



EXECUTIVE SUMMARY

CONDITIONAL USE AUTHORIZATION AND STATE DENSITY BONUS FINDINGS

HEARING DATE: DECEMBER 2, 2021

CONTINUED FROM: NOVEMBER 4, 2021

Record No.: 2017-015678CUA
Project Address: 425 Broadway
Zoning: Broadway Neighborhood Commercial District (NCD) Zoning District
65-A-1 Height and Bulk District
Special District for Sign Illumination
Block/Lot: 0163/002
Project Sponsor: Montgomery Place LLC
1630 Oakland Rd, #A215
San Jose, CA 95131
Property Owner: Montgomery Place LLC
1630 Oakland Rd, #A215
San Jose, CA 95131
Staff Contact: Claudine Asbagh – (628) 652-7329
claudine.asbagh@sfgov.org

Recommendation: Approval with Conditions

Project Description

The Project includes demolition and construction of two new mixed-use buildings reaching heights of 5-stories (56 feet) tall on Broadway and 7-stories (64 feet) tall on Montgomery St. with approximately 52,365 gross square feet of residential use, 4,940 gross square feet of retail use, and 17,995 gross square feet of design professional office use. The proposed project includes a total of 42 dwelling units, with a mix of 16 one-bedroom units, 21 two-bedroom units, and five three-bedroom units with six dwelling units provided as on-site affordable units. The Project would provide 17 off-street vehicle parking spaces and 47 Class 1 and seven Class 2 bicycle parking spaces. The Project is utilizing the Individually Requested State Density Bonus Program to achieve a 21.5% density bonus thereby maximizing residential density on the site.

Required Commission Action

In order for the Project to proceed, the Commission must grant a Conditional Use Authorization, pursuant to Planning Code Sections 121.1, 121.2, 253.1, 303, and 714 to allow the development of a lot size exceeding 4,999 sf, use sizes exceeding 2,999 sf, and to construct buildings greater than 40 feet in height in the Broadway NCD Zoning District and 65-A-1 Height and Bulk District.

Additionally, the Commission must make findings related to the requested incentive for Maximum Use Size (121.1 and 714) and waivers from development standards in the Planning Code for Rear Yard (134), Dwelling Unit Exposure (140), and Bulk Limits (270), pursuant to the State Density Bonus Law.

Issues and Other Considerations

- **State Density Bonus Law, Incentives/Concessions, and Waivers.** The project is located in the Broadway NCD that allows 1 unit per 400 square feet of lot area, or the density permitted in the nearest R District, whichever is greater. In this instance, 1 unit per 400 square feet allows up to 34 units to establish the base density. The Project provides 11% of the 34 dwelling units in the base project (4 units) as affordable to low-income households, defined as those earning up to 80% of area median income and is therefore entitled to a 21.5% density bonus and one incentive under California Government Code Sections 65915-95918, as revised under AB 2345.
 - The project has requested an Incentive/Concession from the maximum non-residential use sizes in the Broadway NCD. The sponsor has demonstrated that the incentive results in identifiable cost reductions for constructing the density bonus project.
 - As mentioned above the project requires waivers from the Planning Code requirements for: minimum rear yard, exposure, and bulk limits.
- **Inclusionary Affordable Housing Program: On-Site.** Planning Code Section 415 sets forth the requirements and procedures for the Inclusionary Affordable Housing Program. Under Planning Code Section 415.3, these requirements apply to projects that consist of 10 or more units. Pursuant to Planning Code Section 415.5 and 415.6, the Inclusionary Affordable Housing Program requirement for the On-site Affordable Housing Alternative is to provide 20% of proposed dwelling units (or 6.8 dwelling units) as affordable with a minimum of 10% of the units affordable to low-income households, 5% of the units affordable to moderate-income households, and the remaining 5% of the units affordable to middle-income households, as defined by the Planning Code and Procedures Manual. The remaining inclusionary affordable housing obligation for the 42-unit project will be satisfied through payment of the Affordable Housing Fee.
- **Public Comment & Outreach.**
 - **Support/Opposition:** The Department has received correspondence from 6 groups regarding the proposed project. Four letters (North Beach Neighbors, Broadway Studios, Housing Action Coalition and YIMBY Action) have expressed support citing the need for more housing and the project's compatibility with the neighborhood. Two letters (Telegraph Hill Dwellers and Chinatown Community Development Corporation (CCDC)) have expressed concerns over the project's height, bulk, and compatibility with the neighborhood. Additionally, concerns over impact to the residents of the SRO located at 401 Broadway have also been raised. The project was continued to allow the project team to meet with neighborhood groups.
 - **Outreach:** The Sponsor has attended over 20 meetings within the community, since March 2018.

- **Entertainment Commission Comments:** The Entertainment Commission has adopted a set of standard conditions of approval known as “Recommended Noise Attenuation Conditions for Chapter 116 Projects.” These conditions have been added to the motion.

Environmental Review

The Project is exempt from the California Environmental Quality Act (“CEQA”) as a Class 32 categorical exemption.

Basis for Recommendation

The Department finds that the Project is, on balance, consistent with the Objectives and Policies of the General Plan. The Project provides a substantial amount of new housing, including new on-site below-market rate units, which is a goal for the City. The Department also finds the project to be necessary, desirable, and compatible with the surrounding neighborhood, and not to be detrimental to persons or adjacent properties in the vicinity.

Attachments:

Draft Motion – Conditional Use Authorization with Conditions of Approval
Exhibit B – Plans and Renderings
Exhibit C – Environmental Determination
Exhibit D – Land Use Data
Exhibit E – Maps and Context Photos
Exhibit G - Project Sponsor Brief
Exhibit H – Inclusionary Affordable Housing Affidavit
Exhibit I – Anti-Discriminatory Housing Affidavit
Exhibit K – First Source Hiring Affidavit



PLANNING COMMISSION DRAFT MOTION

HEARING DATE: December 2, 2021

Record No.: 2017-015678CUA
Project Address: 425 BROADWAY
Zoning: Broadway NCD (Neighborhood Commercial) Zoning District
65-A-1 Height and Bulk District
Special District for Sign Illumination
Block/Lot: 0163/002
Project Sponsor: Montgomery Place LLC
1630 Oakland Rd, #A215
San Jose, CA 95131
Property Owner: Montgomery Place LLC
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San Jose, CA 95131
Staff Contact: Claudine Asbagh – (628) 652-7329
Claudine.asbagh@sfgov.org

ADOPTING FINDINGS: 1) TO APPROVE A CONDITIONAL USE AUTHORIZATION PURSUANT TO PLANNING CODE SECTIONS 303 AND OTHER APPLICABLE SECTIONS TO DEVELOP A LOT THAT EXCEEDS 4,999 SF (121.1), ALLOW NON-RESIDENTIAL USE SIZE GREATER THAN 2,999 SF (121.2), AND TO CONSTRUCT TWO BUILDINGS GREATER THAN 40 FEET IN HEIGHT (253.1) WITHIN THE BROADWAY NEIGHBORHOOD COMMERCIAL (NCD) ZONING DISTRICT AND 65-A-1 HEIGHT AND BULK DISTRICT; 2) FOR AN INDIVIDUALLY REQUESTED STATE DENSITY BONUS PROJECT PURSUANT TO PLANNING CODE 206.6; AND 3) UNDER THE CALIFORNIA ENVIRONMENTAL QUALITY ACT.

THE PROJECT WOULD DEMOLISH AN EXISTING SURFACE PARKING LOT AND CONSTRUCT TWO NEW MIXED-USE BUILDINGS REACHING HEIGHTS OF 5-STORIES (56 FEET) TALL ON BROADWAY AND 7-STORIES (64 FEET) TALL ON MONTGOMERY STREET WITH 42 DWELLING UNITS (52,365 GSF), 4,940 GSF OF RETAIL USE, AND 17,995 GSF OF DESIGN PROFESSIONAL OFFICE USE. THE PROJECT WOULD PROVIDE 17 OFF-STREET VEHICLE PARKING SPACES AND 47 CLASS 1 AND SEVEN CLASS 2 BICYCLE PARKING SPACES. THE PROJECT WOULD UTILIZE THE STATE DENSITY BONUS LAW (CALIFORNIA GOVERNMENT CODE SECTIONS 65915- 65918) AND RECEIVE ONE INCENTIVE FROM MAXIMUM USE SIZE (SECTIONS 121.1, 714) AND WAIVERS FROM THE PLANNING CODE REQUIREMENTS FOR: REAR YARD (SECTION 134), BULK (SECTION 270), AND DWELLING UNIT EXPOSURE (SECTION 140).

PREAMBLE

On December 8, 2017 Montgomery Place LLC(hereinafter "Project Sponsor"), submitted an application with the Planning Department ("Department") for a Preliminary Project Assessment ("PPA") related to the proposed project ("Project") located at 425 Broadway, Block 0163 Lot 002 (hereinafter "Project Site"). The PPA Letter, assigned to Case No. 2017-015678PPA, was issued on March 5, 2018.

On or after December 22, 2017, the Project Sponsor filed an Environmental Evaluation Application with the Department. The application packet was deemed accepted on or after December 22, 2017 and assigned Case Number 2017-015678ENV.

On or after January 31, 2019, the Project Sponsor submitted the following applications with the Department: Conditional Use Authorization and Transportation Demand Management. The application packets were deemed accepted on or after January 31, 2019 and assigned Case Numbers: 2017-015678CUA and 2017-015678TDM, respectively.

The Project involves the construction of two new mixed-use buildings reaching heights of 5-stories (56 feet) tall on Broadway and 7-stories (64 feet) tall on Montgomery St. with approximately 52,365 gross square feet of residential use, 4,940 gross square feet of retail use, and 17,995 gross square feet of design professional office use. The proposed project ("Project") includes a total of 42 dwelling units, with a mix of 16 one-bedroom units, 21 two-bedroom units, and five three-bedroom units with five dwelling units provided as on-site affordable units. The Project would provide 17 off-street vehicle parking spaces and 47 Class 1 and seven Class 2 bicycle parking spaces (hereinafter "Project"). Project Sponsor seeks to utilize the State Density Bonus Law, Government Code Section 65915 et seq ("the State Law"), as amended under Assembly Bill No. 2345 (AB-2345). Under the State Law, a housing development that includes affordable housing is entitled to additional density, concessions and incentives, and waivers from development standards that might otherwise preclude the construction of the project.

On September 22, 2021 the Project was determined to be exempt from the California Environmental Quality Act ("CEQA") as a Class 32 Categorical Exemption under CEQA as described in the determination contained in the Planning Department files for this Project.

On December 2, 2021, the Commission conducted a duly noticed public hearing at a regularly scheduled meeting on Conditional Use Authorization Application No. 2017-015678CUA.

The Planning Department Commission Secretary is the Custodian of Records; the File for Record No. 2017-015678CUA is located at 49 South Van Ness Avenue, Suite 1400, San Francisco, California.

The Commission has heard and considered the testimony presented to it at the public hearing and has further considered written materials and oral testimony presented on behalf of the applicant, Department staff, and other interested parties.

MOVED, that the Commission hereby approves the Conditional Use Authorization as requested in Application No. 2017-015678CUA, subject to the conditions contained in "EXHIBIT A" of this motion, based on the following findings:

FINDINGS

Having reviewed the materials identified in the preamble above, and having heard all testimony and arguments, this Commission finds, concludes, and determines as follows:

1. **The above recitals are accurate and constitute findings of this Commission.**
2. **Project Description.** The Project includes demolition of an existing parking structure and construction of two new mixed-use buildings reaching heights of 5-stories (56 feet) tall on Broadway and 7-stories (64 feet) tall on Montgomery St. with approximately 52,365 gross square feet of residential use, 4,940 gross square feet of retail use, and 17,995 gross square feet of design professional office use. The proposed project ("Project") includes a total of 42 dwelling units, with a mix of 16 one-bedroom units, 21 two-bedroom units, and five three-bedroom units with five dwelling units provided as on-site affordable units. The Project would provide 17 off-street vehicle parking spaces and 47 Class 1 and seven Class 2 bicycle parking spaces. The Project seeks to utilize the State Density Bonus Law, Government Code Section 65915 et seq ("the State Law"), as amended under Assembly Bill No. 2345 (AB-2345).
3. **Site Description and Present Use.** The Project Site ("Site") is an approximately 13,641 square feet L-shaped lot located at 425 Broadway between Montgomery and Kearny Streets, which has approximately 79-ft of frontage along Broadway and 47.5-ft of frontage along Montgomery Street. The existing Site contains a commercial parking structure accommodating 144 parking spaces
4. **Surrounding Properties and Neighborhood.** The Project Site is located within the Broadway NCD Zoning Districts and the Jackson Square Historic District Extension. The immediate context is mixed in character with residential, commercial, and institutional uses. The immediate neighborhood includes two-to-five-story residential, office, and commercial development. Other zoning districts in the vicinity of the project site include: C-2 (Commercial), P (Public), RH-3 (Residential medium density), and RM-2 (Residential Mixed-Use) Zoning Districts.
5. **Public Outreach and Comments.** The Department has received correspondence from 5 groups regarding the proposed project. Four letters from North Beach Neighbors, Broadway Studios, Housing Action Coalition and YIMBY Action have expressed both support and one letter from Telegraph Hill Dwellers has expressed opposition to the project. The opposition expressed concerns over the project's height, bulk, and compatibility with the neighborhood, while support expressed the need for more housing units and the Project's compatibility with the neighborhood and adjacent place of entertainment. The Sponsor has attended over 20 meetings within the community, since March 2018.
6. **Planning Code Compliance.** The Commission finds that the Project is consistent with the relevant provisions of the Planning Code in the following manner:
 - A. **Uses (Section 714).** The Planning Code lists the use controls for residential and non-residential uses within the Broadway Zoning District.

The Project involves construction of two new mixed-use buildings reaching heights of 5-stories (56 feet) tall on Broadway and 7-stories (64 feet) tall on Montgomery St. with approximately 52,365 gross square feet of residential use, 4,940 gross square feet of retail use, and 17,995 gross square feet of design

professional office use. As residential, retail sales and service uses, and design professional office uses are all principally permitted uses within the Broadway NCD Zoning District, the Project complies with Section 714.

- B. Height (Sections 250, 253.1).** The Project Site is located within a 65-A-1 Height and Bulk District. In 65-A districts, height is principally permitted up to 40 feet and requires Conditional Use authorization from 40 feet to 65 feet.

The Project site currently measures approximately 56 feet in height for the Broadway building and 65 feet in height for the Montgomery building (excluding exemptions from height per Planning Code Section 260) and therefore does not comply with the principally permitted Code requirements. The Project Sponsor has submitted a Conditional Use Authorization to exceed the base height of 40 feet up to 65 feet. See required Conditional Use Authorization findings under Section 11.

- C. Bulk (Section 270).** The Planning Code establishes bulk controls by district. For buildings located within the "A" Bulk District, the following bulk controls apply above 40 feet, a maximum length of 110'0" and a maximum diagonal dimension of 125'0".

The Project was designed to maximize the residential density and therefore occupies most of the large L-shaped parcel. The building's plan length of 136'5" and diagonal length of 143'1" exceeding the maximum permitted length dimension of 110'0" by 26'5" and maximum diagonal dimension of 125'0" by 18'1" of the height at which bulk controls apply and therefore, the Project Sponsor is requesting a waiver from this requirement in order to accommodate the bonus units. If the project was designed to comply with these bulk requirements, it would preclude the density bonus project and the requested Incentive. The project is seeking a waiver from the bulk requirements. See required State Density Bonus findings under Section 7.

- D. Rear Yard (Section 134).** The Planning Code requires that the Project provide a rear yard equal to 25 percent of the lot depth at the first level containing a dwelling unit, and at every subsequent level and in no case less than 15 feet. Exceptions to the rear yard requirements may be granted if the building location and configuration assure adequate light and air to the residential units and the open space provided.

The Site is an L-shaped lot, therefore the required rear yard for the subject lot is 3,415 sf. The buildings' massing are positioned closest to Broadway and Montgomery Street frontages, with no setbacks proposed on either street. The rear yard provisions of the Code would place the rear yard in the area burdened by a below-grade sewer easement and as an extension of the Verdi Place Alley into the site separating the two building massing. The yard at this location would be 1,922 square feet, equal to 14.1% of the lot area instead of the 25% of the lot depth that is required. The project proposes an open space of 1,655 sf as an extension of Verdi alley and an additional 2,720 square feet of common open space on the roof of the Broadway building, for a total of 4,295 sf, in excess of 25% of the lot area. The location of the rear yard as an extension of Verdi alley will not adversely affect the interior block open space on this commercial block. If the Project were code-compliant in this regard it would result in substantially fewer units in the building. The Project therefore requires Code relief from Section 134(a)(1) and the Project Sponsor is requesting a waiver from this requirement to accommodate the bonus project. See required State Density Bonus findings under Section 7.

- E. **Dwelling Unit Exposure (Section 140).** The Planning Code requires that at least one room of each dwelling unit must face onto a public street, a rear yard, or other open area that meets minimum requirements for area and horizontal dimensions.

The Site is an L-shaped lot with Broadway, Montgomery Street and Verdi Alley all meeting the minimum requirements established by Code to qualify as open areas. As such, all dwelling units that face onto either street or the alley meet exposure requirements. Along the interior lot lines, the property includes a courtyard as an extension of Verdi Alley. The setback along the courtyard does not meet the minimum requirements established by the Code to qualify as open areas. Therefore, 10 dwelling units do not meet exposure requirements. Overall, the Project's massing is arranged on the Site to maximize access to light and air for all 42 dwelling units. In total, 31 of the 42 dwelling units or 74% meet the exposure requirements of the Code. The Project Sponsor is requesting a waiver from this requirement in order to accommodate the density bonus project. See required State Density Bonus findings under Section 7.

- F. **Streetscape and Pedestrian Improvements (Section 138.1).** The Planning Code requires development projects to include streetscape and pedestrian improvements on all publicly accessible rights-of-ways directly front the property.

The Project provides the required street trees and therefore complies with Code Section 138.1

- G. **Lot Size. (Section 121.1).** The Planning Code principally permits lot sizes in the Broadway NCD zoning district up to 4,999 square feet. Conditional Use authorization is required for lots sized 5,000 square feet and above.

The Site is approximately 13,641 sf. The Project therefore requires Conditional Use authorization for Section 121.1. See additional required Conditional Use findings under Section 10.

- H. **Usable Open Space (Sections 135, 136).** The Planning Code requires that a minimum of 60 square feet if private usable open space, or 80 square feet if common usable open space, be provided for dwelling units in Broadway NCD zoning districts. The area counting as open space must meet minimum requirements for area, horizontal dimensions, and exposure.

The Project provides private balconies for eight of the 42 dwelling units that meet the strict dimensional and locational requirements for private useable open space. For the balance of the remaining units, 2,720 square feet of common usable open space would be required. The Project includes 480 sf of private open spaces and 2,720 sf of common open space as a roof deck on the Broadway building. Both residential buildings will have access to the common open space.

- I. **Off-Street Parking Requirements. (Section 151.1).** The Planning Code does not require any off-street parking spaces be provided, but instead provides maximum parking amount of parking permitted as accessory based on land use type. For residential uses, one off street parking space is principally permitted for every two dwelling units. For non-residential uses in the Broadway NCD zoning district it is limited to one space for each 500 square feet of Occupied Floor Area.

The Project includes 17 off-street accessory parking spaces for the 42 dwelling units, which equates to a parking ratio of approximately .42 spaces/dwelling unit. The independently-accessible parking spaces

would be located with one level of a below-grade garage. The Project does not include any accessory parking for the retail sales and service and design professional office uses. As the parking ratio for residential uses (.42 spaces/dwelling unit) is less than the maximum permitted by Code (0.5 spaces/dwelling unit), the Project therefore complies with Section 151.1.

- J. **Bicycle Parking (Sections 155.1, 155.2).** The Planning Code requires bicycle parking for residential, office and retail uses in the following amounts: 1 Class I space for every dwelling unit, 1 Class I per 5,000 sf of office space and 2 Class II for first 5,000 sf then an additional 1 per 50,000 sf of office space, 1 Class I per 7,500 sf of restaurant/retail space and 1 Class II per 750 sf of restaurant/retail space.

The Project proposes 42 dwelling units and design professional office space and retail sales and services. This requires 54 bicycle parking spaces broken down into 47 Class I bicycle parking spaces, and 7 Class II bicycle parking spaces. The Project proposes 47 Class I and 7 Class II bicycle parking spaces

- K. **Dwelling Unit Density. (Section 207).** The Planning Code allows 1 unit per 400 square foot lot area in the Broadway NCD zoning district, or the density permitted in the nearest R District, whichever is greater.

The lot measures 13,641 square feet and thus permits 34 units. With the 21.5% density bonus utilizing the State Density Bonus law the Project gains eight more units for a total of 42 dwelling units.

- L. **Floor Area Ratio. (Sections 123, 124).** Planning Code Section 124 establishes a non-residential Maximum Floor Area Ratio (FAR) of 2.5 to 1 for properties within the Broadway NCD Zoning District.

The Project Site has a lot area of approximately 13,641 sf. Therefore, up to 34,102.5 sf of Gross Floor Area ("GFA") is allowed under the basic FAR limit. The Project Site's GFA minus the residential GFA is 23,675 sf and therefore complies.

- M. **Use Size. (Section 121.2).** The Planning Code principally permits uses up to 2,999 square feet in size. Conditional Use authorization is required for uses with 3,000 square feet and above.

The Project proposes two of eight non-residential use sizes larger than 2,999 gfa. The retail uses are sized at 900 sf, 1,138 sf and 2,489 sf. The design professional office uses are sized at 3,363 sf, 3,348 sf, 2,120 sf, 1,978 sf, and 1,829 sf in size. Two of the design spaces exceed the use size limit. The Project therefore requires a Conditional Use Authorization per Section 121.2 and 714. The project is entitled to, and has requested, an Incentive/Concession for the use size under State Density Bonus Law. See required State Density Bonus findings under Section 7.

- N. **Street Frontage in Neighborhood Commercial Districts (Section 145.1).** Requires that within NC Districts space for active uses shall be provided within the first 25 feet of building depth on the ground floor and 15 feet on floors above from any facade facing a street at least 30 feet in width. In addition, the floors of street-fronting interior spaces housing non-residential active uses and lobbies shall be as close as possible to the level of the adjacent sidewalk at the principal entrance to these spaces. Frontages with active uses that must be fenestrated with transparent windows and doorways for no less than 60 percent of the street frontage at the ground level and allow visibility to the inside of the

building. The use of dark or mirrored glass shall not count towards the required transparent area. Any decorative railings or grillwork, other than wire mesh, which is placed in front of or behind ground floor windows, shall be at least 75 percent open to perpendicular view. Rolling or sliding security gates shall consist of open grillwork rather than solid material, so as to provide visual interest to pedestrians when the gates are closed, and to permit light to pass through mostly unobstructed. Gates, when both open and folded or rolled as well as the gate mechanism, shall be recessed within, or laid flush with, the building facade.

The subject commercial space has approximately 79-feet of frontage on Broadway and 47.5 feet of frontage on Montgomery Street, all frontages devoted to retail sales and services uses and building lobbies, except for a single width garage entrance and a small utility room on Montgomery Street. Not less than 75 percent is open to perpendicular view with clear and unobstructed windows and thus complies.

- O. Transportation Demand Management (TDM) Plan (Section 169).** Pursuant to Planning Code Section 169 and the TDM Program Standards, the Project shall finalize a TDM Plan prior Planning Department approval of the first Building Permit or Site Permit. As currently proposed, the Project must achieve a target of 13 points for the Residential Use.

As currently proposed, the Project will achieve its required points through the following TDM measures:

- i. Improve Walking Conditions (Option A)
- ii. Bicycle Repair Station
- iii. Real Time Transportation Information Display
- iv. Tailored Transportation Marketing Services (Option A)
- v. Unbundled Parking (Location C)
- vi. Parking Supply (Option C)

- P. Transportation Sustainability Fee.** Planning Code Section 411A is applicable to new development that results in more than twenty dwelling units and non-residential uses in excess of 800 gross square feet.

The Project includes 42 dwelling units and 23,675 gross square feet of non-residential uses, and is therefore subject to the Transportation Sustainability Fee, as outlined in Planning Code Section 411A.

- Q. Residential Child-Care Impact Fee.** Planning Code Section 414A is applicable to new development that results in at least one net new residential unit.

The Project includes a Gross Floor Area of approximately 52,365square feet of new residential use associated with the new construction of 42 dwelling units. This square footage shall be subject to the Residential Child-Care Impact Fee, as outlined in Planning Code Section 414A.

- R. Inclusionary Affordable Housing Program.** Planning Code Section 415 sets forth the requirements and procedures for the Inclusionary Affordable Housing Program. Under Planning Code Section 415.3, these requirements apply to projects that consist of 10 or more units. The applicable percentage is dependent on the number of units in the project, the zoning of the property, and the date that the project submitted a complete Project Application. A complete Project Application was submitted on

December 22, 2017; therefore, pursuant to Planning Code Section 415.3 the Inclusionary Affordable Housing Program requirement for the On-site Affordable Housing Alternative is to provide 20% of the proposed dwelling units as affordable.

Pursuant to Planning Code Section 415.5, the Project may pay the Affordable Housing Fee ("Fee"). This Fee is made payable to the Department of Building Inspection ("DBI") for use by the Mayor's Office of Housing and Community Development for the purpose of increasing affordable housing citywide. The applicable percentage is dependent on the number of units in the project, the zoning of the property, and the date that the project submitted a complete Project Application. The applicable fee rate is 33%.

In addition, under the State Density Bonus Law, Government Code section 65915 et seq, a project is entitled to a density bonus, concessions and incentives, and waivers of development standards only if it provides on-site affordable units. Projects that include on-site units to qualify for a density bonus under the State Law may also be able to satisfy all or part of the Affordable Housing Fee requirement, by receiving a "credit" for the on-site units provided. This "credit" is calculated in accordance with Planning Code Section 415.5(g)(1)(D), referred to as the Combination Alternative. The Combination Alternative allows projects to satisfy the Inclusionary Housing requirement through a combination of payment of the fee and provision of on-site units.

The Project Sponsor has demonstrated that it is eligible for the Combination Alternative under Planning Code Section 415.5 and 415.6 and has submitted an 'Affidavit of Compliance with the Inclusionary Affordable Housing Program: Planning Code Section 415,' to satisfy the requirements of the Inclusionary Affordable Housing Program by providing a combination of affordable housing on-site and partial payment of the Affordable Housing Fee. The Project Sponsor submitted such Affidavit on August 27, 2020 and a revised Affidavit on October 8, 2021, and again on November 12, 2021. The applicable percentage is dependent on the total number of units in the project, the zoning of the property, and the date that the project submitted a complete Project Application. A complete Project Application was submitted on December 22, 2017; therefore, pursuant to Planning Code Section 415.3 the Inclusionary Affordable Housing Program requirement for the On-site Affordable Housing Alternative is to provide 20% of the total proposed dwelling units as affordable. 20% of the total proposed dwelling units as affordable, with a minimum of 10% of the units affordable to low-income households, 5% of the units affordable to moderate-income households, and the remaining 5% of the units affordable to middle-income households, as defined by the Planning Code and Procedures Manual. Six units (2 one-bedroom, 3 two-bedroom, and 1 three-bedroom units) of the total units provided will be affordable units. The remaining inclusionary affordable housing obligation for the 42-unit project will be satisfied through payment of the Affordable Housing Fee.

- 7. Individually Requested State Density Bonus Program Findings.** Before approving an application for a Density Bonus, Incentive, Concession, or waiver, for any Individually Requested Density Bonus Project, the Planning Commission shall make the following findings as applicable:

- A. The Housing Project is eligible for the Individually Requested Density Bonus Program.

The Project consists of five or more dwelling units on a site located in the Broadway Neighborhood Commercial District that is currently developed as a parking structure that is not subject to San

Francisco Rent Stabilization and Arbitration Ordinance and is, therefore, eligible for the Individually Requested Density Bonus Program.

The Project provides 11% of the 34 dwelling units in the base project (4 units) as affordable to low-income households, defined as those earning up to 80% of area median income and is therefore entitled to a 21.5% density bonus under California Government Code Sections 65915-95918, as revised under AB 2345.

- B. The Housing Project has demonstrated that any Concessions or Incentives reduce actual housing costs, as defined in Section 50052.5 of the California Health and Safety Code, or for rents for the targeted units, based upon the financial analysis and documentation provided.

The Project includes eight non-residential tenant spaces, two that exceed the 2,999 square-foot non-residential use size limits of the Broadway Neighborhood Commercial District. The project has requested an Incentive/Concession from this provision of the Planning Code and has demonstrated that the Incentive/Concession offsets the cost of constructing the housing project.

- C. If a waiver or modification is requested, a finding that the Development Standards for which the waiver is requested would have the effect of physically precluding the construction of the Housing Project with the Density Bonus or Concessions and Incentives permitted.

The Project requests the following waivers from the Planning Code Development Standards: 1) Rear Yard (Planning Code Section 134); 2) Dwelling Unit Exposure (Planning Code Section 140; and 3) Bulk (Planning Code Section 270).

The Project provides a maximum density of 34 dwelling units, plus the 21.5% density bonus of eight additional units afforded under the Individually State Density Bonus, for a total of 42 dwelling units. The density is obtained by reducing the rear yard, increasing the building bulk above 40 feet, and reducing the unit exposure requirement for some units. Requiring the proposed building to comply with rear yard, bulk, or dwelling unit exposure would preclude the construction of the density bonus project with Incentives/Concessions; thus, these requirements are eligible for Waiver under the density bonus request.

- D. If the Density Bonus is based all or in part on donation of land, a finding that all the requirements included in Government Code Section 65915(g) have been met.

The Project does not include a donation of land, and this is not the basis for the Density Bonus.

- E. If the Density Bonus, Concession or Incentive is based all or in part on the inclusion of a Child Care Facility, a finding that all the requirements included in Government Code Section 65915(h) have been met.

The Project does not include a Child Care Facility, and this is not the basis for the Density Bonus.

- F. If the Concession or Incentive includes mixed-use development, a finding that all the requirements included in Government Code Section 65915(k) have been met.

The project is mixed-use and includes design professional office and retail in addition to housing. The approval of the retail and design professional office spaces will reduce the cost of the housing development and are compatible with the housing project and the existing development in the area of the proposed project.

- 8. Conditional Use Findings.** Planning Code Section 303 establishes criteria for the Planning Commission to consider when reviewing applications for Conditional Use authorization. On balance, the project complies with said criteria in that:

- A. The proposed new uses and building, at the size and intensity contemplated and at the proposed location, will provide a development that is necessary or desirable, and compatible with, the neighborhood or the community.

The Jackson Square/North Beach neighborhood contains a mix of residential, commercial and institutional uses, and this Project will be compatible with those uses. The Project will provide housing, retail space and design professional office space, replacing the existing parking lot and garage with a mixed-use project that better compliments the neighborhood. The residential portion of the project will provide much need housing, using a diverse mix of one-two-, and three-bedroom units to make the project accessible to families and individuals. The Project, which is contemplated to be a for-sale condominium development, will comply with the City's inclusionary housing program through a mixture of payment of the Affordable Housing Fee and provided five affordable units on-site.

Architectural elements like masonry, cornices, and upper floor setbacks provide buildings that are consistent and compatible with the prevailing development pattern and neighborhood character.

- B. The proposed project will not be detrimental to the health, safety, convenience or general welfare of persons residing or working in the vicinity. There are no features of the project that could be detrimental to the health, safety or convenience of those residing or working the area, in that:

- (1) Nature of proposed site, including its size and shape, and the proposed size, shape and arrangement of structures;

The Project's massing is consistent with the character and scale of the neighborhood and will not impede any development of surrounding properties. The interior courtyard and variable heights throughout the project break up the massing and provide for a pedestrian experience. The Project utilizes the entire lot and provides residential, retail, and design professional space to the surrounding community.

- (2) The accessibility and traffic patterns for persons and vehicles, the type and volume of such traffic, and the adequacy of proposed off-street parking and loading;

The Site is accessible by public transit with multiple transit alternatives withing close walking distance. Broadway and Montgomery Streets, both major thoroughfares, provide ready access to those driving. Off-street parking is available along surrounding neighborhood streets or within the proposed underground parking garage. This garage has 17 vehicle parking spaces available to residential condominium unit owners. The vehicular entrance is located on

Montgomery Street and no off-street loading spaces are proposed.

- (3) The safeguards afforded to prevent noxious or offensive emissions such as noise, glare, dust and odor;

The Project will not emit any noxious odors or emissions. As a primarily residential, office, and small-scale retail buildings, there will be limited generation of dust and odors, and all activities are contained inside the buildings, which prevents noise pollution from emanating.

- (4) Treatment given, as appropriate, to such aspects as landscaping, screening, open spaces, parking and loading areas, service areas, lighting and signs;

The Project proposes common and private open space in the form of private decks and/or terraces with common open space on the roof of the Broadway building. The common open spaces will include landscaping and screening. The Department shall review all lighting and signs proposed for the new business in accordance with Exhibit A. Parking is all located underground, with the parking garage entrance screened and located on Montgomery Street.

- C. That the use as proposed will comply with the applicable provisions of the Planning Code and will not adversely affect the General Plan.

The Project complies with all relevant requirements and standards of the Planning Code except for the above-described density bonus Incentive and Waivers and is consistent with objectives and policies of the General Plan as detailed below.

- D. That use or feature as proposed will provide development that is in conformity with the stated purpose of the applicable Use District.

The proposed project is consistent with the stated purposes of Broadway NCD Districts in that the residential uses, with additional floor area for affordable units, the design professional office suites, the ground floor retail uses are principally permitted and will provide a compatible convenience service for the immediately surrounding neighborhoods during daytime hours.

9. Development of Large Lots in NC District Findings (Section 121.1). In addition to the criteria of Section 303(c) of this Code, the Commission shall consider the extent to which the following criteria are met:

- A. The mass and façade of the proposed structure are compatible with the existing scale of the district.

The existing development in the area surrounding the Project is varied in scale and intensity. Mixed-use buildings are common, with building heights on the Broadway block ranging from two to four stories in height, and on the Montgomery block ranging from three to six stories.

The subject lot is larger than most in the immediate area, but the proposed building is designed to break the mass into a series of smaller elements. The proposed building has been situated on the site so that the majority of its mass falls at the northwest corner, away from both the Broadway and

Montgomery Street frontages. The use of varied window proportions and shapes, outdoor terraces facing Broadway and Montgomery Street, and multiple exterior materials further assists in minimizing the appearance of bulk. The Project will complement and be harmonious with the surrounding neighborhood character. At the streetscape, the façade incorporates features that anchor the building and define a pedestrian scale.

- B. The façade of the proposed structure is compatible with design features of adjacent facades that contribute to the positive visual quality of the district.

Existing buildings in the area exhibit an eclectic architectural character, with no prevailing style establishing a dominant visual pattern for the immediate neighborhood. Two-story retail commercial buildings are interspersed with multi-story, mixed-use structures, and solid brick facades are adjacent to wood structures with bay windows.

Both the Broadway and Montgomery Street façades express a high ratio of solid to windows and glazing, similar to older commercial and mixed-use buildings in the area. The building incorporates forms and detailing that are familiar to the older buildings in the area while harmonizing with newer contemporary materials. Additionally, the ground floor retail can be divided into multiple tenant spaces that will help reinforce the neighborhood commercial scale of the building.

- C. Where 5,000 or more gross square feet of Non-Residential space is proposed, that the project provides commercial spaces in a range of sizes, including one or more spaces of 1,000 gross square feet or smaller, to accommodate a diversity of neighborhood business types and business sizes.

The Project includes eight suites of non-residential space ranging from 900 sf to 3,348 sf in size to accommodate a diversity of neighborhood businesses.

10. Non-Residential Use Size in NC District Findings (Section 121.2). In addition to the criteria of Section 303(c) of this Code, the Commission shall consider the extent to which the following criteria are met:

- A. The intensity of activity in the district is not such that allowing the larger use will be likely to foreclose the location of other needed neighborhood-serving uses in the area.

The Project proposes approximately 14,932 gross floor area (GFA) of design professional office space and 4,527 GFA of retail sales and service uses comprised of eight suites located on the basement level and Floors 1 and 2. Two of the eight suites are larger than 2,999 square feet and the project has requested an Incentive/Concession to exceed the use size limit under State Density Bonus Law. The two suites that exceed use size are 3,363 sf and 3,348 sf. These design professional office suites will not foreclose the location of other needed neighborhood serving uses in the area because the office suites are located in the interior of the project and do not occupy any street-facing ground floor spaces; and the project includes three other retail storefronts smaller than 2,999 square feet along Broadway and Montgomery Street to accommodate other smaller neighborhood-serving uses.

- B. The proposed use will serve the neighborhood, in whole or in significant part, and the nature of the use requires a larger size in order to function.

Design professional firms have traditionally located in Jackson Square and North Beach and are principally permitted in the Broadway NC district on the second story and below in new buildings. Thus, additional space for these businesses will serve a significant part of the neighborhood. Few design professional firms are small enough to be accommodated in suites smaller than 3,000 square feet. For that reason, larger suites are required to serve this use.

- C. The building in which the use is to be located is designed in discrete elements which respect the scale of development in the district.

The proposed buildings have been situated on the site so that the majority of their mass falls at the northwest corner, away from both the Broadway and Montgomery Street facings. The use of varied window proportions and shapes, outdoor terraces facing Broadway and Montgomery Street, and multiple exterior materials further assists in minimizing the appearance of bulk, and the proposed heights are within one-to-three stories of the surrounding buildings. The project will complement and be harmonious with the surrounding neighborhood character.

11. Structures over 40 feet in height within the Broadway NCD Findings (Section 253.1): In addition to the criteria of Section 303(c) of this Code, the Commission shall consider the extent to which the following criteria are met:

- A. The height of the new or expanding development will be compatible with the individual neighborhood character and the height and scale of the adjacent buildings.

The proposed buildings have been situated on the site so that the majority of their mass falls at the northwest corner, away from both the Broadway and Montgomery Street facings. The use of varied window proportions and shapes, outdoor terraces facing Broadway and Montgomery Street, and multiple exterior materials further assists in minimizing the appearance of bulk, and the proposed heights are within one-to-three stories of the surrounding buildings

- B. The height and bulk of the new or expanding development will be designed to allow maximum sun access to nearby parks, plazas, and major pedestrian corridors.

The development has been designed as two separate buildings to allow additional light and air and to minimize the appearance of bulk. It does not impeded sun access to nearby parks, plazas or major pedestrian corridors.

- C. The architectural and cultural character and features of existing buildings shall be preserved and enhanced. The Historic Preservation Commission or its staff shall review any proposed alteration of historic resources and must determine that such alterations comply with the Secretary of Interior's Standards for the Treatment of Historic Properties before the City approves any permits to alter such buildings. For purposes of this section, "historic resources" shall include Article 10 Landmarks and buildings located within Article 10 Historic Districts, buildings and districts identified in surveys adopted by the City, buildings listed or potentially eligible for individual listing on the National or California Registers, and buildings located within listed or potentially eligible National Register or California Register historic districts. The Planning Department shall

also consult materials available through the California Historical Resources Information System (CHRIS) and Inventory to determine eligibility.

The parking lot/structure is located roughly at the center of the Jackson Square Historic District Extension ("JSE"). Listed in the California Register of Historical Resources, the JSE is a distinct district but also effectively extends the original Jackson Square Historic District's northern boundary up to the centerline of Broadway between Sansome and Kearny Streets. As noted in the nomination form for the JSE, the predominant building type is a commercial block of two to three stories with vernacular classical revivalist cornices and deep-set windows in a regular pattern. Though brick is the most ubiquitous cladding material in the district by a good margin, stucco is the second most common facing material. The project picks up on this pattern as it occupies the single largest lot in the JSE, and would construct two structures that, while taller than the typical three stories, are unobtrusive and well connected to their context. Relating directly to its surroundings and not intruding on the lower-scaled portions of the district, clad in the two most common materials in the district, and subtly incorporating typical features like string courses and punched openings. The Montgomery building is sufficiently compatible with the character of the JSE while being differentiated primarily through its broken-up massing and treatment of the secondary, Verdi Alley elevation. The Broadway building achieves general compatibility in a similar fashion. The project on the whole is differentiated from but largely compatible with the JSE. This, paired with the fact that the project would not demolish a contributory property and would alter just one of 32 lots within the district, the project would not cause the JSE to lose its eligibility for the National Register.

- 12. General Plan Compliance.** The Project is, on balance, consistent with the following Objectives and Policies of the General Plan:

HOUSING ELEMENT

Objectives and Policies

OBJECTIVE 1

IDENTIFY AND MAKE AVAILABLE FOR DEVELOPMENT ADEQUATE SITES TO MEET THE CITY'S HOUSING NEEDS, ESPECIALLY PERMANENTLY AFFORDABLE HOUSING.

Policy 1.1

Plan for the full range of housing needs in the City and County of San Francisco, especially affordable housing.

Policy 1.10

Support new housing projects, especially affordable housing, where households can easily rely on public transportation, walking and bicycling for the majority of daily trips.

OBJECTIVE 4

FOSTER A HOUSING STOCK THAT MEETS THE NEEDS OF ALL RESIDENTS ACROSS LIFECYCLES.

Policy 4.1

Develop new housing, and encourage the remodeling of existing housing, for families with children.

Policy 4.5

Ensure that new permanently affordable housing is located in all of the City's neighborhoods, and encourage integrated neighborhoods, with a diversity of unit types provided at a range of income levels.

OBJECTIVE 11

SUPPORT AND RESPECT THE DIVERSE AND DISTINCT CHARACTER OF SAN FRANCISCO'S NEIGHBORHOODS.

Policy 11.1

Promote the construction and rehabilitation of well-designed housing that emphasizes beauty, flexibility, and innovative design, and respects existing neighborhood character.

Policy 11.2

Ensure implementation of accepted design standards in project approvals.

Policy 11.3

Ensure growth is accommodated without substantially and adversely impacting existing residential neighborhood character.

Policy 11.4

Continue to utilize zoning districts which conform to a generalized residential land use and density plan and the General Plan.

Policy 11.6

Foster a sense of community through architectural design, using features that promote community interaction.

Policy 11.8

Consider a neighborhood's character when integrating new uses, and minimize disruption caused by expansion of institutions into residential areas.

OBJECTIVE 12

BALANCE HOUSING GROWTH WITH ADEQUATE INFRASTRUCTURE THAT SERVES THE CITY'S GROWING POPULATION.

Policy 12.1

Encourage new housing that relies on transit use and environmentally sustainable patterns of movement.

Policy 12.2

Consider the proximity of quality of life elements such as open space, child care, and neighborhood services, when developing new housing units.

Policy 12.3

Ensure new housing is sustainably supported by the City's public infrastructure systems.

OBJECTIVE 13

PRIORITIZE SUSTAINABLE DEVELOPMENT IN PLANNING FOR AND CONSTRUCTING NEW HOUSING.

Policy 13.1

Support "smart" regional growth that locates new housing close to jobs and transit.

Policy 13.3

Promote sustainable land use patterns that integrate housing with transportation in order to increase transit, pedestrian, and bicycle mode share.

URBAN DESIGN ELEMENT

Objectives and Policies

OBJECTIVE 1

EMPHASIS OF THE CHARACTERISTIC PATTERN WHICH GIVES TO THE CITY AND ITS NEIGHBORHOODS AN IMAGE, A SENSE OF PURPOSE, AND A MEANS OF ORIENTATION.

Policy 1.3

Recognize that buildings, when seen together, produce a total effect that characterizes the city and its districts.

Policy 1.7

Recognize the natural boundaries of districts, and promote connections between districts.

COMMERCE AND INDUSTRY ELEMENT

Objectives and Policies

OBJECTIVE 1

MANAGE ECONOMIC GROWTH AND CHANGE TO ENSURE ENHANCEMENT OF THE TOTAL CITY LIVING AND WORKING ENVIRONMENT.

Policy 1.7

Encourage development which provides substantial net benefits and minimizes undesirable consequences. Discourage development which has substantial undesirable consequences that cannot be mitigated.

TRANSPORTATION ELEMENT

Objectives and Policies

OBJECTIVE 2

USE THE TRANSPORTATION SYSTEM AS A MEANS FOR GUIDING DEVELOPMENT AND IMPROVING THE ENVIRONMENT.

Policy 2.1

Use rapid transit and other transportation improvements in the city and region as the catalyst for desirable development, and coordinate new facilities with public and private development.

The Project includes 42 dwelling units, adding a significant amount of housing to a site that is currently underutilized, well-served by existing transit, and is within walking distance of substantial goods and services. Future residents can walk, bike, or access BART and MUNI from the Site. The Project includes five on-site affordable housing units, which assist in meeting the City's affordable housing goals. The Project also improves the public rights-of-way with new streetscape improvements, street trees, and landscaping. On balance, the Project is consistent with the Objectives and Policies of the City's General Plan.

13. Planning Code Section 101.1(b) establishes eight priority-planning policies and requires review of permits for consistency with said policies. On balance, the project complies with said policies in that:

- A. That existing neighborhood-serving retail uses be preserved and enhanced and future opportunities for resident employment in and ownership of such businesses be enhanced.

The Project would have a positive effect on existing neighborhood-serving retail uses because provides three new retail storefronts and 42 new dwelling units, which will enhance the nearby retail uses by providing new residents, who may patron and/or own these businesses.

- B. That existing housing and neighborhood character be conserved and protected in order to preserve the cultural and economic diversity of our neighborhoods.

The project site does not possess any existing housing given the site is developed as a surface parking lot and garage. The Project would improve the existing character of the neighborhood by developing mixed-use residential structures with 42 new dwelling units, including on-site affordable units, thus resulting in an overall increase in the neighborhood housing stock. The Project is expressive in design and relates well to the scale and form of the surrounding neighborhood. For these reasons, the Project would protect and preserve the cultural and economic diversity of the neighborhood.

- C. That the City's supply of affordable housing be preserved and enhanced,

The Project does not currently possess any existing affordable housing. The Project will provide 42 dwelling units, adding to the City's housing supply. The Project will comply with the City's Inclusionary Housing Program by providing six below-market rate dwelling units on-site as well as payment of the Affordable housing fee for remaining base project units and the bonus density units conferred through the State Density Bonus Program. Therefore, the Project will increase the stock of affordable housing units in the City.

- D. That commuter traffic does not impede MUNI transit service or overburden our streets or neighborhood parking.

The Project Site is served by nearby public transportation options. The Project is located along a Muni bus line (12-Folsom/Pacific) is near other bus lines and is within walking distance of the BART Station at Montgomery and Market Streets. Future residents and employees of the Project could access both the existing MUNI rail and bus services. The Project also provides off-street parking at the principally permitted amounts and sufficient bicycle parking for residents and their guests such that neighborhood parking will not be overburdened by the addition of new residents.

- E. That a diverse economic base be maintained by protecting our industrial and service sectors from displacement due to commercial office development, and that future opportunities for resident employment and ownership in these sectors be enhanced.

The Project is predominantly residential and would not negatively affect the industrial and service sectors, nor would it displace any existing industrial uses. The Project would also be consistent with the character of existing development in the neighborhood, which is characterized by neighborhood-serving retail and residential medium- and high-rise buildings.

- F. That the City achieve the greatest possible preparedness to protect against injury and loss of life in an earthquake.

The Project will be designed and constructed to conform to the structural and seismic safety requirements of the Building Code. As such, this Project will improve the property's ability to withstand an earthquake.

- G. That landmarks and historic buildings be preserved.

Currently, the Project Site does not contain any City Landmarks or historic buildings.

- H. That our parks and open space and their access to sunlight and vistas be protected from development.

The Project does not cast shadow on any adjacent public parks or impact vistas from any parks or open spaces.

- 14. First Source Hiring.** The Project is subject to the requirements of the First Source Hiring Program as they apply to permits for residential development (Administrative Code Section 83.11), and the Project Sponsor shall comply with the requirements of this Program as to all construction work and on-going employment required for the Project. Prior to the issuance of any building permit to construct or a First Addendum to the Site Permit, the Project Sponsor shall have a First Source Hiring Construction and Employment Program approved by the First Source Hiring Administrator and evidenced in writing. In the event that both the Director of Planning and the First Source Hiring Administrator agree, the approval of the Employment Program may be delayed as needed.

The Project Sponsor submitted a First Source Hiring Affidavit and prior to issuance of a building permit will

execute a First Source Hiring Memorandum of Understanding and a First Source Hiring Agreement with the City's First Source Hiring Administration.

15. The Project is consistent with and would promote the general and specific purposes of the Code provided under Section 101.1(b) in that, as designed, the Project would contribute to the character and stability of the neighborhood and would constitute a beneficial development.
16. The Commission hereby finds that approval of the Conditional Use Authorization would promote the health, safety and welfare of the City.

DECISION

That based upon the Record, the submissions by the Applicant, the staff of the Department and other interested parties, the oral testimony presented to this Commission at the public hearings, and all other written materials submitted by all parties, the Commission hereby **APPROVES Conditional Use Authorization Application No. 2017-015678CUA** subject to the following conditions attached hereto as “EXHIBIT A” in general conformance with plans on file, dated November 12, 2021 and stamped “EXHIBIT B”, which is incorporated herein by reference as though fully set forth.

APPEAL AND EFFECTIVE DATE OF MOTION: Any aggrieved person may appeal this Conditional Use Authorization to the Board of Supervisors within thirty (30) days after the date of this Motion. The effective date of this Motion shall be the date of this Motion if not appealed (after the 30-day period has expired) OR the date of the decision of the Board of Supervisors if appealed to the Board of Supervisors. For further information, please contact the Board of Supervisors at (415) 554-5184, City Hall, Room 244, 1 Dr. Carlton B. Goodlett Place, San Francisco, CA 94102.

