



SAN FRANCISCO PLANNING DEPARTMENT

Executive Summary State Density Bonus Project

HEARING DATE: OCTOBER 17, 2019

Record No.: 2018-004545CRV
Project Address: 351 12th Street
Zoning: WSoMa Mixed Use-General (WMUG) Zoning District
55-X Height and Bulk District
Western SoMa Area Plan
Block/Lot: 3521/055
Project Sponsor: Zac Shore, Panoramic Interests
1321 Mission Street
San Francisco, CA 94103
Property Owner: City Gardens 333 LLC
1321 Mission Street
San Francisco, CA 94103
Staff Contact: Veronica Flores – (415) 575-9173
veronica.flores@sfgov.org
Recommendation: **Approve Findings Related to Requested Concession/Incentive and Waivers**

1650 Mission St.
Suite 400
San Francisco,
CA 94103-2479

Reception:
415.558.6378

Fax:
415.558.6409

Planning
Information:
415.558.6377

PROJECT DESCRIPTION

The Project involves removal of an automobile parking lot and new construction of a 68-foot tall, six-story building with 48 group housing rooms pursuant to the State Density Bonus Law and Planning Code Section 206.6. The area represented by the base project includes 40 group housing rooms and the Project is seeking a density bonus of 20% (or 8 group housing rooms) for a total of 48 group housing rooms. 19% of the area represented by the base project, or 8 rooms, will be affordable. Four of the rooms (11%) will be affordable to low-income households, 2 of the rooms (4%) will be affordable to moderate income households, and the remaining two rooms (4%) will be affordable to middle-income households as defined by the Planning Code and Procedures Manual.

REQUIRED COMMISSION ACTION

In order for the Project to proceed, the Commission must make findings related to requested concession/incentive for Common Usable Open Space (Section 135(g)) and waivers from development standards, including Rear Yard (Section 134), Height (Section 250), and Dwelling Unit Exposure (Section 823) pursuant to State Density Bonus Law and Planning Code Section 206.6.

ISSUES AND OTHER CONSIDERATIONS

- **Public Comment & Outreach.** The Department has received one public comment in opposition to the project due to the amount of construction on the block.

- **Inclusionary Affordable Housing.** The Environmental Evaluation Application was accepted on August 1, 2018; therefore, pursuant to Planning Code Section 415.3, the Inclusionary Affordable Housing Program requirement for the On-site Affordable Housing Alternative is to provide a minimum of 19% of the total proposed group housing rooms as affordable. The on-site Inclusionary rate is broken into three separate income tiers: 11% of the units must be made available to low-income households with affordable rents set 55% AMI, 4% must be made available to moderate income households with rents set at 80% AMI, and 4% must be made available to middle-income households with rents set at 110% AMI. A Project Sponsor may use their on-site Inclusionary units to qualify for a density bonus under the State Density Bonus Law ("State Law"). As applied to the 40 units representing the base proportion of the project, the total on-site requirement is 48 group housing rooms.
- **State Density Bonus Law & Waivers.** The WSoMa Mixed Use-General Zoning District utilizes form-based density, which regulates density by the maximum permitted building volume, not as a ratio of units to lot area. Both the base density and the allowable density bonus are represented as square feet of residential gross floor area. The base density includes the amount of residential development that could occur on the project site as of right without modifications to the physical aspects of the Planning Code (ex: open space, dwelling unit exposure, etc.).

For the Project at 351 12th Street, the base density would permit a residential project that included 13,127 gross square feet of residential uses. Because the Project is providing more than 11% of the units as below market rate to very low-income households (up to 50% AMI), the Project is entitled up to a 35% density bonus or 2,432 gross square feet of residential uses. The Project, including the density bonus, proposes 15,559 residential gross square feet of residential uses and 48 group housing rooms.

Under the State Density Bonus Law and Planning Code Section 206.6, the Project is requesting four waivers from development standards, including: 1) Rear Yard (Section 134); 2) Common Usable Open Space (Section 135(g)); 3) Height (Section 250); and 4) Dwelling Unit Exposure (Section 823). The project is not seeking any incentives or concessions.

ENVIRONMENTAL REVIEW

Pursuant to the Guidelines of the State Secretary of Resources for the implementation of the California Environmental Quality Act (CEQA), on **October 10, 2019**, the Planning Department of the City and County of San Francisco determined that the proposed application was exempt from further environmental review under Section 15183 of the CEQA Guidelines and California Public Resources Code Section 21083.3. The proposed Project is in accordance with the Western SoMa Community Plan, Rezoning of Adjacent Parcels and 350 Eighth Street Project EIR and CEQA findings, which was issued on December 6, 2012. Since the Final EIR was finalized, there have been no substantial changes to the Eastern Neighborhoods Area Plan and no substantial changes in circumstances that would require major revisions to the Final EIR due to the involvement of new significant environmental effects or an increase in the severity of previously identified significant impacts, and there is no new information of substantial importance that would change the conclusions set forth in the Final EIR.

BASIS FOR RECOMMENDATION

The Department finds that the Project is, on balance, consistent with the Western SoMa Area Plan and the Objectives and Policies of the General Plan. The Project would provide 48 group housing rooms helping alleviate San Francisco's severe housing crisis. Additionally, 19% of the proposed group housing rooms (8 units) will be below market rate units.

ATTACHMENTS:

Draft Resolution – State Density Bonus
Exhibit B – Plans and Renderings
Exhibit C – Environmental Determination and Mitigation Monitoring and Reporting Program (MMRP)
Exhibit D – Land Use Data
Exhibit E – Maps and Context Photos
Exhibit F - Project Sponsor Brief
Exhibit G - Inclusionary Affordable Housing Affidavit
Exhibit H – Anti-Discriminatory Housing Affidavit
Exhibit I – First Source Hiring Affidavit

This page intentionally blank.



SAN FRANCISCO PLANNING DEPARTMENT

Planning Commission Draft Resolution HEARING DATE OCTOBER 17, 2019

Record No.: **2018-004545CRV**
Project Address: **351 12TH STREET**
Zoning: WMUG (Western SoMa Mixed Use-General) Zoning District
55-X Height and Bulk District
Block/Lot: 3521 / 055
Project Sponsor: Zac Shore
Panoramic Interests
1321 Mission Street, Suite 101
San Francisco, CA 94103
Property Owner: City Gardens 333, LLC
1321 Mission Street, Suite 101
San Francisco, CA 94103
Staff Contact: Veronica Flores – (415) 575-9173
veronica.flores@sfgov.org

1650 Mission St.
Suite 400
San Francisco,
CA 94103-2479

Reception:
415.558.6378

Fax:
415.558.6409

Planning
Information:
415.558.6377

RESOLUTION APPROVING FINDINGS RELATED TO THE REQUESTED CONCENSSION/INCENTIVE AND WAIVERS FROM DEVELOPMENT STANDARDS PURSUANT TO STATE DENSITY BONUS LAW (CA GOVT. CODE SECTION 65915) AND PLANNING CODE SECTION 206.6; AFFIRMING FINDINGS UNDER THE CALIFORNIA ENVIRONMENTAL QUALITY ACT, AND FINDINGS OF CONSISTENCY WITH THE GENERAL PLAN AND PLANNING CODE SECTION 101.1.

WHEREAS, The Planning Commission (hereinafter “Commission”) conducted a duly noticed public hearing at a regularly scheduled meeting to consider the proposed project on October 17, 2019; and,

WHEREAS, on December 6, 2012, the Commission adopted Motion No. 18756. In that action, the Commission certified the Western SoMa Community Plan, Rezoning of Adjacent Parcels and 350 Eighth Street Project Environmental Impact Report and adopted California Environmental Quality Act Findings related to the Western SoMa Community Plan, Rezoning of Adjacent Parcels and 350 Eighth Street Project. The proposed Project is in accordance with the Western SoMa Community Plan, Rezoning of Adjacent Parcels and 350 Eighth Street Project EIR and CEQA findings. Thus, the proposed Project was determined eligible for a Community Plan Evaluation under Public Resources Code Section 21083.3 and CEQA Guidelines Section 15183, which was issued on October 10, 2019.

WHEREAS, the Planning Commission has heard and considered the testimony presented to it at the public hearing and has further considered written materials and oral testimony presented on behalf of Department staff and other interested parties; and

WHEREAS, all pertinent documents may be found in the files of the Department, as the custodian of records, at 1650 Mission Street, Suite 400, San Francisco; and

MOVED, that the Planning Commission hereby **finds** that the requested waivers from Rear Yard (Section 134); Common Usable Open Space (Section 135(g)); Height (Section 250); and Dwelling Unit Exposure (Section 823) are necessary for the project.

FINDINGS

Having reviewed the materials identified in the preamble above, and having heard all testimony and arguments, this Commission finds, concludes, and determines as follows:

1. **Project Description.** The Project involves removal of an automobile parking lot and new construction of a 68-foot tall, six-story building with 48 group housing rooms pursuant to the State Density Bonus Law and Planning Code Section 206.6. The area represented by the base project includes 40 group housing rooms and the Project is seeking a density bonus of 20% (or 8 group housing rooms). for a total of 48 group housing rooms. 19% of the area represented by the base project, or 8 units, will be affordable. Four of the rooms, or 11%, affordable to low-income households, 2 of the rooms, or 4%, as affordable to moderate income households, and the remaining 2 rooms, or 4% of rooms affordable to middle-income households as defined by the Planning Code and Procedures Manual.
2. **Site Description and Present Use.** The Project Site is located on an irregularly-shaped mid-block parcel (measuring approximately 3,855 square feet). The site is a 100% paved parking lot.
3. **Surrounding Properties and Neighborhood.** The Project Site is located within the WMUG (Western SoMa Mixed-Use General) Zoning District in the Western SoMa Area Plan. The project site has two street frontages: 12th Street, which is identified as a two-way street with horizontal on-street parking on either side of the street; and, Norfolk Street, which is a smaller-scale, one-way alley with on-street parking along the east side of the street. The immediate context is mixed in character with a mix of residential, commercial and industrial development. The immediate neighborhood includes one-to-three-story commercial and industrial buildings, a one-story bar (d.b.a. The Eagle), and three-to-four-story live/work and residential complexes. Along 12th Street adjacent to the Project Site is a new construction seven-story-over-basement residential building with 200 dwelling units to the west, and a two-and-one-half story single-family residence to the east. Other zoning districts in the vicinity of the project site include: RED-MX (Residential Enclave District - Mixed); WMUO (Western SoMa Mixed Use-Office); PDR-1-G (Production, Distribution and Repair-General); and SALI (Service/Arts/Light Industrial).
4. **Planning Code Section 206.6 Findings.** Pursuant to Planning Code Section 206.6(e), the Planning Commission shall make the following findings as applicable for any application for a Density Bonus, Incentive, Concession or Waiver for any Individually Requested Density Bonus Project:
 1. The Housing Project is eligible for the Individually Requested Density Bonus Program.

The Project consists of five or more dwelling units on a site that in the WSoMa Mixed Use-General Zoning District that is currently developed as a paved parking lot and is, therefore, eligible for the Individually Requested Density Bonus Program. The Project would contain a mix of 48 group housing rooms, with eight group housing rooms provided as affordable. A minimum of 19% of the area represented by the base project, or 8 units, will be affordable. Four units (11%) will be affordable to low-income households, 2 rooms (4%) will be affordable to moderate income households, and the remaining 2 rooms (4%) will be affordable to middle-income households as defined by the Planning Code and Procedures Manual.

2. The Housing Project has demonstrated that any Concessions or Incentives reduce actual housing costs, as defined in Section 50052.5 of the California Health and Safety Code, or for rents for the targeted units, based upon the financial analysis and documentation provided.

The Project is requesting a concession and incentive for common usable open space under the Individually Requested Density Bonus Program. Planning Code Section 135 requires that the Project provide 80 square feet of usable open space for each group housing room. The Project is required to provide 3,840 square feet of usable open space, but only provides 1,225 square feet. 735 square feet of this is located on the roof deck, which does not qualify as required private or common useable open space in the WSoMa Mixed-Use-General Zoning District. Therefore, the Project has an open space deficiency of 3,350 square feet, which triggers \$1,095,450 in in-lieu fees for the open space not provided. This incentive would reduce costs of the Project with the increased density provided by Government Code Section 65915(f)(2).

Without the incentive, the Project will be physically precluded from constructing the additional 8 group housing rooms as permitted under the Individually Requested Density Bonus Program, thus preventing the Project from achieving a 20% density bonus.

3. If a waiver or modification is requested, a finding that the Development Standards for which the waiver is requested would have the effect of physically precluding the construction of the Housing Project with the Density Bonus or Concessions and Incentives permitted.

The Project includes construction of a new six-story residential building. The Project would contain a mix of 48 group housing rooms, with eight group housing rooms provided as affordable.

In order to achieve the proposed residential density, the Project is requesting three waivers from development standards, including: 1) Rear Yard (Section 134); 2) Dwelling Unit Exposure (Section 140); and 3) Height (Section 250).

1. **Rear Yard.** Planning Code Section 134 requires that the Project provide a rear yard equal to 25 percent of the total lot depth at the lowest level containing a residential unit, and at each succeeding level or story of the building, but in no case less than 15 feet of lot depth. The Project proposes a rear yard of a minimum of 12 feet to better accommodate the circulation space necessary to accommodate the additional group housing rooms and provides the opportunity to create a double-loaded corridor.
2. **Dwelling Unit Exposure.** Planning Code Section 823 requires that all bedrooms in the Project face directly on one of the open areas specified in Section 140. 28 of the group housing rooms face

onto the reduced rear yard, which does not qualify as one of the open spaces that can be used to satisfy the exposure requirements. Without a waiver from the exposure requirements, the Project and accompanying density bonus would not be feasible.

3. **Height.** *Per Planning Code Section 260, the maximum height limit for the subject property is 55 feet. The Project proposes a height of 68 feet to accommodate an additional story that includes 9 group housing rooms.*

Without the waivers, the Project will be physically precluded from constructing the additional 8 group housing rooms as permitted under the Individually Requested Density Bonus Program, thus preventing the Project from achieving a 20% density bonus.

4. If the Density Bonus is based all or in part on donation of land, a finding that all the requirements included in Government Code Section 65915(g) have been met.

The Density Bonus for the Project is not based on any donation of land; and is therefore not applicable.

5. If the Density Bonus, Concession or Incentive is based all or in part on the inclusion of a Child Care Facility, a finding that all the requirements included in Government Code Section 65915(h) have been met.

The requested Density Bonus for the Project is not based on the inclusion of a Child Care Facility; and is therefore not applicable.

6. If the Concession or Incentive includes mixed-use development, a finding that all the requirements included in Government Code Section 65915(k)(2) have been met.

The requested Density Bonus for the Project does not involve a mixed-use development; and is therefore not applicable.

5. **General Plan Compliance.** The proposed project is consistent with the following Objectives and Policies of the General Plan:

GENERAL PLAN: HOUSING ELEMENT

Objectives and Policies

OBJECTIVE 1:

IDENTIFY AND MAKE AVAILABLE FOR DEVELOPMENT ADEQUATE SITES TO MEET THE CITY'S HOUSING NEEDS, ESPECIALLY PERMANENTLY AFFORDABLE HOUSING.

Policy 1.1

Plan for the full range of housing needs in the City and County of San Francisco, especially affordable housing.

Policy 1.10

Support new housing projects, especially affordable housing, where households can easily rely on public transportation, walking and bicycling for the majority of daily trips.

OBJECTIVE 4:

FOSTER A HOUSING STOCK THAT MEETS THE NEEDS OF ALL RESIDENTS ACROSS LIFECYCLES.

Policy 4.1

Develop new housing, and encourage the remodeling of existing housing, for families with children.

Policy 4.4

Encourage sufficient and suitable rental housing opportunities, emphasizing permanently affordable rental units wherever possible.

Policy 4.5

Ensure that new permanently affordable housing is located in all of the City's neighborhoods, and encourage integrated neighborhoods, with a diversity of unit types provided at a range of income levels.

OBJECTIVE 11:

SUPPORT AND RESPECT THE DIVERSE AND DISTINCT CHARACTER OF SAN FRANCISCO'S NEIGHBORHOODS.

Policy 11.1

Promote the construction and rehabilitation of well-designed housing that emphasizes beauty, flexibility, and innovative design, and respects existing neighborhood character.

Policy 11.2

Ensure implementation of accepted design standards in project approvals.

Policy 11.3

Ensure growth is accommodated without substantially and adversely impacting existing residential neighborhood character.

Policy 11.4:

Continue to utilize zoning districts which conform to a generalized residential land use and density plan and the General Plan.

Policy 11.6

Foster a sense of community through architectural design, using features that promote community interaction.

Policy 11.8

Consider a neighborhood's character when integrating new uses, and minimize disruption caused by expansion of institutions into residential areas.

OBJECTIVE 12

BALANCE HOUSING GROWTH WITH ADEQUATE INFRASTRUCTURE THAT SERVES THE CITY'S GROWING POPULATION.

Policy 12.1

Encourage new housing that relies on transit use and environmentally sustainable patterns of movement.

Policy 12.2

Consider the proximity of quality of life elements, such as open space, child care, and neighborhood services, when developing new housing units.

Policy 12.3

Ensure new housing is sustainably supported by the City's public infrastructure systems.

OBJECTIVE 13

PRIORITIZE SUSTAINABLE DEVELOPMENT IN PLANNING FOR AND CONSTRUCTING NEW HOUSING.

Policy 13.1

Support "smart" regional growth that located new housing close to jobs and transit.

Policy 13.3

Promote sustainable land use patterns that integrate housing with transportation in order to increase transit, pedestrian, and bicycle mode share.

GENERAL PLAN: URBAN DESIGN ELEMENT

Objectives and Policies

OBJECTIVE 1:

EMPHASIS OF THE CHARACTERISTIC PATTERN WHICH GIVES TO THE CITY AND ITS NEIGHBORHOODS AN IMAGE, A SENSE OF PURPOSE, AND A MEANS OF ORIENTATION.

Policy 1.3

Recognize that buildings, when seen together, produce a total effect that characterizes the city and its districts.

Policy 1.7

Recognize the natural boundaries of districts, and promote connections between districts.

WESTERN SOMA AREA PLAN: LAND USE

OBJECTIVE 1.1

BUILD ON AN EXISTING MIXED-USED CHARACTER THAT ENCOURAGES PRODUCTION OF RESIDENTIAL USES IN AREAS MOST APPROPRIATE FOR NEW HOUSING WITH A PROXIMATE MIX OF USES AND SERVICES SERVING LOCAL NEEDS AND THEREBY DEVELOPING A COMPLETE NEIGHBORHOOD.

WESTERN SOMA AREA PLAN: HOUSING

OBJECTIVE 3.2

ENCOURAGE NEW NEIGHBORHOOD RESIDENTIAL USES IN LOCATIONS THAT PROVIDE THE GREATEST OPPORTUNITIES TO BUILD ON THE EXISTING NEIGHBORHOOD PATTERNS.

POLICY 3.2.1

Discourage housing production that is not in scale with the existing neighborhood pattern.

POLICY 3.2.2

Encourage in-fill housing production that continues the existing built housing qualities in terms of heights, prevailing density, yards and unit sizes.

POLICY 3.2.6

Promote the production of housing development programs that provide for families and other Western SoMa SUD special population needs in terms of the mix of unit sizes, affordability and tenure.

OBJECTIVE 3.5

ENSURE THAT NEW RESIDENTIAL DEVELOPMENTS SATISFY AN ARRAY OF HOUSING NEEDS WITH RESPECT TO TENURE, UNIT MIX AND COMMUNITY SERVICES.

POLICY 3.5.1

Target provision of affordable units for traditional and non-traditional family needs.

The proposed project is consistent with the Western SoMa Area Plan and the Objectives and Policies of the General Plan, in that the project would provide 48 group housing rooms helping alleviate San Francisco's severe housing crisis. Additionally, 19% of the proposed group housing rooms (8 rooms) will be below market rate units. The massing of the proposed building's primary front facade has been designed to be compatible with the prevailing street wall pattern as serves as the proposal serves as a transition between the seven-story new construction building directly to the east and the two-story, two-unit building directly to the west. The proposed design, including fenestration pattern and contemporary material palette, is in keeping with the immediate neighborhood character.

6. **Planning Code Section 101 Findings.** The proposed project is consistent with the eight Priority Policies set forth in Section 101.1(b) of the Planning Code in that:
 1. That existing neighborhood-serving retail uses be preserved and enhanced and future opportunities for resident employment in and ownership of such businesses enhanced;

The Project would have a positive effect on existing neighborhood-serving retail uses because it would bring additional residents to the neighborhood, thus increasing the customer base of existing neighborhood-serving retail. Moreover, the Project would not displace any existing neighborhood-serving retail uses.

2. That existing housing and neighborhood character be conserved and protected in order to preserve the cultural and economic diversity of our neighborhoods;

The Project would not negatively affect the existing housing and neighborhood character. The Project would not displace any housing given the existing project site consists wholly of a paved parking lot. The Project would improve the existing character of the neighborhood by developing a residential structure with 48 group housing rooms, including on-site affordable rooms.

3. That the City's supply of affordable housing be preserved and enhanced;

The Project Site is developed as a paved parking lot. As such, no existing resident units will be removed. The Project will provide 48 group housing rooms, adding to the City's housing supply. The Project will comply with the City's Inclusionary Affordable Housing Ordinance by using the combination option described in Section 415.5. The project will pay the Affordable Housing Fee on the entire project, and will receive fee credit by providing 19 percent of the group housing rooms as affordable (eight Below Market Rate rooms provided on-site).

4. That commuter traffic not impede MUNI transit service or overburden our streets or neighborhood parking;

The Project would not impede MUNI transit service or overburden local streets or parking. The Project is at a location well-served by transit as it is located in a major transit corridor and would promote rather than impede the use of MUNI transit service. Future residents and employees of the Project could access both the existing MUNI rail and bus services.

5. That a diverse economic base be maintained by protecting our industrial and service sectors from displacement due to commercial office development, and that future opportunities for resident employment and ownership in these sectors be enhanced;

The Project is wholly a residential building and would not negatively affect the industrial and service sectors, nor would it displace any existing industrial uses.

6. That the City achieve the greatest possible preparedness to protect against injury and loss of life in an earthquake;

The Project will be designed and will be constructed to conform to the structural and seismic safety requirements of the Building Code. This proposal will not impact the property's ability to withstand an earthquake.

7. That the landmarks and historic buildings be preserved;

Currently, the Project Site does not contain any City Landmarks or historic buildings.

8. That our parks and open space and their access to sunlight and vistas be protected from development;

The proposed project would not have an adverse effect on the City's parks and open space and their access to sunlight and vistas.

NOW THEREFORE BE IT RESOLVED that the Commission hereby APPROVES the request for incentives, concessions and waivers as described in this Resolution.

I hereby certify that the foregoing Resolution was adopted by the Commission at its meeting on October 17, 2019.

Jonas P. Ionin
Commission Secretary

AYES:

NOES:

ABSENT:

ADOPTED: October 17, 2019

DESIGN NARRATIVE

OVERVIEW

The site is composed of parcel 3521/022C (3,855 sf). It is currently a 100% paved automobile parking lot.

Panoramic Interests, the Project Sponsor, proposes to redevelop Lot C per the State Density Bonus Law (California Government Code Sections 65915-65918), into an affordable-without-subsidy rental housing project. The proposed project is the "Bonus Project," which includes the density to which the Project Sponsor is entitled per California State Law. The "Base Project" is an illustration showing a hypothetical project that complies with base zoning, which serves

THE "BASE" PROJECT (ALSO SEE P. 17)
The Base Project contains 40 rental Group Housing Bedroom Units.

The building is 5-stories-over-basement with a maximum height of 55 feet.

SFPC Sections 415.6(a)(3) and 415.6(a)(5), 19% of 40, or 8 Units, shall be provided as BMR Units as follows:

11% x 40 = 4 low income Units (@ 55% AMI)*

THE "BONUS" PROJECT (ALSO SEE P. 7)
The Bonus Project proposes 48 rental Group Housing Bedroom Units.

The building is 6-stories with an approximate height of 68 feet.

The Density Bonus Law entitles a Bonus Project with a maximum 20% density bonus if at least 10% of its Base Project Units are affordable to low-income households. This project meets the minimum standard to achieve the maximum percentage listed.

THE ARCHITECTURE

Per District Guidelines, the building shall have a “simple architectural expression” and “consistent range of materials” respecting the area’s “warehouse/art/industrial” context.

PROPOSAL FOR CONCESSIONS, INCENTIVES, AND WAIVERS (ALSO SEE P. 8 & 9)

Under the State Density Bonus Law, the Project Sponsor is entitled to one Concession or Incentive, as well as Waivers of any development standard that would physically preclude construction of the project at the density proposed. The following Waivers are required to physically achieve the density bonus:

- 1. HEIGHT LIMIT: Waive Building Height Limit per Sec. 250 from 55'-0" to approximately 68'-0” because a Code-compliant building height would preclude the development of a 20% increase in Bedroom Unit density.
- 2. BEDROOM EXPOSURE: Waive Exposure requirements & restrictions per Sec. 823(c)(3) because providing Code-compliant exposure for the Bedroom Suites facing the slightly reduced (Rear) Yard would preclude the development of a 20% increase in Bedroom Unit density.

as the basis for calculating bonus density under state law.

This is an application pursuant to Mayoral Executive Directive 17-02 which mandates expedited approval and permitting of the project. This is also an application for a development permit pursuant to the Permit Streamlining Act (Section 65920 et seq. of the California Government Code) and Section 15101 of the CEQA Guidelines. State Law requires the City to determine whether the application is complete within 30 days from submittal. If no written determination is made within 30 days, the application is deemed complete by operation of the Law on the 30th day.

4% x 40 = 2 moderate income Units (@ 80% AMI)
4% x 40 = 2 middle income Units (@ 110% AMI)

*(Note that 2 of these will be provided as very-low income Units per the requirements of the State Density Bonus Law.)

The building has a Residential Gross Floor Area (GFA) of approximately 13,127 SF. 15 Class I and 2 Class II bicycle parking spaces are provided. There is no automobile parking.

Accordingly, there are a total of 48 Group Housing Rooms/Units. (0.20 x 40= 8 Units)

4 low income Units (55% AMI), 2 moderate income Units (@ 80% AMI), and 2 middle income Units (@ 110% AMI) will be provided on site.

The building has a Residential Gross Floor Area (GFA) of approximately 15,559 SF. 18 Class I and 2 Class II bicycle parking spaces are provided. There is no automobile parking.

Facades are currently proposed with a combination of metal and/or plaster and fiber-cement finishes that are found throughout the surrounding neighborhood.

- 3. REAR YARD: Waive Rear Yard requirements per Sec. 134(a)(1) because providing a Code-compliant Rear Yard with a depth of 25% of the Lot depth would preclude the development of a 20% increase in Bedroom Unit density.
- 4. OPEN SPACE: Waive Common Usable Open Space requirements & restrictions per Sec. 135 because providing the required 1,280 SF* of Code-compliant Usable Open Space would preclude the development of a 20% increase in Bedroom Unit density.

*Note: The Bonus Project is still providing 1,225 SF of shared open space located at grade and at the Roof Level.

EXHIBIT B

351 12TH STREET
AFFORDABLE-WITHOUT-SUBSIDY RENTAL GROUP HOUSING

ENVIRONMENTAL EVALUATION APPLICATION (EEA)
& INDIVIDUALLY REQUESTED STATE
DENSITY BONUS SUPPLEMENTAL APPLICATION

TABLE OF CONTENTS

- 1. TABLE OF CONTENTS & DESIGN NARRATIVE
- 2. VIEW OF BONUS PROJECT FROM 12TH STREET
- 3. LOCATION & EXISTING CONDITIONS
- 4. CONTEXT PLAN & SITE PHOTOS
- 5. NEIGHBORHOOD PHOTOS
- 6. SURVEY
- 7. PROPOSED BONUS PROJECT TITLE PAGE
- 8. DENSITY BONUS CALCULATIONS
- 9. DENSITY BONUS WAIVER DIAGRAMS
- 10. DRAFT TRANSPORTATION DEMAND MANAGEMENT PLAN
- 11. BONUS PROJECT DESCRIPTION & DATA
- 12. BONUS PROJECT SITE PLAN
- 13. BONUS PROJECT 1ST (STREET) & 2ND-6TH FLOOR PLANS
- 14. BONUS PROJECT ROOF & UPPER ROOF PLAN
- 15. BONUS PROJECT BUILDING ELEVATIONS
- 16. BONUS PROJECT BUILDING ELEVATIONS
- 17. BONUS PROJECT BUILDING SECTIONS
- 18. APPENDIX - BASE PROJECT TITLE PAGE
- 19. BASE PROJECT DESCRIPTION & DATA
- 20. BASE PROJECT 1ST (GARDEN) & 2ND (STREET) FLOOR PLANS
- 21. BASE PROJECT 3RD THRU 5TH & ROOF FLOOR PLANS
- 22. BASE PROJECT BUILDING ELEVATIONS
- 23. BASE PROJECT BUILDING ELEVATIONS
- 24. BASE PROJECT BUILDING SECTIONS



VIEW OF BONUS PROJECT FROM 12TH STREET

09/06/19

**M A C Y
A R C H
I T E C
T U R E**

315 Linden Street San Francisco CA 94102 Tel 415 551 7630 www.macyarchitecture.com © 2019 Macy Architecture

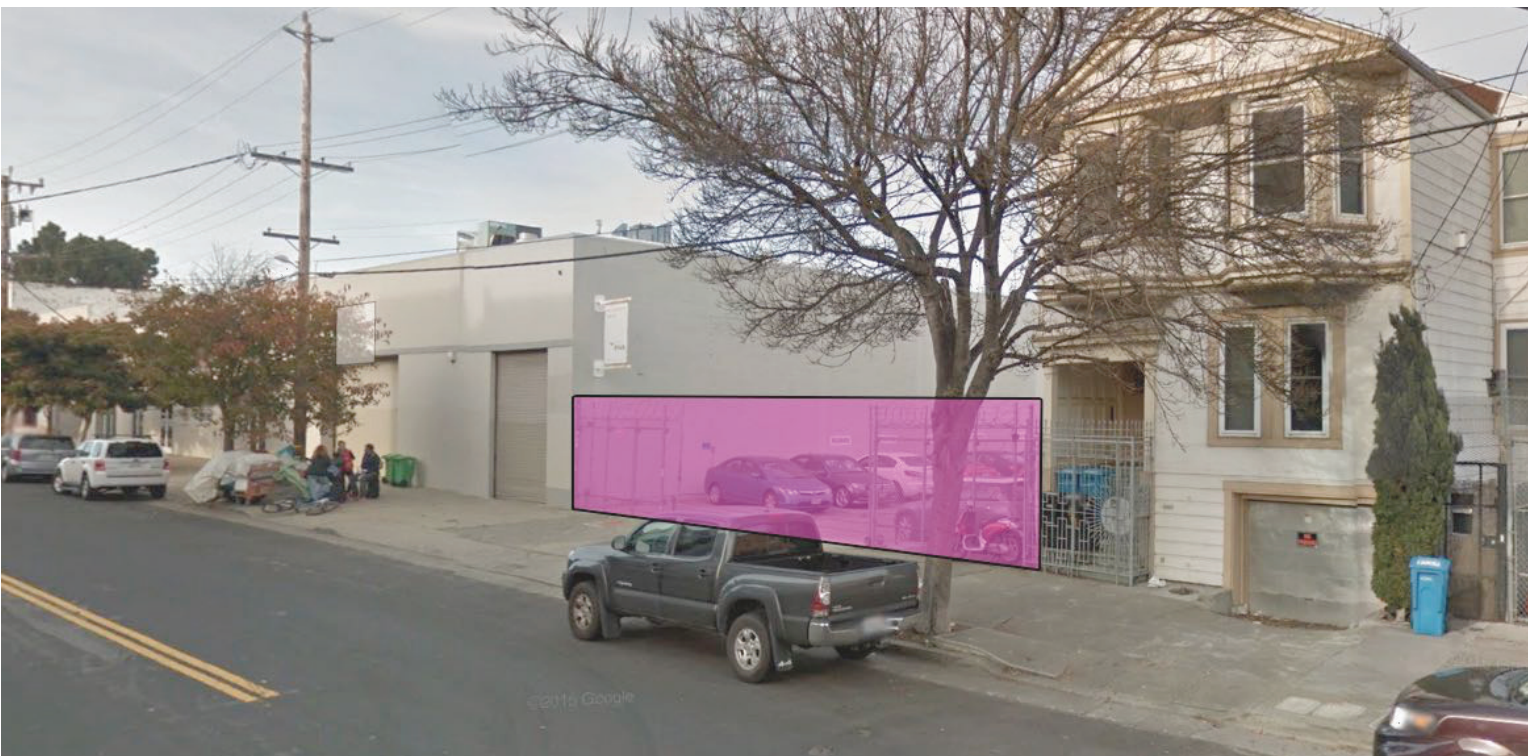
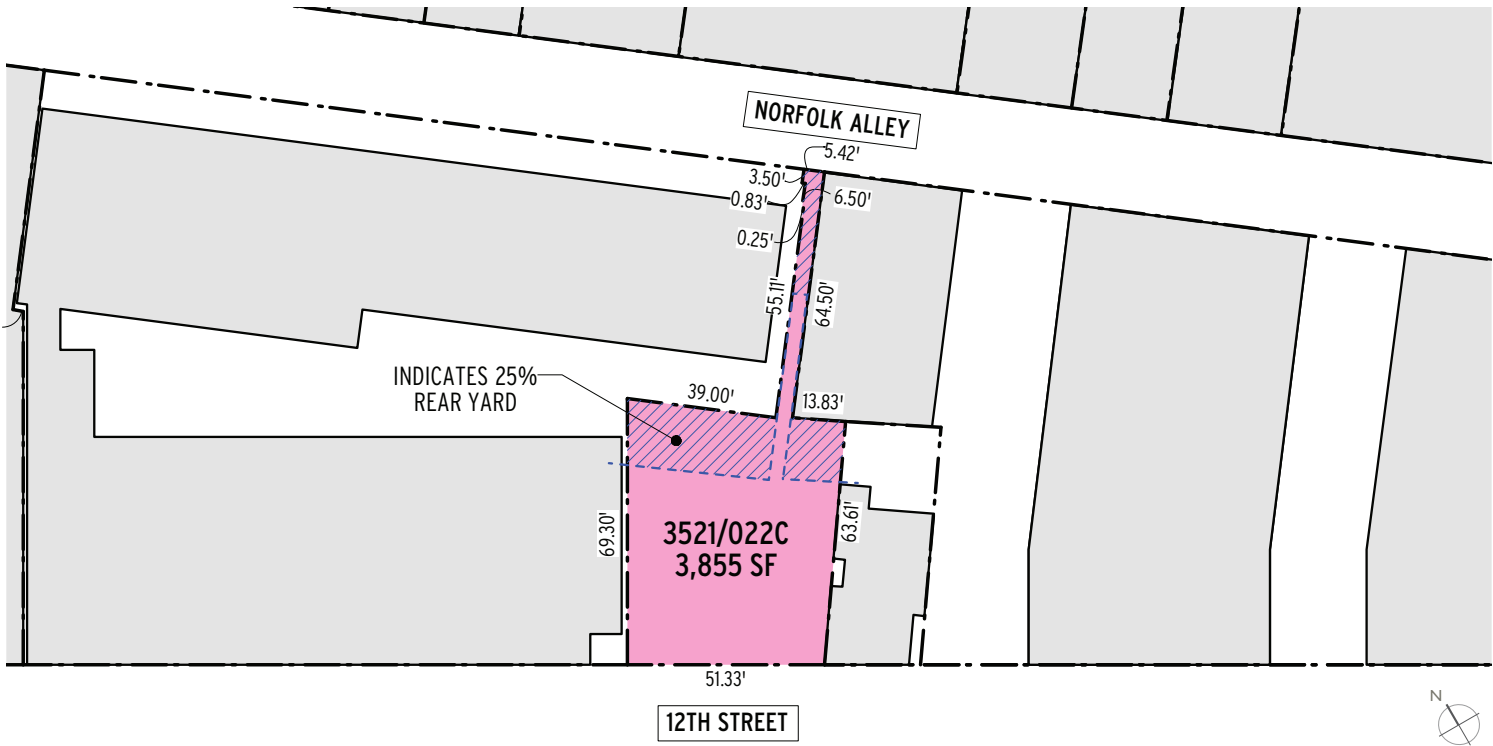
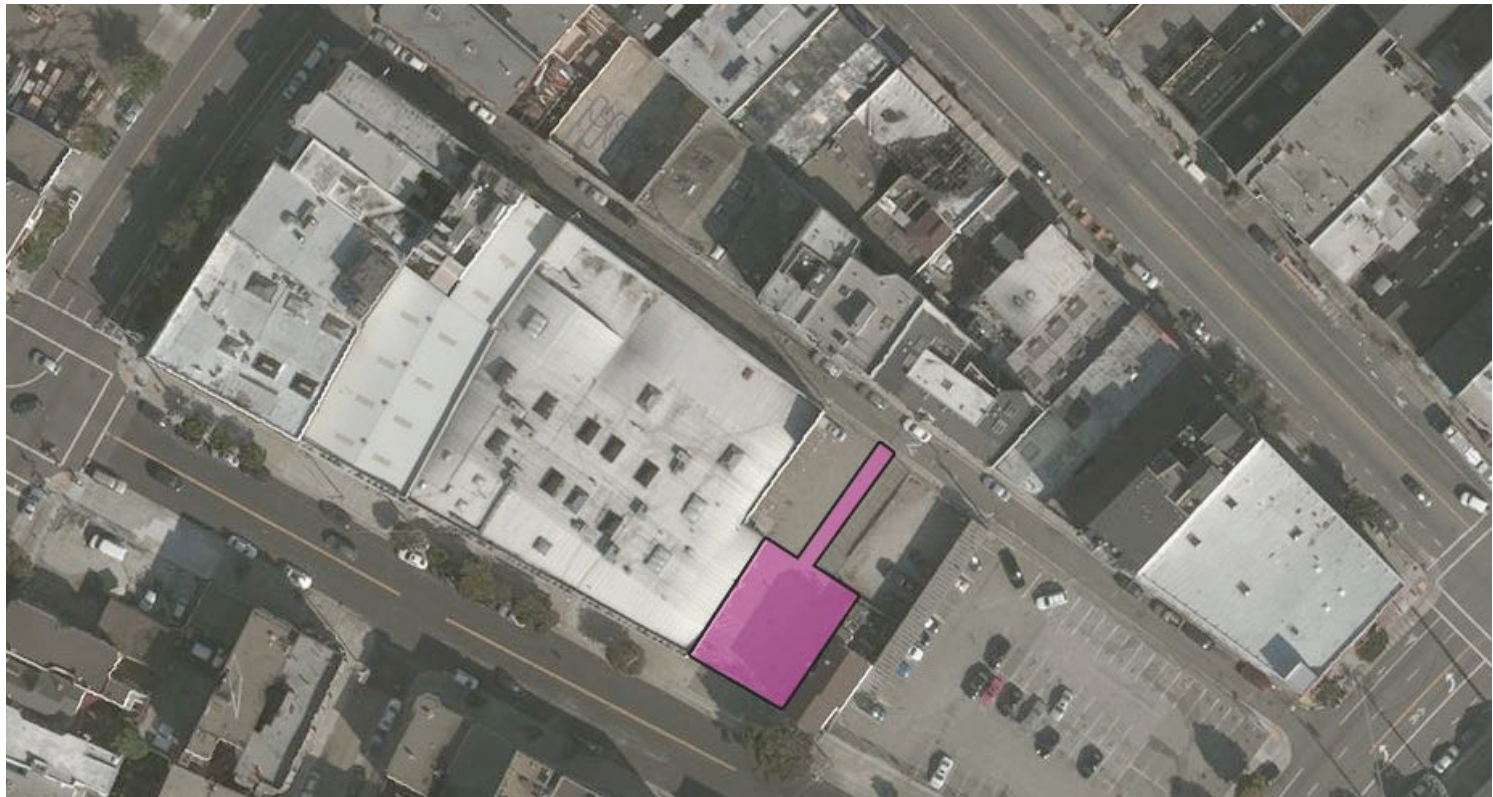


1321 Mission St, San Francisco, CA 94103 Tel 415.701.7000 www.panoramic.com

ENVIRONMENTAL EVALUATION APPLICATION (EEA)
& STATE DENSITY BONUS APPLICATION

351-12TH STREET (BLOCK 3521 LOT 022C) AFFORDABLE-WITHOUT-SUBSIDY RENTAL GROUP HOUSING

PAGE 2 OF 25



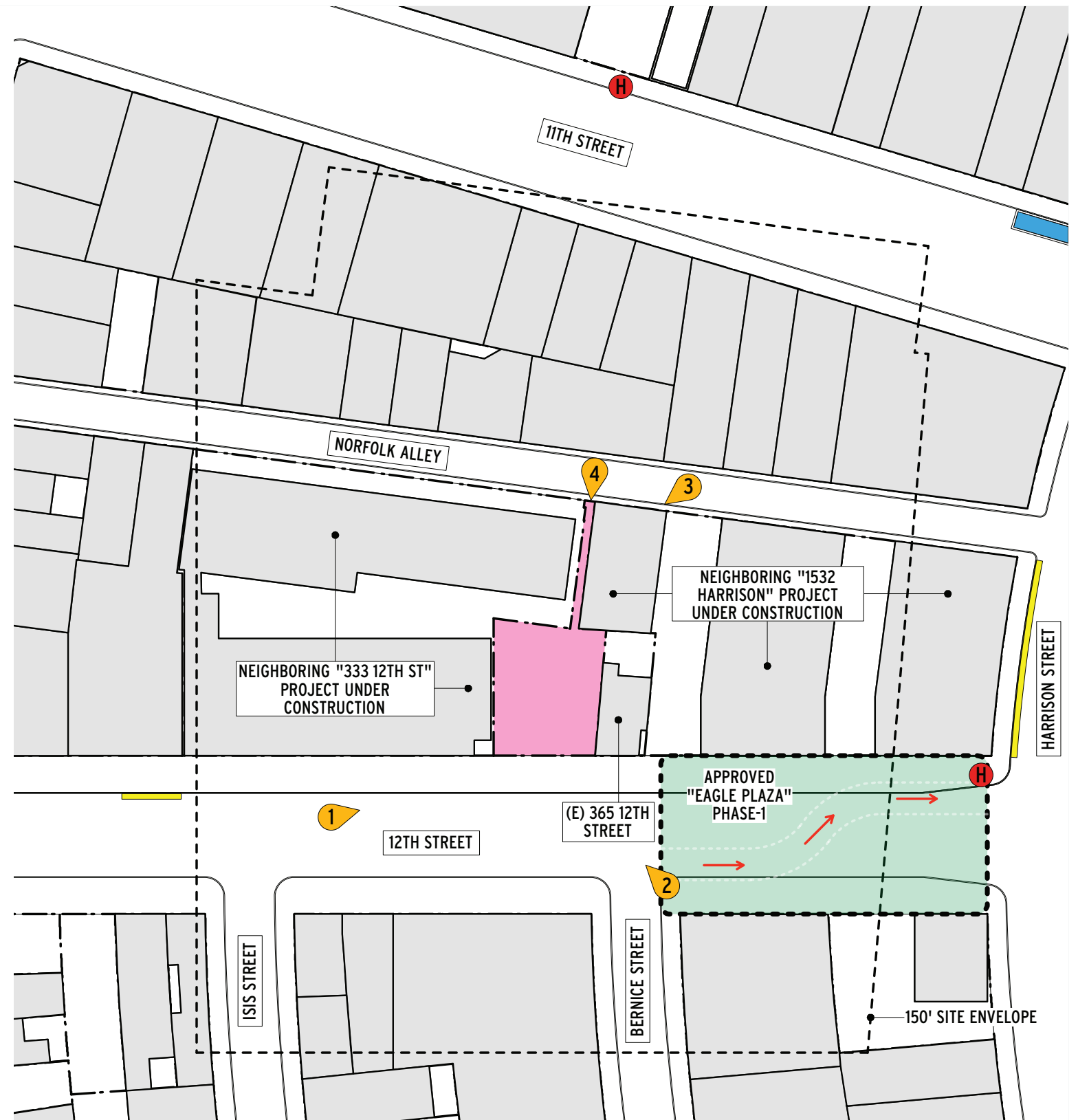
LOCATION & EXISTING CONDITIONS

09/06/19



LEGEND

- H FIRE HYDRANT
- # PHOTO
- BUS STOP
- LOADING ZONE
- 150' SITE ENVELOPE





1 1500 Harrison St. (@ Norfolk)
"CYCLE GEAR"



2 365 12th St.



3 398 12th St. (@ Harrison)
"EAGLE TAVERN"



4 358 12th St.
"LIVE / WORK CONDOMINIUMS"



5 40 Isis St. (between 12th St. and 13 St.)
"TOM TAYLOR COMMERCIAL INTERIORS"



6 Highway 101 / Central Freeway Underpass (@ 13th St.)



7 1501 Harrison St. (@ 11th St.)
FORMER "VETERANS CAB" - Abandoned



8 160 Kissling St. (@ 12th St.)

