



EXECUTIVE SUMMARY CONDITIONAL USE

HEARING DATE: JULY 29, 2021

Record No.: 2016-002728CUA-02
Project Address: 2525 Van Ness Avenue
Zoning: RC-3 (Residential-Commercial, Medium Density) Zoning District
65-A Height and Bulk District
Van Ness Avenue Area Plan
Block/Lot: 0527/004
Project Sponsor: Eduardo Sagues
3456 Sacramento Street
San Francisco, CA 94118
Property Owner: Boubouffe, LLC
San Francisco, CA 94118
Staff Contact: Christopher May – (628) 652-7359
christopher.may@sfgov.org

Recommendation: Disapproval

Project Description

The Project seeks to increase the residential off-street parking ratio from 0.5 spaces per dwelling unit (for a total of 14 spaces) as previously approved by the Planning Commission (Motion No. 20176) to 0.75 spaces per dwelling unit (for a total of 21 spaces). The approved project included the demolition of the existing 24-foot tall, two-story, 9,980 square-foot commercial building and new construction of a 65-foot tall, seven-story, 70,080 square foot building containing 28 dwelling units (24 two-bedroom units and 4 one-bedroom units), up to 2,000 square feet of ground floor retail space, 28 Class 1 bicycle parking spaces and 14 off-street below grade parking spaces.

Required Commission Action

In order for the Project to proceed, the Commission must grant a Conditional Use Authorization, pursuant to Planning Code Sections 151.1, 209.3 and 303 to allow 0.75 off-street parking spaces per dwelling unit.

Issues and Other Considerations

- **Public Comment & Outreach.**

- **Support/Opposition:** The Department has received no public comment regarding the proposed project.

- **Project Background:**

- On May 3, 2018, the Planning Commission approved Conditional Use Authorization Application No. 2016-002728CUA for a new seven-story building with 28 dwelling units and 14 off-street parking spaces.
- On March 6, 2020, the Project Sponsor submitted a building permit application (BPA No. 2020.0306.6413) to revise the previously approved permit (BPA No. 2017.1227.7456) proposing additional excavation within the rear yard to accommodate “tenant storage”. This permit was approved and issued on March 1, 2021.
- Four weeks later, on March 29, 2021, the Project Sponsor filed Conditional Use Authorization Application No. 2016-002728CUA-02 proposing the conversion of this area into seven (7) additional residential off-street parking spaces which would increase the residential parking ratio from 0.5 or 14 spaces as previously approved by the Planning Commission (Motion No. 20176) to 0.75 or 21 residential parking spaces.

Basis for Recommendation

The Department finds that the Project is, on balance, **inconsistent** with the Objectives and Policies of the General Plan in that it will not reduce the demand for off-street parking, will not reduce traffic congestion, does not encourage alternatives to the private automobile, and may compromise the safety and comfort of nearby pedestrians and cyclists. The Department also finds that the project would not be necessary, desirable, and compatible with the surrounding neighborhood, and would be detrimental to persons or adjacent properties in the vicinity.

Attachments:

Draft Motion – Conditional Use Authorization
Exhibit B – Plans and Renderings
Exhibit C – Land Use Data
Exhibit D – Maps and Context Photos
Exhibit E - Project Sponsor Brief



PLANNING COMMISSION DRAFT MOTION

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65-A Height and Bulk District
Van Ness Avenue Area Plan
Block/Lot: 0527/004
Project Sponsor: Eduardo Sagues
3456 Sacramento Street
San Francisco, CA 94118
Property Owner: Boubouffe, LLC
San Francisco, CA 94118
Staff Contact: Christopher May – (628) 562-7359
christopher.may@sfgov.org

ADOPTING FINDINGS RELATING TO THE DISAPPROVAL OF A CONDITIONAL USE AUTHORIZATION PURSUANT TO PLANNING CODE SECTIONS 151.1, 209.3 AND 303 TO INCREASE THE RESIDENTIAL PARKING RATIO FROM 0.5 OR 14 SPACES AS PREVIOUSLY APPROVED BY THE PLANNING COMMISSION (MOTION NO. 20176) TO 0.75 OR 21 RESIDENTIAL PARKING SPACES OFF-STREET PARKING SPACES, LOCATED AT 2525 VAN NESS AVENUE, LOT 004 IN ASSESSOR'S BLOCK 0527, WITHIN THE RC-3 (RESIDENTIAL-COMMERCIAL, MEDIUM DENSITY) ZONING DISTRICT AND A 65-A HEIGHT AND BULK DISTRICT.

PREAMBLE

On May 3, 2018, the San Francisco Planning Commission (hereinafter “Commission”) approved Conditional Use Authorization Application No. 2016-002728CUA proposing to construct a new seven-story, 65-ft tall residential building with 28 dwelling units, up to 2,000 square feet of ground floor retail space and 14 off-street parking spaces at 2525 Van Ness Avenue, Block 0527, Lot 004 (hereinafter “Project Site”).

On March 6, 2020, Eduardo Sagues of Boubouffe, LLC (hereinafter "Project Sponsor") submitted a building permit application (BPA No. 2020.0306.6413) to revise the previously approved permit (BPA No. 2017.1227.7456) proposing additional excavation within the rear yard to accommodate “tenant storage”. This permit was approved and issued on March 1, 2021.

Four weeks later, on March 29, 2021, the Project Sponsor filed Conditional Use Authorization Application No. 2016-002728CUA-02 (hereinafter “Application”) with the Planning Department (hereinafter “Department”) proposing the conversion of this space into seven (7) additional residential off-street parking spaces which would increase the residential parking ratio from 0.5 or 14 spaces as previously approved by the Planning Commission (Motion No. 20176) to 0.75 or 21 residential parking spaces (hereinafter “Project”) at the Project Site.

On July 29, 2021, the Commission conducted a duly noticed public hearing at a regularly scheduled meeting on Conditional Use Authorization Application No. 2016-002728CUA-02.

The Planning Department Commission Secretary is the Custodian of Records; the File for Record No. 2014-000601ENX is located at 49 South Van Ness Avenue, Suite 1400, San Francisco, California.

The Commission has heard and considered the testimony presented to it at the public hearing and has further considered written materials and oral testimony presented on behalf of the applicant, Department staff, and other interested parties.

MOVED, that the Commission hereby **disapproves** the Conditional Use Authorization as requested in Application No. 2016-002728CUA-02, based on the following findings:

FINDINGS

Having reviewed the materials identified in the preamble above, and having heard all testimony and arguments, this Commission finds, concludes, and determines as follows:

1. The above recitals are accurate and constitute findings of this Commission.

- 2. Project Description.** The Project seeks to increase the residential parking ratio from 0.5 or 14 spaces as previously approved by the Planning Commission (Motion No. 20176) to 0.75 or 21 residential parking spaces off-street parking spaces. The approved project included the demolition of the existing 24-foot tall, two-story, 9,980 square-foot commercial building and new construction of a 65-foot tall, seven-story, 70,080 square foot building containing 28 dwelling units (24 two-bedroom units and 4 one-bedroom units), up to 2,000 square feet of ground floor retail space, 28 Class 1 bicycle parking spaces and 14 off-street below grade parking spaces.

On March 6, 2020, the project sponsor submitted a building permit application to revise the previously-approved permit proposing additional excavation within the rear yard to accommodate “tenant storage”. This permit was approved and issued on March 1, 2021. Four weeks later, the project sponsor submitted Application No. 2016-002728CUA-02 proposing the conversion of this space into seven (7) additional residential off-street parking spaces.

- 3. Site Description and Present Use.** The project is located at the west side of Van Ness Avenue, between Filbert and Union Streets. The 11,025 square-foot property has 85 feet of frontage on Van Ness Avenue and a lot depth that ranges from 125.17 to 129.9 feet. The site is currently vacant.
- 4. Surrounding Properties and Neighborhood.** The subject property is located at the center of a seven-block long RC-3 Zoning District that extends north from Broadway to Chestnut Street, and applies only to properties that front onto Van Ness Avenue. Beyond the RC-3 zoned properties fronting on Van Ness Avenue, the surrounding zoning includes lower density RH-3, RM-2 and RM-3 districts, neighborhood commercial districts along Polk, Lombard and Union Streets, and a public use district that captures Fort Mason and Aquatic Park. The surrounding neighborhood character reflects this assemblage of zoning districts and consists of two- to eleven-story buildings that contain a mixture of residential, commercial and institutional uses. More specifically, to the north of the subject property is a six story 27-unit residential building. To the east of the subject property, across Van Ness Avenue is a five-story hotel (d.b.a. da Vinci Villa), and three four-story multi-family residential buildings. South of the subject property are three three-story multi-family buildings, two of which contain ground floor commercial uses. The project site is also located along the Van Ness Bus Rapid Transit corridor and is within a quarter mile of nine Muni bus routes, including the 19 - Polk, 28 – 19th Avenue, 30 - Stockton, 30X – Marina Express, 41 - Union, 45 – Union/Stockton, 47 – Van Ness, 49 – Van Ness/Mission and 76X – Marin Headlands Express.
- 5. Public Outreach and Comments.** The Department has received no public comment regarding the proposed project.
- 6. Planning Code Compliance.** The Commission finds that the Project is **inconsistent** with the relevant provisions of the Planning Code in the following manner:

- A. **Off-Street Parking.** Planning Code Section 151.1 permits a maximum of one off-street parking space for each two dwelling units as of right in the RC-3 Zoning District. Any request for accessory residential parking in excess of what is principally permitted, up to 0.75 spaces per dwelling unit, shall be reviewed by the Planning Commission as a Conditional Use.

The project proposes to add seven (7) accessory off-street parking spaces to the previously approved 14 parking spaces within the development. The Planning Code allows up to 14 accessory off-street parking spaces for residential uses as-of-right, and allows up to seven (7) additional spaces as a Conditional Use. The additional required findings have been summarized in Subsections 7 and 8 below.

- B. **Transportation Demand Management (TDM) Plan.** Pursuant to Planning Code Section 169 and the TDM Program Standards, the Project shall finalize a TDM Plan prior Planning Department approval of the first Building Permit or Site Permit.

The Project submitted a completed Environmental Evaluation Application prior to September 4, 2016. Therefore, the Project must only achieve 50% of the point target established in the TDM Program Standards. The previously approved Project was required to achieve a target of 6 points. As currently proposed, the Project is required to achieve 7 points. The TDM Plan submitted to the Planning Department proposes to achieve only 5 points through the following TDM measures:

- *Unbundled Parking (Location D)*
- *Parking Supply (Option B)*
- *Bicycle Parking (Option A)*

7. **Conditional Use Findings.** Planning Code Section 303(c) establishes criteria for the Planning Commission to consider when reviewing applications for Conditional Use Authorization. On balance, the project **does not comply** with said criteria in that:

- A. The proposed new uses and building, at the size and intensity contemplated and at the proposed location, will provide a development that is necessary or desirable, and compatible with, the neighborhood or the community.

The massing and mix of residential and commercial uses of the previously approved building is in keeping with other buildings within the neighborhood. The proposed increase in the number of off-street parking spaces, however, is not necessary or desirable in that it encourages private automobile use in an area rich with public transit options and ample opportunities for non-motorized transportation.

- B. The proposed project will not be detrimental to the health, safety, convenience or general welfare of persons residing or working in the vicinity. There are no features of the project that could be detrimental to the health, safety or convenience of those residing or working the area, in that:

- (1) Nature of proposed site, including its size and shape, and the proposed size, shape and arrangement of structures;

The height and bulk of the previously approved building will remain the same and will not alter

the existing appearance or character of the project vicinity. The proposed additional off-street parking spaces would be accommodated in an area previously designated as tenant storage within the garage space and will not affect the building envelope.

- (2) The accessibility and traffic patterns for persons and vehicles, the type and volume of such traffic, and the adequacy of proposed off-street parking and loading;

The Planning Code does not require any parking for the mix of residential and commercial uses proposed for the site. The proposed increase in the number of off-street parking spaces could be detrimental to the health, safety or convenience of those residing or working the area, in that there will be more opportunities for vehicle-pedestrian conflicts where the sidewalk and garage entrance intersect. Further, an increase in automobiles entering and leaving the subject property may interfere with the efficient flow of transit vehicles along the soon-to-be completed Van Ness Bus Rapid Transit route.

- (3) The safeguards afforded to prevent noxious or offensive emissions such as noise, glare, dust and odor;

The proposed increase in the number of off-street parking spaces is not expected to result in any noxious or offensive emissions such as noise, glare, dust and odor.

- (4) Treatment given, as appropriate, to such aspects as landscaping, screening, open spaces, parking and loading areas, service areas, lighting and signs;

Access to the previously approved parking garage is not proposed to be altered.

- C. That the use as proposed will comply with the applicable provisions of the Planning Code and will not adversely affect the General Plan.

*The Project complies with all relevant requirements and standards of the Planning Code, but is **inconsistent** with objectives and policies of the General Plan as detailed in Subsection 9 below.*

- D. That use or feature as proposed will provide development that is in conformity with the stated purpose of the applicable Use District.

*The proposed increase in the amount of off-street parking is **inconsistent** with the stated purpose of the compact, walkable, transit-oriented and mixed-use nature of Residential-Commercial Districts that is recognized by having no off-street parking requirements.*

- 8. Accessory Parking Above That Principally Permitted.** Planning Code Section 303(u) requires that in granting approval for parking accessory to Residential Uses above that which is principally permitted, the Planning Commission shall make the following affirmative findings. On balance, the project **does not** comply with said criteria in that:

- A. For projects with 50 units or more, all residential accessory parking in excess of 0.5 parking spaces for each Dwelling Unit shall be stored and accessed by mechanical stackers or lifts, valet, or other

space-efficient means that allow more space above-ground for housing, maximizes space efficiency, and discourages use of vehicles for commuting or daily errands. The Planning Commission may authorize the request for additional parking notwithstanding that the project sponsor cannot fully satisfy this requirement provided that the project sponsor demonstrates hardship or practical infeasibility (such as for retrofit of existing buildings) in the use of space-efficient parking given the configuration of the parking floors within the building and the number of independently accessible spaces above 0.5 spaces per unit is de minimus and subsequent valet operation or other form of parking space management could not significantly increase the capacity of the parking space above the maximums in Table 151.1.

The previously approved project does not contain 50 units or more.

- B. All parking meets the active use and architectural screening requirements in Section 145.1 and the project sponsor is not requesting any exceptions or variances requiring such treatments elsewhere in this Code.

The proposed additional off-street parking spaces meet the active use and architectural screening requirements in Section 145.1 and do not require any exceptions or variances.

- C. Demonstration that trips to the use or uses to be served, and the apparent demand for additional parking, cannot be satisfied by the amount of parking classified by this Code as accessory, by transit service which exists or is likely to be provided in the foreseeable future, by carpool arrangements, by more efficient use of existing on-street and off-street parking available in the area, and by other means.

The subject property is well served by a robust public transit network including the soon-to-be-completed Van Ness Avenue Bus Rapid Transit line and nine (9) MUNI bus routes. Further, the previously approved project will provide 28 Class 1 bicycle parking spaces for building residents. The project sponsor has not adequately demonstrated that there is additional demand to justify the requested increase in off-street parking that cannot be satisfied by the 14 principally permitted off-street parking spaces, the multiple public transit options, carpool arrangements or by nearby on-street parking.

- D. Demonstration that the apparent demand for additional parking cannot be satisfied by the provision by the applicant of one or more car-share parking spaces in addition to those that may already be required by Section 166 of this Code.

Section 166 of the Planning Code requires the provision of car-share parking spaces only in projects with 50 units or more. The previously-approved project contains 28 units; therefore, no car-share parking spaces are required, and none are being provided.

- E. The absence of potential detrimental effects of the proposed parking upon the surrounding area, especially through unnecessary demolition of sound structures, contribution to traffic congestion, or disruption of or conflict with transit services, walking, and cycling.

The proposed additional off-street parking would not require the unnecessary demolition of any

sound structures, but they would occupy an area of the below-grade garage requiring additional excavation. The additional cars entering and exiting the garage may contribute to increased traffic congestion and may result in the disruption or conflict with the transit routes on Van Ness Avenue, as well as cyclists and pedestrians passing by the site.

- F. Accommodating excess accessory parking does not degrade the overall urban design quality of the project proposal nor diminish the quality and viability of existing or planned streetscape enhancements.

The additional proposed off-street parking would not degrade the overall urban design quality of the project proposal nor diminish the quality and viability of existing or planned streetscape enhancements, as it would be located within the previously approved building envelope.

9. **General Plan Compliance.** The Project is, on balance, **inconsistent** with the following Objectives and Policies of the General Plan:

VAN NESS AVENUE AREA PLAN

Objectives and Policies

OBJECTIVE 9

PROVIDE SAFE AND EFFICIENT MOVEMENT AMONG ALL USERS ON VAN NESS AVENUE.

Policy 9.1

Reduce conflicts between transit vehicles and other moving and parked vehicles. Aggressively enforce no parking regulations in bus zones.

TRANSPORTATION ELEMENT

Objectives and Policies

OBJECTIVE 1

MEET THE NEEDS OF ALL RESIDENTS AND VISITORS FOR SAFE, CONVENIENT, AND INEXPENSIVE TRAVEL WITHIN SAN FRANCISCO AND BETWEEN THE CITY AND OTHER PARTS OF THE REGION WHILE MAINTAINING THE HIGH-QUALITY LIVING ENVIRONMENT OF THE BAY AREA.

Policy 1.2

Ensure the safety and comfort of pedestrians throughout the city.

Policy 1.3

Give priority to public transit and other alternatives to the private automobile as the means of meeting San Francisco's transportation needs, particularly those of commuters.

OBJECTIVE 14

DEVELOP AND IMPLEMENT A PLAN FOR OPERATIONAL CHANGES AND LAND USE POLICIES THAT WILL MAINTAIN MOBILITY AND SAFETY DESPITE A RISE IN TRAVEL DEMAND THAT COULD OTHERWISE RESULT IN SYSTEM CAPACITY DEFICIENCIES.

Policy 14.4

Reduce congestion by encouraging alternatives to the single occupant auto through the reservation of right-of-way and enhancement of other facilities dedicated to multiple modes of transportation.

OBJECTIVE 16

DEVELOP AND IMPLEMENT PROGRAMS THAT WILL EFFICIENTLY MANAGE THE SUPPLY OF PARKING AT EMPLOYMENT CENTERS THROUGHOUT THE CITY SO AS TO DISCOURAGE SINGLE-OCCUPANT RIDERSHIP AND ENCOURAGE RIDESHARING, TRANSIT AND OTHER ALTERNATIVES TO THE SINGLE-OCCUPANT AUTOMOBILE.

Policy 16.1

Reduce parking demand through the provision of comprehensive information that encourages the use of alternative modes of transportation.

Policy 16.5

Reduce parking demand through limiting the absolute amount of spaces and prioritizing the spaces for short-term and ride-share uses.