Protest of Fee or Exaction: You may protest any fee or exaction subject to Government Code Section 66000 that is imposed as a condition of approval by following the procedures set forth in Government Code Section 66020. The protest must satisfy the requirements of Government Code Section 66020(a) and must be filed within 90 days of the date of the first approval or conditional approval of the development referencing the challenged fee or exaction. For purposes of Government Code Section 66020, the date of imposition of the fee shall be the date of the earliest discretionary approval by the City of the subject development.

If the City has not previously given Notice of an earlier discretionary approval of the project, the Planning Commission’s adoption of this Motion, Resolution, Discretionary Review Action or the Zoning Administrator’s Variance Decision Letter constitutes the approval or conditional approval of the development and the City hereby gives **NOTICE** that the 90-day protest period under Government Code Section 66020 has begun. If the City has already given Notice that the 90-day approval period has begun for the subject development, then this document does not re-commence the 90-day approval period.

I hereby certify that the Planning Commission ADOPTED the foregoing Motion on December 2, 2021.

Jonas P. Ionin
Commission Secretary

AYES:

NAYS:

ABSENT:

RECUSE:

ADOPTED: December 2, 2021

EXHIBIT A

Authorization

This authorization is for a conditional use to allow a lot size exceeding 4,999 sf, use sizes exceeding 2,999 sf, and a public parking garage in the Broadway NCD Zoning District. The Project is utilizing the Individually Requested State Density Bonus Program to achieve a 21.5% density bonus thereby maximizing residential density on the Site pursuant to California Government Code Sections 65915-95918, as revised under Assembly Bill No. 2345 (AB 2345). The Project requests one incentive/concession for non-residential use size limits and three (3) waivers from: Bulk (Section 270), Rear Yard (Section 134), and Dwelling Unit Exposure (Section 140). The Project is located at 425 Broadway, Block 0163, and Lot 002 pursuant to Planning Code Sections 121, 303 and 714 within the **Broadway NCD District** and a **65-A-1 Height and Bulk District**; in general conformance with plans, dated **November 12, 2021**, and stamped "EXHIBIT B" included in the docket for Record No. **2017-015678CUA** and subject to conditions of approval reviewed and approved by the Commission on **December 2, 2021** under Motion No **XXXXXX**. This authorization and the conditions contained herein run with the property and not with a particular Project Sponsor, business, or operator.

Recordation of Conditions Of Approval

Prior to the issuance of the building permit or commencement of use for the Project the Zoning Administrator shall approve and order the recordation of a Notice in the Official Records of the Recorder of the City and County of San Francisco for the subject property. This Notice shall state that the project is subject to the conditions of approval contained herein and reviewed and approved by the Planning Commission on **December 2, 2021** under Motion No **XXXXXX**.

Printing of Conditions of Approval on Plans

The conditions of approval under the 'Exhibit A' of this Planning Commission Motion No. **XXXXXX** shall be reproduced on the Index Sheet of construction plans submitted with the site or building permit application for the Project. The Index Sheet of the construction plans shall reference to the Conditional Use authorization and any subsequent amendments or modifications.

Severability

The Project shall comply with all applicable City codes and requirements. If any clause, sentence, section or any part of these conditions of approval is for any reason held to be invalid, such invalidity shall not affect or impair other remaining clauses, sentences, or sections of these conditions. This decision conveys no right to construct, or to receive a building permit. "Project Sponsor" shall include any subsequent responsible party.

Changes and Modifications

Changes to the approved plans may be approved administratively by the Zoning Administrator. Significant changes and modifications of conditions shall require Planning Commission approval of a new Conditional Use authorization.

CONDITIONS OF APPROVAL, COMPLIANCE, MONITORING, AND REPORTING

Performance

1. **Validity.** The authorization and right vested by virtue of this action is valid for three (3) years from the effective date of the Motion. The Department of Building Inspection shall have issued a Building Permit or Site Permit to construct the project and/or commence the approved use within this three-year period.

For information about compliance, contact Code Enforcement, Planning Department at 628.652.7463, www.sfplanning.org

2. **Expiration and Renewal.** Should a Building or Site Permit be sought after the three (3) year period has lapsed, the project sponsor must seek a renewal of this Authorization by filing an application for an amendment to the original Authorization or a new application for Authorization. Should the project sponsor decline to so file, and decline to withdraw the permit application, the Commission shall conduct a public hearing in order to consider the revocation of the Authorization. Should the Commission not revoke the Authorization following the closure of the public hearing, the Commission shall determine the extension of time for the continued validity of the Authorization.

For information about compliance, contact Code Enforcement, Planning Department at 628.652.7463, www.sfplanning.org

3. **Diligent Pursuit.** Once a site or Building Permit has been issued, construction must commence within the timeframe required by the Department of Building Inspection and be continued diligently to completion. Failure to do so shall be grounds for the Commission to consider revoking the approval if more than three (3) years have passed since this Authorization was approved.

For information about compliance, contact Code Enforcement, Planning Department at 628.652.7463, www.sfplanning.org

4. **Extension.** All time limits in the preceding three paragraphs may be extended at the discretion of the Zoning Administrator where implementation of the project is delayed by a public agency, an appeal or a legal challenge and only by the length of time for which such public agency, appeal or challenge has caused delay.

For information about compliance, contact Code Enforcement, Planning Department at 628.652.7463, www.sfplanning.org

5. **Conformity with Current Law.** No application for Building Permit, Site Permit, or other entitlement shall be approved unless it complies with all applicable provisions of City Codes in effect at the time of such approval.

For information about compliance, contact Code Enforcement, Planning Department at 628.652.7463, www.sfplanning.org

Entertainment Commission – Noise Attenuation Conditions

6. Chapter 116 Residential Projects. The Project Sponsor shall comply with the “Recommended Noise Attenuation Conditions for Chapter 116 Residential Projects,” which were recommended by the Entertainment Commission on September 21, 2021, as well as four additional conditions. These conditions state:

- A. **Community Outreach.** Project Sponsor shall include in its community outreach process any businesses located within 300 feet of the proposed project that operate between the hours of 9PM-5AM. Notice shall be made in person, written or electronic form.
- B. **Sound Study.** Project sponsor shall conduct an acoustical sound study, which shall include sound readings taken when performances are taking place at the proximate Places of Entertainment, as well as when patrons arrive and leave these locations at closing time. Readings should be taken at locations that most accurately capture sound from the Place of Entertainment to best of their ability. Any recommendation(s) in the sound study regarding window glaze ratings and soundproofing materials including but not limited to walls, doors, roofing, etc. shall be given highest consideration by the project sponsor when designing and building the project.
- C. **Design Considerations.**
 - i. During design phase, project sponsor shall consider the entrance and egress location and paths of travel at the Place(s) of Entertainment in designing the location of (a) any entrance/egress for the residential building and (b) any parking garage in the building.
 - ii. In designing doors, windows, and other openings for the residential building, project sponsor should consider the POE’s operations and noise during all hours of the day and night.
- D. **Construction Impacts.** Project sponsor shall communicate with adjacent or nearby Place(s) of Entertainment as to the construction schedule, daytime and nighttime, and consider how this schedule and any storage of construction materials may impact the POE operations.
- E. **Communication.** Project Sponsor shall make a cell phone number available to Place(s) of Entertainment management during all phases of development through construction. In addition, a line of communication should be created to ongoing building management throughout the occupation phase and beyond.
- F. **Window and Door Assemblies.** Window and door assemblies shall achieve STC ratings of STC 37 on the north side (facing Broadway Street), STC 28 on the east side (facing Montgomery Street), and STC 31 to 34 in Verdi Alley.
- G. **Wall Assembly.** An upgraded wall assembly is recommended at units with exterior walls facing the Broadway Studios + FAME Venue. Insulated single-stud assembly with one layer of gypsum board on the interior side and plywood/weatherproofing/exterior siding on the exterior (approximately STC 45).
- H. **Air Conditioning.** Every unit shall include Air Conditioning (forced air mechanical ventilation) to

control temperature while windows are shut.

- I. **Disclosure.** In addition to including required language from Administrative Code Chapter 116.8 “Disclosure Requirements for Transfer of Real Property for Residential Use,” the disclosure shall also include the disclosure of potential noise exposure to low-frequency (bass) noise levels that will be noticeable inside some of the residences.

Design – Compliance at Plan Stage

7. **Final Materials.** The Project Sponsor shall continue to work with Planning Department on the building design. Final materials, glazing, color, texture, landscaping, and detailing shall be subject to Department staff review and approval. The architectural addenda shall be reviewed and approved by the Planning Department prior to issuance.

For information about compliance, contact the Case Planner, Planning Department at 628.652.7334, www.sfplanning.org

8. **Garbage, Composting and Recycling Storage.** Space for the collection and storage of garbage, composting, and recycling shall be provided within enclosed areas on the property and clearly labeled and illustrated on the building permit plans. Space for the collection and storage of recyclable and compostable materials that meets the size, location, accessibility and other standards specified by the San Francisco Recycling Program shall be provided in the buildings.

For information about compliance, contact the Case Planner, Planning Department at 628.652.7334, www.sfplanning.org

9. **Rooftop Mechanical Equipment.** Pursuant to Planning Code 141, the Project Sponsor shall submit a roof plan to the Planning Department prior to Planning approval of the building permit application. Rooftop mechanical equipment, if any is proposed as part of the Project, is required to be screened so as not to be visible from any point at or below the roof level of the subject building.

For information about compliance, contact the Case Planner, Planning Department at 628.652.7334, www.sfplanning.org

10. **Lighting Plan.** The Project Sponsor shall submit an exterior lighting plan to the Planning Department prior to Planning Department approval of the building / site permit application.

For information about compliance, contact the Case Planner, Planning Department at 628.652.7334, www.sfplanning.org

11. **Streetscape Plan.** Pursuant to Planning Code Section 138.1, the Project Sponsor shall continue to work with Planning Department staff, in consultation with other City agencies, to refine the design and programming of the Streetscape Plan so that the plan generally meets the standards of the Better Streets Plan and all applicable City standards. The Project Sponsor shall complete final design of all required street improvements, including procurement of relevant City permits, prior to issuance of first architectural addenda, and shall complete construction of all required street improvements prior to issuance of first

temporary certificate of occupancy.

For information about compliance, contact the Case Planner, Planning Department at 628.652.7334, www.sfplanning.org

- 12. Signage.** The Project Sponsor shall develop a signage program for the Project which shall be subject to review and approval by Planning Department staff before submitting any building permits for construction of the Project. All subsequent sign permits shall conform to the approved signage program. Once approved by the Department, the signage program/plan information shall be submitted and approved as part of the site permit for the Project. All exterior signage shall be designed to complement, not compete with, the existing architectural character and architectural features of the building.

For information about compliance, contact the Case Planner, Planning Department at 628.652.7334, www.sfplanning.org

- 13. Transformer Vault Location.** The location of individual project PG&E Transformer Vault installations has significant effects to San Francisco streetscapes when improperly located. However, they may not have any impact if they are installed in preferred locations. Therefore, the Planning Department in consultation with Public Works shall require the following location(s) for transformer vault(s) for this project: private site area within the buildings along Broadway and Montgomery Streets. The above requirement shall adhere to the Memorandum of Understanding regarding Electrical Transformer Locations for Private Development Projects between Public Works and the Planning Department dated January 2, 2019.

For information about compliance, contact Bureau of Street Use and Mapping, Department of Public Works at 628.271.2000, www.sfpbpublicworks.org

- 14. Overhead Wiring.** The Property owner will allow MUNI to install eyebolts in the building adjacent to its electric streetcar line to support its overhead wire system if requested by MUNI or MTA.

For information about compliance, contact San Francisco Municipal Railway (Muni), San Francisco Municipal Transit Agency (SFMTA), at 415.701.4500, www.sfmta.org

- 15. Noise, Ambient.** Interior occupiable spaces shall be insulated from ambient noise levels. Specifically, in areas identified by the Environmental Protection Element, Map1, "Background Noise Levels," of the General Plan that exceed the thresholds of Article 29 in the Police Code, new developments shall install and maintain glazing rated to a level that insulate interior occupiable areas from Background Noise and comply with Title 24.

For information about compliance, contact the Environmental Health Section, Department of Public Health at 415.252.3800, www.sfdph.org

- 16. Noise.** Plans submitted with the building permit application for the approved project shall incorporate acoustical insulation and other sound proofing measures to control noise.

For information about compliance, contact the Case Planner, Planning Department at 628.652.7334, www.sfplanning.org

Parking and Traffic

- 17. Transportation Demand Management (TDM) Program.** Pursuant to Planning Code Section 169, the Project shall finalize a TDM Plan prior to the issuance of the first Building Permit or Site Permit to construct the project and/or commence the approved uses. The Property Owner, and all successors, shall ensure ongoing compliance with the TDM Program for the life of the Project, which may include providing a TDM Coordinator, providing access to City staff for site inspections, submitting appropriate documentation, paying application fees associated with required monitoring and reporting, and other actions.

Prior to the issuance of the first Building Permit or Site Permit, the Zoning Administrator shall approve and order the recordation of a Notice in the Official Records of the Recorder of the City and County of San Francisco for the subject property to document compliance with the TDM Program. This Notice shall provide the finalized TDM Plan for the Project, including the relevant details associated with each TDM measure included in the Plan, as well as associated monitoring, reporting, and compliance requirements.

For information about compliance, contact the TDM Performance Manager at tdm@sfgov.org or 628.652.7340, www.sfplanning.org

- 18. Parking for Affordable Units.** All off-street parking spaces shall be made available to Project residents only as a separate “add-on” option for purchase or rent and shall not be bundled with any Project dwelling unit for the life of the dwelling units. The required parking spaces may be made available to residents within a quarter mile of the project. All affordable dwelling units pursuant to Planning Code Section 415 shall have equal access to use of the parking as the market rate units, with parking spaces priced commensurate with the affordability of the dwelling unit. Each unit within the Project shall have the first right of refusal to rent or purchase a parking space until the number of residential parking spaces are no longer available. No conditions may be placed on the purchase or rental of dwelling units, nor may homeowner’s rules be established, which prevent or preclude the separation of parking spaces from dwelling units.

For information about compliance, contact Code Enforcement, Planning Department at 628.652.7463, www.sfplanning.org

- 19. Bicycle Parking** Pursuant to Planning Code Sections 155, 155.1, and 155.2, the Project shall provide no fewer than **54** bicycle parking spaces (**42** Class 1 and **2** Class spaces for the residential portion of the Project and **5** Class 1 and **5** Class 2 spaces for the commercial portion of the Project). SFMTA has final authority on the type, placement and number of Class 2 bicycle racks within the public ROW. Prior to issuance of first architectural addenda, the project sponsor shall contact the SFMTA Bike Parking Program at bikeparking@sfmta.com to coordinate the installation of on-street bicycle racks and ensure that the proposed bicycle racks meet the SFMTA’s bicycle parking guidelines. Depending on local site conditions and anticipated demand, SFMTA may request the project sponsor pay an in-lieu fee for Class II bike racks required by the Planning Code.

For information about compliance, contact Code Enforcement, Planning Department at 628.652.7463, www.sfplanning.org

- 20. Parking Maximum.** Pursuant to Planning Code Section 151 or 151.1, the Project shall provide no more than **twenty (20)** residential off-street parking spaces and no more than **forty-seven (47)** non-residential off-street parking spaces.

For information about compliance, contact Code Enforcement, Planning Department at 628.652.7463, www.sfplanning.org

- 21. Managing Traffic During Construction.** The Project Sponsor and construction contractor(s) shall coordinate with the Traffic Engineering and Transit Divisions of the San Francisco Municipal Transportation Agency (SFMTA), the Police Department, the Fire Department, the Planning Department, and other construction contractor(s) for any concurrent nearby Projects to manage traffic congestion and pedestrian circulation effects during construction of the Project.

For information about compliance, contact Code Enforcement, Planning Department at 628.652.7463, www.sfplanning.org

Provisions

- 22. Anti-Discriminatory Housing.** The Project shall adhere to the requirements of the Anti-Discriminatory Housing policy, pursuant to Administrative Code Section 1.61.

For information about compliance, contact the Case Planner, Planning Department at 628.652.7334, www.sfplanning.org

- 23. First Source Hiring.** The Project shall adhere to the requirements of the First Source Hiring Construction and End-Use Employment Program approved by the First Source Hiring Administrator, pursuant to Section 83.4(m) of the Administrative Code. The Project Sponsor shall comply with the requirements of this Program regarding construction work and on-going employment required for the Project.

For information about compliance, contact the First Source Hiring Manager at 415.581.2335, www.onestopSF.org

- 24. Transportation Sustainability Fee.** The Project is subject to the Transportation Sustainability Fee (TSF), as applicable, pursuant to Planning Code Section 411A.

For information about compliance, contact the Case Planner, Planning Department at 628.652.7334, www.sfplanning.org

- 25. Residential Child Care Impact Fee.** The Project is subject to the Residential Child Care Fee, as applicable, pursuant to Planning Code Section 414A.

For information about compliance, contact the Case Planner, Planning Department at 628.652.7334, www.sfplanning.org

- 26. Affordable Units.** The following Inclusionary Affordable Housing Requirements are those in effect at the time of Planning Commission action. In the event that the requirements change, the Project Sponsor shall comply with the requirements in place at the time of issuance of first construction document.

- A. **State Density Bonus Regulatory Agreement.** Recipients of development bonuses under this Section 206.6 shall enter into a Regulatory Agreement with the City, as follows.

- i. The terms of the agreement shall be acceptable in form and content to the Planning Director, the Director of MOHCD, and the City Attorney. The Planning Director shall have the authority to execute such agreements.
- ii. Following execution of the agreement by all parties, the completed Regulatory Agreement, or memorandum thereof, shall be recorded and the conditions filed and recorded on the Housing Project.
- iii. The approval and recordation of the Regulatory Agreement shall take place prior to the issuance of the First Construction Document. The Regulatory Agreement shall be binding to all future owners and successors in interest.
- iv. The Regulatory Agreement shall be consistent with the guidelines of the City's Inclusionary Housing Program and shall include at a minimum the following:
 - a. The total number of dwelling units approved for the Housing Project, including the number of restricted affordable units;
 - b. A description of the household income group to be accommodated by the State Density Bonus Units, and the standards for determining the corresponding Affordable Rent or Affordable Sales Price. If required by the Procedures Manual, the project sponsor must commit to completing a market survey of the area before marketing restricted affordable units;
 - c. The location, dwelling unit sizes (in square feet), and number of bedrooms of the restricted affordable units;
 - d. Term of use restrictions for the life of the project;¹
 - e. A schedule for completion and occupancy of restricted affordable units;
 - f. A description of any Concession, Incentive, waiver, or modification, if any, being provided by the City;
 - g. A description of remedies for breach of the agreement (the City may identify tenants or qualified purchasers as third party beneficiaries under the agreement); and
 - h. Other provisions to ensure implementation and compliance with Section 206.6.

For information about compliance, contact the Case Planner, Planning Department at 415-575-9087, www.sf-planning.org or the Mayor's Office of Housing and Community Development at (415) 701-5500, www.sfmohcd.org.

- B. **Number of Required Units.** Pursuant to Planning Code Section 415.3, the Project is required to provide 20% of the proposed dwelling units in the base project as affordable to qualifying households. The base project contains 34 units; therefore, 6.8 affordable units are currently required. The Project Sponsor will fulfill this requirement by providing 6 affordable units on-site and providing the rest of the base project units and density bonus units through the affordable housing fee requirement of 33%. If the number of market-rate units change, the number of required affordable units shall be modified accordingly with written approval from Planning Department staff in consultation with the Mayor's Office of Housing and Community Development ("MOHCD").

For information about compliance, contact the Case Planner, Planning Department at (628) 652-7600, www.sfplanning.org or the Mayor's Office of Housing and Community Development at (415) 701-5500, www.sfmohcd.org.

- C. **Unit Mix.** The Project contains 16 one-bedroom, 21 two-bedroom, and five three-bedroom units; therefore, the required affordable unit mix is two one-bedroom and three two-bedroom. If the market-rate unit mix changes, the affordable unit mix will be modified accordingly with written approval from Planning Department staff in consultation with MOHCD.

For information about compliance, contact the Case Planner, Planning Department at (628) 652-7600, www.sfplanning.org or the Mayor's Office of Housing and Community Development at (415) 701-5500, www.sfmohcd.org.

- D. **Mixed Income Levels for Affordable Units.** Pursuant to Planning Code Section 415.3, the Project is required to provide 20% of the proposed dwelling units as affordable to qualifying households. At least 10% must be affordable to low-income households, at least 5% must be affordable to moderate income households, and at least 5% must be affordable to middle income households in the event the Project is a rental project. Ownership Units for low-income households shall have an affordable sales price set at 80% of Area Median Income or less, with households earning up to 100% of Area Median Income eligible to apply for low-income units. Ownership Units for moderate-income households shall have an affordable sales price set at 105% of Area Median Income or less, with households earning from 95% to 120% of Area Median Income eligible to apply for moderate-income units. Ownership Units for middle-income households shall have an affordable sales price set at 130% of Area Median Income or less, with households earning from 120% to 150% of Area Median Income eligible to apply for middle-income units. For any affordable units with sales prices set at 130% of Area Median Income, the units shall have a minimum occupancy of two persons. If the number of market-rate units change, the number of required affordable units shall be modified accordingly with written approval from Planning Department staff in consultation with the Mayor's Office of Housing and Community Development ("MOHCD").

For information about compliance, contact the Case Planner, Planning Department at (628) 652-7600, www.sfplanning.org or the Mayor's Office of Housing and Community Development at (415) 701-5500, www.sfmohcd.org.

- E. **Minimum Unit Sizes.** The affordable units shall meet the minimum unit sizes standards established by the California Tax Credit Allocation Committee (TCAC) as of May 16, 2017. One-bedroom units must be at least 450 square feet, two-bedroom units must be at least 700 square feet, and three-bedroom units must be at least 900 square feet. Studio units must be at least 300 square feet pursuant to Planning Code Section 415.6(f)(2). The total residential floor area devoted to the affordable units shall not be less than the applicable percentage applied to the total residential floor area of the principal project, provided that a 10% variation in floor area is permitted.

For information about compliance, contact the Case Planner, Planning Department at (628) 652-7600, www.sfplanning.org or the Mayor's Office of Housing and Community Development at (415) 701-5500, www.sfmohcd.org.

- F. **Notice of Special Restrictions.** The affordable units shall be designated on a reduced set of plans recorded as a Notice of Special Restrictions on the property prior to architectural addenda.

For information about compliance, contact the Case Planner, Planning Department at (628) 652-7600, www.sfplanning.org or the Mayor's Office of Housing and Community Development at (415) 701-5500, www.sfmohcd.org.

- G. **Phasing.** If any building permit is issued for partial phasing of the Project, the Project Sponsor shall have designated not less than seven point three percent (7.3%), or the applicable percentage as discussed above, of each phase's total number of dwelling units as on-site affordable units.

For information about compliance, contact the Case Planner, Planning Department at (628) 652-7600, www.sfplanning.org or the Mayor's Office of Housing and Community Development at (415) 701-5500, www.sfmohcd.org.

- H. **Duration.** Under Planning Code Section 415.8, all units constructed pursuant to Section 415.6, must remain affordable to qualifying households for the life of the project.

For information about compliance, contact the Case Planner, Planning Department at (628) 652-7600, www.sfplanning.org or the Mayor's Office of Housing and Community Development at (415) 701-5500, www.sfmohcd.org.

- I. **Expiration of the Inclusionary Rate.** Pursuant to Planning Code Section 415.6(a)(10), if the Project has not obtained a site or building permit within 30 months of Planning Commission Approval of this Motion No. XXXXX, then it is subject to the Inclusionary Affordable Housing Requirements in effect at the time of site or building permit issuance.

For information about compliance, contact the Case Planner, Planning Department at (628) 652-7600, www.sfplanning.org or the Mayor's Office of Housing and Community Development at (415) 701-5500, www.sfmohcd.org.

- J. **Reduction of On-Site Units after Project Approval.** Pursuant to Planning Code Section 415.5(g)(3), any changes by the project sponsor which result in the reduction of the number of on-site affordable units shall require public notice for hearing and approval from the Planning Commission.

For information about compliance, contact the Case Planner, Planning Department at (628) 652-7600, www.sfplanning.org or the Mayor's Office of Housing and Community Development at (415) 701-5500, www.sfmohcd.org.

- K. **20% below market sales prices.** Pursuant to PC Section 415.6, the maximum affordable sales price shall be no higher than 20% below market sales prices for the neighborhood within which the project is located, which shall be defined in accordance with the American Community Survey Neighborhood Profile Boundaries Map. MOHCD shall adjust the allowable sales prices, and the eligible households for such units, accordingly, and such potential readjustment shall be a condition of approval upon project

entitlement. The City shall review the updated data on neighborhood sales prices on an annual basis.

- L. **Other Conditions.** The Project is subject to the requirements of the Inclusionary Affordable Housing Program under Section 415 et seq. of the Planning Code and City and County of San Francisco Inclusionary Affordable Housing Program Monitoring and Procedures Manual ("Procedures Manual"). The Procedures Manual, as amended from time to time, is incorporated herein by reference, as published and adopted by the Planning Commission, and as required by Planning Code Section 415. Terms used in these conditions of approval and not otherwise defined shall have the meanings set forth in the Procedures Manual. A copy of the Procedures Manual can be obtained at the MOHCD at 1 South Van Ness Avenue or on the Planning Department or MOHCD websites, including on the internet at: <http://sf-planning.org/Modules/ShowDocument.aspx?documentid=4451>. As provided in the Inclusionary Affordable Housing Program, the applicable Procedures Manual is the manual in effect at the time the subject units are made available for sale.

For information about compliance, contact the Case Planner, Planning Department at (628) 652-7600, www.sfplanning.org or the Mayor's Office of Housing and Community Development at (415) 701-5500, www.sfmohcd.org.

- i. The affordable unit(s) shall be designated on the building plans prior to the issuance of the first construction permit by the Department of Building Inspection ("DBI"). The affordable unit(s) shall (1) be constructed, completed, ready for occupancy and marketed no later than the market rate units, and (2) be evenly distributed throughout the building; and (3) be of comparable overall quality, construction and exterior appearance as the market rate units in the principal project. The interior features in affordable units should be generally the same as those of the market units in the principal project, but need not be the same make, model or type of such item as long they are of good and new quality and are consistent with then-current standards for new housing. Other specific standards for on-site units are outlined in the Procedures Manual.
- ii. If the units in the building are offered for sale, the affordable unit(s) shall be sold to first time home buyer households with a minimum of 10% of the units affordable to low-income households, 5% to moderate-income households, and the remaining 5% of the units affordable to middle-income households. The affordable unit shall be affordable to low-income households, as defined in the Planning Code and Procedures Manual. The initial sales price of such units shall be calculated according to the Procedures Manual. Limitations on (i) reselling; (ii) renting; (iii) recouping capital improvements; (iv) refinancing; and (v) procedures for inheritance apply and are set forth in the Inclusionary Affordable Housing Program and the Procedures Manual.
- iii. The Project Sponsor is responsible for following the marketing, reporting, and monitoring requirements and procedures as set forth in the Procedures Manual. MOHCD shall be responsible for overseeing and monitoring the marketing of affordable units. The Project Sponsor must contact MOHCD at least six months prior to the beginning of marketing for any unit in the building.
- iv. Required parking spaces shall be made available to initial buyers or renters of affordable units according to the Procedures Manual.

- v. Prior to the issuance of the architectural addendum first construction permit by DBI for the Project, the Project Sponsor shall record a Notice of Special Restriction on the property that contains these conditions of approval and a reduced set of plans that identify the affordable units satisfying the requirements of this approval. The Project Sponsor shall promptly provide a copy of the recorded Notice of Special Restriction to the Department and to MOHCD or its successor.
- vi. If the Project Sponsor fails to comply with the Inclusionary Affordable Housing Program requirement, the Director of DBI shall deny any and all site or building permits or certificates of occupancy for the development project until the Planning Department notifies the Director of compliance. A Project's failure to comply with the requirements of Planning Code Section 415 et seq. shall constitute cause for the City to record a lien against the development project and to pursue any and all available remedies at law, including penalties and interest, if applicable

Monitoring - After Entitlement

27. Enforcement. Violation of any of the Planning Department conditions of approval contained in this Motion or of any other provisions of Planning Code applicable to this Project shall be subject to the enforcement procedures and administrative penalties set forth under Planning Code Section 176 or Section 176.1. The Planning Department may also refer the violation complaints to other city departments and agencies for appropriate enforcement action under their jurisdiction.

For information about compliance, contact Code Enforcement, Planning Department at 628.652.7463, www.sfplanning.org

28. Monitoring. The Project requires monitoring of the conditions of approval in this Motion. The Project Sponsor or the subsequent responsible parties for the Project shall pay fees as established under Planning Code Section 351(e) (1) and work with the Planning Department for information about compliance.

For information about compliance, contact Code Enforcement, Planning Department at 628.652.7463, www.sfplanning.org

29. Revocation due to Violation of Conditions. Should implementation of this Project result in complaints from interested property owners, residents, or commercial lessees which are not resolved by the Project Sponsor and found to be in violation of the Planning Code and/or the specific conditions of approval for the Project as set forth in Exhibit A of this Motion, the Zoning Administrator shall refer such complaints to the Commission, after which it may hold a public hearing on the matter to consider revocation of this authorization.

For information about compliance, contact Code Enforcement, Planning Department at 628.652.7463, www.sfplanning.org

Operation

30. Eating and Drinking Uses. As defined in Planning Code Section 202.2, Eating and Drinking Uses, as defined in [Section 102](#), shall be subject to the following conditions:

- A. The business operator shall maintain the main entrance to the building and all sidewalks abutting the subject property in a clean and sanitary condition in compliance with the Department of Public Works Street and Sidewalk Maintenance Standards. In addition, the operator shall be responsible for daily monitoring of the sidewalk within a one-block radius of the subject business to maintain the sidewalk free of paper or other litter associated with the business during business hours, in accordance with Article 1, [Section 34](#) of the San Francisco Police Code.

For information about compliance, contact the Bureau of Street Use and Mapping, Department of Public Works at 628.271.2000, www.sfpublicworks.org.

- B. When located within an enclosed space, the premises shall be adequately soundproofed or insulated for noise and operated so that incidental noise shall not be audible beyond the premises or in other sections of the building, and fixed-source equipment noise shall not exceed the decibel levels specified in the San Francisco Noise Control Ordinance.

For information about compliance of fixed mechanical objects such as rooftop air conditioning, restaurant ventilation systems, and motors and compressors with acceptable noise levels, contact the Environmental Health Section, Department of Public Health at 415.252.3800, www.sfdph.org.

For information about compliance with construction noise requirements, contact the Department of Building Inspection at 628.652.3200, www.sfdbi.org.

For information about compliance with the requirements for amplified sound, including music and television, contact the Police Department at 415.553.0123, www.sf-police.org

- C. While it is inevitable that some low level of odor may be detectable to nearby residents and passersby, appropriate odor control equipment shall be installed in conformance with the approved plans and maintained to prevent any significant noxious or offensive odors from escaping the premises.

For information about compliance with odor or other chemical air pollutants standards, contact the Bay Area Air Quality Management District, (BAAQMD), 1-800-334-ODOR (6367), www.baaqmd.gov and Code Enforcement, Planning Department at 628.652.7600, www.sfplanning.org

- D. Garbage, recycling, and compost containers shall be kept within the premises and hidden from public view, and placed outside only when being serviced by the disposal company. Trash shall be contained and disposed of pursuant to garbage and recycling receptacles guidelines set forth by the Department of Public Works.

For information about compliance, contact the Bureau of Street Use and Mapping, Department of Public Works at 628.271.2000, www.sfpublicworks.org

- 31. Sidewalk Maintenance.** The Project Sponsor shall maintain the main entrance to the building and all sidewalks abutting the subject property in a clean and sanitary condition in compliance with the Department of Public Works Streets and Sidewalk Maintenance Standards.

For information about compliance, contact Bureau of Street Use and Mapping, Department of Public Works,

628.271.2000, www.sfpublicworks.org

- 32. Community Liaison.** Prior to issuance of a building permit to construct the project and implement the approved use, the Project Sponsor shall appoint a community liaison officer to deal with the issues of concern to owners and occupants of nearby properties. The Project Sponsor shall provide the Zoning Administrator and all registered neighborhood groups for the area with written notice of the name, business address, and telephone number of the community liaison. Should the contact information change, the Zoning Administrator and registered neighborhood groups shall be made aware of such change. The community liaison shall report to the Zoning Administrator what issues, if any, are of concern to the community and what issues have not been resolved by the Project Sponsor.

For information about compliance, contact Code Enforcement, Planning Department at 628.652.7463, www.sfplanning.org

- 33. Lighting.** All Project lighting shall be directed onto the Project site and immediately surrounding sidewalk area only, and designed and managed so as not to be a nuisance to adjacent residents. Nighttime lighting shall be the minimum necessary to ensure safety, but shall in no case be directed so as to constitute a nuisance to any surrounding property.

For information about compliance, contact Code Enforcement, Planning Department at 628.652.7463, www.sfplanning.org



PROJECT DESCRIPTION

Replace the existing 144-space car parking garage with a mixed use project. Because of a below grade sewer easement that bisects the site, the project comprises of two mixed-use buildings on a single lot (a 5 story building facing Broadway and a 7 story building facing Montgomery) containing a total of 42 dwelling units, approximately 17,995 gsf of design professional office space, 4,940 gsf of ground floor retail space fronting both Broadway and Montgomery Streets, a below grade parking garage containing 17 vehicle parking spaces (for residential use), 47 Class 1 bicycle parking spaces and 7 Class 2 bicycle parking spaces.

The project is seeking a 21.5% density bonus utilizing the Individually Requested State Density Bonus Program to increase the number of residential units on site.

DRAWING SET

- A-0.0 - SITE PLAN
- A-1.0 - BASEMENT 2
- A-2.0 - BASEMENT 1
- A-3.0 - PLAN LEVEL 1
- A-4.0 - PLAN LEVEL 2
- A-5.0 - PLAN LEVEL 3
- A-6.0 - PLAN LEVEL 4
- A-7.0 - PLAN LEVEL 5
- A-8.0 - PLAN LEVEL 6
- A-9.0 - ROOF PLAN
- A-10.0 - SECTION
- A-11.0 - SECTION
- A-12.0 - SECTION
- A-13.0 - SECTION
- A-14.0 - SECTION
- A-15.0 - SECTION
- A-16.0 - ELEVATION
- A-17.0 - ELEVATION
- A-18.0 - ELEVATION
- A-19.0 - MODEL VIEWS
- A-20.0 - MODEL VIEWS
- A-21.0 - MODEL VIEWS
- A-22.0 - MODEL VIEWS
- A-23.0 - PHOTOGRAPHS
- A-24.0 - PHOTOGRAPHS
- A-25.0 - AERIAL PHOTOS
- A-26.0 - AERIAL PHOTOS
- A-27.0 - DATA SHEET
- A-28.0 - F.A.R. EXHIBIT
- A-29.0 - REAR YARD EXHIBIT
- A-30.0 - MASSING ILLUSTRATION
- A-31.0 - DU EXPOSURE EXHIBIT
- A-32.0 - HT. AND BULK EXHIBIT
- A-33.0 - ESTIMATED EXCAVATION
- A-34.0 - GROUND FLR. CEILING HT.
- A-35.0 - ST. FRONT FENESTRATION
- A-36.0 - LANDSCAPING PLAN
- A-37.0 - STREET-SCAPE PLAN
- A-40.1- EXTERIOR CLADDING DETAIL
- A-40.2- EXTERIOR CLADDING DETAIL
- A-40.3- EXT CLADDING- CORNICE DET
- A-41.0- MATERIAL BOARD
- SU-1.0 - SITE SURVEY

Montgomery Place LLC
1630 Oakland Road #A215
San Jose, CA 95131
Richard Chen - Manager
jhrchen@gmail .com

OWNER

MONTGOMERY PLACE

425 Broadway Street
San Francisco,
CA 94133.
APN : 0163/002

PROJECT

SCALE :

NTS

DRAWING TITLE :

COVER SHEET

SHEET NO. :

COVER

DATE

ISSUE

11/12/2021

DRAWING SET - FOR
CITY SUBMISSION



GENERAL NOTES:

- PROJECT WILL TAKE NECESSARY MEASURES TO COMPLY WITH THE REQUIREMENTS LISTED IN THE BIRD SAFETY GUIDELINES.
- ALL TOILETS AND DRINKING WATER STATIONS FOR THE OFFICE SPACES AND RETAIL SHALL BE PROVIDED AS PER THE T.I SCOPE.

WAIVERS REQUESTED:

- BULK
- REAR YARD
- DWELLING UNIT EXPOSURE

CONCESSION REQUESTED:

- NON-RESIDENTIAL USE SIZE

PROJECT SUMMARY

BLOCK LOT NUMBER: 0163 / 002

ZONING DISTRICT: NCD - Broadway NC

LOT AREA: 13,641 SF

HEIGHT AND BULK DISTRICT: 65 - A - 1

PROPOSED BLDG. HT: 64'-1-1/2" (Montgomery Bldg.)
55'-11-1/2" (Broadway Bldg.)

SITE'S EXISTING USE:
COMMERCIAL PARKING GARAGE

EXISTING PARKING GARAGE GSF: 21,725 SF

GSF AS DEFINED IN PLANNING CODE 102

- RESI USE: 52,365

- RETAIL USE: 4,940 SF (GROSS)

- OFFICE USE: 17,995 SF (GROSS)

- PARKING USE: 3,959 SF (GROSS, NOT INCL. IN GSF CALCS. PER PLANNING CODE)

CAR PARKING SPACES: 17 CARS (INC. STACKERS)

BIKE PARKING - CLASS 1: 47 BIKES

BIKE PARKING - CLASS 2: 7 BIKES

PRIVATE USABLE OPEN SPACE: 480 SF+

COMMON USABLE OPEN SPACE: 2720 SF

TOTAL NUMBER OF UNITS: 42

NUMBER OF MARKET RATE DWELLING UNITS: 36

NUMBER OF ON-SITE AFFORDABLE DWELLING UNITS :
6 UNITS

DWELLING UNIT MIX

- NO. OF 3B UNITS: 5
- NO. OF 2B UNITS: 21
- NO. OF 1B UNITS: 16

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project contact: Ian Birchall

Montgomery Place LLC
1630 Oakland Road #A215
San Jose, CA 95131
Richard Chen - Manager
jhrchen@gmail .com

OWNER

MONTGOMERY PLACE

425 Broadway Street
San Francisco,
CA 94133.
PROJECT
APN : 0163/002

SCALE : 1/32" :1'0"

DRAWING TITLE : SITE PLAN

SHEET NO. : A-0.0

DATE ISSUE

11/12/2021 DRAWING SET - FOR CITY SUBMISSION



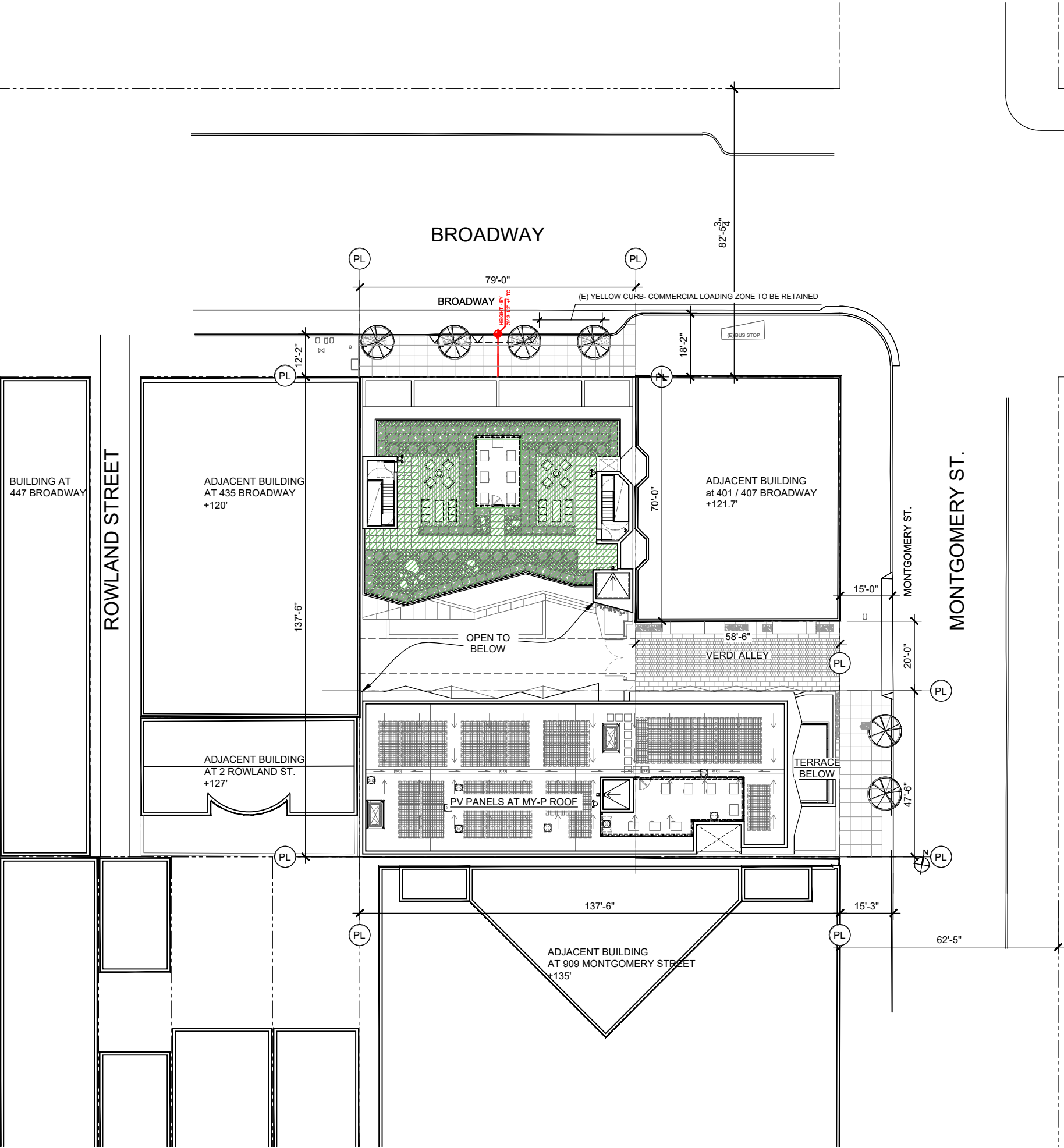
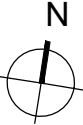
TOTAL NUMBER OF UNITS = 42 UNITS

16 X 1 BEDROOM UNITS = 38.1 %
21 X 2 BEDROOM UNITS = 50.0 %
5 X 3 BEDROOM UNITS = 11.9 %

NO LESS THAN 25% OF THE TOTAL NUMBER OF PROPOSED UNITS SHALL CONTAIN AT LEAST 2 BEDROOMS.
= 0.25 X 42 = 10.5
= 11 (2B/3B UNITS REQ.) MIN.
NO. OF 2 AND 3 BEDROOM UNITS PROVIDED = 21 + 5 = 26. PROJECT COMPLIES

NO LESS THAN 10% OF THE TOTAL NUMBER OF PROPOSED UNITS SHALL CONTAIN AT LEAST 3 BEDROOMS.
= 0.1 X 42 = 4.2
= 4 (3B UNITS REQ.) MIN.
NO. OF 3 BEDROOM UNITS PROVIDED = 5. PROJECT COMPLIES

MONTGOMERY PLACE			
	3B	2B	1B
P6	-	3	2
P5	1	4	4
P4	1	3	5
P3	1	5	3
P2	1	5	1
P1	1	1	1
B1	-	-	-
B2	-	-	-
TOTAL	5	21	16



Montgomery Place LLC
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San Jose, CA 95131
Richard Chen - Manager
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OWNER

MONTGOMERY PLACE

425 Broadway Street
San Francisco,
CA 94133.
APN : 0163/002

SCALE : 1/16" : 1'0"

DRAWING TITLE : BASEMENT 2

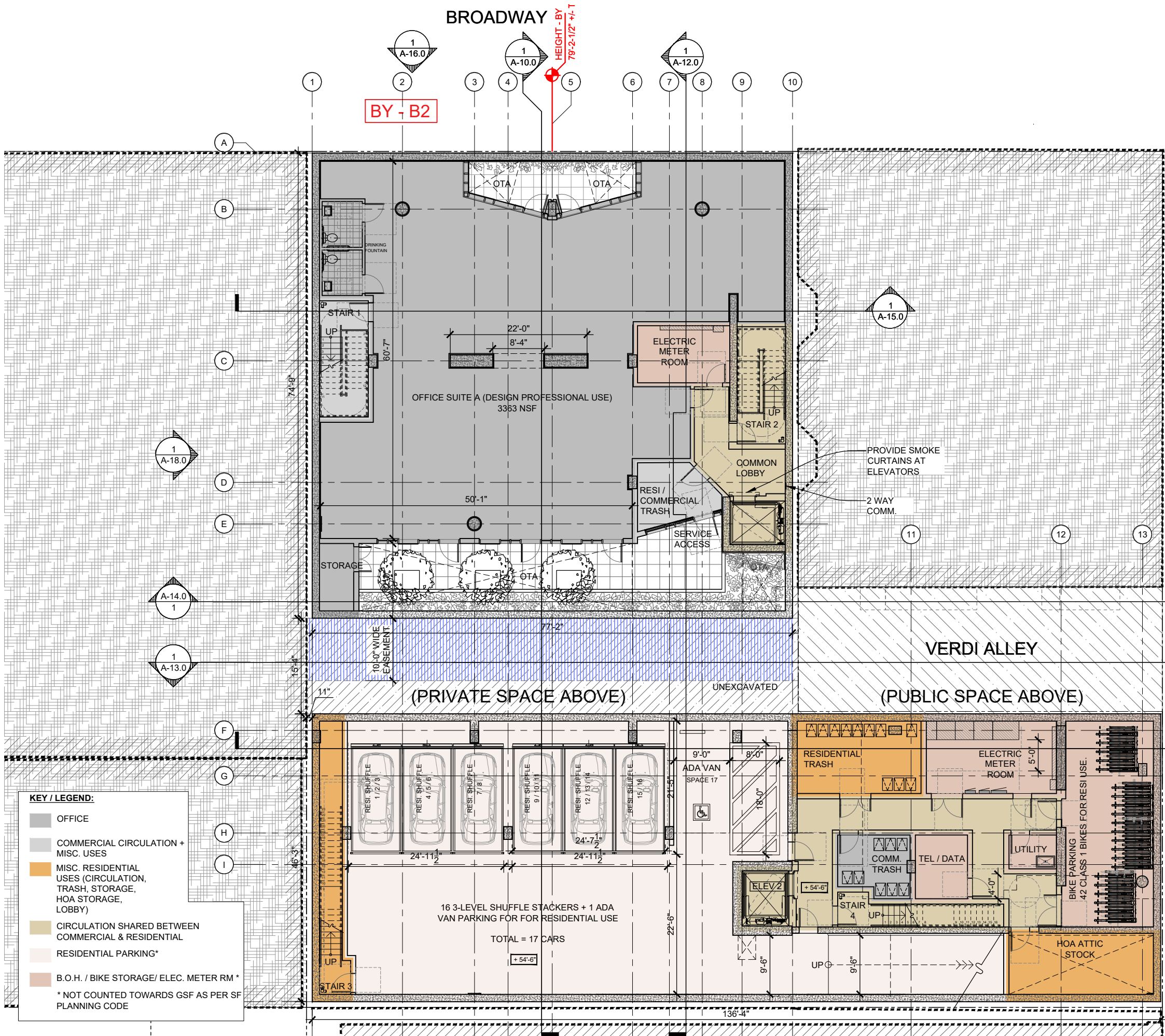
SHEET NO. : A-1.0

DATE : ISSUE

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project contact: Ian Birchall



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Richard Chen - Manager
jhrchen@gmail .com

OWNER

MONTGOMERY PLACE

425 Broadway Street
San Francisco,
CA 94133.
APN : 0163/002

SCALE : 1/16" : 1'0"

DRAWING TITLE : BASEMENT 1

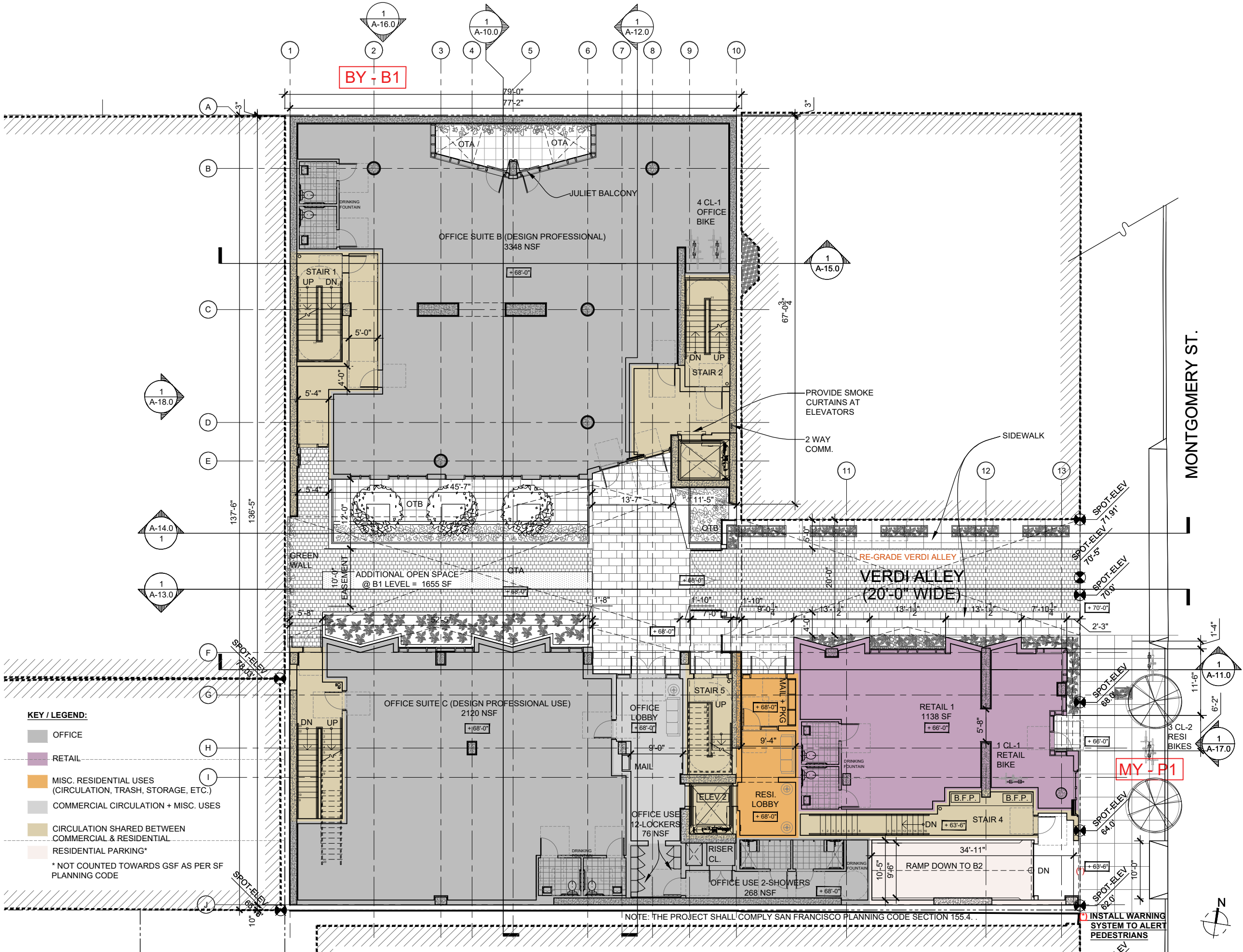
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project contact: Ian Birchall



KEY / LEGEND:

- OFFICE
- RETAIL
- MISC. RESIDENTIAL USES
(CIRCULATION, TRASH, STORAGE, ETC.)
- COMMERCIAL CIRCULATION + MISC. USES
- CIRCULATION SHARED BETWEEN
COMMERCIAL & RESIDENTIAL
- RESIDENTIAL PARKING*
- * NOT COUNTED TOWARDS GSF AS PER SF
PLANNING CODE

NOTE: THE PROJECT SHALL COMPLY SAN FRANCISCO PLANNING CODE SECTION 155.4.