NEIGHBORHOOD PHOTOS

09/06/19

**M A C Y
A R C H
I T E C
T U R E**

315 Linden Street San Francisco CA 94102 Tel 415 551 7630 www.macyarchitecture.com © 2019 Macy Architecture



ENVIRONMENTAL EVALUATION APPLICATION (EEA)
& STATE DENSITY BONUS APPLICATION
351-12TH STREET (BLOCK 3521 LOT 022C) AFFORDABLE-WITHOUT-SUBSIDY RENTAL GROUP HOUSING

1321 Mission St, San Francisco, CA 94103 Tel 415.701.7000 www.panoramic.com

PAGE 5 OF 25



PROPOSED BONUS PROJECT

09/06/19

**M A C Y
A R C H
I T E C
T U R E**

315 Linden Street San Francisco CA 94102 Tel 415.551.7630 www.macyarchitecture.com © 2019 Macy Architecture



1321 Mission St, San Francisco, CA 94103 Tel 415.701.7000 www.panoramic.com

ENVIRONMENTAL EVALUATION APPLICATION (EEA)
& STATE DENSITY BONUS APPLICATION

351-12TH STREET (BLOCK 3521 LOT 022C) AFFORDABLE-WITHOUT-SUBSIDY RENTAL GROUP HOUSING

PAGE 7 OF 25

BONUS DENSITY CALCULATIONS

STEP 1. DEFINE THE BASE PROJECT (A HYPOTHETICAL PROJECT)

THE BASE PROJECT IS A HYPOTHETICAL PROJECT THAT REPRESENTS THE “MAXIMUM ALLOWABLE DENSITY” FOR A PROPOSED PROJECT AT A PARTICULAR SITE.

IA. BASE PROJECT RESIDENTIAL GROSS FLOOR AREA (GFA)
CALCULATE THE BASE PROJECT’S RESIDENTIAL FLOOR AREA, INCLUDING EACH STORY OF A HYPOTHETICAL BUILDING, ON THE PROPOSED SITE.

BASEMENT RESIDENTIAL GFA:	1,286 SF
1ST FLOOR RESIDENTIAL GFA:	2,381 SF
2ND FLOOR RESIDENTIAL GFA:	2,365 SF
3RD FLOOR RESIDENTIAL GFA:	2,365 SF
4TH FLOOR RESIDENTIAL GFA:	2,365 SF
5TH FLOOR RESIDENTIAL GFA:	2,365 SF
ROOF LEVEL RESIDENTIAL GFA:	0 SF
BASE PROJECT RESIDENTIAL FLOOR AREA:	13,127 SF

IB. BONUS PROJECT RESIDENTIAL GROSS FLOOR AREA (GFA) & TOTAL UNITS

1ST FLOOR RESIDENTIAL GFA:	2,469 SF
2ND FLOOR RESIDENTIAL GFA:	2,618 SF
3RD FLOOR RESIDENTIAL GFA:	2,618 SF
4TH FLOOR RESIDENTIAL GFA:	2,618 SF
5TH FLOOR RESIDENTIAL GFA:	2,618 SF
6TH FLOOR RESIDENTIAL GFA:	2,618 SF
ROOF LEVEL RESIDENTIAL GFA:	0 SF
BONUS PROJECT RESIDENTIAL FLOOR AREA:	15,559 SF

TOTAL UNITS: 48 GROUP HOUSING ROOMS

IC. BASE PROJECT MAXIMUM UNITS
CALCULATE THE BASE PROJECT’S MAXIMUM NUMBER OF RESIDENTIAL UNITS BY DIVIDING THE BASE PROJECT GFA BY THE BONUS PROJECT GFA AND MULTIPLY BY THE NUMBER OF BONUS PROJECT TOTAL UNITS.

I.A:	BASE PROJECT RESIDENTIAL GFA:	13,127 SF
I.B:	BONUS PROJECT RESIDENTIAL GFA:	15,559 SF
I.C:	MAXIMUM BASE PROJECT UNITS:	40 UNITS = 13,127 SF / 15,559 SF X 48 UNITS

STEP 2. DEFINE THE DENSITY BONUS PROJECT

DENSITY BONUS UNITS ARE MARKET-RATE UNITS THAT EXCEED THE “MAXIMUM ALLOWABLE RESIDENTIAL DENSITY” ,THE AMOUNT OF AFFORDABLE UNITS, AND THEIR AFFORDABILITY.

- 2A. CONFIRM THE NUMBER AND TYPE (AFFORDABILITY LEVEL) OF BELOW MARKET RATE UNITS:
% AFFORDABLE UNITS: 10% = 0.10 X 40 GROUP HOUSING ROOMS = 4 ROOMS (@ 55% AMI) i.e. ≤ 60% AMI
- 2B. CONSULT THE DENSITY BONUS LAW TO DETERMINE THE DENSITY BONUS PERCENTAGE:
% GRANTED FOR DENSITY BONUS: 20%
- 2C. MULTIPLY THAT PERCENTAGE BY THE NUMBER OF BASE PROJECT UNITS (FROM STEP 1C):
BONUS UNITS: 20% X 40 = 8 ROOMS
- 2D: ADD THE NUMBER OF DENSITY BONUS UNITS TO THE NUMBER OF BASE PROJECT UNITS:
BONUS UNITS: 40 ROOMS + 8 ROOMS = 48 ROOMS
- 2E. THE APPLICANT DECIDES HOW MANY DENSITY BONUS UNITS THEY WANT.ADD THIS NUMBER OF UNITS TO THE NUMBER OF BASE PROJECT UNITS.
PROPOSED DENSITY BONUS UNITS: 48 GROUP HOUSING ROOMS

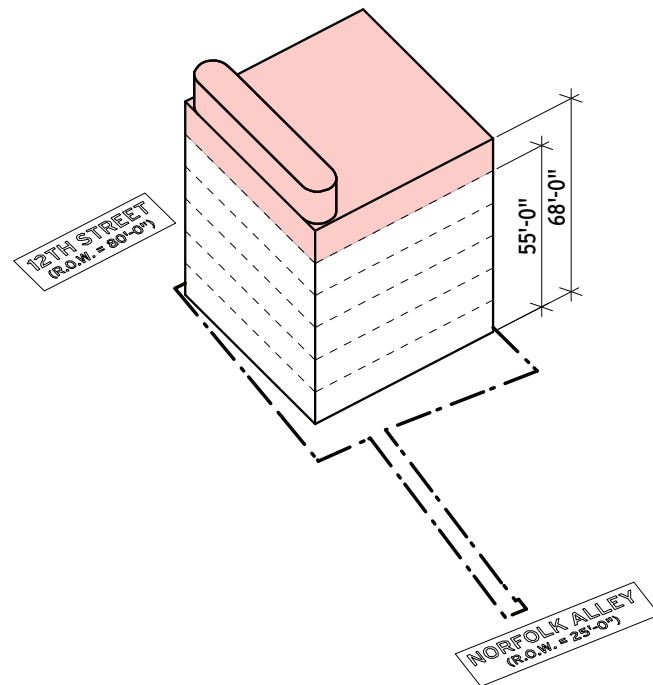
STEP 3. THE PLANNING COMMISSION ACTS ON THE DENSITY BONUS PROJECT

- 3A: REQUESTED WAIVERS (SEE PAGE 9)
1. WAIVE HEIGHT LIMIT PER S.F.P.C.SEC. 250

2. WAIVE REAR YARD REQUIREMENTS PER S.F.P.C. SEC. 134

3. WAIVE EXPOSURE REQUIREMENTS PER S.F.P.C. SEC. 823

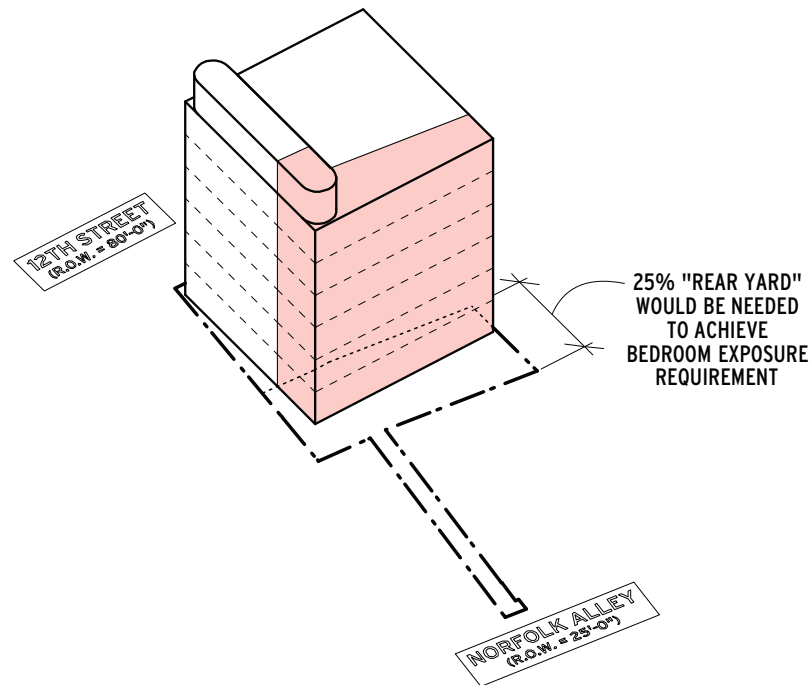
4. WAIVE OPEN SPACE REQUIREMENTS & RESTRICTIONS PER S.F.P.C. SEC 135



WAIVER ①

WAIVE BUILDING HEIGHT
REQUIREMENTS PER S.F.P.C. SEC. 250

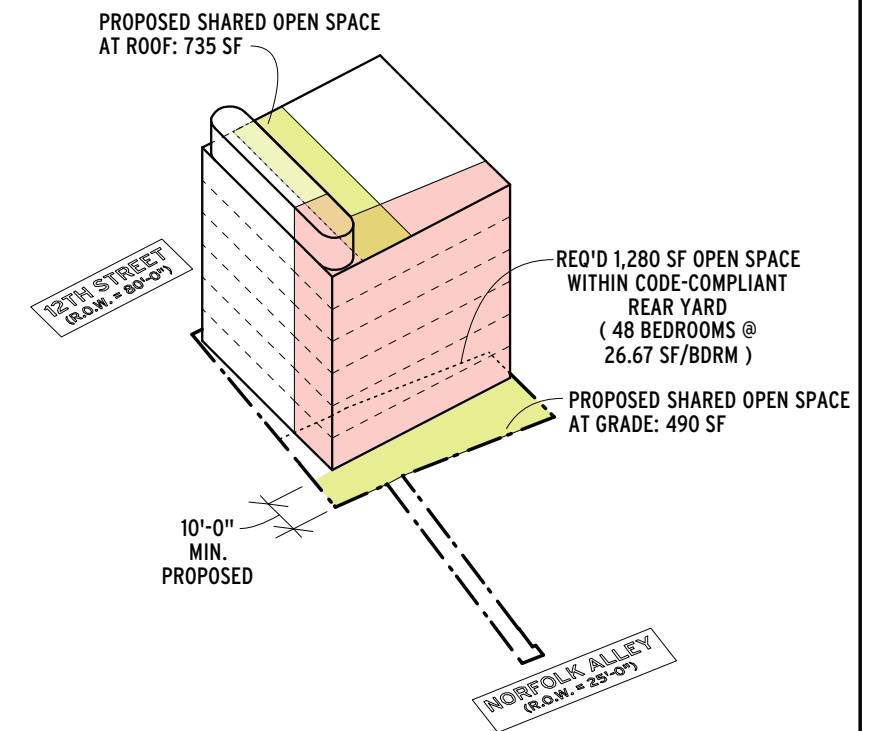
**BUILDING HEIGHT REQUIREMENT
WOULD PHYSICALLY PRECLUDE
8 GROUP HOUSING BEDROOMS**



WAIVERS ② & ③

WAIVE REAR YARD REQUIREMENT PER S.F.P.C. SEC. 134 &
BEDROOM EXPOSURE REQUIREMENT PER S.F.P.C. SEC.823

**REAR YARD & BEDROOM EXPOSURE
REQUIREMENTS WOULD PHYSICALLY PRECLUDE
8 GROUP HOUSING BEDROOMS**



WAIVER ④

WAIVE COMMON USABLE OPEN SPACE
REQUIREMENTS PER S.F.P.C. SEC. 135

NOTE: PROPOSED PROJECT STILL PROVIDES 1,225 SF OF
SHARED OPEN SPACE (AT ROOF: 735 SF, AT GRADE: 490 SF)

**OPEN SPACE REQUIREMENT
WOULD PHYSICALLY PRECLUDE
8 GROUP HOUSING BEDROOMS**

STEP 1: Search or Click on the Map

351 12TH ST

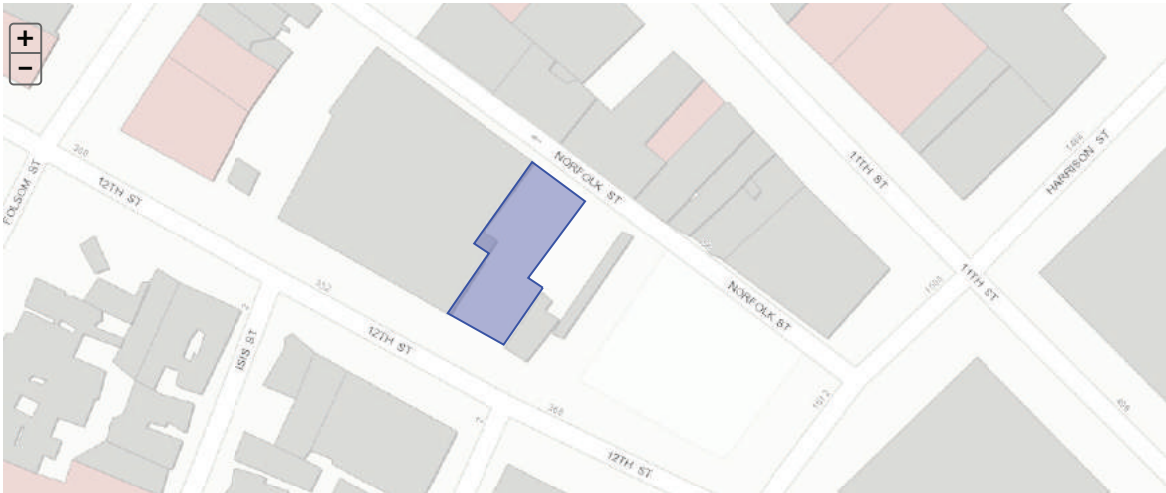
SEARCH

STEP 2: Choose Land Use Categories

- ☐ Category A: Retail Type
☒ Category C: Residential Type
☐ Category B: Office Type
☐ Category D: Other

Clear

Tour
Questions
Instructions



Project
Characteristics

Street Address of Project: 351 12TH ST
Assessors Block/Lot: 3521/055
Transportation Analysis Zone Number: 589

Project Characteristics - Land Use
Category C (Residential Type)

Dwelling Units and Beds in Group Housing: 48
Less than 10 dwelling units or beds is not subject to TDM Program.
% of Dwelling Units Two-Bedrooms or larger: 0
% On-site Affordable Housing (income >55% ≤80% AMI): 4%
100% Affordable Housing projects are not subject to TDM Program.
% On-site Affordable Housing (income ≤55% AMI): 8%
100% Affordable Housing projects are not subject to TDM Program.
Accessory Parking Spaces: 0

Choose the measures from following list

Category C Residential

PARKING

PKG 1 Unbundle Parking (pdf/measure/pkg1.pdf)
No
Neighborhood Parking Rate: 0.64
Location B
PKG 4 Parking Supply (pdf/measure/pkg4.pdf)
Yes
+11
Neighborhood Parking Rate: 0.64
Project Parking Rate: 0.00
Option K

ACTIVE
TRANSPORTATION

ACTIVE-1 Improve Walking Conditions (pdf/measure/act1.pdf)
No
Option A
Option B
ACTIVE-2 Bicycle Parking (pdf/measure/act2.pdf)
No
Option A
Option B
Option C
Option D
ACTIVE-4 Bike Share Membership (pdf/measure/act4.pdf)
No
>1,000feet
<=1,000feet
(Click here for the bay area bike share station map)
(http://www.bayareabikeshare.com/stations)
ACTIVE-5a Bicycle Repair Station (pdf/measure/act5a.pdf)
No
ACTIVE-5b Bicycle Maintenance Services (pdf/measure/act5b.pdf)
No
ACTIVE-6 Fleet of Bicycles (pdf/measure/act6.pdf)
No

CAR SHARE

CSHARE-1 Car-Share Parking (pdf/measure/cshare1.pdf)
No
Option A
Option B
Option C
Option D
Option E

DELIVERY

DELIVERY-1 Delivery Supportive Amenities (pdf/measure/deli1.pdf)
No

FAMILY

FAM-1 Family TDM - Amenities
No

Category C - Residential

Current Point: 13
Target Point: 10

FAM-2 On-site Childcare (pdf/measure/fam2.pdf)
OFF
Option B
No
FAM-3 Family TDM Package (pdf/measure/fam3.pdf)
No

HIGH OCCUPANCY
VEHICLES

HOV-1 Contributions or Incentives (pdf/measure/hov1.pdf)
No
Option A
Option B
Option C
Option D
HOV-2 Shuttle Bus Service (pdf/measure/hov2.pdf)
No
Option A
Option B

COMMUNICATIONS AND
INFORMATION

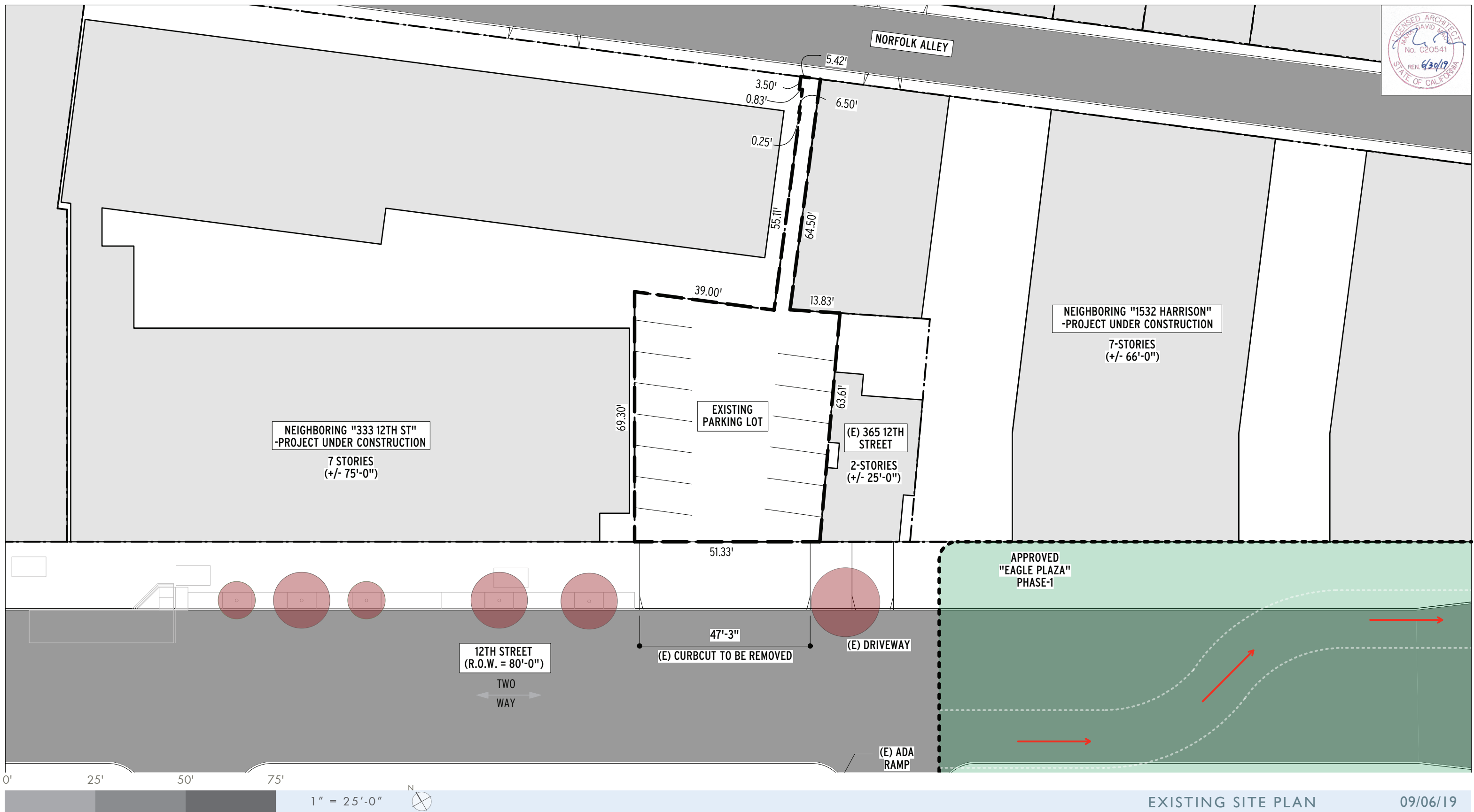
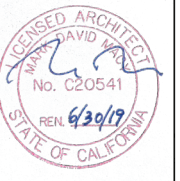
INFO-1 Multimodal Wayfinding Signage (pdf/measure/info1.pdf)
No
INFO-2 Real Time Transportation Displays (pdf/measure/info2.pdf)
No
INFO-3 Tailored Transportation Marketing Services (pdf/measure/info3.pdf)
No
Option A
Option B
Option C
Option D

LAND USE

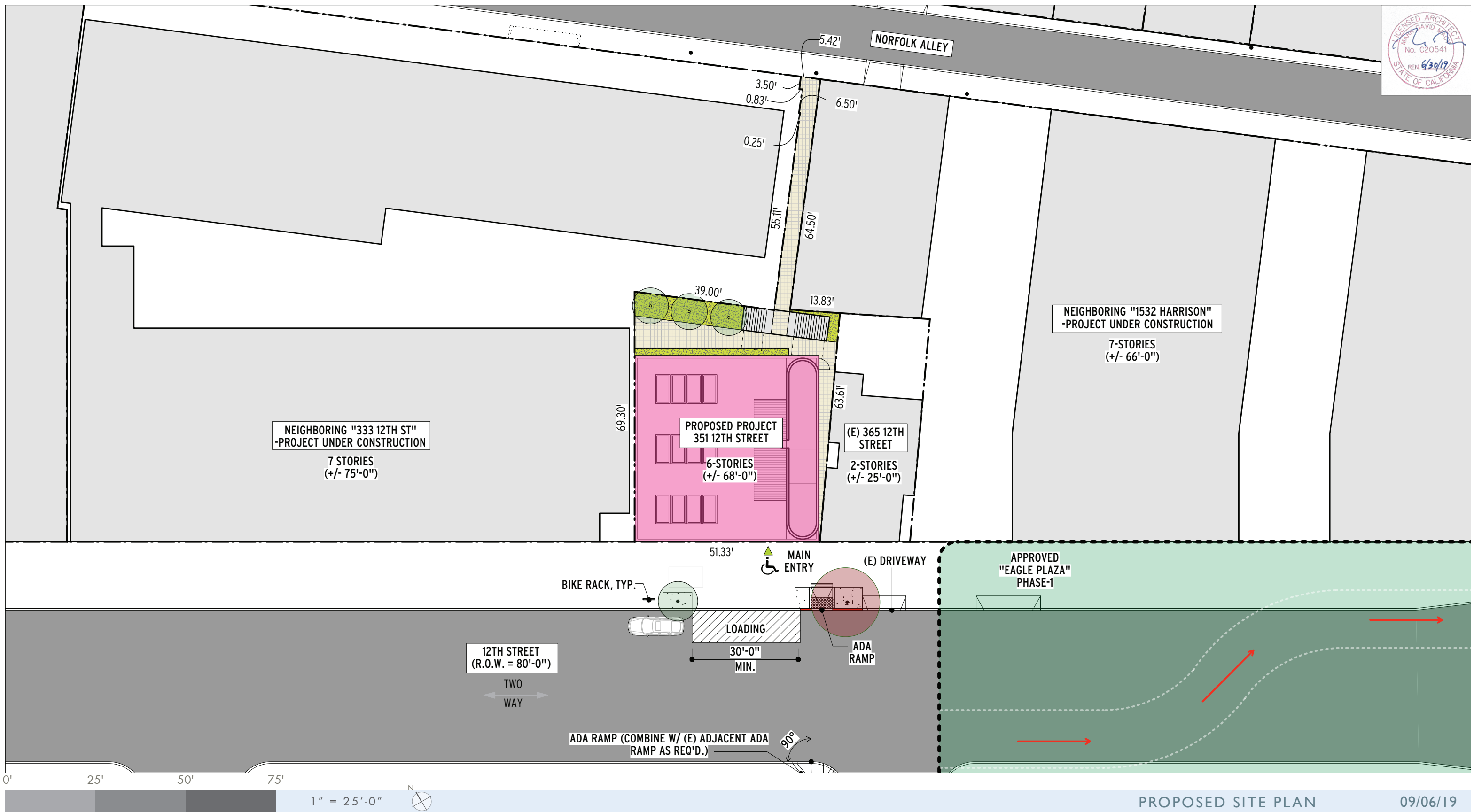
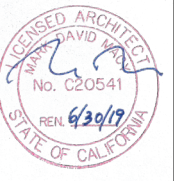
LU-2a On-site Affordable Housing (income >55% ≤80% AMI) (pdf/measure/lu2.pdf)
No
LU-2b On-site Affordable Housing (income ≤55% AMI) (pdf/measure/lu2.pdf)
Yes
+2
Option A

COMMENTS

AREA SUMMARY											DESCRIPTION		
LEVEL	1	2	3	4	5	6	R*	TOTAL GSF	TOTAL GFA**		A STATE DENSITY "BONUS PROJECT" CONSISTING OF RENTAL GROUP HOUSING CONTAINING 48 BEDROOMS.		
RESIDENTIAL - GROUP HOUSING ROOMS	949	1,779	1,779	1,779	1,779	1,779	0	9,844	9,844		<div>PLANNING DATA</div> <div>ASSESSOR PARCEL: BLOCK 3521 LOT 022C ZONING: WMUG (WESTERN SOMA DISTRICT S.U.D.) HEIGHT & BULK DISTRICT: 55-X LOT AREA: 3,855 SF (0.088 AC) GROSS SQUARE FEET OF CONSTRUCTION: 16,175 SF "RESIDENTIAL" GROSS FLOOR AREA: 15,559 SF (PER SFPC SEC.102) TOTAL GROUP HOUSING BEDROOM UNITS: 48 REQ'D ON-SITE BELOW-MARKET-RATE (BMR) UNITS: 4 * (PER STATE DENSITY BONUS LAW) SHARED OPEN SPACE: 1,225 SF PROVIDED -OUTDOOR SPACE AT GRADE = 490 SF; AT ROOF DECK = 735 SF BICYCLE PARKING: 18 CLASS I SPACES -PLUS (2) CLASS II (2 PER 100 BDRMS REQ'D) AUTOMOBILE PARKING: 0 -NONE REQUIRED PER SFPC TABLE 152.1 NOTES: * BASE PROJECT HAS 40 GROUP HOUSING BEDROOMS (SEE PAGE 19). A DENSITY BONUS OF 20% IS BEING SOUGHT, ACCORDINGLY (4) ON-SITE BMR UNITS ARE REQ'D AT LOW-INCOME (60% AMI). (40) X (0.10) = 4 * FOR LOCAL ON-SITE INCLUSIONARY REQUIRMENTS, SEE "PLANNING DATA NOTES" ON PAGE 19</div>		
SHARED AMENITY	659	155	155	155	155	155	0	1,434	1,434				
BICYCLE PARKING	130	0	0	0	0	0	0	130	0				
UTILITY	250	25	25	25	25	25	84	459	375				
CIRCULATION	611	659	659	659	659	659	402	4,308	3,906				
TOTAL	2,599	2,618	2,618	2,618	2,618	2,618	486	16,175	15,559				
*Mechanical, Stair & Elevator Penthouses													
**GFA per San Francisco Planning Code Sec. 102													
GROUP HOUSING ROOM SUMMARY													
LEVEL	1	2	3	4	5	6	R	TOTAL GROUP HOUSING BEDROOM UNITS					
# OF GROUP HOUSING ROOMS	3	9	9	9	9	9	0	48					
OUTDOOR SPACE SUMMARY													
LEVEL	1	2	3	4	5	6	R	TOTAL					
SHARED OPEN SPACE	490	0	0	0	0	0	735	1,225					
											BUILDING DATA		
											STORIES: 6 STORIES		
											CONSTRUCTION TYPE: TYPE IIIA FULLY SPRINKLERED		
											BUILDING HEIGHT: 68'-0"		
											BUILDING USE: PRIVATELY-FUNDED CONGREGATE RESIDENCE		
											OCCUPANCY TYPE: R2		

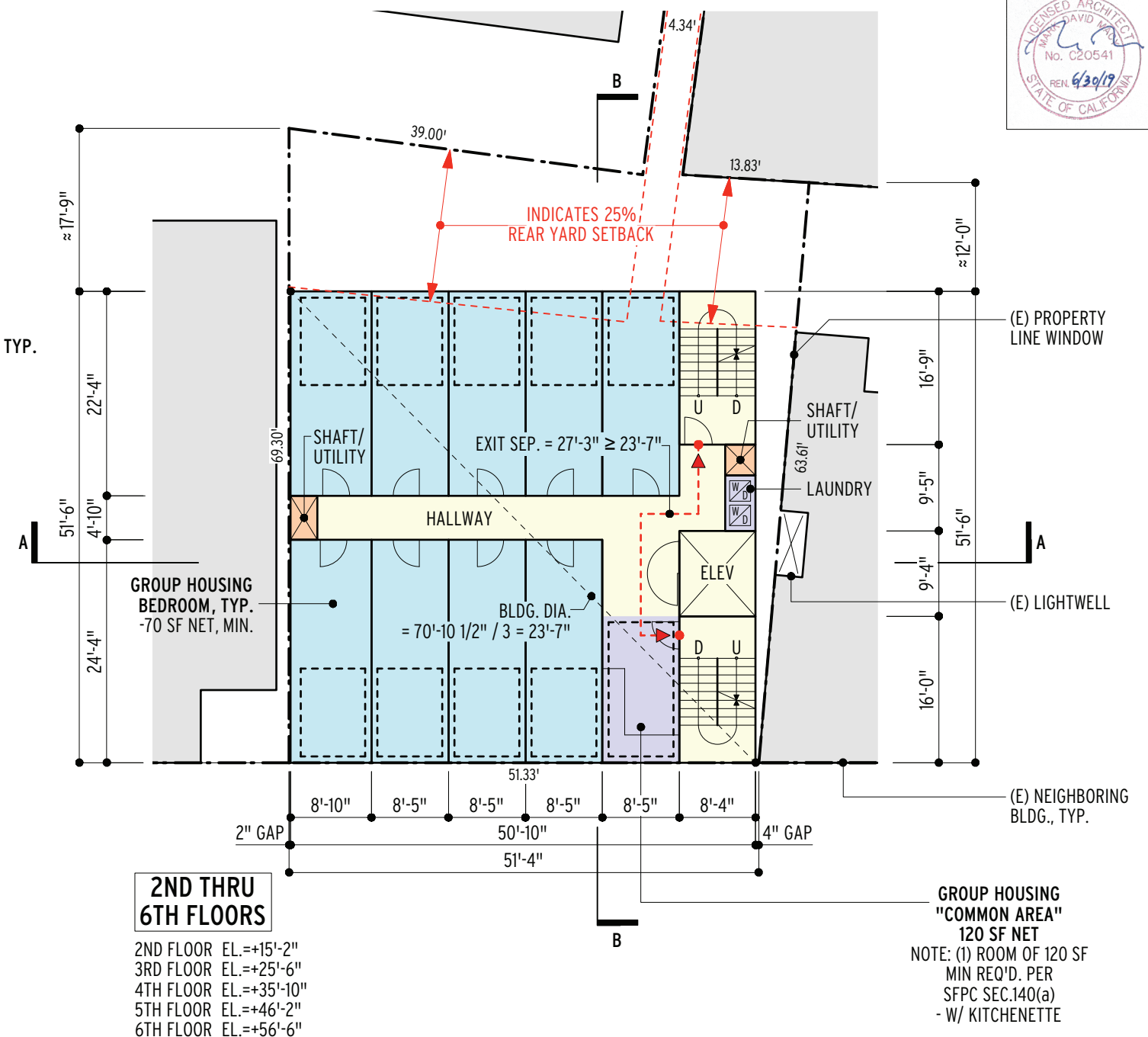
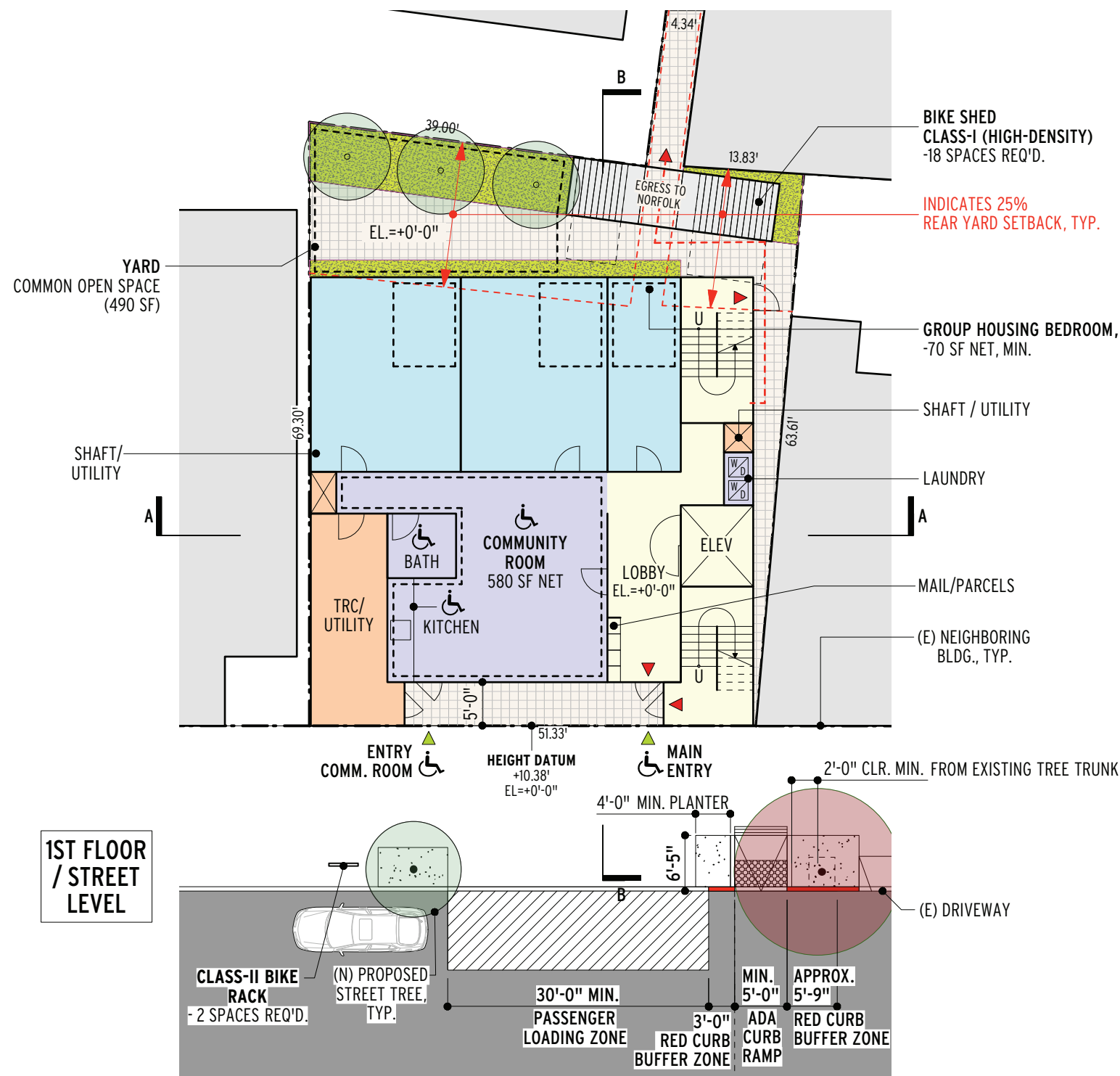
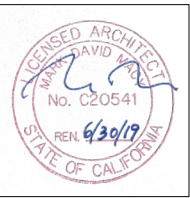


EXISTING SITE PLAN 09/06/19



PROPOSED SITE PLAN

09/06/19



0' 16' 32' 48'

1/16" = 1'-0"



BONUS PROJECT PLAN DIAGRAMS

09/06/19

**MACY
ARCH
ITECTURE**

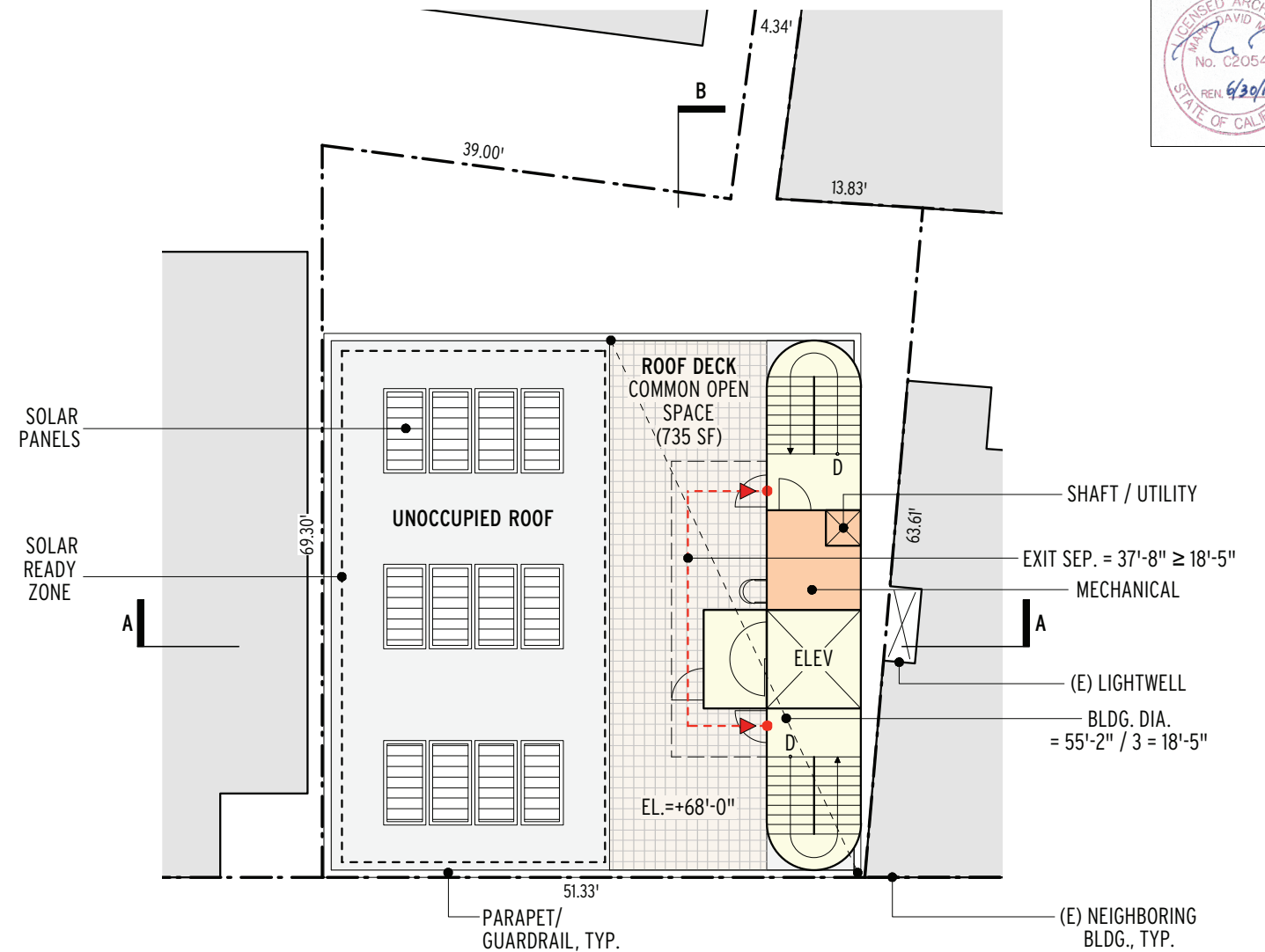
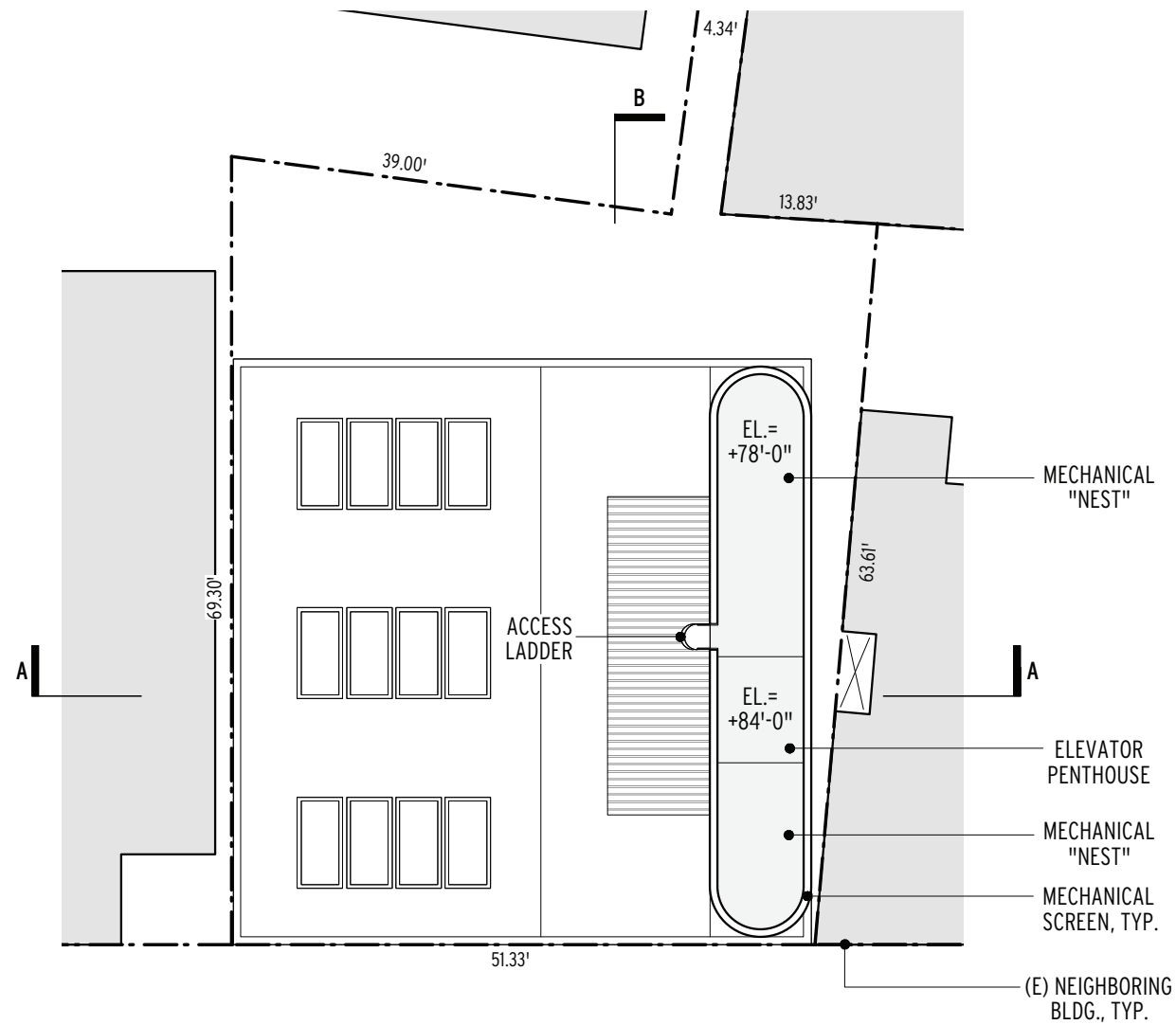
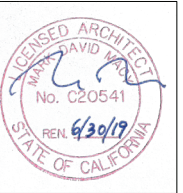
315 Linden Street San Francisco CA 94102 Tel 415 551 7630 www.macyarchitecture.com © 2019 Macy Architecture



ENVIRONMENTAL EVALUATION APPLICATION (EEA)
& STATE DENSITY BONUS APPLICATION
351-12TH STREET (BLOCK 352I LOT 022C) AFFORDABLE-WITHOUT-SUBSIDY RENTAL GROUP HOUSING

1321 Mission St, San Francisco, CA 94103 Tel 415.701.7000 www.panoramic.com

PAGE 14 OF 25



UPPER ROOF PLAN

ROOF PLAN

0' 16' 32' 48'

1/16" = 1'-0"