*The previously approved project included the maximum number of off-street parking spaces permitted as-of-right. The Project is located directly on the soon-to-be-completed Van Ness Avenue Bus Rapid Transit line, is well served by nine (9) MUNI bus routes, and will provide 28 Class 1 bicycle parking spaces. On balance, the proposal to increase the number of off-street parking spaces is **inconsistent** with the Objectives and Policies of the General Plan in that it will not reduce the demand for off-street parking, will not reduce traffic congestion, does not encourage alternatives to the private automobile, and may compromise the safety and comfort of nearby pedestrians and cyclists.*

10. Planning Code Section 101.1(b) establishes eight priority-planning policies and requires review of permits for consistency with said policies. On balance, the project complies with said policies in that:

- A. That existing neighborhood-serving retail uses be preserved and enhanced and future opportunities for resident employment in and ownership of such businesses be enhanced.

The proposal to increase the number of off-street parking spaces will not affect existing neighborhood-serving retail uses.

- B. That existing housing and neighborhood character be conserved and protected in order to preserve the cultural and economic diversity of our neighborhoods.

The proposal to increase the number of off-street parking spaces will not adversely affect existing units in the surrounding neighborhood.

- C. That the City's supply of affordable housing be preserved and enhanced.

The proposal to increase the number of off-street parking spaces will not impact the City's supply of affordable housing.

- D. That commuter traffic not impede MUNI transit service or overburden our streets or neighborhood parking.

The subject property is well served by nearby public transportation options including the soon-to-be-completed Van Ness Bus Rapid Transit line and nine (9) MUNI bus routes including the 19 - Polk, 28 - 19th Avenue, 30 - Stockton, 30X - Marina Express, 41 - Union, 45 - Union/Stockton, 47 - Van Ness, 49 - Van Ness/Mission and 76X - Marin Headlands Express. The previously approved project also provides sufficient bicycle parking for residents and their guests and is in a dense urban environment ideal for walking. The proposal to increase the number of off-street parking spaces in excess of the principally permitted amount may impede MUNI transit service along this transit-rich corridor.

- E. That a diverse economic base be maintained by protecting our industrial and service sectors from displacement due to commercial office development, and that future opportunities for resident employment and ownership in these sectors be enhanced.

The proposal to increase the number of off-street parking spaces will not affect any existing industrial or service sectors.

- F. That the City achieve the greatest possible preparedness to protect against injury and loss of life in an earthquake.

The proposal to increase the number of off-street parking spaces will not affect the property's ability to withstand an earthquake.

- G. That landmarks and historic buildings be preserved.

The Project Site is not occupied by any City Landmarks or historic buildings.

- H. That our parks and open space and their access to sunlight and vistas be protected from development.

The proposal to increase the number of off-street parking spaces will not affect the envelope of the previously approved building, which would not cast shadow on any public parks.

11. The Project is not consistent with and would not promote the general and specific purposes of the Code provided under Section 101.1(b) in that, as designed, the Project would not contribute to the character and stability of the neighborhood and would not constitute a beneficial development.
12. The Commission hereby finds that disapproval of the Conditional Use Authorization would promote the health, safety and welfare of the City.

DECISION

That based upon the Record, the submissions by the Applicant, the staff of the Department and other interested parties, the oral testimony presented to this Commission at the public hearings, and all other written materials submitted by all parties, the Commission hereby **DISAPPROVES Conditional Use Authorization Application No. 2016-002728CUA-02.**

APPEAL AND EFFECTIVE DATE OF MOTION: Any aggrieved person may appeal this Conditional Use Authorization to the Board of Supervisors within thirty (30) days after the date of this Motion. The effective date of this Motion shall be the date of this Motion if not appealed (after the 30-day period has expired) OR the date of the decision of the Board of Supervisors if appealed to the Board of Supervisors. For further information, please contact the Board of Supervisors at (415) 554-5184, City Hall, Room 244, 1 Dr. Carlton B. Goodlett Place, San Francisco, CA 94102.

Protest of Fee or Exaction: You may protest any fee or exaction subject to Government Code Section 66000 that is imposed as a condition of approval by following the procedures set forth in Government Code Section 66020. The protest must satisfy the requirements of Government Code Section 66020(a) and must be filed within 90 days of the date of the first approval or conditional approval of the development referencing the challenged fee or exaction. For purposes of Government Code Section 66020, the date of imposition of the fee shall be the date of the earliest discretionary approval by the City of the subject development.

If the City has not previously given Notice of an earlier discretionary approval of the project, the Planning Commission's adoption of this Motion, Resolution, Discretionary Review Action or the Zoning Administrator's Variance Decision Letter constitutes the disapproval of the development and the City hereby gives **NOTICE** that the 90-day protest period under Government Code Section 66020 has begun. If the City has already given Notice that the 90-day approval period has begun for the subject development, then this document does not re-commence the 90-day approval period.

I hereby certify that the Planning Commission ADOPTED the foregoing Motion on July 29, 2021.

Jonas P. Ionin
Commission Secretary

AYES:

NAYS:

ABSENT:

RECUSE:

ADOPTED: July 29, 2021

Exhibit B:

Plans and Renderings

VAN NESS MIXED-USE DEVELOPMENT

2525 VAN NESS AVENUE
SAN FRANCISCO, CALIFORNIA

ALTERED SITE PERMIT - REV E
19 JANUARY 2021
PERMIT # 202003066413



OWNER
BOUBOUFFE LLC

ARCHITECT
HANDEL ARCHITECTS

735 MARKET STREET
2ND FLOOR
SAN FRANCISCO, CA 94103

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SAN FRANCISCO, CALIFORNIA 94103

TELEPHONE: (415) 551-7884
FACSIMILE: (415) 520-0454

DRAWING LIST	
G000-1	COVER SHEET
G002A	PROJECT INFORMATION AND STATISTICS
G003A	PLOT PLAN
G004A	VICINITY PLAN EXISTING SITE PHOTOS
G004B	RENDERINGS
G004C	RENDERINGS
G005A	SYMBOLS AND ABBREVIATIONS
G016A	NOTICE OF SPECIAL RESTRICTIONS VAR
G016B	NOTICE OF SPECIAL RESTRICTIONS CU
G016C	NOTICE OF SPECIAL RESTRICTIONS CU
G016D	NOTICE OF SPECIAL RESTRICTIONS TDM
G035A	GREEN BUILDING SITE PERMIT SUBMITTAL
G035B	GPR CHECKLIST
G035C	CALGREEN CODE
G035D	CALGREEN CODE
G045A	ENVIRONMENTAL NOISE STUDY
LS100	CODE SITE PLAN
LS101	CODE FLOOR PLANS (LB-1)
LS102	CODE FLOOR PLANS (LB-2)
LS103	CODE FLOOR PLANS (LB-3)
LS104	CODE SECTIONS
HA001A	STANDARD MOUNTING HEIGHTS
HA001B	TYP. ADAPTABLE AND ACCESSIBLE KITCHEN/BATH
HA001C	TYPICAL ACCESS COMPLIANCE DETAILS
HA100	ACCESSIBLE ROUTES
HA101	ACCESSIBLE ROUTES
ALTA1-1	SURVEY
A100-1	BASEMENT LEVEL FLOOR PLAN
A101-1	LEVEL 1 FLOOR PLAN
A102-1	LEVEL 2 FLOOR PLAN
A103-1	LEVEL 3-4 FLOOR PLAN
A105-1	LEVEL 5-6 FLOOR PLAN
A107-1	LEVEL 7 FLOOR PLAN
A108-1	ROOF PLAN
A200	EXTERIOR ELEVATIONS
A201	EXTERIOR ELEVATIONS
A210	BUILDING SECTIONS
A211	BUILDING SECTIONS
A300A	ENLARGED ELEVATION - EAST POOLUM
A300A	WALL SECTIONS
A421A	CANOPY PARTIAL PLAN AND ELEVATION
A501A	EXIT STAIR PLANS AND SECTIONS
A502A	EXIT STAIR PLANS AND SECTIONS
A901A	PARTITION SCHEDULE
44 SHEETS	

SFFD INSP.
FEES REQ.

FIRE



Sagiv Weiss-Ishai - FIRE

FIRE

February 22, 2021

PERMIT NUMBER

2020-0306-6413 / BB Session # 254-678-915

REVIEWED

For Compliance with City
and County Ordinances
and State Codes.
The stamping of this plan and these
specification SHALL NOT be
held to permit or to be an
approval of the violation
of any City and County
ordinance or State Law

Approval is subject to
the inspection by the
Fire Department

Sagiv Weiss-Ishai, PE, SFFD

These plans must be kept
on the premises and
accessible to the
inspection at all times.

SITE PERMIT ONLY. SUBMIT
CONSTRUCTION PLANS.
Sagiv Weiss-Ishai - SFFD
FIRE DEPT. PLAN CHECKER

Provide Construction Plans under separate
addenda:
Arch
MEP
FA
Sprinkler
ERHCS

REVIEWED BY FIRE DEPT.
FIRE DEPT INSPECTIONS
NOT REQUIRED

SFPWBSM sign off on Job Card required prior to DBI
final. Call (650) 271-2000 /
dpe-baninspect@sfidpw.org (sidewalk) & (628)
652-8733 (urbanforestry@sfidpw.org (trees) to
schedule. Subject to all conditions of SFPWBSM:
18IE-0918, 18IV-0045, 18MSE-0724 & BUIF.



Clinton Choy - PWBSM

PUBLIC WORKS
February 02, 2021



David M. Jones

DEPARTMENT OF
BUILDING INSPECTION
January 29, 2021



Capacity Charges

Water: \$ 0

Wastewater: \$ 0

DC 02/02/21



DOHING

PUBLIC UTILITIES
COMMISSION

February 02, 2021



Christopher May - PLN

PLANNING
January 20, 2021

ADDENDA SCHEDULE:

- 1 Foundation and Superstructure
- 2 Architecture and MEP
- 3 Metal Stairs
- 4 Fire Sprinklers
- 5 Fire Alarm and Elevator
Landing Two Way Emergency
- 6 ERRCS



2525 VAN NESS
SAN FRANCISCO, CA 94109

CLIENT
BOUBOUFFE LLC
3455 SACRAMENTO STREET
SAN FRANCISCO, CA 94118

ARCHITECT
HANDEL ARCHITECTS, LLP
735 MARKET ST.
2ND FLOOR
SAN FRANCISCO, CA 94103
415-495-5588

REV.	DESCRIPTION	DATE
E	ALTERED SITE PERMIT - REV E	01-19-2021
D	ALTERED SITE PERMIT - REV D	11-19-2020
C	ALTERED SITE PERMIT - REV C	10-22-2020
B	90% CONSTRUCTION DOCUMENTS	11-04-2020
A	ALTERED SITE PERMIT - REV B	09-28-2020
	90% CONSTRUCTION DOCUMENTS	09-21-2020
	30% CONSTRUCTION DOCUMENTS	08-20-2020
A	ALTERED SITE PERMIT - REV A	08-07-2020

KEY PLAN

APPROVED
Dept. of Building Insp.
- San Francisco -

March 01, 2021

202003066413_ALTER SITE DWGS

PATRICK O'ROURD
INTERIM DIRECTOR
DEPT. OF BUILDING INSPECTION

STAMP



SCALE: 1/2" = 1'-0"
DRAWN BY: HA
DATE: 8/20/2020

COVER SHEET

G000

PROJECT INFORMATION

PROJECT ADDRESS:

2525 VAN NESS AVENUE, SAN FRANCISCO, CA 94109

LEGAL ADDRESS:

THE LAND REFERRED TO HEREIN IS SITUATED IN THE CITY OF SAN FRANCISCO, COUNTY OF SAN FRANCISCO, STATE OF CALIFORNIA AND IS DESCRIBED AS FOLLOWS:

BEGINNING AT THE POINT ON THE WESTERLY LINE OF VAN NESS AVENUE, DISTANT THEREON 100 FEET SOUTHERLY FROM THE SOUTHERLY LINE OF FILBERT STREET; RUNNING THENCE SOUTHERLY ALONG SAID LINE OF VAN NESS AVENUE 65 FEET; THENCE AT A RIGHT ANGLE WESTERLY 125 FEET TO THE SOUTHWESTERLY LINE OF 100 VARA LOT NO. 5 OF LACON SURVEY; THENCE NORTHWESTERLY ALONG THE SOUTHWESTERLY LINE OF SAID LOT 5, 12 FEET, MORE OR LESS, TO A POINT PERPENDICULARLY DISTANT WESTERLY 130 FEET FROM THE WESTERLY LINE OF VAN NESS AVENUE; THENCE NORTHERLY AND PARALLEL WITH THE SAID LINE OF VAN NESS AVENUE 45 FEET, MORE OR LESS, TO A POINT PERPENDICULAR DISTANT SOUTHERLY 131 FEET AND 4 INCHES FROM THE SOUTHERLY LINE OF FILBERT STREET; THENCE AT RIGHT ANGLE EASTERLY 1 INCH; THENCE AT A RIGHT ANGLE NORTHERLY 31 FEET AND 4 INCHES; THENCE AT A RIGHT ANGLE EASTERLY 120 FEET AND 11 INCHES TO THE POINT OF BEGINNING. BEING PART OF WESTERN ADDITION BLOCK NO. 97 APR. LOT 004, BLOCK 0527

BUILDING CODE INFORMATION:

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- NFPA 14 - 2013 EDITION (update)
- NFPA 24 - 2016 EDITION (update)
- NFPA 72 - 2016 EDITION (update)
- NFPA 86 - 2016 EDITION (update)
- NFPA 95 - 2010 EDITION (update)

5. SFFD ADMINISTRATION BULLETINS
2.01, 2.04, 2.06, 2.11, 2.16, 3.01, 3.02, 3.03, 4.05, 4.06, 4.11, 4.12, 4.13, 4.15, 4.18, 4.24, 4.26, 4.27, 4.28

6. ELEVATOR CODES TO INCLUDE:

• CCR-TITLE 8 - ELEVATOR SAFETY ORDERS - ASME A17.1-2004 (update) FOR GROUP 4 ELEVATORS

SPRINKLERS:

• CBC-2016 Section 905 and SFFD AB # 4.06

BUILDING USE:

THIS PROJECT INCLUDES THE CONSTRUCTION OF A SEVEN-STORY ABOVE GRADE MIXED-USE RESIDENTIAL BUILDING WITH ONE SUBTERRANEAN PARKING LEVEL. THE BUILDING WILL BE PROTECTED THROUGHOUT BY AN AUTOMATIC SPRINKLER SYSTEM

USE AND OCCUPANCY:

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- GROUP R-2 - DWELLING UNITS
- GROUP S-2 - ENCLOSED PARKING GARAGE
- GROUP S-1 - TRASH ROOM, TRASH CHUTE ACCESS ROOMS, MECHANICAL ELECTRICAL ROOMS, ELEVATOR ROOM AND OTHER STORAGE.
- GROUP A-3 - OCCUPIED ROOF (USE ONLY)
- GROUP M - MERCANTILE (CURRENTLY DESIGNATED AS RETAIL) - OCCUPANCY MAY CHANGE WITH TENANT.

NON-SEPARATED MIXED-USE OCCUPANCY APPROACH:

THE PROJECT BUILDING WILL CONTAIN GROUPS M, A-3, R-2, S-1, AND S-2 OCCUPANCIES. THE PROJECT BUILDING WILL UTILIZE THE NON-SEPARATED MIXED-USE OCCUPANCY APPROACH IN ACCORDANCE WITH CBC SECTION 508.3. THE GROUP S-1 AND A-3 OCCUPANCIES WILL BE CONSIDERED ACCESSORY OR OF INCIDENTAL USE.

BUILDING CONSTRUCTION TYPE & ALLOWABLE HEIGHT/AREA:

THE PROJECT IS PROPOSED TO UTILIZE TYPE IB CONSTRUCTION.

THE BUILDING ELEMENTS WILL BE REGULATED AS DESCRIBED BELOW. THE IMPACT OF THIS APPROACH ON EXTERIOR WALL PROTECTION WILL BE SUMMARIZED LATER IN THIS REPORT.

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SUMMARY OF CBC TABLES 504.3, 504.4, AND 506.2 - ALLOWABLE HEIGHT AND AREA

IB CONSTRUCTION	
OCCUPANCY GROUP	HEIGHT LIMITATION AREA LIMITATION
A-3	11 STORIES, 160 FEET
	UNLIMITED
M	11 STORIES, 160 FEET
	UNLIMITED
R-2	11 STORIES, 160 FEET
	UNLIMITED
S-2	11 STORIES, 160 FEET
	237,000

ELEVATOR HOISTWAY OPENING PROTECTION:

THE ELEVATORS IN THE BUILDING CONNECT MORE THAN TWO STORIES AND SO HOISTWAY OPENING PROTECTION IS REQUIRED IN ACCORDANCE WITH CBC SECTION 3006.2. HOISTWAY OPENING PROTECTION IS REQUIRED ON ALL LEVELS EXCEPT THE LEVEL OF DISCHARGE (LEVEL 1) IN ACCORDANCE WITH CBC SECTION 3006.2 EXCEPTION 3. HOISTWAY OPENING PROTECTION WILL BE PROVIDED BY A SMOKE GUARD CURTAIN IN ACCORDANCE WITH CBC SECTION 3006.3 ITEM 5.

MECHANICAL HVAC SYSTEMS:

COMMON AREAS:

- CODE REQUIRED GARAGE MECHANICAL VENTILATION.
- STORAGE, TRASH, MECHANICAL AND ELECTRICAL ROOMS WITH VENTILATION ONLY
- CORRIDOR VENTILATION WITH SUPPLY AND EXHAUST AIR
- RETAIL SPACE HVAC UNDER FUTURE TENANT IMPROVEMENT AND IS EXCLUDED FROM THIS DESIGN

RESIDENTIAL UNIT HVAC:

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- THE INDOOR FANCOILS ARE MAINLY CONCEALED DUCTED UNITS BRINGING CONDITIONED AIR TO THE LIVING SPACES AND BEDROOMS
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CARBON MONOXIDE AND SMOKE ALARM PROVISIONS AS PER:

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2 HOUR SURVIVABILITY PROVISION AS PER:

NFPA 72
24.3.13.7 Two-way in-building wired emergency communications systems shall have a pathway survivability of Level 2 or Level 3.