INSTALL WARNING
SYSTEM TO ALERT
PEDESTRIANS

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SCALE : 1/16" = 1'0"

DRAWING TITLE : P1

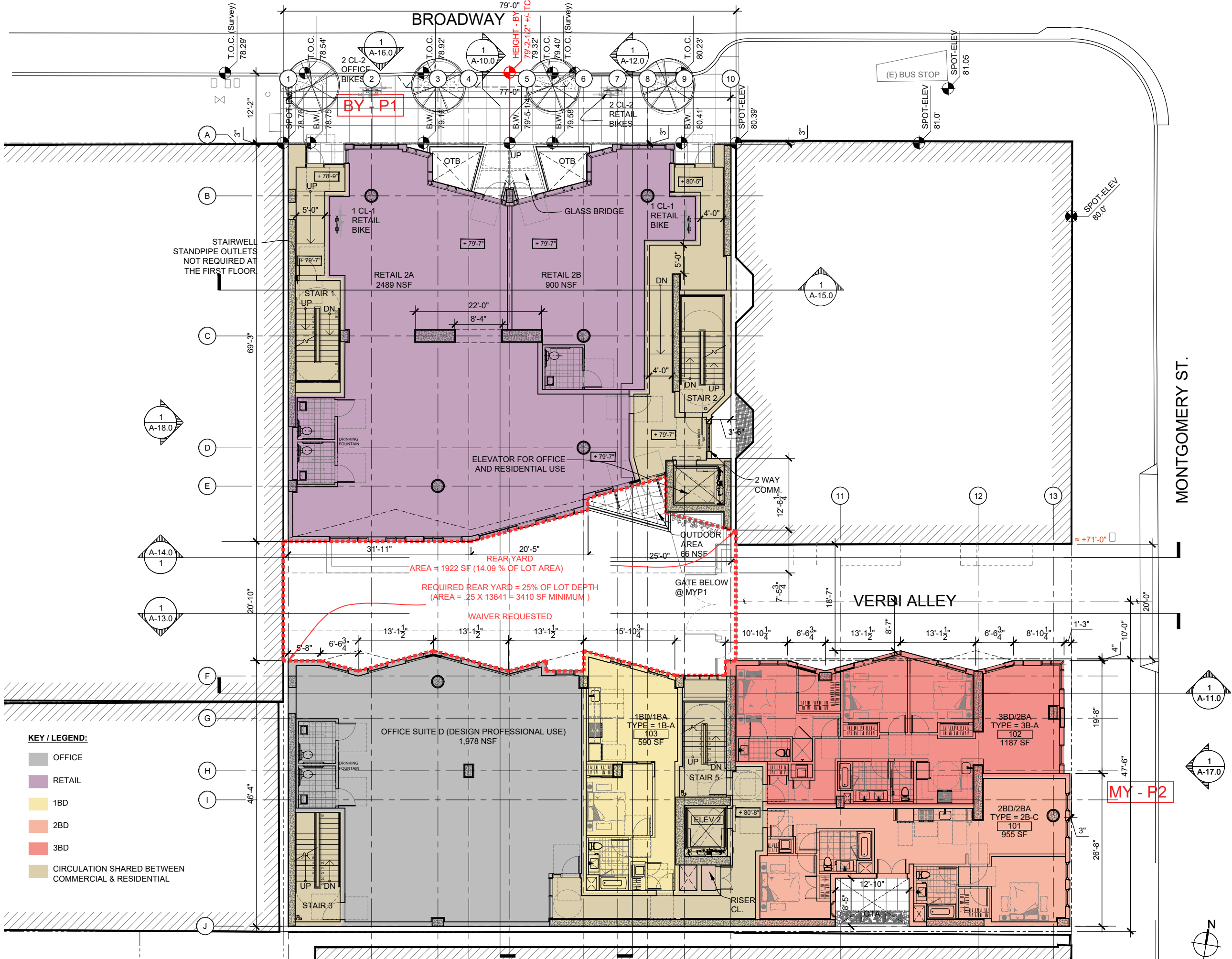
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PROJECT

APN : 0163/002

SCALE : 1/16" : 1'0"

DRAWING TITLE : P2

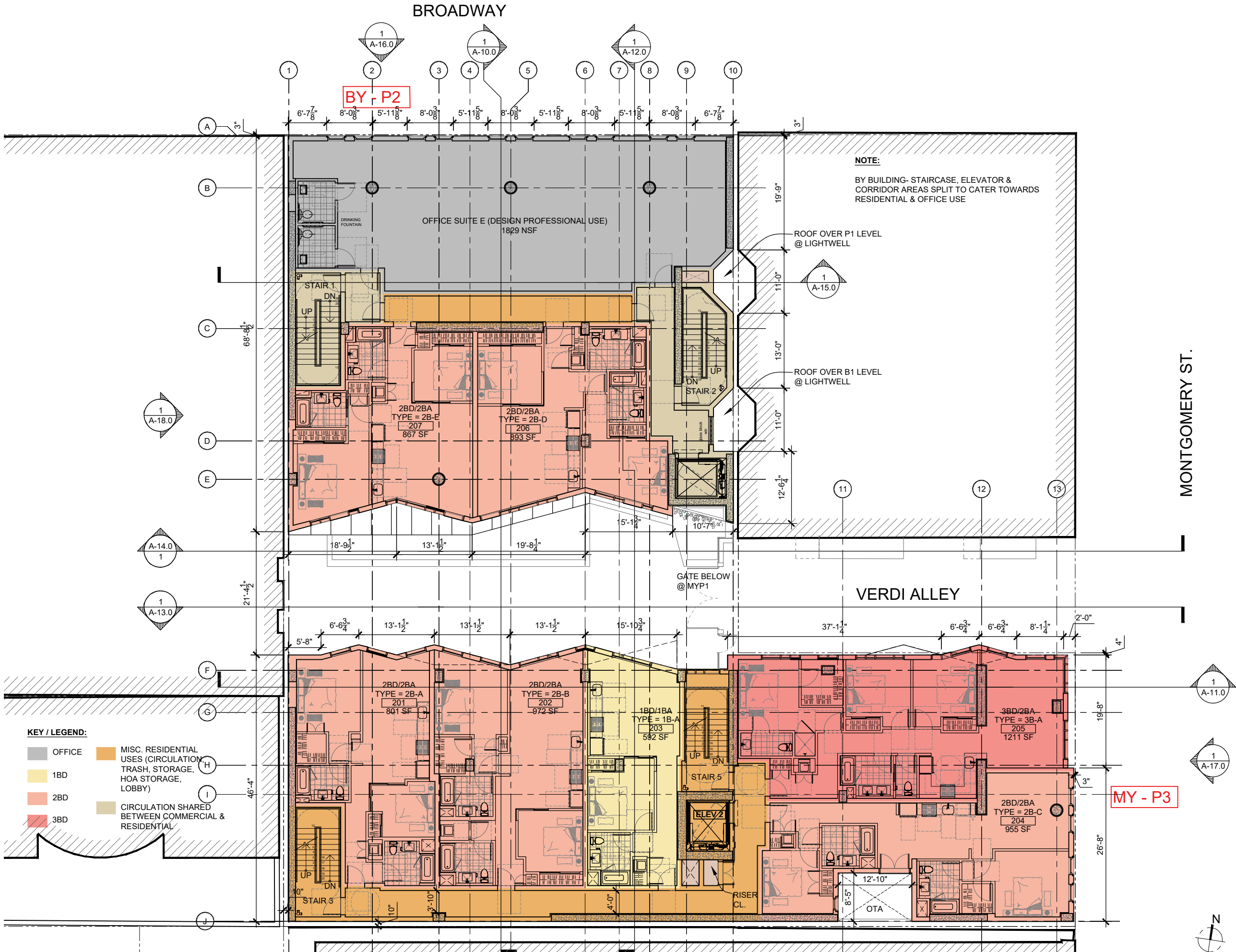
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SCALE : 1/16" : 1'0"






DRAWING TITLE : P3

SHEET NO. : A-5.0

DATE _____ ISSUE _____

11/12/2021 DRAWING SET - FOR CITY SUBMISSION



 1BD
 2BD
 3BD
 MISC. RESIDENTIAL USES (CIRCULATION, TRASH, STORAGE, HOA STORAGE, LOBBY)
 P.U.O.S

MY - P4

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APN : 0163/002

SCALE : 1/16" : 1'0"

DRAWING TITLE : P4

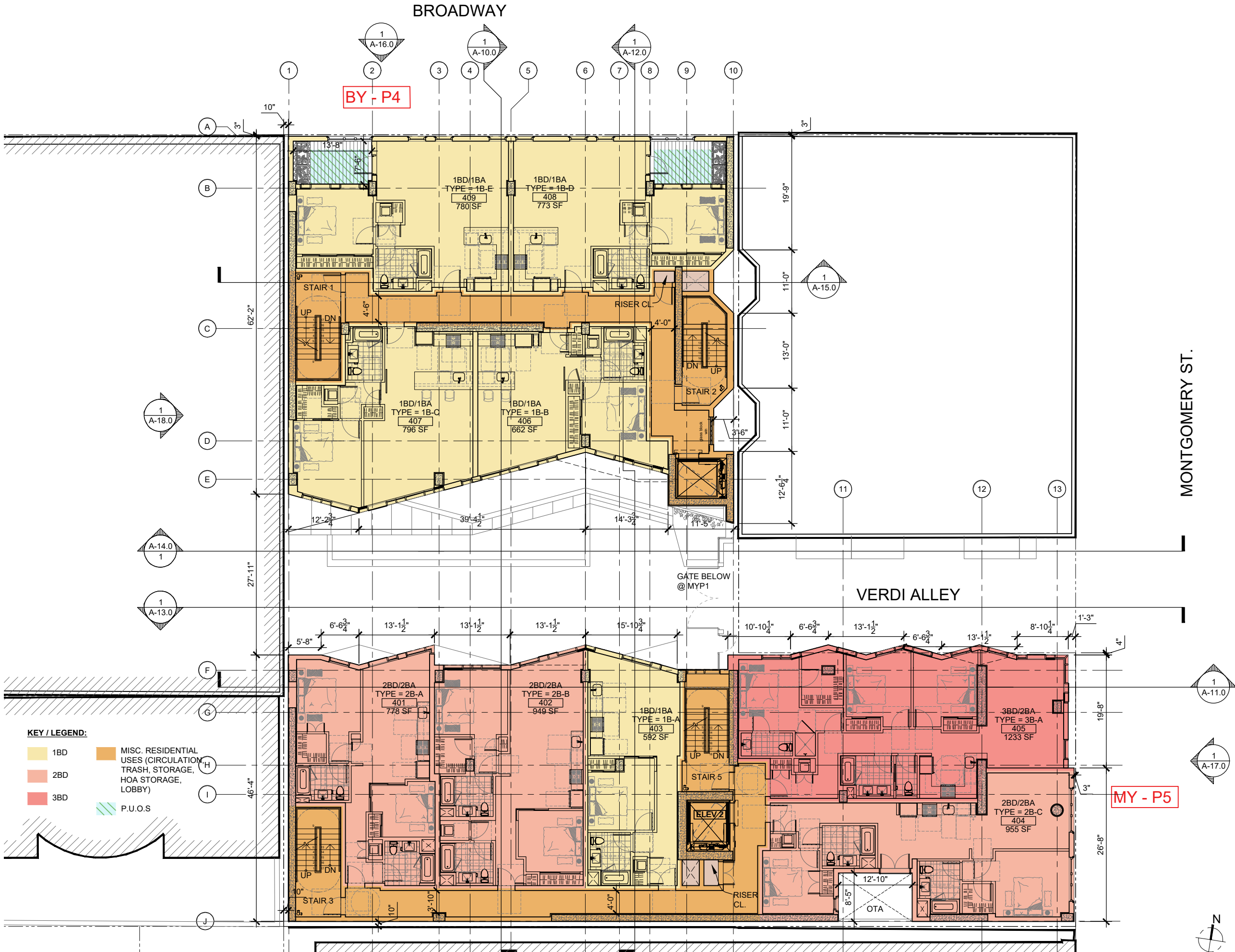
SHEET NO. : A-6.0

DATE ISSUE

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SCALE : 1/16" : 1'0"

DRAWING TITLE : P5

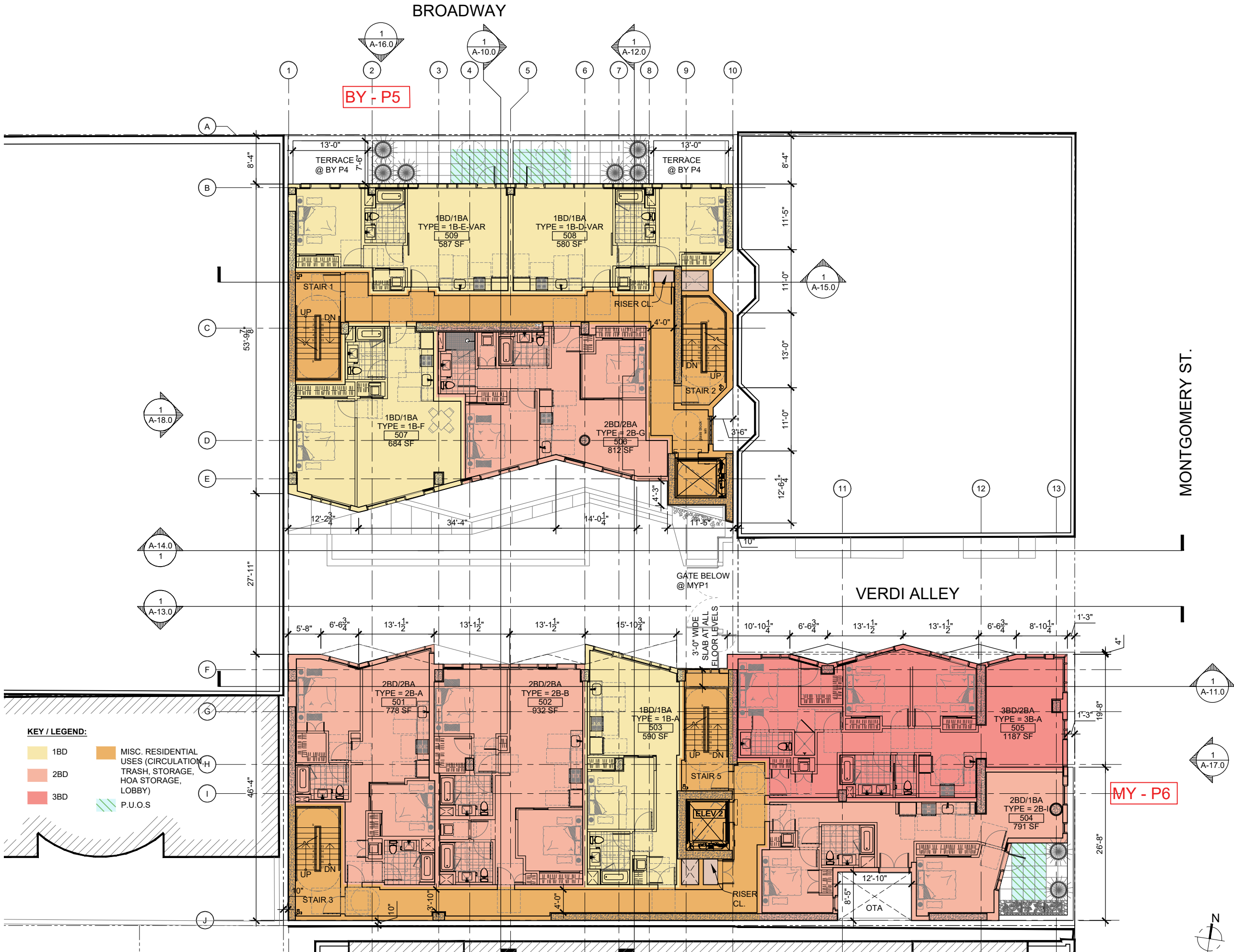
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DATE : ISSUE

11/12/2021
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APN : 0163/002

SCALE : 1/16" : 1'0"

DRAWING TITLE : P6

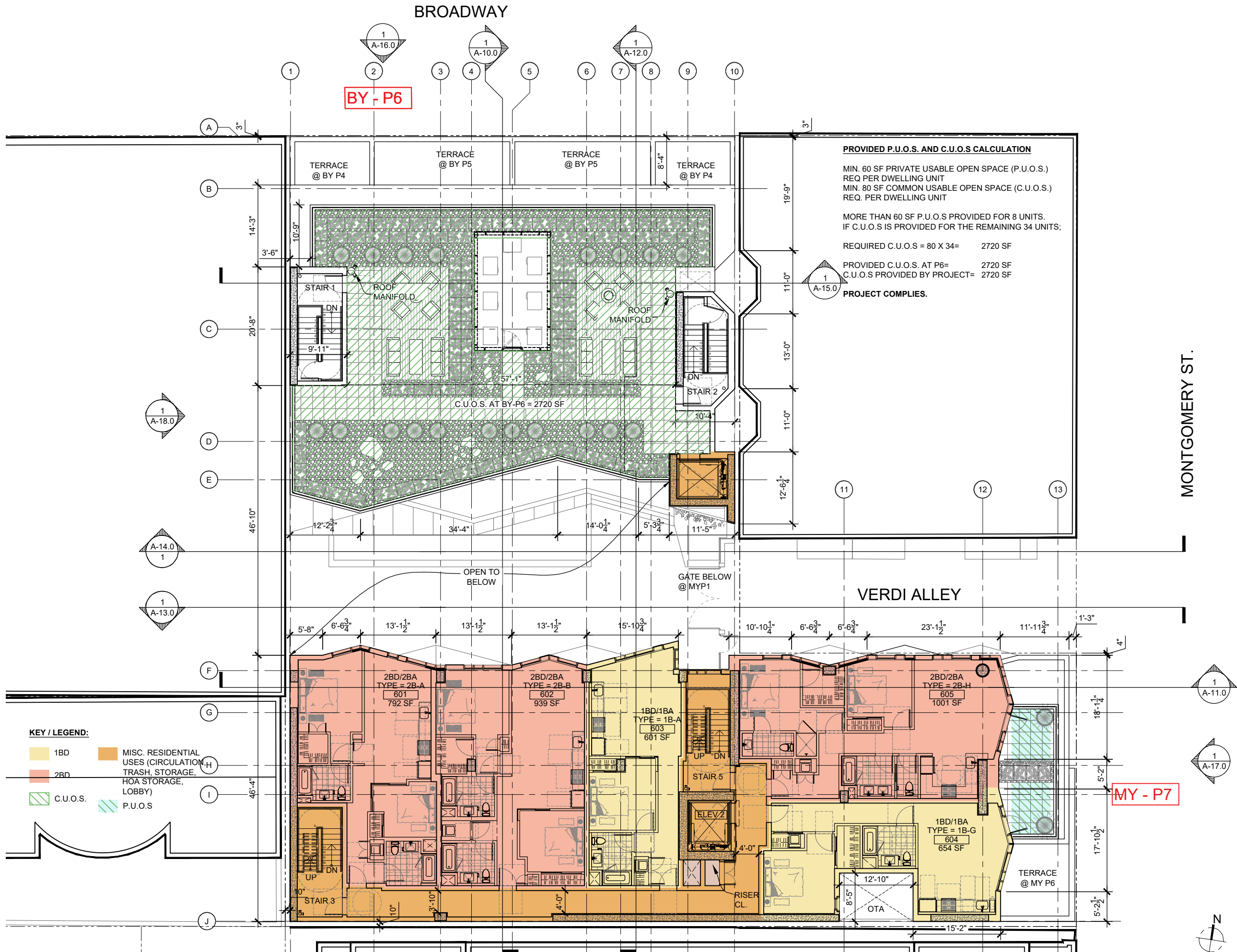
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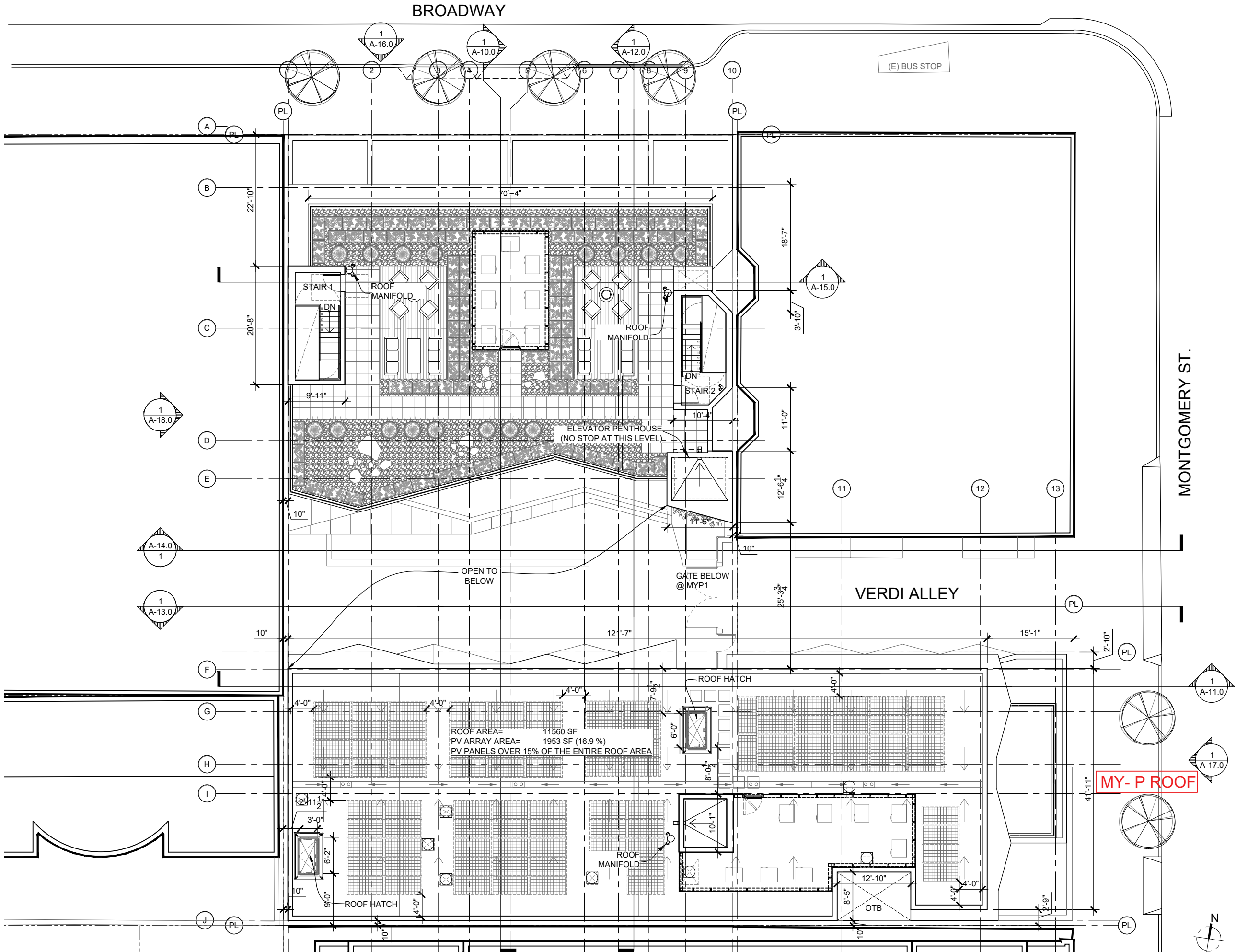
DATE	ISSUE
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PROJECT

APN : 0163/002

SCALE : 1/16" :1'0"

DRAWING TITLE : P ROOF

SHEET NO. : A-9.0

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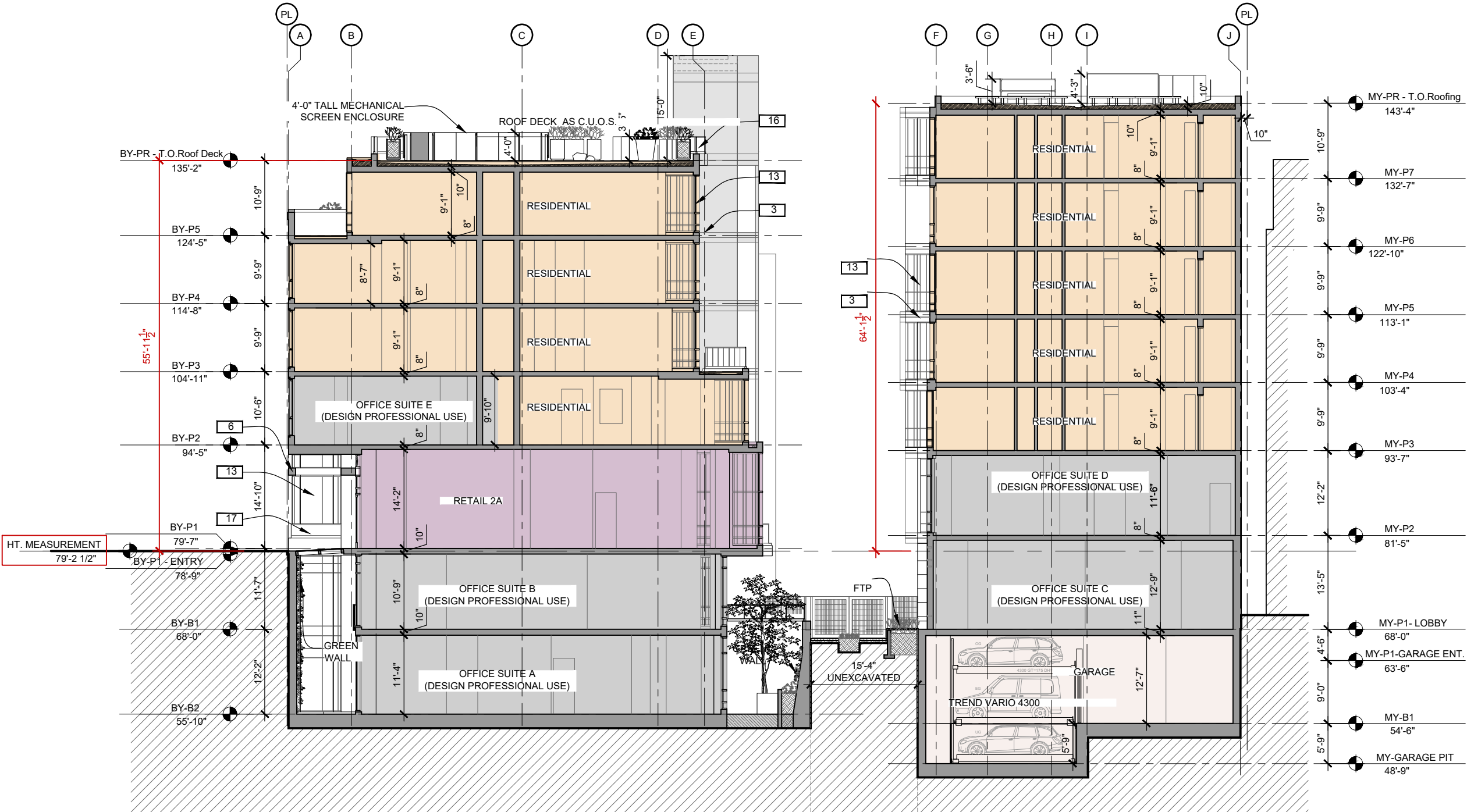
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DRAWING TITLE : SECTION

SHEET NO. : A-10.0

DATE ISSUE

11/12/2021 DRAWING SET - FOR
CITY SUBMISSION



KEY / LEGEND:

- RESIDENTIAL
- RETAIL
- OFFICE
- RESIDENTIAL PARKING*
- MISC. RESIDENTIAL USES (CIRCULATION, TRASH, STORAGE, HOA STORAGE, LOBBY)
- BOH / BIKE STORAGE *
- * NOT COUNTED TOWARDS GSF AS PER SF PLANNING CODE

ELEVATION KEYNOTES

- BRICK-IT THIN BRICK TILE- MANGANESE IRONPOT
- ECO STUCCO- TADELAKT FINISH (DARK GRAY)
- ECO STUCCO- TADELAKT FINISH (SUPER WHITE)
- ALUMINUM COPING (DARK GRAY)
- FLYING BEAM (STAINLESS STEEL - BRUSHED,120 GRIT)
- CANOPY (STAINLESS STEEL - BRUSHED,120 GRIT)
- ANODIZED ALUMINUM RECTANGULAR TUBE - COLOR TO MATCH STAINLESS STEEL PANEL - BRUSHED,120 GRIT
- ALUMINUM WINDOW (DARK GRAY)
- STAINLESS STEEL PANEL - BRUSHED,120 GRIT
- STAINLESS STEEL STOREFRONT - BRUSHED,120 GRIT
- ALUMINUM STOREFRONT (DARK GRAY)
- TILE LARGE FORMAT (DARK GRAY)
- VISION GLASS
- FRITTED GLASS
- SHADOW BOX
- VISION GLASS GUARDRAIL
- FRITTED GLASS GUARDRAIL (BROADWAY RETAIL ENTRY)
- ALUMINUM LOUVERS (DARK GRAY)
- GREEN WALL
- ALUMINUM O' HEAD GARAGE DOOR (DARK GRAY)

SECTION

SCALE: 1/16"=1'-0"

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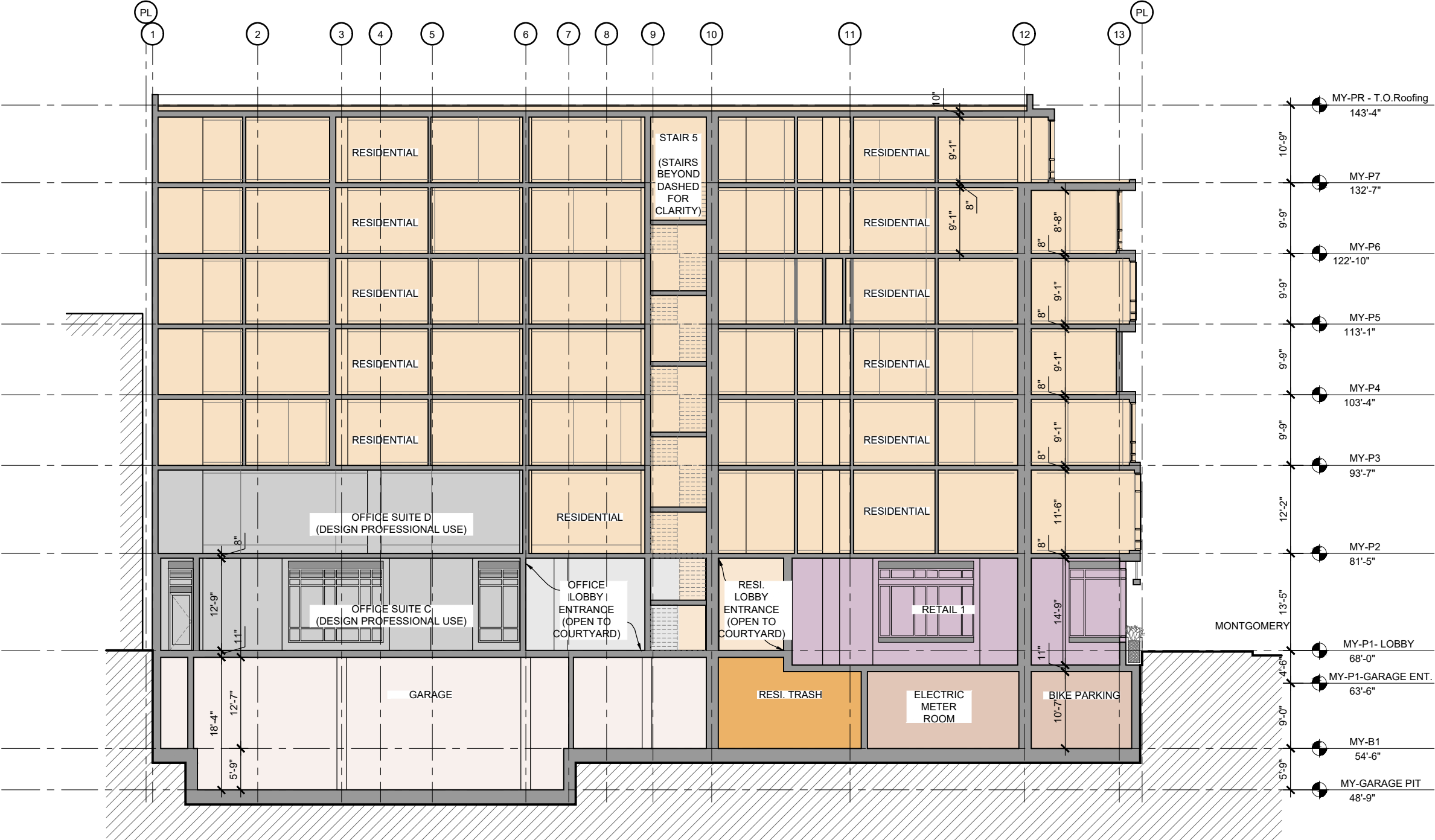
SCALE : 1/16" :1'0"

DRAWING TITLE : SECTION

SHEET NO. : A-11.0

DATE ISSUE

11/12/2021 DRAWING SET - FOR
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- KEY / LEGEND:**
- RESIDENTIAL
 - RETAIL
 - OFFICE
 - RESIDENTIAL PARKING*
 - MISC. RESIDENTIAL USES (CIRCULATION, TRASH, STORAGE, HOA STORAGE, LOBBY)
 - BOH / BIKE STORAGE *
 - * NOT COUNTED TOWARDS GSF AS PER SF PLANNING CODE

- ELEVATION KEYNOTES**
- BRICK-IT THIN BRICK TILE- MANGANESE IRONPOT
 - ECO STUCCO- TADELAKT FINISH (DARK GRAY)
 - ECO STUCCO- TADELAKT FINISH (SUPER WHITE)
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 - ALUMINUM WINDOW (DARK GRAY)
 - STAINLESS STEEL PANEL - BRUSHED,120 GRIT
 - STAINLESS STEEL STOREFRONT - BRUSHED,120 GRIT
 - ALUMINUM STOREFRONT (DARK GRAY)
 - TILE LARGE FORMAT (DARK GRAY)
 - VISION GLASS
 - FRITTED GLASS
 - SHADOW BOX
 - VISION GLASS GUARDRAIL
 - FRITTED GLASS GUARDRAIL (BROADWAY RETAIL ENTRY)
 - ALUMINUM LOUVERS (DARK GRAY)
 - GREEN WALL
 - ALUMINUM O' HEAD GARAGE DOOR (DARK GRAY)

SECTION
SCALE: 1/16"=1'-0"

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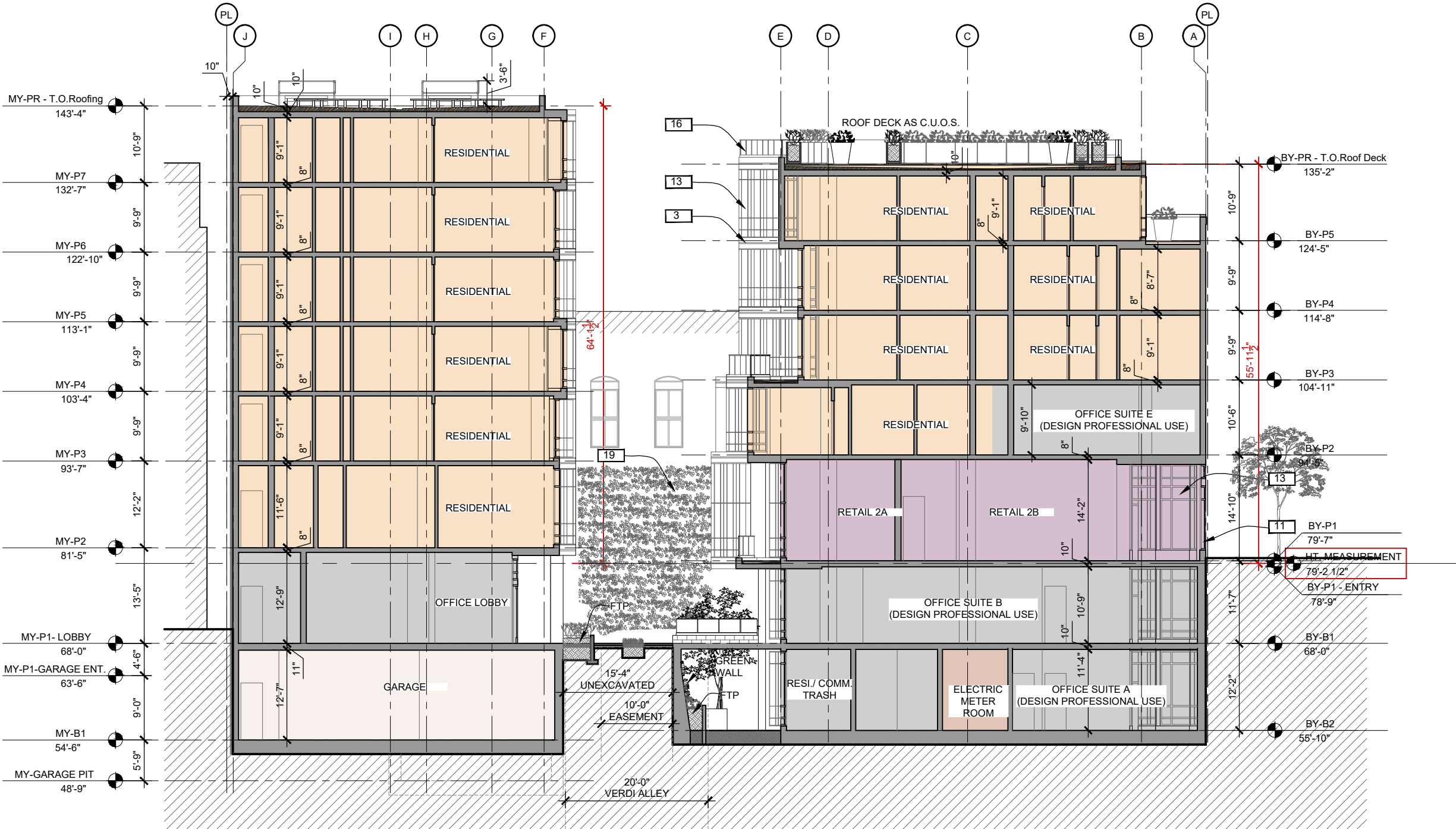
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DRAWING TITLE : SECTION

SHEET NO. : A-12.0

DATE ISSUE

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KEY / LEGEND:

- RESIDENTIAL
- RETAIL
- OFFICE
- RESIDENTIAL PARKING*
- MISC. RESIDENTIAL USES (CIRCULATION, TRASH, STORAGE, HOA STORAGE, LOBBY)
- BOH / BIKE STORAGE *
- * NOT COUNTED TOWARDS GSF AS PER SF PLANNING CODE

ELEVATION KEYNOTES

- BRICK-IT THIN BRICK TILE- MANGANESE IRONPOT
- ECO STUCCO- TADELAKT FINISH (DARK GRAY)
- ECO STUCCO- TADELAKT FINISH (SUPER WHITE)
- ALUMINUM COPING (DARK GRAY)
- FLYING BEAM (STAINLESS STEEL - BRUSHED,120 GRIT)
- CANOPY (STAINLESS STEEL - BRUSHED,120 GRIT)
- ANODIZED ALUMINUM RECTANGULAR TUBE - COLOR TO MATCH STAINLESS STEEL PANEL - BRUSHED,120 GRIT
- ALUMINUM WINDOW (DARK GRAY)
- STAINLESS STEEL PANEL - BRUSHED,120 GRIT
- STAINLESS STEEL STOREFRONT - BRUSHED,120 GRIT
- ALUMINUM STOREFRONT (DARK GRAY)
- TILE LARGE FORMAT (DARK GRAY)
- VISION GLASS
- FRITTED GLASS
- SHADOW BOX
- VISION GLASS GUARDRAIL
- FRITTED GLASS GUARDRAIL (BROADWAY RETAIL ENTRY)
- ALUMINUM LOUVERS (DARK GRAY)
- GREEN WALL
- ALUMINUM O' HEAD GARAGE DOOR (DARK GRAY)

SECTION

SCALE: 1/16"=1'-0"

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MONTGOMERY PLACE

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APN : 0163/002

SCALE : 1/16" :1'0"

DRAWING TITLE : SECTION

SHEET NO. : A-13.0

DATE ISSUE

11/12/2021 DRAWING SET - FOR
CITY SUBMISSION



ELEVATION KEYNOTES

- | | |
|---|---|
| 1. BRICK-IT THIN BRICK TILE- MANGANESE IRONPOT | 11. ALUMINUM STOREFRONT (DARK GRAY) |
| 2. ECO STUCCO- TADELAKT FINISH (DARK GRAY) | 12. TILE LARGE FORMAT (DARK GRAY) |
| 3. ECO STUCCO- TADELAKT FINISH (SUPER WHITE) | 13. VISION GLASS |
| 4. ALUMINUM COPING (DARK GRAY) | 14. FRITTED GLASS |
| 5. FLYING BEAM (STAINLESS STEEL - BRUSHED,120 GRIT) | 15. SHADOW BOX |
| 6. CANOPY (STAINLESS STEEL - BRUSHED,120 GRIT) | 16. VISION GLASS GUARDRAIL |
| 7. ANODIZED ALUMINUM RECTANGULAR TUBE - COLOR TO MATCH STAINLESS STEEL PANEL - BRUSHED,120 GRIT | 17. FRITTED GLASS GUARDRAIL (BROADWAY RETAIL ENTRY) |
| 8. ALUMINUM WINDOW (DARK GRAY) | 18. ALUMINUM LOUVERS (DARK GRAY) |
| 9. STAINLESS STEEL PANEL - BRUSHED,120 GRIT | 19. GREEN WALL |
| 10. STAINLESS STEEL STOREFRONT - BRUSHED,120 GRIT | 20. ALUMINUM O' HEAD GARAGE DOOR (DARK GRAY) |

SECTION

SCALE: 1/16"=1'-0"

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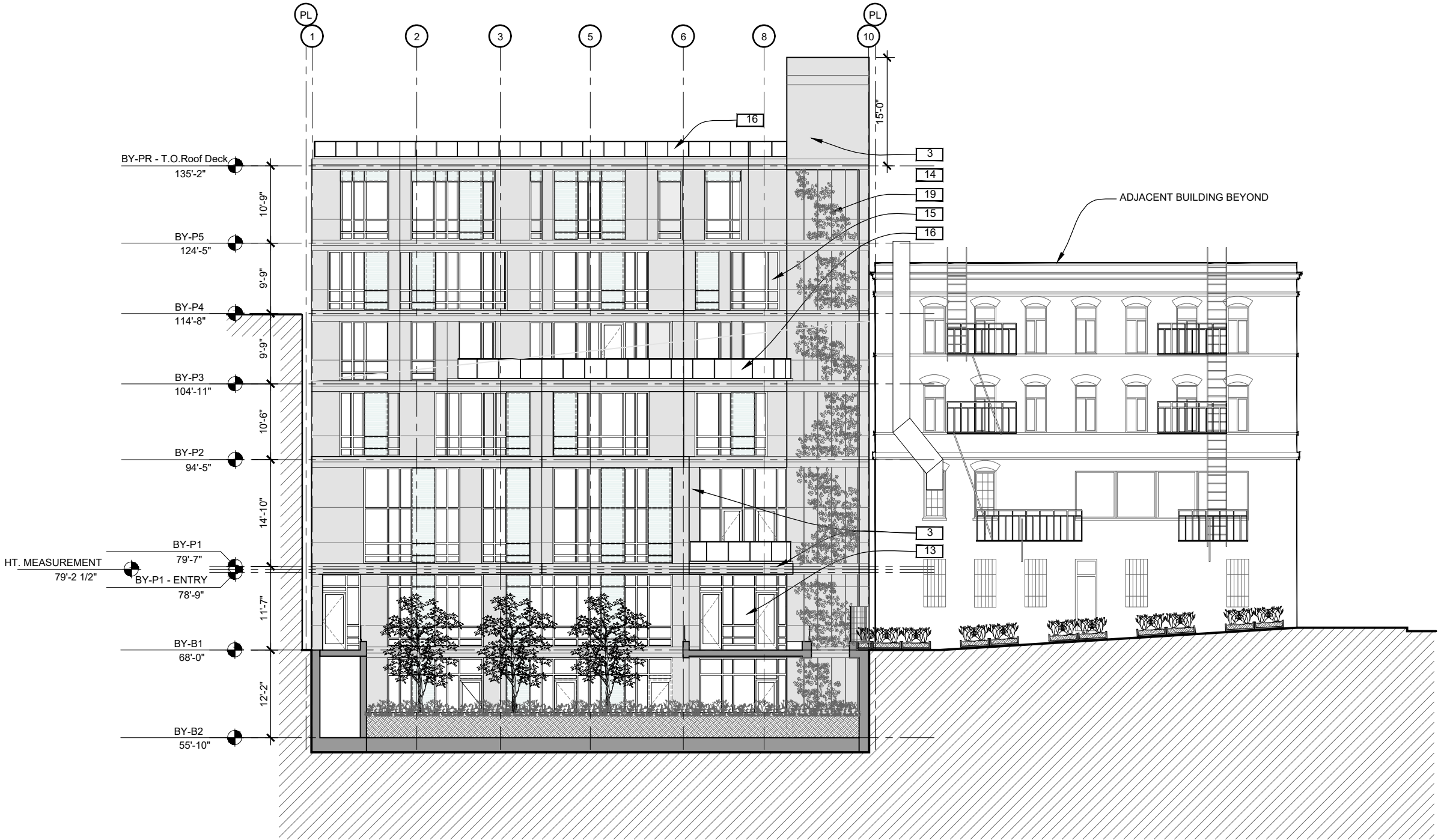
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DRAWING TITLE : SECTION

SHEET NO. : A-14.0

DATE 11/12/2021 ISSUE

DRAWING SET - FOR
CITY SUBMISSION



ELEVATION KEYNOTES

1. BRICK-IT THIN BRICK TILE- MANGANESE IRONPOT
2. ECO STUCCO- TADELAKT FINISH (DARK GRAY)
3. ECO STUCCO- TADELAKT FINISH (SUPER WHITE)
4. ALUMINUM COPING (DARK GRAY)
5. FLYING BEAM (STAINLESS STEEL - BRUSHED,120 GRIT)
6. CANOPY (STAINLESS STEEL - BRUSHED,120 GRIT)
7. ANODIZED ALUMINUM RECTANGULAR TUBE - COLOR TO MATCH STAINLESS STEEL PANEL - BRUSHED,120 GRIT
8. ALUMINUM WINDOW (DARK GRAY)
9. STAINLESS STEEL PANEL - BRUSHED,120 GRIT
10. STAINLESS STEEL STOREFRONT - BRUSHED,120 GRIT
11. ALUMINUM STOREFRONT (DARK GRAY)
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17. FRITTED GLASS GUARDRAIL (BROADWAY RETAIL ENTRY)
18. ALUMINUM LOUVERS (DARK GRAY)
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20. ALUMINUM O' HEAD GARAGE DOOR (DARK GRAY)

SECTION

SCALE: 1/16"=1'-0"

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SCALE : 1/16" :1'0"

