### **Policy 4.13**

Improve pedestrian areas by providing human scale and interest.

*The proposed Urban Design Guideline A8 requires projects to "Design Active Building Fronts" to enhance the pedestrian experience and encourage neighborhood activity. The proposed Urban Design Guidelines A6 and A7 requires projects to "Render Building Facades with Texture and Depth" and "Coordinate Building Elements" to provide visual interest for pedestrians therefore encouraging walking and neighborhood engagement.*

#### **COMMERCE AND INDUSTRY ELEMENT**

#### **OBJECTIVE 6**

**MAINTAIN AND STRENGTHEN VIABLE NEIGHBORHOOD COMMERCIAL AREAS EASILY ACCESSIBLE TO CITY RESIDENTS.**

##### **Policy 6.7**

Promote high quality urban design on commercial streets.

*The proposed Urban Design Guidelines S4, S5, S6, A8, and P5 requires projects to "Harmonize Relationships between Buildings, Streets, and Open Spaces;" "Create a Defined and Active Streetwall," "Organize Uses to Complement the Public Environment;" "Design Active Building Fronts;" and "Design Sidewalks to Enhance the Pedestrian Experience" all to foster neighborhood compatibility and enhance commerce and storefront uses in Neighborhood Commercial Districts and harmonize their compatibility with Residential Districts.*

2. **Planning Code Section 101 Findings.** The proposed amendments to the Planning Code are consistent with the eight Priority Policies set forth in Section 101.1(b) of the Planning Code in that:

1. That existing neighborhood-serving retail uses be preserved and enhanced and future



# SAN FRANCISCO PLANNING DEPARTMENT

Received at CPC Hearing 3/22/18  
M. Small

## Planning Commission Draft Resolution

HEARING DATE MARCH 22, 2018

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**RESOLUTION ADOPTING THE URBAN DESIGN GUIDELINES FOR ALL PROJECTS IN COMMERCIAL, MIXED-USE AND NEIGHBORHOOD COMMERCIAL DISTRICTS AND PROJECTS WITH TWENTY-FIVE OR MORE RESIDENTIAL UNITS, 150' LONGER FRONTAGE, OR NON-RESIDENTIAL USES IN RESIDENTIAL DISTRICTS; ADOPTING FINDINGS, INCLUDING ENVIRONMENTAL FINDINGS, AND FINDINGS OF CONSISTENCY WITH THE GENERAL PLAN AND PLANNING CODE SECTION 101.1.**

WHEREAS, in 2013 the Planning Commission requested that the Planning Department develop design guidelines that will provide consistently-applied design direction for projects during a period of increased development growth with competing public and private priorities;

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include more Neighborhood Commercial examples, and revising numerous aspects of the content as offered and requested;

WHEREAS, Special Area Design Guidelines for North Beach, Polk, and Pacific Neighborhood Commercial Districts have been in development since August 2017, and are currently in draft form;

WHEREAS, the proposed guidelines are not defined as a project under the California Environmental Quality Act Sections 15378 and 15060(c)(2) because they do not intensify development or change or affect zoning or transportation in the built environment;

WHEREAS, the Planning Commission has heard and considered the testimony presented to it at the public hearing and has further considered written materials and oral testimony presented on behalf of Department staff and other interested parties beginning with Planning Commission informationals on January 21, 2016; October 20, 2016; May 11, 2017; and January 11, 2018; and

WHEREAS, all pertinent documents may be found in the files of the Department, as the custodian of records, at 1650 Mission Street, Suite 400, San Francisco; and

MOVED, that the Planning Commission hereby approves the proposed Urban Design Guidelines (included as Exhibit A) for application in Commercial, Mixed-Use, and Neighborhood Commercial Districts except for the North Beach, Polk, and Pacific Neighborhood Commercial Districts and in Residential districts for projects with non-residential uses or residential projects with twenty-five units or more or with a frontage longer than 150'. The application of the Urban Design Guidelines in Residential Districts for residential projects with twenty-five units or more or with a frontage longer than 150' shall no longer apply after the adoption of a revision to the Residential Design Guidelines. Sites in National Register, California Register, Article 10 and Article 11 Historic Districts will be exempt from Urban Design Guidelines conformance.