BONUS PROJECT PLAN DIAGRAMS

09/06/19

**MACY
ARCH
ITEC
TURE**

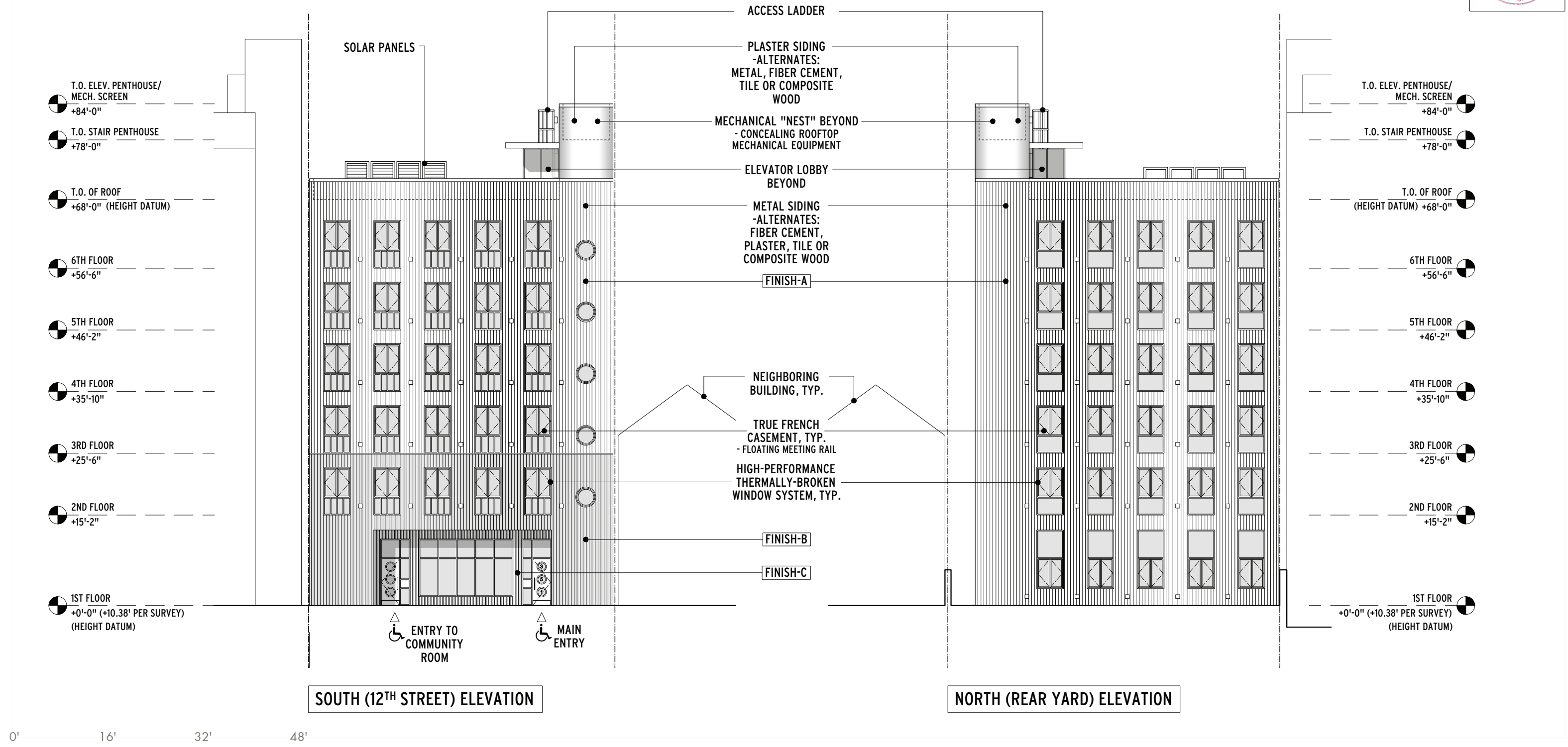
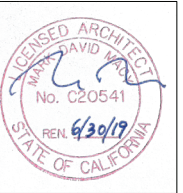
315 Linden Street San Francisco CA 94102 Tel 415 551 7630 www.macyarchitecture.com © 2019 Macy Architecture

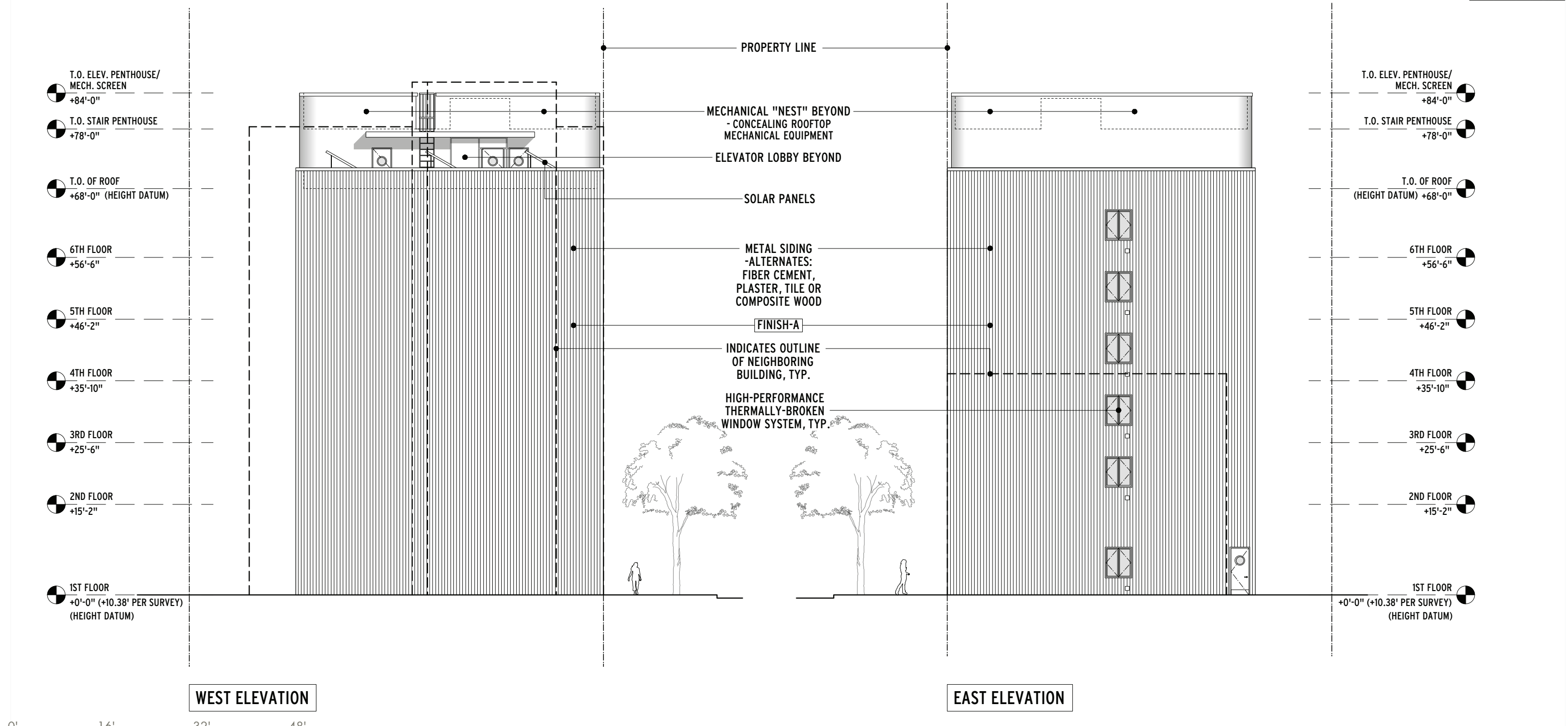
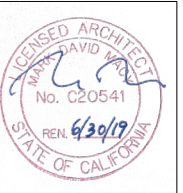


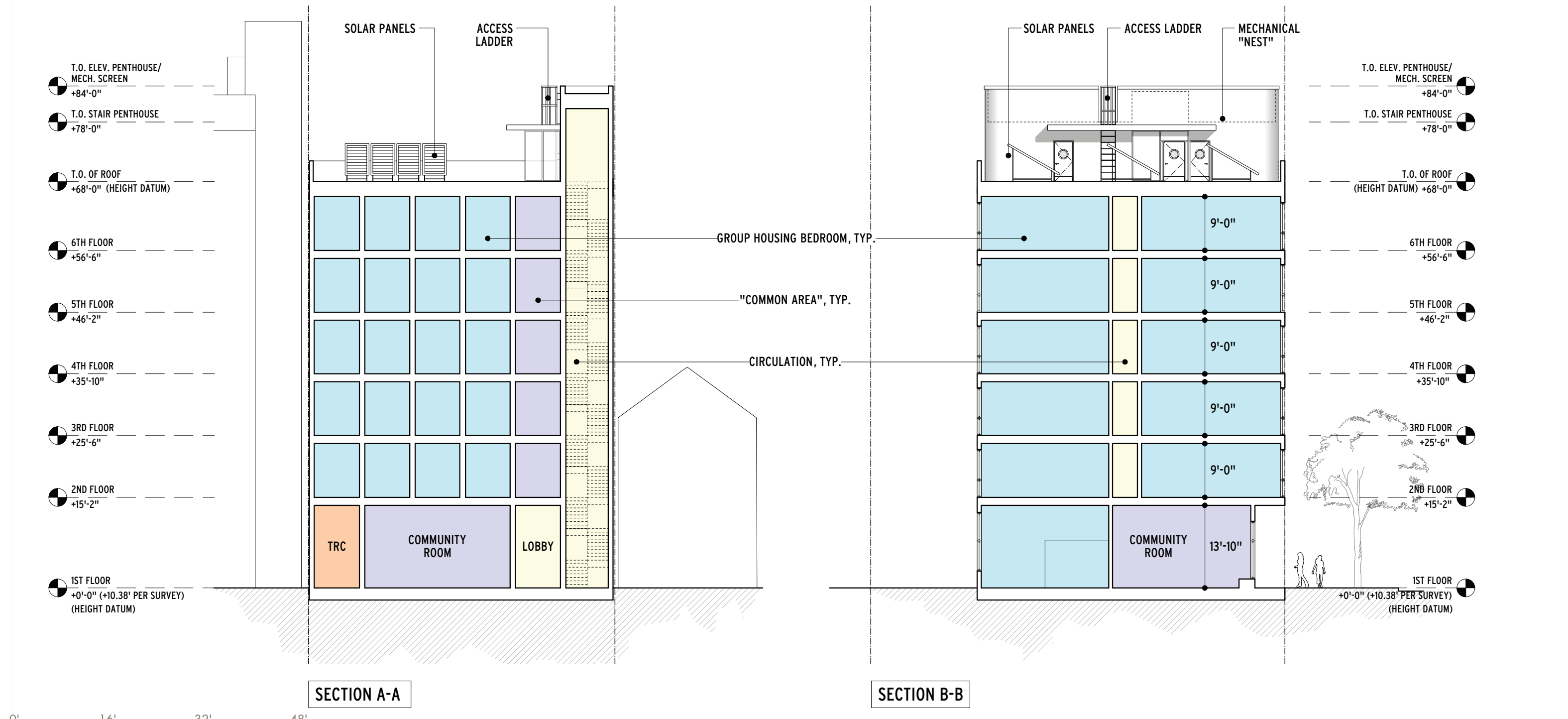
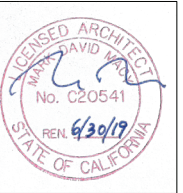
ENVIRONMENTAL EVALUATION APPLICATION (EEA)
& STATE DENSITY BONUS APPLICATION
351-12TH STREET (BLOCK 3521 LOT 022C) AFFORDABLE-WITHOUT-SUBSIDY RENTAL GROUP HOUSING

1321 Mission St, San Francisco, CA 94103 Tel 415.701.7000 www.panoramic.com

PAGE 15 OF 25



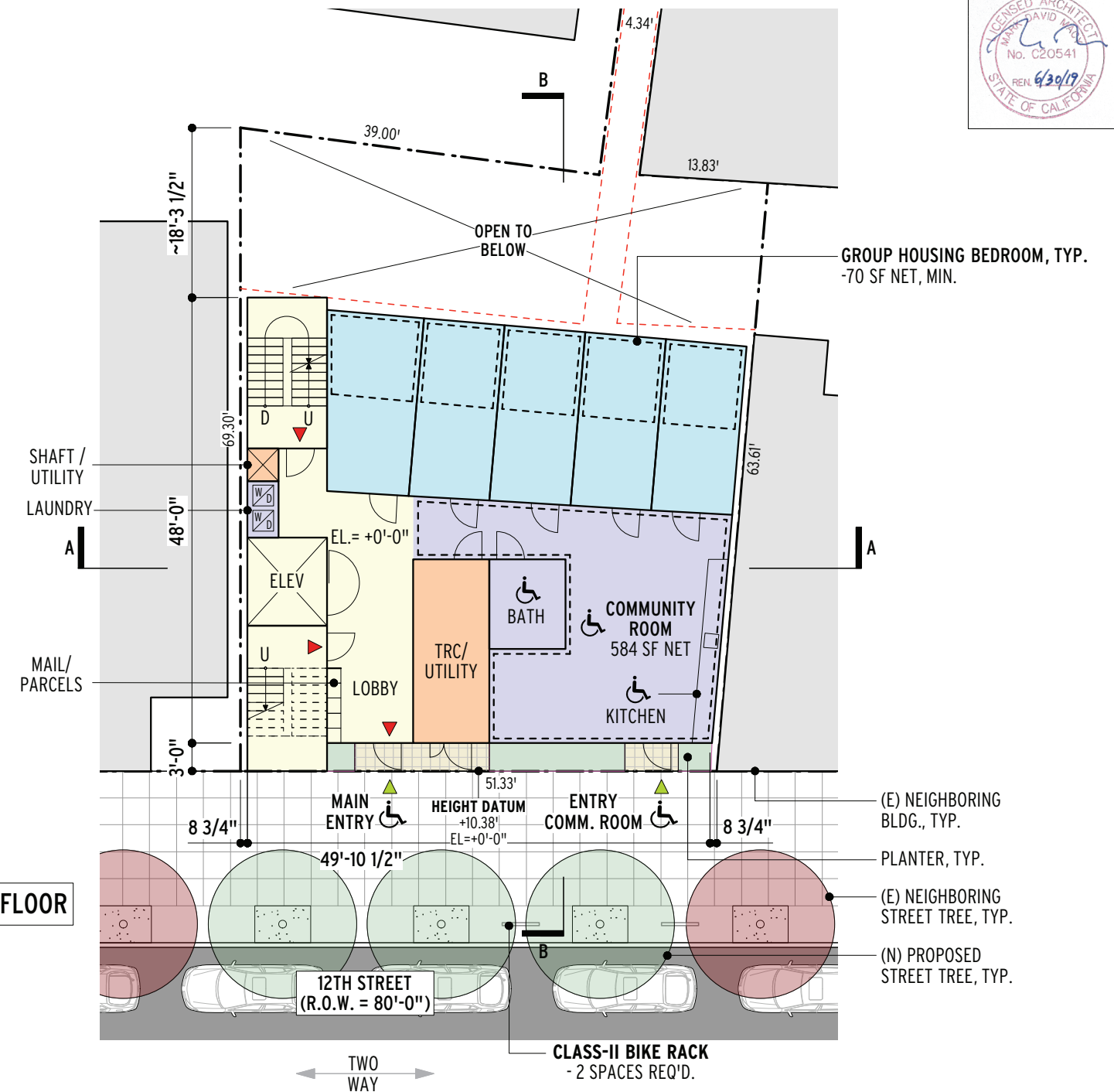
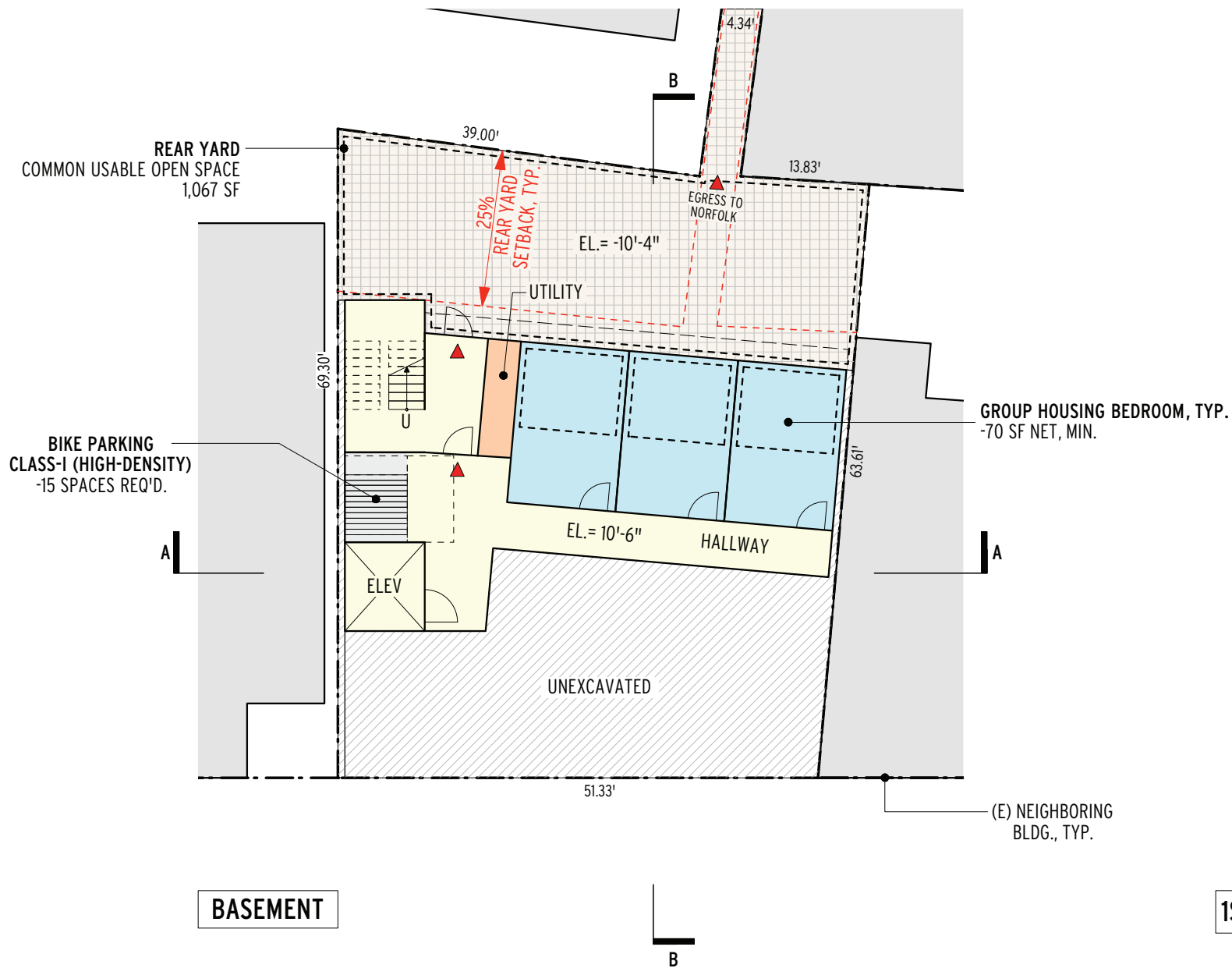
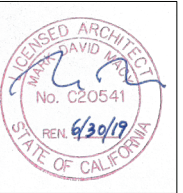




APPENDIX: BASE PROJECT

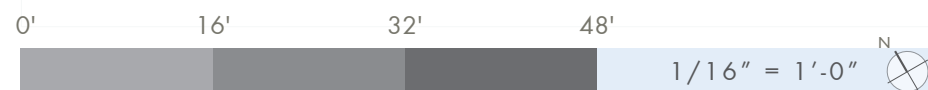
09/06/19

BASE PROJECT - AREA SUMMARY											DESCRIPTION	
LEVEL	B	1	2	3	4	5	R*	TOTAL GSF	TOTAL GFA**		A 100% CODE-COMPLIANT "BASE PROJECT" CONSISTING OF RENTAL GROUP HOUSING CONTAINING 40 BEDROOMS.	
RESIDENTIAL - GROUP HOUSING ROOMS	602	841	1,452	1,452	1,452	1,452	0	7,251	7,251			
SHARED AMENITY	0	711	138	138	138	138	0	1,263	1,263		PLANNING DATA	
BICYCLE PARKING	64	0	0	0	0	0	0	64	0			
UTILITY	44	174	12	12	12	12	297	563	266		ASSESSOR PARCEL: BLOCK 3521 LOT 022C ZONING: WMUG (WESTERN SOMA DISTRICT S.U.D.) HEIGHT & BULK DISTRICT: 55-X LOT AREA: 3,855 SF (0.088 AC) GROSS SQUARE FEET OF CONSTRUCTION: 13,627 SF "RESIDENTIAL" GROSS FLOOR AREA: 13,127 SF (PER SFPC SEC.102) TOTAL GROUP HOUSING BEDROOM UNITS: 40 REQ'D ON-SITE BELOW-MARKET-RATE (BMR) UNITS: 8* USABLE OPEN SPACE: 1,067 SF PROVIDED -1,067 SF REQ'D (40 x 26.67 SF/BDRM = 1,067 SF) BICYCLE PARKING: 15 CLASS I SPACES -PLUS (2) CLASS II (2 PER 100 BDRMS REQ'D) AUTOMOBILE PARKING: 0 -NONE REQUIRED PER SFPC TABLE 152.1 NOTES: * 19% OF 40 GROUP HOUSING BEDROOM UNITS = 8 BMR UNITS TOTAL. 11% LOW (4 @ 55% AMI), 4% MODERATE (2 @ 80% AMI) AND 4% MIDDLE-INCOME (2 @ 110% AMI)	
CIRCULATION	640	655	763	763	763	763	139	4,486	4,347			
TOTAL	1,350	2,381	2,365	2,365	2,365	2,365	436	13,627	13,127			
*Mechanical, Stair & Elevator Penthouses												
**GFA per San Francisco Planning Code Sec. 102												
GROUP HOUSING ROOM SUMMARY												
LEVEL	B	1	2	3	4	5	R	TOTAL GROUP HOUSING BEDROOM UNITS				
# OF GROUP HOUSING ROOMS	3	5	8	8	8	8	0	40				
OUTDOOR SPACE SUMMARY												
LEVEL	B	1	2	3	4	5	R	TOTAL				
COMMON OPEN SPACE	1,067	0	0	0	0	0	0	1,067				
											BUILDING DATA	
											STORIES: 5 STORIES + BASEMENT CONSTRUCTION TYPE: TYPE IIIB FULLY SPRINKLERED BUILDING HEIGHT: 55'-0" BUILDING USE: PRIVATELY-FUNDED CONGREGATE RESIDENCE OCCUPANCY TYPE: R2	



BASEMENT

1ST FLOOR



BASE PROJECT PLAN DIAGRAMS

09/06/19

**MACY
ARCH
ITEC
TURE**

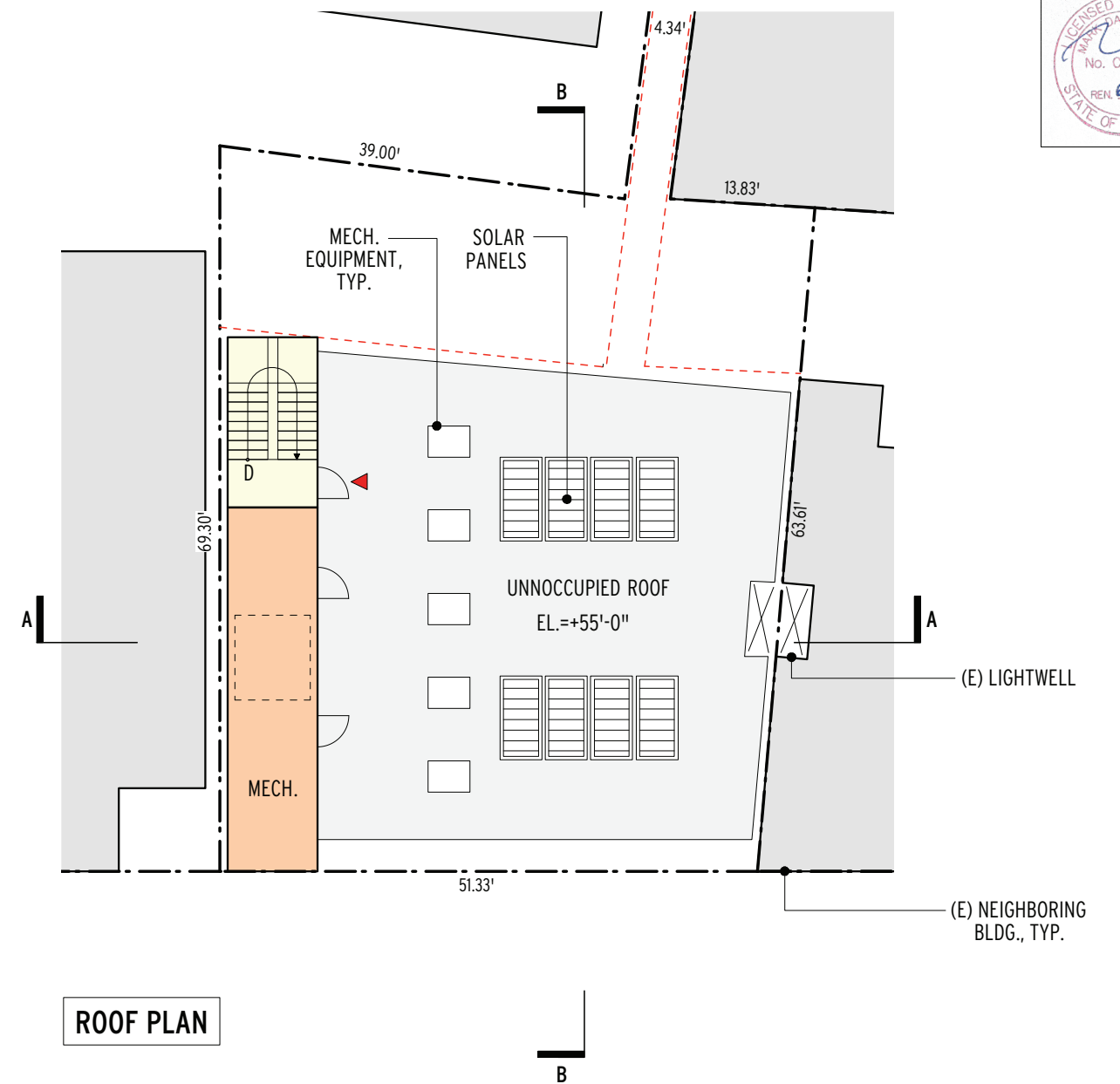
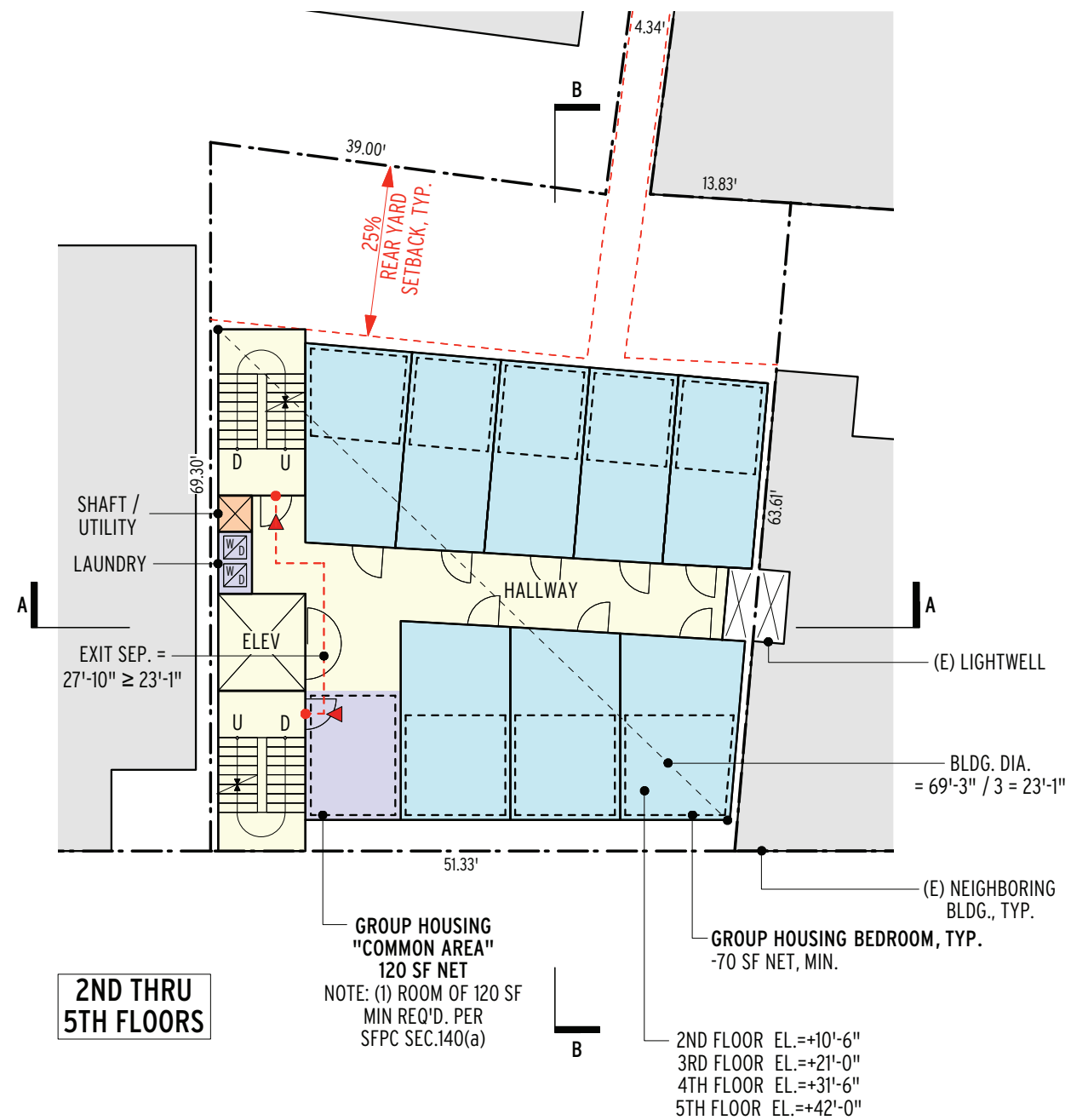
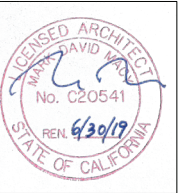
315 Linden Street San Francisco CA 94102 Tel 415 551 7630 www.macyarchitecture.com © 2019 Macy Architecture



ENVIRONMENTAL EVALUATION APPLICATION (EEA)
& STATE DENSITY BONUS APPLICATION
351-12TH STREET (BLOCK 3521 LOT 022C) AFFORDABLE-WITHOUT-SUBSIDY RENTAL GROUP HOUSING

1321 Mission St, San Francisco, CA 94103 Tel 415.701.7000 www.panoramic.com

PAGE 21 OF 25



0' 16' 32' 48'

1/16" = 1'-0"



BASE PROJECT PLAN DIAGRAMS

09/06/19

**MACY
ARCH
ITEC
TURE**

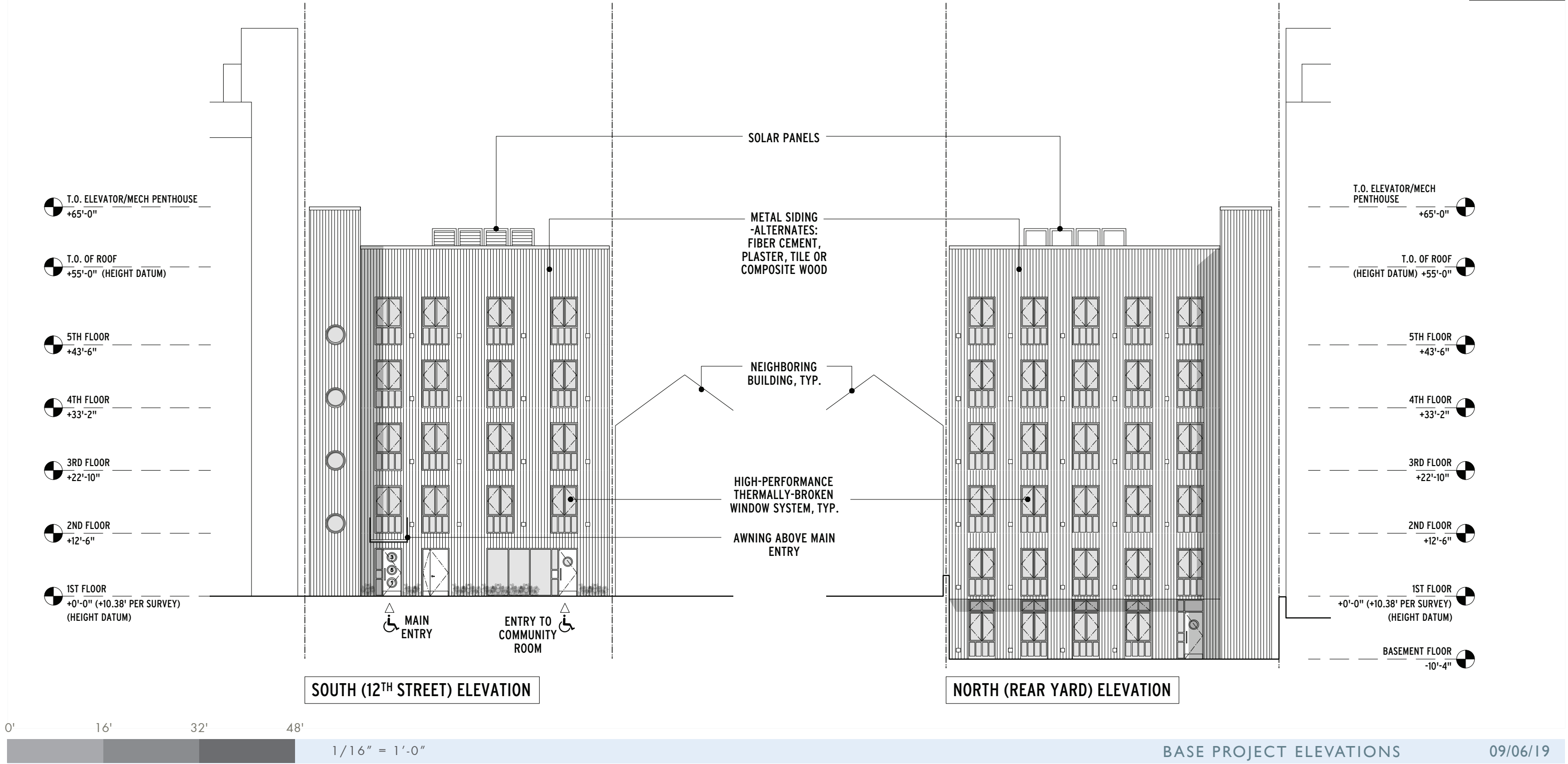
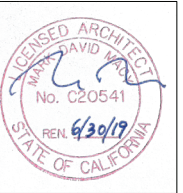
315 Linden Street San Francisco CA 94102 Tel 415 551 7630 www.macyarchitecture.com © 2019 Macy Architecture

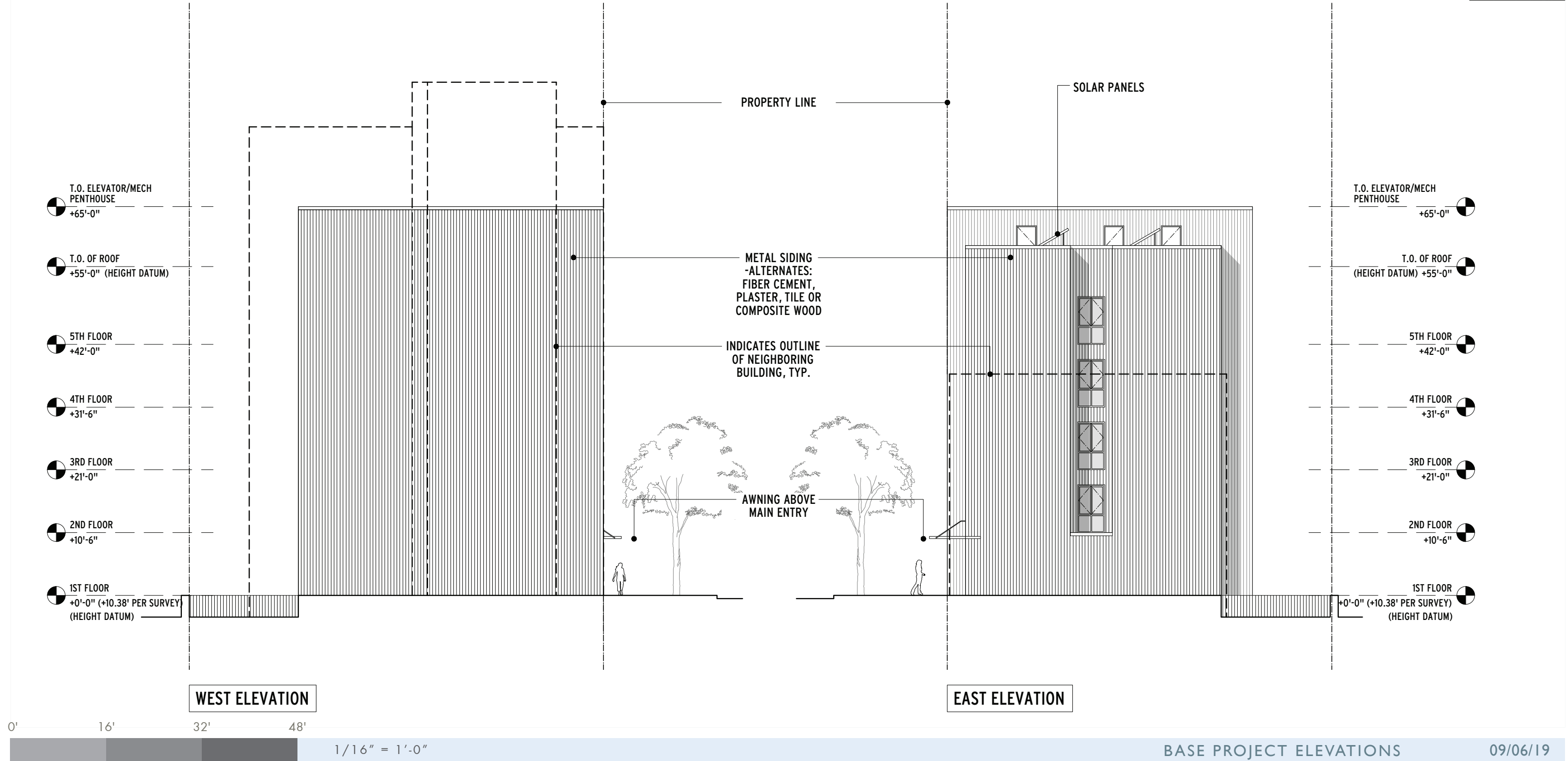
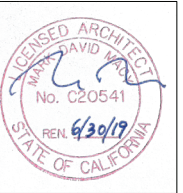


ENVIRONMENTAL EVALUATION APPLICATION (EEA)
& STATE DENSITY BONUS APPLICATION
351-12TH STREET (BLOCK 352I LOT 022C) AFFORDABLE-WITHOUT-SUBSIDY RENTAL GROUP HOUSING

1321 Mission St, San Francisco, CA 94103 Tel 415.701.7000 www.panoramic.com

PAGE 22 OF 25





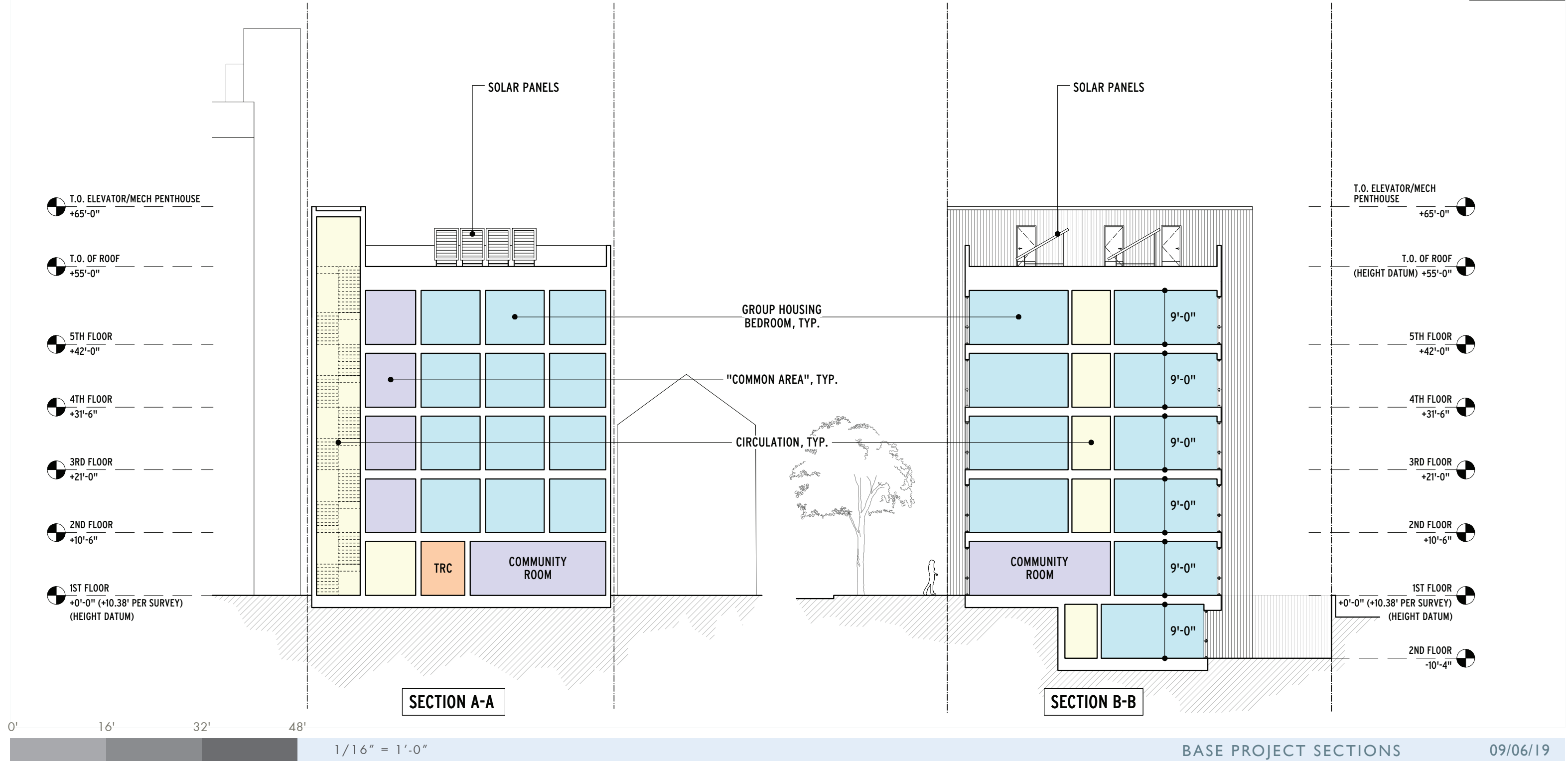
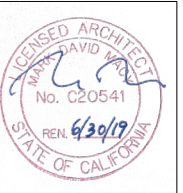


Exhibit C



San Francisco
Planning

1650 MISSION STREET, SUITE 400
SAN FRANCISCO, CA 94103
SFPLANNING.ORG / 415.575.9010

REVISED Certificate of Determination COMMUNITY PLAN EVALUATION

Case No.: **2018-004545ENV**
Project Title: **351 12th Street**
Zoning/Plan Area: Western SoMa Mixed Use-General (WMUG)
55/65-X Height/Bulk District
Western SoMa Community Plan Area
Block/Lot: 3521/022c
Lot Size: 3,855 square feet
Project Sponsor: Zac Shore, Panoramic Interests, (415) 701-7002
Staff Contact: Megan Calpin, (415) 575-9049, megan.calpin@sfgov.org

THIS COMMUNITY PLAN EVALUATION (CPE) SUPERCEDES THE CPE THAT WAS PUBLISHED ON SEPTEMBER 16, 2019. This CPE has been updated to include the accurate description of concessions and waivers; updates to the site vicinity description; update the CEQA approval action; and to update the number of residents anticipated to potentially reside on the site. The text on page 2 of this Certificate of Determination and pages 1 through 37 of Attachment B: Initial Study have been updated in ~~striketrough~~ to indicate text deletions and double underline to indicate text additions. All population and housing, transportation and circulation, recreation, and public services impact conclusions reported in the original CPE remain unchanged as a result of this update.

PROJECT DESCRIPTION

The approximately 3,855-square-foot project site is through-lot parcel located along 12th Street, on the block bounded by Norfolk Street to the northeast, Harrison Street to the southeast, 12th Street to the southwest, and Folsom Street to the northwest in the Western South of Market neighborhood. The project site has frontages along 12th and Norfolk streets. The proposed project site is mostly square, with a 4.34-foot-wide egress to Norfolk Street from the rear yard. The project site previously operated as a surface parking lot with 22 standard vehicle parking spaces with an approximately 50-foot-wide curb cut along 12th Street. Presently, the site is used as a construction staging location for an adjacent construction site

The proposal is to demolish and remove the surface parking lot and construct an approximately 15,600-square-foot, six-story group housing building. The building would be 68 feet tall to the top of the roof, with a stair and elevator penthouse, the tallest portion of which would reach 84 feet above grade. The proposed building would consist of 48 group housing units. The average group housing unit size would be approximately 205 square feet. The ground floor would include a communal kitchen and community room, residential lobby, and three group housing units. The rear yard would be 490 square feet and contain a Class 1 bicycle parking shed for 18 bicycles. The second through sixth floors would each contain nine group housing units, a 120-square-foot common area, and laundry facilities. Half of the roof would contain 735 square feet of common open space for residents. The other half of the roof would be reserved

for solar panels. The proposed project would include 18 Class 1 bicycle parking spaces in the rear yard shed, two Class 2 bicycle parking spaces along 12th Street, and no vehicle parking.