DRAWING TITLE : SECTION

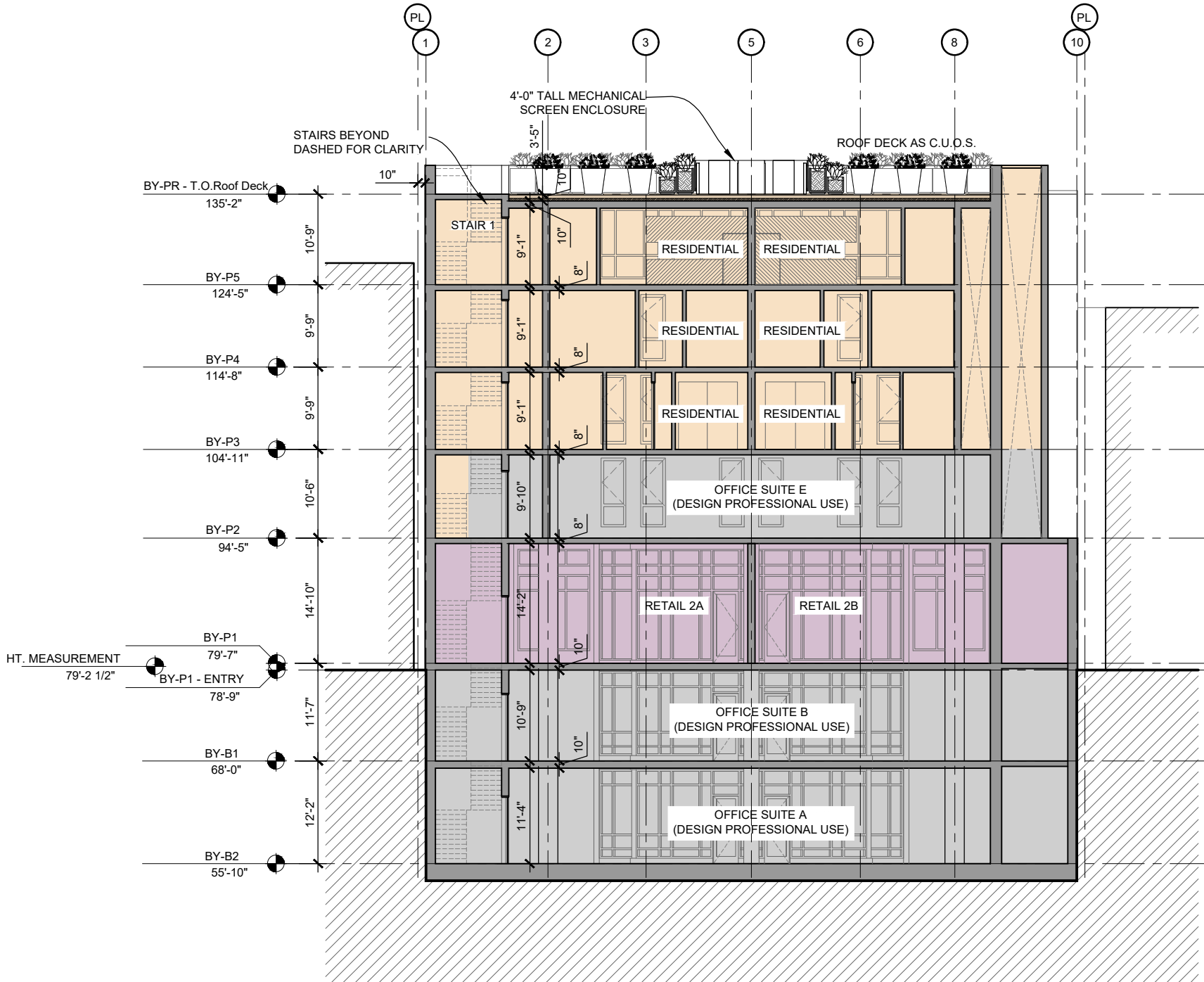
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KEY / LEGEND:

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- BOH / BIKE STORAGE *
- * NOT COUNTED TOWARDS GSF AS PER SF PLANNING CODE

ELEVATION KEYNOTES

- BRICK-IT THIN BRICK TILE- MANGANESE IRONPOT
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- GREEN WALL
- ALUMINUM O' HEAD GARAGE DOOR (DARK GRAY)

SECTION

SCALE: 1/16"=1'-0"

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PROJECT
APN : 0163/002

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DRAWING TITLE : ELEVATION

SHEET NO. : A-16.0

DATE ISSUE

11/12/2021
DRAWING SET - FOR
CITY SUBMISSION



ELEVATION KEYNOTES

- | | |
|---|---|
| 1. BRICK-IT THIN BRICK TILE- MANGANESE IRONPOT | 11. ALUMINUM STOREFRONT (DARK GRAY) |
| 2. ECO STUCCO- TADELAKT FINISH (DARK GRAY) | 12. TILE LARGE FORMAT (DARK GRAY) |
| 3. ECO STUCCO- TADELAKT FINISH (SUPER WHITE) | 13. VISION GLASS |
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| 8. ALUMINUM WINDOW (DARK GRAY) | 18. ALUMINUM LOUVERS (DARK GRAY) |
| 9. STAINLESS STEEL PANEL - BRUSHED,120 GRIT | 19. GREEN WALL |
| 10. STAINLESS STEEL STOREFRONT - BRUSHED,120 GRIT | 20. ALUMINUM O' HEAD GARAGE DOOR (DARK GRAY) |

ELEVATION

SCALE: 1/16"=1'-0"

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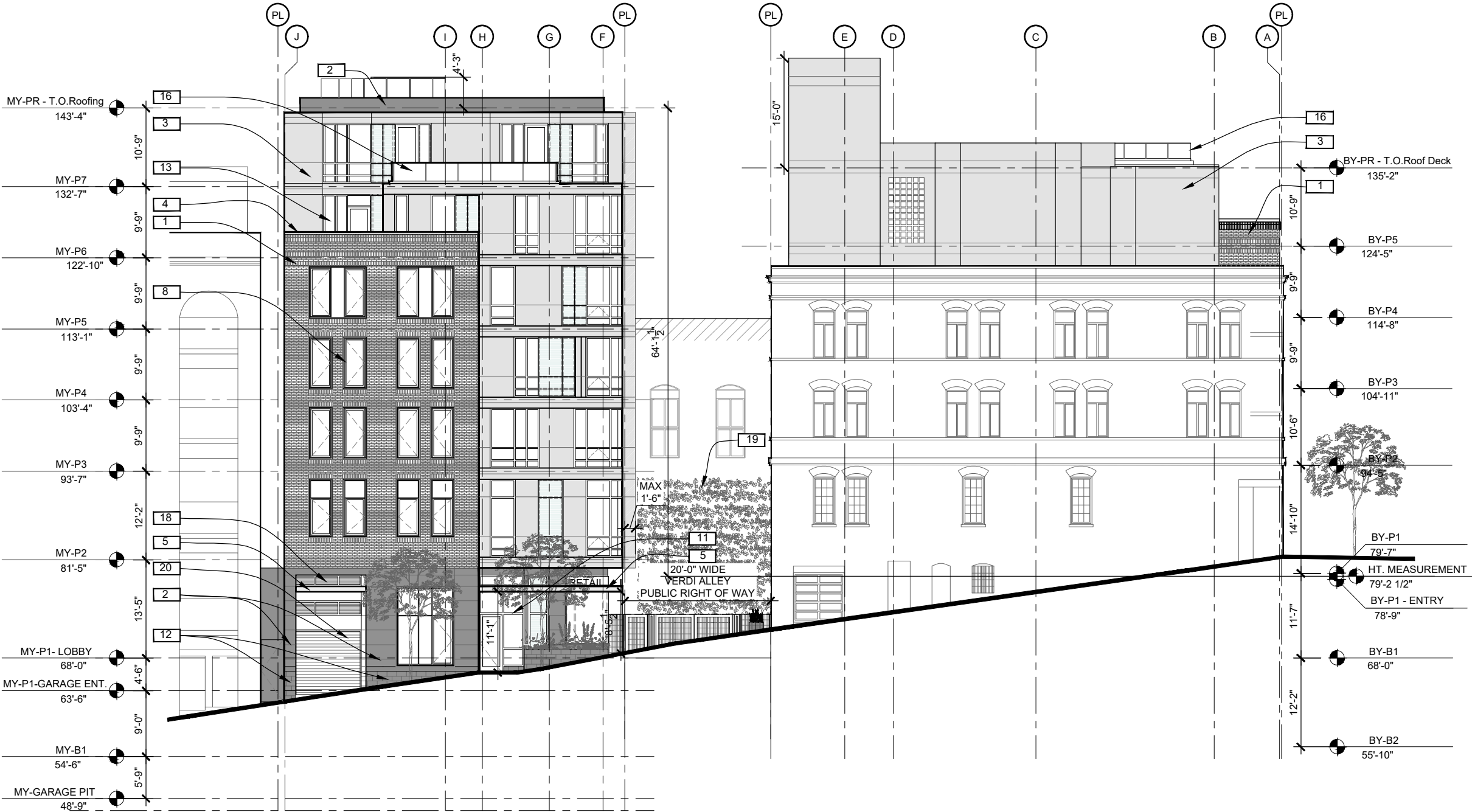
SCALE : 1/16" :1'0"

DRAWING TITLE : ELEVATION

SHEET NO. : A-17.0

DATE ISSUE

11/12/2021 DRAWING SET - FOR CITY SUBMISSION



ELEVATION KEYNOTES

1. BRICK-IT THIN BRICK TILE- MANGANESE IRONPOT

2. ECO STUCCO- TADELAKT FINISH (DARK GRAY)

3. ECO STUCCO- TADELAKT FINISH (SUPER WHITE)

4. ALUMINUM COPING (DARK GRAY)

5. FLYING BEAM (STAINLESS STEEL - BRUSHED,120 GRIT)

6. CANOPY (STAINLESS STEEL - BRUSHED,120 GRIT)

7. ANODIZED ALUMINUM RECTANGULAR TUBE - COLOR TO MATCH STAINLESS STEEL PANEL - BRUSHED,120 GRIT

8. ALUMINUM WINDOW (DARK GRAY)

9. STAINLESS STEEL PANEL - BRUSHED,120 GRIT

10. STAINLESS STEEL STOREFRONT - BRUSHED,120 GRIT
- 11.ALUMINUM STOREFRONT (DARK GRAY)

12. TILE LARGE FORMAT (DARK GRAY)

13. VISION GLASS

14. FRITTED GLASS

15. SHADOW BOX

16. VISION GLASS GUARDRAIL

17. FRITTED GLASS GUARDRAIL (BROADWAY RETAIL ENTRY)

18. ALUMINUM LOUVERS (DARK GRAY)

19. GREEN WALL

20. ALUMINUM O' HEAD GARAGE DOOR (DARK GRAY)

ELEVATION

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CA 94133.
PROJECT
APN : 0163/002

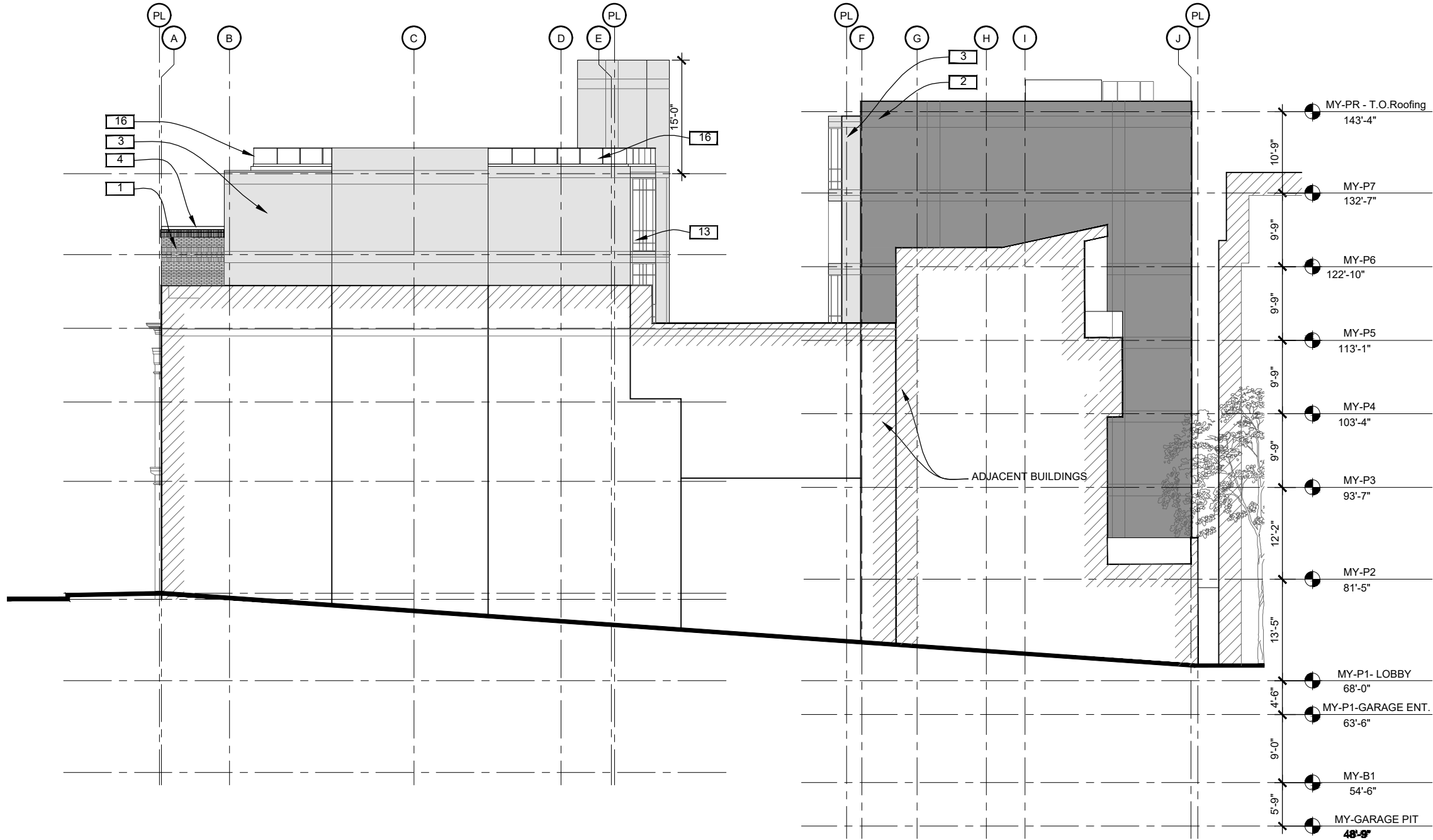
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DRAWING TITLE : ELEVATION

SHEET NO. : A-18.0

DATE ISSUE

11/12/2021 DRAWING SET - FOR
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ELEVATION KEYNOTES

- 1. BRICK-IT THIN BRICK TILE- MANGANESE IRONPOT
- 2. ECO STUCCO- TADELAKT FINISH (DARK GRAY)
- 3. ECO STUCCO- TADELAKT FINISH (SUPER WHITE)
- 4. ALUMINUM COPING (DARK GRAY)
- 5. FLYING BEAM (STAINLESS STEEL - BRUSHED,120 GRIT)
- 6. CANOPY (STAINLESS STEEL - BRUSHED,120 GRIT)
- 7. ANODIZED ALUMINUM RECTANGULAR TUBE - COLOR TO MATCH STAINLESS STEEL PANEL - BRUSHED,120 GRIT
- 8. ALUMINUM WINDOW (DARK GRAY)
- 9. STAINLESS STEEL PANEL - BRUSHED,120 GRIT
- 10. STAINLESS STEEL STOREFRONT - BRUSHED,120 GRIT
- 11. ALUMINUM STOREFRONT (DARK GRAY)
- 12. TILE LARGE FORMAT (DARK GRAY)
- 13. VISION GLASS
- 14. FRITTED GLASS
- 15. SHADOW BOX
- 16. VISION GLASS GUARDRAIL
- 17. FRITTED GLASS GUARDRAIL (BROADWAY RETAIL ENTRY)
- 18. ALUMINUM LOUVERS (DARK GRAY)
- 19. GREEN WALL
- 20. ALUMINUM O' HEAD GARAGE DOOR (DARK GRAY)

ELEVATION

SCALE: 1/16"=1'-0"



VIEW 1



VIEW 2



VIEW 3



VIEW 4

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OWNER

MONTGOMERY PLACE

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PROJECT

APN : 0163/002

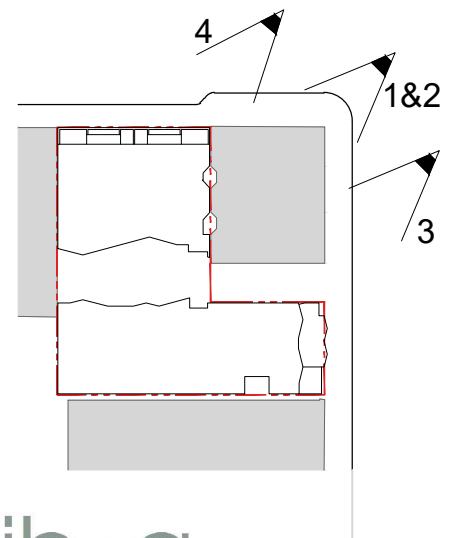
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DRAWING TITLE : MODEL VIEWS

SHEET NO. : A-19.0

DATE ISSUE

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SCALE : NTS

DRAWING TITLE : MODEL VIEWS

SHEET NO. : A-20.0

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VIEW 5



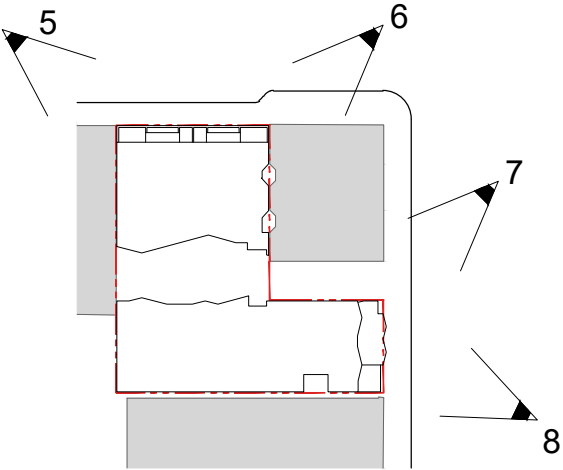
VIEW 6



VIEW 7



VIEW 8



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SCALE : 1/16" :1'0"

DRAWING TITLE : MODEL VIEWS

SHEET NO. : A-21.0

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VIEW 9



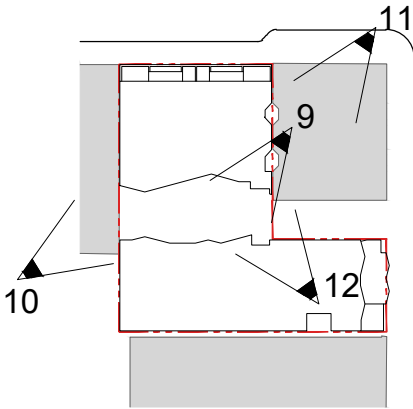
VIEW 10



VIEW 11



VIEW 12



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VIEW 13



VIEW 14



VIEW 15



VIEW 16

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MONTGOMERY PLACE

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SCALE : 1/16" : 1'0"

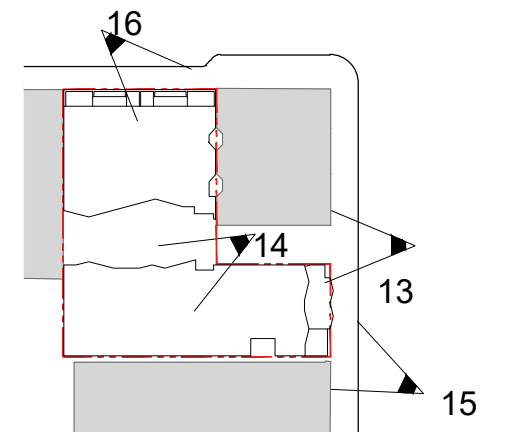
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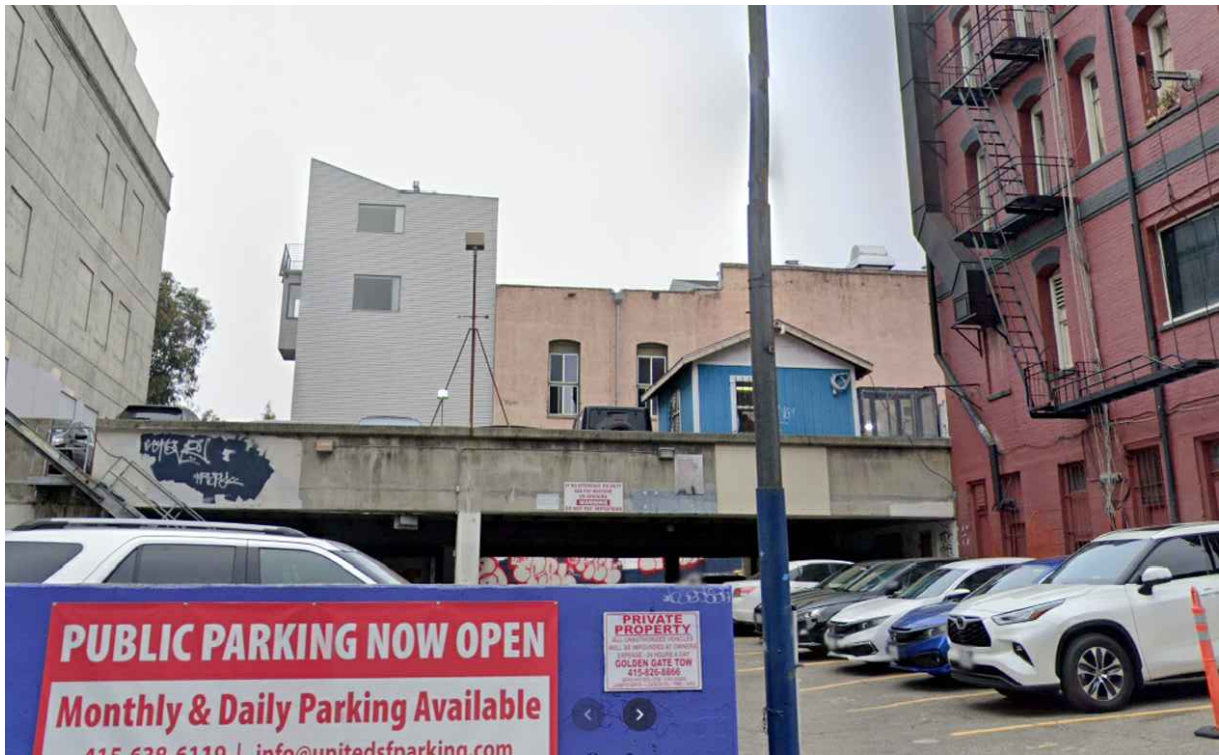
project contact: Ian Birchall



1 BROADWAY - ADJACENT PROPERTIES



2 BROADWAY - ADJACENT PROPERTIES



3 MONTGOMERY ST. - ADJACENT PROPERTIES



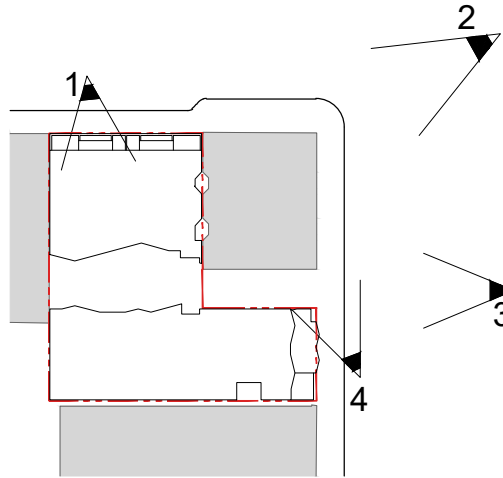
4 MONTGOMERY ST. - ADJACENT PROPERTIES

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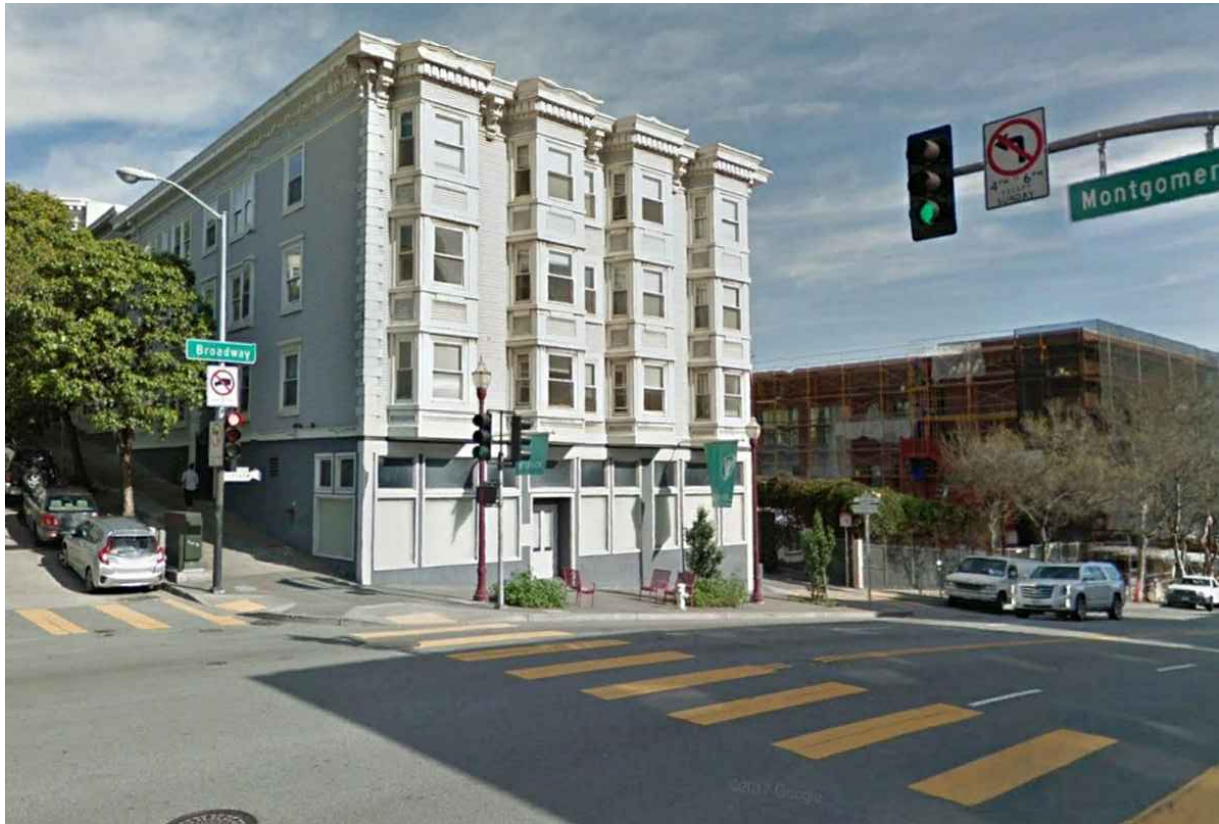
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SCALE : NTS
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SHEET NO. : A-23.0
DATE : ISSUE

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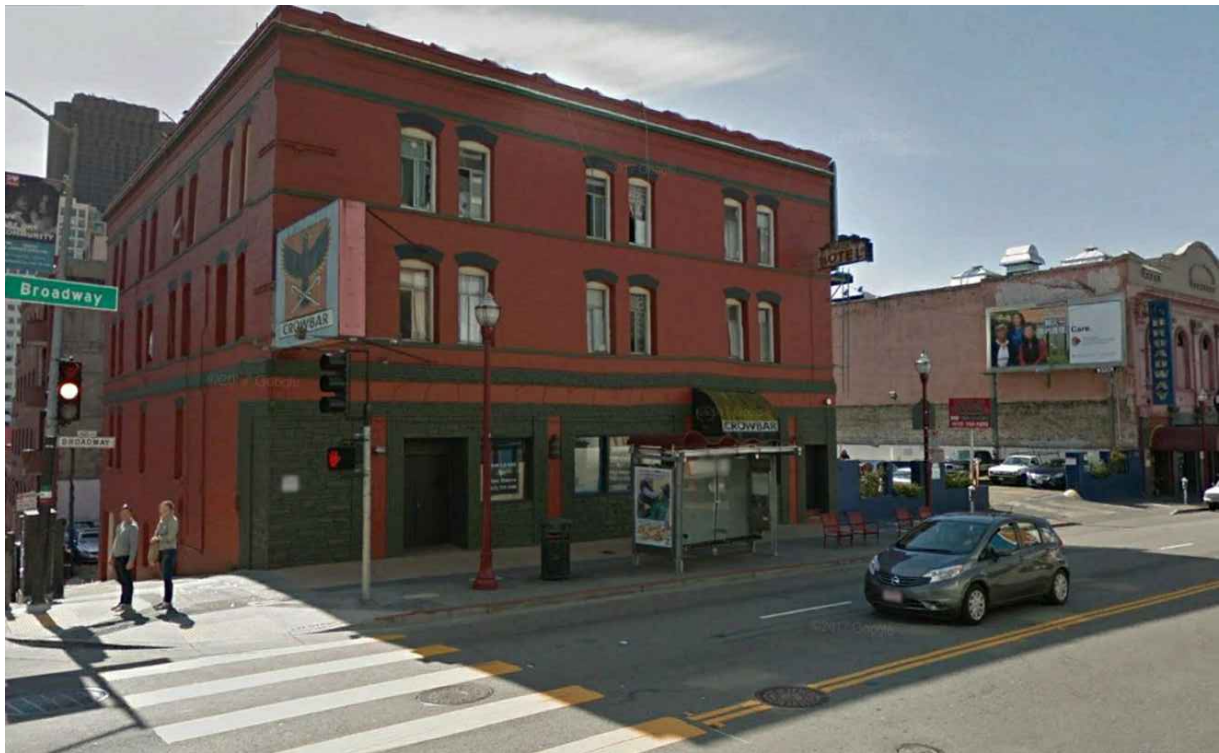
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5 BROADWAY - NORTH EAST CORNER



6 BROADWAY - NORTH WEST CORNER



7 BROADWAY - SOUTH WEST CORNER



8 BROADWAY - SOUTH EAST CORNER

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PROJECT

SCALE : NTS.

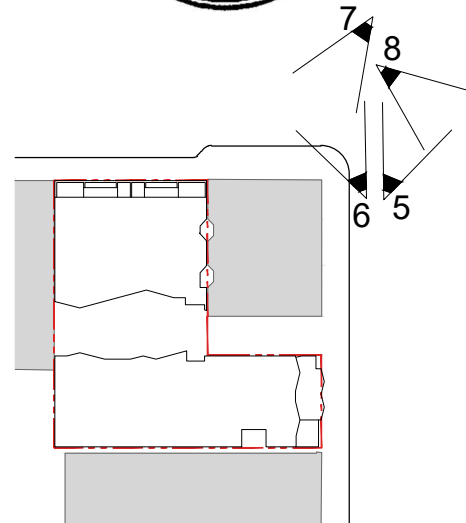
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SHEET NO. : A-24.0

DATE 11/12/2021

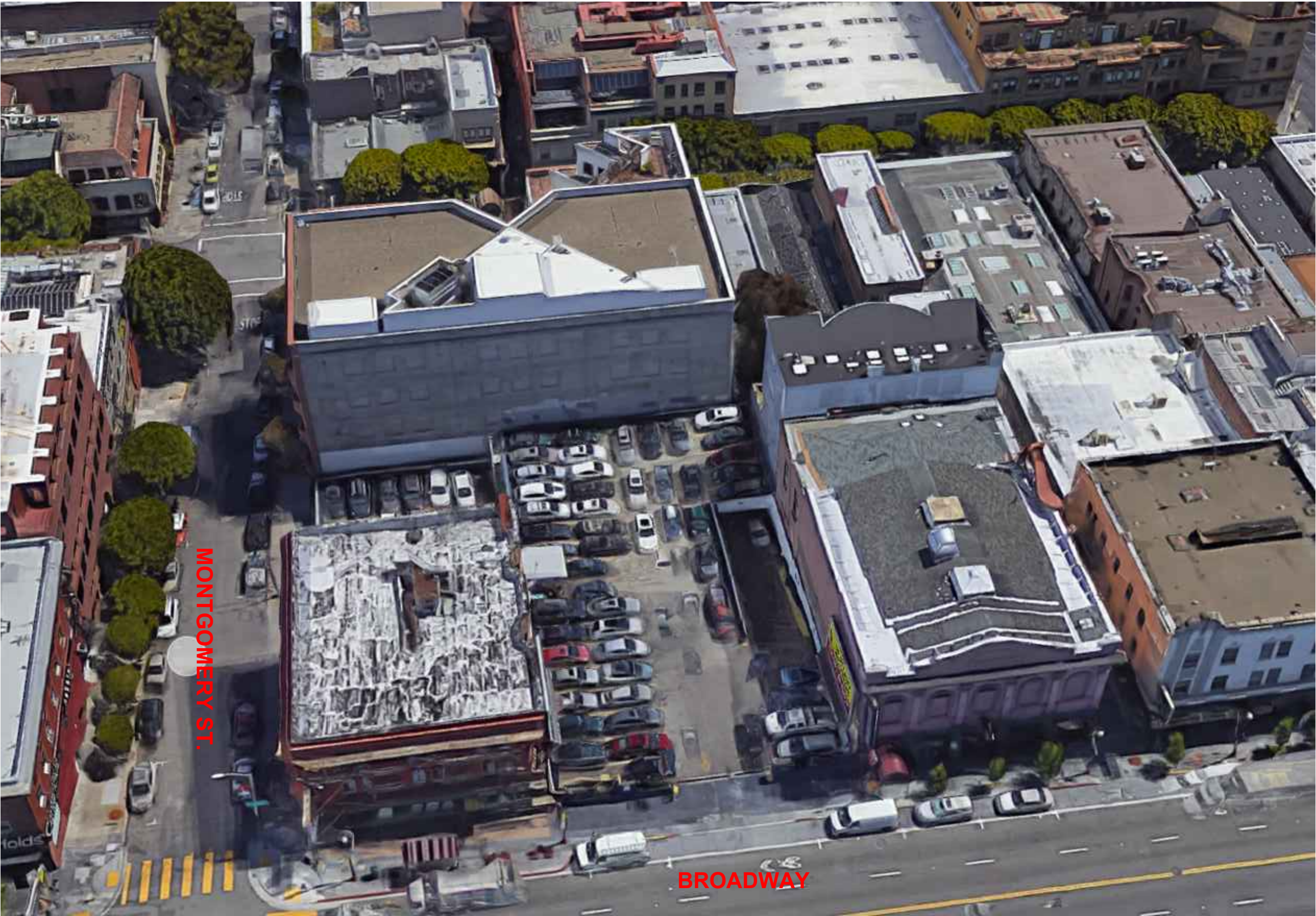
ISSUE

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BROADWAY- LOOKING AT ADJACENT BULDINGS

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APN : 0163/002

SCALE : NTS.

DRAWING TITLE : AERIAL PHOTOGRAPHS

SHEET NO. : A-25.0

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MONTGOMERY- LOOKING AT ADJACENT BULDINGS

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PROJECT
APN : 0163/002

SCALE : NTS.

DRAWING TITLE : AERIAL PHOTOGRAPHS

SHEET NO. : A-26.0

DATE ISSUE

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FLOOR	TOTAL NO. OF UNITS	APT. #	UNIT	SALEABLE AREA	SALEABLE AREA PER FLOOR LEVEL	GROSSFLOOR AREA *	TOTAL GSF	
GSF AREA CALCULATIONS AS PER SF PLANNING CODE								
MY-P ROOF	0	RESIDENTIAL CIRCULATION				0	0	
P6 MY-P7 / BY-P ROOF	5	601	2B-A	792	3,987	4,503	5,677	
		602	2B-B	939				
		603	1B-A	601				
		604	1B-G	654				
		605	2B-H	1001				
		RESIDENTIAL CIRCULATION C.O.U.S.		2,720		1,174		
P5 MY-P6 / BY-P5	9	501	2B-A	778	6,941	7,855	9,913	
		502	2B-B	932				
		503	1B-A	590				
		504	2B-I	791				
		505	3B-A	1187				
		506	2B-G	812				
		507	1B-F	684				
		508	1B-D-VAR	580				
		509	1B-E-VAR	587				
		RESIDENTIAL CIRCULATION						2,058
		P4MY-P5 / BY-P4	9	401	2B-A	778	7,518	8,487
402	2B-B			949				
403	1B-A			592				
404	2B-C			955				
405	3B-A			1233				
406	1B-B			662				
407	1B-C			796				
408	1B-D			773				
409	1B-E			780				
RESIDENTIAL CIRCULATION					2,065			
P3 MY-P4 / BY-P3	9	301	2B-A	792	7,720	8,679	10,744	
		302	2B-B	939				
		303	1B-A	601				
		304	2B-C	955				
		305	3B-A	1200				
		306	1B-B	658				
		307	1B-C	796				
		308	2B-F	884				
		309	2B-F-VAR	895				
		RESIDENTIAL CIRCULATION						2,065
P2 MY-P3 / BY-P2	7	OFFICE E (DESIGN PROF.)		1,829	1,829	1,990	11,128	
		SHARED CIRCULATION				767		
		RESIDENTIAL CIRCULATION				1,308		
		201	2B-A	801	6,291	7,063		
		202	2B-B	972				
		203	1B-A	592				
		204	2B-C	955				
		205	3B-A	1211				
		206	2B-D	893				
		207	2B-E	867				
P1 MY-P2 / BY-P1	3	101	2B-C	955	2,732	3,115	10,923	
		102	3B-A	1187				
		103	1B-A	590				
		OFFICE D (DESIGN PROF.)		1,978	1,978	2,125		
		RETAIL 2A		1,748	1,748	3,653		
		RETAIL 2B		1,591	1,591			
		SHARED CIRCULATION				2,030		
		RETAIL OUTDOOR SEATING AREA		66				
B1 MY-P1 / BY-B1	0	OFFICE: LOCKERS & SHOWERS				476	10,107	
		OFFICE B (DESIGN PROF.)		3,348	5,468	5,963		
		OFFICE C (DESIGN PROF.)		2,120				
		RETAIL 1		1,138				
		COMMERCIAL CIRCULATION (OFFICE LOBBY)				338		
		SHARED CIRCULATION				1,739		
		RESIDENTIAL CIRCULATION (LOBBY)				303		
		RESIDENTIAL PARKING RAMP TO MY-B1			288	341		
MY-B1 / BY-B2	0	OFFICE A (DESIGN PROF.)		3,363	3,363	3,785	6,256	
		COMMERCIAL / RESI TRASH ROOM - BY WING		113		137		
		COMMERCIAL TRASH ROOM - MY WING		108		127		
		COMMERCIAL CIRCULATION				183		
		SHARED CIRCULATION (INCL. COMMON LOBBY)				1,205		
		RESIDENTIAL CIRCULATION				252		
		RESI TRASH		232		282		
		HOA ATTIC STOCK		229		285		
		ELEC METER ROOMS (MY AND BY WING)		380		446		
		UTILITY		41		55		
		BIKE PARKING		473		556		
		TEL DATA		86		102		
		RESIDENTIAL PARKING		17 CAR	3,335	3,618		

NOTE: RAMP TO BASEMENT, BIKE PARKING, ELC METER ROOM, UTILITY, TELL DATA... APPROX ADDN. 4741 SF NOT INCLUDED IN GSF CALCULATIONS PER SF PLANNING CODE

TOTAL RESIDENTIAL SALEABLE AREA		35,189	(NOT INCLUDING RESI. PARKING)
TOTAL RESIDENTIAL SALEABLE PARKING AREA AND COUNT	17 CAR	3,623	
TOTAL RETAIL SALEABLE AREA		4,477	(NOT INC. OUTDOOR SEATING AREA)
TOTAL OFFICE SALEABLE AREA		12,638	(NOT INC. OUTDOOR OPEN SPACE)
TOTAL BUILDING GROSS AREA		75,300	
TOTAL GROSS AREA- SHARED CIRCULATION 50% TOWARDS COMMERCIAL, 50 % TOWARDS RESIDENTIAL		5,741	
TOTAL RESIDENTIAL GROSS AREA- 41 UNITS		39,702	52,365
TOTAL CIRCULATION GROSS AREA- RESIDENTIAL CIRCULATION 50% OF SHARED CIRCULATION		9,792	
		2,871	
TOTAL RETAIL GROSS AREA		4,940	22,935
TOTAL OFFICE GROSS AREA		14,339	
TOTAL CIRCULATION GROSS AREA- COMMERCIAL CIRCULATION 50% OF SHARED CIRCULATION		785	
		2,871	
TOTAL C.U.O.S.		2,720	

* AREAS INCLUDE CIRCULATION CORRIDORS / CORES
GSF AREA CALCULATIONS AS PER SF PLANNING CODE

UNIT	UNIT SIZES (NSF)	NO. OF UNITS	PERCENTAGE
1BD	580-796	16	38.1%
2BD	778-1001	21	50.0%
3BD	1187-1233	5	11.9%
TOTAL NUMBER OF UNITS		42	

NON RESIDENTIAL FAR CALCULATIONS					
MAX NON RESI FAR =	SITE AREA	MAX ALLOWED NON RESIDENTIAL USES =	NON RESIDENTIAL USES PROPOSED =		
2.5	13,641.25	= 2.5 X 13641.25 = 34103.125	22,935	PROJECT COMPLIES. PROPOSED PROJECT DOES NOT EXCEED THE NON-RESIDENTIAL USE FAR RESTRICTIONS.	

BIKE PARKING REQUIRED			
RESIDENTIAL	CLASS 1	1 PER DWELLING UNIT +1 PER 4 UNITS OVER 100	42
	CLASS 2	1 PER 20 DWELLING UNITS	3
RETAIL	CLASS 1 p	1 PER 7500 SQ.FT. OF OCCUPIED FLOOR AREA	1
	CLASS 2	MIN 2 (1 PER 2500SQ.FT. OF OCCUPIED AREA)	2
OFFICE	CLASS 1	1 PER 5,000 SQ.FT. OF OCCUPIED AREA	4
	CLASS 2	MIN 2 FOR OFFICE USE GREATER THAN 5,000 SF	2
			54

USABLE OPEN SPACE		
60 S.F. PRIVATE OR 80 S.F. COMMON USABLE OPEN SPACE REQUIRED PER UNIT.		
ADJACENT PROPERTY (NCD) NEAREST RESIDENTIAL DISTRICT TO 425 BROADWAY (RM-2 DISTRICT)		
MORE THAN 60 SF P.U.O.S. PROVIDED FOR 8 UNITS		
IF C.U.O.S. IS PROVIDED FOR THE REMAINING 34 UNITS		
	REQ'D C.U.O.S.	80 X 34 = 2720 SF
	PROVIDED C.U.O.S AT P6	2720 SF
	C.U.O.S PROVIDED BY PROJECT	2720 SF
PROJECT COMPLIES.		

SOLAR PANEL/ PV ARRAY AREA	
MIN 15% OF THE ROOF AREA IS REQUIRED TO BE COVERED BY PV PANELS	
ROOF AREA =	11560 SF
PV ARRAY AREA =	1953 SF
=	16.9 %
PROJECT COMPLIES.	