Department staff will report annually to the Planning Commission on any proposed updates, functionality, compliance, or guideline interpretation issues. The effective date for application of the Urban Design Guidelines will be for site permits or project applications submitted on or after April 1, 2018 in applicable areas.

The proposed Special Area Guidelines, a separate project underway at the Planning Department, provides essential area-specific design guidance in key neighborhoods and thus should be supported by Department efforts as so-desired by established neighborhood groups that represent Neighborhood Commercial Districts.

## **FINDINGS**

Having reviewed the materials identified in the preamble above, and having heard all testimony and arguments, this Commission finds, concludes, and determines as follows:

1. **General Plan Compliance.** The proposed Urban Design Guidelines are consistent with the following Objectives and Policies of the General Plan:

## URBAN DESIGN ELEMENT

### OBJECTIVE 1

EMPHASIS OF THE CHARACTERISTIC PATTERN WHICH GIVES TO THE CITY AND ITS NEIGHBORHOODS AN IMAGE, A SENSE OF PURPOSE, AND A MEANS OF ORIENTATION.

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#### Policy 1.2

Recognize, protect and reinforce the existing street pattern, especially as it is related to topography.

*The proposed Urban Design Guideline S2 requires projects to "Harmonize Relationships between Buildings, Streets, and Open Spaces." The proposed Urban Design Guideline S5 requires projects to "Create a Defined and Active Streetwall" specifically considering the width and character of the street. The proposed Urban Design Guideline S3 requires projects to "Recognize and Enhance Unique Conditions" including the specific site conditions of street crossings.*

#### Policy 1.4

Protect and promote large-scale landscaping and open space that define districts and topography.

*The proposed Urban Design Guideline S8 requires projects to "Respect and Exhibit Natural Systems and Features" including supporting existing topography and open space.*

#### Policy 1.5

Emphasize the special nature of each district through distinctive landscaping and other features.

*The proposed Urban Design Guideline P3 requires projects to "Express Neighborhood Character in Open Space Designs."*

#### Policy 1.10

Indicate the purposes of streets by adopting and implementing the Better Streets Plan, which identifies a hierarchy of street types and appropriate streetscape elements for each street type.

*The proposed Urban Design Guideline P4 requires projects to "Support Public Transportation and Bicycling" through the design of architecture and public realm amenities that encourage the use of both.*



## **OBJECTIVE 2**

CONSERVATION OF RESOURCES WHICH PROVIDE A SENSE OF NATURE, CONTINUITY WITH THE PAST, AND FREEDOM FROM OVERCROWDING.

### **Policy 2.6**

Respect the character of older development nearby in the design of new buildings.

*The proposed Urban Design Guidelines A2 and A3 require projects to "Modulate Buildings Vertically and Horizontally" and "Harmonize Building Designs with Neighboring Scale and Materials" to direct projects to be compatible with neighboring building context.*

### **Policy 2.7**

Recognize and protect outstanding and unique areas that contribute in an extraordinary degree to San Francisco's visual form and character.

*The proposed Urban Design Guidelines A2 and A3 require projects to "Modulate Buildings Vertically and Horizontally" and "Harmonize Building Designs with Neighboring Scale and Materials" to be compatible with neighboring building context and support the visual form and character of the city.*

## **OBJECTIVE 3**

MODERATION OF MAJOR NEW DEVELOPMENT TO COMPLEMENT THE CITY PATTERN, THE RESOURCES TO BE CONSERVED, AND THE NEIGHBORHOOD ENVIRONMENT.

### **Policy 3.1**

Promote harmony in the visual relationships and transitions between new and older buildings.

*The proposed Urban Design Guideline S2 requires projects to "Harmonize Relationships between Buildings, Streets, and Open Spaces" asks new projects to match massing patterns and sculpt to accommodate existing building massing, setbacks, and block patterns. The proposed Urban Design Guideline A2 requires projects to "Modulate Buildings Vertically and Horizontally" to be compatible with neighboring building lot widths and massing.*

### **Policy 3.2**

Avoid extreme contrasts in color, shape and other characteristics which will cause new buildings to stand out in excess of their public importance.