The proposed project would take advantage of the State Density Bonus Law (California Government Code sections 65915-65918), which allows waivers and concessions from local development standards for projects. Under the State Density Bonus Law, the project would seek ~~concessions~~ waivers for rear yard requirements, ~~dwelling-unit-bedroom~~ exposure, and open space for the new group housing units and ~~would also seek a waiver~~ to increase the permitted height of the new building by one story or 13 feet (the height district allows 55 feet). The law requires ~~5~~ 20 percent of all base project group housing units be provided in the form of on-site affordable units. The project would provide ~~two-eight~~ affordable group housing units, which would comply with this requirement.

The proposal includes removing the existing approximately 50-foot-wide curb cut, adding one vehicle parking space, adding a 30-foot minimum white curb loading zone for passengers, a 3-foot red curb buffer zone, a minimum 5-foot Americans-with-Disabilities-Act-compliant curb ramp and an approximately 5-foot-9-inch red curb buffer zone. Per SFMTA and DPW direction, a portion of the proposed curb changes would occur in front of the adjacent property, 365 12th Street (block and lot 3521/019).

Construction of the proposed project would occur over approximately 12 months. The total amount of excavation for the proposed project would be approximately 242 cubic yards of soil to a maximum depth of approximately 4 feet. The proposed foundation type would be a slab on grade.

COMMUNITY PLAN EVALUATION OVERVIEW

California Environmental Quality Act (CEQA) section 21083.3 and CEQA Guidelines section 15183 provide that projects that are consistent with the development density established by existing zoning, community plan or general plan policies for which an environmental impact report (EIR) was certified, shall not be subject to additional environmental review except as might be necessary to examine whether there are project-specific significant effects which are peculiar to the project or its site. Section 15183 specifies that examination of environmental effects shall be limited to those effects that: a) are peculiar to the project or parcel on which the project would be located; b) were not analyzed as significant effects in a prior EIR on the zoning action, general plan or community plan with which the project is consistent; c) are potentially significant off-site and cumulative impacts that were not discussed in the underlying EIR; or d) are previously identified in the EIR, but which, as a result of substantial new information that was not known at the time that the EIR was certified, are determined to have a more severe adverse impact than that discussed in the underlying EIR. Section 15183(c) specifies that if an impact is not peculiar to the parcel or to the proposed project, then an EIR need not be prepared for the project solely on the basis of that impact.

This determination evaluates the potential project-specific environmental effects of the 351 12th Street project described above and incorporates by reference information contained in the programmatic EIR for the Western South of Market Community Plan, Rezoning of Adjacent Parcels, and 350 Eighth Street (Western SoMa PEIR)¹. Project-specific studies were prepared for the proposed project to determine if the project would result in any significant environmental impacts that were not identified in the Western SoMa PEIR.

¹ Planning Department Case No. 2007.1305E and State Clearinghouse No. 2009082031

FINDINGS

As summarized in the initial study – community plan evaluation prepared for the proposed project²:

1. The proposed project is consistent with the development density established for the project site in the Western SoMa Area Plan;
2. The proposed project would not result in effects on the environment that are peculiar to the project or the project site that were not identified as significant effects in the Western SoMa PEIR;
3. The proposed project would not result in potentially significant off-site or cumulative impacts that were not identified in the Western SoMa PEIR;
4. The proposed project would not result in significant effects, which, as a result of substantial new information that was not known at the time the Western SoMa PEIR was certified, would be more severe than were already analyzed and disclosed in the PEIR; and
5. The project sponsor will undertake feasible mitigation measures specified in the Western SoMa PEIR to mitigate project-related significant impacts.

Mitigation measures are included in this project and the project sponsor has agreed to implement these measures. See the attached Mitigation Monitoring and Reporting Program (MMRP) for the full text of required mitigation measures.

CEQA DETERMINATION

The project is eligible for streamlined environmental review per section 15183 of the CEQA Guidelines and California Public Resources Code section 21083.3.

DETERMINATION

I do hereby certify that the above determination has been made pursuant to State and local requirements.



for Lisa Gibson
Environmental Review Officer



Date

² The initial study – community plan evaluation is available for review at the San Francisco Property Information Map, which can be accessed at <https://sfplanninggis.org/PIM/>. The file can be viewed by clicking on the Planning Applications link, clicking the “More Details” link under the project’s environmental case number (2018-004545ENV) and then clicking on the “Related Documents” link.

ATTACHMENTS

- A. MMRP
- B. REVISED Initial Study – Community Plan Evaluation

CC: Zac Shore, Panoramic Interests, Project Sponsor;
Supervisor Matt Haney, District 6;
Veronica Flores, Current Planning Division

MITIGATION MONITORING AND REPORTING PROGRAM

351 12TH STREET – 2018-004545ENV

Mitigation Measures	Applies to These Project Components	Responsibility for Implementation	Mitigation Schedule	Monitoring/Reporting Responsibility	Monitoring Schedule
D. Cultural and Paleontological Resources					
<p>M-CP-4b: Procedures for Accidental Discovery of Archeological Resources. This mitigation measure is required to avoid any potential adverse effect on accidentally discovered buried or submerged historical resources as defined in <i>CEQA Guidelines</i> Section 15064.5(a)(c).</p> <p>The project sponsor shall distribute the Planning Department archeological resource “ALERT” sheet to the project prime contractor; to any project subcontractor (including demolition, excavation, grading, foundation, pile driving, etc. firms); and to utilities firms involved in soils-disturbing activities within the project site. Prior to any soils-disturbing activities being undertaken, each contractor is responsible for ensuring that the “ALERT” sheet is circulated to all field personnel, including machine operators, field crew, pile drivers, and supervisory personnel. The project sponsor shall provide the Environmental Review Officer (ERO) with a signed affidavit from the responsible parties (prime contractor, subcontractor(s), and utilities firms) to the ERO confirming that all field personnel have received copies of the “ALERT” sheet.</p> <p>Should any indication of an archeological resource be encountered during any soils-disturbing activity of the project, the project head foreman and/or project sponsor shall immediately notify the ERO and shall immediately suspend any soils-disturbing activities in the vicinity of the discovery until the ERO has determined what additional measures should be undertaken.</p> <p>If the ERO determines that an archeological resource may be present within the project site, the project sponsor shall retain the services of an archeological consultant from the pool of qualified archeological consultants maintained by the Planning Department archeologist. The archeological consultant shall advise the ERO as to whether the discovery is an archeological resource, retains sufficient integrity, and is of potential scientific/historical/cultural significance. If an archeological resource is present, the archeological consultant shall identify and evaluate the archeological resource. The archeological consultant shall make a recommendation as to what action, if any, is warranted. Based on this information, the ERO may require, if warranted, specific additional measures to be implemented by the project sponsor.</p> <p>Measures might include preservation in situ of the archeological resource, an archeological monitoring program, or an archeological testing program. If an</p>	<ul style="list-style-type: none"> Excavation During Construction 	<p>Project sponsor, contractor, Planning Department’s archeologist or qualified archaeological consultant, and Planning Department’s Environmental Review Officer for each subsequent project undertaken pursuant to the Western SoMa Community Plan or Rezoning of Adjacent Parcels.</p>	<p>Prior to issuance of any permit for soil-disturbing activities and during construction.</p>	<p>Project Sponsor; ERO; archeologist.</p>	<p>Considered complete upon ERO’s approval of FARR.</p>

Mitigation Measures	Applies to These Project Components	Responsibility for Implementation	Mitigation Schedule	Monitoring/Reporting Responsibility	Monitoring Schedule
<p>archeological monitoring program or archeological testing program is required, it shall be consistent with the Environmental Planning (EP) division guidelines for such programs. The ERO may also require that the project sponsor immediately implement a site security program if the archeological resource is at risk from vandalism, looting, or other damaging actions.</p> <p>The project archeological consultant shall submit a Final Archeological Resources Report (FARR) to the ERO that evaluates the historical significance of any discovered archeological resource and describes the archeological and historical research methods employed in the archeological monitoring/data recovery program(s) undertaken. Information that may put at risk any archeological resource shall be provided in a separate removable insert within the final report.</p> <p>Copies of the Draft FARR shall be sent to the ERO for review and approval. Once approved by the ERO, copies of the FARR shall be distributed as follows: California Archaeological Site Survey Northwest Information Center (NWIC) shall receive one copy and the ERO shall receive a copy of the transmittal of the FARR to the NWIC. The Environmental Planning Division of the Planning Department shall receive one bound copy, one unbound copy, and one unlocked, searchable PDF copy on a CD of the FARR along with copies of any formal site recordation forms (CA DPR 523 series) and/or documentation for nomination to the National Register of Historic Places/California Register of Historical Resources. In instances of high public interest or interpretive value, the ERO may require a different final report content, format, and distribution from that presented above.</p>					
F. Noise and Vibration					
<p>M-NO-2a: General Construction Noise Control Measures. To ensure that project noise from construction activities is minimized to the maximum extent feasible, the sponsor shall undertake the following:</p> <ul style="list-style-type: none"> The sponsor shall require the general contractor to ensure that equipment and trucks used for project construction use the best available noise control techniques (e.g., improved mufflers, equipment redesign, use of intake silencers, ducts, engine enclosures and acoustically attenuating shields or shrouds, wherever feasible). The sponsor shall require the general contractor to locate stationary noise sources (such as compressors) as far from adjacent or nearby sensitive receptors as possible, to muffle such noise sources, and to construct barriers around such sources and/or the construction site, which could reduce construction noise by as much as 5 dBA. To further reduce noise, the contractor shall locate stationary equipment in pit areas or excavated areas, if feasible. 	<ul style="list-style-type: none"> Construction 	Project sponsor and construction contractor.	During construction period.	Project sponsor to provide monthly noise reports during construction.	Considered complete upon final monthly report.

Mitigation Measures	Applies to These Project Components	Responsibility for Implementation	Mitigation Schedule	Monitoring/Reporting Responsibility	Monitoring Schedule
<ul style="list-style-type: none"> The sponsor shall require the general contractor to use impact tools (e.g., jack hammers, pavement breakers, and rock drills) that are hydraulically or electrically powered wherever possible to avoid noise associated with compressed air exhaust from pneumatically powered tools. Where use of pneumatic tools is unavoidable, an exhaust muffler on the compressed air exhaust shall be used, along with external noise jackets on the tools, which could reduce noise levels by as much as 10 dBA. The sponsor shall include noise control requirements in specifications provided to construction contractors. Such requirements could include, but not be limited to, performing all work in a manner that minimizes noise to the extent feasible; undertaking the most noisy activities during times of least disturbance to surrounding residents and occupants, as feasible; and selecting haul routes that avoid residential buildings inasmuch as such routes are otherwise feasible. Prior to the issuance of each building permit, along with the submission of construction documents, the sponsor shall submit to the San Francisco Planning Department and Department of Building Inspection (DBI) a list of measures to respond to and track complaints pertaining to construction noise. These measures shall include: (1) a procedure and phone numbers for notifying DBI, the Department of Public Health, and the Police Department (during regular construction hours and off-hours); (2) a sign posted on-site describing noise complaint procedures and a complaint hotline number that shall be answered at all times during construction; (3) designation of an on-site construction complaint and enforcement manager for the project; and (4) notification of neighboring residents and non-residential building managers within 300 feet of the project construction area at least 30 days in advance of extreme noise-generating activities (defined as activities generating noise levels of 90 dBA or greater) about the estimated duration of the activity. 					
G. Air Quality					
M-AQ-7: Construction Emissions Minimization Plan for Health Risks and Hazards. To reduce the potential health risk resulting from project construction activities, the project sponsor of each development project in the Draft Plan Area and on the Adjacent Parcels shall undertake a project-specific construction health risk analysis to be performed by a qualified air quality specialist, as appropriate and determined by the Environmental Planning Division of the San Francisco Planning Department, for diesel-powered and other applicable construction equipment, using the methodology recommended by the Bay Area Air Quality Management District (BAAQMD) and/or the San Francisco Planning Department. If the health risk analysis determines that construction emissions would exceed health risk significance thresholds identified by the BAAQMD and/or the San Francisco	<ul style="list-style-type: none"> Construction 	Project Sponsor; contractor; certified mechanic	Prior to any demolition or construction activities	Project Sponsor; contractor; certified mechanic; Planning Department	Prior to and during any demolition or construction activities

Mitigation Measures	Applies to These Project Components	Responsibility for Implementation	Mitigation Schedule	Monitoring/Reporting Responsibility	Monitoring Schedule
<p>Planning Department, the project sponsor shall develop a Construction Emissions Minimization Plan for Health Risks and Hazards designed to reduce health risks from construction equipment to less-than-significant levels.</p> <p>All requirements in the Construction Emissions Minimization Plan must be included in contract specifications. The Construction Emissions Minimization Plan is described in Mitigation Measure M-AQ-6, Construction Emissions Minimization Plan for Criteria Air Pollutants.</p>					



REVISED
Attachment B:
Initial Study – Community Plan Evaluation

Case No.: 2018-004545ENV
Project Address: 351 12th Street
Zoning: Western SoMa Mixed Use-General (WMUG)
55-X Height/Bulk District
Block/Lot: 3521/022c
Lot Size: 3,855 square feet (0.08 acres)
Plan Area: Western South of Market Plan Area
Project Sponsor: Zac Shore, Panoramic Interests, (415) 701-7002
Staff Contact: Megan Calpin, megan.calpin@sfgov.org, (415) 575-9049

THIS COMMUNITY PLAN EVALUATION (CPE) SUPERCEDES THE CPE THAT WAS PUBLISHED ON SEPTEMBER 16, 2019. This CPE has been updated to include the accurate description of concessions and waivers; updates to the site vicinity description; to update the CEQA approval action; and to update the number of residents anticipated to potentially reside on the site. The text on page 2 of the Certificate of Determination and pages 1 through 37 of this Attachment B: Initial Study have been updated in ~~strike through~~ to indicate text deletions and double underline to indicate text additions. All population and housing, transportation and circulation, recreation, and public services impact conclusions reported in the original CPE remain unchanged as a result of this update.

A. PROJECT DESCRIPTION

The approximately 3,855-square-foot project site is a through-lot parcel located along 12th Street, on the block bounded by Norfolk Street to the northeast, Harrison Street to the southeast, 12th Street to the southwest, and Folsom Street to the northwest in the Western South of Market neighborhood. The project site has frontages along 12th and Norfolk streets. The proposed project site is mostly square, with a 4.34-foot-wide egress to Norfolk Street from the rear yard. The project site previously operated as a surface parking lot with 22 standard vehicle parking spaces with an approximately 50-foot-wide curb cut along 12th Street. Presently, the site is used as a construction staging location for an adjacent construction site.

The proposed project would demolish and remove the surface parking lot and construct an approximately 15,600-square-foot, six-story group housing building. The building would be 68 feet tall to the top of the roof, with a stair and elevator penthouse, the tallest portion of which would reach 84 feet above grade. The proposed building would include 48 group housing units. The average group housing unit size would be approximately 205 square feet. The ground floor would include a communal kitchen and community room, residential lobby, and three group housing units. The rear yard would be 490 square feet and contain a Class 1 bicycle parking shed for 18 bicycles. The second through sixth floors would each contain nine group housing units, a

120-square-foot common area, and laundry facilities. Half of the roof would contain 735 square feet of common open space for residents. The other half of the roof would be reserved for solar panels. The proposed project would include 18 Class 1 bicycle parking spaces in the rear yard shed, two Class 2 bicycle parking spaces along 12th Street, and no vehicle parking.

The proposed project would take advantage of the State Density Bonus Law (California Government Code sections 65915-65918), which allows waivers and concessions from local development standards for projects in exchange for additional affordable housing. Under this law, the proposed project is seeking ~~concessions~~ waivers for rear yard requirements, ~~bedroom dwelling unit~~ exposure, and open space for the new group housing units and ~~a waiver~~ to increase the permitted height of the new building by one story, or 13 feet (the height district allows 55 feet). The law requires 20 percent of all base project group housing units be provided in the form of on-site affordable units. The project would provide eight affordable group housing units, which would comply with this requirement.

The proposal includes removing the existing approximately 50-foot-wide curb cut, adding one on-street vehicle parking space, adding a 30-foot minimum white curb loading zone for passengers, a 3-foot red curb buffer zone, a minimum 5-foot Americans-with-Disabilities-Act-compliant curb ramp and an approximately 5-foot-9-inch red curb buffer zone (see Figure 3 in Appendix B). Per San Francisco Municipal Transportation Agency (SFMTA) and Department of Public Works (public works) direction, a portion of these proposed curb changes would occur in front of the adjacent property, at 365 12th Street (block/lot 3521/019).

Construction of the proposed project would occur over approximately 12 months. Project construction would require excavation of approximately 242 cubic yards of soil to a maximum depth of approximately 4 feet to install a slab on grade foundation.

Figures 1 - 7 (Appendices A and B) show the proposed project's location, plans, elevations and sections.

Project Approvals

Approval Action: ~~If discretionary review before the planning commission is requested, the discretionary review hearing is the approval action for the project. If no discretionary review is requested, the issuance of the building permit.~~ The adoption of the resolution of findings by the planning commission pursuant to planning code section 206.6 is the approval action for the project. The approval action date establishes the start of the 30-day appeal period for this CEQA determination pursuant to section 31.04(h) of the San Francisco Administrative Code.

The proposed 351 12th Street project would require the following approvals:

Actions by other City Departments

- Site and Building Permits (Department of Building Inspection)
- Site Mitigation Plan (Department of Public Health)
- Approval of modification to on-street loading and removal of on-street parking spaces, special traffic permits for construction staging, if needed (San Francisco Municipal Transportation Agency)

- Recommendation to the San Francisco Board of Supervisors regarding sidewalk legislation, approval of tree planting, and other streetscape improvements (San Francisco Public Works)

B. COMMUNITY PLAN EVALUATION OVERVIEW

CEQA section 21083.3 and 15183 mandate that projects that are consistent with the development density established by existing zoning, community plan, or general plan policies for which an environmental impact report (EIR) was certified, shall not be subject to additional environmental review except as might be necessary to examine whether there are project-specific significant effects that are peculiar to the project or its site. Guidelines section 15183(c) specifies that if an impact is not peculiar to the parcel or to the proposed project, then an EIR need not be prepared for the project solely on the basis of that impact.

This initial study evaluates the potential project-specific environmental effects of the proposed 351 12th Street project described above and incorporates by reference information contained in the programmatic EIR for the Western South of Market Community Plan, Rezoning of Adjacent Parcels, and 350 Eighth Street (Western SoMa PEIR)¹. The following project-specific studies were prepared for the proposed project to determine if the project would result in any significant environmental impacts that were not identified in the PEIR²:

Project Specific Studies	
Geotechnical Report	Shadow Fan
Phase 1 Environmental Site Assessment	Greenhouse Gas Analysis checklist

C. PROJECT SETTING

Site Vicinity

The project vicinity is characterized by a mix of residential, retail, office, nighttime entertainment and production/distribution/repair (PDR) uses. The scale of development in the project vicinity varies in height from one to four stories, with the majority of buildings consisting of two to three stories. Land uses on the same block as the project site include residential, production, distribution, and repair (PDR), and nighttime entertainment uses. Additional land uses within one block of the project site include restaurant, office, and retail uses. Two projects on the same block currently under construction are at 333 12th Street and 1532 Harrison Street, which will be 79 feet tall and 65 feet tall, respectively. In total, those projects would add ~~136~~ 336 new dwelling units, ~~246 "micro," co-living or group housing units,~~ ground floor retail, and 85 off-street parking spaces. The 1532 Harrison Street project also includes the installation of Eagle Plaza, a 13,500-square-foot public open space in the right of way between Harrison and Bernice streets.

¹ San Francisco Planning Department, Western SoMa Community Plan, Rezoning of Adjacent Parcels, and 350 Eighth Street Project Final Environmental Impact Report (PEIR), Planning Department Cases No. 2008.0877E and 2007.1035E, State Clearinghouse No. 2009082031, certified December 6, 2012. Available online at: <https://sfplanning.org/environmental-review-documents>, accessed June 3, 2019.

² Project specific studies prepared for the 351 12th Street project are available for public review at the Planning Department, 1650 Mission Street, 4th Floor, San Francisco, CA 94103 as part of case file no. 2018-004545ENV.

The project site is well served by public transportation. Within one-quarter mile of the project site, the San Francisco Municipal Railway (Muni) operates the following bus lines: 9 San Bruno, 9R San Bruno Rapid, 12 Folsom/Pacific, 27 Bryant, 47 Van Ness, and 83X Mid-Market Express. The closest bus stop is 500 feet northeast at 11th and Harrison streets for Muni Bus Lines 9, 9R, 27, and 47, with AM and PM headways of 15 minutes or less. Another bus stop is 740 feet northwest of the project site at 11th and Folsom streets, serving Muni Bus Line 12, with AM and PM headways of 15 minutes. The BART 16th Street station is located within one half-mile northwest of the project site.

Cumulative Setting

CEQA Guidelines section 15130(b)(1) provides two methods for cumulative impact analysis: the “list-based approach” and the “projections-based approach”. The list-based approach uses a list of projects producing closely related impacts that could combine with those of a proposed project to evaluate whether the project would contribute to significant cumulative impacts. The projections-based approach uses projections contained in a general plan or related planning document to evaluate the potential for cumulative impacts. This project-specific analysis employs both the list-based and projections-based approaches, depending on which approach best suits the resource topic being analyzed.

The proposed project is located within the Western South of Market Plan Area. The Western SoMa PEIR evaluated the physical environmental impacts resulting from the rezoning of this plan area, including impacts resulting from an increase housing units and non-residential uses. The cumulative impact analysis provided in this initial study uses updated projections as needed for certain topics to evaluate whether the proposed project could result in new or substantially more severe cumulative impacts than were anticipated in the Western SoMa PEIR. For example, the cumulative transportation analysis in this initial study is based on projected 2040 cumulative conditions, whereas the Western SoMa PEIR relied on 2025 cumulative transportation projections.

The cumulative analysis for certain localized impact topics (e.g., cumulative shadow and wind effects) uses the list-based approach. The following is a list of reasonably foreseeable projects within the project vicinity (approximately one-quarter mile) that are included (see Figure 8, Appendix C):

- 134 Kissling Street – The project includes the construction of a non-retail car wash structure accessory to the existing motor vehicle repair operation on an adjacent parcel and construction of vehicle storage stackers, to be screened from view by a new screen wall along Howard and Kissling Streets.
- 1394 Harrison Street – The project proposes demolition of an existing car wash facility and construction of a five-story, 76-unit mixed-use residential building. The proposed units are single room occupancy “efficiency studios”.
- 1450 Howard Street – The project proposes construction of a six-story building with 15 residential units (SROs), 15 class 1 bicycle spaces and two class 2 bicycle spaces.
- 1233 Folsom Street – This project proposes to demolish a small industrial building and its associated parking lot/storage yard. The proposed mixed-use project would contain 24

Dwelling Units; two tourist hotel rooms and 639 square feet of ground-floor retail. No automobile parking is proposed.

- 1560 Folsom Street - This project proposes to merge four existing lots into two lots and construct one residential building and one mixed-use residential building. The project would include 231 dwelling units and 6,051 square feet of retail use, with 172 units located in the building facing Folsom Street and 59 units located in the building facing Kissling Street.
- 1675 Howard Street – The project includes constructing an approximately 1,652 sq. ft. addition on top of an existing parking structure.
- 1695 Folsom Street – The project proposes construction of a mixed-use building containing a ground floor limited restaurant and four dwelling units. The existing site is a vacant lot. No off street parking is proposed.
- 220 9th Street – The proposal includes demolition of an existing 23,875 square foot industrial building and construction of a seven-story, 75-foot-tall, mixed-use building, with ground floor commercial space and 74 dwelling units. The proposal includes 23 off-street parking spaces.
- 244 9th Street – The proposal includes a vertical and horizontal addition to an existing two-story office building. The proposal is to add three stories to accommodate 10 dwelling units and a ground floor commercial unit.
- 222 Dore Street – The proposal includes demolition of an existing warehouse building and the construction of 33 residential units. No vehicle parking is proposed.
- 340 Division Street – The proposal is to demolish the existing 1,400-square-foot building and construct a four-story mixed-use residential building, with ground floor retail and three floors of office use.

D. SUMMARY OF ENVIRONMENTAL EFFECTS

The proposed project could potentially affect the environmental factor(s) checked below. The following pages present a more detailed checklist and discussion of each environmental topic.

<input type="checkbox"/> Land Use and Planning	<input checked="" type="checkbox"/> Air Quality	<input type="checkbox"/> Geology and Soils
<input type="checkbox"/> Aesthetics	<input type="checkbox"/> Greenhouse Gas Emissions	<input type="checkbox"/> Hydrology and Water Quality
<input type="checkbox"/> Population and Housing	<input type="checkbox"/> Wind	<input type="checkbox"/> Hazards & Hazardous Materials
<input checked="" type="checkbox"/> Cultural Resources	<input type="checkbox"/> Shadow	<input type="checkbox"/> Mineral Resources
<input type="checkbox"/> Tribal Cultural Resources	<input type="checkbox"/> Recreation	<input type="checkbox"/> Energy
<input type="checkbox"/> Transportation and Circulation	<input type="checkbox"/> Utilities and Service Systems	<input type="checkbox"/> Agriculture and Forestry Resources
<input checked="" type="checkbox"/> Noise	<input type="checkbox"/> Public Services	<input type="checkbox"/> Wildfire

E. EVALUATION OF ENVIRONMENTAL EFFECTS

This initial study evaluates whether the environmental impacts of the proposed project are addressed in the programmatic environmental impact report for the Western SoMa Community Plan, Rezoning of Adjacent Parcels, and 350 Eighth Street Project (Western SoMa PEIR). This initial study considers whether the proposed project would result in significant impacts that: (1) are peculiar to the project or project site; (2) were not identified as significant project-level, cumulative, or off-site effects in the PEIR; or (3) are previously identified significant effects, which as a result of substantial new information that was not known at the time that the Western SoMa PEIR was certified, are determined to have a more severe adverse impact than discussed in the PEIR. Such impacts, if any, will be evaluated in a project-specific, focused mitigated negative declaration or environmental impact report. If no such topics are identified, no additional environmental review shall be required for the project beyond that provided in the Western SoMa PEIR and this project-specific initial study in accordance with CEQA section 21083.3 and CEQA Guidelines section 15183.

Mitigation measures identified in the PEIR are discussed under each topic area, and measure that are applicable to the proposed project are provided under Section G. Mitigation Measures at the end of this initial study.

The Western SoMa PEIR identified significant impacts related to cultural and paleontological resources, transportation and circulation, wind and shadow, noise and vibration, air quality, biological resources, and hazards and hazardous materials. Additionally, the PEIR identified significant cumulative impacts related to cultural and paleontological resources, transportation and circulation, noise air quality, and shadow. Aside from shadow, mitigation measures were identified for all the above impacts and reduced these impacts to less than significant except for those related to cultural and paleontological resources (cumulative impacts from demolition of historic resources); transportation (program-level and cumulative traffic impacts at three intersections; and cumulative transit impacts on several San Francisco Municipal Transportation Agency (Muni) lines); air quality (program-level toxic air contaminants (TACs) and fine particulate matter (PM_{2.5}) pollutant impacts, program-level and cumulative criteria air pollutant impacts); and noise (cumulative noise impacts). No mitigation measures were identified for shadow impacts, which were determined to be significant and unavoidable.

The proposed project would construct a six-story residential building with ~~5048~~ 5048 group housing ~~rooms-units~~ with ground floor common space and kitchen. As discussed below in this initial study, the proposed project would not result in new, significant environmental effects, or effects of greater severity than were already analyzed and disclosed in the Western SoMa PEIR.

Regulatory Changes

Since the certification of the Western SoMa PEIR in 2008, several new policies, regulations, statutes, and funding measures have been adopted, passed, or are underway that affect the physical environment and/or environmental review methodology for projects in the Western SoMa plan areas. As discussed in each topic area referenced below, some of these policies, regulations, statutes, and funding measures have implemented or will implement certain mitigation measures or will reduce impacts determined to be less-than-significant in the PEIR. New and changed policies and regulations relevant to this initial study include:

- State legislation amending CEQA to eliminate consideration of aesthetics and parking impacts for infill projects in transit priority areas, effective January 2014.
- State legislation amending CEQA and San Francisco Planning Commission resolution 19579 replacing level of service analysis of automobile delay with vehicle miles traveled analysis, effective March 2016.
- San Francisco Bicycle Plan update adoption in June 2009, Better Streets Plan adoption in 2010, Transit Effectiveness Project (aka "Muni Forward") adoption in March 2014; Vision Zero adoption by various city agencies in 2014; Propositions A and B passage in November 2014; and the Transportation Sustainability Program consisting of adoption of a transportation sustainability fee, effective January 2016; Planning Commission resolution 19579, effective March 2016; and adoption of a transportation demand management program, effective March 2017.
- San Francisco ordinance establishing Noise Regulations Related to Residential Uses near Places of Entertainment effective June 2015 (see initial study Noise section).
- San Francisco ordinances establishing Construction Dust Control, effective July 2008, and Enhanced Ventilation Required for Urban Infill Sensitive Use Developments, amended December 2014 (see initial study Air Quality section).
- San Francisco Clean and Safe Parks Bond passage in November 2012 and San Francisco Recreation and Open Space Element of the General Plan adoption in April 2014 (see initial study Recreation section).
- Urban Water Management Plan adoption in 2015 (see initial study Utilities and Service Systems section).
- San Francisco Health Code Article 22A amendments effective August 2013 (see initial study Hazardous Materials section).

State Density Bonus

Under Government Code section 65915, the state density bonus law, cities are required to grant density bonuses, waivers from development standards,³ and concessions and incentives⁴ when a developer of a housing project of five or more units includes at least 5 percent of those units as

³ "Development standard" includes a site or construction condition, including but not limited to a height limitation, a setback requirement, a floor area ratio, an onsite open-space requirement, or a parking ratio that applies to a residential development pursuant to any ordinance, general plan element, specific plan, charter, or other local condition, law, policy, resolution, or regulation. (See Government Code section 65915(0)(1)).

⁴ Concessions and incentives mean: (1) a reduction in site development standards or a modification of zoning requirements or architectural design requirements that exceed the minimum building standards approved by the California Building Standards Commission as provided in Part 2.5 (commencing with section 18901) of Division 13 of the Health and Safety Code, including, but not limited to, a reduction in setback and square footage requirements and in the ratio of vehicular parking spaces that would otherwise be required that results in identifiable, financially sufficient, and actual cost reductions; (2) approval of mixed-use zoning in conjunction with the housing project if commercial, office, industrial, or other land uses will reduce the cost of the housing development and if the commercial, office, industrial, or other land uses are compatible with the housing project and the existing or planned development in the area where the proposed housing project will be located; or (3) other regulatory incentives or concessions proposed by the developer or the city, county, or city and county that result in identifiable, financially sufficient, and actual cost reductions. (See Government Code section 65915.)

housing units affordable to moderate, low, or very low income households (between 50 and 120 percent of area median income).⁵ The amount of the density bonus and the number of concessions and incentives varies depending on the percentage of affordable units proposed and the level of affordability; generally, however, state law requires that cities grant between 7 to 35 percent density bonus, and up to three concessions and incentives, if a developer provides between 5 and 40 percent affordable units. Additionally, project sponsors are able to request waivers from development standards if the development standards physically preclude the project with the additional density or with the concessions and incentives.⁶ State law requires that rental units be affordable for a term of no less than 55 years, and that ownership units be affordable to at least the first buyer through a shared equity agreement.⁷ Local jurisdictions are required to adopt an ordinance implementing the state density bonus law; however, absent an ordinance, local jurisdictions are still required to comply with the law.⁸

CEQA Section 21099

In accordance with CEQA section 21099 – Modernization of Transportation Analysis for Transit Oriented Projects – aesthetics and parking shall not be considered in determining if a project has the potential to result in significant environmental effects, provided the project meets the following three criteria:

- a) The project is in a transit priority area;
- b) The project is on an infill site; and
- c) The project is residential, mixed-use residential, or an employment center.

The proposed project meets each of the above three criteria and thus, this checklist does not consider aesthetics or parking in determining the significance of project impacts under CEQA.⁹

E.1 LAND USE AND LAND USE PLANNING

Western SoMa PEIR Land Use and Planning Findings

The Western SoMa PEIR determined that adoption of the Western SoMa Area Plan (the Plan) would not result in a significant impact related to land use and would not result in a cumulative loss of production, distribution, and repair (PDR) uses. The PEIR anticipated additional population and that future development under the Plan would result in more cohesive neighborhoods and

⁵ See generally, Government Code section 65915 et seq.

⁶ See Government Code section 65915(e).

⁷ See Government Code section 65915(c)(1) and (2).

⁸ See Government Code section 65915(a).

⁹ San Francisco Planning Department, *Eligibility Checklist: CEQA Section 21099 – Modernization of Transportation Analysis for 351 12th Street*, June 21, 2019. This document (and all other documents cited in this report, unless otherwise noted), is available for review on the San Francisco Property Information Map, which can be accessed at <https://sfplanninggis.org/PIM/>. Individual files can be viewed by clicking on the Planning Applications link, clicking the “More Details” link under the project’s environmental record number 2018-004545ENV and then clicking on the “Related Documents” link is available for review at the San Francisco Planning Department, 1650 Mission Street, Suite 400 as part of Case File No. 2018-004545ENV.

would include more clearly defined residential, commercial, and industrial areas. No land use mitigation measures were identified in the PEIR. Subsequent CEQA case law since certification of the Western SoMa PEIR has clarified that “community character” itself is not a physical environmental effect.¹⁰ Therefore, consistent with Appendix G of the CEQA Guidelines, analysis concerning land use character has been removed from further evaluation in this project-specific initial study.

Project Analysis

<i>Topics:</i>	<i>Significant Impact Peculiar to Project or Project Site</i>	<i>Significant Impact not Identified in PEIR</i>	<i>Significant Impact due to Substantial New Information</i>	<i>No Significant Impact not Previously Identified in PEIR</i>
Would the project:				
a) Physically divide an established community?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Cause a significant physical environmental impact due to a conflict with any land use plan, policy, or regulation adopted for the purpose of avoiding or mitigating an environmental effect?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

E.1.a) The proposed project would not result in the construction of a physical barrier to neighborhood access or the removal of an existing means of access; it would result in the construction of a new building within established lot boundaries. The proposed project would not alter the established street grid or permanently close any streets or sidewalks. Therefore, the proposed project would not physically divide an established community.

E.1.b) The Planning Department has determined that the proposed project is consistent with the WMUG Zoning District and the 55-X Height and Bulk District and is therefore consistent with the development density principally permitted for the project site under the planning code and zoning map provision. Although the proposed project’s height would be above that of the existing height and bulk district guidelines, CEQA Guidelines section 15183 discusses projects consistent with a community plan or zoning and does not place a cap on development within an area plan. Because the proposed project is located within the Plan area and the individually applied state density bonus program was law when the area plan was enacted, the additional density and height of the proposed building is considered consistent with the Plan.

Furthermore, the proposed project complies with city and state regulations and therefore would not cause a significant physical environmental impact due to a conflict with applicable land use plans, policies, or regulations adopted for the purpose of avoiding or mitigating an environmental effect.

Cumulative Analysis

The proposed project would have no impact with respect to physically dividing a community or causing a significant physical environmental impact due to a conflict with an applicable land use plan or regulation and, therefore, would not have the potential to contribute to a significant cumulative impact related to land use or land use planning.

¹⁰ Preserve Poway v. City of Poway, 245 Cal.App.4th 560.

Conclusion

The proposed project would not result in a significant project-level or cumulative land use impact. Therefore, the proposed project would not result in significant physical environmental land use impacts not already disclosed in the Western SoMa PEIR.

E.2 POPULATION AND HOUSING

Western SoMa PEIR Population and Housing Findings

The Western SoMa PEIR concluded that an increase in population in the plan area is expected to occur as a secondary effect of the rezoning and that any population increase would not, in itself, result in adverse physical effects but would serve to advance key City policy objectives, such as providing housing in appropriate locations next to Downtown and other employment generators and furthering the City's Transit First policies. It was anticipated that the rezoning would result in an increase in both housing development and population throughout the Plan area. The Western SoMa PEIR determined that the anticipated increase in population and density would not result in significant adverse physical effects on the environment. No mitigation measures related to population and housing were identified in the Western SoMa PEIR.

Project Analysis

<i>Topics:</i>	<i>Significant Impact Peculiar to Project or Project Site</i>	<i>Significant Impact not Identified in PEIR</i>	<i>Significant Impact due to Substantial New Information</i>	<i>No Significant Impact not Previously Identified in PEIR</i>
Would the project:				
a) Induce substantial unplanned population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of roads or other infrastructure)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Displace substantial numbers of existing people or housing units necessitating the construction of replacement housing?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

E.2.a) The proposed project would contain 48 group-housing units (9,844 gross square feet of residential use). Each group housing unit ~~would~~ could accommodate up to one two residents, thus at full occupancy, the proposed project would add ~~48~~ up to 96 additional residents to the neighborhood. Though the proposed project would bring new residents to the area, this would not constitute a substantial population increase. Furthermore, these direct effects of the proposed project on population and housing are within the scope of the population growth anticipated under the Plan, and were evaluated in the Western SoMa PEIR.

The Association of Bay Area Governments (ABAG) prepares projections of employment and housing growth for the Bay Area. The latest projections were prepared as part of Plan Bay Area

2040, adopted by ABAG and the Metropolitan Transportation Commission in 2017. The growth projections for San Francisco County anticipate an increase of 137,800 households and 295,700 jobs between 2010 and 2040.¹¹

The project would also be located in a developed urban area with available access to necessary infrastructure and services (transportation, utilities, schools, parks, hospitals, etc.). Since the project site is located in an established urban neighborhood and is not an infrastructure project, it would not indirectly induce substantial population growth. Therefore, the housing and employment growth generated by the project would not result in new or more severe impacts than were identified in the Western SoMa PEIR. The physical environmental impacts resulting from housing and employment growth generated by the project are evaluated in the relevant resource topics in this initial study.

E.2.b) The proposed project would not displace any residents or housing units because no housing units currently exist on the project site. The existing use of the site is a private surface parking lot. Therefore, the proposed project would have no direct impact related to the displacement of housing units or people and would not necessitate the construction of replacement housing elsewhere that could result in physical environmental effects.

Cumulative Analysis

The cumulative context for the population and housing topic is the City and County of San Francisco. The proposed project would provide housing units and commercial space that would result in increases in population (households and jobs). As discussed above, San Francisco is anticipated to grow by 137,800 households and 295,700 jobs between 2010 and 2040. Between 2010 and 2017, San Francisco's population grew by approximately 13,000 households and 137,200 jobs, leaving approximately 124,839 households and 158,486 jobs projected for San Francisco through 2040.^{12,13} As of the fourth quarter of 2018, approximately 70,960 net new housing units are in the pipeline, i.e., are either under construction, have building permits approved or filed, or applications filed, including remaining phases of major multi-phased projects.¹⁴ The pipeline also includes projects with land uses that would result in an estimated 94,600 new employees.^{15,16} As such, cumulative household and employment growth is below the ABAG projections for planned growth in San Francisco. Therefore, the proposed project in combination with citywide development would not result in significant cumulative environmental effects associated with

¹¹ Metropolitan Transportation Commission and Association of Bay Area Government, *Plan Bay Area 2010 Final Supplemental Report: Land Use and Modeling Report*. July 2017. This document is available online at: <http://2040.planbayarea.org/reports>. Accessed November 7, 2018.

¹² U.S. Census Bureau, American Fact Finder, 2010 Demographic Profile Data and 2010 Business Patterns, San Francisco County. Available online at: <https://factfinder.census.gov/faces/nav/jsf/pages/programs.xhtml?program=dec>. Accessed April 10, 2019.

¹³ U.S. Census Bureau, Quick Facts, San Francisco County, California, Population Estimates July 1, 2017 and Households 2013-2017. Available online at: <https://www.census.gov/quickfacts/sanfranciscocountycalifornia>. Accessed April 10, 2019.

¹⁴ San Francisco Planning Department, 2018 Q4. Housing Development Pipeline. Available online at: <https://sfplanning.org/project/pipeline-report>. Accessed April 10, 2019.

¹⁵ Ibid.

¹⁶ San Francisco Planning Department, Citywide Division, Information and Analysis Group, Scott Edmundson, March 19, 2019.

inducing unplanned population growth or displacing substantial numbers of people or housing, necessitating the construction of replacement housing elsewhere.

Conclusion

The proposed project would contribute a small portion of the growth anticipated within the Plan area under the Plan as well as for San Francisco as a whole under Plan Bay Area. The project's incremental contribution to this anticipated growth would not result in a significant individual or cumulative impact related to population and housing. Therefore, the proposed project would not result in significant physical environmental impacts related to population and housing that were not identified in the Western SoMa PEIR.

E.3 CULTURAL RESOURCES

Western SoMa PEIR Cultural Findings

The Western SoMa PEIR identified significant and unavoidable impacts related to causing a substantial adverse change in the significance of a historic resource through demolition. The PEIR determined that future development facilitated through the changes in use districts and height limits under the Western SoMa Area Plan could have substantial adverse changes on the significance of both individual historical resources and on historical districts within the plan areas. The PEIR also anticipated that project-specific construction activity could result in substantial damage to adjacent properties identified as historic resources.

The Western SoMa PEIR also determined that implementation of the Western SoMa Area Plan could result in significant impacts on archeological resources and identified two mitigation measures that would reduce these potential impacts to a less than-significant level. Western SoMa PEIR Mitigation Measure M-CP-4a: Project-Specific Preliminary Archeological Assessment and M-CP-4b: Procedures for Accidental Discovery of Archeological Resources apply to projects involving any soils-disturbing or soils-improving activities including excavation to a depth of 5 or more feet below grade.

Project Analysis

<i>Topics:</i>	<i>Significant Impact Peculiar to Project or Project Site</i>	<i>Significant Impact not Identified in PEIR</i>	<i>Significant Impact due to Substantial New Information</i>	<i>No Significant Impact not Previously Identified in PEIR</i>
Would the project:				
a) Cause a substantial adverse change in the significance of a historical resource pursuant to §15064.5, including those resources listed in article 10 or article 11 of the San Francisco <i>Planning Code</i> ?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Cause a substantial adverse change in the significance of an archaeological resource pursuant to §15064.5?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Disturb any human remains, including those interred outside of formal cemeteries?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

E.3.a) Pursuant to CEQA Guidelines sections 15064.5(a)(1) and 15064.5(a)(2), historical resources are buildings or structures that are listed, or are eligible for listing, in the California Register of Historical Resources or are identified in a local register of historical resources, such as Articles 10 and 11 of the San Francisco Planning Code. The project would demolish an existing surface parking lot. The existing lot was evaluated as part of the South of Market Area Historic Resource Survey. Based on this survey, the lot was assigned a California Historic Resource Status Code of 6Z, which defines the properties as “ineligible for [National Register], [California Register], or local designation through survey evaluation.” Therefore, the existing parking lot is not considered to be a historic resource for purposes of CEQA. As such, the project would not result in the demolition or alteration of any historic resource and would not contribute to the significant historic resource impact identified in the Western SoMa PEIR, and Mitigation Measures M-CP-1a, M-CP-1b, and M-CP-1c would not apply to the proposed project. The project site is not located in a historic district. The closest historic district is the Western SoMa Light Industrial and Residential Historic District, which is located across 12th Street from the project site at 338 12th Street, approximately 75 feet away. The proposed project would not directly or indirectly impact the character defining features of the district or its setting. Therefore, the proposed project would not impact the adjacent historic district.

E.3.b) The proposed project would involve excavation to a depth of approximately 4 feet below grade to construct the proposed project’s building’s slab on grade foundation. As part of project analysis and review, the Planning Department’s archeologists conducted a preliminary archeology review of the project site and the proposed excavation.¹⁷ The Central SoMa architectural research design and treatment plan indicated that the project site has very high sensitivity for buried prehistoric resources and moderate potential for submerged (deeply buried) prehistoric resources.

Mitigation Measure M-CP-4a would not apply to the 4-foot depth of excavation for the proposed foundation, as this shallow amount of excavation would not be at a sufficient depth to likely encounter prehistoric resources. To avoid the accidental discovery of prehistoric resources during site preparation and grading, the project sponsor shall implement Mitigation Measure M-CP-4b (**Project Mitigation Measure 1**, described in Section G. Mitigation Measures below). The project would not result in significant impacts related to archeological resources with implementation of this mitigation measure.

E.3.c) Archeological resources may include human burials. Human burials outside of formal cemeteries often occur in prehistoric or historic period archeological contexts. The potential for the proposed project to affect archeological resources, which may include human burials, is addressed above under E.3.b. Furthermore, the treatment of human remains and of associated or unassociated funerary objects must comply with applicable state laws. This includes immediate notification to the county coroner (San Francisco Office of the Chief Medical Examiner) and, in the event of the coroner’s determination that the human remains are Native American,

¹⁷ San Francisco Planning Department, *Preliminary Archeological Review, 351 12th Street*, November 13, 2018.

notification of the California Native American Heritage Commission, which shall appoint a most likely descendant.¹⁸

Cumulative Analysis

As discussed above, the proposed project would have no effect on historic architectural resources and therefore would not have the potential to contribute to any cumulative historic resources impact.

The cumulative context for archeological resources and human remains is site specific and generally limited to the immediate construction area. For these reasons, the proposed project, in combination with other cumulative projects, would not result in a cumulatively considerable impact on archeological resources or human remains.