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PROJECT
APN : 0163/002

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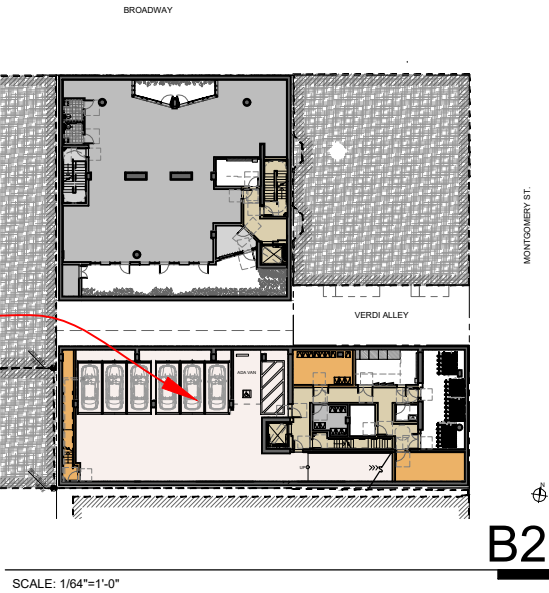
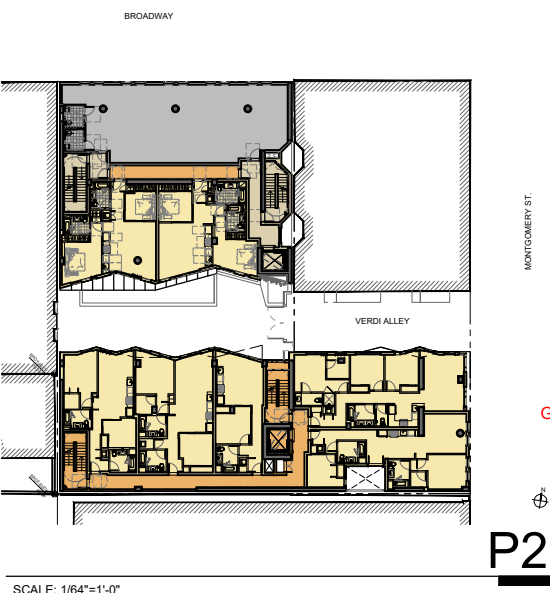
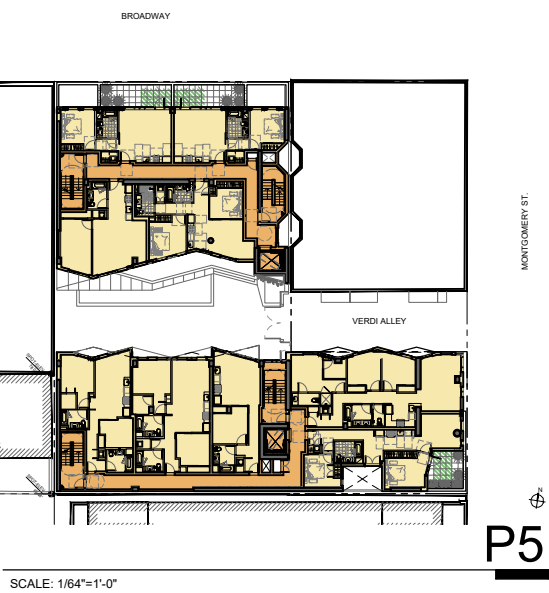
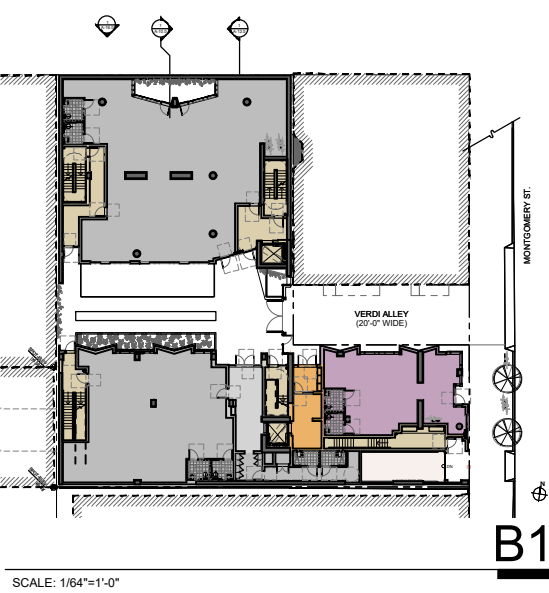
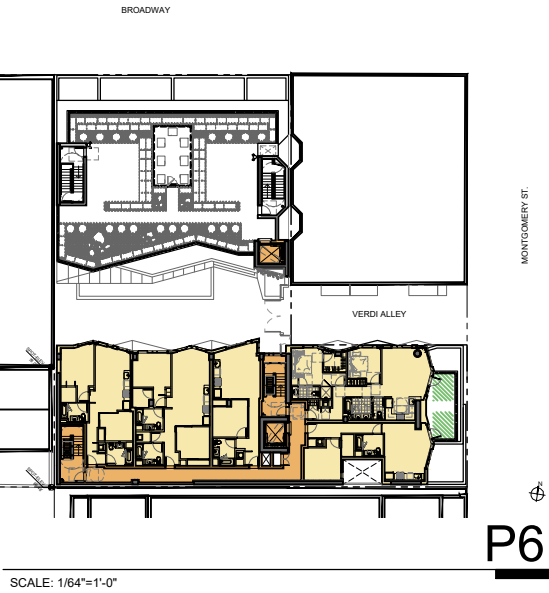
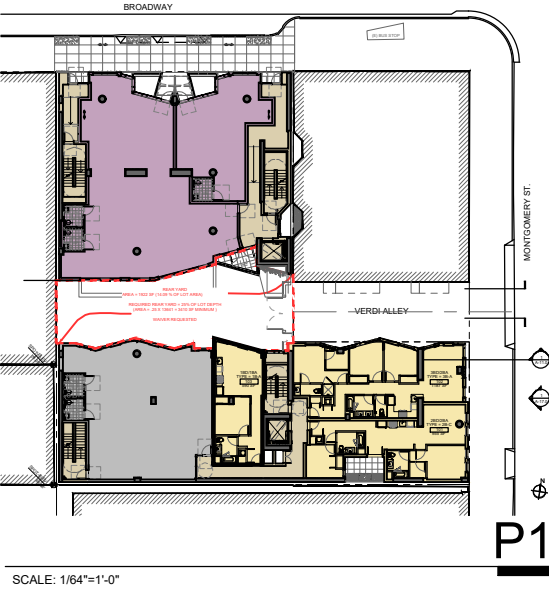
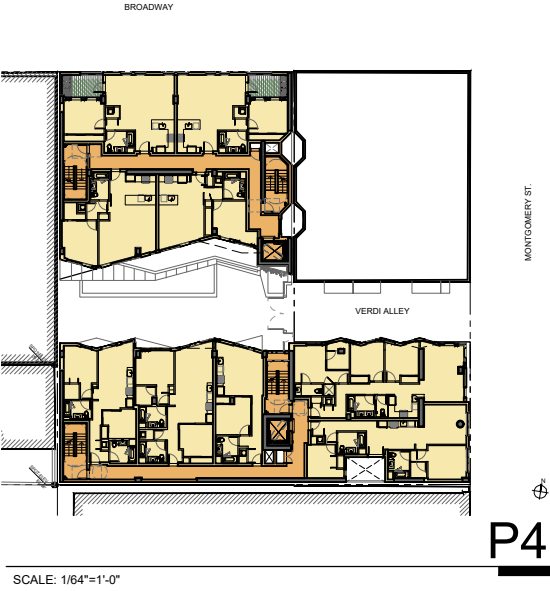
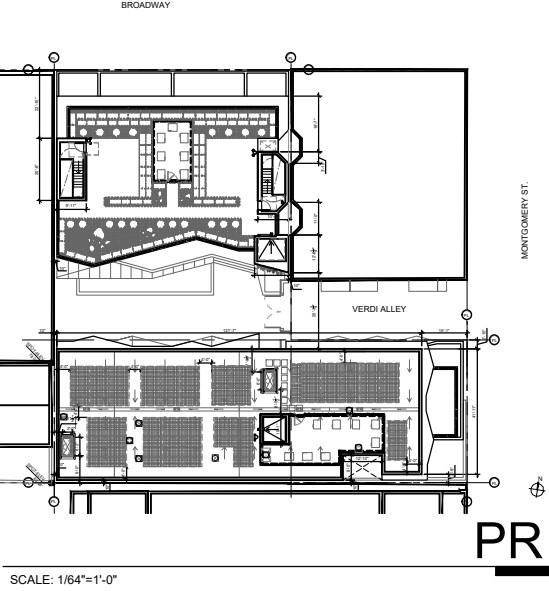
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DRAWING TITLE : F.A.R. EXHIBIT

SHEET NO. : A-28.0

DATE ISSUE

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RAMP TO BASEMENT, BIKE
PARKING, ELC METER
ROOM, UTILITY, TELL
DATA... APPROX ADDN.
5118 SF NOT INCLUDED IN
GSF CALCULATIONS PER SF
PLANNING CODE

TOTAL GSF:
TOTAL (BOTH BUILDINGS COMBINED):
75,300 GSF
RESIDENTIAL: 52,365 GSF
COMMERCIAL: 22,935 GSF
(RETAIL: 4,940 GSF, OFFICE: 17,995 GSF)

F.A.R. CALCULATION:
MAX. NON RESI. FAR= 2.5 TO 1
MAX. NON RESI. FAR = 2.5 X LOT AREA
13,641 X 2.5 = 34,102 GSF
TOTAL NON RESI GSF = 22,935 GSF
TOTAL NON RESI FAR = 1.68 < 2.5

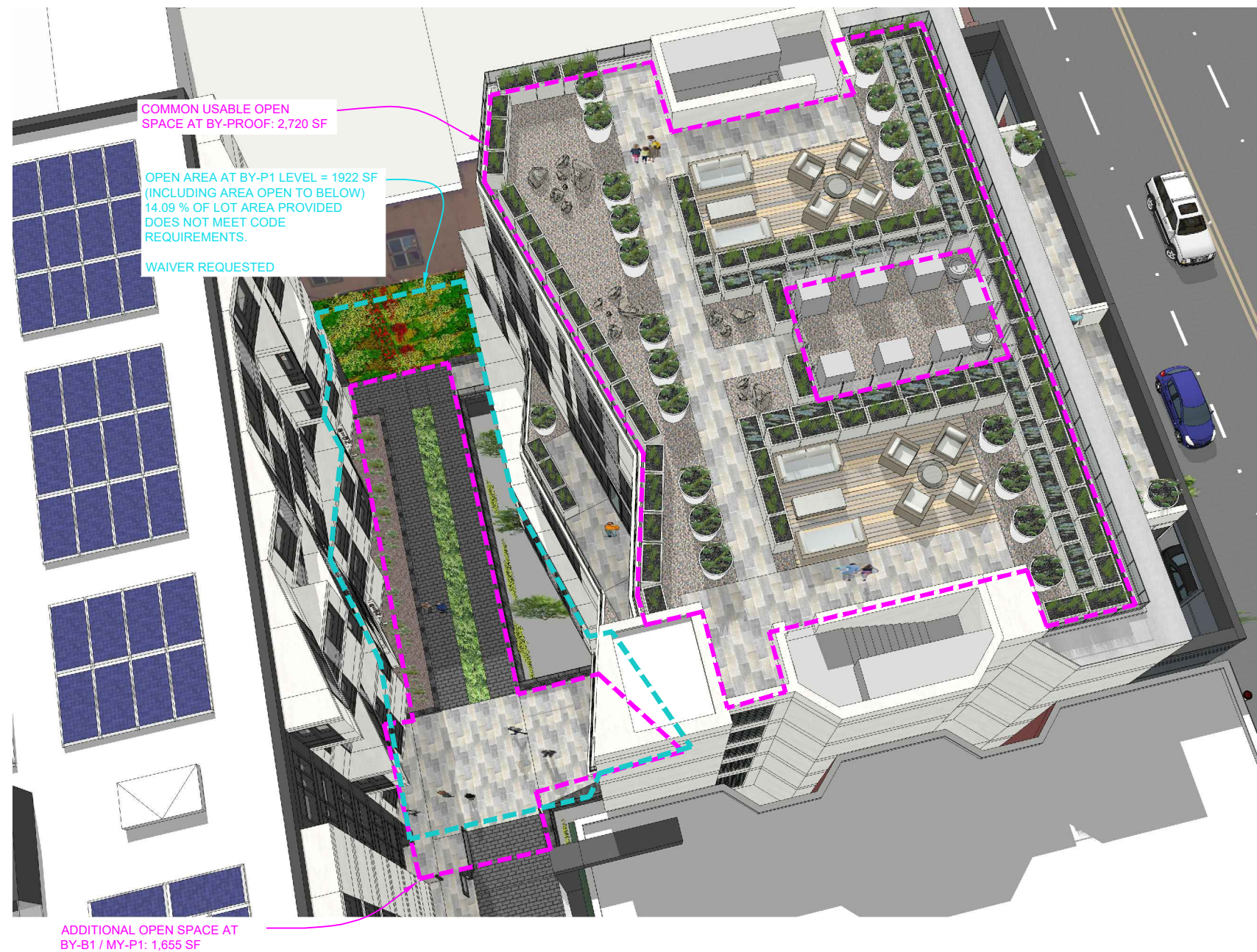
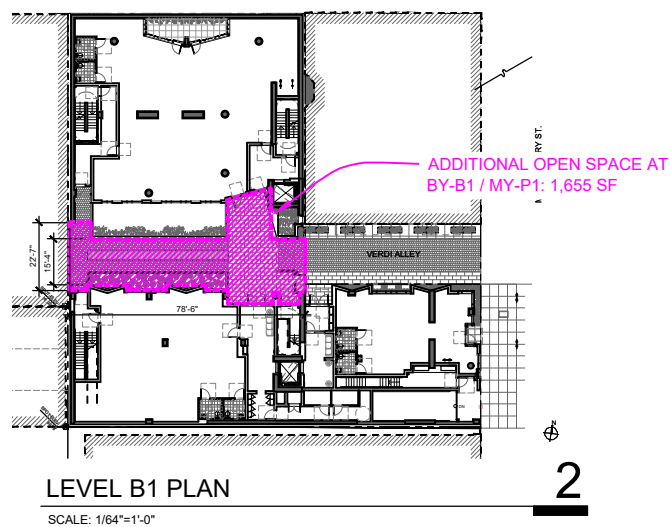
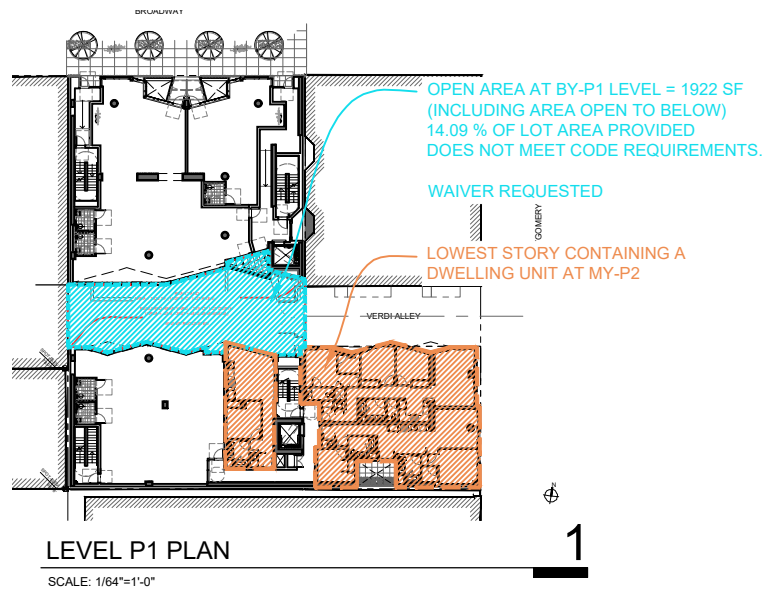
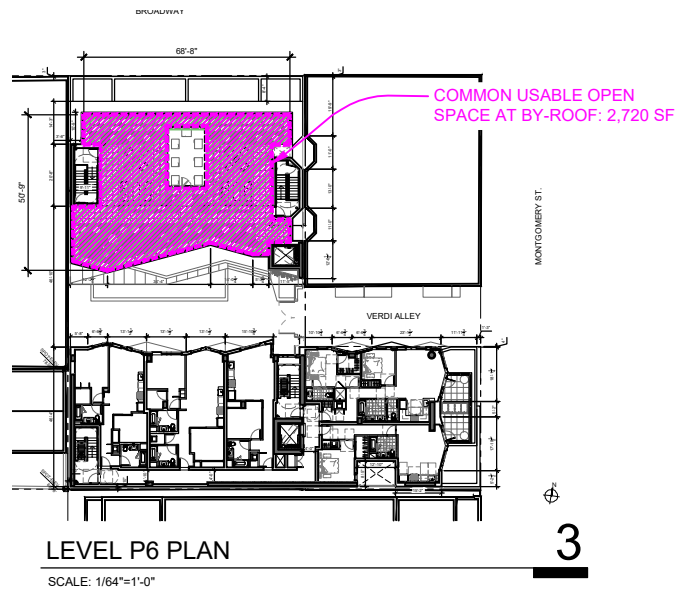
PROJECT COMPLIES.

KEY / LEGEND:

- RESIDENTIAL USE
- RETAIL USE
- OFFICE USE
- COMMERCIAL PARKING
- COMMON CIRCULATION
- OFFICE CIRCULATION
- RESIDENTIAL CIRCULATION

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REAR YARD: SF PLANNING 134 (a)(1)

THE MINIMUM REAR YARD DEPTH SHALL BE EQUAL TO 25% OF THE TOTAL DEPTH OF THE LOT ON WHICH THE BUILDING IS SITUATED, BUT IN NO CASE LESS THAN 15 FEET.

REAR YARD - MODIFICATION OF REQUIREMENTS IN NC DISTRICTS: SF PLANNING 134 (h)

THE REAR YARD REQUIREMENT IN NC DISTRICTS MAY BE MODIFIED OR WAIVED BY THE ZONING ADMINISTRATOR PURSUANT TO THE PROCEDURES WHICH ARE APPLICABLE TO VARIANCES, AS SET FORTH IN SECTIONS 306.1 THROUGH 306.5 AND 308.2, IF THE FOLLOWING CRITERIA ARE MET

- RESIDENTIAL USES ARE INCLUDED IN THE NEW OR EXPANDING DEVELOPMENT AND A COMPERABLE AMOUNT OF USABLE OPEN SPACE IS PROVIDED ELSEWHERE ON THE LOT OR WITHIN THE DEVELOPMENT WHERE IT IS MORE ACCESSIBLE TO THE RESIDENTS; AND
- THE PROPOSED NEW OR EXPANDING STRUCTURE WILL NOT SIGNIFICANTLY IMPEDE THE ACCESS OF LIGHT AND AIR TO AND VIEWS FROM ADJACENT PROPERTIES; AND
- THE PROPOSED NEW OR EXPANDING STRUCTURE WILL NOT ADVERSELY AFFECT THE INTERIOR BLOCK OPEN SPACE FORMED BY THE REAR YARDS OF ADJACENT PROPERTIES

REQ. REAR YARD PER SF PLANNING CODE:
25% OF LOT DEPTH. (AREA = .25 X 13,641 SF = 3,410 SF)

PROPOSED REAR YARD DOES NOT MEET THE REQUIREMENTS OF SF PLANNING CODE 134(a)(1)

WAIVER REQUESTED.

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APN : 0163/002

SCALE :

AS NOTED

DRAWING TITLE :

REAR YARD EXHIBIT

SHEET NO. :

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project contact: Ian Birchall

ZONING DISTRICT:

NCD - BROADWAY NEIGHBORHOOD COMMERCIAL

HEIGHT AND BULK DISTRICTS:

65-A-1

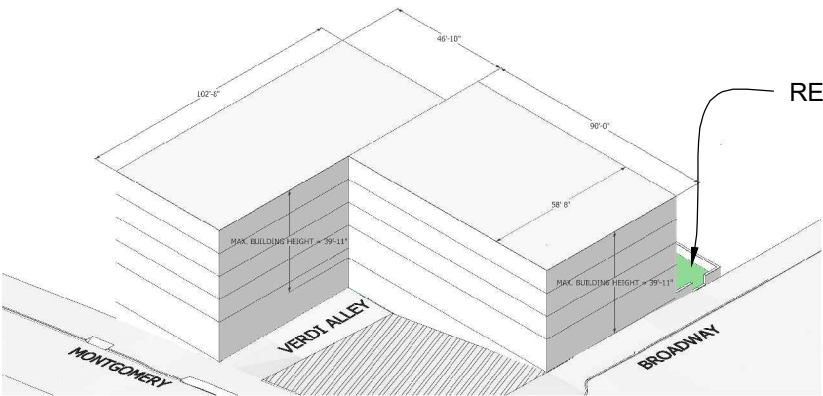
(PERMITTED UP TO 40'-0", CONDITIONAL USE REQ. 40'-0" TO 65'-0")

HEIGHT ABOVE WHICH MAX. DIMENSIONS APPLY = 40'-0"
MAX PLAN DIMENSIONS:
LENGTH = 110'-0"
DIAGONAL DIMENSION = 125'-0"

DWELLING UNIT DENSITY:

1 UNIT PER 400 SF OF LOT AREA
LOT AREA = 13641.25
13641.25 / 400 = 34.10

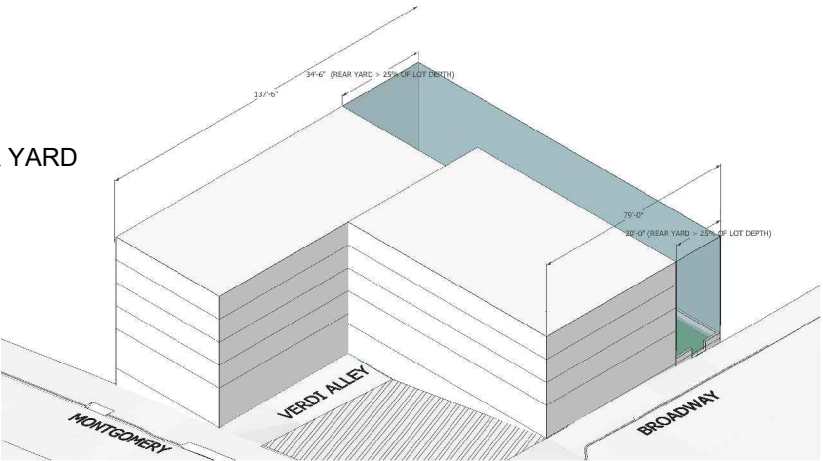
MAX 34 UNITS ALLOWED IN THE BASE PROJECT



BASELINE:

4 STORY BUILDING PROVIDING RESIDENTIAL UNITS AND RETAIL USE FRONTING THE MONTGOMERY AND BROADWAY STREETS

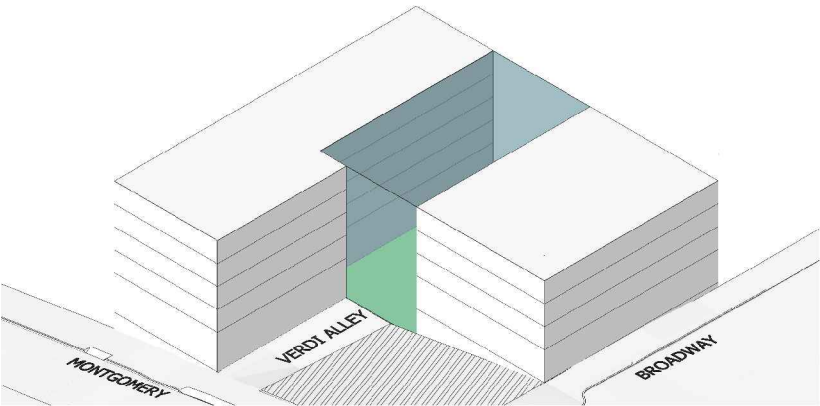
34 UNITS IN THE BASE PROJECT



STEP 1 :

SIZE OF CODE COMPLYING REAR YARD = 25% OF THE LOT DEPTH
REAR YARD LOCATED AT THE LOWEST LEVEL CONTAINING A DWELLING UNIT.

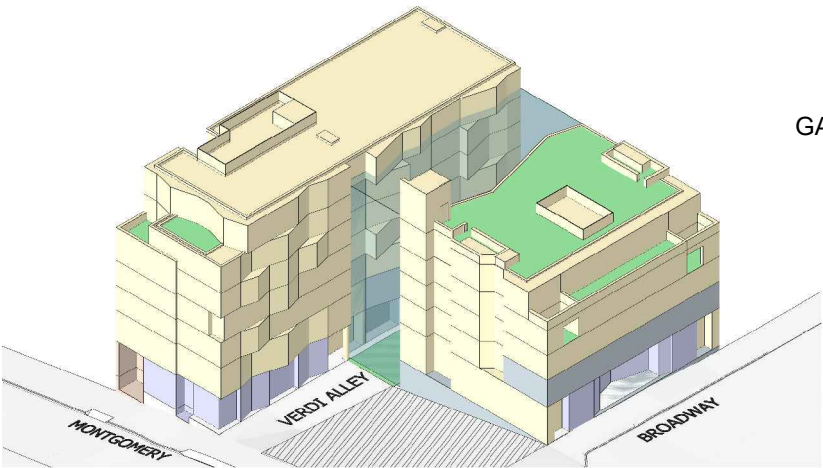
AREA PROVIDED = 3,415 SF



STEP 2 :

RELOCATE THE REAR YARD TO THE CENTER OF THE MASSING AS A COURT - AN EXTENSION OF VERDI ALLEY RESULTING IN A MASSING SPLIT.

2 BUILDINGS AS PART OF ONE DEVELOPMENT ON THE LOT.

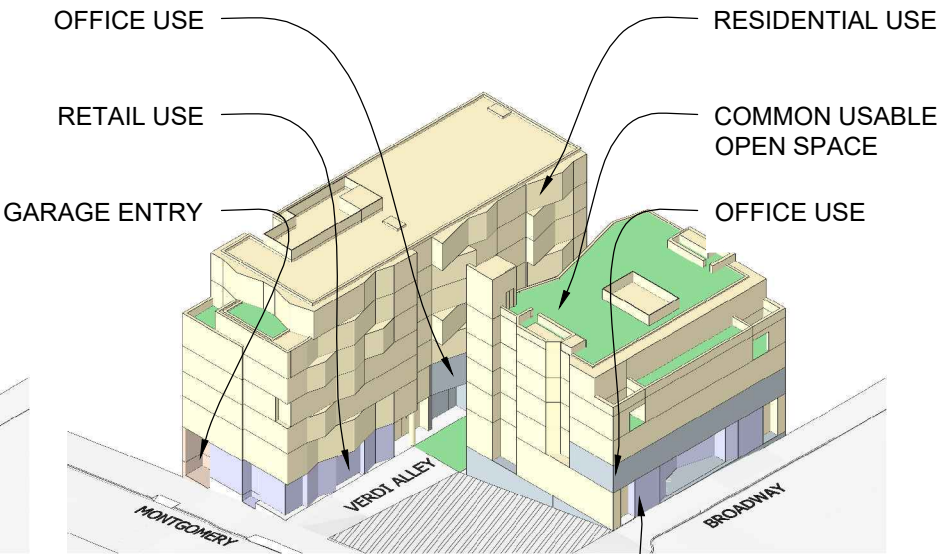


STEP 3 AND 4 :

REDUCE AND RESHAPE THE REAR YARD TO EXTEND VERDI ALLEY.

WAIVER REQUESTED FROM THE BULK, REAR YARD AND DU EXPOSURE REQUIREMENTS.
CONCESSION REQUESTED FROM THE NON-RESIDENTIAL USE SIZE LIMITS.

2 STORIES OF UNITS WITH OFFICE USE ADDED TO THE PROGRAM AT THE LOWER LEVELS.



FINAL MASSING :

5 STORY BUILDING FACING BROADWAY AND 7 STORY BUILDING FACING MONTGOMERY WITH HOUSING, RETAIL, OFFICE USE. THE MONTGOMERY BUILDING ALSO INCLUDES CAR PARKING FOR RESIDENTS IN THE BASEMENT.

NUMBER OF UNITS IN PROPOSED SCHEME = 42 RESI. UNITS (REQUESTING A 21.5% BONUS OVER THE 34 UNIT BASE SCHEME).

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San Jose, CA 95131
Richard Chen - Manager
jhrchen@gmail .com

OWNER

MONTGOMERY PLACE

425 Broadway Street
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PROJECT
APN : 0163/002

SCALE : AS NOTED
DRAWING TITLE : MASSING ILLUSTRATION
SHEET NO. : A-30.0
DATE : ISSUE

11/12/2021
DRAWING SET - FOR CITY SUBMISSION



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APN : 0163/002

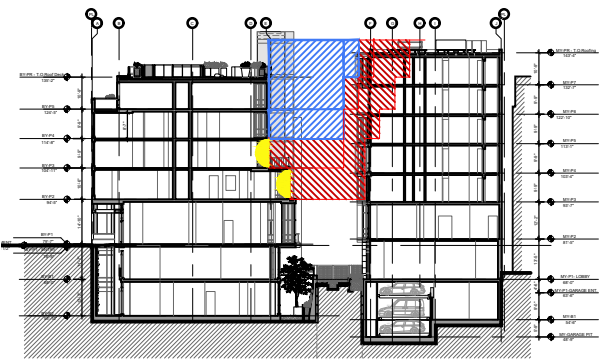
SCALE : AS NOTED

DRAWING TITLE : DU EXPOSURE EXHIBIT

SHEET NO. : A-31.0

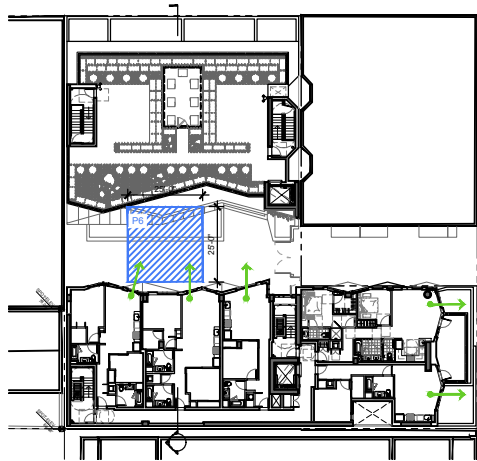
DATE ISSUE

11/12/2021 DRAWING SET - FOR
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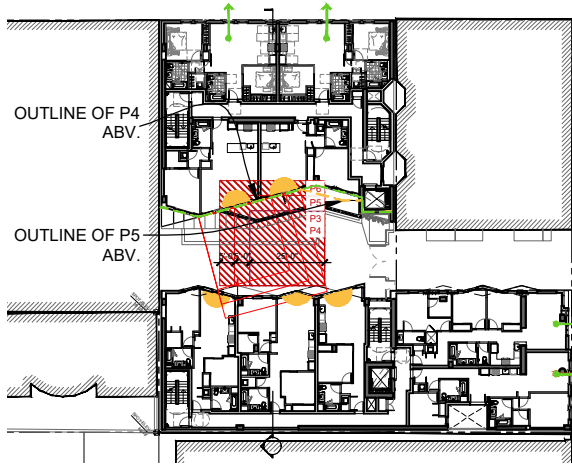
SECTION
SCALE: 1/64"=1'-0"

S2



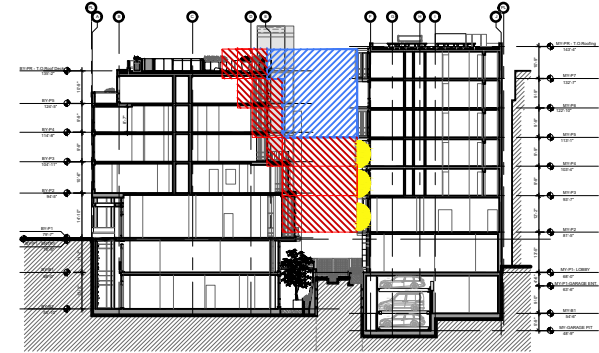
SCALE: 1/64"=1'-0"

P6



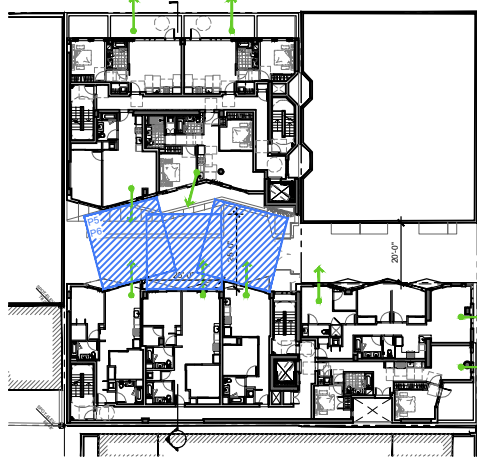
SCALE: 1/64"=1'-0"

P3



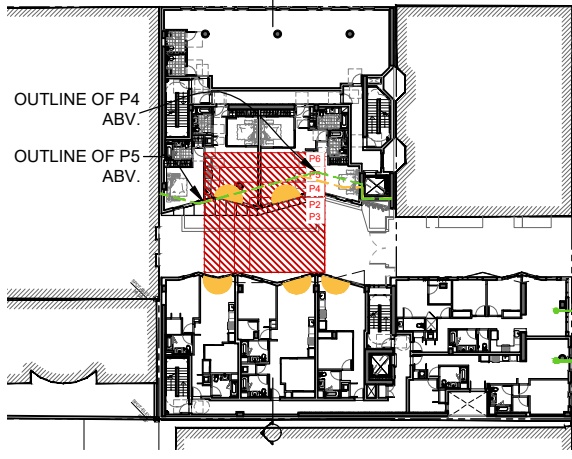
SECTION
SCALE: 1/64"=1'-0"

S1



SCALE: 1/64"=1'-0"

P5



SCALE: 1/64"=1'-0"

P2

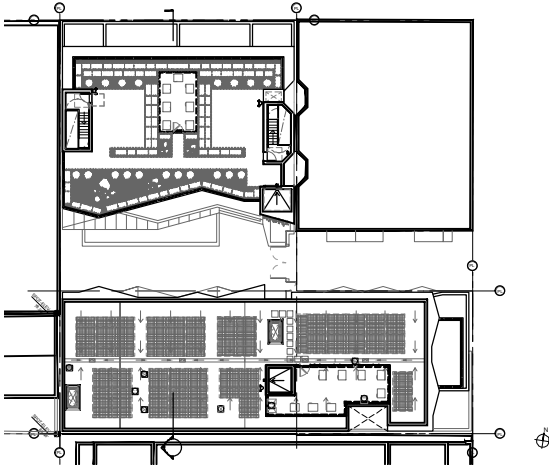
COMPLIANCE CHART			
LEVEL	TOTAL UNITS	# OF UNITS THAT MEET SEC. 140 REQ.	# OF UNITS THAT DO NOT MEET SEC. 140 REQ.
P6	5	5	0
P5	10	10	0
P4	9	9	0
P3	9	4	5
P2	7	2	5
P1	3	2	1
B1	0	0	0
B2	0	0	0
TOTAL	42	31	11

NON- COMPLIANT UNITS:

- P1: UNIT 103
P2: UNIT 201
UNIT 202
UNIT 203
UNIT 206
UNIT 207
P3: UNIT 301
UNIT 302
UNIT 303
UNIT 306
UNIT 307

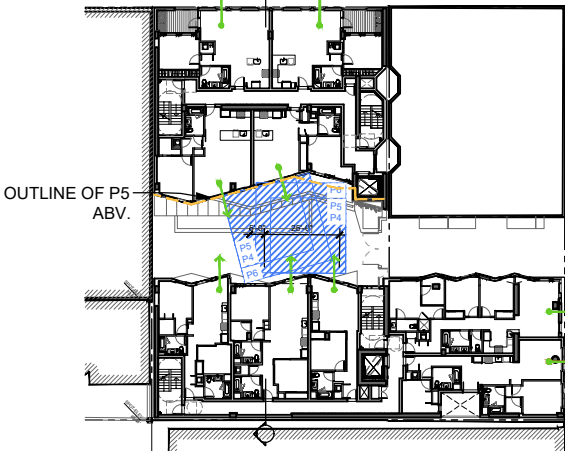
LEGEND

- COMPLIANT TO UNIT EXPOSURE AREA PER SEC. 140.A.2
NON-COMPLIANT TO UNIT EXPOSURE AREA PER SEC. 140.A.2
NON-COMPLIANT UNIT
OUTLINE OF P5 ABV.
OUTLINE OF P4 ABV.



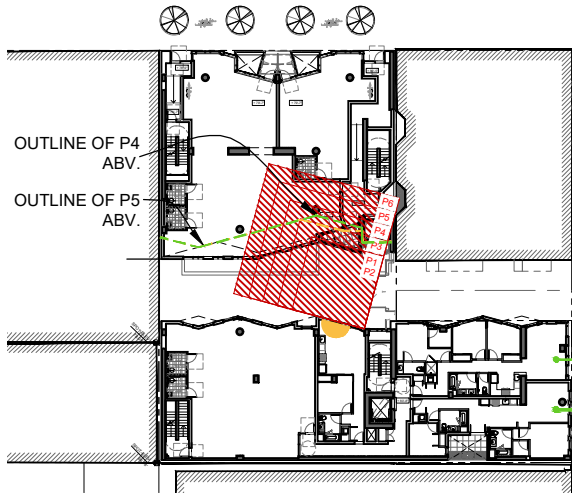
SCALE: 1/64"=1'-0"

PR



SCALE: 1/64"=1'-0"

P4



SCALE: 1/64"=1'-0"

P1

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PER SEC. 260. HEIGHT LIMITS: MEASUREMENT.

(A) METHOD OF MEASUREMENT. The limits upon the height of buildings and structures shall be as specified on the zoning map, except as permitted by section 206. in the measurement of height, the following rules shall be applicable:

(1) The point above which such measurements shall be taken shall be as specified as follows.

(A) In the case of either (b) or (c) below, such point shall be taken at the centerline of the building or, where the building steps laterally in relation to a street that is the basis for height measurement, separate points shall be taken at the centerline of each building step.

(B) Where the lot is level with or slopes downward from a **street at the centerline of the building or building step, such point shall be taken at curb level on such a street. This point shall be used for height measurement only for a lot depth not extending beyond a line 100 feet from and parallel to such street, or beyond a line equidistant between such street and the street on the opposite side of the block, whichever depth is greater.** Measurement of height for any portion of the lot extending beyond such line shall be considered in relation to the opposite (lower) end of the lot, and that portion shall be considered an upward sloping lot in accordance with subsection (c) below, whether or not the lot also has frontage on a lower street.

(C) Where the lot slopes upward from a street at the centerline of the building or building step, such point shall be taken at curb level for purposes of measuring the height of the closest part of the building within 10 feet of the property line of such street; at every other cross-section of the building, at right angles to the centerline of the building or building step, such point shall be taken as the average of the ground elevations at either side of the building or building step at that cross-section. The ground elevations used shall be either existing elevations or the elevations resulting from new grading operations encompassing an entire block. Elevations beneath the building shall be taken by projecting a straight line between ground elevations at the exterior walls at either side of the entire building in the same plane.

(D) **Where the lot has frontage on two or more streets, the owner may choose the street or streets from which the measurement of height is to be taken, within the scope of the rules stated above.**



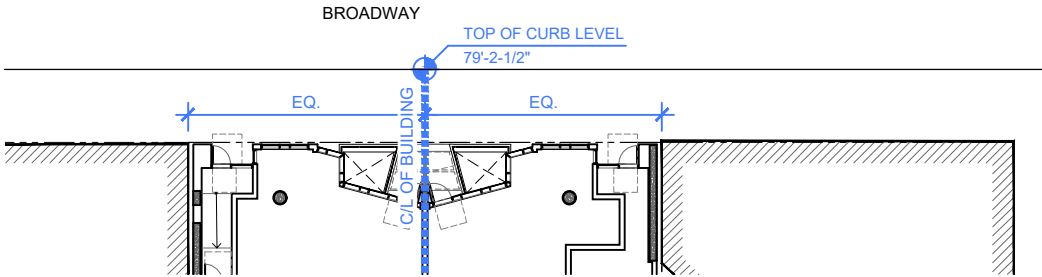
SITE AERIAL PHOTO

NTS

6

LINE EQUIDISTANT BETWEEN BROADWAY AND STREET ON THE OPPOSITE SIDE OF THE BLOCK

HT. MEASUREMENT OF BOTH BUILDINGS FROM BROADWAY



BUILDING LENGTH AT BROADWAY

SCALE: 1/32"=1'-0"

5

PER PLANNING SEC. 260. (a)(1)(B) HEIGHT MEASUREMENT

THE ENTIRE SITE IS WITHIN 1/2 BLOCK BETWEEN BROADWAY AND PACIFIC.

THIS PROJECT IS DESIGNED TO A MAX BUILDING HEIGHT OF 64'-2".

PROPOSED BUILDING HEIGHT WILL NOT EXCEED 65'-0"

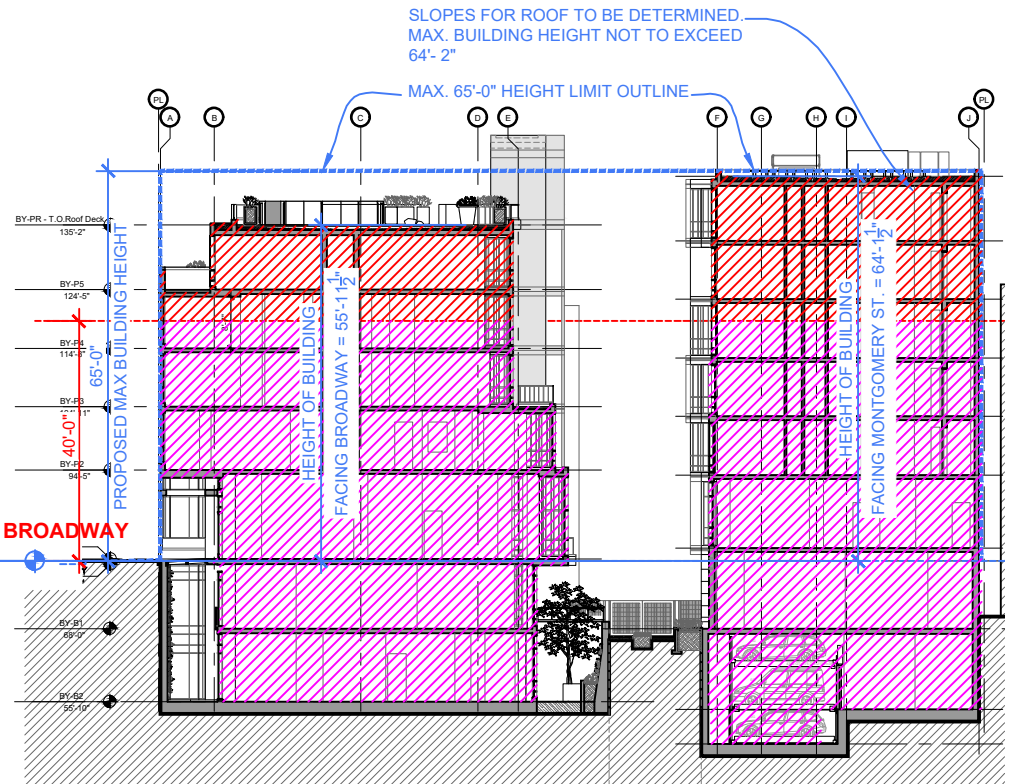
HEIGHT DIAGRAM KEY / LEGEND:

BUILDING MASS

PROPOSED MAX BUILDING HEIGHT HT. MEASUREMENT - BY 79'-2-1/2" +/-

PROPOSED BUILDING HEIGHT

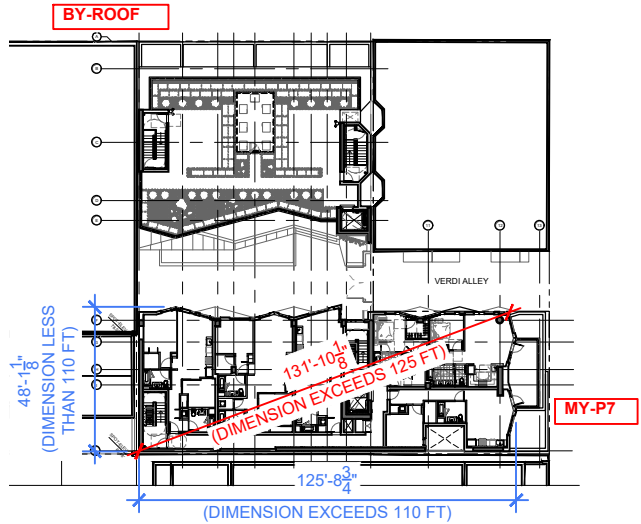
HEIGHT ABOVE WHICH BULK REQUIREMENTS APPLY



BUILDING HEIGHT

SCALE: 1/32"=1'-0"

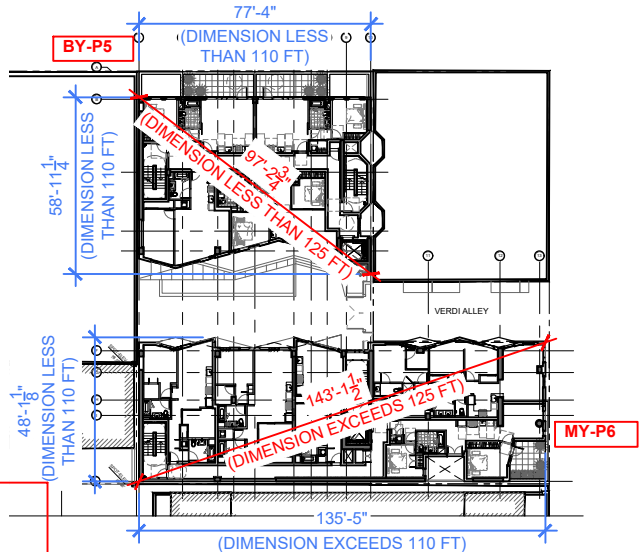
4



BULK EXCEPTION REQ. AT LEVEL MY-P7

SCALE: 1/64"=1'-0"

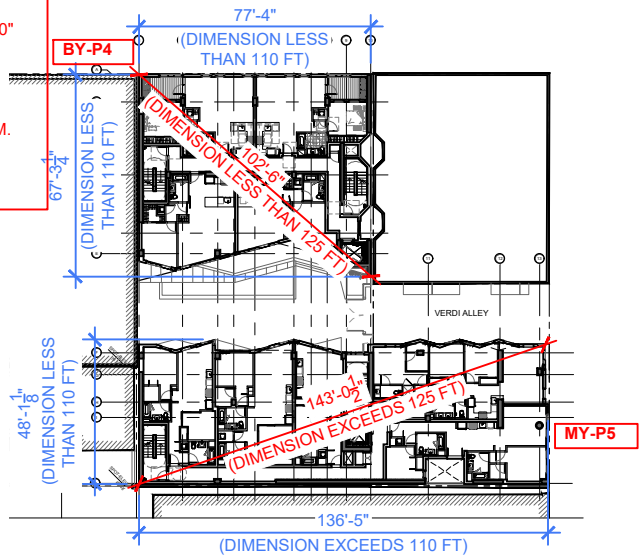
3



BULK EXCEPTION REQ. AT LEVEL MY-P6

SCALE: 1/64"=1'-0"

2



BULK EXCEPTION REQ. AT LEVEL MY-P5

SCALE: 1/64"=1'-0"

1

SITE IS LOCATED IN A BROADWAY NCD ZONING DISTRICT WITH A 65-A-1 HEIGHT + BULK DISTRICT

PER PLANNING SEC. 270. (TABLE 270)

FOR BUILDING HEIGHT ABOVE 40 FT
MAX. LENGTH = 110'-0"
MAX DIAGONAL DIMENSION = 125'-0"

LEVELS BY P4/ P5/ ROOF COMPLY

LEVELS MY P5/ P6/ P7 DO NOT COMPLY, SEE ATTACHED DIAGRAM.

WAIVER REQUESTED FOR BULK REQUIREMENT.

BULK DIAGRAM KEY / LEGEND:

DIAGONAL DIMENSION

BUILDING LENGTH

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OWNER

MONTGOMERY PLACE

425 Broadway Street
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PROJECT

APN : 0163/002

SCALE :

AS NOTED

DRAWING TITLE :

HEIGHT+BULK DIAGRAM

SHEET NO. :

A-32.0

DATE

ISSUE

11/12/2021

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ESTIMATED EXCAVATION FOR PROJECT (APPROX):

SECTION 1: (VOLUME BELOW THE + 68'-0" DATUM
[(5830+6382)SF x 14'-2"] + [1358SF x 5'-9"] = [12212 X 14.166] + [1358 X 5.75] = 180,803 CUBIC FT

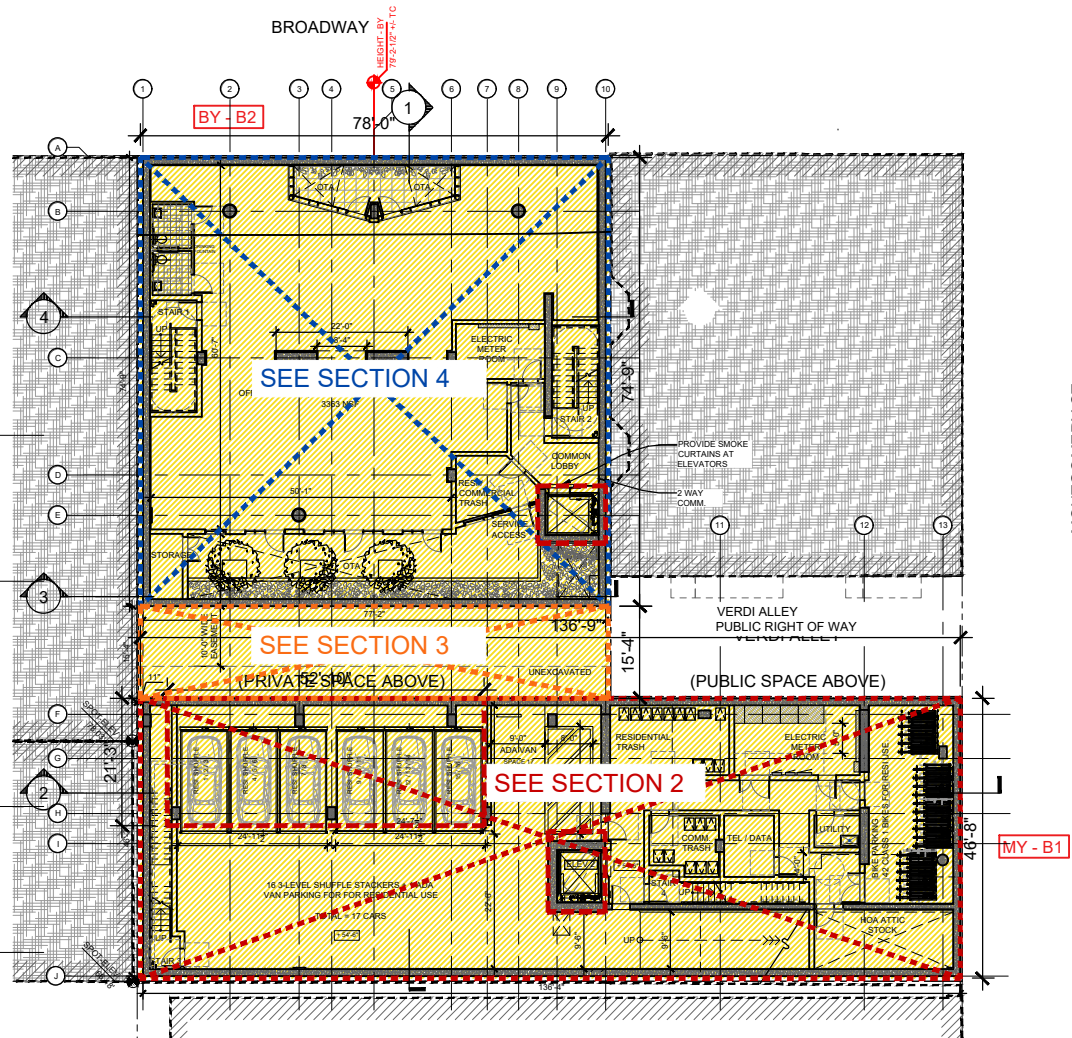
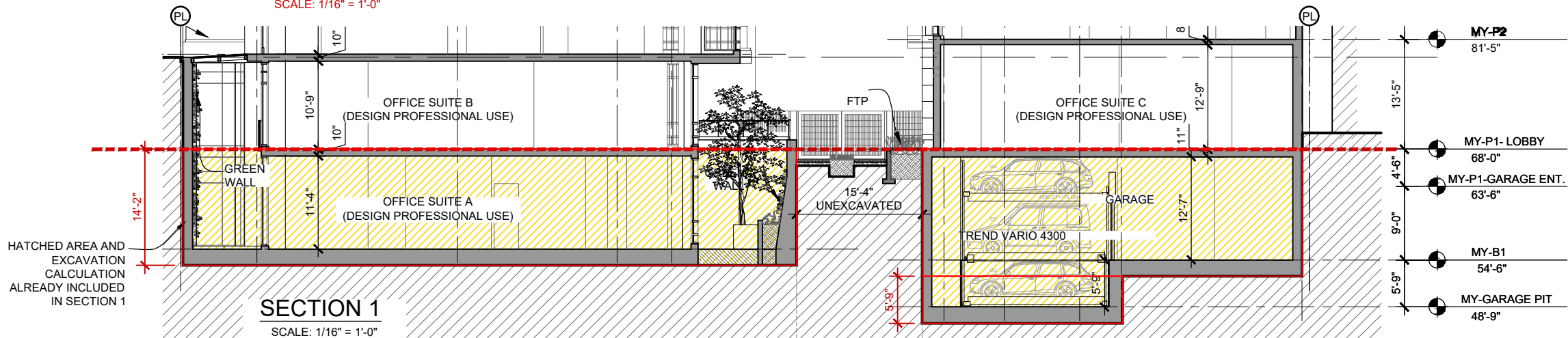
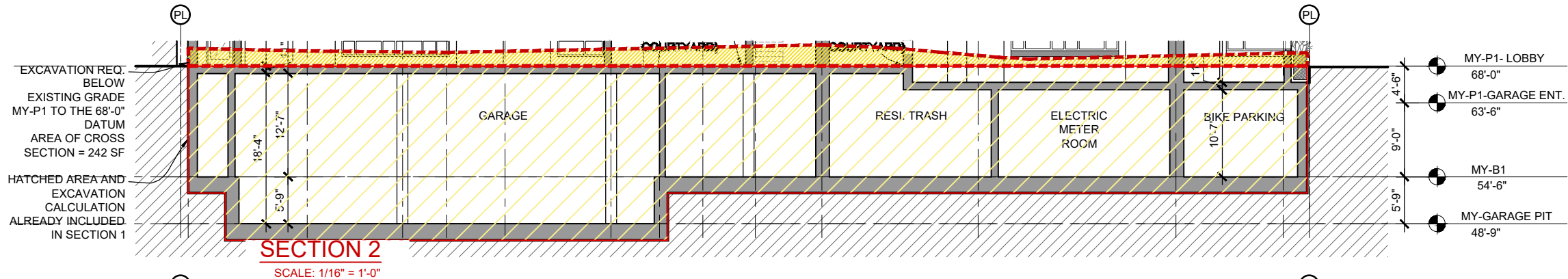
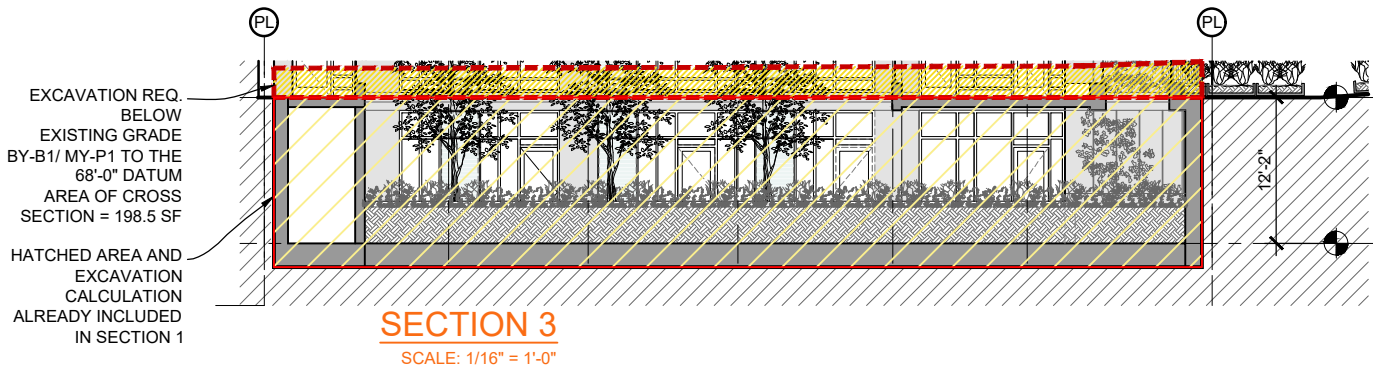
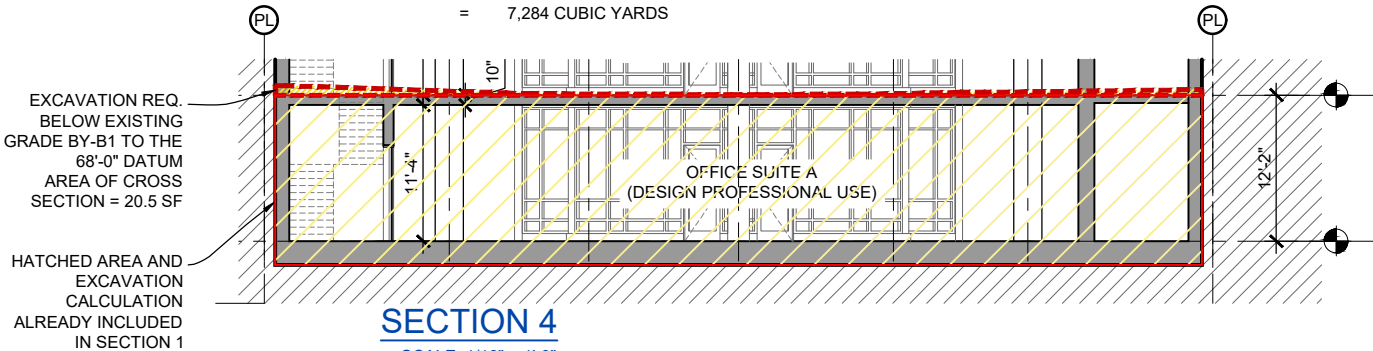
SECTION 2:
242 SF x 46'-8" = 11,294 CUBIC FT