ADDENDUM "F" (ELEVATOR CHECKLIST)	
BUILDING HEIGHT (Top of M.P. Floor)	65 FEET
LOWRISE(LR) - HIGHRISE(HR) # OF STORIES	L/R - 7
NEW OR EXISTING BUILDING	NEW
ELEVATOR I.D. #	#1
LEVELS SERVED BY ELEVATOR	B1 - ROOF
PASSENGER ELEVATOR	YES
ELEVATOR RISE	78 FT 7 IN
ELEVATOR SPEED	200 F.P.M.
ELEVATOR CAR PLATFORM DIMENSIONS	77.5" X 65.5"
ELEV. CAR CAN ACCOMMODATE AMBULANCE STRETCHER 24" X 84"	YES
ELEVATOR HOISTWAY CONSTRUCTION: COMBUSTIBLE/NC 1-HR/NC 2-HR	NC 2-HR
ELEVATOR MACHINE RM. CONSTRUCTION: COMBUSTIBLE/NC 1-HR/NC 2-HR	OTIS GEN2
ELEVATOR CONTROL RM. CONSTRUCTION: COMBUSTIBLE/NC 1-HR/NC 2-HR	NO
FIRE SERVICE ACCESS ELEVATOR	NO
OCCUPANT EVACUATION ELEVATOR	NO
SERVICE ELEVATOR	YES
TRACTION DRIVE	YES
TRACTION SUSPENSION MEANS (FT-1 RATED STEEL COATED BELTS)	YES
ELEVATOR HAS MACHINE ROOM	NO
ELEVATOR HAS CONTROL ROOM	YES
ELEV. IS A MACHINE ROOM-LESS TYPE (M.R.L.)	YES
CONTROL ROOM HAS SPRINKLER PROTECTION	NO
TOP OF HOISTWAY HAS SPRINKLER PROTECTION	NO
ELEVATOR PIT HAS SPRINKLER PROTECTION	YES
CONTROL RM. HAS SMOKE DETECTION PROTECTION	NO
ELEVATOR PIT HAS SMOKE DETECTION PROTECTION	NO
TOP OF HOISTWAY HAS HEAT DETECTION	NO
ELEVATOR PIT HAS HEAT DETECTION	NO
LOCATION OF ELEVATOR CONTROL ROOM	ROOF
LOCATION OF ELEVATOR CONTROLLER	ROOF
ELEV. CAB IS PROVIDED W/ PHASE 2 KEYED SWITCH	YES
WRITING ON PHASE 1 RECALL KEYED SWITCH	YES
WRITING ON PHASE 2 KEYED SWITCH	1ST FLR RES. LOBBY
ELEV. CAB IS PROVIDED W/ FIREFIGHTERS LIGHT SYMBOL	RESET/OFF
LOCATION OF PHASE 1 RECALL KEYED SWITCH	OFF/HOLD/DO
ELEVATOR CAB KEYED SWITCH IS BEHIND LOCKED DOOR INSIDE THE CAB	YES
ELEVATOR IS PROVIDED WITH PHASE 1 AUTOMATIC RECALL TO PRIMARY LEVEL	YES
ELEVATOR IS PROVIDED WITH PHASE 1 AUTOMATIC RECALL TO ALTERNATE LEVEL	YES

PROJECT DESCRIPTION:

THE PROJECT SITE IS LOCATED ON THE WEST SIDE OF VAN NESS AVENUE, ON THE BLOCK BOUNDED BY FILBERT STREET TO THE NORTH, VAN NESS AVENUE TO THE EAST, UNION STREET TO THE SOUTH, AND FRANKLIN STREET TO THE WEST. THE SITE IS CURRENTLY OCCUPIED BY A TWO-STORY, 3,885-SQUARE-FOOT (SF) COMMERCIAL BUILDING, BUILT IN 1942 AND THE SUBJECT LOT AREA MEASURES 11,025 SF. THE PROPOSAL IS TO DEMOLISH THE EXISTING COMMERCIAL BUILDING AND CONSTRUCT A SEVEN-STORY-OVER-BASEMENT, 65-FOOT-TALL MIXED-USE BUILDING, STAR PENTHOUSES, ROOF DECK FEATURES AND MECHANICAL ENCLOSURES WOULD EXTEND APPROXIMATELY 10 FEET ABOVE ROOF LEVEL. THE PROPOSED BUILDING WOULD CONTAIN 28 DWELLING UNITS (6 THREE-BEDROOM UNITS AND 20 TWO-BEDROOM UNITS), 2 ONE-BEDROOM + DEN UNITS), 1,120 SF OF RETAIL SPACE AT GROUND LEVEL, AND 14 OFF-STREET PARKING SPACES IN A BASEMENT GARAGE ACCESSED FROM A CURB CUT ALONG VAN NESS AVENUE. 100% OF OFF STREET PARKING SPACES WILL BE CAPABLE OF PROVIDING FUTURE EV CHARGING PER SFFD 4.108.4. THE BASEMENT LEVEL WOULD REACH APPROXIMATELY 14 FEET BELOW GRADE AND ADDITIONAL EXCAVATION FOR FOUNDATION MAY BE REQUIRED.

ZONING:

RC-3 (RESIDENTIAL-COMMERCIAL) MEDIUM DENSITY USE DISTRICT
65-A HEIGHT AND BULK DISTRICT

FAR REQUIREMENT PER 124.0 OF THE SAN FRANCISCO PLANNING CODE:

GROSS BUILDABLE SITE AREA 10,966
GROSS FLOOR AREA ABOVE GRADE W/ EXCLUSIONS 52,256
CALCULATED FAR REQUIRED 4.7
EXEMPT

(b) In R, RC, NC, and Mixed Use Districts, floor area ratio limits shall not apply to dwellings or to other residential uses.

SITE COVERAGE:

SITE AREA 10,966
BUILDING FOOTPRINT 8,117
CALCULATE SITE COVERAGE 74%

SETBACK REQUIREMENTS PER 134.0 OF THE SAN FRANCISCO PLANNING CODE:

SIDE YARD SETBACK PROVIDED REQUIRED 0'-4" n/a
FRONT YARD SETBACK PROVIDED REQUIRED 0'-5" n/a
REAR YARD SETBACK (25% OF LOT DEPTH, FIRST RESIDENTIAL LEVEL & ABOVE PER SF PLANNING CODE 209.3) PROVIDED REQUIRED 32'-4" (128.92' x 0.25 = 32'-5.73') 32'-5.73' (MIN.)

OPEN SPACE REQUIREMENT PER 135.0 OF THE SAN FRANCISCO PLANNING CODE:

PRIVATE OPEN SPACE REQUIRED 60 SF PER UNIT (15 UNITS X 60 = 900 SF)
COMMON OPEN SPACE EQUIVALENT REQUIRED 80 SF PER UNIT (13 UNITS X 80 = 1040 SF)
PRIVATE OPEN SPACE PROVIDED 2,333 SF
COMMON OPEN SPACE PROVIDED 5,241 SF
TOTAL OPEN SPACE PROVIDED 2,333 + 5,241 = 7,574 SF
AVERAGE PER UNIT 270 SF (7,514 SF / 28 UNITS)

BETTER ROOF

THE DESIGN UTILIZES THE "LIVING ROOF ALTERNATE" TO MEET THE "BETTER ROOF" CRITERIA.

BETTER ROOF REQUIRED 15% OF ROOF AREA
LIVING ROOF ALTERNATE 30% OF ROOF AREA
TOTAL ROOF AREA 7,229 SF
TOTAL LIVING ROOF REQ. 2,169 SF (30%)
TOTAL LIVING ROOF PROVIDED 2,169 SF (30%)

SITE EXCAVATION

EXCAVATION DEPTH APPROX. 14'-0" PLUS FOOTINGS
EXCAVATION VOLUME APPROX. 134,596 CUFT

VEHICULAR PARKING

Proposed Program (Dedicated)
RESIDENTIAL UNITS 0.5
MULTIPLIER 28
SPACES 14
COMMENTS
TOTAL 14
ACCESSIBLE 0.2
1 VAN ADA ASSIGNED
CLEAR AIR VEHICLE STALL 2 DEDICATED
Total Residential Parking Provided 14

BICYCLE PARKING

PROPOSED PROGRAM
RESIDENTIAL UNITS 28 UNITS
CLASS I (LONG TERM) 1/UNIT 28 SPACES
CLASS II (SHORT TERM) 120 UNITS 1 SPACE
RETAIL 1,157 SF
1/7,500 SF 0 SPACES
1/2,500 SF 2 SPACES
Total Bike Parking Required 28
Total Bike Parking Provided 3

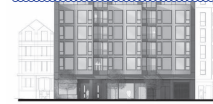
GROSS FLOOR AREA														
Area (Square Feet)														
Level	Description	No. Residential Units	Residential Units	Residential Parking	Common Circulation	Bldg. Service and Storage	Commercial	Tenant Storage	Bike Storage	Balc./Roof Deck	Total Gross (Without Exclusions)	Bldg. Service and Storage	Bike Storage	Total Gross With Exclusions
Basement	Parking/Storage/Services				5,930	945					10,376	2,771	404	13,551
Level 1	Residential/Commercial	3	3,774	443	2,136	252	1,120				7,725			7,725
Level 2	Residential	3	3,774		962	176					4,912			4,912
Level 3	Residential	5	6,488		935	112				345	7,880			7,535
Level 4	Residential	5	6,488		935	112				345	7,880			7,535
Level 5	Residential	4	6,608			874				255	7,849			7,594
Level 6	Residential	4	6,608			874				255	7,849			7,594
Level 7	Residential	4	6,500			874				255	7,741			7,486
Roof	Roof Deck				460	66					7,229	66		7,295
Totals		28	40,240	6,373	8,995	3,825	1,120	895	335	8,684	70,467	2,837	404	82,621

RESIDENTIAL UNIT AREA SUMMARY / OPEN SPACE (SF)													
Unit Type	Unit A-1		Unit A-2 & (Unit A-2 Alt.)		Unit B-1		Unit B-2		Unit C-1		Unit C-2		Totals
	Conditioned	Private Open	Conditioned	Private Open	Conditioned	Private Open	Conditioned	Private Open	Conditioned	Private Open	Conditioned	Private Open	
Gross Area	1193	395	1166 (1213)	67	1397	533	1423	119	1347	0	1293	0	1931
# of Units	4		2 & (2 Alt. Units)		2		2		8		2		6
# Units with Private Open		2		4		1		2		0		0	6
Total Conditioned Space	4772		4758		2794		2846		10776		2586		40116 SF
Total Private Open Space		790		288		533		238		0		0	2333 SF
Total Common Open Space													6679 SF

RESIDENTIAL UNIT TABULATION						HABITABLE SPACE, SF*
LEVEL	1BR+ 2.5BA	2BR 2BA	2BR 2.5BA	3BR 3.5BA	TOTAL UNITS	
7			2	2	4	3400
6			2	2	4	3507
5			2	2	4	3507
4		3	2		5	3,677
3		3	2		5	3,677
2	1	1	1	1	3	2,225
1	1	1	1	1	3	2,225
TOTAL	2	8	12	6	28	22,216

* CBC 202 DEFINITION: "HABITABLE SPACE: A SPACE IN A BUILDING FOR LIVING, SLEEPING, EATING OR COOKING. BATHROOMS, TOILET ROOMS, CLOSETS, HALLS, STORAGE OR UTILITY SPACES AND SIMILAR AREAS ARE NOT CONSIDERED HABITABLE SPACES. COMMON AREAS ARE NOT INCLUDED IN THIS DEFINITION.

PROJECT DESCRIPTION HAS BEEN UPDATED TO MATCH APPROVED UNIT TABULATION AND AREA CHARTS FROM REV D



2525 VAN NESS
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SAN FRANCISCO, CA 94109

CLIENT
BOULBOUFFE LLC
3456 SACRAMENTO STREET
SAN FRANCISCO, CA 94118

ARCHITECT
HANDEL ARCHITECTS, LLP
35 MARKET ST.
2ND FLOOR
SAN FRANCISCO, CA 94103
415-495-5588

REV.	DESCRIPTION	DATE
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PROJECT
INFORMATION AND
STATISTICS

G002A



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ADDENDUM "F" (ELEVATOR CHECKLIST)	
BUILDING HEIGHT (Top of M.P. Floor)	65 FEET
LOWRISE(LR) - HIGHRISE(HR) # OF STORIES	L/R - 7
NEW OR EXISTING BUILDING	NEW
ELEVATOR I.D. #	#1
LEVELS SERVED BY ELEVATOR	B1 - ROOF
PASSENGER ELEVATOR	YES
ELEVATOR RISE	78 FT 7 IN
ELEVATOR SPEED	200 F.P.M.
ELEVATOR CAR PLATFORM DIMENSIONS	77.5" X 65.5"
ELEV. CAR CAN ACCOMMODATE AMBULANCE STRETCHER 24" X 84"	YES
ELEVATOR HOISTWAY CONSTRUCTION: COMBUSTIBLE/NC 1-HR/NC 2-HR	NC 2-HR
ELEVATOR MACHINE RM. CONSTRUCTION: COMBUSTIBLE/NC 1-HR/NC 2-HR	OTIS GEN2
ELEVATOR CONTROL RM. CONSTRUCTION: COMBUSTIBLE/NC 1-HR/NC 2-HR	NO
FIRE SERVICE ACCESS ELEVATOR	NO
OCCUPANT EVACUATION ELEVATOR	NO
SERVICE ELEVATOR	YES
TRACTION DRIVE	YES
TRACTION SUSPENSION MEANS (FT-1 RATED STEEL COATED BELTS)	YES
ELEVATOR HAS MACHINE ROOM	NO
ELEVATOR HAS CONTROL ROOM	YES
ELEV. IS A MACHINE ROOM-LESS TYPE (M.R.L.)	YES
CONTROL ROOM HAS SPRINKLER PROTECTION	NO
TOP OF HOISTWAY HAS SPRINKLER PROTECTION	NO
ELEVATOR PIT HAS SPRINKLER PROTECTION	YES
CONTROL RM. HAS SMOKE DETECTION PROTECTION	NO
ELEVATOR PIT HAS SMOKE DETECTION PROTECTION	NO
TOP OF HOISTWAY HAS HEAT DETECTION	NO
ELEVATOR PIT HAS HEAT DETECTION	NO
LOCATION OF ELEVATOR CONTROL ROOM	ROOF
LOCATION OF ELEVATOR CONTROLLER	ROOF
ELEV. CAB IS PROVIDED W/ PHASE 2 KEYED SWITCH	YES
WRITING ON PHASE 1 RECALL KEYED SWITCH	YES
WRITING ON PHASE 2 KEYED SWITCH	1ST FLR RES. LOBBY
ELEV. CAB IS PROVIDED W/ FIREFIGHTERS LIGHT SYMBOL	RESET/OFF
LOCATION OF PHASE 1 RECALL KEYED SWITCH	OFF/HOLD/ON
ELEVATOR CAB KEYED SWITCH IS BEHIND LOCKED DOOR INSIDE THE CAB	YES
ELEVATOR IS PROVIDED WITH PHASE 1 AUTOMATIC RECALL TO PRIMARY LEVEL	YES
ELEVATOR IS PROVIDED WITH PHASE 1 AUTOMATIC RECALL TO ALTERNATE LEVEL	YES

PROJECT DESCRIPTION:

THE PROJECT SITE IS LOCATED ON THE WEST SIDE OF VAN NESS AVENUE, ON THE BLOCK BOUNDED BY FILBERT STREET TO THE NORTH, VAN NESS AVENUE TO THE EAST, UNION STREET TO THE SOUTH, AND FRANKLIN STREET TO THE WEST. THE SITE IS CURRENTLY OCCUPIED BY A TWO-STORY 9,880-SQUARE-FOOT (SQ) COMMERCIAL BUILDING, BUILT IN 1942 AND THE SUBJECT LOT AREA MEASURES 11,025 SF. THE PROPOSAL IS TO DEMOLISH THE EXISTING COMMERCIAL BUILDING AND CONSTRUCT A SEVEN-STORY OVER BASEMENT, 65-FOOT-TALL MIXED-USE BUILDING. STAIR PENHOUSES, ROOF DECK FEATURES AND MECHANICAL ENCLOSURES WOULD EXTEND APPROXIMATELY 10 FEET ABOVE ROOF LEVEL. THE PROPOSED BUILDING WOULD CONTAIN 28 DWELLING UNITS (6 THREE-BEDROOM UNITS AND 20 TWO-BEDROOM UNITS 2 ONE-BEDROOM + DEN UNITS), 1,120 SF OF RETAIL SPACE AT GROUND LEVEL, AND 14 OFF-STREET PARKING SPACES IN A BASEMENT GARAGE ACCESSED FROM A CURB CUT ALONG VAN NESS AVENUE. 100% OF OFF STREET PARKING SPACES WILL BE CAPABLE OF PROVIDING FUTURE EV CHARGING PER SF/BC 4.189.4. THE BASEMENT LEVEL WOULD REACH APPROXIMATELY 14 FEET BELOW GRADE AND ADDITIONAL EXCAVATION FOR FOUNDATION MAY BE REQUIRED.

ZONING:

RC-3 (RESIDENTIAL-COMMERCIAL) MEDIUM DENSITY USE DISTRICT
65-A HEIGHT AND BULK DISTRICT

FAR REQUIREMENT PER 124.0 OF THE SAN FRANCISCO PLANNING CODE:

GROSS BUILDABLE SITE AREA 10,986
GROSS FLOOR AREA ABOVE GRADE W/ EXCLUSIONS 52,256
CALCULATED FAR REQUIRED 4.7
EXEMPT

(b) In R, RC, NC, and Mixed Use Districts, floor area ratio limits shall not apply to dwellings or to other residential uses.

SITE COVERAGE:

SITE AREA 10,986
BUILDING FOOTPRINT 8,117
CALCULATE SITE COVERAGE 74%

SETBACK REQUIREMENTS PER 134.0 OF THE SAN FRANCISCO PLANNING CODE:

SIDE YARD SETBACK PROVIDED 0'-4" REQUIRED n/a
FRONT YARD SETBACK PROVIDED 0'-8" REQUIRED n/a
REAR YARD SETBACK (25% OF LOT DEPTH, FIRST RESIDENTIAL LEVEL & ABOVE PER SF PLANNING CODE 209.3) PROVIDED 35'-4" REQUIRED (128.92' x 0.25 = 32'-5.75') 32'-5.75' (MIN.)

OPEN SPACE REQUIREMENT PER 135.0 OF THE SAN FRANCISCO PLANNING CODE:

PRIVATE OPEN SPACE REQUIRED 60 SF PER UNIT (15 UNITS X 60 = 900 SF)
COMMON OPEN SPACE EQUIVALENT REQUIRED 80 SF PER UNIT (13 UNITS X 80 = 1040 SF)
PRIVATE OPEN SPACE PROVIDED 2,333 SF
COMMON OPEN SPACE PROVIDED 5,241 ROOF COMMON OPEN SPACE
TOTAL OPEN SPACE PROVIDED 2,333 + 5,241 = 7,574 SF
AVERAGE PER UNIT 270 SF (7,514 SF / 28 UNITS)

BETTER ROOF

THE DESIGN UTILIZES THE "LIVING ROOF ALTERNATE" TO MEET THE "BETTER ROOF" CRITERIA.

BETTER ROOF REQUIRED 15% OF ROOF AREA
LIVING ROOF ALTERNATE 30% OF ROOF AREA
TOTAL ROOF AREA 7,229 SF
TOTAL LIVING ROOF REQ. 2,169 SF (30%)
TOTAL LIVING ROOF PROVIDED 2,169 SF (30%)

SITE EXCAVATION

EXCAVATION DEPTH APPROX. 14'-0" PLUS FOOTINGS
EXCAVATION VOLUME APPROX. 134,596 CUFT

VEHICULAR PARKING

Proposed Program (Dedicated)
RESIDENTIAL MULTIPLIER 0.5 # SPACES 14 COMMENTS
TOTAL 28
ACCESSIBLE 0.2 1 VAN ADA ASSIGNED
CLEAR AIR VEHICLE STALL 2 DEDICATED
Total Residential Parking Provided 14

BICYCLE PARKING

PROPOSED PROGRAM CLASS (LONG TERM) CLASS II (SHORT TERM)
RESIDENTIAL 10/UNIT 28 SPACES 120 UNITS 1 SPACE
RETAIL 1,157 SF 1/7,500 SF 0 SPACES 1/2,500 SF 2 SPACES MIN. 2
Total Bike Parking Required 3
Total Bike Parking Provided 3

RESIDENTIAL UNIT TABULATION						HABITABLE SPACE, SF*
LEVEL	1BR+ 2.5BA	2BR 2BA	2BR 2.5BA	3BR 3.5BA	TOTAL UNITS	
7			2	2	4	3400
6			2	2	4	3507
5			2	2	4	3507
4		3	2		5	3,677
3		3	2		5	3,677
2	1	1	1		3	2,225
1	1	1	1		3	2,225
TOTAL	2	8	12	6	28	22,218

* CBC 202 DEFINITION: "HABITABLE SPACE: A SPACE IN A BUILDING FOR LIVING, SLEEPING, EATING OR COOKING. BATHROOMS, TOILET ROOMS, CLOSETS, HALLS, STORAGE OR UTILITY SPACES AND SIMILAR AREAS ARE NOT CONSIDERED HABITABLE SPACES. COMMON AREAS ARE NOT INCLUDED IN THIS DEFINITION.