Conclusion

The proposed project would not result in significant impacts to historic resources and impacts to archeological resources would be mitigated to less-than-significant levels with implementation of mitigation measures identified in the Western SoMa PEIRs. The project sponsor has agreed to implement **Project Mitigation Measure 1**. Therefore, the proposed project would not result in significant impacts on cultural resources that were not identified in the Western SoMa PEIR.

E.4 TRANSPORTATION AND CIRCULATION

Western SoMa PEIR Transportation and Circulation Findings

The Western SoMa PEIR anticipated that growth resulting from the zoning changes would not result in significant impacts related to pedestrians, bicyclists, emergency access, or construction. Transportation system improvements included as part of the Western SoMa Area Plan were identified to have significant impacts related to loading, but the impacts were reduced to less-than-significant levels with mitigation.

The Western SoMa PEIR anticipated that adoption of the Western SoMa Area Plan could result in significant impacts on loading and identified two transportation mitigation measures. M-TR-4 would reduce loading impacts along Folsom Street to a less-than-significant level. Even with mitigation, however, it was anticipated that the significant cumulative impacts on transit lines could not be fully mitigated. Thus, these impacts were found to be significant and unavoidable.

The Western SoMa PEIR did not evaluate vehicle miles traveled (VMT) or the potential for induced automobile travel; however, the analysis below evaluates the project's transportation effects using the VMT metric. This initial study reflects two changes because of state and local actions. The state amended CEQA to remove automobile delay as a consideration (CEQA section 21099(b)(2)). In March 2016, Planning Commission resolution 19579 implemented this state-level change in San Francisco. In February 2019, the department updated its Transportation Impact Analysis Guidelines (2019 guidelines). With that update, the department deleted the transit capacity criterion. The deletion is consistent with state guidance about the environmental benefits of new transit riders and to reflect funding sources for and policies that encourage additional

¹⁸ California Public Resources Code section 5097.98

ridership.¹⁹ Accordingly, this initial study does not evaluate the project's impact on automobile delay or transit capacity.

The Western SoMa PEIR anticipated that growth resulting from the zoning changes could result in significant cumulative impacts on transit ridership, and identified a transportation mitigation measure, M-C-TR-2: Impose Development Impact Fees to Offset Transit Impacts. Even with mitigation, however, it was anticipated that the significant adverse cumulative impact on transit lines could not be fully mitigated. Thus, this impact was found to be significant and unavoidable.

Project Analysis

Topics:	Significant Impact Peculiar to Project or Project Site	Significant Impact not Identified in PEIR	Significant Impact due to Substantial New Information	No Significant Impact not Previously Identified in PEIR
Would the project:				
a) Conflict with a program, plan, ordinance or policy addressing the circulation system, including transit, roadway, bicycle and pedestrian facilities?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Conflict or be inconsistent with CEQA Guidelines section 15064.3, subdivision (b)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Substantially increase hazards due to a geometric design feature (e.g., sharp curves or dangerous intersections) or incompatible uses?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Result in inadequate emergency access?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

E.4.a to d) The department estimated the number of trips and ways people would travel to and from the site. The department estimated these trips using data and methodology in the department's 2019 guidelines.²⁰ Table 1 presents daily estimates. Table 2 presents p.m. peak hour estimates.

Table 1: Person and Vehicle Trip Estimates – Daily

Land Use	Daily Person Trips						Daily Vehicle Trips ¹
	Automobile	For-Hire	Transit	Walking	Bicycling	Total	
Residential	54	14	61	81	6	216	62
Project Total	54	14	61	81	6	216	62
1. Automobile person trips, accounting for average vehicle occupancy data. Source: San Francisco Planning Department, Transportation Impact Analysis Guidelines.							

¹⁹ San Francisco Planning Department, "Transportation Impact Analysis Guidelines Update: Summary of Changes Memorandum", February 14, 2019.

²⁰ San Francisco Planning Department, *Transportation Calculations for 351 12th Street*, June 21, 2019.

Table 2: Person and Vehicle Trip Estimates – P.M. Peak Hour

Land Use	P.M. Peak Hour Person Trips						P.M Peak Hour Vehicle Trips ¹
	Automobile	For-Hire	Transit	Walking	Bicycling	Total	
Residential	4	1	5	6	1	16	5
Project Total	4	1	5	6	1	16	5
1. Automobile person trips, accounting for average vehicle occupancy data. Source: San Francisco Planning Department, Transportation Impact Analysis Guidelines.							

The department used these estimates to inform the analysis of the project's impacts on transportation and circulation during both construction and operation. Due to low P.M. peak hour person trips, the proposal did not require a consultant-prepared transportation study. The following considers effects on potentially hazardous conditions, accessibility (including emergency access), public transit delay, vehicle miles traveled, and loading.

Construction

The 2019 guidelines set forth screening criteria for types of construction activities that would typically not result in significant construction-related transportation effects. Project construction would last approximately 12 months. During construction, the project could require temporary closures of the public right-of-way. These closures may include the sidewalk along 12th Street. Given the project site context and construction duration and magnitude, the project meets the screening criteria.

Further, the project would be subject to the San Francisco Regulations for Working in San Francisco Streets (the blue book). The blue book is prepared and regularly updated by the San Francisco Municipal Transportation Agency, under the authority derived from the San Francisco Transportation Code. It serves as a guide for contractors working in San Francisco streets. The blue book establishes rules and guidance so that construction work can be done safely and with the least possible interference with pedestrians, bicycle, transit and vehicular traffic. Therefore, the project would have a less-than-significant construction-related transportation impact.

Potentially Hazardous Conditions and Accessibility

The project would remove an existing surface parking lot with 22 vehicle parking spaces and construct a six-story, 48-unit group housing building. The existing site has a 50-foot-wide curb cut, which is proposed to be removed. As shown in Figure 3 in Appendix B, the proposal includes requesting the SFMTA and public works to install a 30-foot minimum white curb loading zone for passengers, a 3-foot red curb buffer zone, a minimum 5-foot Americans-with-Disabilities-Act-compliant curb ramp and an approximately 5-foot-9-inch red curb buffer zone. The project would add an estimated five p.m. peak hour vehicle trips. These vehicle trips would likely start from or end at project's 12th Street frontage or convenient loading zones and be dispersed along nearby streets. As the project does not propose any vehicle parking or driveways, this number of vehicles trips that would not be substantial and none of the trips would cross bike lanes or any pedestrian right of ways. No bike routes exist along 12th street. Therefore, the project would have less-than-significant potentially hazardous conditions and accessibility impacts.

Public Transit Delay

The 2019 guidelines set forth a screening criterion for projects that would typically not result in significant public transit delay effects. The project would add 5 inbound p.m. peak hour vehicle trips, which is less than the screening criterion of 300. Therefore, the project meets the screening criterion and the project would have a less-than-significant public transit delay impact.

Vehicle Miles Traveled

The 2019 guidelines set forth screening criteria for types of projects that would typically not result in significant vehicle miles traveled impacts. The project site is an area where existing vehicle miles traveled per capita is more than 15 percent below the existing regional per capita average. The project meets this locational screening criterion and the project would have a less-than-significant vehicle miles traveled impact.

The project also meets the proximity to transit screening criterion. The project site is within one-half mile of an existing major transit stop or an existing stop along a high-quality transit corridor and the project meets other characteristic requirements. This screening criterion also indicates the project's uses would not cause substantial additional VMT.

Loading

During the average and peak period, the proposed project's freight and delivery loading demand is 0.02 trips. The project would not provide any on-street or off-street freight and delivery loading facilities. At the adjacent 333 12th Street project, a 40-foot-long yellow curb for freight loading has been requested to be installed by the SFMTA. That loading space would be approximately 100 feet from the project site on the same side of the street, which would meet the demand.

During the peak period, the project's passenger loading demand is 0.03 spaces. The project would request the SFMTA to install a minimum 30-foot-long, on-street white loading zone for passenger loading, as discussed above. Thus, the project would meet the demand. Overall, the project would have a less-than-significant loading impact.

Cumulative Analysis

Construction

All projects currently under construction on the project block are anticipated to complete construction before the proposed project would break ground. The 333 12th Street project is anticipated to complete construction in May 2020 and 1532 Harrison Street is anticipated to complete construction in August 2020. Eagle Plaza is anticipated to complete construction by May 2020. No reasonably foreseeable cumulative projects are currently proposed within the same block of the project site, thus there would be no cumulative ~~loading-construction~~ impacts. Therefore, the project would not result in a significant cumulative construction-related transportation impact.

Potentially Hazardous Conditions and Accessibility

The PEIR disclosed that vehicular and other ways of travel (e.g., walking, bicycling) volumes would increase in the Western SoMa because of the plan and other cumulative projects. This volume increase would result in a potential for more conflicts between various ways of travel. The vehicle trips from these cumulative projects would not combine to result in a potentially

hazardous condition at any nearby vehicular turning movement. These cumulative projects would also not block access to a substantial number of people walking within the sidewalk. The proposed project would add additional pedestrians and bicycles to the neighborhood. The currently under construction Eagle Plaza would support traffic calming along 12th Street by narrowing the right of way for vehicles between Harrison and Bernice streets to a single 14-foot-wide “slow lane” of traffic, allowing one lane of vehicles traveling south on 12th Street toward Harrison Street. Additional vehicle trips would be generated by the projects adjacent to the proposed project at 333 12th Street and 1532 Harrison Street. In total, the adjacent projects and the 351 12th Street project would add up to about 1,300 new residents to the area. The 1532 Harrison Street building would have 85 parking spaces in a garage accessed off of Norfolk Street and a passenger loading zone along Harrison Street. The 333 12th Street project would include no vehicle parking and a 40-foot-long passenger loading zone along the 12th Street frontage. Of the projects under construction or proposed on the project block, all would decrease pedestrian/vehicle conflicts by removing the ability of vehicles to cross the sidewalk. All existing curb cuts for vehicular use at 333 12th Street, 351 12th Street, and 1532 Harrison Street have been or are proposed to be removed.

Therefore, the project, in combination with cumulative projects, would not result in significant cumulative potentially hazardous conditions or accessibility impacts.

Public Transit Delay

Western SoMa Mitigation Measure M-C-TR-2: Impose Development Impact Fees to Offset Transit Impacts was adopted to address significant transit impacts. Subsequently, as part of the Transportation Sustainability Program, the San Francisco Board of Supervisors approved amendments to the San Francisco Planning Code, referred to as the Transportation Sustainability Fee (Ordinance 200-154, effective December 25, 2015).²¹ The Transportation Sustainability Fee updated, expanded, and replaced the prior Transit Impact Development Fee.

The SFMTA is implementing Muni Forward, formerly known as the Transit Effectiveness Project, which was approved by the SFMTA Board of Directors in March 2014. Muni Forward includes system-wide review, evaluation, and recommendations to improve service and increase transportation efficiency. Service improvements in the Western SoMa plan area include travel time reduction measure for the 14 Mission and 14 Mission Rapid route and the addition of the 49 Van Ness/Mission Rapid service.

The project would also add five p.m. peak hour transit trips. These trips would be dispersed along Folsom, Harrison, and Division streets among Muni bus routes 9 San Bruno, 9R San Bruno, 12 Pacific/Folsom, and 47 Van Ness. Additionally, the project would add five p.m. peak hour vehicle trips to and from the project site. This minimal increase in transit and vehicle trips would not contribute considerably to cumulative transit delay through increased transit ridership or vehicle traffic. Cumulative projects would also improve public transit, including the Muni Forward Van Ness Bus Rapid Transit network, which would increase effectiveness of the 47 Van

²¹ Two additional files were created at the Board of Supervisors for TSF regarding hospitals and health services, grandfathering, and additional fees for larger projects: see Board file nos. 151121 and 151257.

Ness route. Therefore, the proposed project would not result in new or more severe transit delay impacts than were identified in the Western SoMa PEIR.

Vehicle Miles Traveled

VMT by its nature is largely a cumulative impact. As described above, the project would not exceed the project-level quantitative thresholds of significance for VMT. Furthermore, the project site is an area where projected year 2040 vehicle miles traveled per capita is more than 15 percent below the future regional per capita average. Therefore, the project, in combination with cumulative projects, would not result in a significant cumulative vehicle miles traveled impact.

Loading

None of the above-listed cumulative projects would overlap with the project's loading demand. Given that cumulative projects would not result in a loading deficit, the project, in combination with cumulative projects, would not result in a significant cumulative loading impact.

Conclusion

The Western SoMa PEIR projected substantial increases in public transit delay. The proposed project would not result in new or more severe transportation and circulation impacts than were identified in the Western SoMa PEIR.

E.5 NOISE

Western SoMa PEIR Noise Findings

The Western SoMa PEIR determined that implementation of the Western SoMa Area Plan would result in significant noise impacts during construction activities and due to conflicts between noise-sensitive uses in proximity to traffic-generated noise levels along major streets throughout the plan area. The Western SoMa PEIR identified six noise mitigation measures, three of which may be applicable to subsequent development projects.²² These mitigation measures would reduce noise impacts from construction and noisy land uses to less-than-significant levels.

²² Western SoMa PEIR Mitigation Measures M-NO-1a, M-NO-1b, and M-NO-1d address the siting of sensitive land uses in noisy environments. In a decision issued on December 17, 2015, the California Supreme Court held that CEQA does not generally require an agency to consider the effects of the existing environmental conditions on a proposed project's future users or residents except where a project or its residents may exacerbate existing environmental hazards (*California Building Industry Association v. Bay Area Air Quality Management District*, December 17, 2015, Case No. S213478. Available at <<http://www.courts.ca.gov/opinions/documents/S213478.PDF>>). As noted above, the Western SoMa PEIR determined the incremental increase in traffic-related noise attributable to implementation of the Western SoMa Area Plan would be less than significant, and thus would not exacerbate the existing noise environment. Therefore, Western SoMa PEIR Mitigation Measure M-NO-1a, M-NO-1b, and M-NO-1d are not applicable. Nonetheless, for all noise sensitive uses, the general requirements for adequate interior noise levels of Mitigation Measures M-NO-1a and M-NO-1b are met by compliance with the acoustical standards required under the California Building Standards Code (California Code of Regulations Title 24).

Project Analysis

Topics:	Significant Impact Peculiar to Project or Project Site	Significant Impact not Identified in PEIR	Significant Impact due to Substantial New Information	No Significant Impact not Previously Identified in PEIR
Would the project:				
a) Generate substantial temporary or permanent increase in ambient noise levels in the vicinity of the project in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Generate excessive groundborne vibration or groundborne noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) For a project located within the vicinity of a private airstrip or an airport land use plan area, or, where such a plan has not been adopted, in an area within two miles of a public airport or public use airport, would the project expose people residing or working in the area to excessive noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

E.5.a) Increases in ambient noise levels could result from increases in traffic and/or noise-generating equipment or activities. A potentially significant increase in the ambient noise level due to traffic resulting from a proposed project is unlikely unless the project would cause a doubling of existing traffic levels, which is generally assumed to result in a 3 dBA increase in the existing ambient noise environment.²³ An increase of less than 3 dBA is generally not perceptible outside of controlled laboratory conditions.²⁴ The proposed project would generate 62 daily vehicle trips. These vehicle trips would be dispersed along the local roadway network and would not result in a doubling of vehicle trips on roadways in the vicinity of the project site. Therefore, traffic noise impacts resulting from the project would be less than significant.

Western SoMa PEIR Mitigation Measure M-NO-1c: Siting of Noise-Generating Uses requires a noise analysis for new development including commercial, industrial, or other uses that would be expected to generate noise levels in excess of ambient noise in the project vicinity in order to reduce potential conflicts between existing sensitive receptors and new noise-generating uses. The proposed residential development would not include uses that would be expected to generate noise levels in excess of ambient noise in the project vicinity. Therefore, Mitigation Measure M-NO-1c would not apply to the proposed project.

Mitigation Measures M-NO-2a General Construction Noise Control measures and M-NO-2b: Noise Control Measures during Pile Driving require implementation of noise controls during construction in order to reduce construction-related noise impacts. The proposed project would involve construction of a six-story residential building and, therefore would contribute to construction-related noise impacts. The project would be subject to PEIR Mitigation Measure M-NO-2a —detailed under **Project Mitigation Measure 2** in Section G. Mitigation Measures below —

²³ Caltrans, *Technical Noise Supplement*, November 2009. Available at: <http://www.dot.ca.gov/env/noise/docs/tens-sep2013.pdf>. Accessed: December 18, 2017.

²⁴ California Department of Transportation, *Technical Noise Supplement to the Traffic Noise Analysis Protocol*, pp. 2-44 to 2-45, September 2013. Available: http://www.dot.ca.gov/hq/env/noise/pub/TeNS_Sept_2013B.pdf. Accessed July 30, 2017.

in order to reduce these impacts to a less-than-significant level. Project construction would not require pile driving; the proposed foundation is slab on grade with the potential for drilled piers. Thus, Mitigation Measure M-NO-2b would not apply to the proposed project.

In addition, all construction activities for the proposed project (approximately 12 months) would be subject to the San Francisco Noise Ordinance (Article 29 of the San Francisco Police Code). The San Francisco Department of Building Inspection is responsible for enforcing the noise ordinance for private construction projects during normal business hours (8 a.m. to 5 p.m.). The police department is responsible for enforcing the noise ordinance during all other hours. With implementation of Project Mitigation Measure M-NO-2a, the proposed project would not result in significant construction noise or vibration impacts.

E.5.b) The proposed project would not include pile driving and therefore would not have the potential for vibration impacts to nearby buildings. Development projects, such as the proposed project, are not typically sources of operational vibration. Therefore, the proposed project would not result in significant impacts related to vibration.

E.5.c) The project site is not located within an airport land use plan area, within 2 miles of a public airport, or in the vicinity of a private airstrip. Therefore, initial study checklist question E.5.c is not applicable to the proposed project.

Cumulative Analysis

The cumulative context for traffic noise analyses are typically confined to the local roadways nearest the project site. As project generated vehicle trips disperse along the local roadway network, the contribution of traffic noise along any given roadway segment would be reduced. As discussed in initial study checklist question E.5.a, the proposed project would not result in a perceptible increase in traffic noise. Therefore, the proposed project would not result in a considerable contribution to ambient noise levels from project traffic.

The cumulative context for point sources of noise, such as building heating, ventilation and air conditioning systems and construction noise are typically confined to nearby noise sources, usually not further than about 900 feet from the project site.²⁵ The cumulative projects within 900 feet of the project site are: 1695 Folsom Street, 1675 Howard Street, 134 Kissling St, 1394 Harrison Street, 1560 Folsom Street, and 1233 Folsom Street. These projects are also required to comply with the Noise Ordinance, which establishes noise limits from stationary sources and construction equipment. As a result no significant cumulative noise impact would occur.

Conclusion

The Western SoMa PEIR determined that implementation of the Western SoMa Area Plan would result in significant noise impacts during construction activities and due to conflicts between noise-sensitive uses in proximity to traffic-generated noise levels along major streets throughout the plan area. The proposed project would implement mitigation measures identified in the Western SoMa PEIR to reduce construction noise, referred to as **Project Mitigation Measures 2**. With implementation of this mitigation measure identified in the PEIR, the proposed project

²⁵ This distance was selected because typical construction noise levels can affect a sensitive receptor at a distance of 900 feet if there is a direct line-of-sight between a noise source and a noise receptor (i.e., a piece of equipment generating 85 dBA would attenuate to 60 dBA over a distance of 900 feet). An exterior noise level of 60 dBA will typically attenuate to an interior noise level of 35 dBA with the windows closed and 45 dBA with the windows open.

would not result in new or more severe noise impacts than were identified in the Western SoMa PEIR.

E.6 AIR QUALITY

Western SoMa PEIR Air Quality Findings

The Western SoMa PEIR identified significant and unavoidable impacts related to violation of an air quality standard, uses that emit diesel particulate matter (DPM), and construction emissions. The Western SoMa PEIR identified five mitigation measures that would help reduce air quality impacts; however, they would not be able to reduce these impacts to a less-than-significant level and these impacts would remain significant and unavoidable.

Project Analysis

<i>Topics:</i>	<i>Significant Impact Peculiar to Project or Project Site</i>	<i>Significant Impact not Identified in PEIR</i>	<i>Significant Impact due to Substantial New Information</i>	<i>No Significant Impact not Previously Identified in PEIR</i>
Would the project:				
a) Conflict with or obstruct implementation of the applicable air quality plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal, state, or regional ambient air quality standard?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Expose sensitive receptors to substantial pollutant concentrations?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Result in other emissions (such as those leading to odors) adversely affecting a substantial number of people?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

E.6.a) The most recently adopted air quality plan for the air basin is the Bay Area Air Quality Management District's 2017 Clean Air Plan. The primary goals of the clean air plan are to: (1) protect air quality and health at the regional and local scale; (2) eliminate disparities among Bay Area communities in cancer health risk from toxic air contaminants; and (3) reduce greenhouse gas emissions. The clean air plan recognizes that to a great extent, community design dictates individual travel mode, and that a key long-term control strategy to reduce emissions of criteria pollutants, air toxics, and greenhouse gases from motor vehicles is to channel future Bay Area growth into vibrant urban communities where goods and services are close at hand, and people have a range of viable transportation options. The compact development of the proposed project and the availability of non-auto transportation options in the project area would ensure that the project would avoid substantial growth in automobile trips and consequent air pollutant emissions. In addition, as discussed above in the Population and Housing resource topic, the project site is located within the Western SoMa priority development area. Channeling development within such areas is a key land use strategy under Plan Bay Area to meet statewide greenhouse gas reduction goals pursuant to Senate Bill 375. Furthermore, for the reasons described below under topics E.6.b through d, the proposed project would not result in

significant air pollutant emissions or expose sensitive receptors to substantial pollutant concentrations. Therefore, the proposed project would not obstruct implementation of the 2017 Clean Air Plan.

E.6.b) The Western SoMa PEIR determined that at a program-level the Western SoMa Area Plan would result in significant regional air quality impacts. Additionally, an individual development project is subject to a significance determination based on the air district's quantitative threshold for individual projects.

In accordance with the state and federal Clean Air Acts, air pollutant standards are identified for the following six criteria air pollutants: ozone, carbon monoxide (CO), particulate matter (PM_{2.5}, and PM₁₀²⁶), nitrogen dioxide (NO₂), sulfur dioxide (SO₂), and lead. These air pollutants are termed criteria air pollutants because they are regulated by developing specific public health- and welfare-based criteria as the basis for setting permissible levels. The Bay Area air basin is designated as either in attainment or unclassified for most criteria pollutants except for ozone, PM_{2.5}, and PM₁₀. For these pollutants, the air basin is designated as non-attainment for either the state or federal standards. By its very nature, regional air pollution is largely a cumulative impact in that no single project is sufficient in size to, by itself, result in non-attainment of air quality standards. Instead, a project's individual emissions contribute to existing cumulative air quality impacts. If a project's contribution to cumulative air quality impacts is considerable, then the project's impact on air quality would be considered significant.²⁷ Regional criteria air pollutant impacts resulting from the proposed project are evaluated below.

Construction Dust Control

To reduce construction dust impacts, the San Francisco Board of Supervisors approved a series of amendments to the San Francisco Building and Health Codes, generally referred to as the Construction Dust Control Ordinance (Ordinance 176-08, effective July 30, 2008). The intent of the construction dust control ordinance is to reduce the quantity of fugitive dust generated during site preparation, demolition, and construction work to protect the health of the general public and of construction workers, minimize public nuisance complaints, and to avoid orders to stop work in response to dust complaints. Project-related construction activities would result in construction dust, primarily from ground-disturbing activities. The proposed project would disturb less than a half-acre of land area and is not required to submit a Dust Control Plan to the public health department; however, the proposed project would still be required to comply with the construction dust control measures. In compliance with the dust control ordinance, the project sponsor and contractor responsible for construction activities at the project site would be required to control construction dust on the site through a combination of watering disturbed areas, covering stockpiled materials, street and sidewalk sweeping, and other measures.

The regulations and procedures set forth by the San Francisco Dust Control Ordinance would ensure that construction dust impacts would not be significant. Therefore, compliance with the dust control ordinance would ensure that the proposed project would not result in substantial amounts of fugitive dust, including particulate matter, during construction activities.

²⁶ PM₁₀ is often termed "coarse" particulate matter and is made of particulates that are 10 microns in diameter or smaller. PM_{2.5}, termed "fine" particulate matter, is composed of particles that are 2.5 microns or less in diameter.

²⁷ Bay Area Air Quality Management District (BAAQMD), *California Environmental Quality Act Air Quality Guidelines*, May 2017, page 2-1.

Criteria Air Pollutants

The Bay Area Air Quality Management District prepared updated 2017 BAAQMD CEQA Air Quality Guidelines,²⁸ which provide methodologies for analyzing air quality impacts. These guidelines also provide thresholds of significance for non-attainment criteria air pollutants (ozone and particulate matter). The planning department uses these thresholds to evaluate air quality impacts under CEQA.

The air district has developed screening criteria to determine whether to undertake detailed analysis of criteria pollutant emissions for construction and operations of development projects. Projects that are below the screening criteria would result in less-than-significant criteria air pollutant impacts, and no further project-specific analysis is required. The proposed project would construct 48 group housing units, which is well below the low-rise apartment screening criteria of 451 dwelling units.²⁹ Therefore, because the proposed project is below the construction and operational screening levels for criteria air pollutants, the proposed project would not result in a significant impact with regards to violating an air quality standard or resulting in a cumulatively considerable net increase in criteria air pollutants.

E.6.c) In addition to regional criteria air pollutants analyzed above, the following air quality analysis evaluates localized health risks to determine whether sensitive receptors would be exposed to substantial pollutant concentrations. Since certification of the PEIR, the San Francisco Board of Supervisors approved amendments to the San Francisco Building and Health Codes, referred to as Enhanced Ventilation Required for Urban Infill Sensitive Use Developments or Health Code, Article 38 (Ordinance 224-14, amended December 8, 2014). The purpose of Article 38 is to protect the public health and welfare by establishing an Air Pollutant Exposure Zone and imposing an enhanced ventilation requirement for all new sensitive uses within this zone. The Air Pollutant Exposure Zone as defined in Article 38 includes areas that exceed health protective standards for cumulative PM_{2.5} concentration and cumulative excess cancer risk, and incorporates health vulnerability factors and proximity to freeways. Projects within the Air Pollutant Exposure Zone require special consideration to determine whether the project's activities would expose sensitive receptors to substantial air pollutant concentrations or add emissions to areas already adversely affected by poor air quality.

Projects located within the Air Pollutant Exposure Zone, such as the proposed project, must provide filtration to protect occupants from PM_{2.5} (fine particulate matter). Health Code Article 38 requires that the project sponsor submit an Enhanced Ventilation Proposal for approval by the Department of Public Health (health department) that achieves protection from PM_{2.5} (fine particulate matter) equivalent to that associated with a Minimum Efficiency Reporting Value 13 filtration. The building department will not issue a building permit without written notification from the Director of Public Health that the applicant has an approved Enhanced Ventilation Proposal. In compliance Article 38, the project sponsor has submitted an initial application to the health department.³⁰

²⁸ Bay Area Air Quality Management District, CEQA Air Quality Guidelines, updated May 2017.

²⁹ Bay Area Air Quality Management District, CEQA Air Quality Guidelines, updated May 2017. Table 3-1.

³⁰ Zac Shore, Panoramic Interests, *Application for Article 38 Compliance Assessment*, July 17, 2018.

Construction Health Risk

The project site is located within an identified Air Pollutant Exposure Zone; therefore, the ambient health risk to sensitive receptors from air pollutants is considered substantial. The proposed project would require heavy-duty off-road diesel vehicles and equipment during 6 months of the anticipated 12-month construction period. Thus, Project Mitigation Measure M-AQ-7, Construction Emissions Minimization Plan for Health Risks and Hazards would be required to minimize exhaust emissions of particulates and other pollutants. The project sponsor would be required to submit a Construction Emissions Minimization Plan to the Environmental Review Officer for review and approval. This measure would reduce diesel particulate matter exhaust from construction equipment by 89 to 94 percent compared to uncontrolled construction equipment.³¹ Therefore, impacts related to construction health risks would be less than significant through implementation of **Project Mitigation Measure 3** Construction Air Quality, described below in Section G. Mitigation Measures.

Operational Health Risks

The proposed project would not be expected to generate 100 trucks per day or 40 refrigerated trucks per day. Therefore, Western SoMa PEIR Mitigation Measure M-AQ-4: Siting of Uses that Emit PM_{2.5} or DPM and Other TACs is not applicable. The project's incremental increase in localized TAC emissions resulting from new vehicle trips would be minor and would not contribute substantially to localized health risks.

E.6.d) Typical odor sources of concern include wastewater treatment plants, sanitary landfills, transfer stations, composting facilities, petroleum refineries, asphalt batch plants, chemical manufacturing facilities, fiberglass manufacturing facilities, auto body shops, rendering plants, and coffee roasting facilities. During construction, diesel exhaust from construction equipment would generate some odors. However, construction-related odors would be temporary and would not persist upon project completion. The proposed project includes solely residential uses that would not be expected to create significant sources of new odors. Therefore, odor impacts would be less than significant.

Cumulative Analysis

As discussed above, regional air pollution is by its nature a cumulative impact. Emissions from past, present, and future projects contribute to the region's adverse air quality on a cumulative basis. No single project by itself would be sufficient in size to result in regional nonattainment of ambient air quality standards. Instead, a project's individual emissions contribute to existing

³¹ PM emissions benefits are estimated by comparing off-road PM emission standards for Tier 2 with Tier 1 and 0. Tier 0 off-road engines do not have PM emission standards, but the U.S. Environmental Protection Agency's *Exhaust and Crankcase Emissions Factors for Nonroad Engine Modeling – Compression Ignition* has estimated Tier 0 engines between 50 hp and 100 hp to have a PM emission factor of 0.72 g/hp-hr and greater than 100 hp to have a PM emission factor of 0.40 g/hp-hr. Therefore, requiring off-road equipment to have at least a Tier 2 engine would result in between a 25 percent and 63 percent reduction in PM emissions, as compared to off-road equipment with Tier 0 or Tier 1 engines. The 25 percent reduction comes from comparing the PM emission standards for off-road engines between 25 hp and 50 hp for Tier 2 (0.45 g/bhp-hr) and Tier 1 (0.60 g/bhp-hr). The 63 percent reduction comes from comparing the PM emission standards for off-road engines above 175 hp for Tier 2 (0.15 g/bhp-hr) and Tier 0 (0.40 g/bhp-hr). In addition to the Tier 2 requirement, ARB Level 3 VDECSs are required and would reduce PM by an additional 85 percent. Therefore, the mitigation measure would result in between an 89 percent (0.0675 g/bhp-hr) and 94 percent (0.0225 g/bhp-hr) reduction in PM emissions, as compared to equipment with Tier 1 (0.60 g/bhp-hr) or Tier 0 engines (0.40 g/bhp-hr).

cumulative adverse air quality impacts.³² The project-level thresholds for criteria air pollutants are based on levels by which new sources are not anticipated to contribute to an air quality violation or result in a considerable net increase in criteria air pollutants. Therefore, because the proposed project's construction and operational (Topics E.6.b and c) emissions would not exceed the project-level thresholds for criteria air pollutants, the proposed project would not be considered to result in a cumulatively considerable contribution to regional air quality impacts.

As discussed above, the project site is located in an area that already experiences poor air quality. The project would add diesel construction equipment and construction worker vehicle trips within an area already adversely affected by poor air quality, resulting in a considerable contribution to cumulative health risk impacts on nearby sensitive receptors. This would be a significant cumulative impact. The proposed project would be required to implement Mitigation Measure M-AQ-7, **Project Mitigation Measure 3**, which could reduce construction period emissions by as much as 94 percent. Implementation of this mitigation measure would reduce the project's contribution to cumulative localized health risk impacts to a less-than-significant level. Furthermore, compliance with Article 38 would ensure that new sensitive receptors are not substantially affected by existing or proposed sources of toxic air contaminants.

Conclusion

As described above, the proposed project would implement Project Mitigation Measure 3 and therefore not result in any significant air quality impacts, either individually or cumulatively, that were not previously identified in the PEIR.

E.7 GREENHOUSE GAS

Western SoMa PEIR Greenhouse Gas Emissions Findings

The Western SoMa PEIR analyzed greenhouse (GHG) emissions that could result from the anticipated development. The Western SoMa Area Plan and the Rezoning of Adjacent Parcels was found to be consistent with the City's GHG Reduction Strategy and the AB 32 Scoping Plan, and therefore would not result in cumulatively considerable GHG emissions. No mitigation measures were identified in the PEIR.

Project Analysis

<i>Topics:</i>	<i>Significant Impact Peculiar to Project or Project Site</i>	<i>Significant Impact not Identified in PEIR</i>	<i>Significant Impact due to Substantial New Information</i>	<i>No Significant Impact not Previously Identified in PEIR</i>
Would the project:				
a) Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Conflict with any applicable plan, policy, or regulation of an agency adopted for the purpose of reducing the emissions of greenhouse gases?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

³² BAAQMD, *CEQA Air Quality Guidelines*, May 2017, page 2-1.

E.7.a and b) The following analysis of the proposed project's GHG impact focuses on the project's contribution to cumulatively significant GHG emissions. Because no individual project could emit GHGs at a level that could result in a significant impact on global climate, this analysis is in a cumulative context only, and the analysis of this resource topic does not include a separate cumulative impact discussion.

Subsequent to adoption of the Western SoMa Area Plan, the air district updated its guidelines (see discussion in Topic E.6, Air Quality). The updated guidelines address the analysis of GHGs. These guidelines are consistent with CEQA Guidelines sections 15064.4 and 15183.5 which address the analysis and determination of significant impacts from a proposed project's GHG emissions and allow for projects that are consistent with an adopted GHG reduction strategy to conclude that the project's individual GHG impact is less than significant. San Francisco's Strategies to Address Greenhouse Gas Emissions³³ presents a comprehensive assessment of policies, programs, and ordinances that collectively represent San Francisco's GHG reduction strategy in compliance with the air district and CEQA guidelines. These GHG reduction actions resulted in a 36 percent reduction in GHG emissions in 2017 compared to 1990 levels,³⁴ exceeding the year 2020 reduction goals outlined in the air district's 2010 Clean Air Plan,³⁵ Executive Order S-3-0536, and Assembly Bill 32 (also known as the Global Warming Solutions Act).^{37,38} In addition, San Francisco's GHG reduction goals are consistent with, or more aggressive than, the long-term goals established under Executive Orders S-3-05³⁹, B-30-15,^{40,41} and Senate Bill 32.^{42,43,44}

³³ San Francisco Planning Department, *Strategies to Address Greenhouse Gas Emissions in San Francisco*, November 2010. Available at http://sfnea.sfplanning.org/GHG_Reduction_Strategy.pdf, accessed April 24, 2019.

³⁴ San Francisco Department of the Environment, *San Francisco's Carbon Footprint*. Available at <https://sfenvironment.org/carbon-footprint>, accessed April 24, 2019.

³⁵ Bay Area Air Quality Management District, *Clean Air Plan*, September 2010. Available at <http://www.baaqmd.gov/plans-and-climate/air-quality-plans/current-plans>, accessed March 3, 2016.

³⁶ Office of the Governor, *Executive Order S-3-05*, June 1, 2005. Available at <https://www.gov.ca.gov/news.php?id=1861>, accessed March 3, 2016.

³⁷ California Legislative Information, *Assembly Bill 32*, September 27, 2006. Available at http://www.leginfo.ca.gov/pub/05-06/bill_asm/ab_0001-0050/ab_32_bill_20060927_chaptered.pdf, accessed March 3, 2016.

³⁸ Executive Order S-3-05, Assembly Bill 32, and the Bay Area 2010 Clean Air Plan set a target of reducing GHG emissions to below 1990 levels by year 2020.

³⁹ Executive Order S-3-05 sets forth a series of target dates by which statewide emissions of GHGs need to be progressively reduced, as follows: by 2010, reduce GHG emissions to 2000 levels (approximately 457 million MTCO₂E); by 2020, reduce emissions to 1990 levels (approximately 427 million MTCO₂E); and by 2050 reduce emissions to 80 percent below 1990 levels (approximately 85 million MTCO₂E).

⁴⁰ Office of the Governor, *Executive Order B-30-15*, April 29, 2015. Available at <https://www.gov.ca.gov/news.php?id=18938>, accessed March 3, 2016. Executive Order B-30-15 sets a state GHG emissions reduction goal of 40 percent below 1990 levels by the year 2030.

⁴¹ San Francisco's GHG reduction goals are codified in Section 902 of the Environment Code and include: (i) by 2008, determine City GHG emissions for year 1990; (ii) by 2017, reduce GHG emissions by 25 percent below 1990 levels; (iii) by 2025, reduce GHG emissions by 40 percent below 1990 levels; and by 2050, reduce GHG emissions by 80 percent below 1990 levels.

⁴² Senate Bill 32 amends California Health and Safety Code Division 25.5 (also known as the California Global Warming Solutions Act of 2006) by adding Section 38566, which directs that statewide greenhouse gas emissions to be reduced by 40 percent below 1990 levels by 2030.

⁴³ Senate Bill 32 was paired with Assembly Bill 197, which would modify the structure of the State Air Resources Board; institute requirements for the disclosure of greenhouse gas emissions criteria pollutants, and toxic air contaminants;

Therefore, projects that are consistent with San Francisco's GHG Reduction Strategy would not result in GHG emissions that would have a significant effect on the environment and would not conflict with state, regional, and local GHG reduction plans and regulations.

The proposed project would be subject to regulations adopted to reduce GHG emissions as identified in the GHG reduction strategy as demonstrated in the GHG checklist completed for the proposed project.⁴⁵ The proposed project would comply with applicable regulations that would reduce the project's GHG emissions related to transportation, energy use, waste disposal, wood burning, and use of refrigerants. Therefore, the proposed project would not generate significant GHG emissions and would not conflict with state, regional, and local GHG reduction plans and regulations.

Conclusion

For the reasons stated above, the proposed project would not result in a significant individual or cumulative GHG impact. Therefore, the proposed project would not result in significant GHG impacts that were not identified in the Western SoMa PEIR.

E.8 WIND

Western SoMa PEIR Wind Findings

The Western SoMa PEIR determined that implementation of the Western SoMa Area Plan would have a potentially significant impact related to the alteration of wind in a manner that would substantially affect public areas. However, the PEIR determined that this impact could be reduced to a less-than-significant level with implementation of Mitigation Measure M-WS-1: Screening-Level Wind Analysis and Wind Testing, which would require a wind analysis for any new structures within the plan area that have a proposed height of 80 feet or taller.

Project Analysis

<i>Topics:</i>	<i>Significant Impact Peculiar to Project or Project Site</i>	<i>Significant Impact not Identified in PEIR</i>	<i>Significant Impact due to Substantial New Information</i>	<i>No Significant Impact not Previously Identified in PEIR</i>
Would the project:				
a) Alter wind in a manner that substantially affects public areas?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

and establish requirements for the review and adoption of rules, regulations, and measures for the reduction of greenhouse gas emissions.

⁴⁴ Executive Order B-15-18, which was signed in September 2018, establishes a statewide goal to achieve carbon neutrality as soon as possible, and no later than 2045, and achieve and maintain net negative emissions after. Available at <https://www.gov.ca.gov/wp-content/uploads/2018/09/9.10.18-Executive-Order.pdf>, accessed September 25, 2018. The statewide executive order is slightly more aggressive than the commitment made by Mayor Mark Farrell in April 2018 for the City to reach net-zero greenhouse gas emissions by 2050. The San Francisco Department of the Environment is currently developing a plan to meet the goal of carbon neutrality.

⁴⁵ San Francisco Planning Department, *Greenhouse Gas Analysis: Compliance Checklist for 351 12th Street*, March 14, 2019.

E.8.a) To determine whether a project would alter wind in a manner that substantially affects public areas, the planning department applies the wind hazard criterion established in section 148 of the San Francisco Planning Code. In accordance with section 148, a project would result in hazardous wind conditions if it would cause ground-level wind speeds that exceed 26 mph for one hour or more per year.⁴⁶

In most cases, projects under 80 feet in height do not result in wind impacts in accordance with this criterion. The project site is currently a surface parking lot. The proposal would construct a 66-foot-tall building with a stair and elevator overrun. Both overruns would be enclosed together in a 16-foot-tall mechanical screen located on the eastern edge of the building and extending the entire depth of the building. The adjacent building to the west that is currently under construction (seven stories, 80 feet tall) will be taller than the proposed project and provide some degree of shelter from prevailing winds. Given this context, it is unlikely that the mechanical screen would intercept overhead winds and redirect them downward to the 12th Street sidewalk. Based on the orientation and location of the mechanical screen along the eastern edge of the proposed project, any overhead winds that are intercepted by the mechanical screen would be redirected eastward at roof level. For these reasons, the proposed project would not result in any significant wind impacts beyond those identified in the Western SoMa PEIR.

Cumulative

All of the projects listed within the Cumulative Setting would be within 1500 feet of the project site. Of those projects, only one of the projects would be over 80 feet tall, with roof equipment: 1560 Folsom Street. The elevator and stair penthouses on 1560 Folsom Street are set back from the property line and any wind would be redirected onto the roof, not the sidewalk along Folsom Street. There are no cumulative wind impacts anticipated as a result of the Cumulative Setting projects list. For these reasons, the proposed project would not combine with reasonably foreseeable projects in the project vicinity to create significant cumulative wind impacts.

Conclusion

For the reasons stated above, the proposed project would not result in significant wind impacts, either individually or cumulatively. Therefore, the proposed project would not result in significant wind impacts that were not identified in the Western SoMa PEIR.

E.9 SHADOW

Western SoMa PEIR Shadow Findings

The Western SoMa PEIR determined that implementation of the Western SoMa Plan would have a significant and unavoidable impact related to the creation of new shadows in a manner that would substantially affect outdoor recreation facilities or other public areas. No mitigation measures were identified in the PEIR.

⁴⁶ San Francisco Planning Code Section 148. Available at: [http://library.amlegal.com/nxt/gateway.dll/California/planning/article12dimensionsareasandopenspaces?f=templates\\$fn=default.htm\\$3.0\\$vid=amlegal:sanfrancisco_ca\\$anc=JD_138.1](http://library.amlegal.com/nxt/gateway.dll/California/planning/article12dimensionsareasandopenspaces?f=templates$fn=default.htm$3.0$vid=amlegal:sanfrancisco_ca$anc=JD_138.1)

Project Analysis

<i>Topics:</i>	<i>Significant Impact Peculiar to Project or Project Site</i>	<i>Significant Impact not Identified in PEIR</i>	<i>Significant Impact due to Substantial New Information</i>	<i>No Significant Impact not Previously Identified in PEIR</i>
----------------	---	--	--	--

Would the project:

- | | | | | |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|
| a) Create new shadow that substantially and adversely affects the use and enjoyment of publicly accessible open spaces? | <input type="checkbox"/> | <input type="checkbox"/> | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
|---|--------------------------|--------------------------|--------------------------|-------------------------------------|

E.9.a) The proposed project would construct a 68-foot-tall building; therefore, a preliminary shadow fan analysis was prepared to determine whether the project would have the potential to cast new shadow on nearby parks.⁴⁷ The preliminary shadow fan showed that the proposed project would not cast shadow on any public open space, share schoolyard, or property under the jurisdiction of the recreation and park department.

The proposed project would shade portions of nearby streets and sidewalks and private property at times within the project vicinity. Shadows on streets and sidewalks would not exceed levels commonly expected in urban areas and would be considered a less-than-significant effect under CEQA. Although occupants of nearby properties may regard the increase in shadow as undesirable, the limited increase in shading of private properties as a result of the proposed project would not be considered a significant impact under CEQA.

Cumulative

As the proposed project would not cast new shadow on any existing parks or publicly accessible open space, the proposed project would not have the ability to contribute considerably to a cumulative impact. For these reasons, the proposed project would not combine with past, present, and reasonably foreseeable future projects in the project vicinity to create significant cumulative shadow impacts.

Conclusion

For the reasons stated above, the proposed project would not result in significant shadow impacts, either individually or cumulatively. Therefore, the proposed project would not result in significant shadow impacts that were not identified in the Western SoMa PEIR.

E.10 RECREATION

Western SoMa PEIR Recreation Findings

The Western SoMa PEIR concluded that implementation of the Western SoMa Area Plan would not result in substantial or accelerated deterioration of existing recreational resources or require the construction or expansion of recreational facilities that may have an adverse effect on the environment. No mitigation measures related to recreational resources were identified in the PEIR.

⁴⁷ San Francisco Planning Department, *Shadow Fan*, 351 12th Street, March 14, 2019.

An update of the Recreation and Open Space Element (ROSE) of the General Plan was adopted in April 2014. Policy 2.1 of the ROSE prioritizes acquisition of open space in high needs areas, and the Western SoMa neighborhood is recognized in the ROSE as a high needs area. Policy 2.11 of the ROSE encourages that privately developed residential open spaces, including common spaces, in the downtown and multi-family zoning districts be increased.

Project Analysis

<i>Topics:</i>	<i>Significant Impact Peculiar to Project or Project Site</i>	<i>Significant Impact not Identified in PEIR</i>	<i>Significant Impact due to Substantial New Information</i>	<i>No Significant Impact not Previously Identified in PEIR</i>
Would the project:				
a) Increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facilities would occur or be accelerated?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Include recreational facilities or require the construction or expansion of recreational facilities that might have an adverse physical effect on the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

E.10.a) As discussed in Topic E.2, Population and Housing, the proposed project would add new residential space resulting in approximately ~~48~~96 new residents. New residents would be adjacent to the proposed Eagle Plaza, a 175-foot linear public pedestrian plaza on 12th Street between the 351 12th Street project site's southern boundary and Harrison Street. The proposed project would provide limited open space for future residents due to waivers requested as part of the state density bonus program. Although the proposed project would introduce a new permanent population to the project site, the number of new residents projected would not be large enough to substantially increase demand for, or use of, neighborhood parks or recreational facilities, such that substantial physical deterioration of the facilities would be expected.