SECTION 3:
198.5 SF x 15'-4" = 3,043 CUBIC FT

SECTION 4:
20.5 SF x 74'-9" = 1,532 CUBIC FT

TOTAL = 180,803 + 11,294 + 3,043 + 1,532 = 196,672 CUBIC FT

IN CUBIC YARDS = VALUE / 27
= 7,284 CUBIC YARDS



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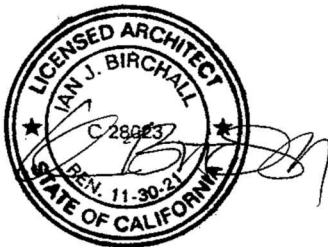
SCALE : SCALE AS NOTED

DRAWING TITLE : ESTIMATED EXCAVATION

SHEET NO. : A-33.0

DATE : 11/12/2021

ISSUE : DRAWING SET - FOR CITY SUBMISSION



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APN : 0163/002

SCALE

DRAWING TITLE :

SHEET NO. :

DATE _____

SCALE AS NOTED

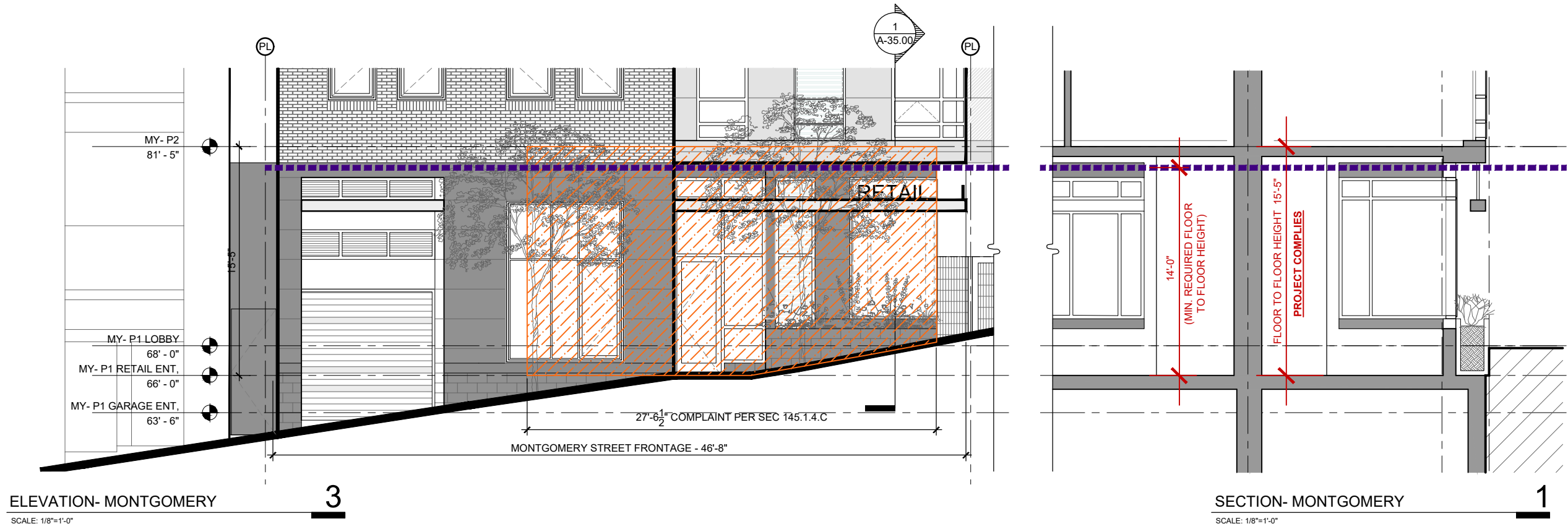
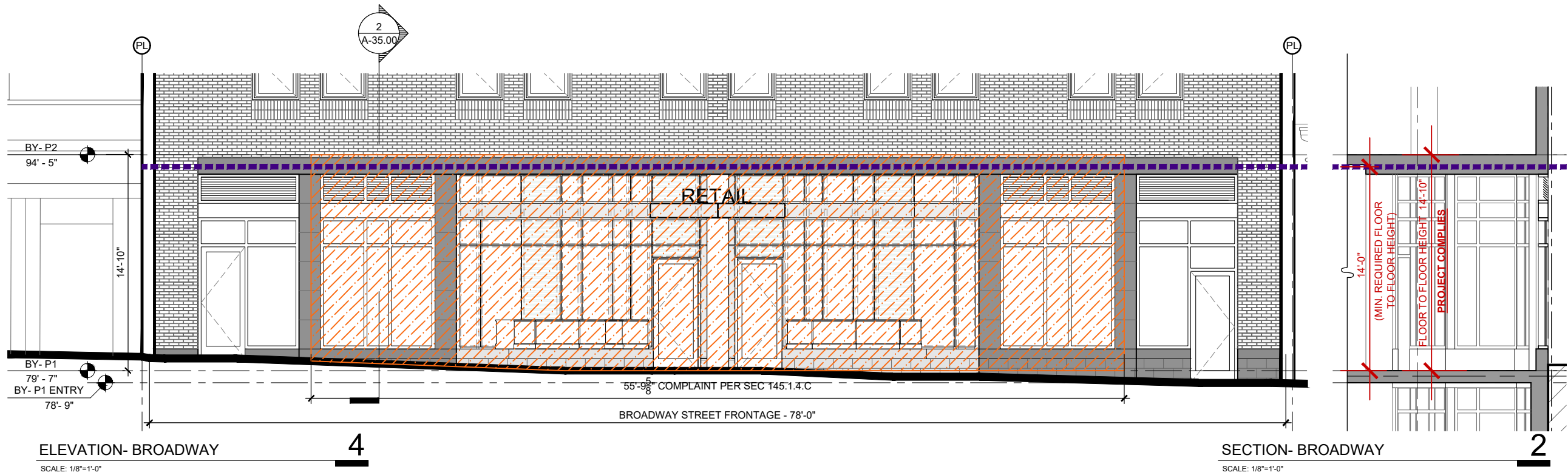
GROUND FLOOR HEIGHT
DIAGRAM (NON-RES)

A-34.0

ISSUE

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CITY SUBMISSION

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LEGEND



COMPLIANT PER
SEC 145.1(C)(4)



4'-0" FLOOR TO FLOOR HEIGHT
PER SF PLANNING CODE
SEC 145.1(C)(4)

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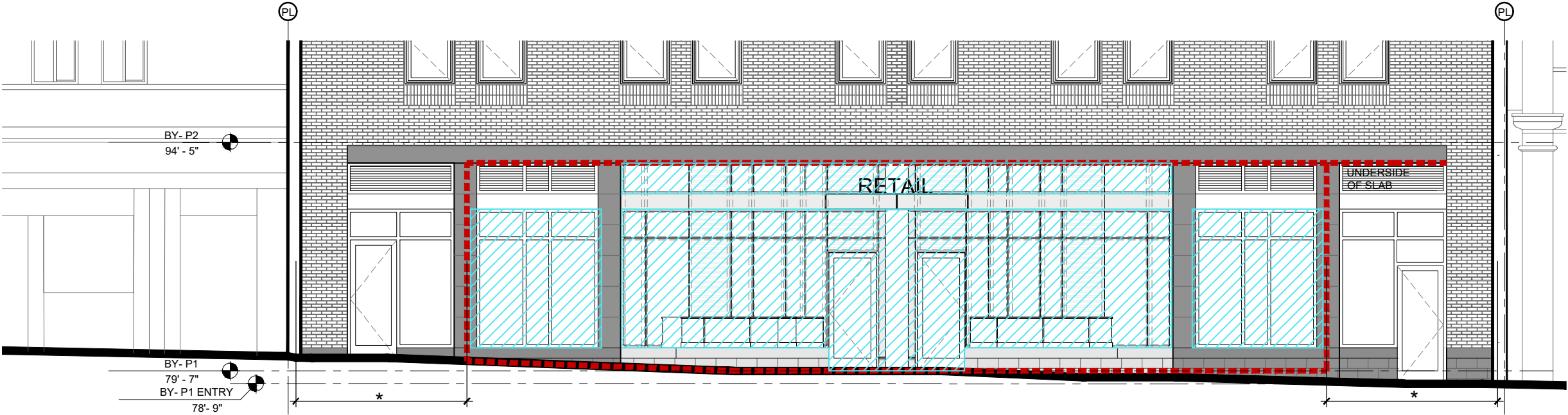
OWNER

MONTGOMERY PLACE

425 Broadway Street
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APN : 0163/002

SCALE : SCALE AS NOTED
DRAWING TITLE : STREET FRONT FENESTRATION
SHEET NO. : A-35.0
DATE : ISSUE

11/12/2021 DRAWING SET - FOR CITY SUBMISSION



ELEVATION- BROADWAY
SCALE: 1/8"=1'-0"

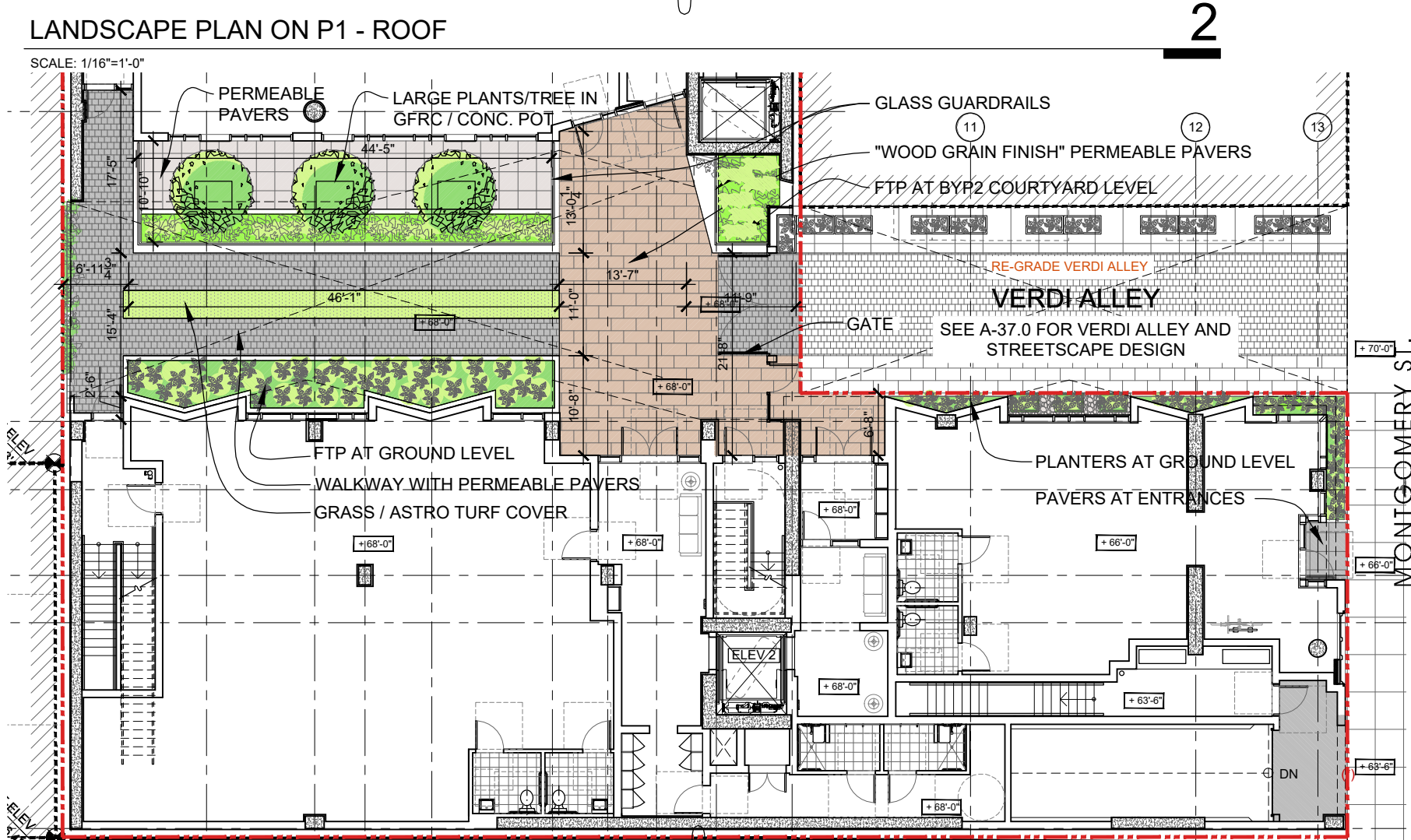
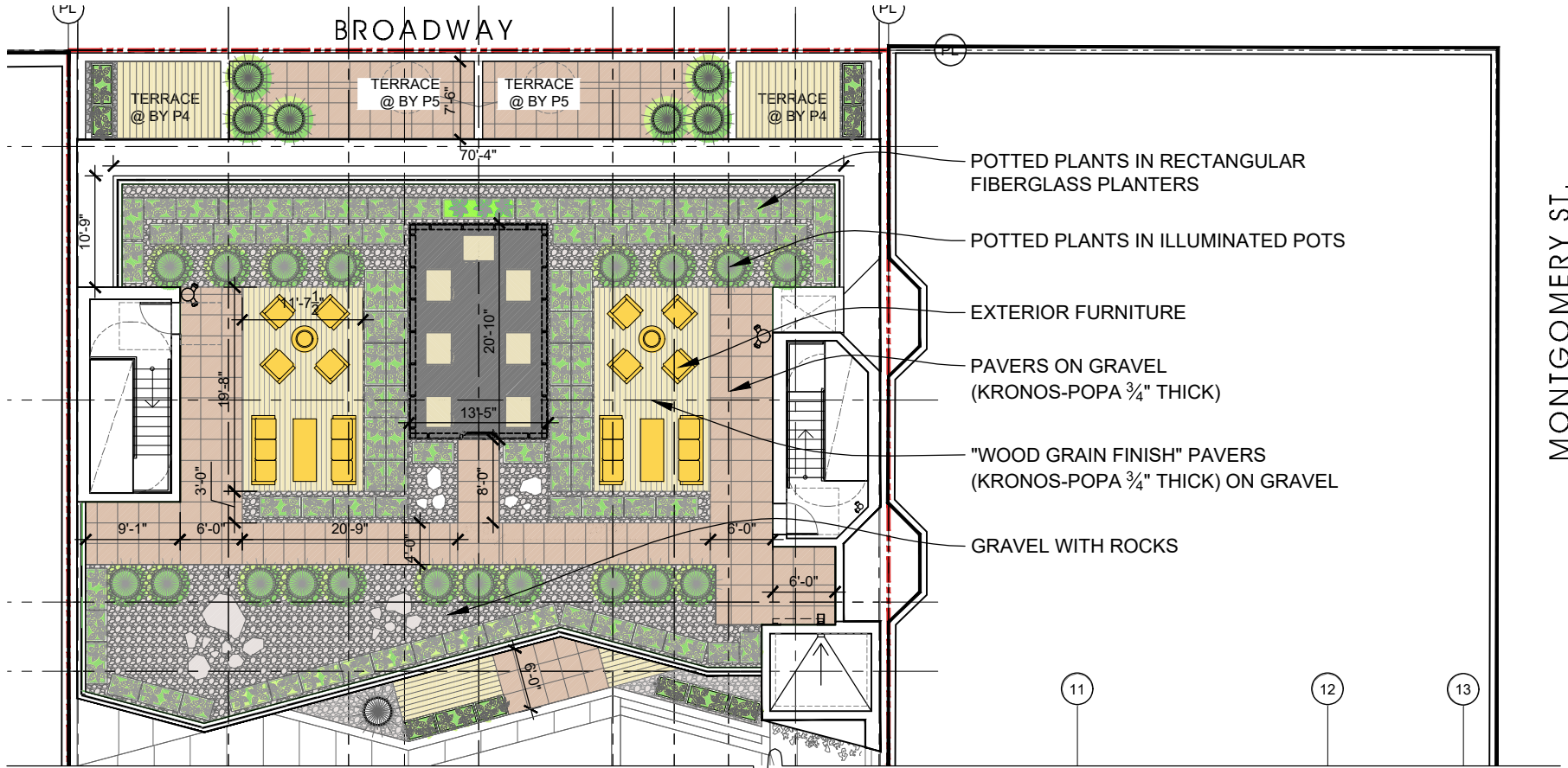


ELEVATION- MONTGOMERY
SCALE: 1/8"=1'-0"

STREET FRONTAGE AREA TOTAL:	1141 SF
REQUIRED FENESTRATION (60% OF ACTIVE STREET FRONTAGE AREA) PER SFPC SEC 145.1(6):	690.6 SF
PROPOSED FENESTRATION:	783 SF
:	68.62 %
PROJECT COMPLIES	

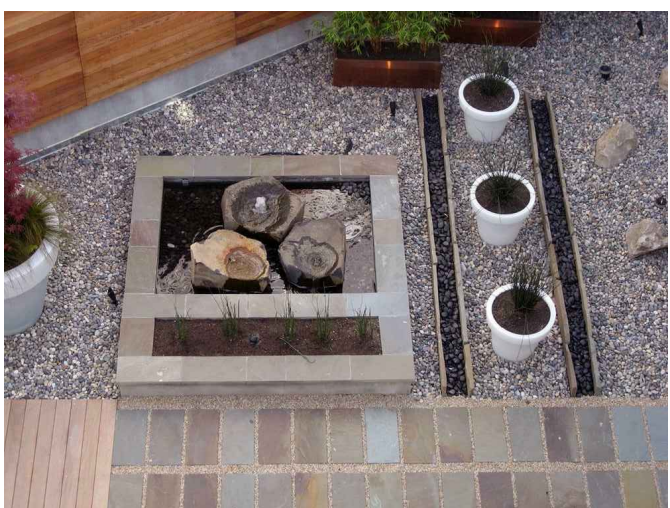
- LEGEND:
- PROPOSED
TRANSPARENT AREA
 - STREET FRONTAGE
ACTIVE USE AREA
 - NON-ACTIVE USE ALLOWED FOR
PARKING/ LOADING, BUILDING EGRESS,
AND ACCESS TO MEP BLDG SYSTEMS
PER SFPC SEC 145.1(3)

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MONTGOMERY ST.

MONTGOMERY ST.



ON-SITE LANDSCAPE REFERENCE IMAGES

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MONTGOMERY PLACE
425 Broadway Street
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APN : 0163/002

PROJECT

SCALE : 1/16" :1'-0"

DRAWING TITLE : LANDSCAPE PLAN

SHEET NO. : A-36.0

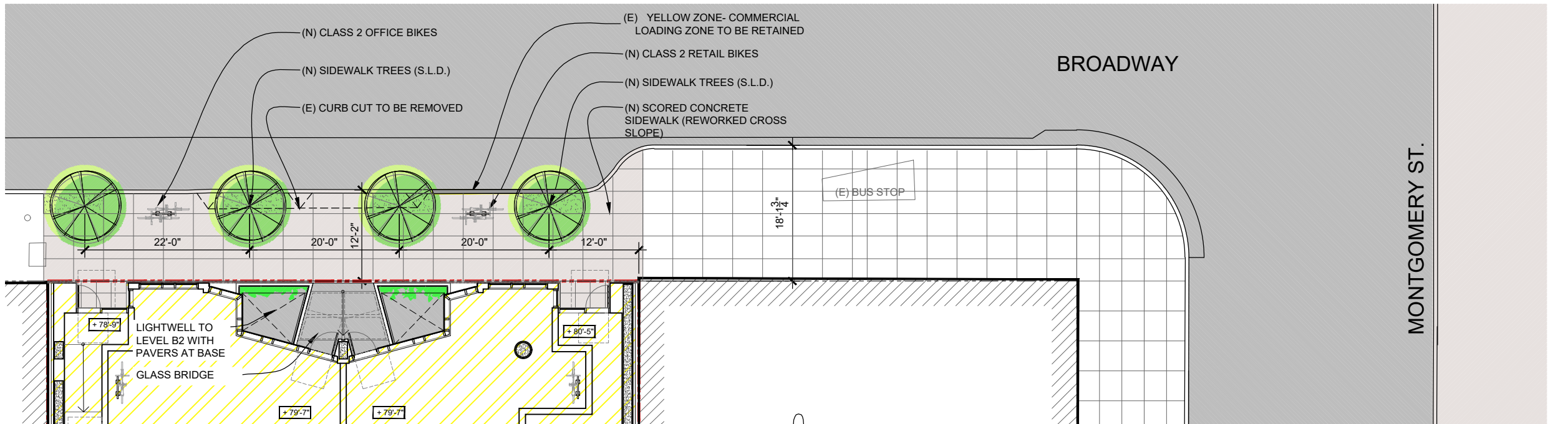
DATE : 11/12/2021

ISSUE : DRAWING SET - FOR CITY SUBMISSION



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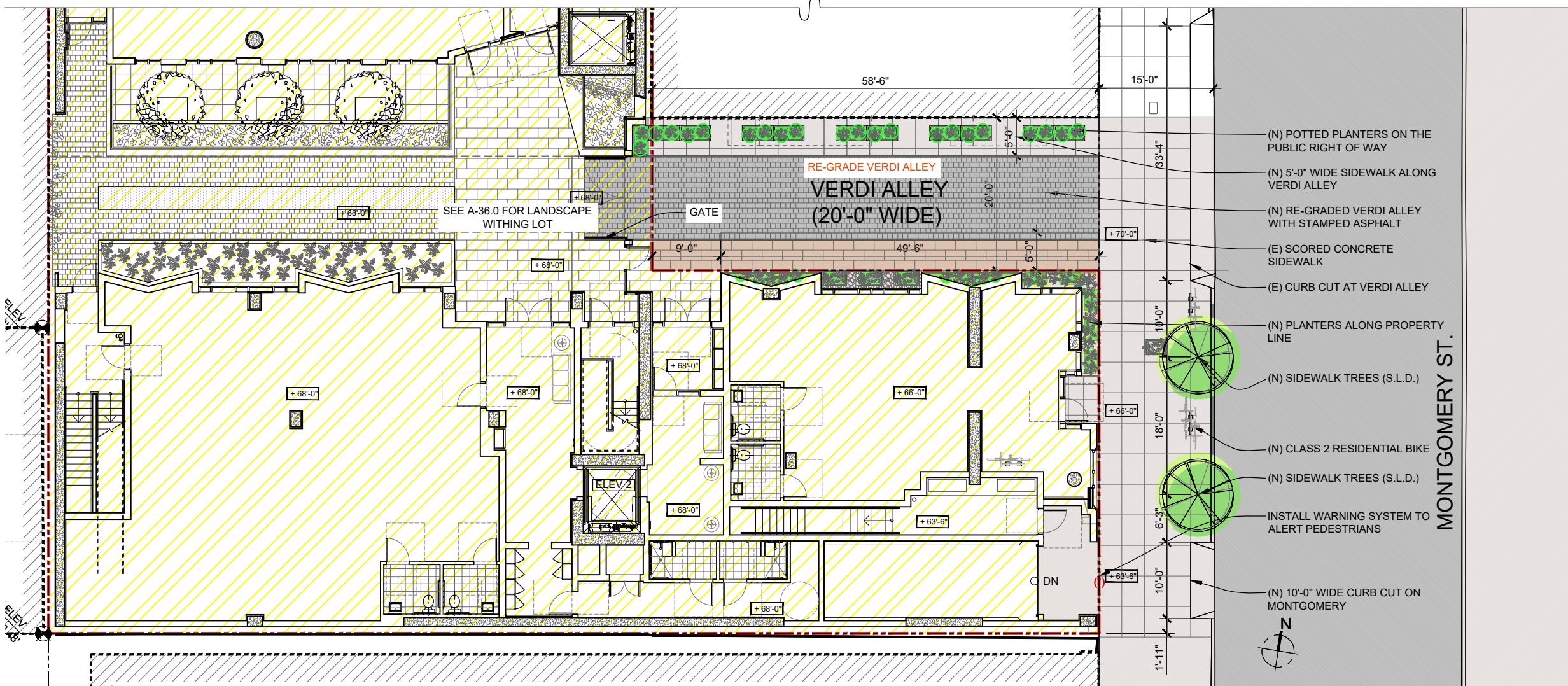
project contact: Ian Birchall



STREETSCAPE ALONG BROADWAY

SCALE: 1/16"=1'-0"

2



STREETSCAPE ALONG MONTGOMERY ST. AND VERDI ALLEY

SCALE: 1/16"=1'-0"

1

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PROJECT

APN : 0163/002

SCALE :

1/16" :1'0"

DRAWING TITLE :

STREETSCAPE PLAN

SHEET NO. :

A-37.0

DATE

ISSUE

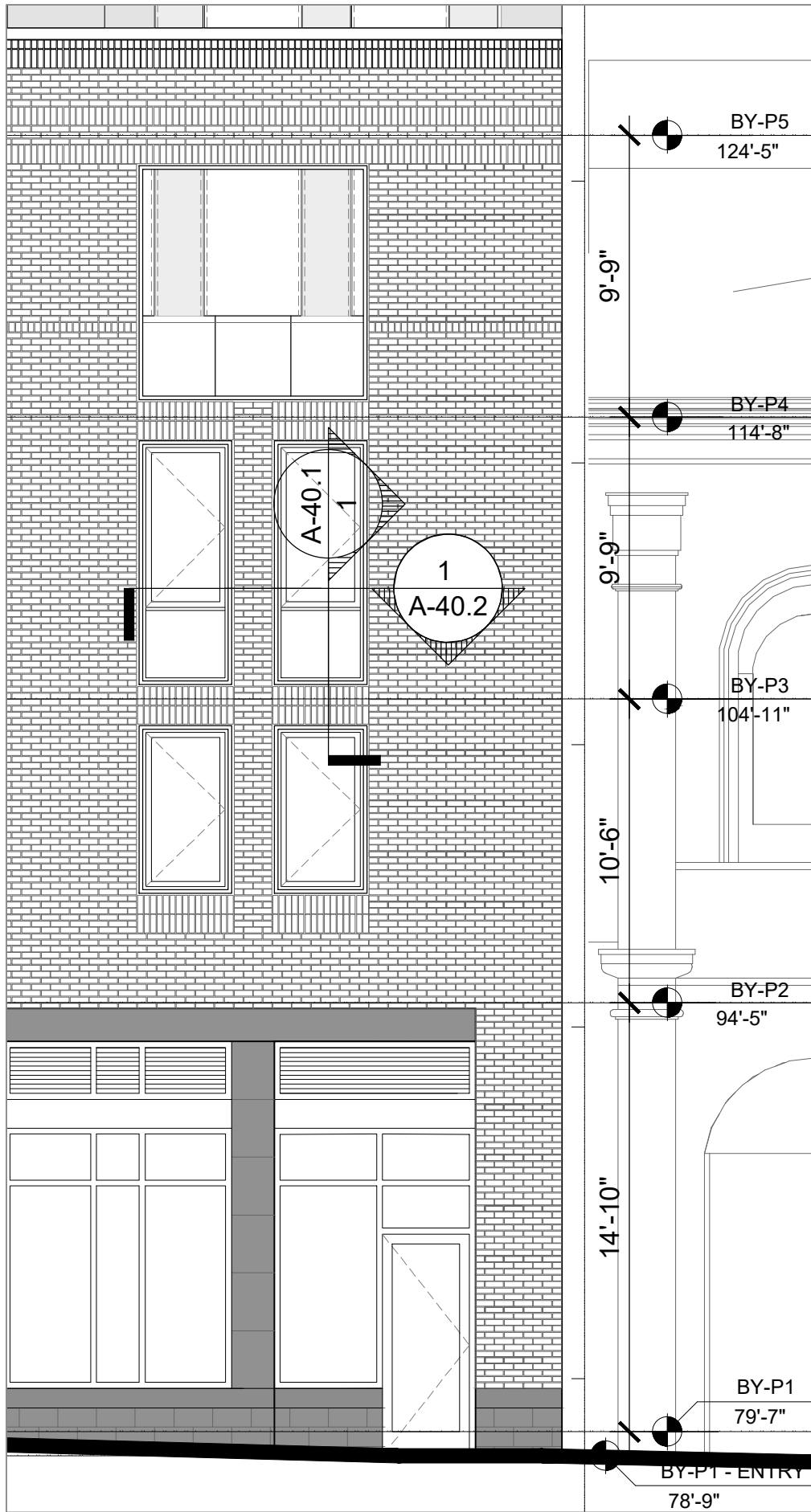
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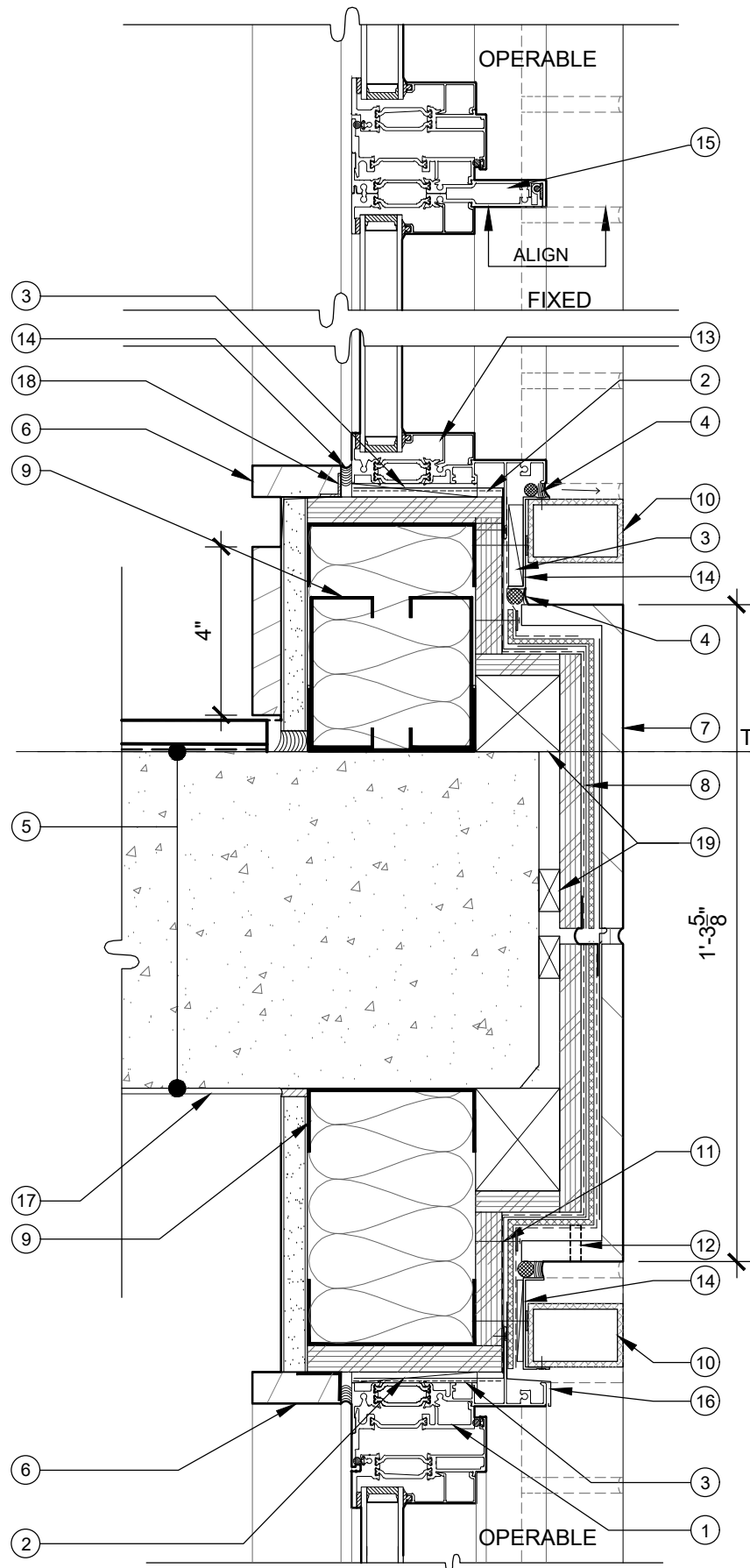
project contact: Ian Birchall



PART ELEVATION- BROADWAY

SCALE: 3/16"=1'-0"

2



DETAIL 1- WINDOW SILL AND HEAD

SCALE: 3/16"=1'-0"

1

- ① BLOMBERG T-450N CASEMENT WINDOW HEAD- OPERABLE, SEE SPECS.
- ② LIQUID FLASHING AT R.O. - EXTEND 1" BEYOND INTERIOR FACE OF WINDOW FRAME
- ③ SHIM AS REQUIRED
- ④ BACKER ROD AND SEALANT- PROVIDE WEEPS AT SILL CONDITIONS
- ⑤ 8 " CONCRETE PT SLAB
- ⑥ 3/4" TYP. WOODEN TRIM, AT JAMB, SILL AND HEAD CONDITIONS,
- ⑦ 1/2" THK THIN BRICK ON 1" BRICKIT MCS SYSTEM
- ⑧ WRB TL: (3) LAYERS: GRADE D PAPER BACKED GROOVED LATH O/ KEEN DRIWALL RAINSCREEN O/ COMMERCIAL TYVEK
- ⑨ FRAMING, S.S.D
- ⑩ 1 1/2" X 2 1/4" ALUMINUM TUBE- COLOR TO MATCH WINDOW FRAME
- ⑪ SAF
- ⑫ 1/4" Ø PLASTIC WEEP PIPES IN MORTAR JOINTS SET FLUSH W/ MORTAR- 1 EVERY 2 TILES
- ⑬ BLOMBERG T-450N CASEMENT WINDOW SILL - OPERABLE, SEE SPECS.
- ⑭ BRIGHT POLISHED ALUMINUM PROFILE
- ⑮ BLOMBERG T-450N CASEMENT WINDOW MULLION, SEE SPECS.
- ⑯ ALUM. HEAD FLASHING - MATCH WINDOW FRAME COLOR
- ⑰ SKIM COAT
- ⑱ BACK DAM IN FULL BED OF SEALANT
- ⑲ F.R.T WOOD BLOCKING AT SLAB EDGE

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PROJECT

APN : 0163/002

SCALE : 1/16" :1'0"

THIN BRICK EXT CLADDING
DRAWING TITLE : PART ELV & DETAILS

SHEET NO. : A-40.1

DATE ISSUE

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MONTGOMERY PLACE

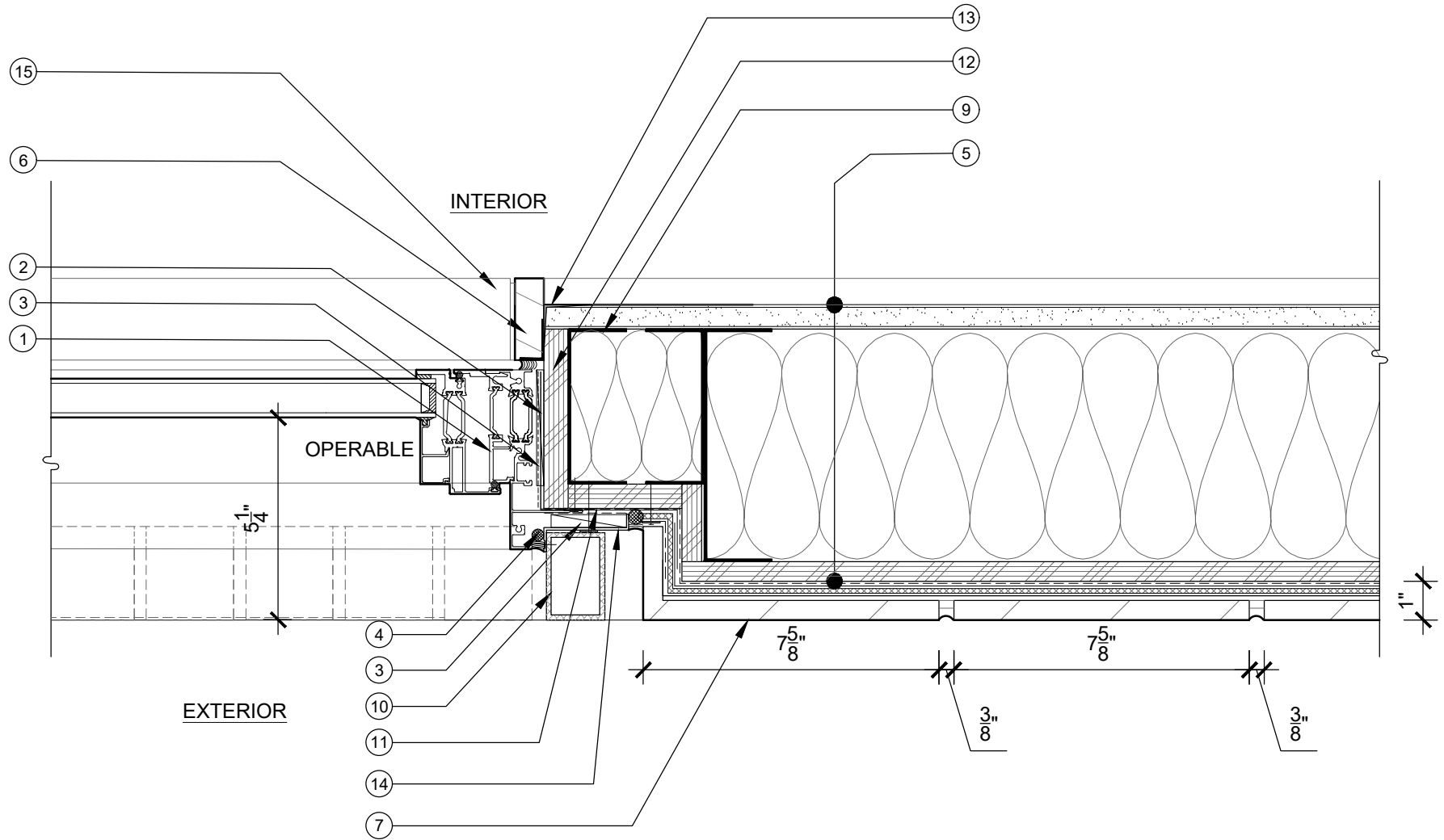
425 Broadway Street
San Francisco,
CA 94133.
PROJECT
APN : 0163/002

SCALE : 1/16" : 1'0"
THIN BRICK EXT CLADDING
DRAWING TITLE : PART ELV & DETAILS

SHEET NO. : A-40.2

DATE ISSUE

11/12/2021
DRAWING SET - FOR
CITY SUBMISSION



- | | |
|---|--|
| ① BLOMBERG T-450N CASEMENT WINDOW JAMB
- OPERABLE, SEE SPECS. | ⑧ WRB TL: (3) LAYERS: GRADE D PAPER BACKED
GROOVED LATH O/ KEEN DRIWALL RAINSCREEN O/
COMMERCIAL TYVEK |
| ② LIQUID FLASHING AT R.O. - EXTEND 1" BEYOND
INTERIOR FACE OF WINDOW FRAME | ⑨ FRAMING, S.S.D |
| ③ SHIM AS REQ | ⑩ 1 1/2" X 2 1/4" ALUMINUM TUBE- COLOR TO MATCH
WINDOW FRAME |
| ④ BACKER ROD AND SEALANT | ⑪ SAF |
| ⑤ EXTERIOR WALL ASSEMBLY | ⑫ 5/8" F.R.T. PLYWOOD |
| ⑥ 3/4" TYP. WOODEN TRIM, AT JAMB, SILL AND HEAD
CONDITIONS, | ⑬ CORNER BEAD + SPACKLE CONTINUOUS AROUND OPENING |
| ⑦ 1/2" THK THIN BRICK ON 1" BRICKIT MCS SYSTEM | ⑭ SEALANT |
| | ⑮ SILL BELOW |

DETAIL 1- WINDOW JAMB WITH FIXED WINDOW
SCALE: 3/16"=1'-0"

ib+a
architecture
ian birchall and associates
177 post street, suite 920
san francisco, ca 94108
t: 415.512.9660 f: 415.512.9663
www.ibadesign.com
project contact: Ian Birchall

MONTGOMERY PLACE
425 Broadway Street
San Francisco,
CA 94133.
PROJECT
APN : 0163/002

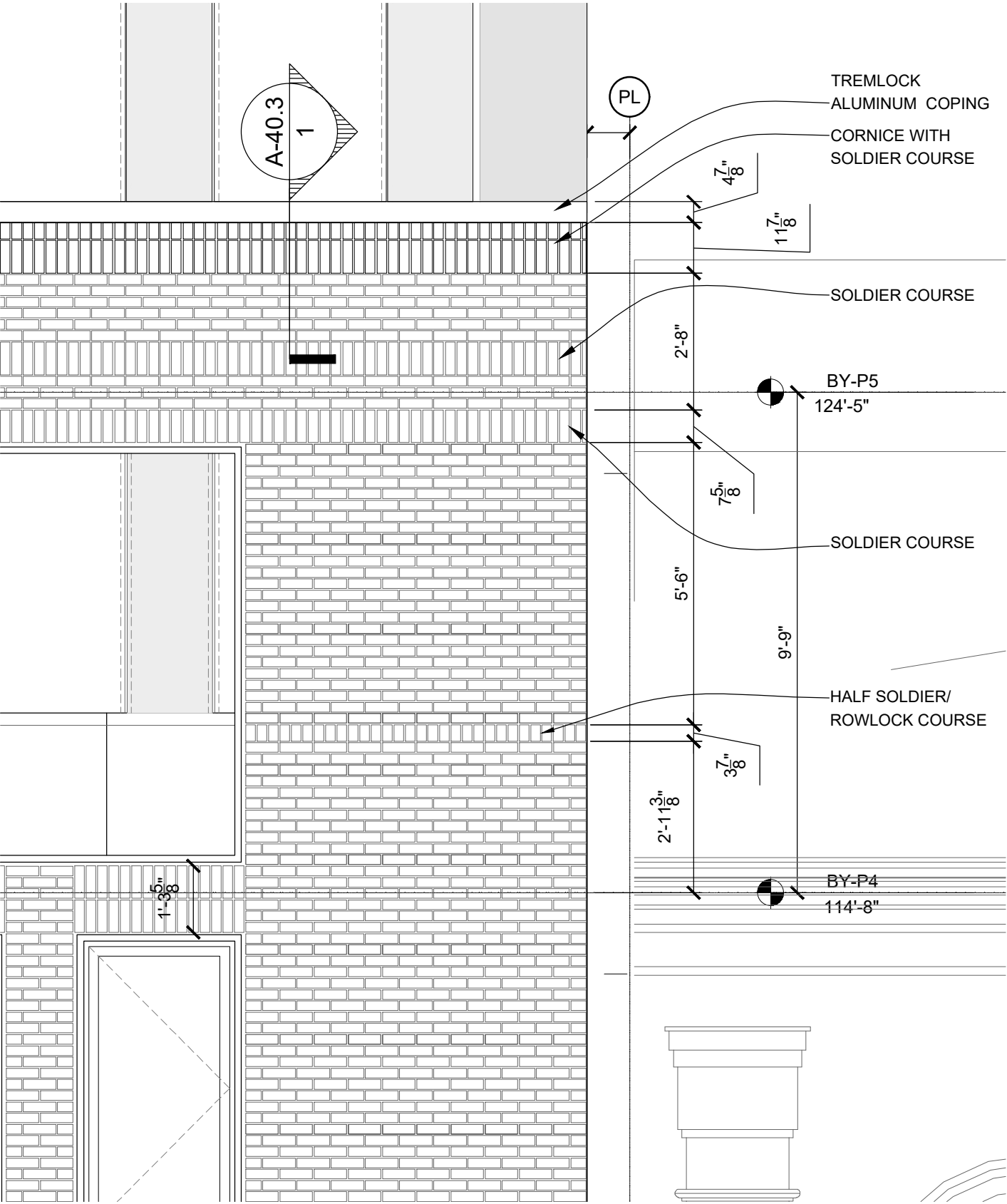
SCALE : 1/16" :1'0"
DRAWING TITLE : THIN BRICK EXT CLADDING
PART ELV & DETAILS
SHEET NO. : A-40.3
DATE : ISSUE :

11/12/2021
DRAWING SET - FOR
CITY SUBMISSION

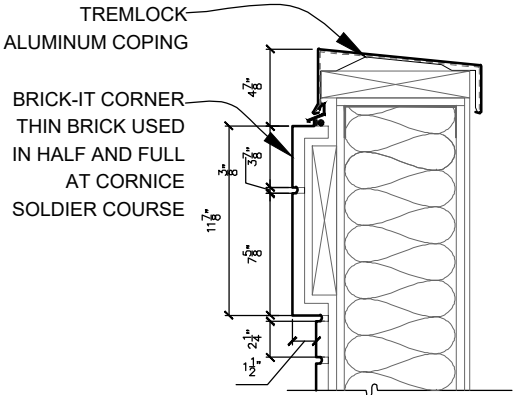


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project contact: Ian Birchall



PART ELEVATION BROADWAY
SCALE: 3/8"=1'-0"



DETAIL 1- CORNICE & COPING DETAIL
SCALE: 1"=1'-0"



ELEVATION- BROADWAY

2



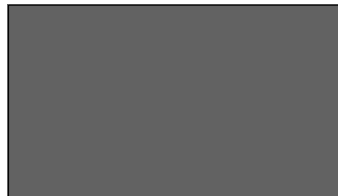
ELEVATION- MONTGOMERY STREET

1

LEGEND



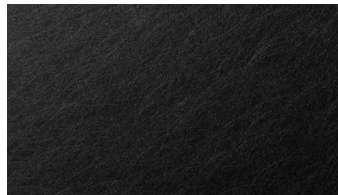
1. BRICK-IT THIN BRICK TILE-
MANGANESE IRONPOT



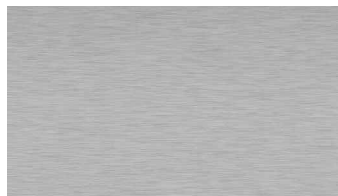
2. ECOSTUCCO -
TADELAKT FINISH
DARK GRAY



3. ECOSTUCCO -
TADELAKT FINISH
SUPER WHITE



4. DARK GRAY ALUMINUM
- STOREFRONT
- WINDOW FRAMES
- LOUVERS
- GARAGE DOOR



5. BRUSHED STAINLESS
STEEL- 120 GRIT
- FLYING BEAMS
- CANOPIES
- PANELS AT BROADWAY
RETAIL ENTRY



6. ANODIZED ALUMINUM
RECTANGULAR TUBE-
COLOR TO MATCH SUPER
WHITE ECOSTUCCO

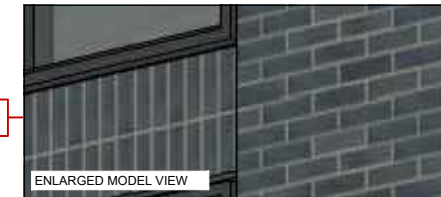


7. FRITTED GLASS
- SELECT WINDOW GLAZING
- BROADWAY RETAIL
ENTRY GUARDRAIL

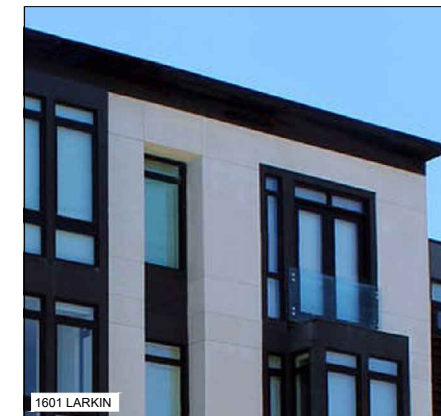


8. DARK GRAY PORCELAIN
TILE

ADDITIONAL REFERENCE IMAGES



REFERENCE IMAGES BELOW
SHOW OUR 1601 LARKIN, SF
PROJECT WHERE WE HAVE USED
ECO STUCCO / THERMOCROMEX.