2525 VAN NESS

2525 VAN NESS
SAN FRANCISCO, CA 94109

CLIENT
BOUBOUFFIE LLC
3456 SACRAMENTO STREET
SAN FRANCISCO, CA 94118

ARCHITECT
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35 MARKET ST.
2ND FLOOR
SAN FRANCISCO, CA 94103
415-495-5588

REV.	DESCRIPTION	DATE
G	PG&E REV1	06-25-2021
D	ALTERED SITE PERMIT - REV D	11-19-2020
	ADDENDUM 2 ARCH/MEP	11-19-2020
C	ALTERED SITE PERMIT - REV C	10-22-2020
	90% CONSTRUCTION DOCUMENTS	11-04-2020
	60% CONSTRUCTION DOCUMENTS	09-21-2020
	30% CONSTRUCTION DOCUMENTS	08-20-2020
A	ALTERED SITE PERMIT - REV A	08-07-2020

KEY PLAN

STAMP



SCALE 1/4" = 1'-0"
DRAWN BY: HA
DATE: 8/29/2020

PROJECT INFORMATION AND STATISTICS

G002A

2525 VAN NESS
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KEY PLAN

APPROVED
Dept. of Building Insp.
- San Francisco -

March 01, 2021
202003066413_ALTER SITE DWGS


PATRICK O'RIORDAN
INTERIM DIRECTOR
DEPT. OF BUILDING INSPECTION

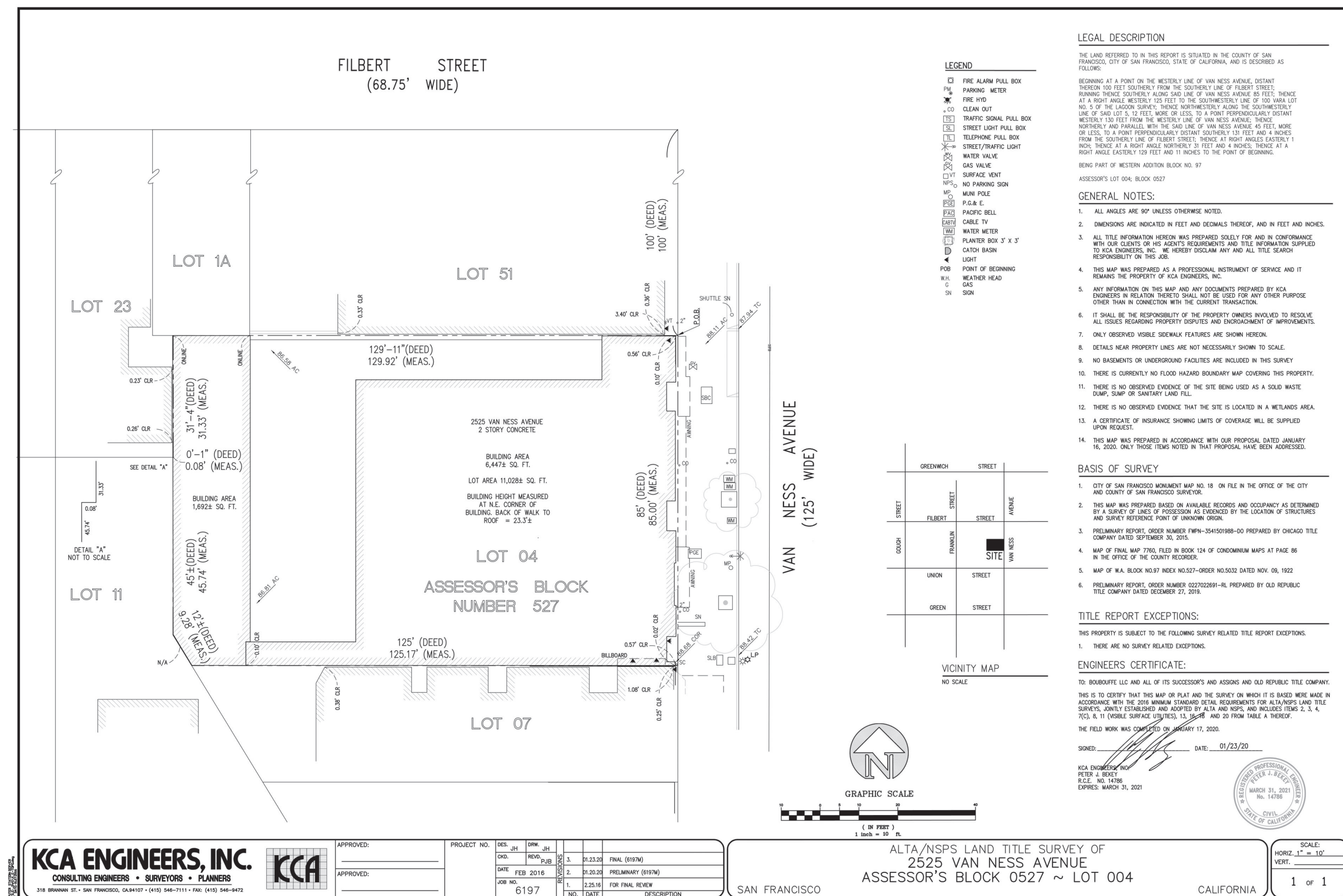
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DATE: 8/20/2020

SURVEY

ALTA1-1





2525 VAN NESS

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VICINITY PLAN EXISTING SITE PHOTOS

G004A

2525 VAN NESS
2525 VAN NESS
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KEY PLAN

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Dept. of Building Insp.
- San Francisco -
March 01, 2021
202003066413_ALTER SITE DWGS
Patrick J. Riordan
PATRICK J. RIORDAN
INTERIM DIRECTOR
DEPT. OF BUILDING INSPECTION

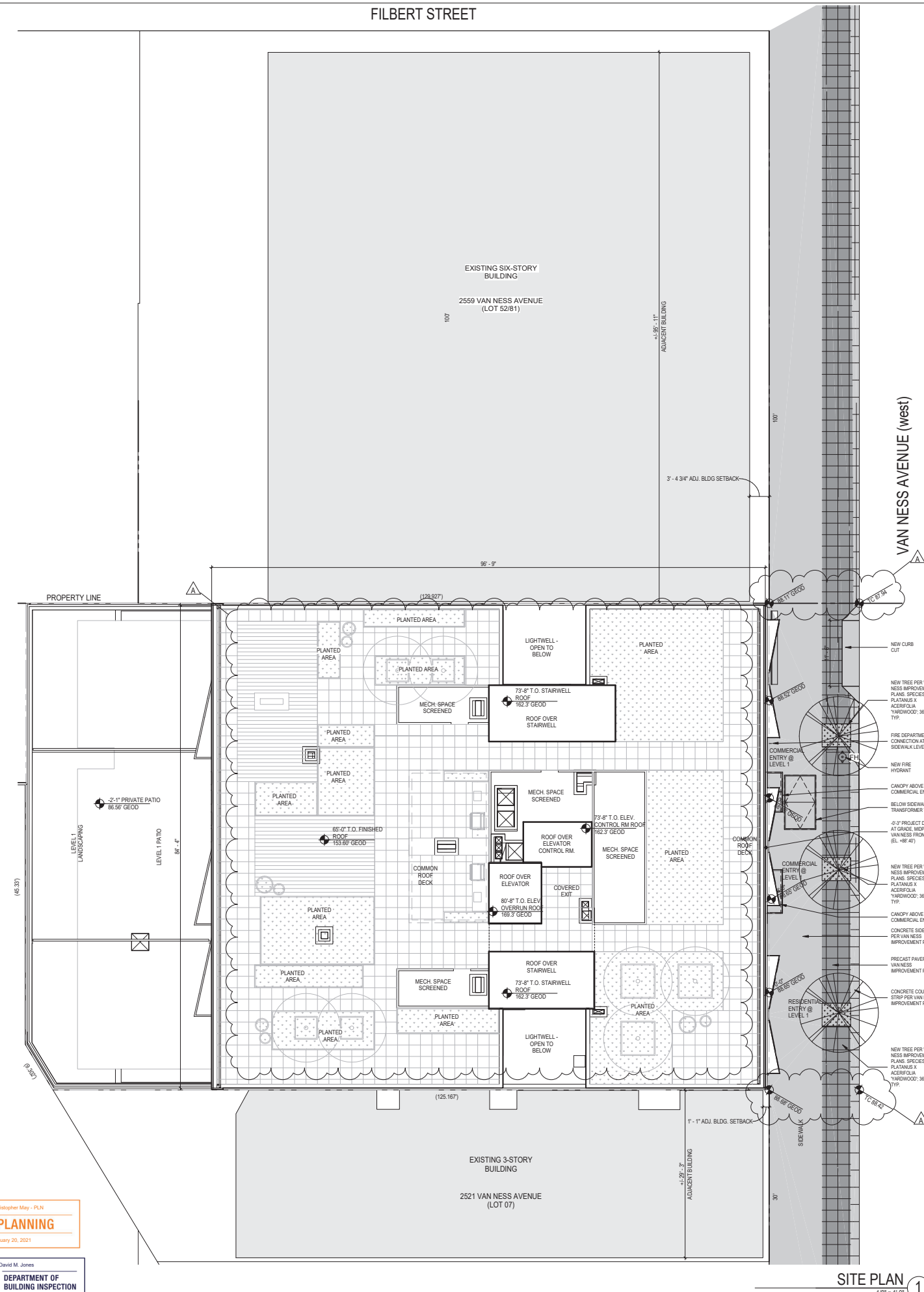
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SCALE 1/8" = 1'-0"
DRAWN BY: HA
DATE: 8/20/2020

PLOT PLAN

G003A



SITE PLAN 1
1/8" = 1'-0"





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CHAIN LINK FENCE STORAGE
PARTITION. PROVIDE CHAIN LINK
CEILING AT 8'-0". 3'-0" GATES TYPE
WITH PADLOCK CLASP (9 STALLS).
ACCESSIBLE DOOR HARDWARE AND
10" KICKPLATE TO PROVIDE
SMOOTH BOTTOM.

REV.	DESCRIPTION	DATE
D	ALTERED SITE PERMIT - REV D 90% CONSTRUCTION DOCUMENTS	11-19-20 11-04-20
B	ALTERED SITE PERMIT - REV B 60% CONSTRUCTION DOCUMENTS 30% CONSTRUCTION DOCUMENTS	09-28-20 09-21-20 08-20-20
A	ALTERED SITE PERMIT - REV A	08-07-20

KEY PLAN



March 01, 2021
202003086413 ALTER SITE DW

212

PATRICK O'RIORDAN
INTERIM DIRECTOR
DEPT. OF BUILDING INSPECT

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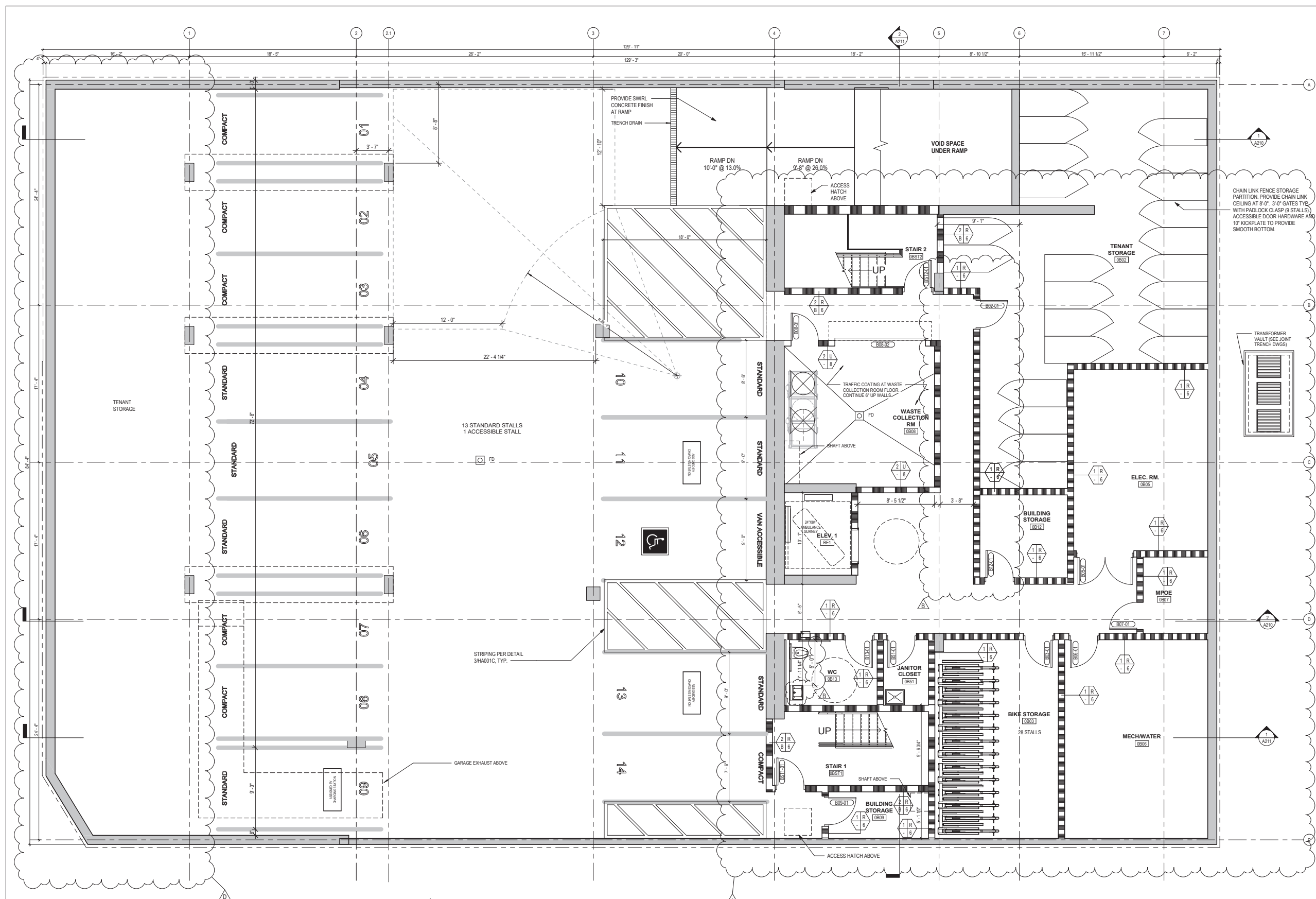


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DRAWN BY: HA
DATE: 8/20/2020

BASEMENT LEVEL
FLOOR & SLAB EDGE
PLAN

A100-1

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GENERAL NOTES

- REFER TO GLOSSARY FOR ABBREVIATIONS, AND GENERAL NOTES.
- REFER TO SHEETS 9000 FOR HANDICAPPED ACCESSIBILITY STANDARDS.
- REFER TO SHEET 9001 FOR PARTITION TYPE SCHEDULE.
- REFER TO SHEET 600A, 600B, AND 6007A FOR MATERIAL FINISH, EQUIPMENT, FURNITURE, AND ACCESSORY SCHEDULES.
- PARTITIONS @ PERMITTER TO BE PERPENDICULAR TO EXTERIOR WALL ASSEMBLY, U.O.N., AND THE FINISHED FACE OF GYP IS CENTERED ON MILLION, U.O.N.
- DIMENSIONS OF INTERIOR PARTITIONS ARE MEASURED FROM GRID LINES. SHIRT CHANGES, P.O. CONC. & P.O. PARTITIONS. SEE SLAB PLAN SHEETS FOR DIMENSIONS TO SLAB OPENINGS.
- EDGE OF DOOR FRAMES ARE LOCATED @ FROM ADJACENT WALL ON HINGE SIDE, OR DOOR CHANGES, P.O. CONC. & P.O. PARTITIONS. SEE SLAB PLAN SHEETS FOR DIMENSIONS TO SLAB OPENINGS.
- DOOR TAGS ARE UNIQUE TO EACH ROOM AT RESIDENTIAL LEVELS. DOOR TAGS ARE A PART OF THE PARTITION TYPE SCHEDULE.
- FINISHED FLOORING TO RUN UNDERNEATH REFRIGERATORS, DISHWASHERS, AND SINK BASE CUPBORDS.

10. FOR UNIT ELECTRICAL PANEL LOCATION ON WALLS LESS THAN 4'-0" PANELS SHALL BE TOP CENTERED ON THE WALL. FOR ALL WALLS MORE THAN 4'-0" LOCATE PANEL 1'-0" FROM EDGE OF WALL. REFER TO ARCHITECTURAL DRAWINGS FOR GENERAL LOCATION. CONTRACTOR TO ALSO COORDINATE WITH ELECTRICAL DRAWINGS.
11. EXPOSED STRUCTURAL WALLS AND COLUMNS WITHFURNISHING, AND CEILINGS ARE TO BE SKIM COATED AND PAINTED TO MATCH ADJACENT WALL.
12. GC AND DESIGNER SHALL PROVIDE MEANS TO PROVIDE METHOD OF PREVENTING WATER FROM ENTERING THE FIRE SERVICE ACCESS ELEVATOR LOBBY AND FIRE SERVICE ELEVATOR PER 2016 IBC CODE 3007.3.10. FLOOR PROFILE TRECH DRAWS WITH ARCHITECTURAL DRAWN PLATE / GRADE SHOWN IN DRAWINGS. SLOPE AWAY FROM LOBBIES OR OTHER PROVISIONS MAY BE REQUIRED FOR DRAINAGE BY THE ARCHITECT.
13. ACQUISITION OF PERMITS FOR EXTERIOR SLOPE TO BE PER SHEETS GAGA.
14. REFER TO LANDSCAPE DRAWINGS FOR ALL INFORMATION PERTAINING TO LANDSCAPING DESIGN SHOWN ON ARCHITECTURAL DRAWINGS.
15. THERE SHALL BE NO DIRECT CONTACT BETWEEN MECH. DUCTS / PLUMBING PIPES AND BUILDING STRUCTURE OR FINISHES. SEE MECH. CLEARANCE REQUIREMENTS.
16. REFER TO ELECTRICAL DRAWING FOR ALL EXIST SIGNS AND LIFE SAFETY DEVICES.