*The proposed Urban Design Guideline A3 requires projects to "Harmonize Building Designs with Neighboring Scale and Materials" to be compatible with neighboring building context and avoid standing out without a larger civic purpose.*

### **Policy 3.4**

Promote building forms that will respect and improve the integrity of open spaces and other public areas.

*The proposed Urban Design Guideline S7 requires projects to "Integrate Common Open Space and Landscape with Architecture" to better organize building massing for the benefit of natural ground and open space.*

#### **OBJECTIVE 4**

**IMPROVEMENT OF THE NEIGHBORHOOD ENVIRONMENT TO INCREASE PERSONAL SAFETY, COMFORT, PRIDE AND OPPORTUNITY**

##### **Policy 4.12**

Install, promote and maintain landscaping in public and private areas.

*The proposed Urban Design Guideline P5 requires projects to "Design Sidewalks to Enhance the Pedestrian Experience" including adding landscaping to sidewalk areas for public enjoyment and stormwater management.*

##### **Policy 4.13**

Improve pedestrian areas by providing human scale and interest.

*The proposed Urban Design Guideline A8 requires projects to "Design Active Building Fronts" to enhance the pedestrian experience and encourage neighborhood activity. The proposed Urban Design Guidelines A6 and A7 requires projects to "Render Building Facades with Texture and Depth" and "Coordinate Building Elements" to provide visual interest for pedestrians therefore encouraging walking and neighborhood engagement.*

### **COMMERCE AND INDUSTRY ELEMENT**

#### **OBJECTIVE 6**

**MAINTAIN AND STRENGTHEN VIABLE NEIGHBORHOOD COMMERCIAL AREAS EASILY ACCESSIBLE TO CITY RESIDENTS.**

##### **Policy 6.7**

Promote high quality urban design on commercial streets.

*The proposed Urban Design Guidelines S4, S5, S6, A8, and P5 requires projects to "Harmonize Relationships between Buildings, Streets, and Open Spaces;" "Create a Defined and Active Streetwall," "Organize Uses to Complement the Public Environment;" "Design Active Building Fronts;" and "Design Sidewalks to Enhance the Pedestrian Experience" all to foster neighborhood compatibility and enhance commerce and storefront uses in Neighborhood Commercial Districts and harmonize their compatibility with Residential Districts.*

2. **Planning Code Section 101 Findings.** The proposed amendments to the Planning Code are consistent with the eight Priority Policies set forth in Section 101.1(b) of the Planning Code in that:

1. That existing neighborhood-serving retail uses be preserved and enhanced and future



opportunities for resident employment in and ownership of such businesses enhanced;

*The proposed Urban Design Guidelines would enhance neighborhood-serving retail by retaining and supporting the maintenance of its use and built environment character.*

2. That existing housing and neighborhood character be conserved and protected in order to preserve the cultural and economic diversity of our neighborhoods;

*The proposed Urban Design Guidelines would enhance the retention and maintenance of neighborhood character by requiring that new projects be compatible with neighborhood characteristics at the site design, architecture and public realm scales.*

3. That the City's supply of affordable housing be preserved and enhanced;

*The proposed Urban Design Guidelines would not have an adverse effect on the City's supply of affordable housing.*

4. That commuter traffic not impede MUNI transit service or overburden our streets or neighborhood parking;

*The proposed Urban Design Guidelines would not result in commuter traffic impeding MUNI transit service or overburdening the streets or neighborhood parking.*

5. That a diverse economic base be maintained by protecting our industrial and service sectors from displacement due to commercial office development, and that future opportunities for resident employment and ownership in these sectors be enhanced;

*The proposed Urban Design Guidelines would not cause displacement of the industrial or service sectors due to office development, and future opportunities for resident employment or ownership in these sectors would not be impaired.*

6. That the City achieve the greatest possible preparedness to protect against injury and loss of life in an earthquake;

*The proposed Urban Design Guidelines would not have an adverse effect on City's preparedness against injury and loss of life in an earthquake.*