Montgomery Place LLC
1630 Oakland Road #A215
San Jose, CA 95131
Richard Chen - Manager
jhrchen@gmail .com

OWNER

MONTGOMERY PLACE

425 Broadway Street
San Francisco,
CA 94133.

PROJECT

APN : 0163/002

SCALE :

1/16" :1'0"

DRAWING TITLE :

MATERIAL BOARD

SHEET NO. :

A-41.0

DATE

ISSUE

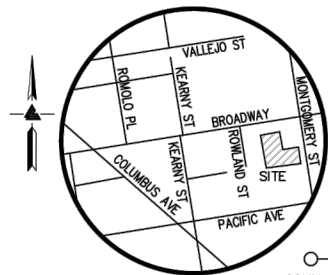
11/12/2021

DRAWING SET - FOR
CITY SUBMISSION



ib+a
architecture
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san francisco, ca 94108
t: 415.512.9660 f: 415.512.9663
www.ibadesign.com

project contact: Ian Birchall



VICINITY MAP
NO SCALE

UTILITY NOTE

PARKING LOT WAS OCCUPIED AT TIME OF FIELD WORK. UTILITIES MAY EXIST NOT SHOWN ON THIS SURVEY.

LEGEND AND NOTES

---	BOUNDARY LINE	RP	ROOF PEAK
---	BUILDING OVERHANG LINE	SSCO	SANITARY SEWER CLEAN-OUT
-E-	ELECTRICAL OVERHEAD LINE	SSMH	SANITARY SEWER MANHOLE
---	EASEMENT	SS	SANITARY SEWER VAULT
SS	SANITARY SEWER LINE	*	STREET LIGHT
-C-C-	COMMUNICATION LINE (PAINT MARKINGS)	TEL	TELEPHONE BOX
-E-E-	ELECTRICAL LINE (PAINT MARKINGS)	TC	TOP OF CURB
SS	SANITARY SEWER LINE (PAINT MARKINGS)	TOS	TOP OF SLAB
AD	AREA DRAIN	MH	UTILITY MANHOLE
B	BENCHMARK	WM	WATER METER
BOL	BOLLARD	WV	WATER VALVE
CB	CATCH BASIN	XXX.XX	SPOTGRADE
CO	CLEAN-OUT BOX		
EB	ELECTRICAL BOX		
EM	ELECTRICAL METER		
FF	FINISH FLOOR		
FDC	FIRE DEPARTMENT CONNECTION		
GH	FIRE HYDRANT		
SP	FIRE SPRINKLER		
FL	FLOW LINE		
GV	GAS VALVE		
HVE	HIGH VOLTAGE ELECTRICAL VAULT		
INV	INVERT		
PBMH	PAC BELL MANHOLE		
P	PARKING METER		
PG&E	PG&E BOX		
PG&E	PG&E MANHOLE		

ASPHALT	BRICK	CONCRETE	METAL
SSCO	SSMH	SS	*
TEL	TC	TOS	MH
WM	WV	XXX.XX	

NOTES

ALL DISTANCES AND DIMENSIONS ARE IN FEET AND DECIMALS.

UNDERGROUND UTILITY LOCATION IS BASED ON SURFACE EVIDENCE.

BUILDING FOOTPRINTS ARE SHOWN TO FINISHED MATERIAL (STUCCO/SIDING) AT GROUND LEVEL.

FINISH FLOOR ELEVATIONS ARE TAKEN AT DOOR THRESHOLD (EXTERIOR).

EASEMENT NOTE

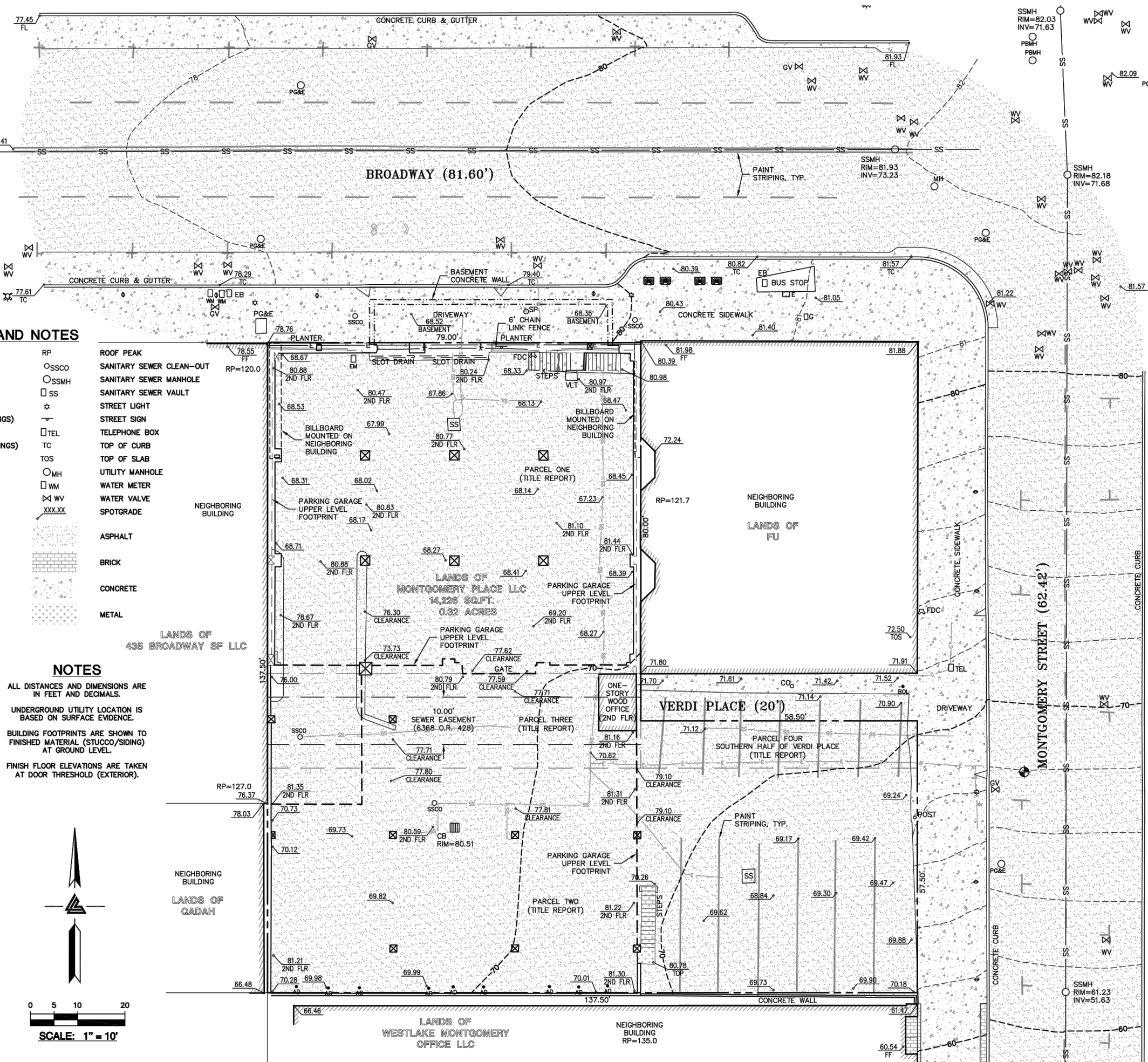
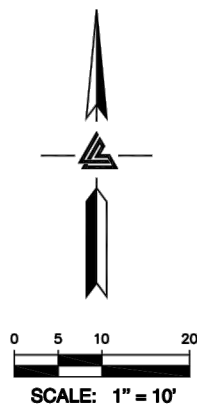
EASEMENTS SHOWN ARE PER TITLE REPORT PREPARED BY FIRST AMERICAN TITLE INSURANCE COMPANY, ORDER NO. NCS-827742-SC, DATED JANUARY 27, 2017 (AMENDED & UPDATED 2/17/17).

BENCHMARK

CITY OF SAN FRANCISCO BENCHMARK BM11323 SET CCSF STANDARD 1/2" DOMED STAINLESS STEEL ANCHOR SCREW WITH WASHER STAMPED "CCSF CONTROL" IN CURB AT THE NORTHEASTERLY RETURN OF COLUMBUS AVE. AND KEARNY ST. 0.3' EASTERLY FROM FACE OF CURB ON KEARNY ST. 5.3' NORTHWESTERLY FROM CENTER/CORNER STREET LIGHT/MUNI GUY POLE/TRAFFIC SIGNAL/STREET SIGN POLE. 13.8' WESTERLY FROM MOST NORTHWESTERLY CORNER OF PLANTER 2.7' HIGH AT #170 COLUMBUS AVE. 20.6' SOUTHERLY FROM CENTER/CORNER CATCH BASIN AT THE SOUTHEASTERLY RETURN OF KEARNY ST. AND PACIFIC AVE. ELEVATION = 40.786' (NAVD 88 DATUM)

SITE BENCHMARK

SURVEY CONTROL POINT MAG AND SHINER SET IN ASPHALT ELEVATION = 67.95' (NAVD 88 DATUM)



LEA & BRAZE ENGINEERING, INC.
CIVIL ENGINEERS • LAND SURVEYORS
SACRAMENTO REGION
3017 DOUGLAS BLVD., # 300
ROSEVILLE, CA 95661
BAY AREA REGION
2495 INDUSTRIAL PKWY WEST
HAYWARD, CALIFORNIA 94545
(916) 936-5335
(916) 887-3017
(916) 887-7363
WWW.LEABRAZE.COM

425 BROADWAY STREET
SAN FRANCISCO
CALIFORNIA

APN: 003-022-045

SAN FRANCISCO COUNTY

TOPOGRAPHIC
SURVEY

REVISIONS	BY
JOB NO:	2170697
DATE:	8-14-17
SCALE:	1" = 10'
FIELD BY:	ES
DRAWN BY:	DB
SHEET NO:	

SU1

1 OF 1 SHEETS

PRINTED: NTS



CEQA Exemption Determination

PROPERTY INFORMATION/PROJECT DESCRIPTION

Project Address		Block/Lot(s)
425 BROADWAY		0163002
Case No.		Permit No.
2017-015678ENV		
<input checked="" type="checkbox"/> Addition/ Alteration	<input checked="" type="checkbox"/> Demolition (requires HRE for Category B Building)	<input checked="" type="checkbox"/> New Construction
Project description for Planning Department approval. The project site is currently developed with a 144-space, 21,725 square-foot commercial parking structure that is accessed via Broadway and Montgomery Street. On Broadway, there is also a Muni bus stop serviced by the 12-Folsom Muni bus route adjacent to the site, and a commercial loading zone along the property frontage. The proposed project would demolish the existing parking structure and construct two new mixed-use buildings on the L-shaped project site. The Broadway building would be five stories (56 feet) tall with two basement levels and contain approximately 6,700 square feet of design professional office space in the two basement levels, approximately 3,450 square feet of retail on ground level, approximately 1,850 square feet of design professional office and two residential units on floor 2, and 12 residential units on floors 3 to 5 (i.e., four residences per floor). The Broadway building would also include 2,640 square feet of common open space on the rooftop. Select dwelling units would also have a private terrace. The Montgomery building would be seven stories (64 feet) tall with one basement level and would contain a 17-space basement parking garage in the basement level, approximately 2,150 square feet of design professional office space and 1,150 square feet of retail space on floor 1 (street level), FULL PROJECT DESCRIPTION ATTACHED		

STEP 1: EXEMPTION TYPE

The project has been determined to be exempt under the California Environmental Quality Act (CEQA).	
<input type="checkbox"/>	Class 1 - Existing Facilities. Interior and exterior alterations; additions under 10,000 sq. ft.
<input type="checkbox"/>	Class 3 - New Construction. Up to three new single-family residences or six dwelling units in one building; commercial/office structures; utility extensions; change of use under 10,000 sq. ft. if principally permitted or with a CU.
<input checked="" type="checkbox"/>	Class 32 - In-Fill Development. New Construction of seven or more units or additions greater than 10,000 sq. ft. and meets the conditions described below: (a) The project is consistent with the applicable general plan designation and all applicable general plan policies as well as with applicable zoning designation and regulations. (b) The proposed development occurs within city limits on a project site of no more than 5 acres substantially surrounded by urban uses. (c) The project site has no value as habitat for endangered rare or threatened species. (d) Approval of the project would not result in any significant effects relating to traffic, noise, air quality, or water quality. (e) The site can be adequately served by all required utilities and public services.
<input type="checkbox"/>	Other _____
<input type="checkbox"/>	Common Sense Exemption (CEQA Guidelines section 15061(b)(3)). It can be seen with certainty that there is no possibility of a significant effect on the environment.

STEP 2: ENVIRONMENTAL SCREENING ASSESSMENT**TO BE COMPLETED BY PROJECT PLANNER**

<input checked="" type="checkbox"/>	Air Quality: Would the project add new sensitive receptors (specifically, schools, day care facilities, hospitals, residential dwellings, and senior-care facilities within an Air Pollution Exposure Zone? Does the project have the potential to emit substantial pollutant concentrations (e.g. use of diesel construction equipment, backup diesel generators, heavy industry, diesel trucks, etc.)? <i>(refer to the Environmental</i>
<input checked="" type="checkbox"/>	Hazardous Materials: <input checked="" type="checkbox"/> Mahe or <input type="checkbox"/> Cortese Is the project site located within the Maher area or on a site containing potential subsurface soil or groundwater contamination and would it involve ground disturbance of at least 50 cubic yards or a change of use from an industrial use to a residential or institutional use? Is the project site located on a Cortese site or would the project involve work on a site with an existing or former gas station, parking lot, auto repair, dry cleaners, or heavy manufacturing use, or a site with current or former underground storage tanks? <i>if Maher box is checked, note below whether the applicant has enrolled in or received a waiver from the San Francisco Department of Public Health (DPH) Maher program, or if Environmental Planning staff has determined that hazardous material effects would be less than significant.</i> Note that a categorical exemption shall not be issued for a project located on the Cortese List
<input type="checkbox"/>	Transportation: Does the project involve a child care facility or school with 30 or more students, or a location 1,500 sq. ft. or greater? Does the project have the potential to adversely affect transit, pedestrian and/or bicycle safety (hazards) or the adequacy of nearby transit, pedestrian and/or bicycle facilities? Would the project involve the intensification of or a substantial increase in vehicle trips at the project site or elsewhere in the region due to autonomous vehicle or for-hire vehicle fleet maintenance, operations or
<input checked="" type="checkbox"/>	Archeological Resources: Would the project result in soil disturbance/modification greater than two (2) feet below grade in an archeological sensitive area or eight (8) feet in a non-archeological sensitive area? If yes, archeology review is required.
<input type="checkbox"/>	Subdivision/Lot Line Adjustment: Does the project site involve a subdivision or lot line adjustment on a lot with a slope average of 20% or more? <i>(refer to the Environmental Information tab on https://sfplanninggis.org/PIM/)</i> If box is checked, Environmental Planning must issue the exemption.
<input type="checkbox"/>	Average Slope of Parcel = or > 25%, or site is in Edgehill Slope Protection Area or Northwest Mt. Sutro Slope Protection Area: Does the project involve any of the following: (1) New building construction, except one-story storage or utility occupancy, (2) horizontal additions, if the footprint area increases more than 50%, or (3) horizontal and vertical additions increase more than 500 square feet of new projected roof area? <i>(refer to the Environmental Information tab on https://sfplanninggis.org/PIM/)</i> If box is checked, a geotechnical report is likely required and Environmental Planning must issue the exemption.
<input type="checkbox"/>	Seismic Hazard: <input type="checkbox"/> Landslide or <input type="checkbox"/> Liquefaction Hazard Zone: Does the project involve any of the following: (1) New building construction, except one-story storage or utility occupancy, (2) horizontal additions, if the footprint area increases more than 50%, (3) horizontal and vertical additions increase more than 500 square feet of new projected roof area, or (4) grading performed at a site in the landslide hazard zone? <i>(refer to the Environmental Information tab on https://sfplanninggis.org/PIM/)</i> If box is checked, a geotechnical report is required and Environmental Planning must issue the exemption.
Comments and Planner Signature (optional): Ryan Shum PLEASE SEE ATTACHED	

STEP 3: PROPERTY STATUS - HISTORIC RESOURCE
TO BE COMPLETED BY PROJECT PLANNER

PROPERTY IS ONE OF THE FOLLOWING: (refer to *Property Information Map*)

<input checked="" type="checkbox"/>	Category A: Known Historical Resource. GO TO STEP 5.
<input type="checkbox"/>	Category B: Potential Historical Resource (over 45 years of age). GO TO STEP 4.
<input type="checkbox"/>	Category C: Not a Historical Resource or Not Age Eligible (under 45 years of age). GO TO STEP 6.

STEP 4: PROPOSED WORK CHECKLIST
TO BE COMPLETED BY PROJECT PLANNER

Check all that apply to the project.

<input type="checkbox"/>	1. Change of use and new construction. Tenant improvements not included.
<input type="checkbox"/>	2. Regular maintenance or repair to correct or repair deterioration, decay, or damage to building.
<input type="checkbox"/>	3. Window replacement that meets the Department's <i>Window Replacement Standards</i> . Does not include storefront window alterations.
<input type="checkbox"/>	4. Garage work. A new opening that meets the <i>Guidelines for Adding Garages and Curb Cuts</i> , and/or replacement of a garage door in an existing opening that meets the Residential Design Guidelines.
<input type="checkbox"/>	5. Deck, terrace construction, or fences not visible from any immediately adjacent public right-of-way.
<input type="checkbox"/>	6. Mechanical equipment installation that is not visible from any immediately adjacent public right-of-way.
<input type="checkbox"/>	7. Dormer installation that meets the requirements for exemption from public notification under <i>Zoning Administrator Bulletin No. 3: Dormer Windows</i> .
<input type="checkbox"/>	8. Addition(s) that are not visible from any immediately adjacent public right-of-way for 150 feet in each direction; does not extend vertically beyond the floor level of the top story of the structure or is only a single story in height; does not have a footprint that is more than 50% larger than that of the original building; and does not cause the removal of architectural significant roofing features.

Note: Project Planner must check box below before proceeding.

<input type="checkbox"/>	Project is not listed. GO TO STEP 5.
<input type="checkbox"/>	Project does not conform to the scopes of work. GO TO STEP 5.
<input type="checkbox"/>	Project involves four or more work descriptions. GO TO STEP 5.
<input type="checkbox"/>	Project involves less than four work descriptions. GO TO STEP 6.

STEP 5: ADVANCED HISTORICAL REVIEW
TO BE COMPLETED BY PRESERVATION PLANNER

Check all that apply to the project.

<input type="checkbox"/>	1. Reclassification of property status. (Attach <i>HRER Part I</i>) <div style="display: flex; justify-content: space-between; align-items: flex-start; margin-top: 10px;"> <div style="width: 45%;"> <input type="checkbox"/> Reclassify to Category A a. Per HRER b. Other (specify): </div> <div style="width: 45%; text-align: right;"> <input type="checkbox"/> Reclassify to Category C (No further historic review) </div> </div>
<input type="checkbox"/>	2. Project involves a known historical resource (CEQA Category A) as determined by Step 3 and conforms entirely to proposed work checklist in Step 4.
<input type="checkbox"/>	3. Interior alterations to publicly accessible spaces that do not remove, alter, or obscure character defining features.
<input type="checkbox"/>	4. Window replacement of original/historic windows that are not "in-kind" but are consistent with existing historic character.
<input type="checkbox"/>	5. Façade/storefront alterations that do not remove, alter, or obscure character-defining features.

<input type="checkbox"/>	6. Raising the building in a manner that does not remove, alter, or obscure character-defining features.
<input type="checkbox"/>	7. Restoration based upon documented evidence of a building's historic condition, such as historic photographs, plans, physical evidence, or similar buildings.
<input type="checkbox"/>	8. Work consistent with the <i>Secretary of the Interior Standards for the Treatment of Historic Properties</i> (Analysis required):
<input checked="" type="checkbox"/>	9. Work compatible with a historic district (Analysis required): Demolition of non-contributory parking lot/parking structure within the Jackson Square Historic District Extension, construction of two new buildings. See HRER Part 2 for analysis.
<input checked="" type="checkbox"/>	10. Work that would not materially impair a historic resource (Attach HRER Part II).
Note: If ANY box in STEP 5 above is checked, a Preservation Planner MUST sign below.	
<input checked="" type="checkbox"/>	Project can proceed with exemption review. The project has been reviewed by the Preservation Planner and can proceed with exemption review. GO TO STEP 6.
Comments (optional):	
Preservation Planner Signature: Jonathan Vimr	

STEP 6: EXEMPTION DETERMINATION

TO BE COMPLETED BY PROJECT PLANNER

<input checked="" type="checkbox"/>	No further environmental review is required. The project is exempt under CEQA. There are no unusual circumstances that would result in a reasonable possibility of a significant effect.	
	Project Approval Action: Planning Commission Hearing	Signature: Ryan Shum 09/22/2021
	<p>Supporting documents are available for review on the San Francisco Property Information Map, which can be accessed at https://sfplanninggis.org/PIM/. Individual files can be viewed by clicking on the Planning Applications link, clicking the "More Details" link under the project's environmental record number (ENV) and then clicking on the "Related Documents" link. Once signed or stamped and dated, this document constitutes an exemption pursuant to CEQA Guidelines and Chapter 31 of the Administrative Code.</p> <p>In accordance with Chapter 31 of the San Francisco Administrative Code, an appeal of an exemption determination to the Board of Supervisors can only be filed within 30 days of the project receiving the approval action.</p>	

Full Project Description

The project site is currently developed with a 144-space, 21,725 square-foot commercial parking structure that is accessed via Broadway and Montgomery Street. On Broadway, there is also a Muni bus stop serviced by the 12-Folsom Muni bus route adjacent to the site, and a commercial loading zone along the property frontage.

The proposed project would demolish the existing parking structure and construct two new mixed-use buildings on the L-shaped project site. The Broadway building would be five stories (56 feet) tall with two basement levels and contain approximately 6,700 square feet of design professional office space in the two basement levels, approximately 3,450 square feet of retail on ground level, approximately 1,850 square feet of design professional office and two residential units on floor 2, and 12 residential units on floors 3 to 5 (i.e., four residences per floor). The Broadway building would also include 2,640 square feet of common open space on the rooftop. Select dwelling units would also have a private terrace.

The Montgomery building would be seven stories (64 feet) tall with one basement level and would contain a 17-space basement parking garage in the basement level, approximately 2,150 square feet of design professional office space and 1,150 square feet of retail space on floor 1 (street level), approximately 2,800 square feet of design professional office and two residential units on floor 2, and 25 residential units on floors 3 to 7 (i.e., five residences per floor). The Montgomery building rooftop would be solar-ready to accommodate solar panels. The two proposed buildings would be separated by Verdi Alley, which is a public right-of-way that extends west from Montgomery Street for approximately 58 feet and a private alley following the western terminus of the public right-of-way; the private segment of the alley is burdened with a below-grade sewer easement held by the San Francisco Public Utilities Commission. The private segment of Verdi Alley would provide project open space, and the public segment would provide pedestrian access to the residential and office lobbies in the Montgomery building and vehicular access for emergency services, deliveries, trash pick-ups, and maintenance of the below-grade sewer.

In total, the proposed project includes approximately 51,625 gross square feet of residential use, 4,940 gross square feet of retail use, and 18,735 gross square feet of design professional office use. The dwelling unit mix would comprise of 15 one-bedroom units, 21 two-bedroom units, and five three-bedroom units. Of the 41 total residential units, three of the units would be affordable units. The proposed project would include 17 vehicular residential parking spaces, 46 secure class 1 bicycle parking spaces and 7 class 2 parking spaces along the project's frontage on Broadway and Montgomery Street. Neither building would include back-up diesel generators. The existing commercial loading zone on Broadway would be retained.

The proposed project would be supported by a mat foundation. Construction of the project would disturb approximately 13,641 square feet of soil and require excavation to a depth of 15 feet below ground surface, except at the location of vehicle stackers and elevator pits, which would have a maximum depth of 21 feet. The project would excavate approximately 6,997 cubic yards of soil. Project construction is anticipated to last 26 months.

Step 2: Environmental Screening Comments

Air Quality: The project site is located within an air pollutant exposure zone but would not add new stationary sources of toxic air contaminants. Pursuant to Director's Bulletin No. 2 for Type 3, Clean Construction projects, the project sponsor has committed to using Tier 4 engines on all diesel-fueled construction equipment. Thus, no significant construction or operational air quality impacts would occur.

Archeological Resources: The department's staff archeologist conducted preliminary archeological review on February 17, 2021 and determined that the project would have no effect on archeological resources.

Hazardous Materials: The project is subject to the Maher Ordinance (Article 22A of the Health Code), which is administered by the Department of Public Health. The project sponsor enrolled in the Maher Program on March 11, 2019.

Noise: The project would use typical construction equipment that would be regulated by Article 29 of the Police Code (section 2907, Construction Equipment). No impact pile driving or nighttime construction is required. Construction vibration would not be anticipated to affect adjacent buildings. The proposed project would not generate sufficient vehicle trips to noticeably increase ambient noise levels, and the project's fixed noise sources, such as heating, ventilation, and air conditioning systems, would be subject to noise limits in Article 29 of the Police Code (section 2909, Noise Limits).

Traffic: The department's transportation staff reviewed the proposed project on November 3, 2020 and determined that additional transportation review is not required.

Water Quality: The project's construction activities are required to comply with the Construction Site Runoff Ordinance (Public Works Code, article 2.4, section 146). Since the project would disturb over 5,000 square feet, the project sponsor would be required to submit an Erosion Sediment Control Plan or a Stormwater Pollution Prevention Plan, and a Construction Site Runoff Control Project Application to SFPUC. Stormwater and wastewater discharged from the project site during operations would flow to the City's combined sewer system and be treated to the standards in the City's National Pollution Discharge Elimination System permit.

Natural Habitat: The project site is paved and within a developed urban area. The project site has no significant riparian corridors, estuaries, marshes, wetlands, or any other potential wildlife habitat that might contain endangered, rare or threatened species. Thus, the project site has no value as habitat for rare, threatened, or endangered species.

Public Notice: A "Notification of Project Receiving Environmental Review" was mailed on November 5, 2020 to adjacent occupants and owners of buildings within 300 feet of the project site, the historic preservation group list, and neighborhood group lists.

STEP 7: MODIFICATION OF A CEQA EXEMPT PROJECT

TO BE COMPLETED BY PROJECT PLANNER

In accordance with Chapter 31 of the San Francisco Administrative Code, when a California Environmental Quality Act (CEQA) exempt project changes after the Approval Action and requires a subsequent approval, the Environmental Review Officer (or his or her designee) must determine whether the proposed change constitutes a substantial modification of that project. This checklist shall be used to determine whether the proposed changes to the approved project would constitute a "substantial modification" and, therefore, be subject to additional

MODIFIED PROJECT DESCRIPTION

Modified Project Description:

DETERMINATION IF PROJECT CONSTITUTES SUBSTANTIAL MODIFICATION

Compared to the approved project, would the modified project:

- | | |
|--------------------------|--|
| <input type="checkbox"/> | Result in expansion of the building envelope, as defined in the Planning Code; |
| <input type="checkbox"/> | Result in the change of use that would require public notice under Planning Code Sections 311 or 312; |
| <input type="checkbox"/> | Result in demolition as defined under Planning Code Section 317 or 19005(f)? |
| <input type="checkbox"/> | Is any information being presented that was not known and could not have been known at the time of the original determination, that shows the originally approved project may no longer qualify for the exemption? |

If at least one of the above boxes is checked, further environmental review is required

DETERMINATION OF NO SUBSTANTIAL MODIFICATION

- | | |
|--------------------------|---|
| <input type="checkbox"/> | The proposed modification would not result in any of the above changes. |
|--------------------------|---|

If this box is checked, the proposed modifications are exempt under CEQA, in accordance with prior project approval and no additional environmental review is required. This determination shall be posted on the Planning Department website and office and mailed to the applicant, City approving entities, and anyone requesting written notice. In accordance with Chapter 31, Sec 31.08j of the San Francisco Administrative Code, an appeal of this determination can

Planner Name:

Date:



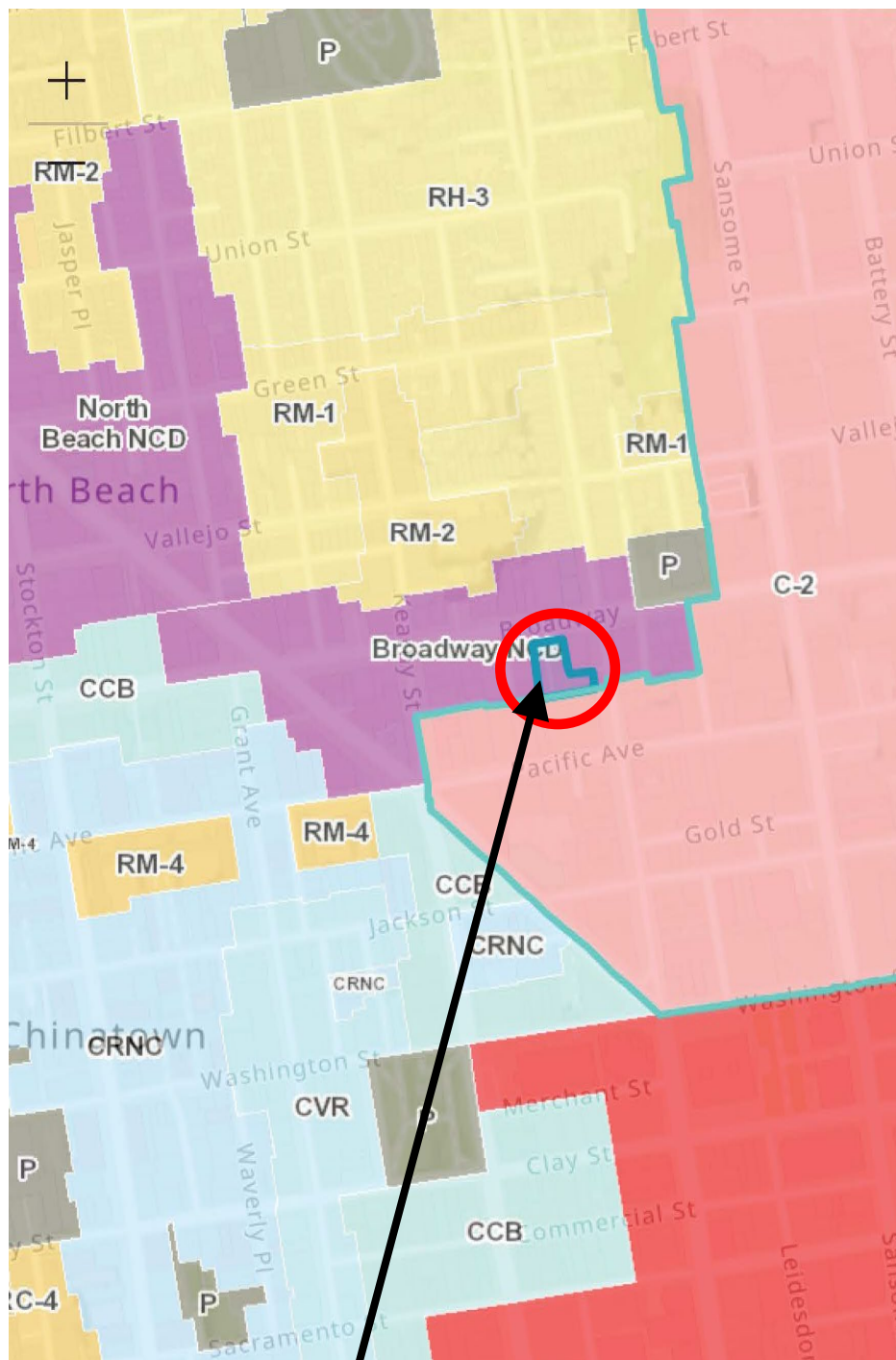
LAND USE INFORMATION

PROJECT ADDRESS: 425 BROADWAY

RECORD NO.: 2017-015678CUA

	EXISTING	PROPOSED	NET NEW
GROSS SQUARE FOOTAGE (GSF)			
Residential GSF	0	52,365	52,365
Retail/Commercial GSF	0	4,940	4,940
Office GSF	0	17,995	17,995
Usable Open Space	0	3,200	3,200
TOTAL GSF	0	78,500	78,500
	EXISTING	NET NEW	TOTALS
PROJECT FEATURES (Units or Amounts)			
Dwelling Units - Affordable	0	6	6
Dwelling Units - Market Rate	0	36	36
Dwelling Units - Total	0	42	42
Number of Buildings	0	2	2
Number of Stories	0	5 & 7	5 & 7
Parking Spaces	144	17	-12
Bicycle Spaces	0	54	534
	EXISTING	PROPOSED	NET NEW
LAND USE - RESIDENTIAL			
One Bedroom Units	0	16	16
Two Bedroom Units	0	21	21
Three Bedroom (or +) Units	0	5	5

Zoning Map

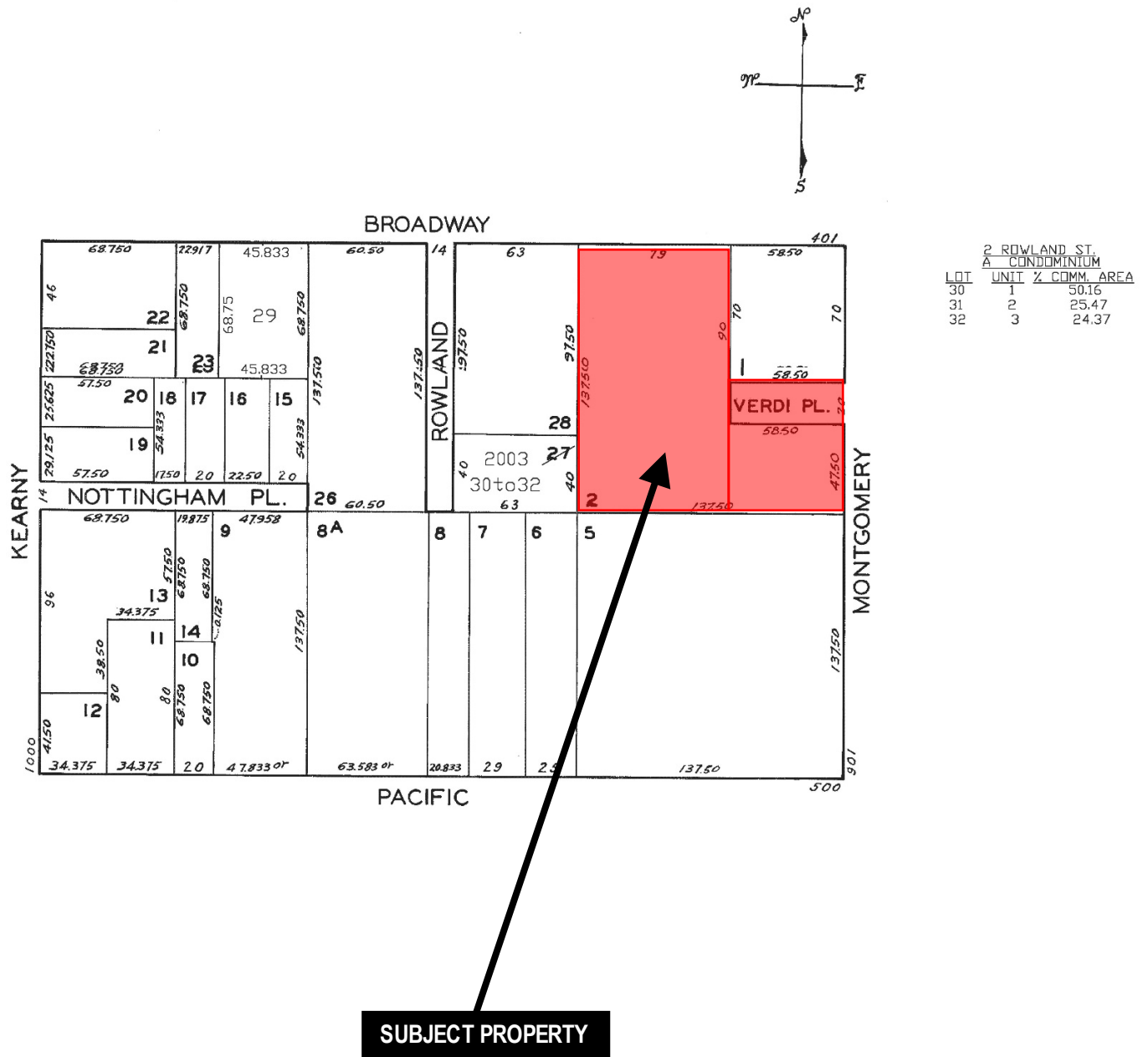


SUBJECT PROPERTY



Conditional Use Authorization Hearing
Case Number 2017-015678CUA
425 Broadway

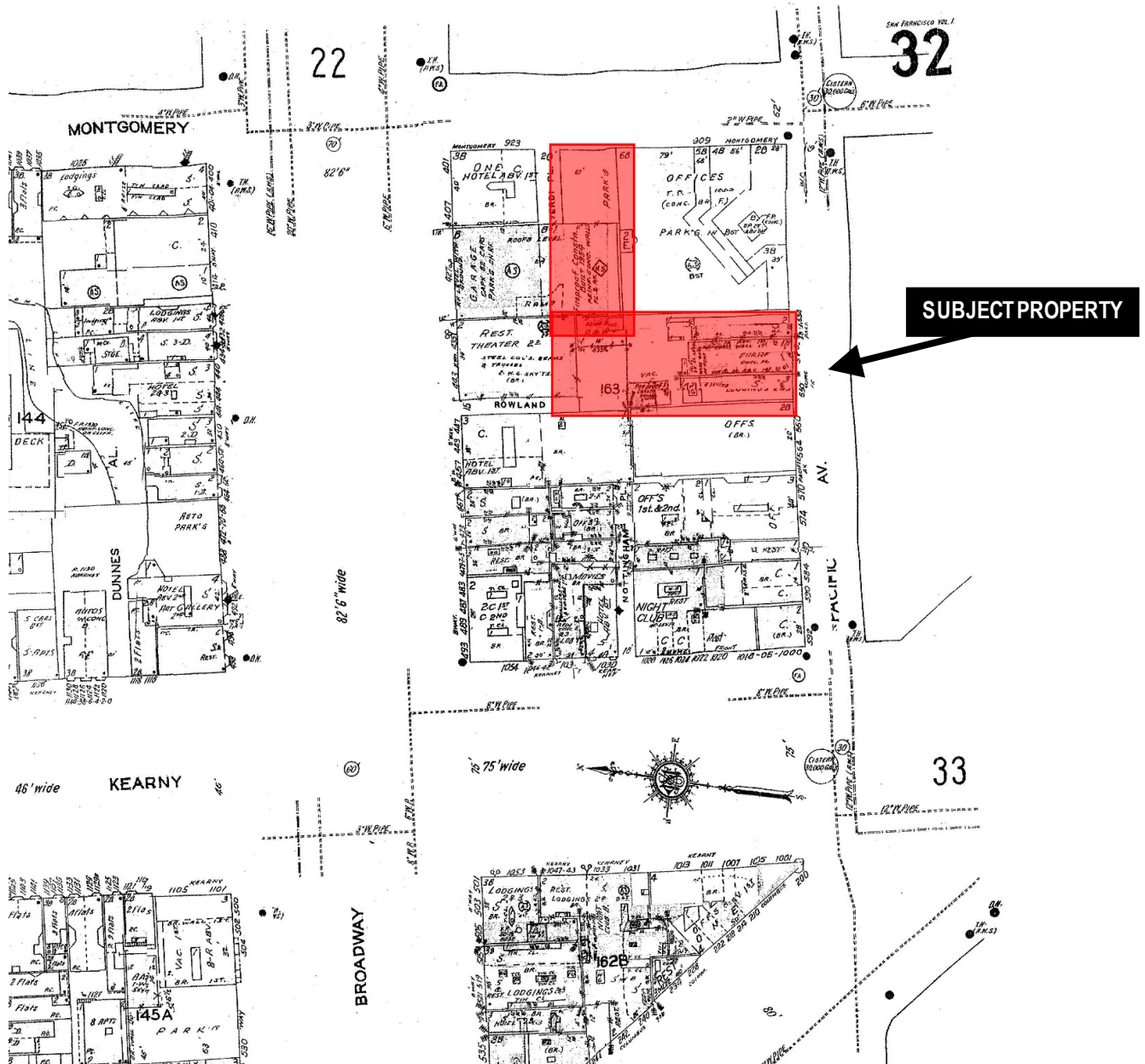
Parcel Map



Conditional Use Authorization Hearing
Case Number 2017-015678CUA
425 Broadway

Sanborn Map*

THESE SANBORN MAPS ARE DATED TO THE MID 1990's
USE ONLY FOR HISTORICAL CONTEXT

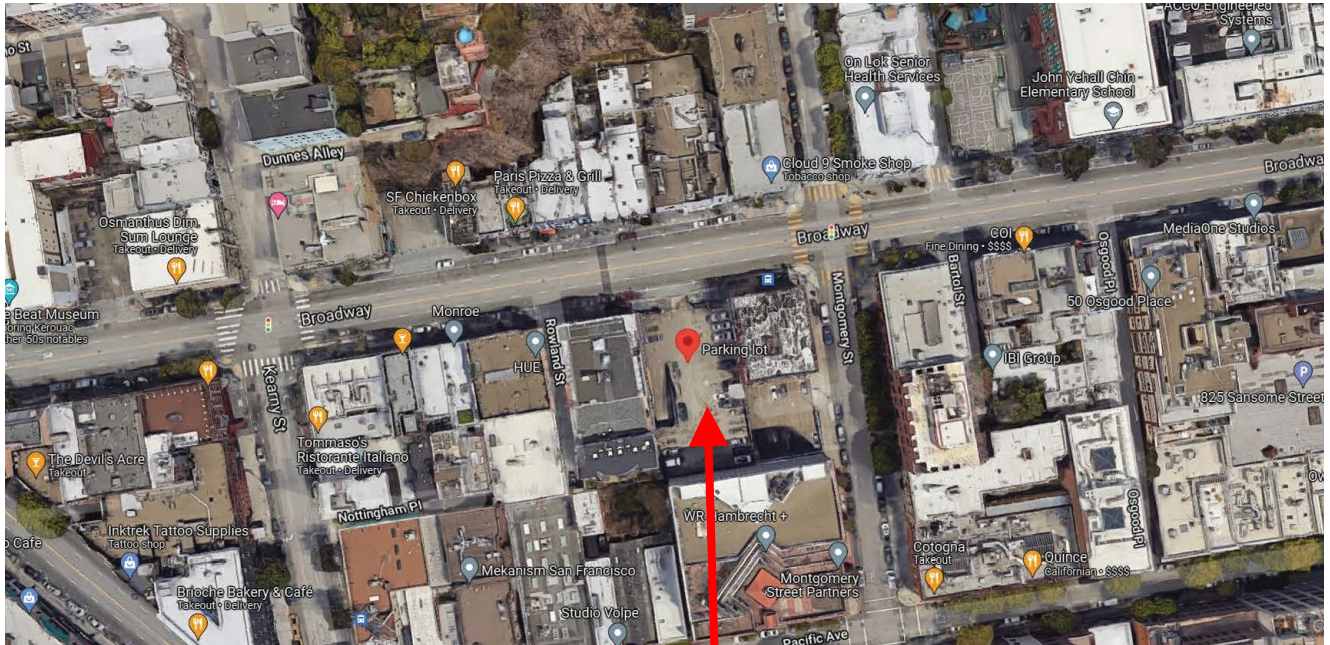


*The Sanborn Maps in San Francisco have not been updated since 1998, and this map may not accurately reflect existing conditions.

Conditional Use Authorization Hearing
Case Number 2017-015678CUA
425 Broadway



Aerial Photo

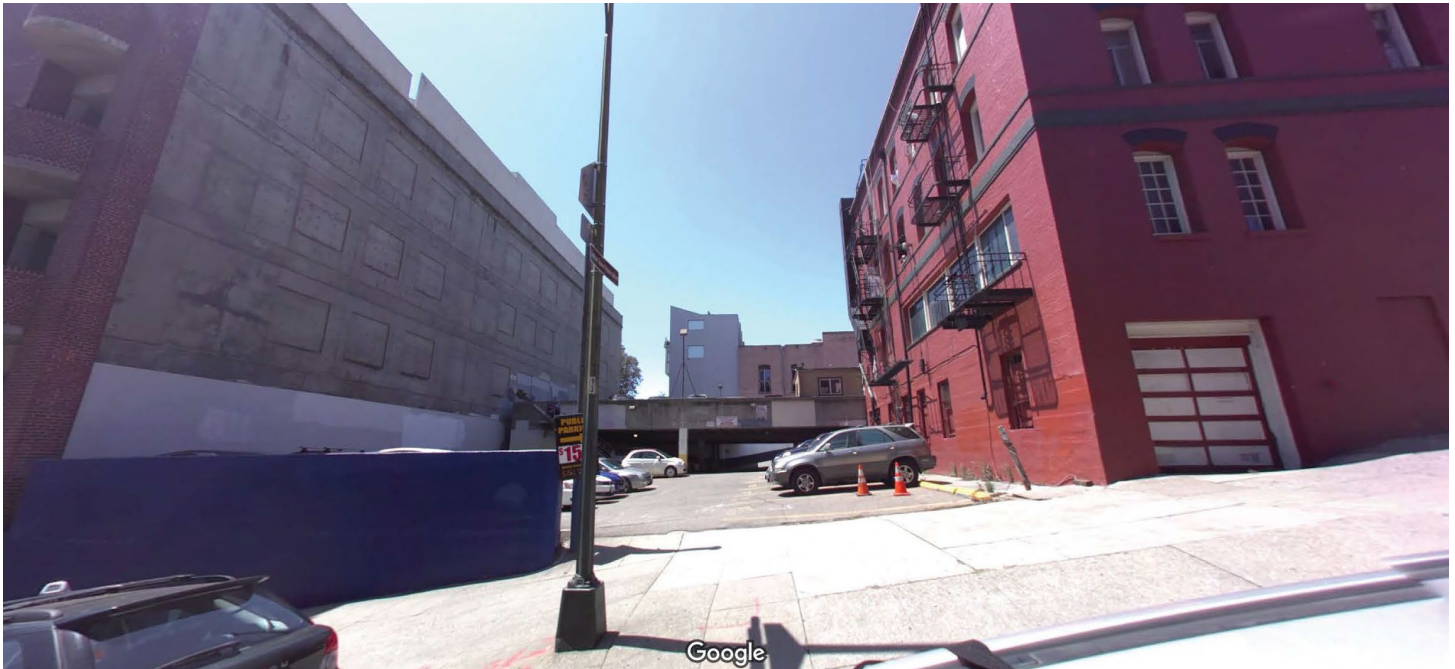


SUBJECT PROPERTY



Conditional Use Authorization Hearing
Case Number 2017-015678CUA
425 Broadway

Site Photos



Montgomery Frontage



Broadway Frontage

Conditional Use Authorization Hearing
Case Number 2017-015678CUA
425 Broadway

September 28, 2021

Hon. Joel Koppel, President
San Francisco Planning Commission
49 South Van Ness Avenue, Suite 1400
San Francisco, CA 94103

Re: 425 Broadway (Case No. 2017-015678CUA)
Conditional Use/State Density Bonus
Hearing Date: October 7, 2021

Dear President Koppel and Commissioners:

I am writing on behalf of Montgomery Place LLC, the project sponsor of the 41-unit 425 Broadway project (the "Project") within the Broadway Neighborhood Commission zoning district and 65-A-1 height and bulk district. The sponsor team has worked diligently and tirelessly with the Planning Department for the last 3-1/2 years to refine the project, resulting in an exceptional project that advance the City's housing goals, while preserving the integrity of the adjacent Jackson Square Historic District.

The Project proposes to replace a 144-space private parking garage with two mixed-use buildings, one facing Broadway and one facing Montgomery Street within the North Beach/Jackson Square neighborhood. The Project includes 41 for-sale dwelling units (1-, 2-, and 3-bedroom units), 4,940 square feet of ground floor retail space on Broadway and Montgomery Streets, five predominantly below grade design professional office suites (a retail use under the Planning Code), and 17 residential parking spaces. Neither building exceeds the applicable 65-foot height limit.

Conditional use approval by the Commission is sought because the project site, at 13,641 square feet, is somewhat larger than the 10,000 square foot conditional use threshold in the Broadway NCD, and three of the seven commercial spaces in the building are slightly above the 2,999 square foot use C/U size threshold. Certain density bonus waivers are also requested and described below.

Two-thirds of the project's square footage is residential; 9% is retail; and 25% is design professional office space. On Broadway, the dwelling units begin on the third floor to minimize potential conflicts with nearby places of entertainment; on Montgomery Street, away from the places of entertainment, the residential units are on the second floor and above. This mix of uses is fully consistent with and implements the policies underlying the Broadway NCD zoning district set forth in Planning Code Section 714:

The Broadway District controls are designed to encourage development that is compatible with the existing moderate building scale and mixed-use character, and maintain the district's balance of entertainment uses, restaurants, and small-scale retail stores. . . . Most commercial uses in new buildings are permitted at the first two stories. Neighborhood-serving businesses are strongly encouraged. . . . Nonretail offices are prohibited in order to prevent encroachment of the adjoining downtown office uses. . . . Housing development in new buildings is encouraged above the second story.

The project architect is Ian Birchall & Associates. Your Commission packet includes the full plan set, including renderings, floor plans, elevation, sections and materials.