17. ROOF DRAINS INSTALLED AT SLAB LEVEL, BELOW PAVER FINISH, DRAINS NOT SHOWN IN PLAN. REFER TO SLAB EDGE DRAWINGS FOR DRAIN LAYOUT AND INTEGRAL SLOPE TO DRAIN SLAB SURFACE LAYOUT.
18. TWO-WAY COMMUNICATION SYSTEM SHALL BE PROVIDED AT THE ELEVATOR LANDING FOR ANY ACCESSIBLE FLOOR THAT IS ONE OR MORE STORIES ABOVE OR BELOW THE STORY OF EXIT. THE FOLLOWING CONCERNING THE COMMUNICATION SYSTEM SHALL BE SUBMITTED:
 - 20. ALL REQUIRED SHARED SPEAKER AND RESIDENT AREAS ARE REPRESENTED IN THIS SUBMISSION. REFER TO THE UPDATED MEP NARRATIVE FOR RESIDENT SYSTEMS STRATEGIES. ALL EXPOSED STRUCTURAL ELEMENTS AT FOC COMMON AND REVENED AREAS SHALL RECEIVE FINISH WALL. FURNISH ALL STRUCTURAL ELEMENTS AT FOC COMMON AND REVENED AREAS SHALL RECEIVE FINISH WALL FURNISHING.
 - 20. A MINIMUM VERTICAL CLEARANCE OF 114" SHALL BE PROVIDED FROM VEHICULAR ENTRANCES TO THE PASSENGER DROP-OFF AND LOADING ZONE, AND CONTINUE TO THE VEHICULAR EXITS.
 - 21. MINIMUM WIDTH DIMENSIONED FOR CROWN WIDTHS ARE TO FACE OF FINISHES, DOOR THRESHOLD, AND FINISHES.

BASEMENT PLAN

$$\frac{1}{4}'' = 1':0''$$

David M. Jones



**DEPARTMENT OF
BUILDING INSPECTION**

Christopher May - PL



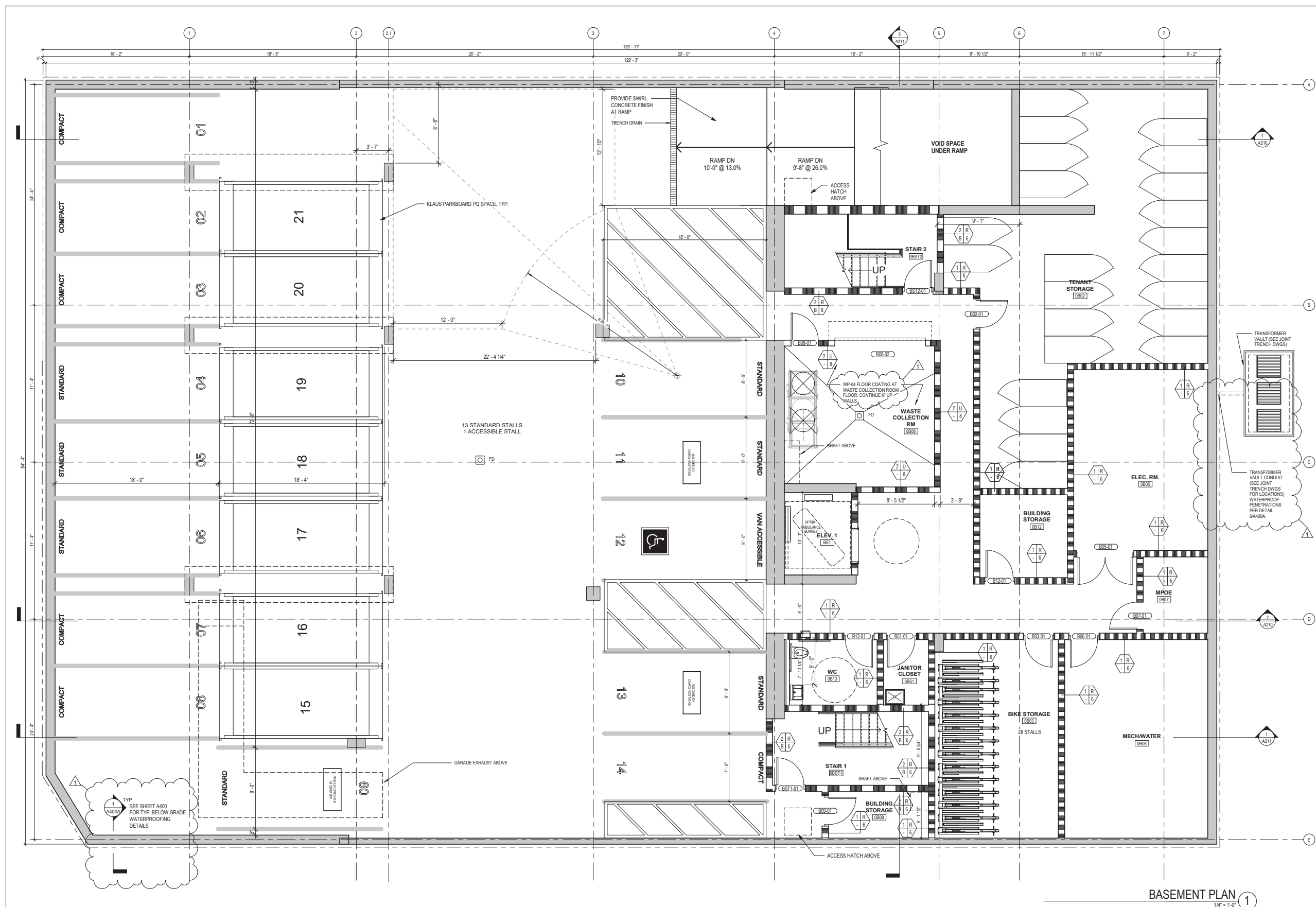
ARCHITECT
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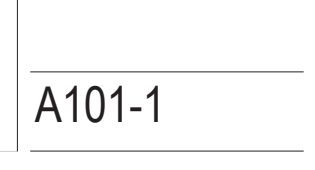
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BASEMENT LEVEL
FLOOR & SLAB EDGE
PLAN

A100-1





A101-1

17. ROOF DRAINS INSTALLED AT SLAB LEVEL BELOW PAVER FINISH. DRAINS NOT SHOWN IN PLAN. REFER TO SLAB EDGE DRAWINGS FOR DRAIN LAYOUT AND INTEGRAL SLOPE TO DRAIN SLAB SURFACE LAYOUT.
18. TWO - WAY COMMUNICATION SYSTEM SHALL BE PROVIDED AT THE ELEVATOR LANDING ON ALL ACCESSIBLE FLOOR THAT IS ONE OR MORE STORIES ABOVE OR BELOW THE STORY OF EXIT OR ENTRANCE AND TWO WITH AREAS 1000+.
19. NOT ALL REQUIRED SHAFR AND RISER ELEMENTS ARE REPRESENTED IN THIS SUBMISSION. REFER TO THE UPDATED MEP NARRATIVE FOR REVISED SYSTEMS STRATEGIES. ALL EXPOSED STRUCTURAL ELEMENTS FOR COMMON AND RESIDENT AREAS SHALL RECEIVE FINISH WALL FURRING. STRUCTURAL ELEMENTS AT COMMON AND RESIDENT AREAS SHALL RECEIVE FINISH WALL FURRING.
20. A MINIMUM VERTICAL CLEARANCE OF 114" SHALL BE PROVIDED FROM VEHICULAR ENTRANCES TO THE PASSENGER DROP-OFF AND LOADING ZONE, AND CONTINUE TO THE VEHICULAR EXITS.
21. MINIMUM WIDTHS DEDUCTED FOR CLOAK WIDTHS ARE TO FACE OF FINISHES, DOOR TRIM, AND BASEBOARDS WHERE PROVIDED.

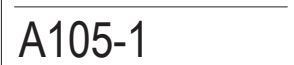
The image shows the front cover of a planning document. At the top, the title "LEVEL 01 PLAN" is written in a large, bold, black sans-serif font. To the right of the title, the scale "1/4\" = 1'-0\"" is printed. A circular graphic on the right side contains the number "1". Below the title, the text "Christopher May - PLAN" is centered. A large, light blue rectangular box with a thin black border contains the word "PLANNING" in a large, bold, black sans-serif font. Below this box, the date "January 20, 2021" is centered. On the left side, there is a circular seal of the City and County of Denver, featuring an eagle with a shield and the text "CITY AND COUNTY OF DENVER" and "1860 • 2020". At the bottom, the text "David M. Jones" is centered. Below this, a light blue rectangular box with a thin black border contains the text "DEPARTMENT OF BUILDING INSPECTION" in a bold, black sans-serif font. The seal of the City and County of Denver is also visible at the bottom left.

 David M. Jones
**DEPARTMENT OF
BUILDING INSPECTION**
January 29, 2021

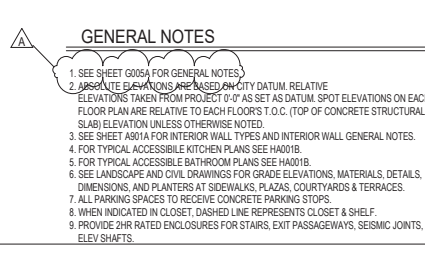
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 Christopher May - PLN
PLANNING
 January 20, 2021



 Christopher May - PLN
PLANNING
January 20, 2021



A107-1

 Christopher May - PLN
PLANNING
January 20, 2021

2525 VAN NESS
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REV.	DESCRIPTION	DATE
B	ALTERED SITE PERMIT - REV B	09-28-2020
	60% CONSTRUCTION DOCUMENTS	09-21-2020
	30% CONSTRUCTION DOCUMENTS	08-20-2020
A	ALTERED SITE PERMIT - REV A	08-07-2020

KEY PLAN

APPROVED
Dept. of Building Insp.
- San Francisco -

March 01, 2021
02003086413 ALTER SITE DWGS


PATRICK O'RIORDAN
INTERIM DIRECTOR
EPT. OF BUILDING INSPECTION

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DRAWN BY: HA
DATE: 8/20/2020

RENDERINGS

G004B



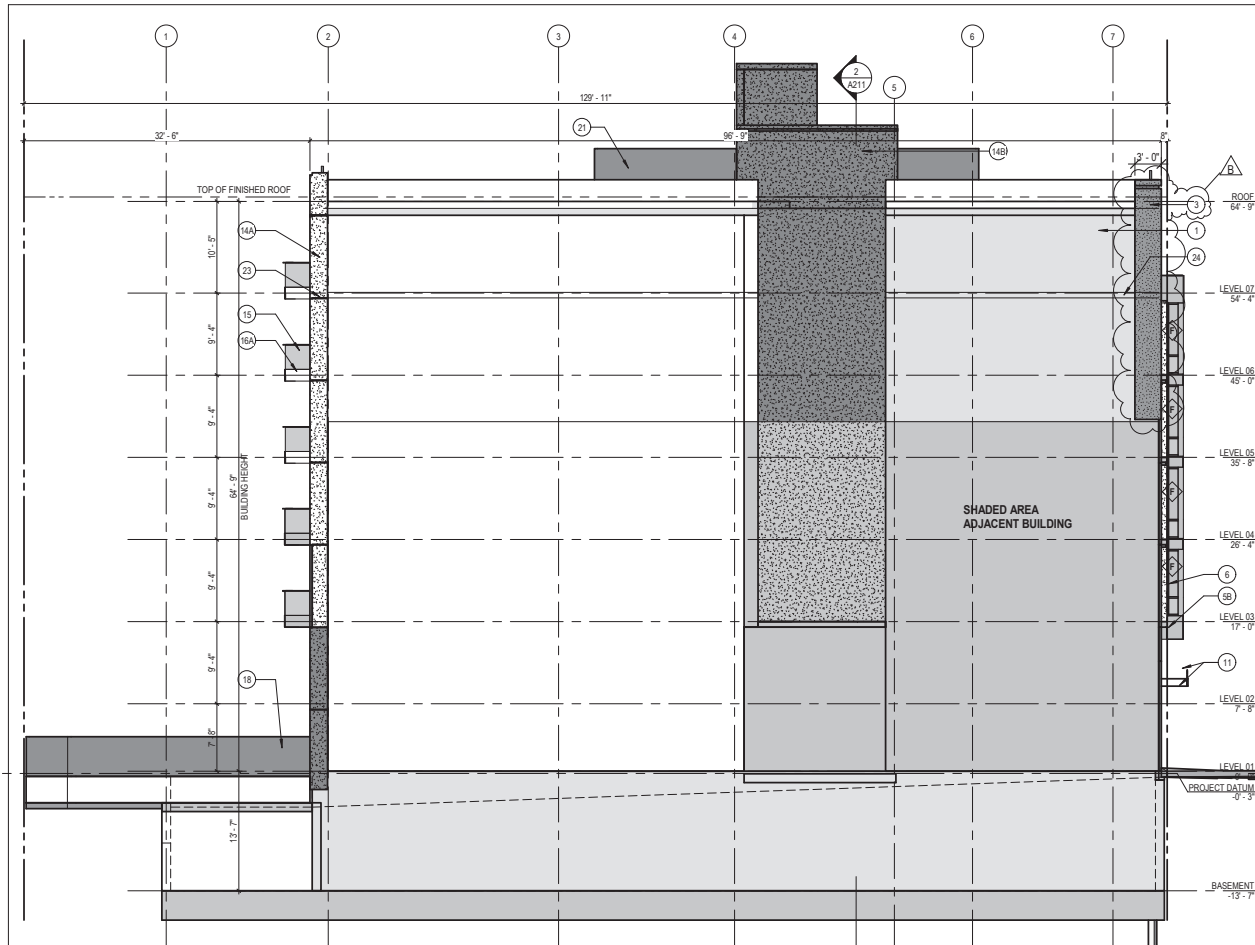
David M. Jones
DEPARTMENT OF
BUILDING INSPECTION
January 29, 2021



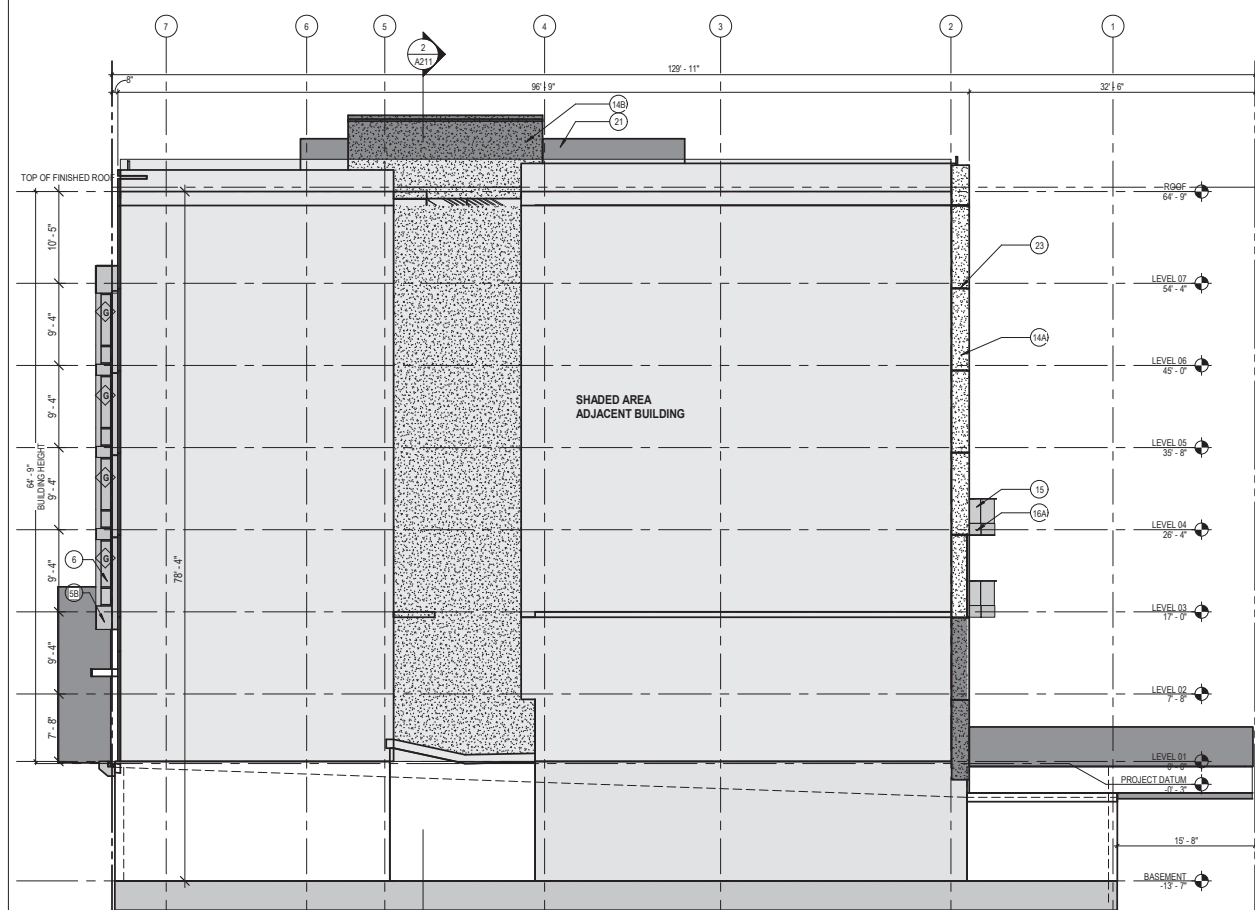
Christopher May - PLN

PLANNING

January 20, 2021



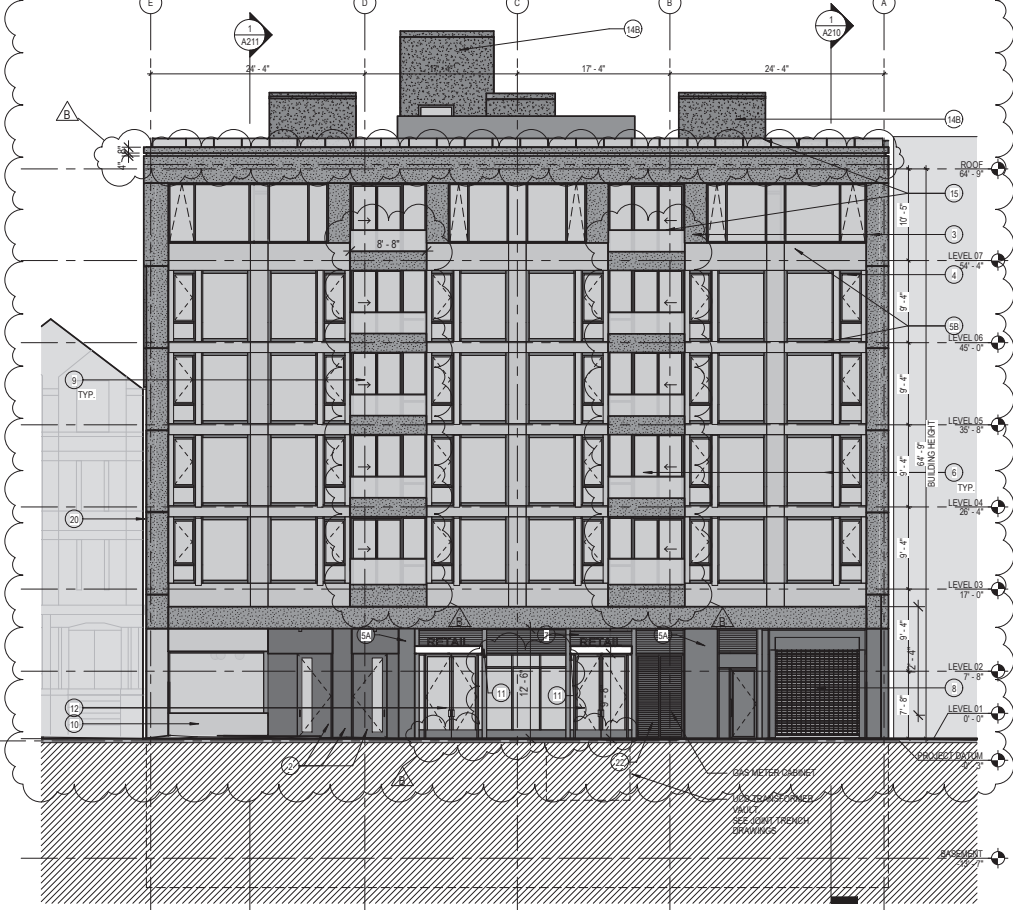
SOUTH ELEVATION 4
1/8" = 1'-0"