E.10.b) The permanent residential population on the site would not require the construction of new recreational facilities or the expansion of existing facilities.

Cumulative

Cumulative development in the project vicinity would result in an intensification of land uses and an increase in the use of nearby recreational resources and facilities. The Recreation and Open Space Element of the General Plan provides a framework for providing a high quality open space system for its residents, while accounting for expected population growth through year 2040. In addition, San Francisco voters passed two bond measures, in 2008 and 2012, to fund the acquisition, planning, and renovation of the City's network of recreational resources. As discussed above, the linear pedestrian Eagle Plaza is under construction adjacent to the project site. Other existing recreational facilities would be able to accommodate the increase in demand for recreational resources generated by nearby cumulative development projects without resulting in physical degradation of those resources. For these reasons, the proposed project would not combine with reasonably foreseeable future projects in the project vicinity to create a significant cumulative impact on recreational facilities.

Conclusion

As discussed above, the proposed project would not result in a significant individual or cumulative impact related to recreational resources. Therefore, the proposed project would not result in a significant recreational impact that was not disclosed in the Western SoMa PEIR.

E.11 UTILITIES AND SERVICE SYSTEMS

Western SoMa PEIR Utilities and Service System Findings

The Western SoMa PEIR determined that the anticipated increase in population would not result in a significant impact to the provision of water, wastewater collection and treatment, and solid waste collection and disposal. No mitigation measures were identified in the PEIR.

<i>Topics:</i>	<i>Significant Impact Peculiar to Project or Project Site</i>	<i>Significant Impact not Identified in PEIR</i>	<i>Significant Impact due to Substantial New Information</i>	<i>No Significant Impact not Previously Identified in PEIR</i>
11. Would the project:				
a) Require or result in the relocation or construction of new or expanded wastewater treatment, stormwater drainage, electric power, natural gas, or telecommunications facilities, the construction or relocation of which could cause significant physical environmental effects?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Have sufficient water supplies available to serve the project and reasonably foreseeable future development during normal, dry and multiple dry years? Require or result in the relocation of new or expanded water facilities, the construction or relocation of which could cause significant environmental effects?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Result in a determination by the wastewater treatment provider that would serve the project that it has inadequate capacity to serve the project's projected demand in addition to the provider's existing commitments?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Generate solid waste in excess of state or local standards, or in excess of the capacity or local infrastructure, or otherwise impair the attainment of solid waste reduction goals?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Comply with federal, state, and local management and reduction statutes and regulations related to solid waste?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

E.11.a and c) The project site is served by San Francisco's combined sewer system, which handles both sewage and stormwater runoff. The Southeast Water Pollution Control Plant provides wastewater and stormwater treatment and management for the east side of the city, including the project site. Project related wastewater and stormwater would flow into the city's combined sewer system and would be treated to standards contained in the city's National Pollutant

Discharge Elimination System (NPDES) Permit for the Southeast Water Pollution Control Plant prior to discharge into the San Francisco Bay. The NPDES standards are set and regulated by the Regional Water Quality Control Board. The Southeast Plant is designed to treat up to 85 million gallons per day of average dry weather wastewater flows and up to 250 million gallons per day of wet weather combined wastewater and stormwater flows. Average dry weather flows to the Southeast Plant ranged from 58 to 61 million gallons per day for the years 2012 to 2014 and are projected to increase to 69 million gallons per day by 2045.⁴⁸

The San Francisco Public Utilities Commission (SFPUC) is in the process of implementing the Sewer System Improvement Program, which is a multi-billion dollar citywide upgrade to the city's sewer and stormwater infrastructure to ensure a reliable and seismically safe system. The program includes planned improvements that will serve development in the Western SoMa plan area including at the Southeast Water Pollution Control Plant, the Central Bayside System, and green infrastructure projects, such as the Mission and Valencia Green Gateway.

The proposed project would not substantially increase the amount of stormwater entering the combined sewer system because the project would not increase impervious surfaces at the project site. Compliance with the city's Stormwater Management Ordinance and the Stormwater Management Requirements and Design Guidelines would ensure that the design of the proposed project includes installation of appropriate stormwater management systems that retain runoff on site, promote stormwater reuse, and limit discharges from the site from entering the city's combined stormwater/sewer system. Under the Stormwater Management ordinance, stormwater generated by the proposed project is required to meet a performance standard that reduces the existing runoff flow rate and volume by 25 percent for a two-year 24-hour design storm and therefore would not contribute additional volume of polluted runoff to the city's stormwater infrastructure.

The project site is located within a developed area served by existing electric power, natural gas, and telecommunications. While the project would require local connection to those utilities, it would not necessitate the construction of new power generation, natural gas, or telecommunications infrastructure. Although the proposed project would add ~~48–96~~ new residents to the project site, the combined sewer system has capacity to serve projected growth through year 2045. Therefore, the incremental increase in wastewater treatment resulting from the project would be met by the existing sewer system and would not require expansion of existing wastewater facilities or construction of new facilities.

E.11.b) Water would be supplied to the proposed project from the SFPUC's Hetch-Hetchy regional water supply system. Under sections 10910 through 10915 of the California Water Code, urban water suppliers like the SFPUC must prepare water supply assessments for certain large "water demand" projects, as defined in CEQA Guidelines section 15155.⁴⁹ The proposed project

⁴⁸ San Francisco Planning Department, *Biosolids Digester Facilities Project, Final Environmental Impact Report*, Case No. 2015-000644ENV, State Clearinghouse No. 2015062073, certified March 8, 2018.

⁴⁹ Pursuant to CEQA Guidelines section 15155(1), "a water-demand project" means:

- (A) A residential development of more than 500 dwelling units.
- (B) A shopping center or business establishment employing more than 1,000 persons or having more than 500,000 square feet of floor space.
- (C) A commercial office building employing more than 1,000 persons or having more than 250,000 square feet of floor area.

does not qualify as a “water-demand” project as defined by CEQA Guidelines section 15155(a)(1); therefore a water supply assessment has not been prepared for the project. However, the SFPUC estimates that a typical development project in San Francisco comprised of either 100 dwelling units, 100,000 square feet of commercial use, 50,000 square feet of office, 100 hotel rooms, or 130,000 square feet of PDR use would generate demand for approximately 10,000 gallons of water per day, which is the equivalent of 0.011 percent of the total water demand anticipated for San Francisco in 2040 of 89.9 million gallons per day.⁵⁰ Because it would result in 48 group housing dwelling units, the proposed project would generate less than 0.011 percent of water demand for the city as a whole in 2040, which would constitute a negligible increase in anticipated water demand.

The SFPUC uses population growth projections provided by the planning department to develop the water demand projections contained in the urban water management plan. As discussed in the Population and Housing Section above, the proposed project would be encompassed within planned growth in San Francisco and is therefore also accounted for in the water demand projections contained in the urban water management plan. Because the proposed project would comprise a small fraction of future water demand that has been accounted for in the city’s urban water management plan, sufficient water supplies would be available to serve the proposed project in normal, dry, and multiple dry years, and the project would not require or result in the relocation or construction of new or expanded water supply facilities the construction or relocation of which could cause significant environmental effects. This impact would be less than significant, and no mitigation measures are necessary.

E.11.d and e) The city disposes of its municipal solid waste at the Recology Hay Road Landfill, and that practice is anticipated to continue until 2025, with an option to renew the agreement thereafter for an additional six years. San Francisco Ordinance No. 27-06 requires mixed construction and demolition debris to be transported to a facility that must recover for reuse or recycling and divert from landfill at least 65 percent of all received construction and demolition debris. San Francisco’s Mandatory Recycling and Composting Ordinance No. 100-09 requires all properties and persons in the city to separate their recyclables, compostables, and landfill trash.

The proposed project would incrementally increase total city waste generation; however, the proposed project would be required to comply with San Francisco ordinance numbers 27-06 and 100- 09. Due to the existing and anticipated increase of solid waste recycling in the city and the requirements to divert construction debris from the landfill, any increase in solid waste resulting from the proposed project would be accommodated by the existing Hay Road landfill. Thus, the proposed project would have less-than-significant impacts related to solid waste.

(D) A hotel or motel, or both, having more than 500 rooms, (e) an industrial, manufacturing, or processing plant, or industrial park planned to house more than 1,000 persons, occupying more than 40 acres of land, or having more than 650,000 square feet of floor area.

(F) a mixed-use project that includes one or more of the projects specified in subdivisions (a)(1)(A), (a)(1)(B), (a)(1)(C), (a)(1)(D), (a)(1)(E), and (a)(1)(G) of this section.

(G) A project that would demand an amount of water equivalent to, or greater than, the amount of water required by a 500 dwelling unit project.

⁵⁰ San Francisco Public Utilities Commission, 2015 *Urban Water Management Plan for the City and County of San Francisco*, June 2016. This document is available at <https://sfwater.org/index.aspx?page=75>

Cumulative Analysis

As explained in the analysis above, existing service management plans for water, wastewater, and solid waste disposal account for anticipated citywide growth. Furthermore, all projects in San Francisco would be required to comply with the same regulations described above which reduce stormwater, potable water, and waste generation. Therefore, the proposed project, in combination with other cumulative development projects would not result in a cumulative utilities and service systems impact.

Conclusion

As discussed above, the proposed project would not result in a significant individual or cumulative impact with respect to utilities and service systems. Therefore, the proposed project would not result in a significant utilities and service system impact that was not disclosed in the Western SoMa PEIR.

E.12 PUBLIC SERVICES

Western SoMa PEIR Public Services Findings

The Western SoMa PEIR determined that the anticipated increase in population would not result in substantial adverse physical impacts associated with the provision of or need for new or physically altered public services, including fire protection, police protection, and public schools. No mitigation measures were identified in the PEIR.

Project Analysis

<i>Topics:</i>	<i>Significant Impact Peculiar to Project or Project Site</i>	<i>Significant Impact not Identified in PEIR</i>	<i>Significant Impact due to Substantial New Information</i>	<i>No Significant Impact not Previously Identified in PEIR</i>
Would the project:				
a) Result in substantial adverse physical impacts associated with the provision of, or the need for, new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times, or other performance objectives for any public services such as fire protection, police protection, schools, parks, or other services?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

E.12.a) Project residents and employees would be served by the San Francisco Police Department and Fire Departments. The closest police station to the project site is at 850 Bryant Street, located approximately 0.8 miles from the site. The closest fire station to the project site is San Francisco Fire Department Station 36, located approximately 0.6 miles from the project site. The increased population at the project site could result in more calls for police, fire, and emergency response. However, the increase in demand for these services would not be substantial given the overall demand for such services on a citywide basis. Moreover, the proximity of the project site to police

and fire stations would help minimize the response time for these services should incidents occur at the project site.

The San Francisco Unified School District (school district) maintains a property and building portfolio that has capacity for almost 64,000 students.⁵¹ A decade-long decline in district enrollment ended in the 2008-2009 school year at 52,066 students, and total enrollment in the district has increased to about 54,063 in the 2017-2018 school year, an increase of approximately 1,997 students since 2008.^{52,53} Thus, even with increasing enrollment, the school district currently has more classrooms district-wide than needed.⁵⁴ However, the net effect of housing development across San Francisco is expected to increase enrollment by at least 7,000 students by 2030 and eventually enrollment is likely to exceed the capacity of current facilities.⁵⁵

Lapkoff & Gobalet Demographic Research, Inc. conducted a study in 2010 for the school district that projected student enrollment through 2040.⁵⁶ This study is being updated as additional information becomes available. The study considered several new and ongoing large-scale developments (Mission Bay, Candlestick Point, Hunters Point Shipyard/San Francisco Shipyard, and Treasure/Yerba Buena Islands, Parkmerced, and others) as well as planned housing units outside those areas.⁵⁷ In addition, it developed student yield assumptions informed by historical yield, building type, unit size, unit price, ownership (rented or owner-occupied), whether units are subsidized, whether subsidized units are in standalone buildings or in inclusionary buildings, and other site-specific factors. For most developments, the study establishes a student generation rate of 0.80 Kindergarten through 12th grade students per residential unit in a standalone affordable housing site, 0.25 students per unit for inclusionary affordable housing developments, and 0.10 students per unit for market-rate housing.

The Leroy F. Greene School Facilities Act of 1998, or SB 50, restricts the ability of local agencies to deny land use approvals on the basis that public school facilities are inadequate. SB 50, however, permits the levying of developer fees to address local school facility needs resulting from new development. Local jurisdictions are precluded under state law from imposing school-enrollment-related mitigation beyond the school development fees. The school district collects these fees, which are used in conjunction with other school district funds, to support efforts to

⁵¹ This analysis was informed, in part, by a Target Enrollment Survey the San Francisco Unified School District performed of all schools in 2010.

⁵² San Francisco Unified School District, Facts at a Glance, 2018, <http://www.sfusd.edu/en/assets/sfusd-staff/about-SFUSD/files/sfusd-facts-at-a-glance.pdf>, accessed September 13, 2018.

⁵³ Note that Enrollment summaries do not include charter schools. Approximately 4,283 students enrolled in charter schools are operated by other organizations but located in school district facilities.

⁵⁴ San Francisco Unified School District, San Francisco Bay Area Planning and Urban Research (SPUR) Forum Presentation, Growing Population, Growing Schools, August 31, 2016, https://www.spur.org/sites/default/files/events_pdfs/SPUR%20Forum_August%2031%202016.pptx_.pdf, accessed October 5, 2018.

⁵⁵ Lapkoff & Gobalet Demographic Research, Inc., Demographic Analyses and Enrollment Forecasts for the San Francisco Unified School District, February 16, 2018, p. 2, <http://www.sfusd.edu/en/assets/sfusd-staff/about-SFUSD/files/demographic-analyses-enrollment-forecast.pdf>, accessed October 5, 2018.

⁵⁶ Ibid.

⁵⁷ Ibid.

complete capital improvement projects within the city. The proposed project would be subject to the school impact fees.

The proposed project would ~~not~~ be expected to generate ~~any about six~~ school-aged children, ~~as the proposed project would construct 48 group housing units, for which each unit could legally house one individual, assumed not to be a minor or school-aged child.~~⁵⁸ Six school-aged children would be considered a minor increase to local school enrollment. Thus, the proposed project would result in a less-than-significant impact as a result of the proposed project, there would be expected to have no effect on school district capacity and no additional school facilities would be necessary.

Impacts on parks and recreational facilities are addressed above in Topic E.10, Recreation.

Cumulative Analysis

The proposed project, combined with projected citywide growth through 2040, would increase demand for public services, including police and fire protection. The fire department, the police department, the school district, and other city agencies have accounted for such growth in providing public services to the residents of San Francisco. For these reasons, the proposed project, in combination with projected cumulative development, would not result in a significant cumulative impact resulting from the construction of new or expanded governmental facilities beyond those identified in the Western SoMa PEIR.

Conclusion

As discussed above, the proposed project would not result in a significant individual or cumulative impact with respect to public services. Therefore, the proposed project would not result in a significant public services impact that was not disclosed in the Western SoMa PEIR.

E.13 BIOLOGICAL RESOURCES WESTERN SOMA PEIR BIOLOGICAL FINDINGS As discussed in the Western SoMa PEIR, the Plan area is almost fully developed with buildings and other improvements such as streets and parking lots. Most of the plan area consists of structures that have been industrial uses for many years. As a result, landscaping and other vegetation is sparse, except for a few parks. Because future development projects in the Western SoMa plan area would largely consist of new construction of mixed uses in these heavily built-out former industrial neighborhoods, vegetation loss or disturbance of wildlife other than common urban species would be minimal. Therefore, the Western SoMa PEIR concluded that implementation of the Plan would not result in any significant effects related to riparian habitat, wetlands, movement of migratory species, local policies or ordinances protecting biological resources, or habitat conservation plans.

⁵⁸ The proposed project includes 48 group housing units, which could be occupied by up to two residents per group housing unit. Using the Lapkoff & Gobalet student yield calculations, it was assumed that about two school-aged children would potentially live in the eight on-site affordable units ($8 \times 0.25 = 2$) and about four school-aged children would be generated from the 40 market-rate units ($4 \times 0.10 = 4$). In total, about six school-aged children may live in the proposed project.

The Western SoMa PEIR determined that the Plan would result in significant but mitigable impacts on special-status birds and bats that may be nesting in trees or roosting in buildings that are proposed for removal/demolition as part of an individual project. As identified in the PEIR, Mitigation Measure M-BI-1a, Pre-Construction Special-Status Bird Surveys, and M-BI-1b, Pre-Construction Special Status Bat Surveys would reduce these impacts to less-than-significant levels.

Project Analysis

<i>Topics:</i>	<i>Significant Impact Peculiar to Project or Project Site</i>	<i>Significant Impact not Identified in PEIR</i>	<i>Significant Impact due to Substantial New Information</i>	<i>No Significant Impact not Previously Identified in PEIR</i>
Would the project:				
a) Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special-status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Wildlife or U.S. Fish and Wildlife Service?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations or by the California Department of Fish and Wildlife or U.S. Fish and Wildlife Service?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Have a substantial adverse effect on state or federally protected wetlands (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) Conflict with the provisions of an adopted habitat conservation plan, natural community conservation plan, or other approved local, regional, or state habitat conservation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

E.13.a-f) The project site is located within the Western SoMa Plan area and therefore, the project site does not support habitat for any candidate, sensitive or special status species. Further, there are no riparian corridors, estuaries, marshes or wetlands on or adjacent to the project site and there are no environmental conservation plans applicable to the project site. Additionally, the project would be required to comply with the Urban Forestry Ordinance, *Public Works Code* section 801 *et. seq.*, which requires a permit from Public Works to remove any protected trees (landmark, significant, and street trees). The proposed project does not involve the removal of any existing trees as there are none associated with this project site. The proposed project would plant

one new street tree along the 12th Street frontage. Further, the project site is an existing surface parking lot with no structures and would not result in the loss of habitat for any special status species, including bats. Therefore, the proposed project would not result in significant biological resource impacts.

Cumulative Analysis

As the proposed project would have no impact on special status species or sensitive habitats, the project would not have the potential to contribute to cumulative impacts to special status species or sensitive habitats. All projects within San Francisco are required to comply with the Urban Forestry Ordinance, *Public Works Code* section 801 *et seq.*, which would ensure that any cumulative impact resulting from conflicts with the city ordinance protecting trees would be less than significant.

Conclusion

As discussed above, the proposed project would not result in a significant individual or cumulative impact with respect to biological resources. Therefore, the proposed project would not result in a significant biological resources impact that was not disclosed in the Western SoMa PEIR.

E.14 GEOLOGY AND SOILS

Western SoMa PEIR Geology and Soils Findings

The Western SoMa PEIR concluded that implementation of the Area Plan would indirectly increase the population that would be subject to an earthquake, including seismically induced ground-shaking, liquefaction, and landslides. The PEIR also noted that new development is generally safer than comparable older development due to improvements in building codes and construction techniques. Compliance with applicable codes and recommendations made in project-specific geotechnical analyses would not eliminate earthquake risks, given the seismically active characteristics of the Bay Area, but would reduce them to an acceptable level. Thus, the PEIR concluded that implementation of the Plan would not result in significant impacts with regards to geology and soils, and no mitigation measures were identified in the Western SoMa PEIR.

Project Analysis

<i>Topics:</i>	<i>Significant Impact Peculiar to Project or Project Site</i>	<i>Significant Impact not Identified in PEIR</i>	<i>Significant Impact due to Substantial New Information</i>	<i>No Significant Impact not Previously Identified in PEIR</i>
----------------	---	--	--	--

Would the project:

- a) Directly or indirectly cause potential substantial adverse effects, including the risk of loss, injury, or death involving:

☐
☐
☐
☒

Topics:	Significant Impact Peculiar to Project or Project Site	Significant Impact not Identified in PEIR	Significant Impact due to Substantial New Information	No Significant Impact not Previously Identified in PEIR
i) Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? (Refer to Division of Mines and Geology Special Publication 42.)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
ii) Strong seismic ground shaking?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
iii) Seismic-related ground failure, including liquefaction?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
iv) Landslides?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Result in substantial soil erosion or the loss of topsoil?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Be located on geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction, or collapse?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code, creating substantial direct or indirect risks to life or property?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Have soils incapable of adequately supporting the use of septic tanks or alternative wastewater disposal systems where sewers are not available for the disposal of wastewater?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

E.14.a, c, and d) A geotechnical investigation was prepared for the proposed project.^{59,60} The site is currently a paved surface parking lot entirely covered with impervious surfaces. Boring test results from the project site indicate up to 5 feet below ground surface of loose to very loose sand. Between 5 and 9 feet below ground surface, very soft to soft clay was encountered. Beneath the clay was medium dense to very dense Dune sand intermixed with layers of medium dense to dense clayey and silty sand up to the boring's test depth of 66.5 feet below ground surface. Groundwater was measured between 6.5 and 13 feet below ground surface. The investigation concluded that groundwater could fluctuate seasonally and to assume a 5-foot depth of encounter of groundwater for design. The project is located in a seismic hazard zone for liquefaction and was jointly reviewed in an interdepartmental review meeting on July 16, 2019, by the building department, fire department, public works, and planning department.⁶¹

⁵⁹ Rockridge Geotechnical, *Final Geotechnical Investigation Proposed Residential Building 333 12th Street*, April 14, 2016.

⁶⁰ Rockridge Geotechnical, *Addendum to 333 12th Street Geotechnical Investigation*, July 2, 2019.

⁶¹ Mark Macy, *Application for Interdepartmental Project Review Meeting*, June 19, 2019.

The proposed project would excavate up to 4 feet below ground surface. The proposed foundation is slab on grade. Excavation dewatering is proposed for the start of construction activities to mitigate any groundwater. Following site demolition and excavation, backfill of the project site with compacted fill is proposed as a soil improvement measure.

To ensure that the potential for adverse effects related to geology and soils are adequately addressed, San Francisco relies on the state and local regulatory process for review and approval of building permits pursuant to the California Building Code (state building code), California Code of Regulations, Title 24); the local building code, which is the state building code plus local amendments that supplement the state code, including the building department's administrative bulletins. The building department also provides its implementing procedures in information sheets. The project is required to comply with the building code, which ensures the safety of all new construction in the City. The building department will review the project plans for conformance with the recommendations in the project-specific geotechnical report during its review of the building permit for the project. In addition, the building department may require additional site-specific report(s) through the building permit application process and its implementing procedures, as needed. The building department's requirement for a geotechnical report and review of the building permit application pursuant to its implementation of the building code would ensure that the proposed project would have not result in any significant impacts related to soils, seismicity or other geological hazards.

E.14.b) The project site is occupied by a paved parking area and is entirely covered with impervious surfaces. For these reasons, construction of the proposed project would not result in the loss of substantial topsoil. Site preparation and excavation activities would disturb soil to a depth of approximately 4 feet below ground surface, creating the potential for windborne and waterborne soil erosion. Furthermore, the project would be required to comply with the Construction Site Runoff Ordinance, which requires all construction sites to implement best management practices to prevent the discharge of sediment, non-stormwater and waste runoff from a construction site. For construction projects disturbing 5,000 sf or more, a project must also submit an Erosion and Sediment Control Plan that details the use, location and emplacement of sediment and control devices. These measures would reduce the potential for erosion during construction. Therefore, the proposed project would not result in significant impacts related to soil erosion or the loss of topsoil.

E.14.e) The project would connect to the City's existing sewer system. Therefore, septic tanks or alternative waste disposal systems would not be required and this topic is not applicable to the project.

E.14.f) The project site is presently a surface parking lot. The proposed project would excavate up to 4 feet below ground surface. Paleontological resources include fossilized remains or traces of animals, plants, and invertebrates, including their imprints, from a previous geological period. A unique geologic or physical feature embodies distinctive characteristics of any regional or local geologic principles, provides a key piece of information important to geologic history, contains minerals not known to occur elsewhere in the county, and/or is used as a teaching tool. There are no known unique geologic or physical features at the project site. Construction activities are not anticipated to encounter any below-grade paleontological resources as the site is underlain with

fill and sand up to 66.5 feet below ground surface. Therefore, the project would have no impact on paleontological resources or unique geologic features.

Cumulative Analysis

The proposed project would have no impact with regards to environmental effects of septic systems or alternative waste disposal systems and paleontological resources or unique geologic features. Therefore, the proposed project would not have the potential to combine with effects of reasonably foreseeable projects to result in cumulative impacts to those resource topics.

Environmental impacts related to geology and soils are generally site-specific. All development within San Francisco would be subject to the same seismic safety standards and design review procedures of the California and local building codes and be subject to the requirements of the Construction Site Runoff Ordinance. These regulations would ensure that cumulative effects of development on seismic safety, geologic hazards, and erosion are less than significant. For these reasons, the proposed project would not combine with cumulative projects in the project vicinity to create a significant cumulative impact related to geology and soils.

Conclusion

As discussed above, the proposed project would not result in a significant individual or cumulative impact with respect to geology and soils. Therefore, the proposed project would not result in a significant geology and soils impact that was not disclosed in the Western SoMa PEIR.

E.15 HYDROLOGY AND WATER QUALITY

Western SoMa PEIR Hydrology and Water Quality Findings

The Western SoMa PEIR determined that the anticipated increase in population resulting from implementation of the Plan would not result in a significant impact on hydrology and water quality, including the combined sewer system and the potential for combined sewer outflows. No mitigation measures were identified in the PEIR.

<i>Topics:</i>	<i>Significant Impact Peculiar to Project or Project Site</i>	<i>Significant Impact not Identified in PEIR</i>	<i>Significant Impact due to Substantial New Information</i>	<i>No Significant Impact not Previously Identified in PEIR</i>
Would the project:				
a) Violate any water quality standards or waste discharge requirements or otherwise substantially degrade surface or ground water quality?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Substantially decrease groundwater supplies or interfere substantially with groundwater recharge such that the project may impede sustainable groundwater management of the basin?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river or through the addition of impervious surfaces, in a manner that would:	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Topics:	Significant Impact Peculiar to Project or Project Site	Significant Impact not Identified in PEIR	Significant Impact due to Substantial New Information	No Significant Impact not Previously Identified in PEIR
(i) Result in substantial erosion or siltation on- or off-site;	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
(ii) Substantially increase the rate or amount of surface runoff in a manner which would result in flooding on- or off-site;	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
(iii) Create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff; or	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
(iv) Impede or redirect flood flows?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) In flood hazard, tsunami, or seiche zones, risk release of pollutants due to project inundation?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Conflict or obstruct implementation of a water quality control plan or sustainable groundwater management plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Project Analysis

E.15.a) The project would generate wastewater and stormwater discharges typical of urban residential and commercial uses. Wastewater and stormwater from the project site would be accommodated by the city's sewer system and treated at the Southeast Water Pollution Control Plant to the standards contained in the city's NPDES permit. The NPDES standards are set and regulated by the San Francisco Bay Regional Water Quality Control Board, therefore, the proposed project would not exceed the waste discharge requirements of the water quality board. Furthermore, as discussed in topic E. 14.b, the project is required to comply with the Construction Site Runoff Ordinance, which requires all construction sites to implement best management practices to prevent the discharge of sediment, non-stormwater and waste runoff from a construction site. The city's compliance with the requirements of its NPDES permit and the project's compliance with Construction Site Runoff Ordinance would ensure that the project would not result in significant impacts to water quality.

E.15.b) As discussed under Topic E.14, groundwater was discovered between approximately 6.5 and 13 feet below ground surface at the project site and may be encountered during excavation. Therefore, dewatering may be necessary during construction and is recommended in the geotechnical investigation.⁶² The project would not require long-term dewatering, and does not propose to extract any underlying groundwater supplies. In addition, the project site is located in the Downtown San Francisco Groundwater Basin. This basin is not used as a drinking water supply and there are no plans for development of this basin for groundwater production.⁶³ For

⁶² Rockridge Geotechnical, *Final Geotechnical Investigation Proposed Residential Building 333 12th Street*, April 14, 2016.

⁶³ The San Francisco Public Utilities Commission (SFPUC) supplies water to all of San Francisco residents and businesses. The SFPUC's groundwater supply program includes two groundwater projects: one along the peninsula and the other supplying groundwater from San Francisco's Westside Groundwater Basin aquifer, approximately 400 feet below ground surface. For more information see: <https://sfwater.org/index.aspx?page=184>. Accessed November 19, 2018.

these reasons, the proposed project would not deplete groundwater supplies or substantially interfere with groundwater recharge. This impact would be less than significant, and no mitigation measures are necessary.

E.15.c) No streams or rivers exist in the vicinity of the project site. Therefore, the proposed project would not alter the course of a stream or river, or substantially alter the existing drainage pattern of the project site or area. For the reasons discussed in Topics E.11.a and E.14.b, the proposed project would not substantially increase the rate or amount of surface runoff such that substantial flooding, erosion, or siltation would occur on- or offsite.

E.15.d) The project site is not located within a 100-year flood hazard zone, a dam failure area, or a tsunami or seiche hazard area. Therefore, Topic 15.d is not applicable to the proposed project.

E.15.e) For the reasons discussed in Topic E.15a, the project would not interfere with the San Francisco Bay water quality control plan. Further, the project site is not located within an area subject to a sustainable groundwater management plan and the project would not extract groundwater supplies.

Cumulative Analysis

The proposed project would have no impact with respect to the following topics and therefore would not have the potential to contribute to any cumulative impacts for those resource areas: location of the project site within a 100-year flood hazard area, tsunami or seiche zone, alterations to a stream or river or changes to existing drainage patterns. The proposed project and other development within San Francisco would be required to comply with the stormwater management and construction site runoff ordinances that would reduce the amount of stormwater entering the combined sewer system and prevent discharge of construction-related pollutants into the sewer system. As the project site is not located in a groundwater basin that is used for water supply, the project would not combine with cumulative projects to result in significant cumulative impacts to groundwater. Therefore, the proposed project in combination with other projects would not result in significant cumulative impacts related to hydrology and water quality.

Conclusion

As discussed above, the proposed project would not result in a significant individual or cumulative impact with respect to hydrology and water quality. Therefore, the proposed project would not result in a significant hydrology and water quality impact that was not disclosed in the Western SoMa PEIR.

E.16 HAZARDS AND HAZARDOUS MATERIALS

Western SoMa PEIR Hazards and Hazardous Materials Findings

The Western SoMa PEIR identified less-than-significant impacts related to the routine transport, use, or disposal of hazardous materials, the potential for the community plan and subsequent development projects within the plan area to interfere with an adopted emergency response plan, and the potential for subsequent projects to expose people or structures to a significant risk with respect to fires.

The Western SoMa PEIR identified potentially significant impacts related to hazardous building materials and determined that PEIR Mitigation Measure M-HZ-2, Hazardous Building Materials Abatement, would reduce these impacts to a less-than-significant level. The PEIR also identified potentially significant impacts related to exposing the public or the environment to unacceptable levels of hazardous materials as a result of subsequent projects within the plan area. The PEIR determined that Mitigation Measure M-HZ-3: Site Assessment and Corrective Action would reduce these impacts to a less-than-significant level.

The San Francisco Board of Supervisors amended Health Code article 22A, which is administered and overseen by the health department and is also known as the Maher Ordinance. Amendments to the Maher Ordinance became effective August 24, 2013, which requires sponsors of projects that disturb more than 50 cubic yards of soil to retain the services of a qualified professional to prepare a Phase I environmental site assessment (site assessment) that meets the requirements of Health Code section 22.A.6. Mitigation Measure M-HZ-3 of the Western SoMa PEIR related to contaminated soil and groundwater is therefore superseded by the Maher Ordinance.

Project Analysis

Topics:	<i>Significant Impact Peculiar to Project or Project Site</i>	<i>Significant Impact not Identified in PEIR</i>	<i>Significant Impact due to Substantial New Information</i>	<i>No Significant Impact not Previously Identified in PEIR</i>
Would the project:				
a) Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard or excessive noise for people residing or working in the project area?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Topics:	<i>Significant Impact Peculiar to Project or Project Site</i>	<i>Significant Impact not Identified in PEIR</i>	<i>Significant Impact due to Substantial New Information</i>	<i>No Significant Impact not Previously Identified in PEIR</i>
g) Expose people or structures, either directly or indirectly, to a significant risk of loss, injury, or death involving wildland fires?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

E.16.a) The proposed project's residential uses could use hazardous materials for building maintenance such as household chemicals for cleaning, and herbicides and pesticides for landscape maintenance. These materials are properly labeled to inform the user of potential risks as well as handling procedures. The majority of these hazardous materials would be consumed upon use and would produce very little waste. Any hazardous wastes that are produced would be managed in accordance with article 22 of the San Francisco Health Code. In addition, the transportation of hazardous materials, are regulated by the California Highway Patrol and the California Department of Transportation. The use of any of these hazardous materials are not expected to cause any substantial health or safety hazards. Therefore, potential impacts related to the routine use, transport, and disposal of hazardous materials would be less than significant.

E.16.b and c) The following discusses the project's potential to emit hazardous materials.

Hazardous Building Materials

Some building materials commonly used in older buildings could present a public health risk if disturbed during an accident or during demolition or renovation of an existing building. Hazardous building materials addressed in the PEIR include asbestos, electrical equipment such as transformers and fluorescent light ballasts that contain PCBs or di (2 ethylhexyl) phthalate (DEHP), fluorescent lights containing mercury vapors, and lead-based paints. Asbestos and lead based paint may also present a health risk to existing building occupants if they are in a deteriorated condition. If removed during demolition of a building, these materials would also require special disposal procedures. Regulations are in place to address the proper removal and disposal of asbestos containing building materials and lead based paint. PEIR Mitigation Measure M-HZ-2, addressing the proper removal and disposal of other hazardous building materials, would not apply to this project as there is no building demolition involved.

Soil and Groundwater Contamination

Since certification of the PEIR, article 22A of the Health Code, also known as the Maher Ordinance, was expanded to include properties throughout the city where there is potential to encounter hazardous materials, primarily industrial zoning districts, sites with current or former industrial uses or underground storage tanks, sites with historic bay fill, and sites close to freeways or underground storage tanks. The Maher Ordinance, which is implemented by the San Francisco Department of Public Health, requires appropriate handling, treatment, disposal, and remediation of contaminated soils that are encountered in the building construction process. All projects in the city that disturb 50 cubic yards or more of soil that are located on sites with potentially hazardous soil or groundwater are subject to this ordinance.

The proposed project would disturb up to 242 cubic yards soil on the site with historic bay fill. Therefore, the project is subject to the Maher Ordinance. The Maher Ordinance requires the project sponsor to retain the services of a qualified professional to prepare a site assessment.

The site assessment would determine the potential for site contamination and level of exposure risk associated with the project. Based on that information, the project sponsor may be required to conduct soil and/or groundwater sampling and analysis known as a *phase 2 environmental site assessment*. Where such analysis reveals the presence of hazardous substances that exceed state or federal standards, the project sponsor is required to submit a site mitigation plan to the health department or other appropriate state or federal agency(ies), and to remediate any site contamination prior to the issuance of any building permit.

In compliance with the Maher Ordinance, the project sponsor has submitted an application for a Maher permit to the health department and a site assessment has been prepared to assess the potential for site contamination.^{64, 65} One recognized environmental condition was discovered to be associated with the property: lead and petroleum hydrocarbon contaminated soil. Other potential contamination at the site included past operations at the site and surrounding areas. The public health department approved the site assessment in November 2016 and additional information was requested.⁶⁶

The proposed project would be required to remediate potential soil and groundwater contamination described above in accordance with Article 22A to standards that would be acceptable for the proposed residential use(s). Compliance with these requirements would ensure that the proposed project would not result in any significant impacts related to hazardous materials.

E.16.d) The proposed project is not located on a list of hazardous materials sites compiled pursuant to Government Code section 65962.5. For the reasons described in the analysis of Topic E.16.b and c, above, the proposed project would not create a significant hazard to the public or environment.

E.16.e) The project site is not located within an airport land use plan area or within two miles of a public airport. Therefore, topic 16.e is not applicable to the proposed project.

E.16.f) The proposed project, located within a city block, would not impair implementation of an emergency response or evacuation plan adopted by the City of San Francisco. Project construction and operation would not close roadways or impede access to emergency vehicles or emergency evacuation routes. Thus, the proposed project would not obstruct implementation of the city's emergency response and evacuation plans, and potential impacts would be less than significant.

E.16.g) As discussed above, the Western SoMa plan area is not located in or near wildland areas with high fire risk. Construction of the proposed project would conform to the provisions of the

⁶⁴ San Francisco Department of Public Health, Phase 1 Environmental Site Assessment Approval, Residential Development, 333 – 12th Street, San Francisco, CA 94103, EHB-SAM No. SMED: 1325, November 16, 2016.

⁶⁵ Ramboll US Corporation, Phase I Environmental Site Assessment, 333 and 351 12th Street, San Francisco, California, March 20, 2018.

⁶⁶ San Francisco Department of Public Health, Phase 1 Environmental Site Assessment Approval, Residential Development, 333 – 12th Street, San Francisco, CA 94103, EHB-SAM No. SMED: 1325, November 16, 2016.

building code and fire code. Final building plans would be reviewed by the building and fire departments to ensure conformance with the applicable life-safety provisions, including development of an emergency procedure manual and an exit drill plan. Therefore, the proposed project would not obstruct implementation of the city's emergency response plan, and potential emergency response and fire hazard impacts would be less than significant.

Cumulative Analysis

Environmental impacts related to hazards and hazardous materials are generally site-specific. Nearby cumulative development projects would be subject to the same regulations addressing use of hazardous waste (Article 22 of the health code), hazardous soil and groundwater (Article 22B of the health code) and building and fire codes addressing emergency response and fire safety. For these reasons, the proposed project would not combine with past, present, or reasonably foreseeable future projects in the project vicinity to create a significant cumulative impact related to hazards and hazardous materials.

Conclusion

The proposed project's impact related to hazardous materials would be less than significant and would not result in significant hazards and hazardous materials impacts that were not identified in the Western SoMa PEIR.

E.17 MINERAL RESOURCES

Western SoMa PEIR Mineral Resources Findings

The plan area does not include any natural resources routinely extracted and the rezoning does not result in any natural resource extraction programs. Therefore, the Western SoMa PEIR concluded that implementation of the area plan and rezoning would not result in a significant impact on mineral resources. No mitigation measures were identified in the PEIR.

Project Analysis

<i>Topics:</i>	<i>Significant Impact Peculiar to Project or Project Site</i>	<i>Significant Impact not Identified in PEIR</i>	<i>Significant Impact due to Substantial New Information</i>	<i>No Significant Impact not Previously Identified in PEIR</i>
Would the project:				
a) Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Result in the loss of availability of a locally important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

E.17.a,b) The project site is not located in an area with known mineral resources and would not routinely extract mineral resources. Therefore, the proposed project would have no impact on mineral resources.

Cumulative

The proposed project would have no impact on mineral resources and therefore would not have the potential to contribute to any cumulative mineral resource impact.

Conclusion

For the reasons stated above, the proposed project would not result in significant impacts either individually or cumulatively related to mineral resources. Therefore, the proposed project would not result in new or more severe impacts on mineral resources not identified in the Western SoMa PEIR.

E.18 ENERGY RESOURCES

Western SoMa PEIR Energy Resources Findings

The Western SoMa PEIR determined that development under the area plans and rezoning would not encourage the use of large amounts of fuel, water, or energy or use these in a wasteful manner. Therefore, the Western SoMa PEIR concluded that implementation of the area plan and rezoning would not result in a significant impact on energy resources. No mitigation measures were identified in the PEIR.

Project Analysis

<i>Topics:</i>	<i>Significant Impact Peculiar to Project or Project Site</i>	<i>Significant Impact not Identified in PEIR</i>	<i>Significant Impact due to Substantial New Information</i>	<i>No Significant Impact not Previously Identified in PEIR</i>
Would the project:				
a) Result in a potentially significant environmental impact due to wasteful, inefficient, or unnecessary consumption of energy resources, during project construction or operation?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Cause a significant environmental impact due to a conflict with or obstruct a state or local plan for renewable energy or energy efficiency?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

E.18.a) Energy demand for the proposed project would be typical of residential projects and would meet, or exceed, current state and local codes and standards concerning energy consumption, including the Green Building Ordinance and Title 24 of the California Code of Regulations. As documented in the GHG compliance checklist for the proposed project, the project would be required to comply with applicable regulations promoting water conservation and reducing potable water use. As discussed in topic E.4, Transportation and Circulation, the project site is located in a transportation analysis zone that experiences low levels of VMT per capita. Therefore, the project would not encourage the use of large amounts of fuel, water, or energy or use these in a wasteful manner.

E.18.b) In 2002, California established its Renewables Portfolio Standard Program, with the goal of increasing the percentage of renewable energy in the state's electricity mix to 20 percent of

retail sales by 2017. In November 2008, Executive Order S-14-08 was signed requiring all retail sellers of electricity to serve 33 percent of their load with renewable energy by 2020. In 2015, Senate Bill 350 codified the requirement for renewables portfolio standard to achieve 50 percent renewable by 2030, and in 2018, Senate Bill 100 requires 60 percent renewable by 2030 and 100 percent by 2045.⁶⁷

San Francisco's electricity supply is 41 percent renewable, and San Francisco's goal is to meet 100 percent of its electricity demand with renewable power.⁶⁸ CleanPowerSF is the city's Community Choice Aggregation Program operated by the SFPUC, which provides renewable energy to residents and businesses. GreenFinanceSF allows commercial property owners to finance renewable energy projects, as well as energy and water efficiency projects, through a municipal bond and repay the debt via their property tax account.

As discussed above in Topic E.18.a, the project would comply with the energy efficiency requirements of the state and local building codes and would not conflict with or obstruct implementation of city and State plans for renewable energy and energy efficiency.

Cumulative

All development projects within San Francisco would be required to comply with applicable regulations in the City's Green Building Ordinance and Title 24 of the California Code of Regulations that reduce both energy use and potable water use. The majority of San Francisco is located within a transportation analysis zone that experiences low levels of VMT per capita compared to regional VMT levels. Therefore, the proposed project, in combination with other reasonably foreseeable cumulative projects would not encourage activities that result in the use of large amounts of fuel, water, or energy or use these in a wasteful manner.

Conclusion

For the reasons stated above, the proposed project would not result in significant impacts either individually or cumulatively related to energy resources. Therefore, the proposed project would not result in new or more severe impacts on energy resources not identified in the Western SoMa PEIR.

E.19 AGRICULTURE AND FOREST RESOURCES

Western SoMa PEIR Agriculture and Forest Resources Findings

The Western SoMa PEIR determined no agricultural resources exist in the plan area; therefore, the rezoning and area plans would have no effect on agricultural resources. The Western SoMa PEIR did not analyze the plan's effects on forest resources.

Project Analysis

⁶⁷ California Energy Commission, California Renewable Energy Overview and Programs. Available at: <https://www.energy.ca.gov/renewables/>. Accessed April 24, 2019.

⁶⁸ San Francisco Mayor's Renewable Energy Task Force Recommendations Report, September 2012. Accessed on April 24, 2019. Available at: https://sfenvironment.org/sites/default/files/fliers/files/sfe_re_renewableenergytaskforcerecommendationsreport.pdf.

<i>Topics:</i>	<i>Significant Impact Peculiar to Project or Project Site</i>	<i>Significant Impact not Identified in PEIR</i>	<i>Significant Impact due to Substantial New Information</i>	<i>No Significant Impact not Previously Identified in PEIR</i>
Would the project:				
a) Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance, as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Conflict with existing zoning for agricultural use, or a Williamson Act contract?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code Section 12220(g)) or timberland (as defined by Public Resources Code Section 4526)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Result in the loss of forest land or conversion of forest land to non-forest use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Involve other changes in the existing environment which, due to their location or nature, could result in conversion of Farmland to non-agricultural use or forest land to non-forest use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

E.19.a-e) The project site is within an urbanized area in the City and County of San Francisco that does not contain any prime farmland, unique farmland, or farmland of statewide importance; forest land; or land under Williamson Act contract. The area is not zoned for any agricultural uses. Topics 19 a through e are not applicable to the proposed project and the project would have no impact either individually or cumulatively on agricultural or forest resources.

Conclusion

For the above reasons, the proposed project would not result in new or more severe impacts to agricultural or forest resources not identified in the Western SoMa PEIR.

E.20 WILDFIRE

Western SoMa PEIR Mineral Resources Findings

The plan area is located within an urbanized area that lacks an urban-wildland interface. Therefore, the Western SoMa PEIR concluded that implementation of the area plan and rezoning would not result in a significant impact related to risk of loss, injury or death involving wildland fires. No mitigation measures were identified in the PEIR.

Project Analysis

<i>Topics:</i>	<i>Significant Impact Peculiar to Project or Project Site</i>	<i>Significant Impact not Identified in PEIR</i>	<i>Significant Impact due to Substantial New Information</i>	<i>No Significant Impact not Previously Identified in PEIR</i>
If located in or near state responsibility areas or lands classified as very high fire hazard severity zones, would the project:				
a) Substantially impair an adopted emergency response plan or emergency evacuation plans?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Due to slope, prevailing winds, and other factors, exacerbate wildfire risks, and thereby expose project occupants to, pollutant concentrations from a wildfire or the uncontrolled spread of a wildfire?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Require the installation or maintenance of associated infrastructure (such as roads, fuel breaks, emergency water sources, power lines or other utilities) that may exacerbate fire risk or that may result in temporary or ongoing impacts to the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Expose people or structures to significant risks including downslope or downstream flooding or landslides as a result of runoff, post-fire slope instability, or drainage changes?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

E.20.a - d) The project site is not located in or near state responsibility lands for fire management or lands classified as very high fire hazard severity zones. Therefore, this topic is not applicable to the project.