7. That the landmarks and historic buildings be preserved;

*The proposed Urban Design Guidelines would not have an adverse effect on the City's Landmarks and historic buildings.*

8. That our parks and open space and their access to sunlight and vistas be protected from development;

*The proposed Urban Design Guidelines would not have an adverse effect on the City's parks and open space and their access to sunlight and vistas.*

3. The application of the Urban Design Guidelines will support neighborhood compatibility and encourage the use of best professional design practices and standards for projects in Commercial, Mixed-Use, and Neighborhood Commercial Districts and in Residential districts for projects with non-residential uses or residential projects with twenty-five units or more or with a frontage longer than 150', in particular on sites where few or no design guidelines apply.

NOW THEREFORE BE IT RESOLVED that the Commission hereby APPROVES the proposed Urban Design Guidelines as described in this Resolution.

I hereby certify that the foregoing Resolution was adopted by the Commission at its meeting on MARCH 22, 2018.

Jonas P. Ionin  
Commission Secretary

AYES:

NOES:

ABSENT:

ADOPTED:



VAGUE UDGs ORCHESTRATE FUTURE CHANGES TO PLANNING CODE (PCODE) AS THEY VIOLATE THEM TODAY

PCOMMISSION = ENFORCERS OF PCODE, WHY THEN PASS UDGs TODAY THAT DON'T FOLLOW PCODE & IS ILLEGAL?

UDG IDEAS CRAFTED TO CHANGE EXISTING/FUTURE DESIGN GUIDELINES & MUCH OF PCODE TO ALIGN.

TO FIX ILLEGALITY OF UDGs NOT FOLLOWING PCODE TODAY, AMENDMENTS TO PCODE TO LEGITIMIZE ITS EXISTENCE WILL COME BEFORE THE COMMISSION & BOARD OF SUPERVISORS TO REMEDY.

UDGs = WRITTEN AS A FORCE OF LAW WITHOUT THE REVIEW OF LAW

WITHOUT HAVING THE PCODE AMENDMENTS IN HAND, THOUGH PLANNING STATED MURKY UDGs SUPPOSED TO GIVE GREATER CERTAINTY TO NEIGHBORHOODS BUT CONTRARY, CAN'T MAKE FULLY INFORMED DECISION TO PASS TODAY.

IF COMMISSION & PLANNERS REVIEW PROJECTS BASED ON FLAWED & ILLEGAL UDGs, ANYTHING IN IT NOT FOLLOWING PCODE NEEDS TO BE REJECTED OR YOU'LL HAVE MADE ILLEGAL LAND USE DECISIONS.

I'M HERE TO TELL PEOPLE THAT WHAT'S HAPPENING HERE TODAY IS THAT THE

- UDGs ORCHESTRATE THE FUTURE CHANGES TO PLANNING CODE (PCODE) IN ALL THEIR VAGUENESS AND IN CERTAIN OTHER ASPECTS TO VIOLATE THE CODE.
- YOU'RE ENFORCERS OF PCODE... WHY WOULD YOU PASS THE UDGs THAT DON'T FOLLOW PCODE & IS ILLEGAL TODAY?

THE "IT'S-ONLY-ABOUT-DESIGN" IDEAS OF UDGs ARE

- CRAFTED TO CHANGE ALL OTHER EXISTING AND FUTURE DESIGN GUIDELINES \*AND\* NEEDS CHANGES TO PCODE TO ALIGN WITH IT.
- IN ORDER TO FIX THE ILLEGALITY OF UDGs NOT FOLLOWING WITH PCODE TODAY, AMENDMENTS TO LEGITIMIZE ITS EXISTENCE WILL COME BEFORE THE COMMISSION AND BOARD OF SUPERVISORS TO REMEDY.
- THE UDGs ARE WRITTEN AS A FORCE OF LAW WITHOUT THE REVIEW OF LAW.
- WITHOUT HAVING THE PCODE AMENDMENTS IN HAND TODAY, THOUGH PLANNING STATED THE MURKY UDGs SUPPOSED TO GIVE GREATER CERTAINTY TO NEIGHBORHOODS BUT CONTRARY, YOU CAN'T MAKE FULLY INFORMED DECISION TO PASS TODAY.
- IF THE COMMISSION AND PLANNERS REVIEW PROJECTS BASED ON THE FLAWED AND ILLEGAL UDGs, ANYTHING IN IT THAT DOESN'T FOLLOW PCODE NEEDS TO BE REJECTED OR YOU WILL HAVE ILLEGALLY MADE LAND USE DECISIONS.