NORTH ELEVATION 2
1/8" = 1'-0"



WEST ELEVATION 3
1/8" = 1'-0"



EAST ELEVATION 1
1/8" = 1'-0"

GENERAL NOTES	
1	ELASTOMERIC COATING AT EXPOSED CONCRETE SURFACE
2	FLUTED BRONZE METAL PANEL
3	FIBER CEMENT PANEL CLADDING - EQUITONE NATURA N 014 FINISH
4	OPERABLE CASEMENT WINDOW IN WINDOW WALL
5A	COMPOSITE METAL PANEL - DARK GREY
5B	COMPOSITE METAL PANEL - DURANAR HARVEST GOLD PEARL
6	ANODIZED ALUMINUM WINDOW WALL WITH LOW-E VISION GLASS
7	ALUMINUM LOUVERS, PREFINISHED
8	OVERHEAD GARAGE DOOR
9	ALUMINUM BALCONY DOOR, GLAZED
10	STONE VENEER RAINSCREEN
11	ALUMINUM RETAIL CANOPY WITH SIGNAGE
12	ALUMINUM DOOR, GLAZED
13	PREFINISHED ALUMINUM STOREFRONT SYSTEM WITH LOW-E VISION IRON GLASS
14A	CEMENT PLASTER - DARK GREY
14B	CEMENT PLASTER - CHAMPAGNE
15	BALCONY GLASS GUARDRAIL
16A	COMPOSITE METAL PANEL AT BALCONY SLAB BYPASS - DARK GREY
16B	COMPOSITE METAL PANEL AT BALCONY SLAB BYPASS - CHAMPAGNE
17	ALUMINUM PATIO DOOR, GLAZED
18	IPE PATIO FENCE
19	ROOF DECK GLASS GUARD IN PREFINISHED ALUMINUM FRAME
20	VERTICAL ALUMINUM EXPANSION COVER
21	ALUMINUM SCREEN, DARK GREY
22	GAS METER CABINET WITH LOUVERED DOORS
23	CEMENT PLASTER CONTROL JOINT
24	ELASTOMERIC COATING AT EXPOSED CONCRETE SLAB EDGE

REV.	DESCRIPTION	DATE
B	ALTERED SITE PERMIT - REV B	09-29-2020
	60% CONSTRUCTION DOCUMENTS	09-21-2020
	30% CONSTRUCTION DOCUMENTS	08-20-2020
A	ALTERED SITE PERMIT - REV A	08-07-2020

2525 VAN NESS

2525 VAN NESS
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SCALE 1/8" = 1'-0"

DRAWN BY: HA

DATE: 8/20/2020

EXTERIOR ELEVATIONS

A200

KEY PLAN

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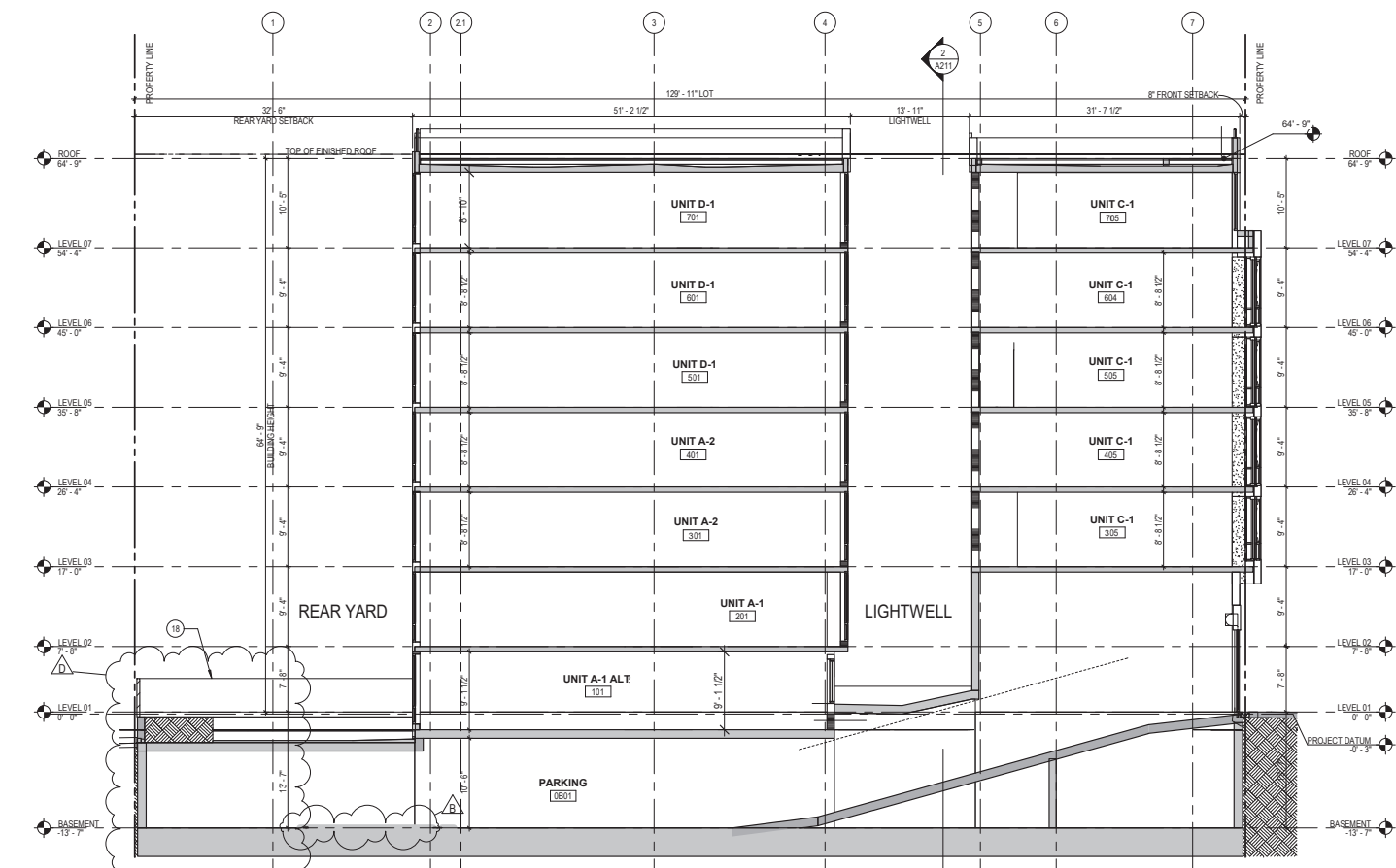
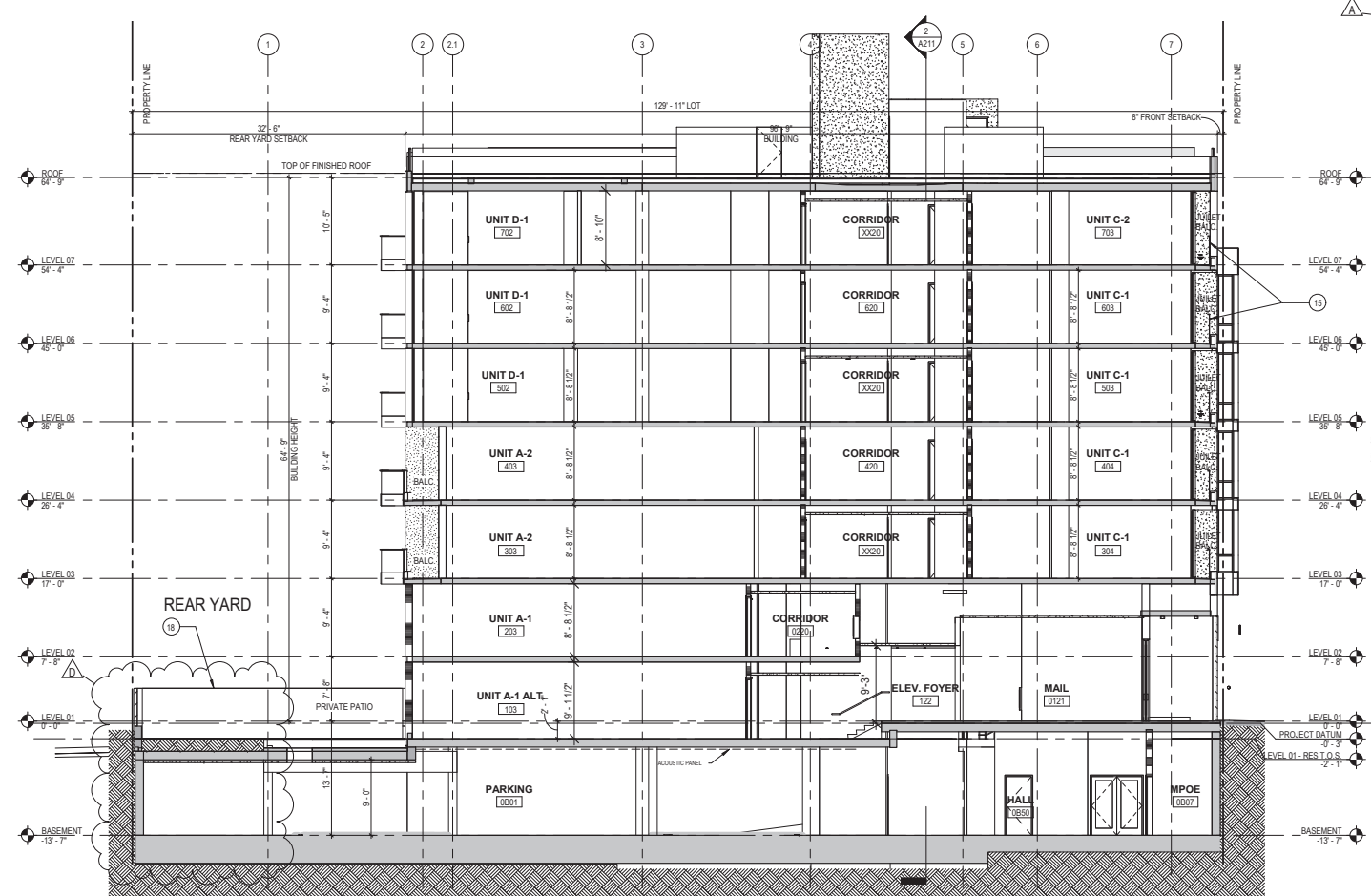
March 01, 2021
20200306413 ALTER SITE DWGS

Patrick O'Riordan
PATRICK O'RORDAN
WATERM. DIRECTOR
DEPT. OF BUILDING INSPECTION

STAMP

DEPARTMENT OF BUILDING INSPECTION
January 29, 2021

PLANNING
January 20, 2021



- | MATERIALS LEGEND | |
|------------------|---|
| 1 | ELASTOMERIC COATING AT EXPOSED CONCRETE SURFACE |
| 2 | FLUTED BRONZE METAL PANEL |
| 3 | FIBER CEMENT PANEL CLADDING - (CP-01) |
| 4 | OPERABLE CASEMENT WINDOW IN WINDOW WALL |
| 5A | ALUMINUM METAL PANEL - (M-1.04) |
| 5B | ALUMINUM METAL PANEL - (M-1.01) |
| 6 | ANODIZED ALUMINUM WINDOW WALL WITH LOW-E VISION GLASS |
| 7 | ALUMINUM LOUVERS, PREFINISHED |
| 8 | OVERHEAD GARAGE DOOR |
| 9 | ALUMINUM BALCONY DOOR, GLAZED |
| 10 | STONE VENEER RAINSCREEN |
| 11 | ALUMINUM METAL CANOPY WITH SIGNAGE |
| 12 | ALUMINUM DOOR, GLAZED |
| 13 | PREFINISHED ALUMINUM STOREFRONT SYSTEM WITH LOW-E VISION IRON GLASS |
| 14A | CEMENT PLASTER - COLOR: DARK GREY TO MATCH CP-01 |
| 14B | CEMENT PLASTER - COLOR: CHAMPAGNE TO MATCH CP-01 |
| 15 | BALCONY GLASS GUARDRAIL (GL-04) |
| 16A | ALUMINUM METAL PANEL AT BALCONY SLAB BYPASS - (M-1.04) |
| 16B | ALUMINUM METAL PANEL AT BALCONY SLAB BYPASS - (M-1.01) |
| 17 | ALUMINUM PATIO DOOR, GLAZED |
| 18 | IFE PATIO FENCE |
| 19 | ROOF DECK GLASS GUARDRAIL |
| 20 | VERTICAL ALUMINUM EXPANSION COVER |
| 21 | ALUMINUM SCREEN, DARK GREY |
| 22 | GAS METER CABINET WITH LOUVERED DOORS |
| 23 | CEMENT PLASTER CONTROL JOINT |
| 24 | NOT USED |
| 25 | 4" METAL REVEAL (M-1.04) |
| 26 | BRONZE ADDRESS SIGNAGE WITH LED BACKLIGHTING |



CLIENT
BOUBOUFFE LLC
3456 SACRAMENTO STREET
SAN FRANCISCO, CA 94118

ARCHITECT
HANDEL ARCHITECTS, LLP
735 MARKET ST.
2ND FLOOR
SAN FRANCISCO, CA 94103
415-495-5588

REV.	DESCRIPTION	DATE
D	ALTERED SITE PERMIT - REV D	11-19-2020
B	60% CONSTRUCTION DOCUMENTS ALTERED SITE PERMIT - REV B	11-04-2020 09-28-2020
	60% CONSTRUCTION DOCUMENTS	09-21-2020
	30% CONSTRUCTION DOCUMENTS	08-20-2020
A	ALTERED SITE PERMIT - REV A	08-07-2020

KEY PLAN



March 01, 2021
202003066413_ALTER SITE DWGS


PATRICK O'RIORDAN
INTERIM DIRECTOR
DEPT. OF BUILDING INSPECTION



STAMP



SCALE 1/8" = 1'-0"
DRAWN BY: HA
DATE: 8/20/2020

BUILDING SECTIONS

A210



CLIENT
BOUBOUFFE LLC
3456 SACRAMENTO STREET
SAN FRANCISCO, CA 94118

ARCHITECT
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REV.	DESCRIPTION	DATE
D	ALTERED SITE PERMIT - REV D	11-19-2020
	80% CONSTRUCTION DOCUMENTS	11-04-2020
	60% CONSTRUCTION DOCUMENTS	09-21-2020
	30% CONSTRUCTION DOCUMENTS	08-20-2020
A	ALTERED SITE PERMIT - REV A	08-07-2020