F. PUBLIC NOTICE AND COMMENT

A "Notification of Project Receiving Environmental Review" was mailed on October 29, 2018 to adjacent occupants and owners of properties within 300 feet of the project site, Western SoMa and City-wide neighborhood group lists. No comments were received.

G. MITIGATION MEASURES

Project Mitigation 1 - Procedures for Accidental Discovery of Archeological Resources (Western SoMa PEIR Mitigation Measure M-CP-4b).

This mitigation measure is required to avoid any potential adverse effect on accidentally discovered buried or submerged historical resources as defined in CEQA Guidelines Section 15064.5(a)(c).

The project sponsor shall distribute the Planning Department archeological resource "ALERT" sheet to the project prime contractor; to any project subcontractor (including demolition,

excavation, grading, foundation, pile driving, etc. firms); and to utilities firms involved in soils-disturbing activities within the project site. Prior to any soils-disturbing activities being undertaken, each contractor is responsible for ensuring that the "ALERT" sheet is circulated to all field personnel, including machine operators, field crew, pile drivers, and supervisory personnel. The project sponsor shall provide the Environmental Review Officer (ERO) with a signed affidavit from the responsible parties (prime contractor, subcontractor(s), and utilities firms) to the ERO confirming that all field personnel have received copies of the "ALERT" sheet.

Should any indication of an archeological resource be encountered during any soils-disturbing activity of the project, the project head foreman and/or project sponsor shall immediately notify the ERO and shall immediately suspend any soils-disturbing activities in the vicinity of the discovery until the ERO has determined what additional measures should be undertaken.

If the ERO determines that an archeological resource may be present within the project site, the project sponsor shall retain the service of an archeological consultant from the pool of qualified archeological consultants maintained by the Planning Department archeologist. The archeological consultant shall advise the ERO as to whether the discovery is an archeological resource, retains sufficient integrity, and is of potential scientific/historical/cultural significance. If an archeological resource is present, the archeological consultant shall identify and evaluate the archeological resource. The archeological consultant shall make a recommendation as to what action, if any, is warranted. Based on this information, the ERO may require, if warranted, specific additional measures to be implemented by the project sponsor.

Measures might include preservation in situ of the archeological resource, an archeological monitoring program, or an archeological testing program. If an archeological monitoring program or archeological testing program is required, it shall be consistent with the Environmental Planning (EP) division guidelines for such programs. The ERO may also require that the project sponsor immediately implement a site security program if the archeological resource is at risk from vandalism, looting, or other damaging actions.

The project archeological consultant shall submit a Final Archeological Resources Report (FARR) to the ERO that evaluates the historical significance of any discovered archeological resource and describes the archeological and historical research methods employed in the archeological monitoring/data recovery program(s) undertaken. Information that may put at risk any archeological resource shall be provided in a separate removable insert within the final report.

Copies of the Draft FARR shall be sent to the ERO for review and approval. Once approved by the ERO, copies of the FARR shall be distributed as follows: California Archaeological Site Survey Northwest Information Center (NWIC) shall receive one copy and the ERO shall receive a copy of the transmittal of the FARR to the NWIC. The Environmental Planning Division of the Planning Department shall receive one bound copy, one unbound copy, and one unlocked, searchable PDF copy on a CD of the FARR along with copies of any formal site recordation forms (CA DPR 523 series) and/or documentation for nomination to the National Register of Historic Places/California Register of Historical Resources. In instances of high public interest or interpretive value, the ERO may require a different final report content, format, and distribution from that presented above.

Project Mitigation 2 – General Construction Noise Control Measures (Western SoMa PEIR Mitigation Measure M-NO-2a).

To ensure that project noise from construction activities is minimized to the maximum extent feasible, the sponsor shall undertake the following:

- The sponsor shall require the general contractor to ensure that equipment and trucks used for project construction use the best available noise control techniques (e.g., improved mufflers, equipment redesign, use of intake silencers, ducts, engine enclosures and acoustically attenuating shields or shrouds, wherever feasible).
- The sponsor shall require the general contractor to locate stationary noise sources (such as compressors) as far from adjacent or nearby sensitive receptors as possible, to muffle such noise sources, and to construct barriers around such sources and/or the construction site, which could reduce construction noise by as much as 5 dBA. To further reduce noise, the contractor shall locate stationary equipment in pit areas or excavated areas, if feasible.
- The sponsor shall require the general contractor to use impact tools (e.g., jack hammers, pavement breakers, and rock drills) that are hydraulically or electrically powered wherever possible to avoid noise associated with compressed air exhaust from pneumatically powered tools. Where use of pneumatic tools is unavoidable, an exhaust muffler on the compressed air exhaust shall be used, along with external noise jackets on the tools, which could reduce noise levels by as much as 10 dBA.
- The sponsor shall include noise control requirements in specifications provided to construction contractors. Such requirements could include, but not be limited to, performing all work in a manner that minimizes noise to the extent feasible; undertaking the most noisy activities during times of least disturbance to surrounding residents and occupants, as feasible; and selecting haul routes that avoid residential buildings inasmuch as such routes are otherwise feasible.

Prior to the issuance of each building permit, along with the submission of construction documents, the sponsor shall submit to the San Francisco Planning Department and Department of Building Inspection (DBI) a list of measures to respond to and track complaints pertaining to construction noise. These measures shall include: (1) a procedure and phone numbers for notifying DBI, the Department of Public Health, and the Police Department (during regular construction hours and off-hours); (2) a sign posted on-site describing noise complaint procedures and a complaint hotline number that shall be answered at all times during construction; (3) designation of an on-site construction complaint and enforcement manager for the project; and (4) notification of neighboring residents and non-residential building managers within 300 feet of the project construction area at least 30 days in advance of extreme noise-generating activities (defined as activities generating noise levels of 90 dBA or greater) about the estimated duration of the activity.

Project Mitigation 3 – Construction Emissions Minimization Plan for Health Risks and Hazards (Western SoMa PEIR Mitigation Measure M-AQ-7).

To reduce the potential health risk resulting from project construction activities, the project sponsor of each development project in the Draft Plan Area and on the Adjacent Parcels shall undertake a

project-specific construction health risk analysis to be performed by a qualified air quality specialist, as appropriate and determined by the Environmental Planning Division of the San Francisco Planning Department, for diesel-powered and other applicable construction equipment, using the methodology recommended by the Bay Area Air Quality Management District (BAAQMD) and/or the San Francisco Planning Department. If the health risk analysis determines that construction emissions would exceed health risk significance thresholds identified by the BAAQMD and/or the San Francisco Planning Department, the project sponsor shall develop a Construction Emissions Minimization Plan for Health Risks and Hazards designed to reduce health risks from construction equipment to less-than-significant levels.

All requirements in the Construction Emissions Minimization Plan must be included in contract specifications. The Construction Emissions Minimization Plan is described in Mitigation Measure M-AQ-6, Construction Emissions Minimization Plan for Criteria Air Pollutants.

APPENDIX A: PROJECT SITE LOCATION

FIGURE 1 – PROJECT SITE LOCATION

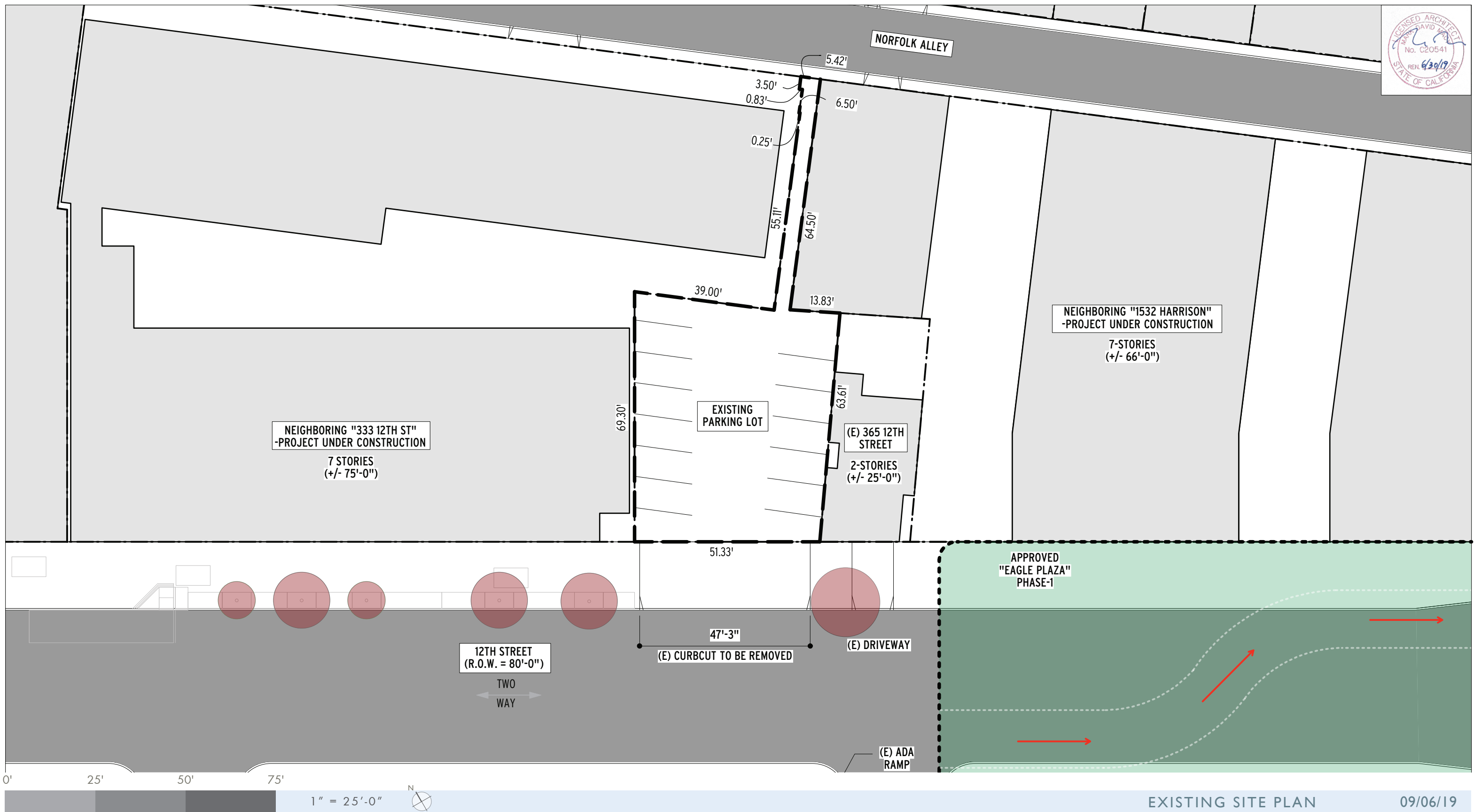
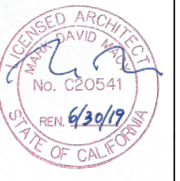


APPENDIX B: PROJECT PLANS

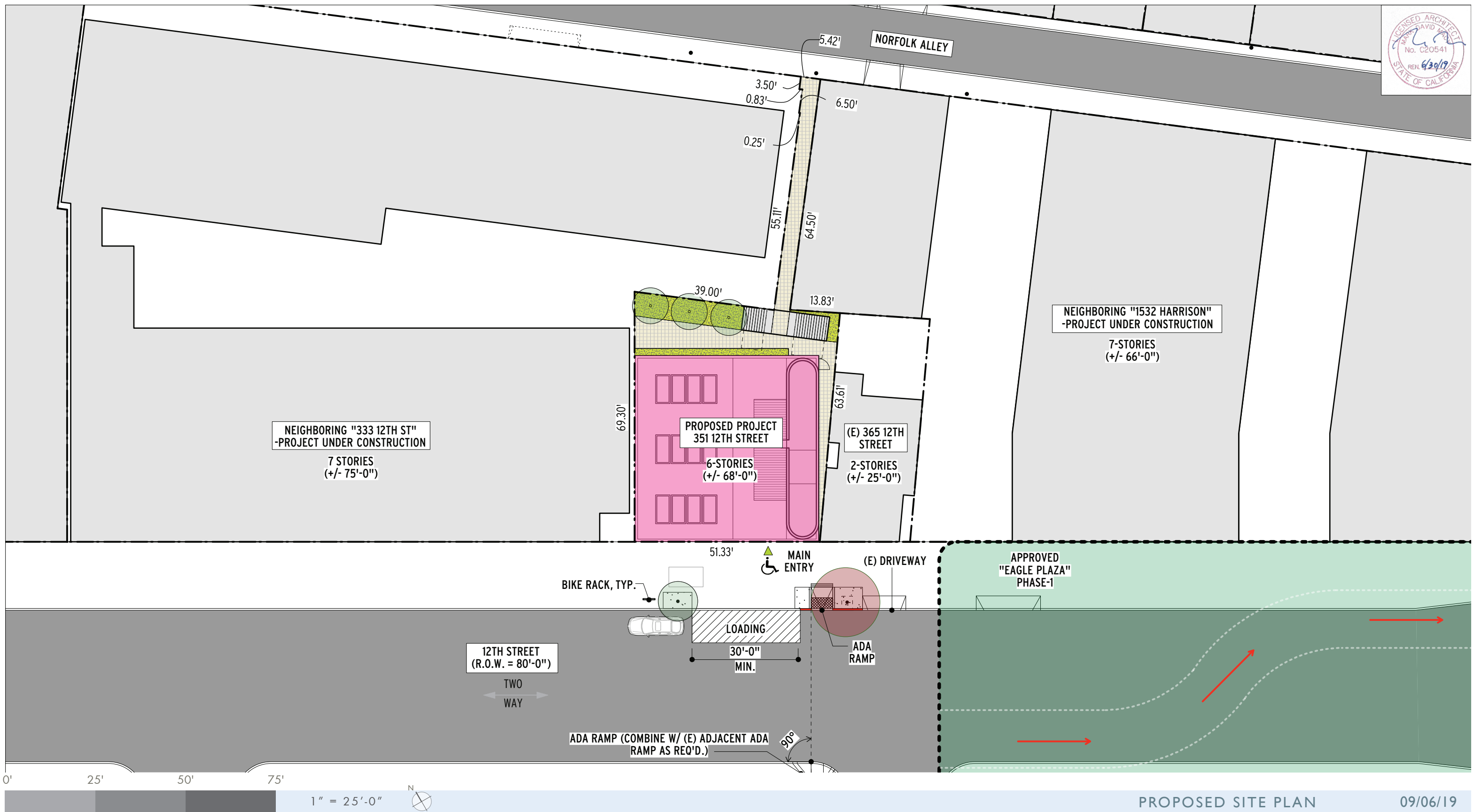
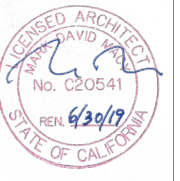
351 12th Street

CONTENTS

Figure 2	Existing Site Plan
Figure 3	Proposed Site Plan
Figure 4	Proposed Floor Plans – First Floor and Second through Sixth Floors
Figure 5	Proposed Upper Roof Plan and Roof Plan
Figure 6	Proposed South (12th Street) Elevation and North (Rear Yard) Elevation
Figure 7	Proposed West Elevation and East Elevation

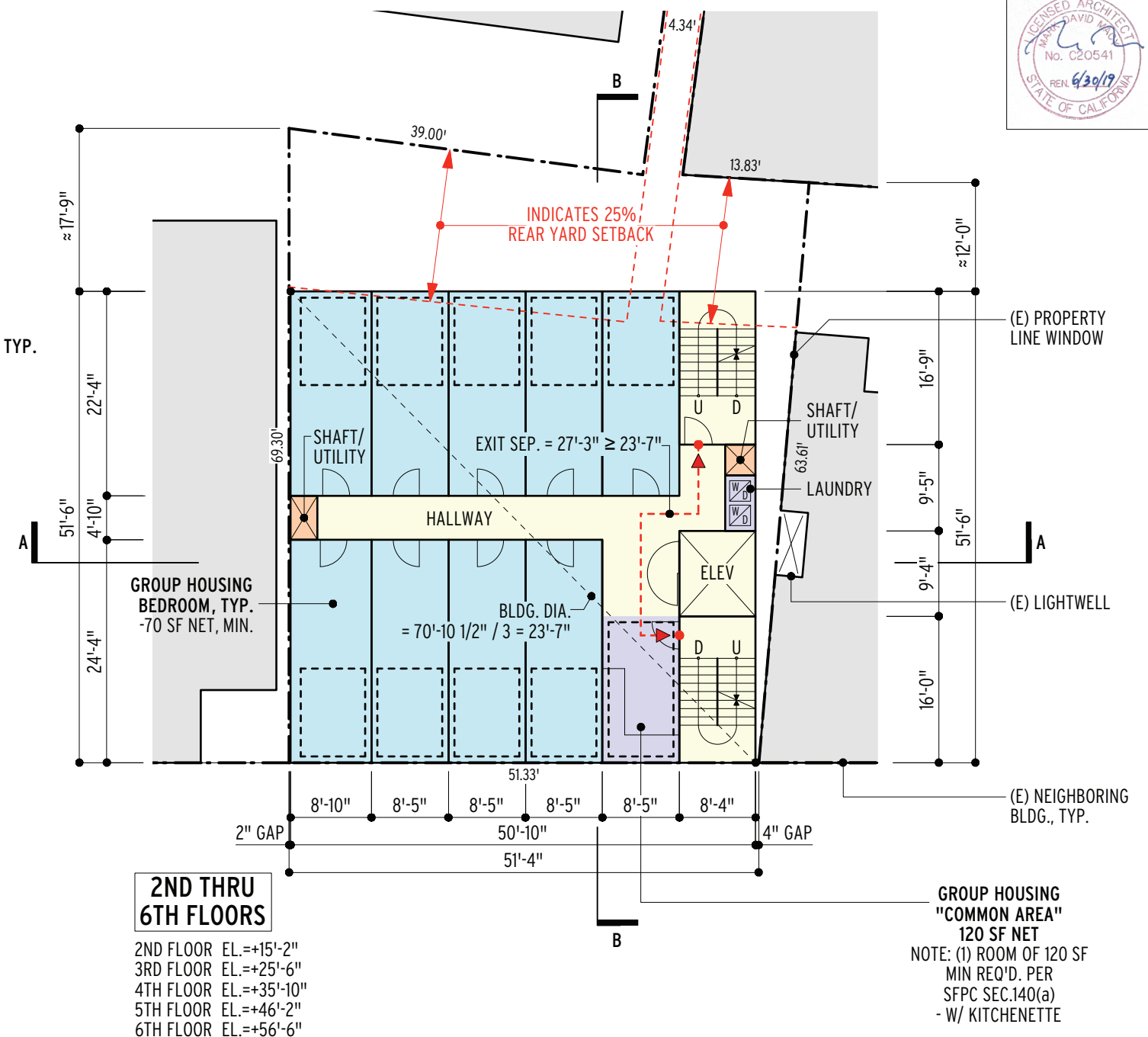
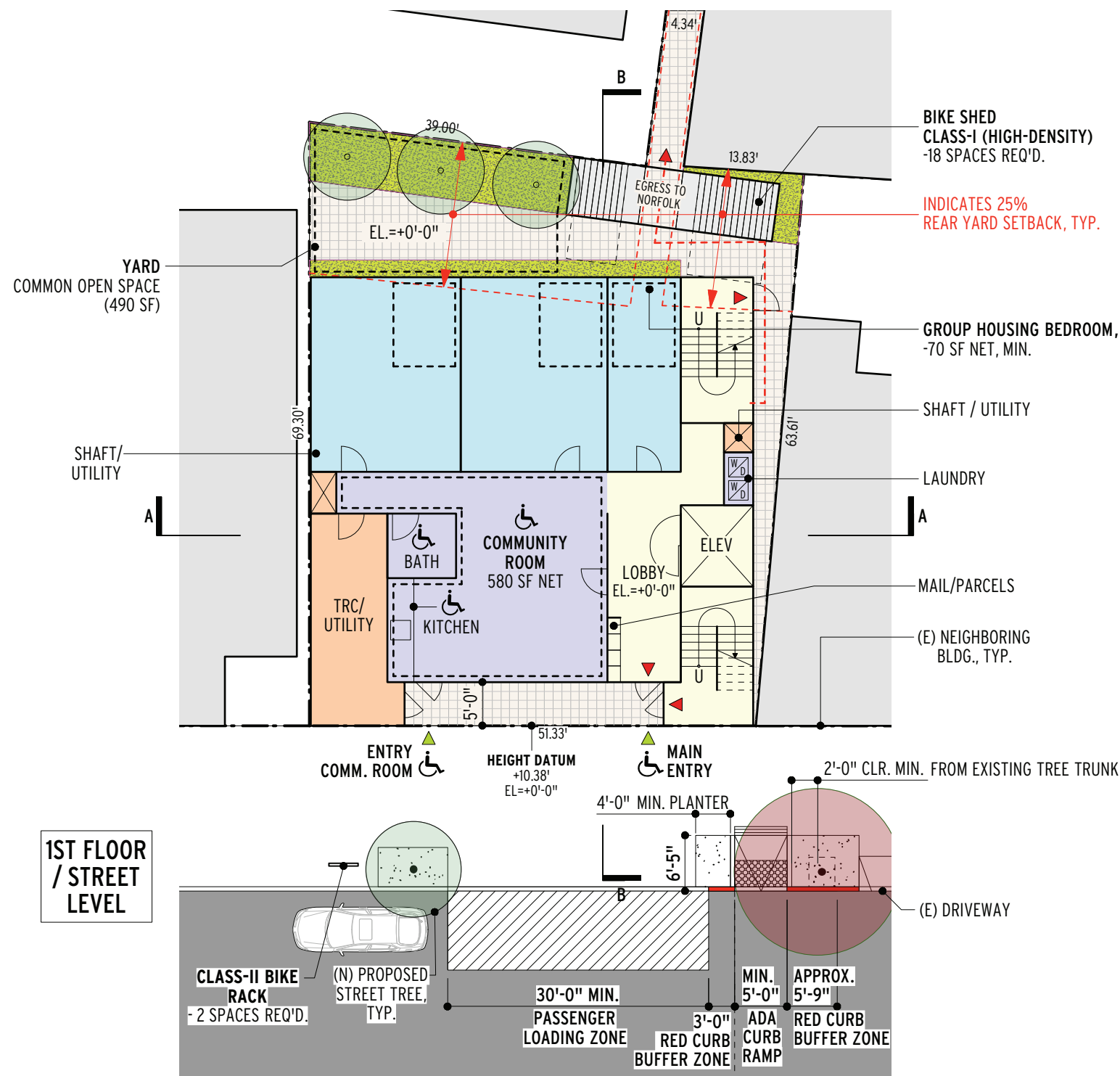


EXISTING SITE PLAN 09/06/19



PROPOSED SITE PLAN

09/06/19



0' 16' 32' 48'

1/16" = 1'-0"



BONUS PROJECT PLAN DIAGRAMS

09/06/19

**MACY
ARCH
ITECT
TURE**

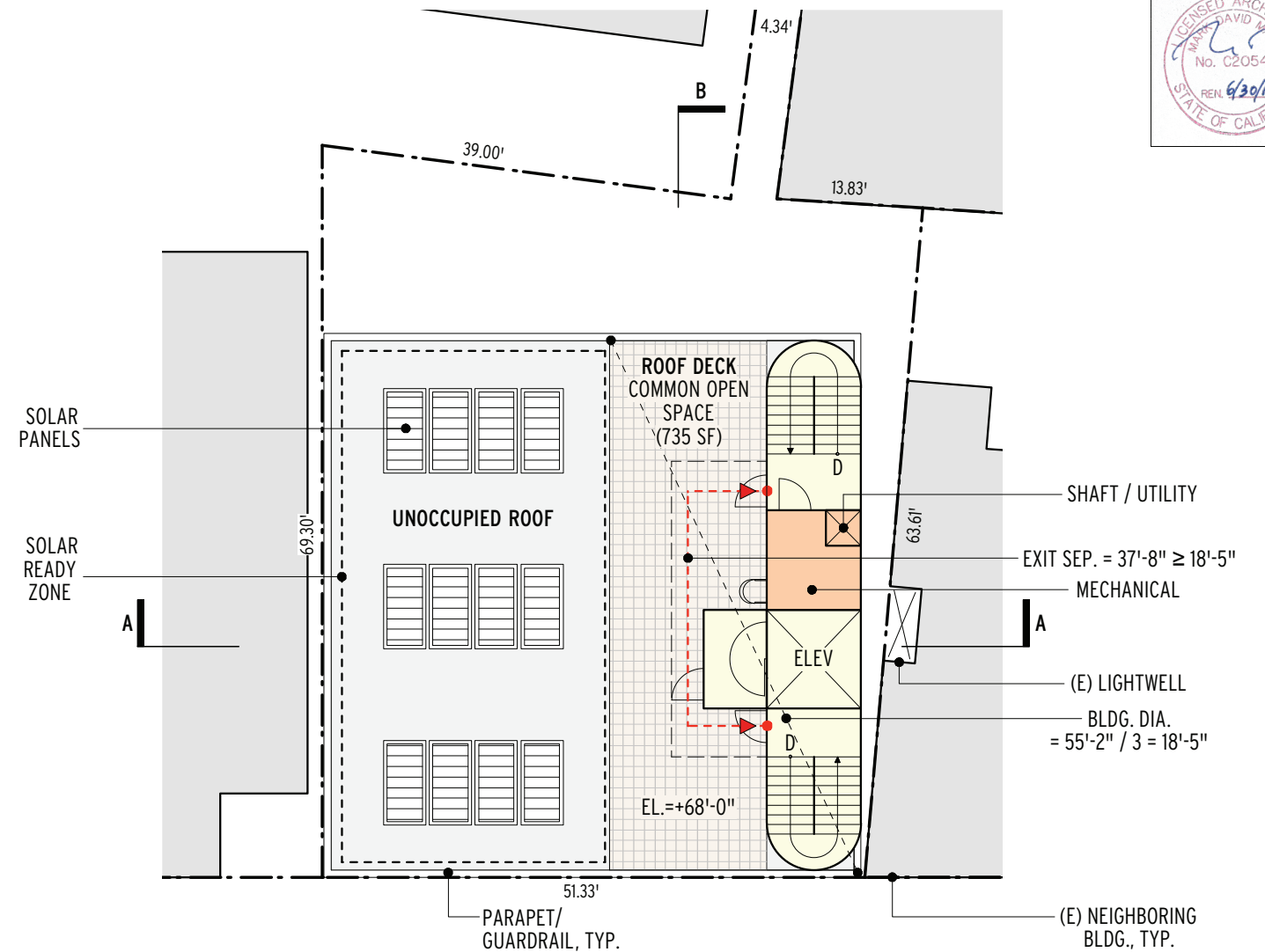
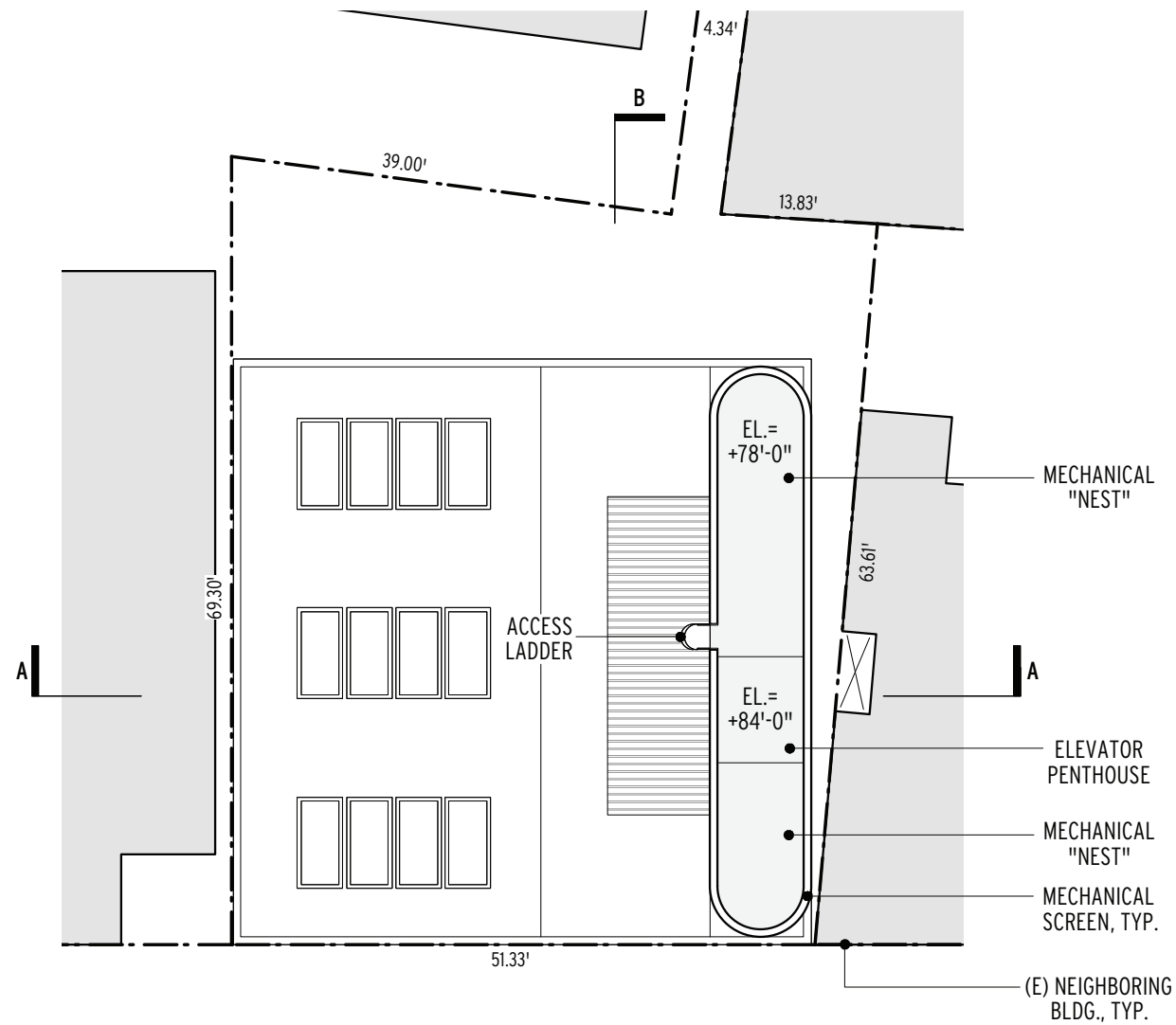
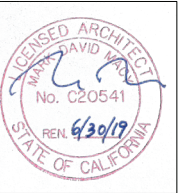
315 Linden Street San Francisco CA 94102 Tel 415 551 7630 www.macyarchitecture.com © 2019 Macy Architecture



ENVIRONMENTAL EVALUATION APPLICATION (EEA)
& STATE DENSITY BONUS APPLICATION
351-12TH STREET (BLOCK 352I LOT 022C) AFFORDABLE-WITHOUT-SUBSIDY RENTAL GROUP HOUSING

1321 Mission St, San Francisco, CA 94103 Tel 415.701.7000 www.panoramic.com

PAGE 14 OF 25



UPPER ROOF PLAN

ROOF PLAN

0' 16' 32' 48'

1/16" = 1'-0"



BONUS PROJECT PLAN DIAGRAMS

09/06/19

**MACY
ARCH
ITEC
TURE**

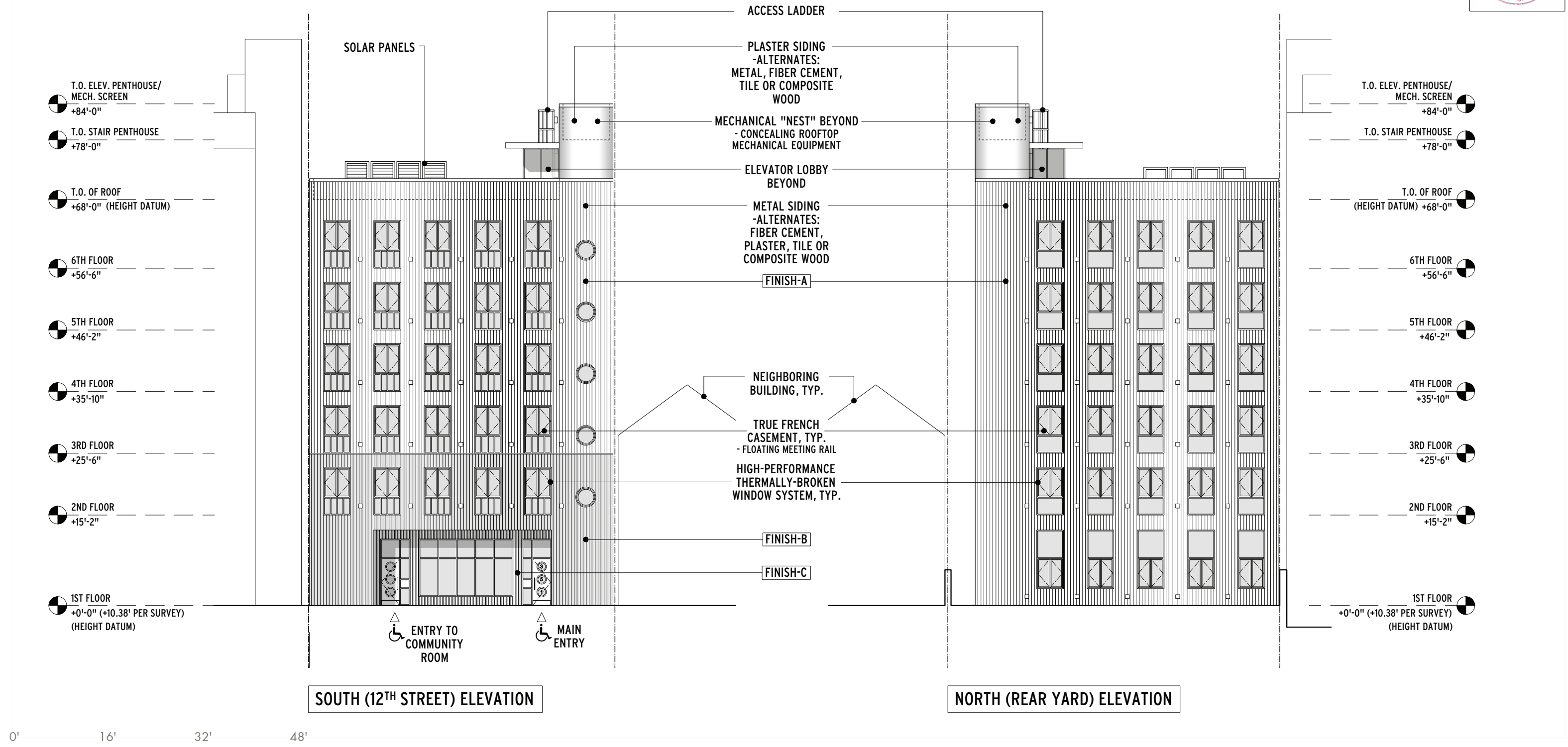
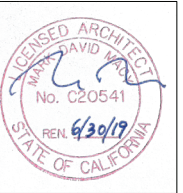
315 Linden Street San Francisco CA 94102 Tel 415 551 7630 www.macyarchitecture.com © 2019 Macy Architecture

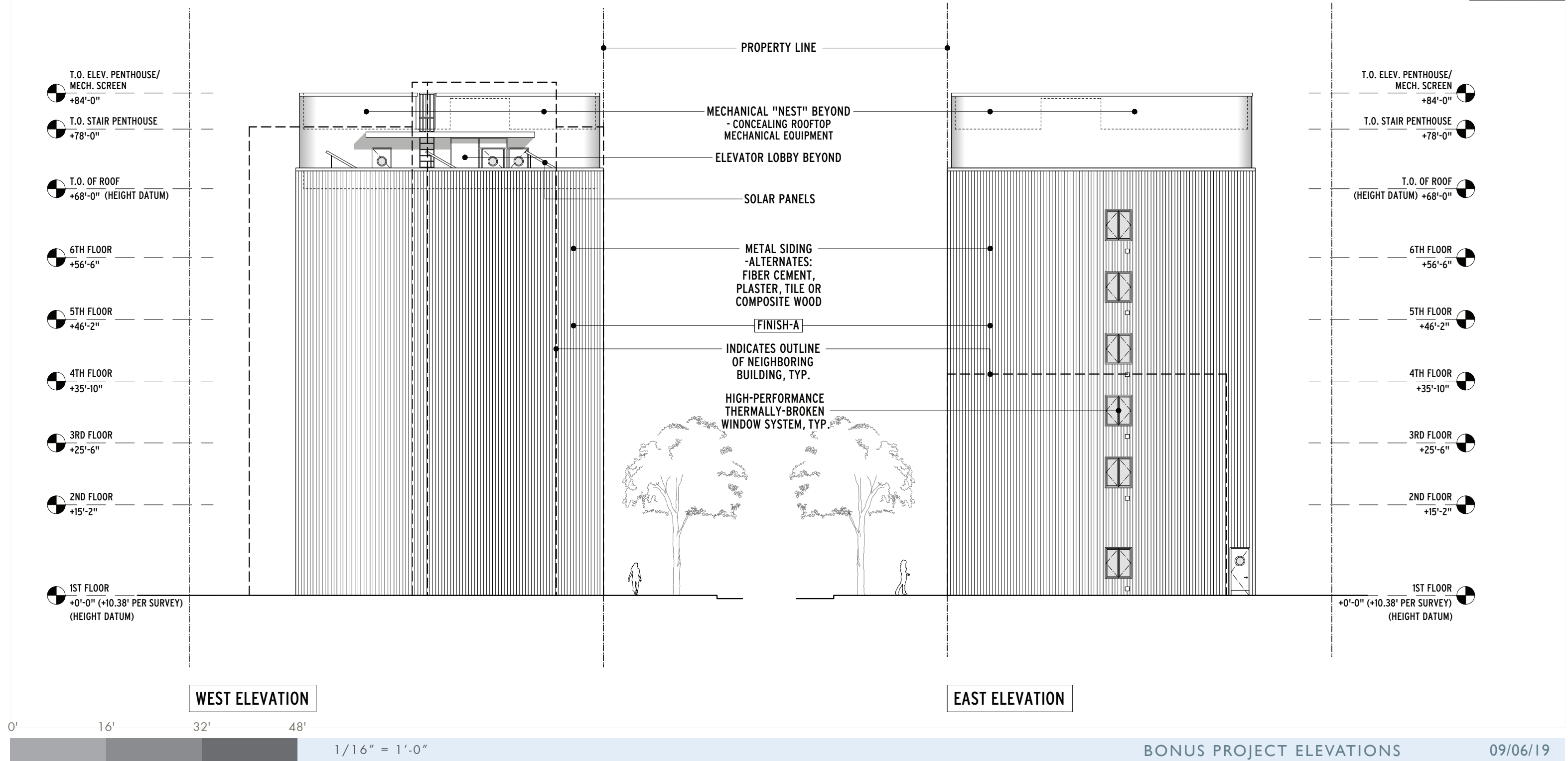
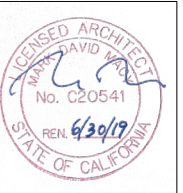


ENVIRONMENTAL EVALUATION APPLICATION (EEA)
& STATE DENSITY BONUS APPLICATION
351-12TH STREET (BLOCK 3521 LOT 022C) AFFORDABLE-WITHOUT-SUBSIDY RENTAL GROUP HOUSING

1321 Mission St, San Francisco, CA 94103 Tel 415.701.7000 www.panoramic.com

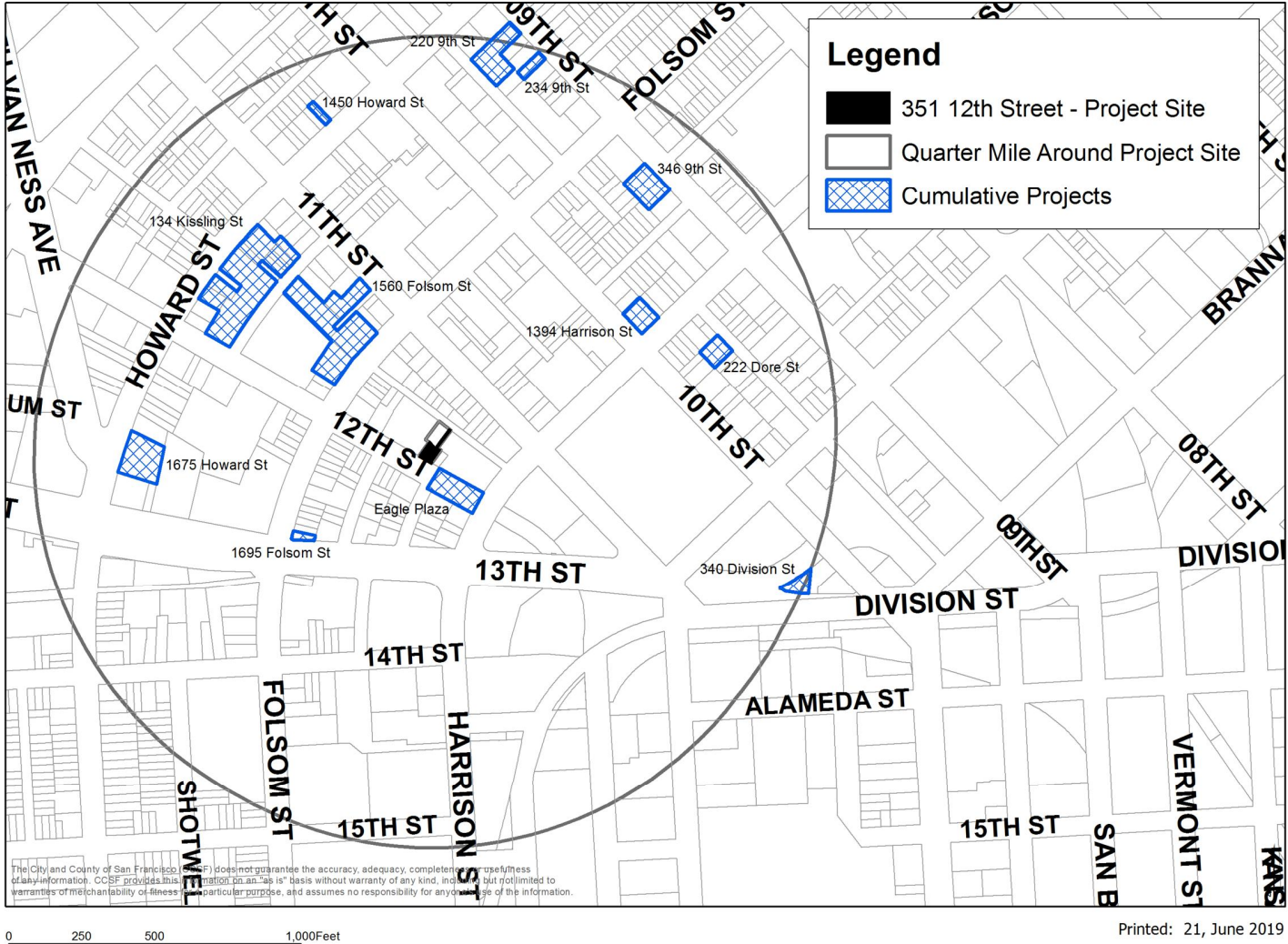
PAGE 15 OF 25





APPENDIX C: CUMULATIVE PROJECTS MAP

Figure 8 – Cumulative Projects Map – 351 12th Street





SAN FRANCISCO PLANNING DEPARTMENT

Land Use Information

PROJECT ADDRESS: 351 12TH ST
RECORD NO.: 2018-004545PRJ

1650 Mission St.
Suite 400
San Francisco,
CA 94103-2479

Reception:
415.558.6378

Fax:
415.558.6409

Planning
Information:
415.558.6377

	EXISTING	PROPOSED	NET NEW
GROSS SQUARE FOOTAGE (GSF)			
Parking GSF	3,909	0	-3,909
Residential GSF	0	15,559	15,559
TOTAL GSF	0	15,559	11,650
	EXISTING	NET NEW	TOTALS
PROJECT FEATURES (Units or Amounts)			
Number of Stories	0	6	6
Bicycle Spaces	0	20	20

	EXISTING	PROPOSED	NET NEW
LAND USE - RESIDENTIAL			
Group Housing - Rooms	0	48	48
Group Housing - Beds	0	48	48

This page intentionally blank.