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# Project 2514 Balboa Opposition

Public Comment  
for the record

Steve Cuporelli  
2524 Balboa St #3  
San Francisco, CA 94121

13-year resident at the same location

My main two concerns are:

- I
1. Qualification of the Architect  
What are this firm's background and education
  2. How many of these projects have they completed in the past.

- II
- What type of construction materials are being used:
1. Are above building specification
  2. Are they earthquake proof

---

Shannel Busuioc (adjacent to property)  
2526 Balboa St. #2

I appreciate the changes that have been made but continue to have concerns.

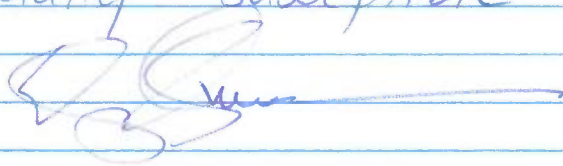
1. Legally the hearing notice sign is supposed to be on their house 10 days prior to hearing. However, they posted it only seven days before this hearing.

Shel



Elias Feldman  
2526 Balboa St #2 (adjacent  
to property)

1. Community relations have not improved, all 20 neighbors who submitted opposition letters continue to oppose the project
2. We learned that 3 USF students are crammed into a questionable unit and have not been informed about the project until I spoke to them 2 weeks ago
3. The layout continues to be highly suspicious. The equivalent of 3 living rooms in unit #1 does not seem realistic. Possibly a boarding house?
4. We have concerns about a project that uses plywood and other cheap material, given that we are situated on a sandy hill and especially susceptible to earthquakes.



Received at CPC Hearing 3/22/18  
H. Foster

To: San Francisco Planning Commission  
1650 Mission Street, Suite 400  
San Francisco, CA 94103

From: Compton's Transgender Cultural District  
234 Eddy St, San Francisco, CA 94102  
Tel. 650-219-7073

Re: 229 Ellis Street / Case No. 2016-007593ENVCUAVARTDM

Hearing Date: March 22, 2018

Dear Commissioners,

I am writing as the District Manager of the Compton's Transgender Cultural District to lend my support for the Starcity Project at 229 Ellis.

The 229 Ellis project is located within the boundaries of the Compton's Transgender Cultural District in a building once known as the San Francisco Turkish Baths, a historically significant site as identified by the San Francisco Planning Department's LGBT Historic Context Statement. Because the building holds historic value to the LGBT community and is located within its boundaries, Compton's met with Starcity to discuss how the development would impact the community, and what could be done to insure that the District's values and goals were upheld and supported and that the history of the space was acknowledged.

It is my belief that Starcity has consistently operated in good faith when negotiating with the district including being willing to ask for a continuance at the initial hearing date so that negotiations could take place. Starcity has addressed the key concerns of the Compton's District, including making a commitment to build more on site inclusionary housing in future developments within the district, providing opportunities for economic growth and development within the district, providing community space, and also making charitable donations to the community within the district.

Finally, we are also glad to see that a building that has remained vacant for several years will finally be turned into much needed housing.

We look forward to working with Starcity on the 229 Ellis project to make sure that San Francisco and the Tenderloin remains a safe and welcoming place for the transgender community, and we hope you will also lend your support to this project.

Sincerely,

Honey Mahogany  
District Manager  
Compton's Transgender Cultural District



Received at CPC Hearing 3/22/18  
S. Velle



March 12, 2018

Day 10 sign should be  
up

Filed at CPC Hearing 3/22/18  
S. Velhe



March 13, 2018



*The proposed Urban Design Guidelines would not have an adverse effect on the City's parks and open space and their access to sunlight and vistas.*

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