KEY PLAN



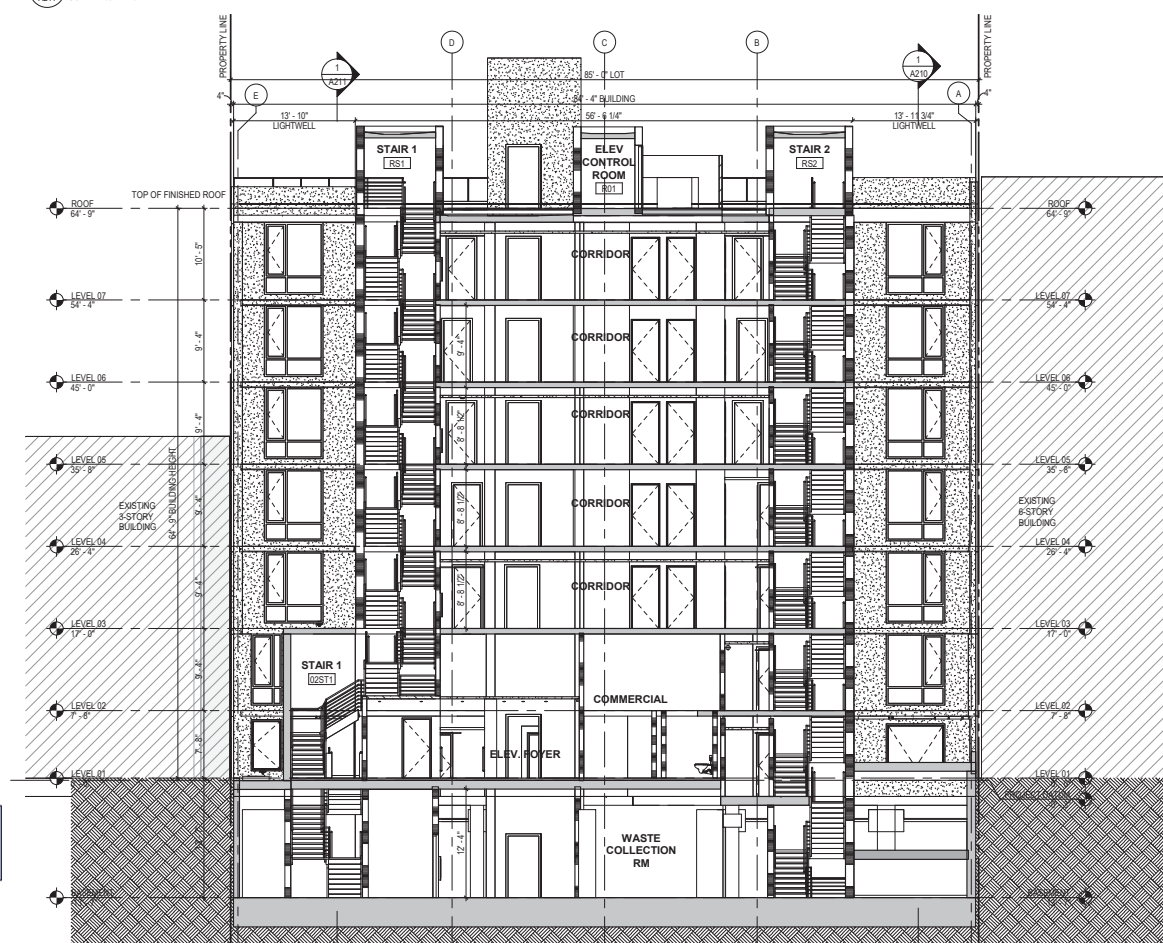
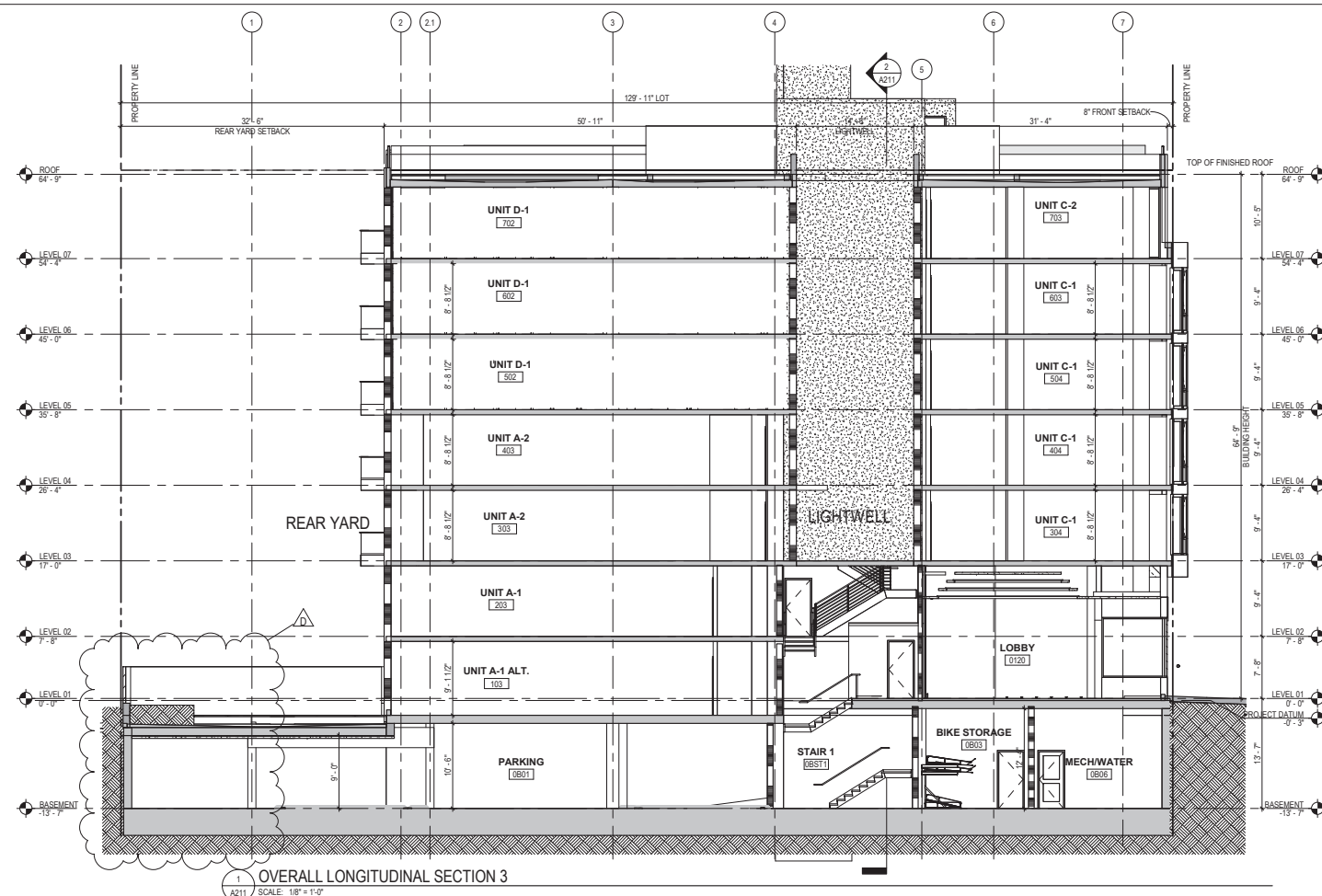
STAMP



SCALE 1/8" = 1'-0"
DRAWN BY: HA
DATE: 8/20/2020

BUILDING SECTIONS

A211



2 OVERALL TRANSVERSE SECTION
A211 SCALE: 1/8" = 1'-0"



Exhibit C:

Land Use Data

LAND USE INFORMATION

Project Address: 2525 Van Ness Ave

Record No.: 2016-002728CUA-02

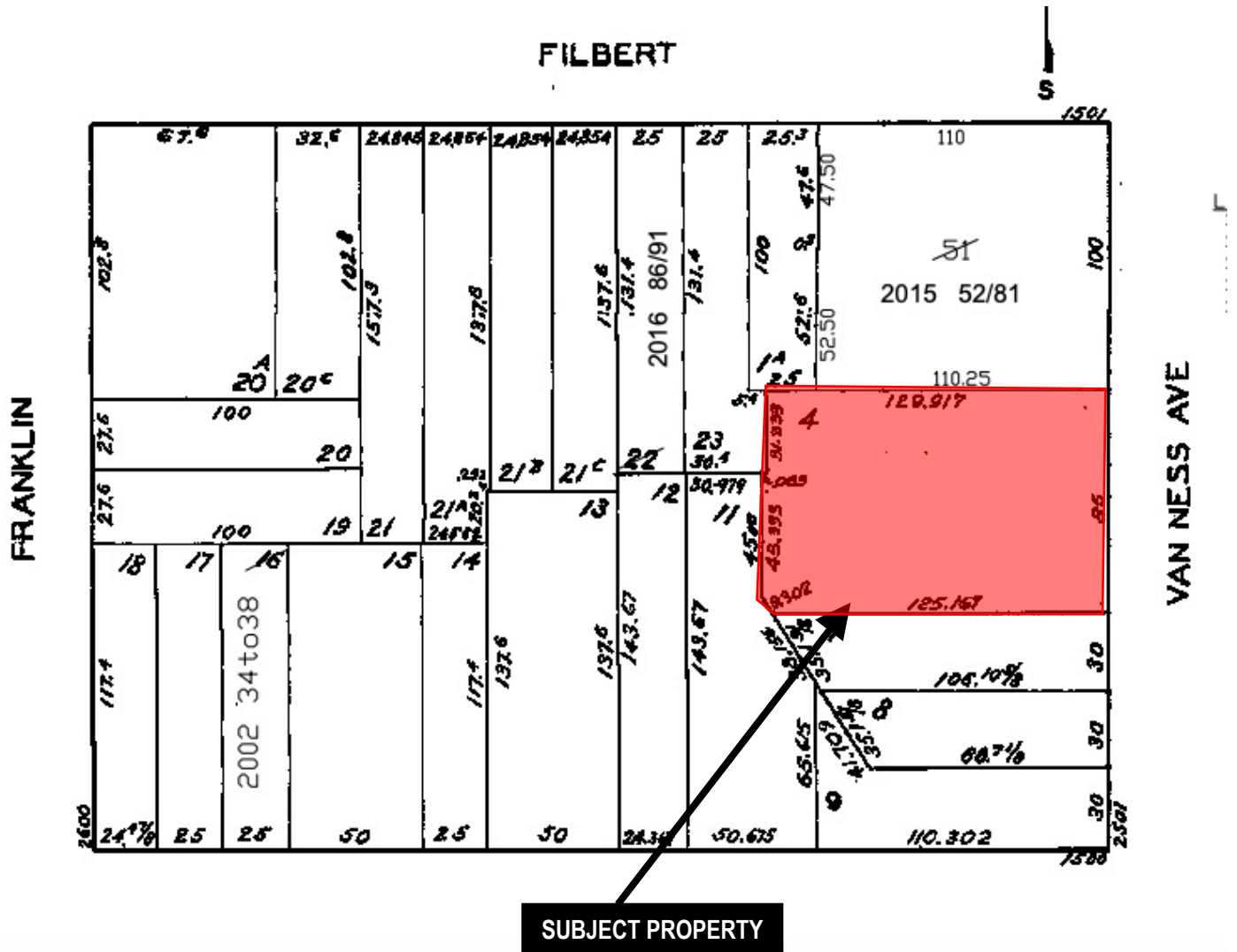
	EXISTING	PROPOSED	NET NEW
GROSS SQUARE FOOTAGE (GSF)			
Parking GSF	5,930	8,701	2,771
Residential GSF	40,240	40,240	0
Retail/Commercial GSF	1,120	1,120	0
Office GSF	0	0	0
Industrial/PDR GSF <i>Production, Distribution, & Repair</i>	0	0	0
Medical GSF	0	0	0
Visitor GSF	0	0	0
CIE GSF	0	0	0
Usable Open Space	7,229	7,229	0
Public Open Space	0	0	0
Other	0	0	0
TOTAL GSF			
	EXISTING	NET NEW	TOTALS
PROJECT FEATURES (Units or Amounts)			
Dwelling Units - Affordable	0	0	0
Dwelling Units - Market Rate	28	0	28
Dwelling Units - Total	28	0	28
Hotel Rooms	0	0	0
Number of Buildings	1	0	1
Number of Stories	7	0	7
Parking Spaces	14	7	21
Loading Spaces	0	0	0
Bicycle Spaces	28	0	28
Car Share Spaces	0	0	0
Other			

	EXISTING	PROPOSED	NET NEW
LAND USE - RESIDENTIAL			
Studio Units	0	0	0
One Bedroom Units	2	2	0
Two Bedroom Units	20	20	0
Three Bedroom (or +) Units	6	6	0
Group Housing - Rooms	0	0	0
Group Housing - Beds	0	0	0
SRO Units	0	0	0
Micro Units	0	0	0
Accessory Dwelling Units	0	0	0

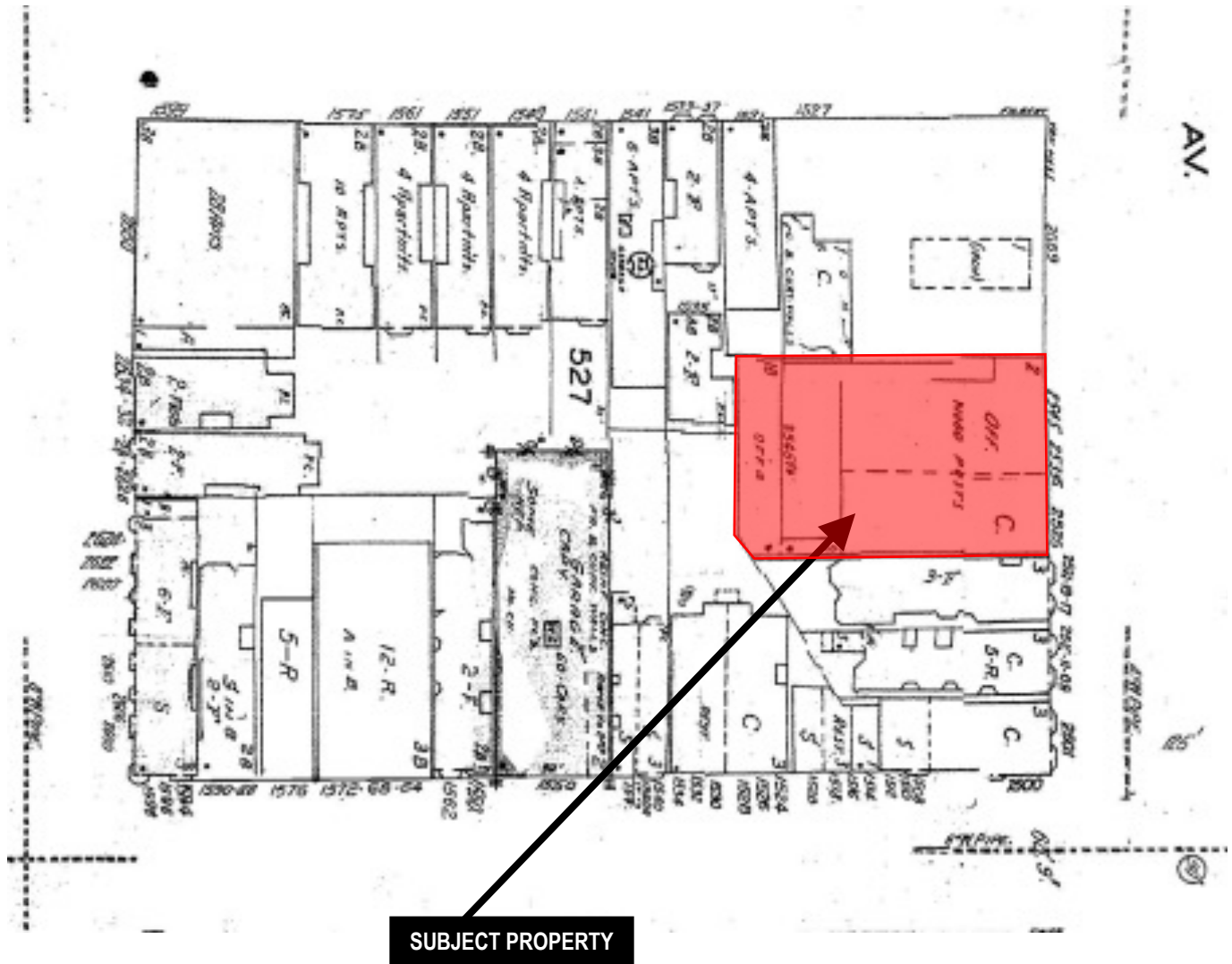
Exhibit D:

Maps and Context Photos

Parcel Map



Sanborn Map*

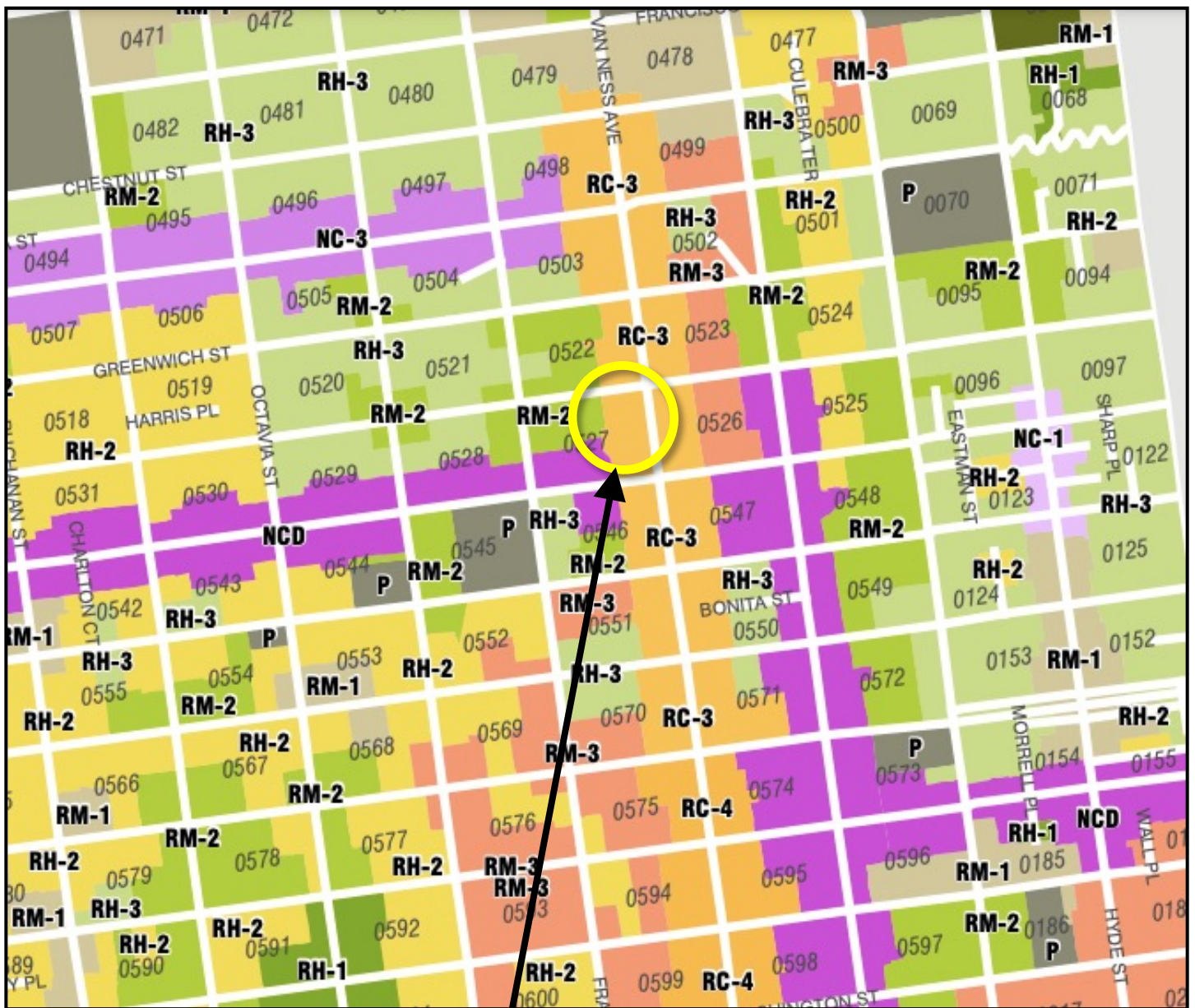


*The Sanborn Maps in San Francisco have not been updated since 1998, and this map may not accurately reflect existing conditions.