EXHIBIT E

Parcel Map

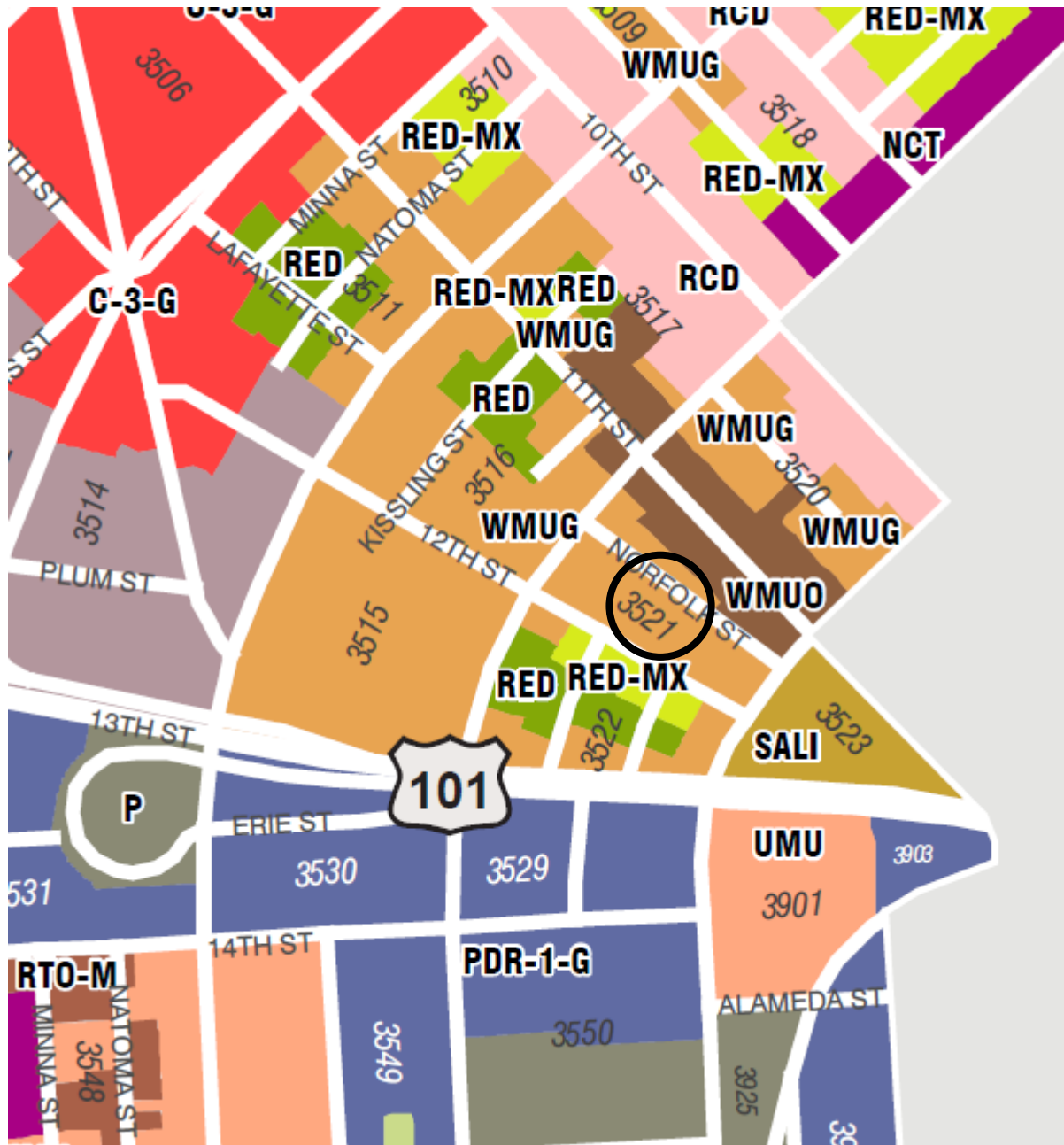
SAN FRANCISCO
PLANNING DEPARTMENT

Sanborn Map*



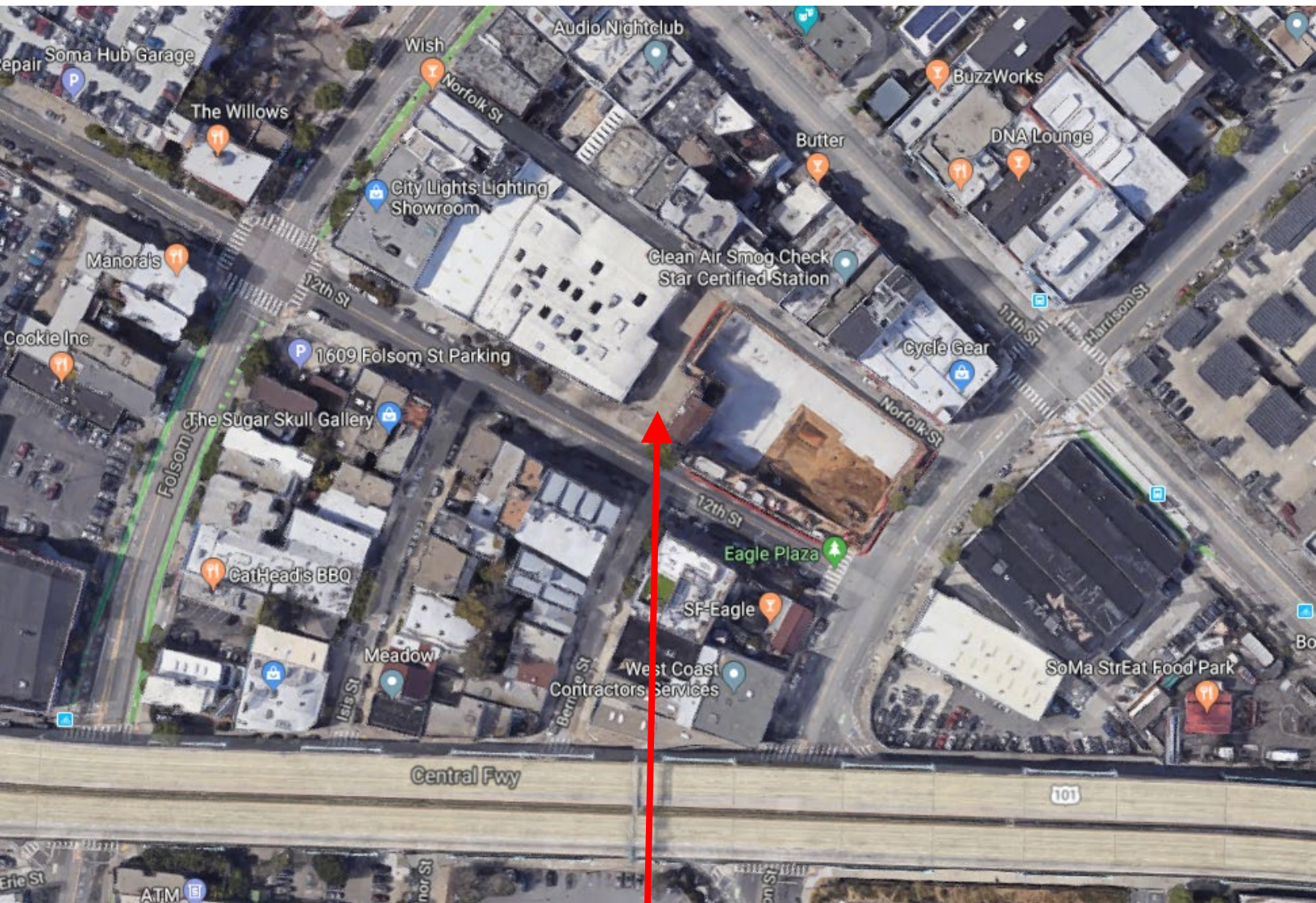
State Density Bonus Project Hearing
Case Number 2018-004545PRJ
351 12th Street

Zoning Map



State Density Bonus Project Hearing
Case Number 2018-004545PRJ
351 12th Street

Aerial Photo

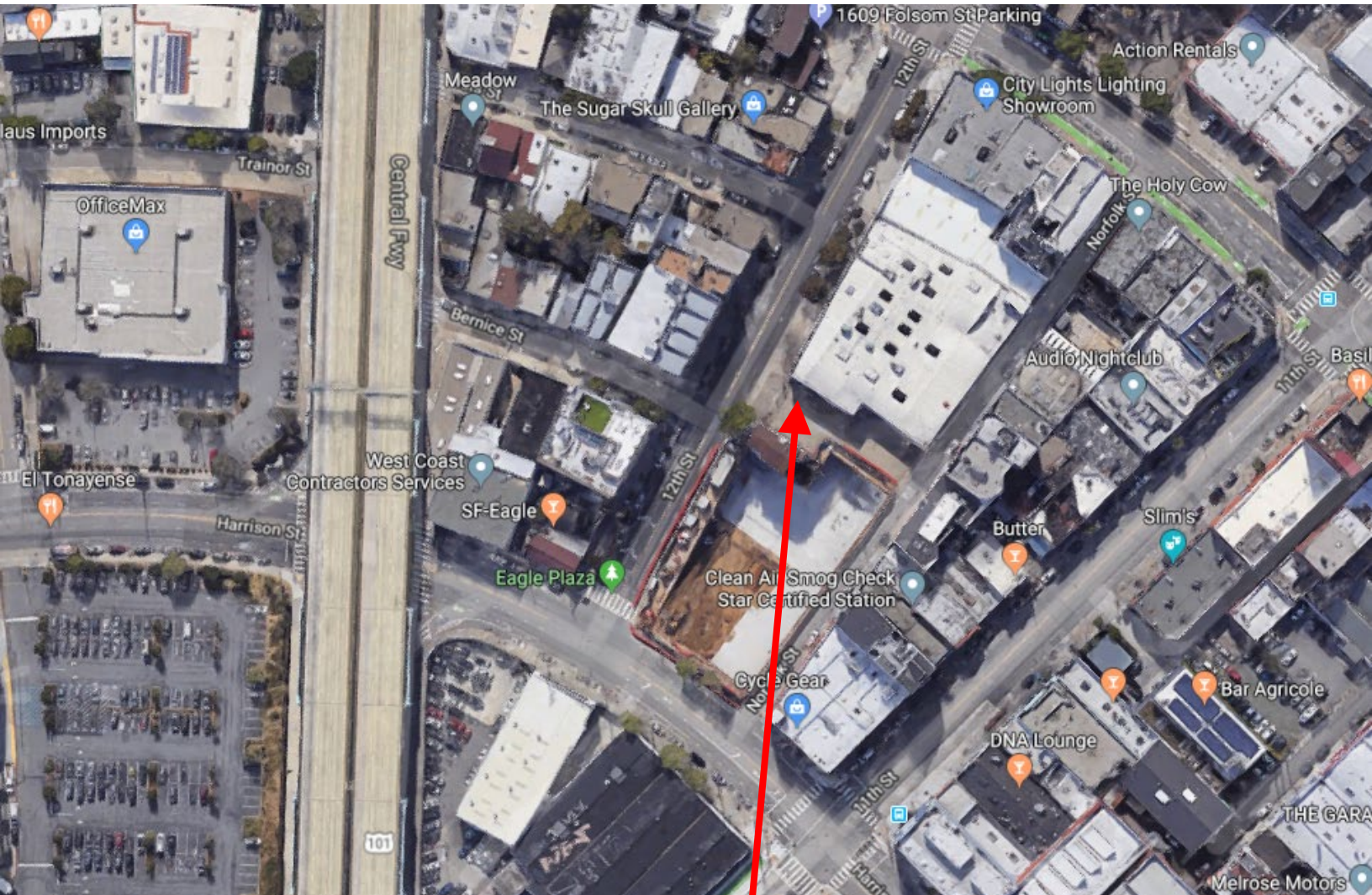


SUBJECT PROPERTY



State Density Bonus Project Hearing
Case Number 2018-004545PRJ
351 12th Street

Aerial Photo

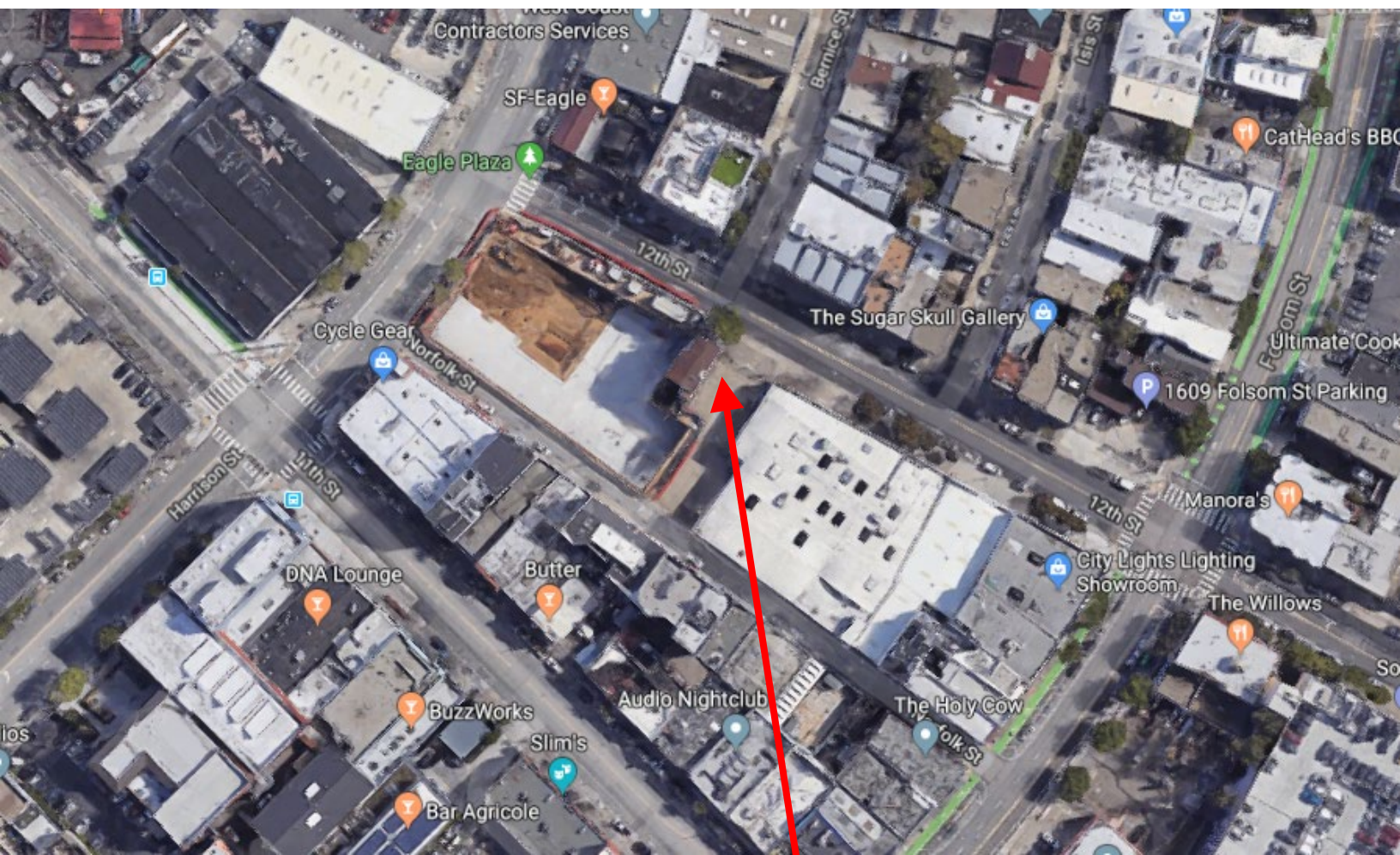


SUBJECT PROPERTY



State Density Bonus Project Hearing
Case Number 2018-004545PRJ
351 12th Street

Aerial Photo

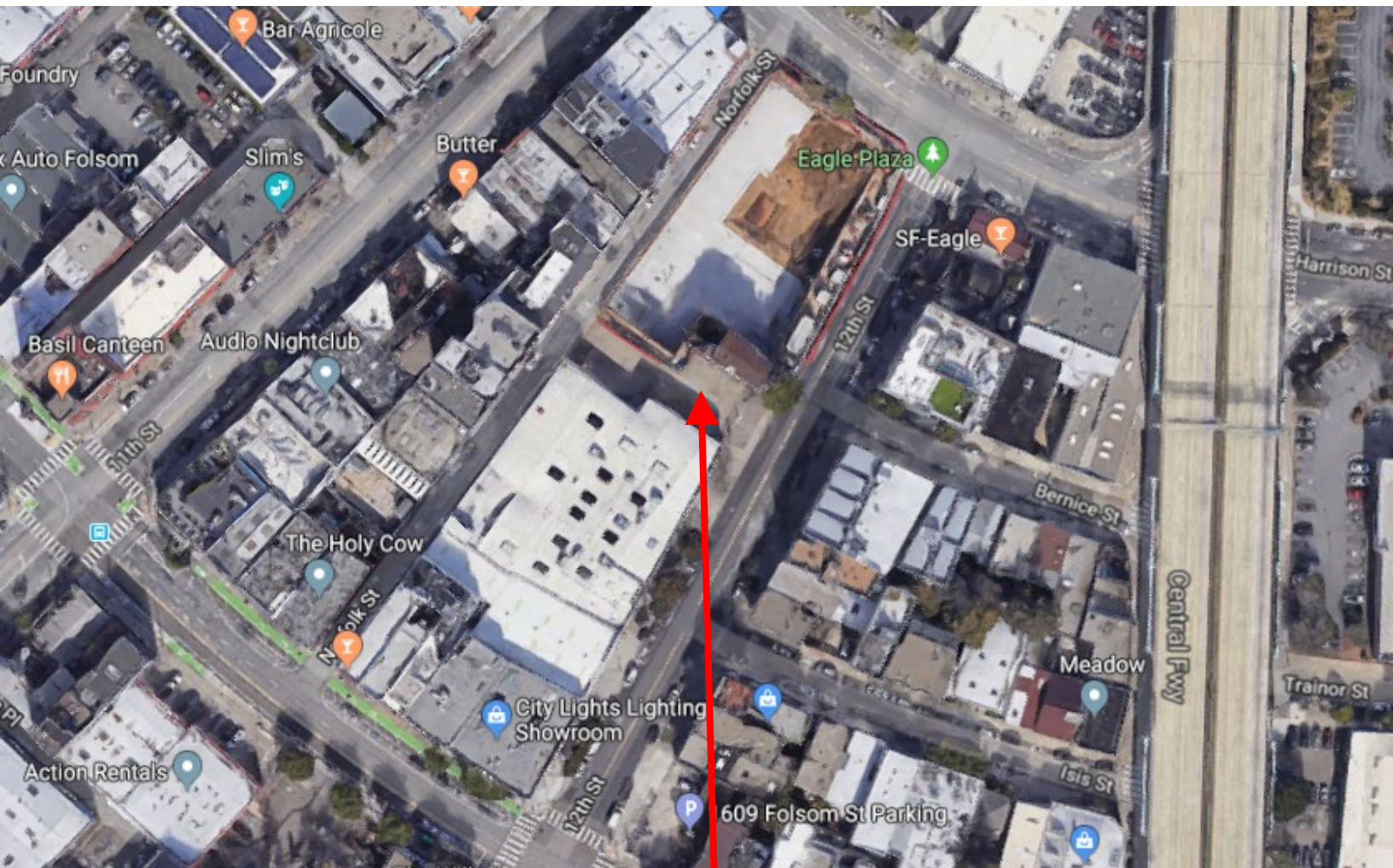


SUBJECT PROPERTY



State Density Bonus Project Hearing
Case Number 2018-004545PRJ
351 12th Street

Aerial Photo



SUBJECT PROPERTY



State Density Bonus Project Hearing
Case Number 2018-004545PRJ
351 12th Street

Site Photo



SUBJECT PROPERTY

State Density Bonus Project Hearing
Case Number 2018-004545PRJ
351 12th Street

EXHIBIT F

REUBEN, JUNIUS & ROSE, LLP

Daniel Frattin
dfrattin@reubenlaw.com

October 3, 2019

Delivered Via Email (veronica.flores@sfgov.org)

Myrna Melgar, Commission President
San Francisco Planning Commission
1650 Mission Street, 4th Floor
San Francisco, CA 94107

Re: 351 12th Street
Planning Case Number: 2018-004545
Hearing Date: October 17, 2019
Our File No.: 6954.15

Dear President Melgar and Commissioners:

This office represents Panoramic Interests (“Panoramic”), the sponsor of a project at 351 12th Street (the “Project”). The Project proposes to a 6-story residential project featuring 48 affordable-by-design group housing units, in an appropriate infill location currently underutilized as a parking and construction staging lot. The ground floor of the Project will include a common room with a full kitchen; each floor above will have a common lounge with a kitchenette. The rooms will come fully furnished with a private bathroom, fridge, and microwave. A prototype of the unit is available for tours and we would be happy to give you a walk through prior to the hearing on October 17, 2019. The Project will provide affordable units on site and is entitled to a density bonus under State Law. Because no other entitlements are required, this Project is on your consent calendar solely for the adoption of findings pursuant to the State Density Bonus Law.

Panoramic has been building high-density infill development projects in the Bay Area since 1990. Its work in downtown Berkeley and San Francisco includes 15 projects, adding more than a 1,000 new units of housing, and 100,000 square feet of commercial space. Panoramic has spent the last nine years designing and developing stylish and efficient urban housing called CITYSPACES®. The design ethos can be summarized in three words: More in Less. Panoramic has built various unit types over the years including studio apartments and multiple bedroom apartments. After building several projects with studio apartments in San Francisco, Panoramic concluded that studio apartments were too expensive to build and as a result could only be rented to the most affluent of tenants. Upon this realization, Panoramic began working on a group housing concept with smaller group housing rooms and shared spaces in the building. The end result is 351 12th Street, which is a pilot project for this new housing concept. This product could be used for a variety of tenants including formerly homeless, student housing, temporary housing, or long term housing for an urban minimalist.

San Francisco Office
One Bush Street, Suite 600, San Francisco, CA 94104
tel: 415-567-9000 | fax: 415-399-9480

Oakland Office
827 Broadway, 2nd Floor, Oakland, CA 94607
tel: 510-527-5589

www.reubenlaw.com

B. The Project Complies with the Density Bonus Law

The Project is proposed under the State Density Bonus Law.¹ The Density Bonus Law incentivizes the production of affordable housing, particularly in mixed-income projects. It entitles a project providing on-site affordable units to additional residential density above what would otherwise be permitted at the property. Planning department staff determined that a development at the Project site without any bonus density, i.e. a “base project,” consists of 40 group housing units. By setting aside units for low income households, the Project is entitled to a modest bonus of 8 additional units, for a total of 48 group housing units.

The Density Bonus Law requires the City to waive development standards that would physically preclude construction at the bonus density. In addition, certain Projects are entitled to “concessions or incentives” that help offset the cost of the subsidized units. Panoramic is not requesting any concessions or incentives. Rather, it is seeking waivers from the following development standards: (1) height; (2) rear yard; (3) usable open space; and (4) unit exposure. Compliance with these requirements would physically preclude the development at the density permitted under the Density Bonus Law.

As noted above, the Project does not require any other entitlements, and is only before the Planning Commission to adopt findings related to the density bonus. Specifically, that the Project is entitled to a density bonus and that the waivers are necessary to allow the permitted bonus density.

C. The Project Will Have Significant Public Benefits.

The Project’s public benefits program is significant. Among its contributions to the neighborhood and the City at large are the following:

- **Affordable by Design.** In addition to providing on-site affordable units, the Project’s market rate units are affordable by design. All units will feature highly efficient standardized plans that are optimized to achieve lower overhead costs—savings which will be passed on to occupants and reflected in the units’ relative affordability.
- **Impact Fees.** The Project will pay into a number of impact fees, supporting child care services, public schools, and transportation and infrastructure improvements. Specifically, the Project will be subject to the following fees: Child Care; Eastern Neighborhoods Infrastructure; Schools; and Transportation Sustainability.
- **Public Realm Improvements.** The Project will enhance the site by redeveloping an underutilized parking lot with a high-quality building, improving the pedestrian experience along 12th Street. In addition, the Project will add a landscaped rear yard that will

¹ California Government Code § 65915 *et. seq.*; Planning Code § 206.6.

complement the proposed open space for the adjacent building at 333 12th Street, creating a midblock open space on a block that has historically been lacking open space.

- **On-Site Affordable Housing.** Nineteen percent of the base scheme units will be below market rate, which will increase the affordable housing stock by eight units. Of the eight affordable housing units, four will be available to households at 55% AMI, two units will be available to households at 80% AMI, and two units will be available to households at 110% AMI. To promote diversity and inclusion, below market rate units will have similar finishes to market-rate units and will be evenly distributed throughout the building

D. Conclusion

The Project will improve the existing conditions on the site, adding much-needed affordable-by-design housing along with on-site affordable housing. The Project is consistent with applicable, objective development standards and, as confirmed by its Community Plan Exemption, will not cause significant impacts to public health or safety. As an appropriate infill residential project with on-site affordable units, the Project is entitled to its bonus density and a waiver of certain development standards under State Law. Consistent with this mandate, we urge you to adopt the density bonus findings.

Very truly yours,

REUBEN, JUNIUS & ROSE, LLP



Daniel Frattin

cc: Joel Koppel, Commission Vice-President
Frank S. Fung, Commissioner
Milicent A. Johnson, Commissioner
Rich Hillis, Commissioner
Kathrin Moore, Commissioner
Dennis Richards, Commissioner
Veronica Flores, Project Planner

This page intentionally blank.

COMPLIANCE WITH THE INCLUSIONARY AFFORDABLE HOUSING PROGRAM

PLANNING CODE SECTION 415, 417 & 419



**San Francisco
Planning**

SAN FRANCISCO PLANNING DEPARTMENT
1650 MISSION STREET, SUITE 400
SAN FRANCISCO, CA 94103-2479
MAIN: (415) 558-6378 SFPLANNING.ORG

7/18/2019

Date

I, Zac Shore,
do hereby declare as follows:

A The subject property is located at (address and block/lot):

351 12th Street

Address

3521/022C

Block / Lot

The subject property is located within the following Zoning District:

WMUG

Zoning District

55-X

Height and Bulk District

Special Use District, if applicable

Is the subject property located in the SOMA NCT, North of Market Residential SUD, or Mission Area Plan?

☐ Yes ☒ No

B The proposed project at the above address is subject to the *Inclusionary Affordable Housing Program*, Planning Code Section 415 and 419 et seq.

The Planning Case Number and/or Building Permit Number is:

2018-004545

Planning Case Number

201809261525

Building Permit Number

This project requires the following approval:

- ☐ Planning Commission approval (e.g. Conditional Use Authorization, Large Project Authorization)
- ☐ Zoning Administrator approval (e.g. Variance)

☒ This project is principally permitted.

The Current Planner assigned to my project within the Planning Department is:

Veronica Flores

Planner Name

A complete Environmental Evaluation Application or Project Application was accepted on:

3/29/2018

Date

The project contains 48 total dwelling units and/or group housing rooms.

This project is exempt from the *Inclusionary Affordable Housing Program* because:

- ☐ This project is 100% affordable.
- ☐ This project is 100% student housing.

Is this project in an UMU Zoning District within the Eastern Neighborhoods Plan Area?

☐ Yes ☐ No

(If yes, please indicate Affordable Housing Tier)

Is this project a HOME-SF Project?

☐ Yes ☐ No

(If yes, please indicate HOME-SF Tier)

Is this project an Analyzed or Individually Requested State Density Bonus Project?

☐ Yes ☐ No

C Please indicate the tenure of the project.

☐ **Ownership.** If affordable housing units are provided on-site or off-site, all affordable units will be sold as ownership units and will remain as ownership units for the life of the project. The applicable fee rate is the ownership fee rate.

☒ **Rental.** If affordable housing units are provided on-site or off-site, all affordable units will be rental units and will remain rental units for the life of the project. The applicable fee rate is the rental fee rate.

D This project will comply with the Inclusionary Affordable Housing Program by:

☐ Payment of the Affordable Housing Fee prior to the first construction document issuance (Planning Code Section 415.5)

☒ On-site Affordable Housing Alternative (Planning Code Sections 415.6)

☐ Off-site Affordable Housing Alternative (Planning Code Sections 415.7)

☐ Combination of payment of the Affordable Housing Fee and the construction of on-site or off-site units
(Planning Code Section 415.5 - required for Individually Requested State Density Bonus Projects)

☐ Eastern Neighborhoods Alternate Affordable Housing Fee (Planning Code Section 417)

☐ Land Dedication (Planning Code Section 419)

The applicable inclusionary rate is:

19%

On-site, off-site or fee rate as a percentage

If the method of compliance is the payment of the Affordable Housing Fee pursuant to Planning Code Section 415.5, please indicate the total residential gross floor area in the project.

Residential Gross Floor Area

E The Project Sponsor acknowledges that any change which results in the reduction of the number of on-site affordable units following the project approval shall require public notice for a hearing and approval by the Planning Commission.

F The Project Sponsor acknowledges that failure to sell or rent the affordable units or to eliminate the on-site or off-site affordable units at any time will require the Project Sponsor to:

- (1) Inform the Planning Department and the Mayor's Office of Housing and Community Development and, if applicable, fill out a new affidavit;
- (2) Record a new Notice of Special Restrictions; and
- (3) Pay the Affordable Housing Fee plus applicable interest (using the fee schedule in place at the time that the units are converted from ownership to rental units) and any applicable penalties by law.

G The Project Sponsor acknowledges that in the event that one or more rental units in the principal project become ownership units, the Project Sponsor shall notify the Planning Department of the conversion, and shall either reimburse the City the proportional amount of the Inclusionary Affordable Housing Fee equivalent to the then-current requirement for ownership units, or provide additional on-site or off-site affordable units equivalent to the then-current requirements for ownership units.

I For projects with over 25 units and with EEA's accepted between January 1, 2013 and January 12 2016, in the event that the Project Sponsor does not procure a building or site permit for construction of the principal project before December 7, 2018, rental projects will be subject to the on-site rate in effect for the Zoning District in 2017, generally 18% or 20%.

J For projects with EEA's/PRJ's accepted on or after January 12 2016, in the event that the Project Sponsor does not procure a building or site permit for construction of the principal project within 30 months of the Project's approval, the Project shall comply with the Inclusionary Affordable Housing Requirements applicable thereafter at the time the Sponsor is issued a site or building permit.

K If a Project Sponsor elects to completely or partially satisfy their Inclusionary Housing requirement by paying the Affordable Housing Fee, the Sponsor must pay the fee in full sum to the Development Fee Collection Unit at the Department of Building Inspection for use by the Mayor's Office of Housing prior to the issuance of the first construction document.

UNIT MIX TABLES

Number of All Units in PRINCIPAL PROJECT:

TOTAL UNITS:	SRO / Group Housing:	Studios:	One-Bedroom Units:	Two-Bedroom Units:	Three (or more) Bedroom Units:
48	48	0	0	0	0

If you selected the On-site, Off-Site, or Combination Alternative, please fill out the applicable section below. The On-Site Affordable Housing Alternative is required for HOME-SF Projects pursuant to Planning Code Section 206.4. State Density Bonus Projects that have submitted an Environmental Evaluation Application prior to January 12, 2016 must select the On-Site Affordable Housing Alternative. State Density Bonus Projects that have submitted an Environmental Evaluation Application on or after to January 12, 2016 must select the Combination Affordable Housing Alternative to record the required fee on the density bonus pursuant to Planning Code Section 415.3. If the Project includes the demolition, conversion, or removal of any qualifying affordable units, please complete the Affordable Unit Replacement Section.

☒ **On-site Affordable Housing Alternative** (Planning Code Section 415.6, 419.3, or 206.4): % of the unit total.

Number of Affordable Units to be Located ON-SITE:

TOTAL UNITS:	SRO / Group Housing:	Studios:	One-Bedroom Units:	Two-Bedroom Units:	Three (or more) Bedroom Units:
8	8	0	0	0	0

LOW-INCOME	Number of Affordable Units	% of Total Units	AMI Level
	4	11%	55% AMI
MODERATE-INCOME	Number of Affordable Units	% of Total Units	AMI Level
	2	4%	80% AMI
MIDDLE-INCOME	Number of Affordable Units	% of Total Units	AMI Level
	2	4%	110% AMI

☐ **Off-site Affordable Housing Alternative** (Planning Code Section 415.7 or 419.3): % of the unit total.

Number of Affordable Units to be Located OFF-SITE:

TOTAL UNITS:	SRO / Group Housing:	Studios:	One-Bedroom Units:	Two-Bedroom Units:	Three (or more) Bedroom Units:
Area of Dwellings in Principal Project (in sq. feet):		Off-Site Project Address:			
Area of Dwellings in Off-Site Project (in sq. feet):					
Off-Site Block/Lot(s):		Motion No. for Off-Site Project (if applicable):		Number of Market-Rate Units in the Off-site Project:	

AMI LEVELS:	Number of Affordable Units	% of Total Units	AMI Level

UNIT MIX TABLES: CONTINUED

- ☐ **Combination** of payment of a **fee**, **on-site affordable units**, or **off-site affordable units** with the following distribution:
Indicate what percent of each option will be implemented (from 0% to 99%) and the number of on-site and/or off-site below market rate units for rent and/or for sale.

1. On-Site % of affordable housing requirement.

If the project is a State Density Bonus Project, please enter "100%" for the on-site requirement field and complete the Density Bonus section below.

Number of Affordable Units to be Located ON-SITE:

TOTAL UNITS:	SRO / Group Housing:	Studios:	One-Bedroom Units:	Two-Bedroom Units:	Three (or more) Bedroom Units:

2. Off-Site % of affordable housing requirement.

Number of Affordable Units to be Located OFF-SITE:

TOTAL UNITS:	SRO / Group Housing:	Studios:	One-Bedroom Units:	Two-Bedroom Units:	Three (or more) Bedroom Units:
Area of Dwellings in Principal Project (in sq. feet):	Off-Site Project Address:				
Area of Dwellings in Off-Site Project (in sq. feet):					
Off-Site Block/Lot(s):	Motion No. for Off-Site Project (if applicable):		Number of Market-Rate Units in the Off-site Project:		

Income Levels for On-Site or Off-Site Units in Combination Projects:

AMI LEVELS:	Number of Affordable Units	% of Total Units	AMI Level

3. Fee % of affordable housing requirement.

Is this Project a State Density Bonus Project? ☒ Yes ☐ No

If yes, please indicate the bonus percentage, up to 35% 20%, and the number of bonus units and the bonus amount of residential gross floor area (if applicable) 8 units & 2,432 square feet

I acknowledge that Planning Code Section 415.4 requires that the Inclusionary Fee be charged on the bonus units or the bonus residential floor area. * This acknowledgement does not constitute a waiver of any rights to challenge imposition of the fee on bonus square footage under the density bonus law, the mitigation fee act, or other applicable laws.

Affordable Unit Replacement: Existing Number of Affordable Units to be Demolished, Converted, or Removed for the Project

TOTAL UNITS:	SRO / Group Housing:	Studios:	One-Bedroom Units:	Two-Bedroom Units:	Three (or more) Bedroom Units:

This project will replace the affordable units to be demolished, converted, or removed using the following method:

- ☐ On-site Affordable Housing Alternative
- ☐ Payment of the Affordable Housing Fee prior to the first construction document issuance
- ☐ Off-site Affordable Housing Alternative (Section 415.7)
- ☐ Combination of payment of the Affordable Housing Fee and the construction of on-site or off-site units (Section 415.5)

Contact Information and Declaration of Sponsor of PRINCIPAL PROJECTCity Gardens 333 LLC*Company Name*Zac Shore*Name (Print) of Contact Person*1321 Mission Street, Ste. 101*Address*San Francisco, CA 94103*City, State, Zip*415.701.7002*Phone / Fax*zac@panoramic.com*Email*

I am a duly authorized agent or owner of the subject property. I declare under penalty of perjury under the laws of the State of California that the foregoing is true and correct. I hereby declare that the information herein is accurate to the best of my knowledge and that I intend to satisfy the requirements of Planning Code Section 415 as indicated above.

Sign Here**Zac Shore, Director of Development***Signature:**Name (Print), Title:*

Executed on this day in:

*Location:*1321 Mission Street, Ste. 101, San Francisco, CA 94103*Date:*7/19/2019**Contact Information and Declaration of Sponsor of OFF-SITE PROJECT (If Different)***Company Name**Name (Print) of Contact Person**Address**City, State, Zip**Phone / Fax**Email*

I hereby declare that the information herein is accurate to the best of my knowledge and that I intend to satisfy the requirements of Planning Code Section 415 as indicated above.

Sign Here*Signature:**Name (Print), Title:*

This page intentionally blank.

EXHIBIT H

SUPPLEMENTAL INFORMATION FOR Anti-Discriminatory Housing Policy

1. Owner/Applicant Information

PROPERTY OWNER'S NAME:	
City Gardens 333 LLC	
PROPERTY OWNER'S ADDRESS:	TELEPHONE:
1321 Mission Street, Ste. 101	(415) 701-7002
San Francisco, CA 94103	EMAIL:
	zac@panoramic.com

APPLICANT'S NAME:	
Zac Shore	Same as Above <input type="checkbox"/>
APPLICANT'S ADDRESS:	TELEPHONE:
1321 Mission Street, Ste. 101	(415) 701-7002
San Francisco, CA 94103	EMAIL:
	zac@panoramic.com

CONTACT FOR PROJECT INFORMATION:	
	Same as Above <input checked="" type="checkbox"/>
ADDRESS:	TELEPHONE:
	()
	EMAIL:

COMMUNITY LIAISON FOR PROJECT (PLEASE REPORT CHANGES TO THE ZONING ADMINISTRATOR):	
	Same as Above <input type="checkbox"/>
ADDRESS:	TELEPHONE:
	()
	EMAIL:

2. Location and Project Description

STREET ADDRESS OF PROJECT:		ZIP CODE:
351 12th Street, San Francisco, CA 94103		
CROSS STREETS:		
Norfolk Street/12th Street		
ASSESSORS BLOCK/LOT:	ZONING DISTRICT:	HEIGHT/BULK DISTRICT:
3521 / 022C	WMUG	55-X

PROJECT TYPE: (Please check all that apply)	EXISTING DWELLING UNITS:	PROPOSED DWELLING UNITS:	NET INCREASE:
<input checked="" type="checkbox"/> New Construction	0	48	48
<input type="checkbox"/> Demolition			
<input type="checkbox"/> Alteration			
<input type="checkbox"/> Other: _____			

Compliance with the Anti-Discriminatory Housing Policy

1. Does the applicant or sponsor, including the applicant or sponsor's parent company, subsidiary, or any other business or entity with an ownership share of at least 30% of the applicant's company, engage in the business of developing real estate, owning properties, or leasing or selling individual dwelling units in States or jurisdictions outside of California?

☐ YES ☒ NO

1a. If yes, in which States? _____

- 1b. If yes, does the applicant or sponsor, as defined above, have policies in individual States that prohibit discrimination based on sexual orientation and gender identity in the sale, lease, or financing of any dwelling units enforced on every property in the State or States where the applicant or sponsor has an ownership or financial interest?

☐ YES ☐ NO

- 1c. If yes, does the applicant or sponsor, as defined above, have a national policy that prohibits discrimination based on sexual orientation and gender identity in the sale, lease, or financing of any dwelling units enforced on every property in the United States where the applicant or sponsor has an ownership or financial interest in property?

☐ YES ☐ NO

If the answer to 1b and/or 1c is yes, please provide a copy of that policy or policies as part of the supplemental information packet to the Planning Department.

Human Rights Commission contact information
hrc.info@sfgov.org or (415)252-2500

Applicant's Affidavit

Under penalty of perjury the following declarations are made:

- a: The undersigned is the owner or authorized agent of the owner of this property.
- b: The information presented is true and correct to the best of my knowledge.
- c: Other information or applications may be required.

Signature: _____



Date: 7/19/2019

Print name, and indicate whether owner, or authorized agent:

Zac Shore

Owner / Authorized Agent (circle one)

PLANNING DEPARTMENT USE ONLY

PLANNING DEPARTMENT VERIFICATION:

- ☐ Anti-Discriminatory Housing Policy Form is **Complete**
- ☐ Anti-Discriminatory Housing Policy Form is **Incomplete**

Notification of Incomplete Information made:

To: _____ Date: _____

BUILDING PERMIT NUMBER(S):	DATE FILED:
RECORD NUMBER:	DATE FILED:
VERIFIED BY PLANNER:	
Signature: _____ Date: _____	
Printed Name: _____ Phone: _____	
ROUTED TO HRC:	DATE:
<input type="checkbox"/> Emailed to: _____	

This page intentionally blank.

EXHIBIT I



SAN FRANCISCO
PLANNING
DEPARTMENT

AFFIDAVIT FOR FIRST SOURCE HIRING PROGRAM

Administrative Code

Chapter 83

1650 Mission Street, Suite 400 • San Francisco CA 94103-2479 • 415.558.6378 • <http://www.sfplanning.org>

Section 1: Project Information

PROJECT ADDRESS		BLOCK/LOT(S)	
351 12th Street		3521/022C	
BUILDING PERMIT APPLICATION NO.		CASE NO. (IF APPLICABLE)	MOTION NO. (IF APPLICABLE)
201809261525		2018-004545	
PROJECT SPONSOR		MAIN CONTACT	PHONE
City Gardens 333 LLC		Zac Shore	415.701.7002
ADDRESS			
1321 Mission Street, Ste. 101			
CITY, STATE, ZIP		EMAIL	
San Francisco, California, 94103		zac@panoramic.com	
ESTIMATED RESIDENTIAL UNITS	ESTIMATED SQ FT COMMERCIAL SPACE	ESTIMATED HEIGHT/FLOORS	ESTIMATED CONSTRUCTION COST
48	0	68'-0" / 6 stories	\$3.1 million
ANTICIPATED START DATE			
September 2019			

Section 2: First Source Hiring Program Verification

CHECK ALL BOXES APPLICABLE TO THIS PROJECT	
<input checked="" type="checkbox"/>	Project is wholly Residential
<input type="checkbox"/>	Project is wholly Commercial
<input type="checkbox"/>	Project is Mixed Use
<input checked="" type="checkbox"/>	A: The project consists of ten (10) or more residential units;
<input type="checkbox"/>	B: The project consists of 25,000 square feet or more gross commercial floor area.
<input type="checkbox"/>	C: Neither 1A nor 1B apply.
NOTES:	
<ul style="list-style-type: none">• If you checked C, this project is <u>NOT</u> subject to the First Source Hiring Program. Sign Section 4: Declaration of Sponsor of Project and submit to the Planning Department.• If you checked A or B, your project <u>IS</u> subject to the First Source Hiring Program. Please complete the reverse of this document, sign, and submit to the Planning Department prior to any Planning Commission hearing. If principally permitted, Planning Department approval of the Site Permit is required for all projects subject to Administrative Code Chapter 83.• For questions, please contact OEWD's CityBuild program at CityBuild@sfgov.org or (415) 701-4848. For more information about the First Source Hiring Program visit www.workforcedevelopmentsf.org• If the project is subject to the First Source Hiring Program, you are required to execute a Memorandum of Understanding (MOU) with OEWD's CityBuild program prior to receiving construction permits from Department of Building Inspection.	

Continued...

Section 3: First Source Hiring Program – Workforce Projection

Per Section 83.11 of Administrative Code Chapter 83, it is the developer's responsibility to complete the following information to the best of their knowledge.


Provide the estimated number of employees from each construction trade to be used on the project, indicating how many are entry and/or apprentice level as well as the anticipated wage for these positions.

Check the anticipated trade(s) and provide accompanying information (Select all that apply):

TRADE/CRAFT	ANTICIPATED JOURNEYMAN WAGE	# APPRENTICE POSITIONS	# TOTAL POSITIONS	TRADE/CRAFT	ANTICIPATED JOURNEYMAN WAGE	# APPRENTICE POSITIONS	# TOTAL POSITIONS
Abatement Laborer				Laborer			
Boilermaker				Operating Engineer			
Bricklayer				Painter			
Carpenter				Pile Driver			
Cement Mason	TBD when GC is hired			Plasterer	TBD when GC is hired		
Drywall/Latherer				Plumber and Pipefitter			
Electrician				Roofer/Water proofer			
Elevator Constructor				Sheet Metal Worker			
Floor Coverer				Sprinkler Fitter			
Glazier				Taper			
Heat & Frost Insulator				Tile Layer/ Finisher			
Ironworker				Other:			
		TOTAL:				TOTAL:	

- | | | |
|--|--------------------------|-------------------------------------|
| | YES | NO |
| 1. Will the anticipated employee compensation by trade be consistent with area Prevailing Wage? | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| 2. Will the awarded contractor(s) participate in an apprenticeship program approved by the State of California's Department of Industrial Relations? | <input type="checkbox"/> | <input checked="" type="checkbox"/> |
| 3. Will hiring and retention goals for apprentices be established? | <input type="checkbox"/> | <input type="checkbox"/> |
| 4. What is the estimated number of local residents to be hired? | <hr/> | |

Section 4: Declaration of Sponsor of Principal Project

PRINT NAME AND TITLE OF AUTHORIZED REPRESENTATIVE	EMAIL	PHONE NUMBER
Zac Shore, Director of Development	zac@panoramic.com	415.701.7002
<p>I HEREBY DECLARE THAT THE INFORMATION PROVIDED HEREIN IS ACCURATE TO THE BEST OF MY KNOWLEDGE AND THAT I COORDINATED WITH OEWD'S CITYBUILD PROGRAM TO SATISFY THE REQUIREMENTS OF ADMINISTRATIVE CODE CHAPTER 83.</p>		
 (SIGNATURE OF AUTHORIZED REPRESENTATIVE)		7/19/2019 (DATE)
<p>FOR PLANNING DEPARTMENT STAFF ONLY: PLEASE EMAIL AN ELECTRONIC COPY OF THE COMPLETED AFFIDAVIT FOR FIRST SOURCE HIRING PROGRAM TO OEWD'S CITYBUILD PROGRAM AT CITYBUILD@SFGOV.ORG</p>		
<p>Cc: Office of Economic and Workforce Development, CityBuild Address: 1 South Van Ness 5th Floor San Francisco, CA 94103 Phone: 415-701-4848 Website: www.workforcedevelopmentsf.org Email: CityBuild@sfgov.org</p>		