Using the State Density Bonus Law, the Proposed Number of Dwelling Units was Increased from 34 Units to 41 Units. A maximum of 34 residential units and 34,102.5 square feet of commercial use is permitted on this site. At the urging of the Planning Department, the sponsor proposes using the State Density Bonus law to add 7 bonus units for a total 41 units, but keeping the commercial retail space (23,675 square feet) well below that principally permitted. No density bonus increase in height above the 65-foot height limit or any reduction in required open space is proposed. The Project will fulfill its inclusionary housing obligation through a combination of three on-site BMR units (10% of the base project) and payment of the Affordable Housing Fee.

Four waivers of development standards are requested through the State Density Bonus: building bulk, rear yard, unit exposure, and height. The need for the first three of these waivers is driven principally by the need to accommodate a public sewer easement that bisects the site, requiring two separate buildings organized around the 20-foot wide Verdi Alley open space, rather than a single building. Verdi Alley is a public street running from Montgomery Street 58.5 feet westerly, but is a private alley owned by the sponsor west of that point with the sewer easement below. No development standard concessions are sought.

The fourth waiver, for height, is associated with the 65-A-1 height and bulk district in which the Project is located. Within this district, the Planning Commission has discretion to not permit building height above 40 feet despite the 65-foot height limit, pursuant to Section 253.1 of the Planning Code. Because a building any shorter than 65 feet would not accommodate the 41 dwelling units, including the 7 bonus units, the Project is entitled to a waiver of this subjective Code provision.

The Project's Mix of Uses, Design and Noise Attenuation Features are Responsive to the Neighborhood Context. The Project site is at the border of the Jackson Square Historic District to the south, the Broadway entertainment district to the north and east, and Telegraph Hill further north. As described above, the Project's mix of uses is compatible with the several places of entertainment within 300 feet. There are no residential units below the third floor on Broadway, and only six bedrooms face Broadway on the upper three floors. Pursuant to guidance from the Entertainment Commission, a full acoustical report by Charles M. Salter & Associates was prepared to determine the STC ratings necessary in the Project's window and wall assemblies to

prevent disturbances to Project residents from nearby places of entertainment. The Project will incorporate those requirements.

At its September 21, 2021 meeting, the Entertainment Commission unanimously recommended approval of the Project to the Planning Commission, with the addition of certain conditions of approval consistent with the Salter study. We have agreed to those conditions and they are included in your draft motion.

The Project also includes small retail spaces on both the Broadway and Montgomery Street frontages, consistent with the Broadway NCD zoning and neighborhood norms. Finally, the five modestly-sized office suites in the Project will be limited to occupancy by design professionals, a use compatible with and supportive of the many other design businesses within Jackson Square.

With the guidance of the Department's preservation staff, the Project has been designed to be in conformity with the character defining features of the Jackson Square Historic District. The primary façade materials are masonry tile and stucco; the windows are deep set punched openings; top floor setbacks are incorporated into both the Broadway and Montgomery facades; and the more modern prismatic façade is limited to the interior of the site facing Verdi Alley. The Department's Historic Resource Evaluation Part 2 determined that the Project is compatible with and will have no adverse impacts on the historic district.

Finally, in consideration of downtown views from Telegraph Hill, the building height is limited to 56 feet on Broadway and 64 feet on Montgomery Street, comparable to the height of other buildings within Jackson Square.

The Project Does Not Cast Any Significant Shadow. The Planning Department determined that the Project will not cast any shadow on any Recreation and Park Department property. At the request of the Department, we also commissioned a shadow analysis to determine whether the Project would cast any shadow on the John Yehall Chin Elementary School property a block and one-half to the northeast. As shown in the analysis attached as Exhibit A, only a small occasional shadow cast late in the afternoon/early evening will affect primarily the school's trash area and one parking space.

The Project Sponsor Conducted Significant Community Outreach. The project team has undertaken significant outreach in the community, including with the North Beach Neighbors, Telegraph Hill Dwellers and nearby places of entertainment. Attached as Exhibit B is a log of those outreach efforts. As of today's date, the Project has received the endorsement of the Housing Action Coalition and YIMBY Action (copies attached), and we are aware of no formal opposition. No party opposed the Project at the September 21 Entertainment Commission hearing.

Pursuant to the Housing Accountability Act, the Project is Entitled to Your Approval. Under the Density Bonus Law, "[in] no case may a city, county, or city and county apply any development standard that will have the effect of physically precluding the construction of a

development meeting the criteria of [the Density Bonus Law] at the densities or with the concessions or incentives permitted by this section." (Cal. Gov't. Code § 65915(e)(1)). Accordingly, we submit that this State Density Bonus project is entitled to the requested conditional use approval, as well as our requested bulk, rear yard, unit exposure and height waivers.

Even without these provisions of state law, the Project would warrant the Commission's approval. It meets the objectives and policies of the General Plan, particularly the Housing Element, the Planning Code Section 206.6 density bonus requirements, the Urban Design Guidelines, and the Priority Policies of the Planning Code. The Project is well designed to be compatible with the Jackson Square Historic District and nearby places of entertainment. The Project will add 41 new units to the City's housing supply, including on-site inclusionary units, as well as an over \$2 million payment to the City's Affordable Housing fund, and will also pay significant transportation, child care and school impact fees.

We look forward to the October 7 hearing. Please contact me prior to the hearing if we can provide any additional information.

Very truly yours,



Steven L. Vettel

cc: Christy Alexander, planner
Montgomery Place LLC
Ian Birchall & Associates

EXHIBIT A

425 BROADWAY

FULL YEAR NET NEW SHADOW FAN DIAGRAM

FACTORIZING IN EXISTING SHADOWS DURING SECTION 295 TIMES

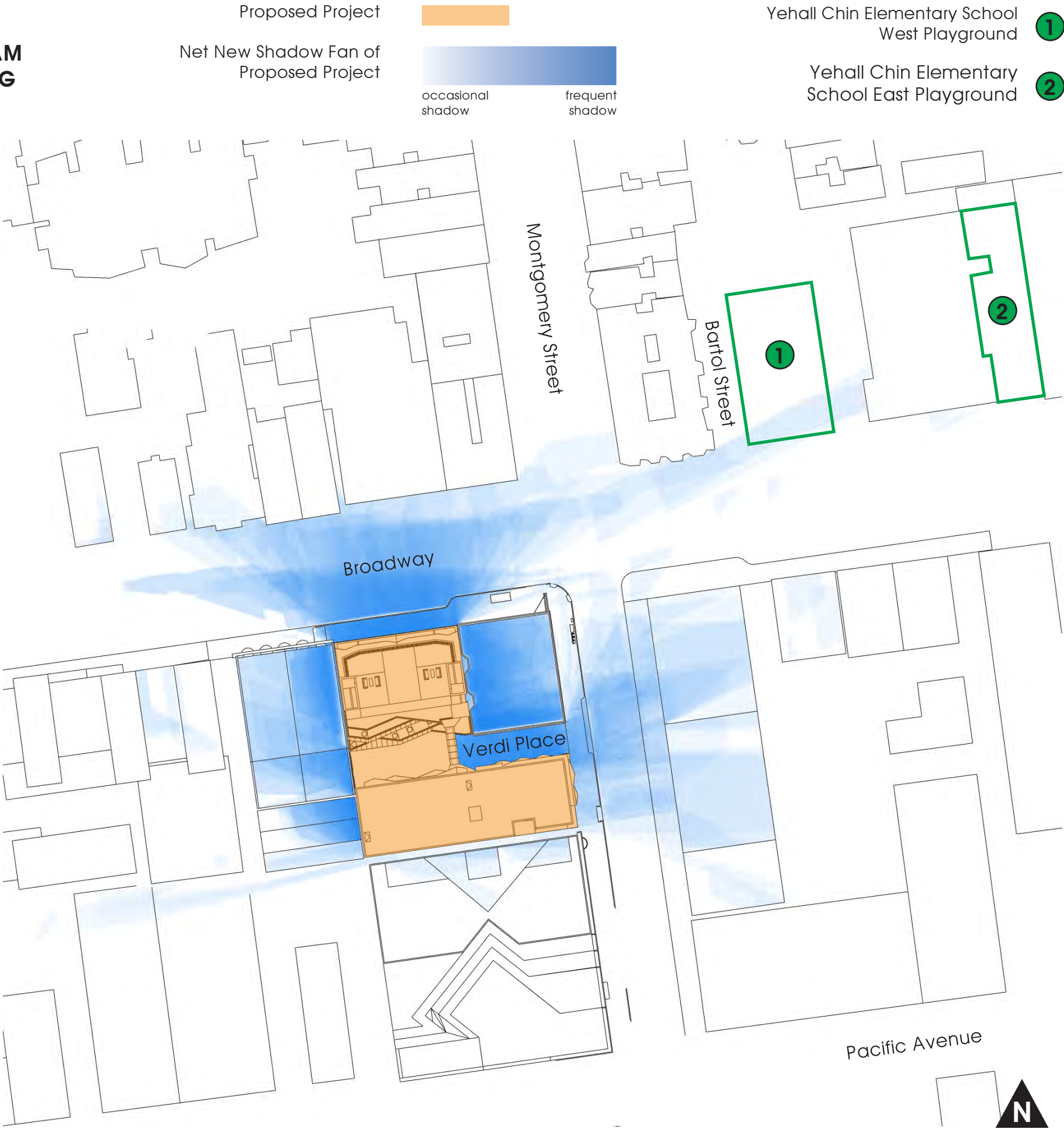


Yehall Chin Elementary West Playground

- Max Affected Area of Playground by Net New Shadow of Proposed Project - - -
- Handicap Parking Space 1
- Equipment Storage / Trash 2



Yehall Chin Elementary East Playground
Not Affected by Proposed Project



Yehall Chin Elementary School
West Playground 1

Yehall Chin Elementary School
East Playground 2

OWNER
Montgomery Place LLC
1630 Oakland Road #A215
San Jose, CA 95131
Richard Chen - Manager
jhrchen@gmail.com

MONTGOMERY PLACE
425 Broadway Street
San Francisco, CA 94133.
PROJECT
APN : 0163/002

SCALE : NTS

DRAWING TITLE : SHADOW FAN EXHIBIT

SHEET NO. : SH- 1.0

DATE 01/15/2020 ISSUE
REVISED DRAWINGS
RESPONSE TO PLAN
CHECK LETTER
DATED 08/14/2019



ib+a
architecture
ian birchall and associates
177 post street, suite 920
san francisco, ca 94108
t: 415.512.9660 f: 415.512.9663
www.ibadesign.com
project contact: Ian Birchall

EXHIBIT B

425 BROADWAY - Community Outreach Meeting Matrix:

last updated on 21_0928 by SH for IB+A

No.	Date of Meeting	Meeting/Call with:	Facebook	Tel.	Zoom	Emails	Website	Attendees	Contact:	Email	Tel	Notes / Main issues brought up	Meeting organizer	Pro	Neutral	Anti
GENERAL COMMUNITY OUTREACH																
TELEGRAPH HILL DWELLERS																
1.1	xx-Mar-18.	Telegraph Hill Dwellers, Planning & Zoning Committee				x	https://www.thd.org/about	Stan Hayes Ian Birchall	Stan Hayes, Chair, Planning & Zoning Committee	stanhayes1967@gmail.com		Email exchange and phone calls with IB.	IBA			
1.2	6-Apr-18	Telegraph Hill Dwellers, Planning & Zoning Committee	x					Stan Hayes Ian Birchall	Stan Hayes, Chair, Planning & Zoning Committee	stanhayes1967@gmail.com		IBA's project presentation to the Planning & Zoning Committee -Telegraph Hill Dwellers - 36 unit scheme with approx 26,300 sf of design professional office use.. 2 retail spaces along the streets and garage parking for 39 cars (18 resi and 21 commercial). - Non state density bonus project.	IBA /THD			
1.3	8-May-18	Telegraph Hill Dwellers, Planning & Zoning Committee				x		Stan Hayes	Stan Hayes, Chair, Planning & Zoning Committee	stanhayes1967@gmail.com		IBA rec'd letter expressing the consensus of the committee- from Stan Hayes (LETTER 1) Concerns expressed: - Height and scale - prposed project must be compatible with historic resources. - open space and parking should be eliminated. - greater housing affordability - verification of the right of way - shadowing of nearby elementary school.	THD			
1.4	22-Jan-19	Telegraph Hill Dwellers, Planning & Zoning Committee	x					Stan Hayes Ian Birchall	Stan Hayes, Chair, Planning & Zoning Committee	stanhayes1967@gmail.com		IBA's 2nd presentation to the Planning & Zoning Committee -Telegraph Hill Dwellers - 36 unit scheme Concerns expressed: - height scale size of the project - suggestions to trim office space - 100% affordable project considerations. - compatibility with the Jackson Square Historic District	IBA / THD			
1.5	21-Feb-19	Telegraph Hill Dwellers, Planning & Zoning Committee				x		email from Stan Hayes	Stan Hayes, Chair, Planning & Zoning Committee	stanhayes1967@gmail.com		IBA rec'd letter expressing the consensus of the committee- from Stan Hayes (LETTER 2) Concerns expressed: - height and mass are too great and must be in scale with surrounding buildings. - project must be compatible with historic resources - lost opportunity for on-site affordable housing	THD			

425 BROADWAY - Community Outreach Meeting Matrix:

last updated on 21_0928 by SH for IB+A

No.	Date of Meeting	Meeting/Call with:	Th-	Phone	Tel.	Zoom	Emails	Website	Attendees	Contact:	Email	Tel	Notes / Main issues brought up	Meeting organizer	Pro	Neutral	Anti
1.6	19-Nov-20	Telegraph Hill Dwellers, Planning & Zoning Committee					x		Stan Hayes Ian Birchall Christy Alexander	Stan Hayes, Chair, Planning & Zoning Committee	stanhayes1967@gmail.com		IBA rec'd letter re THD's comments on the environmental review of the project at 425 Broadway Street forwarded to IBA by the Planner Christy Alexander (LETTER 3) Concerns expressed: - building height - requirements of compatibility with the architectural and cultural character and features of the Jackson Square Historic District Extension and Broadway NCD. - building form - materials - windows - shadow impacts				
1.7	24-Jun-21	Telegraph Hill Dwellers, Planning & Zoning Committee and full Board					x		Ian Birchall, Vidhi Patel, Steve Saray, THD Planning + Zoning Committee (Stan Hayes, Stewart Morton, Nadya Williams, Al Fontes, Lance Carnes, Nancy Shanahan, Nick Ferris, Mary Lipian, Greg Giachino, CJ Carol Verburg, Constance Deng, Michael	Stan Hayes, Chair, Planning & Zoning Committee	stanhayes1967@gmail.com		IBA's 3rd presentation to the Planning & Zoning Committee -Telegraph Hill Dwellers - 41 unit scheme Concerns expressed: - height scale size of the project - concern over quantity of office space + use - quantity of on-site affordable units and in-lieu fees - brick compatible with historic distric - questions regarding lighter material - suggested terracotta - questions regarding response to THD letter to Planning - questions regarding viability of ground-floor retail				
NORTH BEACH NEIGHBORS																	
2.1	19-Nov-18	North Beach Neighbors		x				http://northbeachneighbors.org/		Bruno Kanter V.P., North Beach Neighbors	brunokanter@gmail.com	415-921-5456	IBA project Presentation to the P&Z committee Bruno's response: <i>"Thanks for coming out last night and including us in your process. I'll let you know if the group has any questions. Looking forward to see the project develop."</i>	IBA			

425 BROADWAY - Community Outreach Meeting Matrix:

last updated on 21_0928 by SH for IB+A

No.	Date of Meeting	Meeting/Call with:	M- person	Tel.	Zoom	Emails	Website	Attendees	Contact:	Email	Tel	Notes / Main issues brought up	Meeting organizer	Pro	Neutral	Anti
2.2	10-Dec-18	North Beach Neighbors	x					Steve Saray Ian Birchall Vidhi Patel <u>North Beach Neighbors</u> (approx. 50+ attendees at the meeting) Bruno Kanter Danny Sauter Trish Herman Susan Pate Gino Bellessa + Tam Koster Findre Chinoise Emanuele Sartori Rod Freebairn – Smith	Bruno Kanter V.P., North Beach Neighbors	brunokanter@gmail.com	415-921-5456	IBA's project presentation at the North Beach Neighbors' neighborhood meeting. Comments and key questions: - has any community space been proposed? - considered bay windows? - any concerns re. filling office space. - will the community have a chance to comment on the number of affordable units proposed at this project? - very nice project. How can we support the project?	Bruno Kanter for NBN			
2.3	28-Jan-19	North Beach Neighbors				x			Bruno Kanter V.P., North Beach Neighbors	brunokanter@gmail.com	415-921-5456	IBA requested the North Beach Neighbors to consider providing us a letter in support of this project. Response: " <i>Happy to get a letter together. Were there any changes requested by Planning or anything different than what you presented to us? Are there final drawings after the dust settled so to speak?</i> "	IBA			
2.4	23-Feb-21	North Beach Neighbors				x			Danny Sauter North Beach neighbors - President	sauterdj@gmail.com	415-562-6441	Danny enquired if IBA could share updated plans and if we could present at their Planning & Zoning committee in March. IBA response - " <i>We have just finished the latest responses to City Planning. I will discuss your request with the project sponsors and get back to you.</i> "				

425 BROADWAY - Community Outreach Meeting Matrix:

last updated on 21_0928 by SH for IB+A

No.	Date of Meeting	Meeting/Call with:	Info	Person	Tel.	Zoom	Emails	Website	Attendees	Contact:	Email	Tel	Notes / Main issues brought up	Meeting organizer	Pro	Neutral	Anti
2.5	16-Jun-21	North Beach Neighbors				x			North Beach Neighbors - Full Board and Planning and Zoning Committee	Danny Sauter North Beach neighbors - President	sauterdj@gmail.com	415-562-6441	IBA's project presentation at the North Beach Neighbors' neighborhood meeting. Comments and key questions: - architecturally very attractive; huge improvements from prior design - retail space - how large is it, can it be sub-divided - any concerns re. filling office space, especially due to pandemic-related vacancies - how the SDBL is calculated - like the overall mass - thinks it fits nicely with th context - like the overall mass - thinks it fits nicely with th context				
PRE APPLICATION MEETING																	
3.1	18-Dec-18	Pre App meeting				x				Steve Saray Steve Vettel Ian Birchall Vidhi Patel Dominic Li Mandri Andrew Diamond Gamze Basar			IBA's project presentation at the Pre-App meeting Comments and key questions: - did the sponsor consider a State Density Bonus project? - would you be willing to present to the Top Of Broadway CBD - Land Use Committee.? - TOB CBD should be able to provide a letter of support after we have had a chance to review the project with our committee. - possible outreach to the North Beach Merchant's Association and the NBBA - North Beach Business Association.	IBA			
SFHAC (San Francisco Housing Action Committee)																	
4.1	25-May-21	SFHAC				x		https://www.sfhac.org/project-review-committee/	Ian Birchall, Steve Saray, Vidhi Patel, Sasha Heuer, Jake Price, Corey Smith, Kayle Barnes	Jacob Price - Development + Operations Associate (Membership, Fundraising, Communications, Project Review + Tours)	jacob@sfhac.org		IBA gave brief description of the project. SFHAC outlined general review and advocacy process, + membership options	IBA			

425 BROADWAY - Community Outreach Meeting Matrix:

last updated on 21_0928 by SH for IB+A

No.	Date of Meeting	Meeting/Call with:	Person	Tel.	Zoom	Emails	Website	Attendees	Contact:	Email	Tel	Notes / Main issues brought up	Meeting organizer	Pro	Neutral	Anti
4.2	7-Jul-21	SFHAC			x		https://www.sfhac.org/project-review-committee/	Project Review Committee	Jacob Price - Development + Operations Associate (Membership, Fundraising, Communications, Project Review + Tours)	jacob@sfhac.org						
4.3	1-Sep-21	SFHAC		x			https://www.sfhac.org/project-review-committee/	Ian Birchall, Steve Saray, Vidhi Patel, Steve Vettel, Corey Smith,	Corey Smith	corey@sfhac.org		Meeting to discuss SFHAC advocacy options and strategy				
YIMBY Action																
5.1	16-Sep-21	YIMBY		x			yimbyaction.org	Ian Birchall, Vidhi Patel, Gillian Pressman	Gillian Pressman	gillian@yimbyaction.org		IBA gave brief description of the project. YIMBY outlined general review and advocacy process	IBA			

425 BROADWAY - Community Outreach Meeting Matrix:

last updated on 21_0928 by SH for IB+A

No.	Date of Meeting	Meeting/Call with:	Th-	Phone	Tel.	Zoom	Emails	Website	Attendees	Contact:	Email	Tel	Notes / Main issues brought up	Meeting organizer	Pro	Neutral	Anti
PLACES OF ENTERTAINMENT (POE) OUTREACH																	
Broadway Studios + FAME Venue																	
6.1	10-Jul-19	Broadway Studios + FAME Venue (Events venue located at 435 Broadway, San Francisco, CA 94133 (next door on Broadway))	x					https://www.broadwaystudios.com/	Ian Birchall Vidhi Patel Francesca Karl B. Pleskot Jake M. Schpero (CM Salter)	Francesca Karl B. Pleskot	eventsvenue@gmail.com info@BroadwayStudios.com	415-272-2743 415-559-5010	10_0710 - Meeting with Francesca + Karl Pleskot at 10 am. IBA distributed meeting notes on 19_0711	IBA			
6.2	9/16/21 @ 11am	Broadway Studios + FAME Venue (Events venue located at 435 Broadway, San Francisco, CA 94133 (next door on Broadway))				x		https://www.broadwaystudios.com/	Ian Birchall Sasha Heuer Francesca Karl B. Pleskot Steve Saray Steve Vettel	Francesca Karl B. Pleskot	eventsvenue@gmail.com info@BroadwayStudios.com	415-272-2743 415-559-5010	IB+A presented updates to the project. QUESTIONS + DISCUSSION ITEMS: - FAME windows along Verdi Alley - will they be blocked? - No, those will be open to Verdi Alley - Acoustical issues - an acoustical consultant has been retained to provide the STC ratings for windows and walls to prevent noise. Buyers will be required to sign off on disclosures that they are along an entertainment corridor. Fresh air will be ducted into each unit so that residents don't need to rely on natural air ventilation from windows - price units will be sold for - unknown at this time since that process is 2+ years away; will be sold at market rates; 3 BMR units onsite - sequence of construction - both building will be build at the same time - timeline - looking at 2+ years for permitting and construction - Verdi Alley name staying the same? - yes, it will be retained - POE Outreach process, have others been contacted - yes, we have reached out to all POE's within a 300ft radius. We met with Monroe and they are supportive. Vanity declined a meeting. The others never responded to our requests. COMMENTS: - building looks great - would be a good addition to neighborhood - will send through any additional neighborhood contacts that we think would be good for you to reach out to	IBA			

425 BROADWAY - Community Outreach Meeting Matrix:

last updated on 21_0928 by SH for IB+A

No.	Date of Meeting	Meeting/Call with:	Info	perso	Tel.	Zoom	Emails	Website	Attendees	Contact:	Email	Tel	Notes / Main issues brought up	Meeting organizer	Pro	Neutral	Anti
The Penthouse Club																	
7	Only email correspondence. See Contact Log for more information. NO MEETING TO DATE	The Penthouse Club (Adult entertainment club / restaurant located at 412 Broadway, San Francisco, CA 94133 (building across the street -					x										
Hue																	
8	Only email correspondence. See Contact Log for more information. NO MEETING TO DATE	Hue (Nightclub & lounge located at 447 Broadway, San Francisco, CA 94133)					x			Bennett Montoya	bennettmontoya@gmail.com		19_0717 - GoToMeeting set up for presenting the project to Bennett. (Bennett cancelled the Wed, Jul 17, 2019 video conference at the last minute.)				
Top Of Broadway - CBD (NO LONGER EXISTS)																	
9	Only email correspondence. See Contact Log for more information. NO MEETING TO DATE	Top Of Broadway - CBD					x			Dominic LiMandri District Director Top of Broadway CBD	dominic@newcityamerica.com	619-241-1900					
Monroe SF																	
10	8/18/2021 @ 2pm	Monroe SF (Nightclub located at 473 Broadway, San Francisco, CA 94133)				x		https://www.monroesf.com/	Steve Saray, Steven Vettel, Ian Birchall, Vidhi Patel, Sasha Heuer, Payam Marvin	Payam Arvin	monroeloungeinfo@gmail.com	415-323-8515	IB+A presented updates to the project. Payam said the project looks great and looks forward to something being built. Offered to provide letter of support. Payam's only question was whether or not the proposal included the Crowbar building and if we know whether or not that building was sold. We told him "No, it is not included and we don't know the status of that building"	IBA			

425 BROADWAY - Community Outreach Meeting Matrix:

last updated on 21_0928 by SH for IB+A

No.	Date of Meeting	Meeting/Call with:	Info person	Tel.	Zoom	Emails	Website	Attendees	Contact:	Email	Tel	Notes / Main issues brought up	Meeting organizer	Pro	Neutral	Anti
Horizon																
11	Only phone/email correspondence. See Contact Log for more information. NO MEETING TO DATE	Horizon (Horizon – Restaurant and Lounge located at 498 Broadway, San Francisco, CA 94133)		x		x			Shon Stubbee		925-817-9461 - if any issues contacting Shon, please try Ronnie Lau at 415-509-6770					
Cosmo Bar and Lounge																
12	Only email correspondence. See Contact Log for more information. NO MEETING TO DATE	Cosmo Bar (The Cosmo Bar and Lounge located at 440 Broadway, San Francisco, CA 94133)				x			Calvin Louie	cylouiecpa@aol.com						
Bamboo Hut																
13	Only email correspondence. See Contact Log for more information. NO MEETING TO DATE	Bamboo Hut (Tiki Dive Bar located at 479 Broadway, San Francisco, CA 94133)				x	https://www.bamboohutsf.com/		Max Young	max@maximumproductions.com	415-722-6299					



**HOUSING
ACTION
COALITION**

PROJECT REVIEW REPORT CARD

Project Address: 425 Broadway
Project Sponsor: Montgomery Place, LLC
Date of HAC Review: July 7th, 2021

Grading Scale

- ★ = The project meets the high standard set by local jurisdiction and/or HAC
- ★★ = The project exceeds HAC standards
- ★★★ = The project far exceeds HAC's standards and exhibits creativity in its proposed solutions

Criteria for HAC Endorsement

1. The development must have been presented to the HAC Project Review Committee
2. The Project must score a minimum of ★ on any given guideline

<i>Guideline</i>	<i>Comments</i>	<i>Score</i>
Summary	HAC is pleased to endorse 425 Broadway, as the project will bring 43 homes to a currently underutilized site in a neighborhood that has not built much housing in recent decades. The project utilizes the state density bonus and employs a strong urban design, ultimately helping to alleviate San Francisco's housing shortage.	★★
Land Use	This building converts a surface level parking lot into a mixed-use, multi-family project that includes office, retail, and residential space. The committee commends the project's location, as the area is well-served by public transit and has built little housing in previous decades. The committee also supports the team's decision to include office space instead of parking.	★★
Density	The project will consist of 43 for-sale homes. The committee commends the project team for increasing the density by 20% through the State Density Bonus. While we would like to see the project utilize the entirety of the 50% bonus, we understand the need to be responsive to neighbors.	★★
Affordability	This project includes 3 on-site affordable homes and an in-lieu fee of approximately \$2.5 million. By combining on-site with in-lieu fees, this project brings more affordable homes to one of the most affluent parts of the city.	★
Parking & Alternative Transportation	The project is in a transit-rich area and includes 17 parking spaces, as well as 53 total bike spaces. We are exceptionally happy with the project team exceeding a 1:1 ratio for bike parking, and would also encourage the team to explore adding bike parking at grade. We are likewise pleased with the small number of car parking spaces (all of which are unbundled), and encourage the implementation of additional TDM strategies to become GreenTRIP certified.	★
Preservation		n/a

Urban Design	There are light wells in the basement office spaces to provide natural light, as well as usable open space for 8 of the 41 units. Building amenities include a roof deck on the Montgomery building as well as landscaped space between the two buildings. The Committee appreciates the façades on Verdi alley, as well as the activation of the alley. Upper floor levels of the buildings facing Broadway and Montgomery have been set back and improved in response to feedback from the City. We would like to see the project team plan for possible ADU conversions for the retail on Montgomery.	★★
Environmental Features	This building meets the City's requirements. The project team is aiming for LEED Gold equivalency, with a design that is all-electric, includes PV array units, VRF HVAC, and employs a variety of water conservation measures.	★
Community Benefits	The Committee commends the project for the positive impact it will have on the alley and the improvements to the sidewalk/pedestrian experience. The Committee also believes adding housing and design office space is a community benefit, and commends the building's design for blending in with the area's general aesthetic. We encourage the project team to continue engaging with union labor as they are seeking a general contractor.	★★
Community Input	The team has received letters with feedback from the Telegraph Hill Dwellers and North Beach Neighbors. In response to feedback from the City, the project team changed the proposed rainscreen cladding to brick tile.	★



YIMBY Action advocates for welcoming communities where everyone can thrive.
yimbyaction.org

San Francisco Planning Commission

City Hall

1 Dr. Carlton B. Goodlett Place

San Francisco, CA 94102

September 17, 2021

RE: Support for 41 new homes at 425 Broadway

Dear Planning Commission:

YIMBY Action is pleased to support the proposed project at 425 Broadway. This project would bring 41 new homes to San Francisco, helping to address our staggering citywide housing shortage. It would also allow more people to access the rich cultural and commercial opportunities of the Jackson Square neighborhood, and bring more customers to local businesses. The project's conversion of a parking garage into mixed-use development also helps create a more environmentally friendly and healthier San Francisco.

YIMBY Action is a network of pro-housing activists fighting for more inclusive housing policies. Our vision is an integrated and environmentally sustainable society where every person has access to a safe, affordable home near jobs, services, and opportunity.

San Francisco's severe housing shortage is causing skyrocketing homelessness and poverty, crippling our economy, and leading to debilitating commutes that

exacerbate our global climate crisis. These impacts fall disproportionately on our city's low-income workers and families, and disproportionately on people of color. The segregationist practice of denying housing in our highest-opportunity neighborhoods additionally marginalizes communities of color. If we strive to be a society that advances racial and class justice, we must do more to ensure abundant housing in all of our city's neighborhoods. This project will help address the housing shortage and ensure a welcoming San Francisco where everyone can thrive.

Best regards,

Laura Foote

YIMBY Action, Executive Director



YIMBY Action advocates for welcoming communities where everyone can thrive.
yimbyaction.org

AFFIDAVIT

COMPLIANCE WITH THE INCLUSIONARY AFFORDABLE HOUSING PROGRAM

PLANNING CODE SECTION 415, 417 & 419



**San Francisco
Planning**

SAN FRANCISCO PLANNING DEPARTMENT
1650 MISSION STREET, SUITE 400
SAN FRANCISCO, CA 94103-2479
MAIN: (415) 558-6378 SFPLANNING.ORG

Date

I, _____,
do hereby declare as follows:

A The subject property is located at (address and block/lot):

Address

Block / Lot

The subject property is located within the following Zoning District:

Zoning District

Height and Bulk District

Special Use District, if applicable

Is the subject property located in the SOMA NCT, North of Market Residential SUD, or Mission Area Plan?

☐ Yes ☐ No

B The proposed project at the above address is subject to the *Inclusionary Affordable Housing Program*, Planning Code Section 415 and 419 et seq.

The Planning Case Number and/or Building Permit Number is:

Planning Case Number

Building Permit Number

This project requires the following approval:

- ☐ Planning Commission approval (e.g. Conditional Use Authorization, Large Project Authorization)
- ☐ Zoning Administrator approval (e.g. Variance)
- ☐ This project is principally permitted.

The Current Planner assigned to my project within the Planning Department is:

Planner Name

A complete Environmental Evaluation Application or Project Application was accepted on:

Date

The project contains _____ total dwelling units and/or group housing rooms.

This project is exempt from the *Inclusionary Affordable Housing Program* because:

- ☐ This project is 100% affordable.
- ☐ This project is 100% student housing.

Is this project in an UMU Zoning District within the Eastern Neighborhoods Plan Area?

☐ Yes _____ ☐ No

(If yes, please indicate Affordable Housing Tier)

Is this project a HOME-SF Project?

☐ Yes _____ ☐ No

(If yes, please indicate HOME-SF Tier)

Is this project an Analyzed or Individually Requested State Density Bonus Project?

☐ Yes ☐ No

C Please indicate the tenure of the project.

- ☐ **Ownership.** If affordable housing units are provided on-site or off-site, all affordable units will be sold as ownership units and will remain as ownership units for the life of the project. The applicable fee rate is the ownership fee rate.
- ☐ **Rental.** If affordable housing units are provided on-site or off-site, all affordable units will be rental units and will remain rental units for the life of the project. The applicable fee rate is the rental fee rate.

D This project will comply with the Inclusionary Affordable Housing Program by:

- ☐ Payment of the Affordable Housing Fee prior to the first construction document issuance (Planning Code Section 415.5)
- ☐ On-site Affordable Housing Alternative (Planning Code Sections 415.6)
- ☐ Off-site Affordable Housing Alternative (Planning Code Sections 415.7)
- ☐ Combination of payment of the Affordable Housing Fee and the construction of on-site or off-site units (Planning Code Section 415.5 - required for Individually Requested State Density Bonus Projects)
- ☐ Eastern Neighborhoods Alternate Affordable Housing Fee (Planning Code Section 417)
- ☐ Land Dedication (Planning Code Section 419)

The applicable inclusionary rate is:

On-site, off-site or fee rate as a percentage

If the method of compliance is the payment of the Affordable Housing Fee pursuant to Planning Code Section 415.5, please indicate the total residential gross floor area in the project.

Residential Gross Floor Area

E The Project Sponsor acknowledges that any change which results in the reduction of the number of on-site affordable units following the project approval shall require public notice for a hearing and approval by the Planning Commission.

F The Project Sponsor acknowledges that failure to sell or rent the affordable units or to eliminate the on-site or off-site affordable units at any time will require the Project Sponsor to:

- (1) Inform the Planning Department and the Mayor's Office of Housing and Community Development and, if applicable, fill out a new affidavit;
- (2) Record a new Notice of Special Restrictions; and
- (3) Pay the Affordable Housing Fee plus applicable interest (using the fee schedule in place at the time that the units are converted from ownership to rental units) and any applicable penalties by law.

G The Project Sponsor acknowledges that in the event that one or more rental units in the principal project become ownership units, the Project Sponsor shall notify the Planning Department of the conversion, and shall either reimburse the City the proportional amount of the Inclusionary Affordable Housing Fee equivalent to the then-current requirement for ownership units, or provide additional on-site or off-site affordable units equivalent to the then-current requirements for ownership units.

I For projects with over 25 units and with EEA's accepted between January 1, 2013 and January 12 2016, in the event that the Project Sponsor does not procure a building or site permit for construction of the principal project before December 7, 2018, rental projects will be subject to the on-site rate in effect for the Zoning District in 2017, generally 18% or 20%.

J For projects with EEA's/PRJ's accepted on or after January 12 2016, in the event that the Project Sponsor does not procure a building or site permit for construction of the principal project within 30 months of the Project's approval, the Project shall comply with the Inclusionary Affordable Housing Requirements applicable thereafter at the time the Sponsor is issued a site or building permit.

K If a Project Sponsor elects to completely or partially satisfy their Inclusionary Housing requirement by paying the Affordable Housing Fee, the Sponsor must pay the fee in full sum to the Development Fee Collection Unit at the Department of Building Inspection for use by the Mayor's Office of Housing prior to the issuance of the first construction document.

UNIT MIX TABLES

Number of All Units in PRINCIPAL PROJECT:

TOTAL UNITS:	SRO / Group Housing:	Studios:	One-Bedroom Units:	Two-Bedroom Units:	Three (or more) Bedroom Units:
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If you selected the On-site, Off-Site, or Combination Alternative, please fill out the applicable section below. The On-Site Affordable Housing Alternative is required for HOME-SF Projects pursuant to Planning Code Section 206.4. State Density Bonus Projects that have submitted an Environmental Evaluation Application prior to January 12, 2016 must select the On-Site Affordable Housing Alternative. State Density Bonus Projects that have submitted an Environmental Evaluation Application on or after to January 12, 2016 must select the Combination Affordable Housing Alternative to record the required fee on the density bonus pursuant to Planning Code Section 415.3. If the Project includes the demolition, conversion, or removal of any qualifying affordable units, please complete the Affordable Unit Replacement Section.

☐ **On-site Affordable Housing Alternative** (Planning Code Section 415.6, 419.3, or 206.4): % of the unit total.

Number of Affordable Units to be Located ON-SITE:

TOTAL UNITS:	SRO / Group Housing:	Studios:	One-Bedroom Units:	Two-Bedroom Units:	Three (or more) Bedroom Units:
--------------	----------------------	----------	--------------------	--------------------	--------------------------------

LOW-INCOME	Number of Affordable Units	% of Total Units	AMI Level
MODERATE-INCOME	Number of Affordable Units	% of Total Units	AMI Level
MIDDLE-INCOME	Number of Affordable Units	% of Total Units	AMI Level

☐ **Off-site Affordable Housing Alternative** (Planning Code Section 415.7 or 419.3): % of the unit total.

Number of Affordable Units to be Located OFF-SITE:

TOTAL UNITS:	SRO / Group Housing:	Studios:	One-Bedroom Units:	Two-Bedroom Units:	Three (or more) Bedroom Units:
Area of Dwellings in Principal Project (in sq. feet):		Off-Site Project Address:			
Area of Dwellings in Off-Site Project (in sq. feet):					
Off-Site Block/Lot(s):		Motion No. for Off-Site Project (if applicable):		Number of Market-Rate Units in the Off-site Project:	

AMI LEVELS:	Number of Affordable Units	% of Total Units	AMI Level
	Number of Affordable Units	% of Total Units	AMI Level
	Number of Affordable Units	% of Total Units	AMI Level

UNIT MIX TABLES: CONTINUED

- ☐ **Combination** of payment of a **fee, on-site affordable units, or off-site affordable units** with the following distribution:
Indicate what percent of each option will be implemented (from 0% to 99%) and the number of on-site and/or off-site below market rate units for rent and/or for sale.

1. On-Site % of affordable housing requirement.

If the project is a State Density Bonus Project, please enter "100%" for the on-site requirement field and complete the Density Bonus section below.

Number of Affordable Units to be Located ON-SITE:

TOTAL UNITS:	SRO / Group Housing:	Studios:	One-Bedroom Units:	Two-Bedroom Units:	Three (or more) Bedroom Units:

2. Off-Site % of affordable housing requirement.

Number of Affordable Units to be Located OFF-SITE:

TOTAL UNITS:	SRO / Group Housing:	Studios:	One-Bedroom Units:	Two-Bedroom Units:	Three (or more) Bedroom Units:
Area of Dwellings in Principal Project (in sq. feet):	Off-Site Project Address:				
Area of Dwellings in Off-Site Project (in sq. feet):					
Off-Site Block/Lot(s):	Motion No. for Off-Site Project (if applicable):		Number of Market-Rate Units in the Off-site Project:		

Income Levels for On-Site or Off-Site Units in Combination Projects:

AMI LEVELS:	Number of Affordable Units	% of Total Units	AMI Level

3. Fee % of affordable housing requirement.

Is this Project a State Density Bonus Project? ☐ Yes ☐ No

If yes, please indicate the bonus percentage, up to 35% _____, and the number of bonus units and the bonus amount of residential gross floor area (if applicable) _____

I acknowledge that Planning Code Section 415.4 requires that the Inclusionary Fee be charged on the bonus units or the bonus residential floor area.

Affordable Unit Replacement: Existing Number of Affordable Units to be Demolished, Converted, or Removed for the Project

TOTAL UNITS:	SRO / Group Housing:	Studios:	One-Bedroom Units:	Two-Bedroom Units:	Three (or more) Bedroom Units:

This project will replace the affordable units to be demolished, converted, or removed using the following method:

- ☐ On-site Affordable Housing Alternative
- ☐ Payment of the Affordable Housing Fee prior to the first construction document issuance
- ☐ Off-site Affordable Housing Alternative (Section 415.7)
- ☐ Combination of payment of the Affordable Housing Fee and the construction of on-site or off-site units (Section 415.5)

Contact Information and Declaration of Sponsor of PRINCIPAL PROJECT

Ian Birchall and Associates

Company Name

Ian Birchall

Name (Print) of Contact Person

177 Post Street, Suite 920

Address

San Francisco, CA 94108

City, State, Zip

(415) 512-9660 x1

Phone / Fax

ian@ibadesign.com

Email

I am a duly authorized agent or owner of the subject property. I declare under penalty of perjury under the laws of the State of California that the foregoing is true and correct. I hereby declare that the information herein is accurate to the best of my knowledge and that I intend to satisfy the requirements of Planning Code Section 415 as indicated above.

Sign Here

Signature:



Name (Print), Title:

Ian Birchall, Senior Principal/Owner

Executed on this day in:

Location:

177 Post St, Suite 920 San Francisco, CA 94108

Date:

11/12/2021

Contact Information and Declaration of Sponsor of OFF-SITE PROJECT (If Different)

Company Name

Name (Print) of Contact Person

Address

City, State, Zip

Phone / Fax

Email

I hereby declare that the information herein is accurate to the best of my knowledge and that I intend to satisfy the requirements of Planning Code Section 415 as indicated above.

Sign Here

Signature:

Name (Print), Title:

SUPPLEMENTAL INFORMATION FOR Anti-Discriminatory Housing Policy

1. Owner/Applicant Information

PROPERTY OWNER'S NAME: <i>Montgomery Place LLC</i>	
PROPERTY OWNER'S ADDRESS: <i>1630 Oakland Rd #A215 San Jose, CA 95131 Attn: Richard Chen</i>	TELEPHONE: <i>(408) 680-3880</i>
	EMAIL: <i>jhrchen@gmail.com</i>

APPLICANT'S NAME: <i>Ian Birchall</i>	
Same as Above <input type="checkbox"/>	
APPLICANT'S ADDRESS: <i>177 Post Street, Suite 920 San Francisco, CA 94108</i>	TELEPHONE: <i>(415) 512-9660</i>
	EMAIL: <i>ian@ibadesign.com</i>

CONTACT FOR PROJECT INFORMATION:	
Same as Above <input checked="" type="checkbox"/>	
ADDRESS:	TELEPHONE: ()
	EMAIL:

COMMUNITY LIAISON FOR PROJECT (PLEASE REPORT CHANGES TO THE ZONING ADMINISTRATOR):	
Same as Above <input checked="" type="checkbox"/>	
ADDRESS:	TELEPHONE: ()
	EMAIL:

2. Location and Project Description

STREET ADDRESS OF PROJECT: <i>425 Montgomery Street</i>		ZIP CODE:
CROSS STREETS: <i>Montgomery and Kearny</i>		
ASSESSORS BLOCK/LOT: <i>0163 1002</i>	ZONING DISTRICT: <i>Broadway NCD</i>	HEIGHT/BULK DISTRICT: <i>65-A-1</i>

PROJECT TYPE: (Please check all that apply)	EXISTING DWELLING UNITS:	PROPOSED DWELLING UNITS:	NET INCREASE:
<input checked="" type="checkbox"/> New Construction	<i>0</i>	<i>41</i>	<i>+41</i>
<input checked="" type="checkbox"/> Demolition			
<input type="checkbox"/> Alteration			
<input type="checkbox"/> Other: _____			

Compliance with the Anti-Discriminatory Housing Policy

1. Does the applicant or sponsor, including the applicant or sponsor's parent company, subsidiary, or any other business or entity with an ownership share of at least 30% of the applicant's company, engage in the business of developing real estate, owning properties, or leasing or selling individual dwelling units in States or jurisdictions outside of California?

☐ YES ☒ NO

1a. If yes, in which States? _____

- 1b. If yes, does the applicant or sponsor, as defined above, have policies in individual States that prohibit discrimination based on sexual orientation and gender identity in the sale, lease, or financing of any dwelling units enforced on every property in the State or States where the applicant or sponsor has an ownership or financial interest?

☐ YES ☒ NO

- 1c. If yes, does the applicant or sponsor, as defined above, have a national policy that prohibits discrimination based on sexual orientation and gender identity in the sale, lease, or financing of any dwelling units enforced on every property in the United States where the applicant or sponsor has an ownership or financial interest in property?

☐ YES ☒ NO

If the answer to 1b and/or 1c is yes, please provide a copy of that policy or policies as part of the supplemental information packet to the Planning Department.

Human Rights Commission contact information
Mullane Ahern at (415)252-2514 or mullane.ahern@sfgov.org

Applicant's Affidavit

Under penalty of perjury the following declarations are made:

- a: The undersigned is the owner or authorized agent of the owner of this property.
- b: The information presented is true and correct to the best of my knowledge.
- c: Other information or applications may be required.

Signature: _____


MULLANE

Date: _____

Print name, and indicate whether owner, or authorized agent:

MONTGOMERY PLACE LLC
Owner / Authorized Agent (circle one)



AFFIDAVIT FOR FIRST SOURCE HIRING PROGRAM - ADMINISTRATIVE CODE CHAPTER 83

APPLICATION

Project Sponsor's Information

Name: Montgomery Place LLC

Address: 1630 Oakland Road, #A215 San Jose, CA 95131 Attn: Richard Chen

Email Address: jhrchen@gmail.com

Telephone: 408-680-3880

Property Information and Related Applications

Project Address: 425 Broadway

Block/Lot(s): 0163/002

Building Permit Application No(s):

Estimated Residential Units: 41

Estimated SQ FT Commercial Space: 23,675

Estimated Height/Floors: 65 feet

Estimated Construction Cost: \$30,600,000

Anticipated Start Date: Fall 2022

FIRST SOURCE HIRING PROGRAM VERIFICATION

CHECK ALL BOXES APPLICABLE TO THIS PROJECT	YES
Project is wholly residential	
Project is wholly commercial	
Project is mixed use	✓
A: The project consists of ten (10) or more residential units.	✓
B: The project consists of 25,000 square feet or more of gross commercial floor area/	
C: Neither A nor B apply	

Notes:

- If you checked C, this project is NOT subject to the First Source Hiring Program. Sign Section 4: Declaration of Sponsor of Project and submit to the Planning Department.
- If you checked A or B, your project IS subject to the First Source Hiring Program. Please complete the reverse of this document, sign, and submit to the Planning Department prior to any Planning Commission hearing. If principally permitted, Planning Department approval of the Site Permit is required for all projects subject to Administrative Code Chapter 83.
- For questions, please contact OEWD's CityBuild program at CityBuild@sfgov.org or 415.701.4848. For more information about the First Source Hiring Program visit www.workforcedevelopmentsf.org
- If the project is subject to the First Source Hiring Program, you are required to execute a Memorandum of Understanding (MOU) with OEWD's CityBuild program prior to receiving construction permits from Department of Building Inspection.

FIRST SOURCE HIRING PROGRAM - WORKFORCE PROJECTION

Per Section 83.11 of Administrative Code Chapter 83, it is the developer's responsibility to complete the following information to the best of their knowledge.

Provide the estimated number of employees from each construction trade to be used on the project, indicating how many are entry and/or apprentice level as well as the anticipated wage for these positions.

Check the anticipated trade(s) and provide accompanying information (Select all that apply):

TRADE/CRAFT	ANTICIPATED JOURNEYMAN WAGE	# APPRENTICE POSITIONS	# TOTAL POSITIONS	TRADE/CRAFT	ANTICIPATED JOURNEYMAN WAGE	# APPRENTICE POSITIONS	# TOTAL POSITIONS
Abatement Laborer		1	2	Laborer		3	6
Boilermaker				Operating Engineer		1	
Bricklayer		2	7	Painter		3	9
Carpenter		5	17	Pile Driver		1	5
Cement Mason		3	12	Plasterer		3	10
Drywaller/Latherer		2	12	Plumber and Pipefitter		3	
Electrician		3	8	Roofer/Water proofer		2	18
Elevator Constructor		1	4	Sheet Metal Worker		2	6
Floor Coverer		3	10	Sprinkler Fitter		3	7
Glazier				Taper		4	6
Heat & Frost Insulator		2	7	Tile Layer/Finisher		4	9
Ironworker		1	6	Other:			
		TOTAL:	85			TOTAL:	68

- Will the anticipated employee compensation by trade be consistent with area Prevailing Wage? YES ☒ NO ☐
- Will the awarded contractor(s) participate in an apprenticeship program approved by the State of California's Department of Industrial Relations? ☐ ☐
- Will hiring and retention goals for apprentices be established? ☐ ☐
- What is the estimated number of local residents to be hired? _____

DECLARATION OF SPONSOR OF PRINCIPAL PROJECT

PRINT NAME AND TITLE OF AUTHORIZED REPRESENTATIVE	EMAIL	PHONE NUMBER
MONTGOMERY PLACE LLC	JARREN@GMAIL.COM	415-680-7888
I HEREBY DECLARE THAT THE INFORMATION PROVIDED HEREIN IS ACCURATE TO THE BEST OF MY KNOWLEDGE AND THAT I COORDINATED WITH OEWD'S CITYBUILD PROGRAM TO SATISFY THE REQUIREMENTS OF ADMINISTRATIVE CODE CHAPTER 83.		
(SIGNATURE OF AUTHORIZED REPRESENTATIVE)		(DATE)

FOR PLANNING DEPARTMENT STAFF ONLY: PLEASE EMAIL AN ELECTRONIC COPY OF THE COMPLETED AFFIDAVIT FOR FIRST SOURCE HIRING PROGRAM TO OEWD'S CITYBUILD PROGRAM AT CITYBUILD@SFGOV.ORG

Cc: Office of Economic and Workforce Development, CityBuild
Address: 1 South Van Ness 5th Floor San Francisco, CA 94103 Phone: 415.701.4848
Website: www.workforcedevelopmentst.org Email: CityBuild@sfgov.org