Conditional Use Hearing
Case Number 2016-002728CUA-02
2525 Van Ness Avenue

Zoning Map

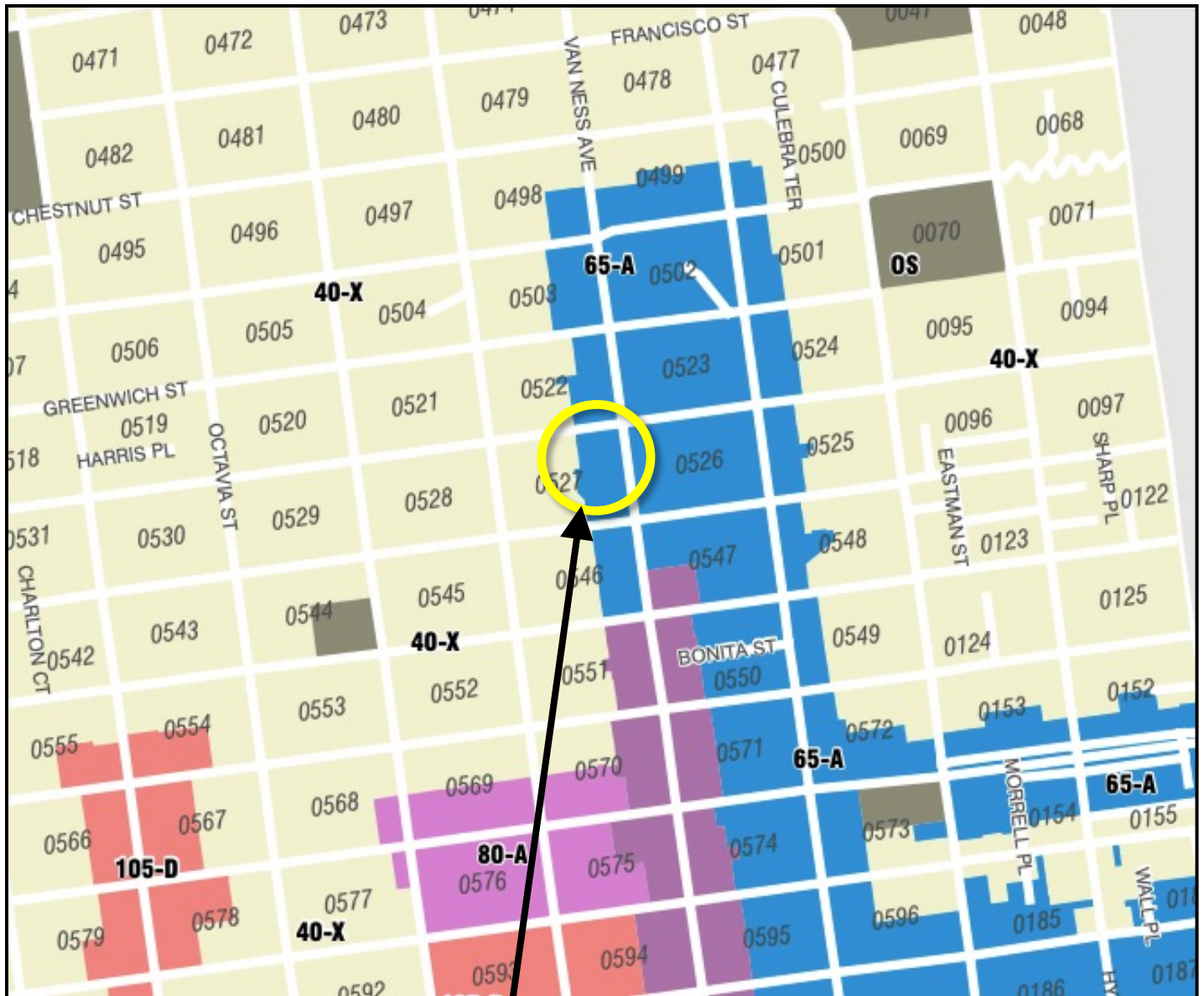


SUBJECT PROPERTY



Conditional Use Hearing
Case Number 2016-002728CUA-02
2525 Van Ness Avenue

Height & Bulk Map



SUBJECT PROPERTY



Conditional Use Hearing
Case Number 2016-002728CUA-02
2525 Van Ness Avenue

Aerial Photo

(facing west)



Site Photo

(facing west)



Conditional Use Hearing
Case Number 2016-002728CUA-02
2525 Van Ness Avenue

Exhibit E:

Project Sponsor Brief



July 20, 2021

Mr. Joel Koppel, President
San Francisco Planning Commission
49 South Van Ness, Ste 1400
San Francisco, CA 94103

**Re: 2525 Van Ness Street (Case No. 2016-002728CUA-02)– July 29, 2021
Hearing**

Dear President Koppel and Commissioners,

On July 29, 2021, the Planning Commission will hear the above request for a Conditional Use Authorization (CUA) to add seven (7) parking storage spaces to the approved 28-unit residential development at 2525 Van Ness Avenue (Project) for a parking ratio of 0.75 spaces per unit. We believe that this request is justified and warranted given the unique nature of the Project which has unit sizes that are nearly double the size of “typical” new units. While Planning Department staff is recommending denial, we believe that our findings, which are included as **Attachment 1**, more than support our request and our request is supported by the neighbors and surrounding community as indicated by the 26 letters of support, a signed petition in support of the Project totaling 41 signatures, as well as a letter from the Chamber of Commerce in support of the Project. All are included as **Attachment 2**. As noted below, the request for additional parking is consistent with parking ratios allowed in other areas of the City for units of a certain size and bedroom count, and if this request is denied, we believe it will discourage the development of large, family size units and will impact the ability for the City to attract and retain families. Moreover, denial of this request jeopardizes the Project’s viability as the lenders or equity partners that finance development and construction have indicated approval of the CUA is necessary because market conditions require that larger units have parking.

As noted in our findings, the units at 2525 Van Ness Avenue are family size.¹ They range from 1,200 to over 1,900 square feet and every unit has minimum of two bathrooms. There are six (6) 3-bedrooms units and 20 2-bedroom units. In fact, only two (2) units are 1-bedroom or less than 10% of the Project and these units, while 1-bedroom, all have a full den. These units were designed with families in mind but to attract families providing parking is key as families, especially families with small children, rely on cars. They need them to transport kids to and from daycare, school, doctor’s appointments, and other activities all of which can be in areas not easily accessible from public transit. We recognize that the Project is adjacent to the Van Ness Corridor Bus Transit improvements, but for families that proximity is not relevant and if we want families to occupy these large units, we need to meet their needs, which means more parking.

The amount of additional parking being requested is minimal (7 spaces), can be accommodated within the existing garage by adding a system of horizontal (parkboard) stackers that

¹ The Project also includes 28 Class I and 3 Class II bicycle parking spaces and 6,679 square feet of outdoor Open Space.



really are parking storage and is consistent with parking ratios granted for other developments in the area. The development immediately adjacent to the Project at 2559 Van Ness (1501 Filbert) was approved with a 1:1 parking ratio and the development at 2465 Van Ness (1515 Union) was approved with a 0.75:1 parking ratio. In fact, the additional parking granted for 2465 Van Ness (1515 Union) was specifically to address and avoid impacts to on-street parking in the neighborhood which is severely impacted given the robust commercial areas of Polk Street and Union Street. It also is consistent with parking allowed in other zoning districts, such as the UMU, when large units are proposed.

Finally, granting the additional parking will not increase congestion or alter traffic patterns. A circulation memo prepared for an adjacent development confirms this determination. It will, however, exponentially increase the likelihood that these units will be occupied by families as evidenced by the justification used to authorize additional parking for large units in the UMU. Approval will help ensure that the Project is built as the Project's two lenders or equity partners may not continue to fund construction without additional parking.

For these reasons, and for all the reasons outlined in more detail below, we respectfully ask the Planning Commission to approve the CUA.

I. The Project's Family Sized Units Require More Parking

In 2019, according to Zumper, the average size of a 1-bedroom unit in San Francisco was 700 square feet and the average size of a 2-bedroom unit was 1,000 square feet. The range of unit sizes in the Project, which average 1,400 square feet are significantly larger and intended for families or multi-generational households. The units range from 1,200 square feet for a 1-bedroom + den unit to over 1,931 square feet for a 3-bedroom unit with all units having a minimum of two (2) bathrooms. The Project is comprised of six (6) 3-bedroom, 3.5 bath units, 12 2-bedroom 2.5 bath units, eight (8) 2-bedroom 2-bath units and two (2) 1-bedroom 2.5 bath + den units. The large size and greater percentage of 2-bedroom+ units mean the units to be served by the additional parking are significantly more likely to be occupied by families or multi-generational households. Surrounding new developments are not comparable in terms of unit sizes or mixes making the Project a unique product type in the market and one targeted toward families that require additional parking. Families have an increased demand for parking given the need to drive to and from school, sports, appointments, and other activities.² Evidence of this increased demand by families and/or occupants of larger units is found in data from the MLS. Since 2019, a review of condo sales in San Francisco indicates that the average size of a condo unit sold without parking was 732 square feet while the average size of a condo sold with parking was 1,293 square feet. In terms of the number of units sold, 68 were without parking while 256 were with parking, indicating that in the larger condo unit market, off-street parking is in high demand with almost 400% more units sold with parking than without parking. The Project

² The Planning Code recognizes this need in other Zoning Districts where larger units are allowed more off-street parking spaces. For example, in the UMU Zoning District residential developments are permitted parking at a ratio of 0.75 to 1 but that ratio increases to 1 to 1 for units with 2 bedrooms and at least 1,000 square feet of residential space. (See Planning Code Section 151.1).



would achieve the requested seven (7) additional parking storage spaces through installation of stackers and would not require additional excavation or expansion of the garage footprint.

While family sized units could theoretically utilize surrounding transit service, families with non-school age children have schedules that fall outside the traditional peak transit hours and visit areas of the City not traditionally accessed by high volume transit lines making utilization of transit more challenging. For example, the No. 49 bus does run on a schedule on a north/south route from the Mission District on Van Ness Avenue to Fisherman's Wharf, but this is not a line frequently accessed by families. As a result, families look for residential units with parking spaces to provide them with the flexibility that they need to meet their transportation demands. While carpool or car share are also an option for families that could reduce parking demand, the ability to utilize carpools varies over time and depends extensively on the age of the children in the family and the ability to identify other families in the area heading to the same location. Finally, on-street parking is not a viable option as on-street parking along the Van Ness Avenue corridor and along Union Street is very challenging. There are numerous older, smaller flats without parking or with limited parking and retail uses as well as restricted parking area and curb cuts limiting on-street parking access and recent work on Van Ness Avenue has removed significant street parking, including approximately 24 parking spaces along the block from Union Street to Filbert Street, which is the block immediately around the Project. Private off-street parking lots in the vicinity have disappeared and been replaced with new residential developments thereby eliminating options for other off-street parking.

II. Insufficient Off-Street Parking Impacts Occupancy of Housing Units

The City has an ongoing housing shortage and it is noteworthy that where large units do not have parking, they sit vacant longer as the group most likely to occupy those units, families, are unlikely to do so because of the lack of parking. Evidence in support of this is the Project sponsor's recent experience marketing for-sale units at 3620 Cesar Chavez a 24-unit project. In reviewing sales data, when parking was not available to be added to a unit, the unit took four (4) to five (5) months longer to sell. Today, almost 10 months after the first sale in that project, four (4) units remain unsold including three (3) 2-bedroom, 2-bath units over 1,000 square feet. There is no more parking available, making these larger units less desirable for families limiting the demand and resulting in them sitting on the market longer. This trend exists in other projects as well and indicates that there is a high demand for parking for larger units. A housing unit unoccupied exacerbates an existing shortage that could be simply rectified by a modest increase in off-site parking allotments.

III. Additional Off-Street Parking will not Increase Traffic Congestion

The addition of seven (7) parking storage spaces at 2525 Van Ness Avenue will not have a detrimental effect on the surrounding area. The number of spaces proposed is de minimis and will utilize the existing curb cut and can be accommodated in mechanical vehicle stackers installed in the area already planned for off-street parking without additional excavation. The seven (7) additional parking storage spaces would not cause detrimental impacts and would not contribute to traffic congestion, or disrupt or conflict with transit, walking and cycling. In fact, failure to provide the additional parking could create a detrimental effect by increasing the number of vehicles searching for



street parking in an area already burdened by limited on- and off-street parking. The Project is adjacent to two (2) separate thriving commercial corridors on Union Street and Polk Street and competition for street parking from residents of the Project will negatively impact those commercial districts and actually increase the likelihood of more traffic generation as vehicles compete for the limited street parking. This conclusion is consistent with a circulation memorandum prepared by CHS in 2017 for the nearby project at 2465 Van Ness (1515 Union Street), which is 100 feet away from 2525 Van Ness, which concluded the minor additional parking for that project's 31 parking spaces (at a ratio of .75 to 1) would not contribute to traffic congestion and would not result in significant cumulative effects related to transit, pedestrian and bicycle safety and circulation (See **Attachment 2**)

IV. Adjacent Projects have Similar Parking Ratios

As noted above, there are two other adjacent or nearly adjacent newly constructed residential developments with increased off-street parking. The directly adjacent residential development at 2559 Van Ness (1501 Filbert Street) is a 30-unit residential development with 30 residential parking spaces (a ratio of 1 to 1) and four (4) commercial parking spaces. The 41-unit residential development at 2465 Van Ness (1515 Union Street), approximately 100 feet from the Project, has 31 off-street parking spaces at a ratio of .75 to 1. These developments were approved with increased parking partially because of the nature of on-street parking concerns in the neighborhood, which are the same concerns facing the Project considering the Project's family-sized units.

V. Viability of Project at Risk

The Project currently has two lenders or equity partners that are financing the development and construction. Both have indicated that given the unit type, size, and location, they would typically require 100 percent parking for the Project as a condition of underwriting Project financing. The Project sponsor has explained that the maximum parking allowed is 0.75 spaces per unit (or 75%) with a CUA. Without the 0.75 parking ratio, it is uncertain whether either lender will continue to fund the Project. The unit sizes are large, as noted above, and designed for and marketed to families. Families have cars and without parking for half of the units, the ability to sell these larger units to the target market is uncertain as evidenced by 1515 Union Street (adjacent to the Project) which has significant challenges selling larger units without parking. Lenders already are facing rising hard costs for construction and without the additional parking, may decide to stop funding the Project due to market uncertainty. This is a very real risk and one that makes granting the CUA for the additional seven (7) spaces even more critical.

* * * * *

The Project clearly meets the CUA findings requirements for a minor increase in on-site parking from 14 parking spaces to 21 parking spaces. As a public policy matter, the City has sought to increase housing opportunities for families which the Project sponsor has met with larger residential units. To meet that housing need adequately, a minor addition of seven (7) parking storage spaces in the existing garage footprint is necessary. This addition of parking is supported by the neighborhood



and community and will exponentially increase the likelihood that families will occupy the units. As such, we respectfully request that the Planning Commission approve the CUA for the Project to ensure the Project may provide much needed family housing.

If you have any questions or need any additional information, please feel free to contact me at 415-273-9670.

Very truly yours,

A handwritten signature in blue ink, appearing to read "Alexis M. Pelosi".

Alexis M. Pelosi

Attachments

ATTACHMENT 1

Conditional Use Findings

Pursuant to Planning Code Section 303(c), before approving a conditional use authorization, the Planning Commission needs to find that the facts presented are such to establish the findings stated below.

1. That the proposed use or feature, at the size and intensity contemplated and at the proposed location, will provide a development that is necessary or desirable for, and compatible with, the neighborhood or the community. If the proposed use exceeds the non-residential use size limitations for the zoning district, additional findings must be provided per Planning Code Section 303(c)(1)(A-C).

The Project is seeking to increase the parking at 2525 Van Ness Avenue from 0.5 parking spaces per unit to 0.75 parking space per unit or an increase of seven (7) parking spaces. The addition of seven (7) parking spaces is desirable and compatible with the neighborhood, which has limited street parking available and numerous smaller flats and apartments constructed without parking. The limited street parking has been exacerbated by the removal of street parking as part of the Van Ness Bus Rapid Transit (BRT) project. Adding a few more off-street parking spaces to a new development in the area will reduce competition with existing residents for street parking.

2. That such use or feature as proposed will not be detrimental to the health, safety, convenience or general welfare of persons residing or working in the vicinity, or injurious to property, improvements or potential development in the vicinity, with respect to aspects including but not limited to the following:
 - a. The nature of the proposed site, including its size and shape, and the proposed size, shape and arrangement of structures;
 - b. The accessibility and traffic patterns for persons and vehicles, the type and volume of such traffic, and the adequacy of proposed off-street parking and loading;
 - c. The safeguards afforded to prevent noxious or offensive emissions such as noise, glare, dust and odor;
 - d. Treatment given, as appropriate, to such aspects as landscaping, screening, open spaces, parking and loading areas, service areas, lighting, and signs.

Adding seven (7) parking spaces to the 2525 Van Ness Development will not be detrimental to the health, safety, convenience or general welfare of persons residing or working in the project vicinity and will not be injurious to property, improvements or potential development in the project area.

The additional parking will be provided in vehicle stackers within the space planned for off-street parking. Given the nature of the site and the excavation required for site development, vehicle stackers can be accommodated without additional excavation. Adding seven (7) parking spaces will not change accessibility or traffic patterns as these spaces will use the same curb cut planned for 2525 Van Ness Avenue and the seven (7) parking spaces proposed would be a de minimis number of spaces. In fact, by providing the additional parking, the

project will improve the neighborhood and livability by reducing the number of vehicles circling the block looking for street parking, reducing vehicular exhaust and noise from circling vehicles and reducing the number of double parked vehicles waiting for street parking.

3. That such use or feature as proposed will comply with the applicable provisions of this Code and will not adversely affect the General Plan.

The Project is seeking CU authorization to increase the Project's parking from a ratio of 0.5 to 1 to a ratio of 0.75 to 1. The Project will comply with all other requirements of the Planning Code and the General Plan.

4. The use or feature satisfies any criteria specific to the use of features listed in Planning Code Section 303(g), et seq

See below.

(u) Accessory Parking Above That Principally Permitted.

(A) In granting approval for parking accessory to Residential Uses above that principally permitted in Table 151.1, the Planning Commission shall make the following affirmative findings in addition to those stated in Section 303(c):

(i) For projects with 50 units or more, all residential accessory parking in excess of 0.5 parking spaces for each Dwelling Unit shall be stored and accessed by mechanical stackers or lifts, valet, or other space-efficient means that allow more space above-ground for housing, maximizes space efficiency, and discourages use of vehicles for commuting or daily errands. The Planning Commission may authorize the request for additional parking notwithstanding that the project sponsor cannot fully satisfy this requirement provided that the project sponsor demonstrates hardship or practical infeasibility (such as for retrofit of existing buildings) in the use of space-efficient parking given the configuration of the parking floors within the building and the number of independently accessible spaces above 0.5 spaces per unit is de minimis and subsequent valet operation or other form of parking space management could not significantly increase the capacity of the parking space above the maximums in Table 151.1;

Not Applicable. The Project would add parking to 2525 Van Ness Avenue, which was approved for 28 units, which is below than the 50-unit threshold. The project, however, which seeks to add seven (7) additional parking spaces for a parking ratio of 0.75 spaces per unit, would provide the additional seven (7) spaces in mechanical stackers.

(ii) All parking meets the active use and architectural screening requirements in Section 145.1 and the project sponsor is not requesting any exceptions or variances requiring such treatments elsewhere in this Code;

Criteria met. The seven (7) additional parking spaces would be located in a basement garage, in mechanical vehicular stackers that would be added to the already excavated basement area. The additional spaces would be accessed through the same curb cut and parking entrance as 2525 Van Ness Avenue, which is 11 feet in width, 9 feet smaller than the 20-foot maximum permitted by Section 145.1. The proposed additional parking would not change any ground floor active use requirements of the Planning Code.

(iii) Demonstration that trips to the use or uses to be served, and the apparent demand for additional parking, cannot be satisfied by the amount of parking classified by this Code as accessory, by transit service which exists or is likely to be provided in the foreseeable future, by carpool arrangements, by more efficient use of existing on-street and off-street parking available in the area, and by other means;

Criteria met. The demand for the seven (7) additional parking spaces cannot be satisfied by transit service, carpool arrangement or more efficient use of existing on-street and off-street parking given the likely occupants of the residential development that additional parking spaces would serve.

In 2019, according to Zumper, the average size of a 1-bedroom unit in San Francisco was 700 square feet and the average size of a 2-bedroom unit was 1,000 square feet. The range of unit sizes in the development that the seven (7) additional parking spaces would serve is significantly larger. The units range from 1,193 square feet for a 1-bedroom unit to over 1,931 square feet for a 3-bedroom unit. These units are almost double (2x) the size of an average San Francisco unit and over ninety-two percent (92%) of the units are 2-bedroom or 3-bedroom units with all units having a minimum of two (2) bathrooms. Specifically, the development to be served by the additional seven (7) parking spaces is comprised of six (6) 3-bedroom, 3.5 bath units, 12 2-bedroom 2.5 bath units, eight (8) 2-bedroom 2-bath units and two (2) 1-bedroom 2.5 bath + den units. The large size and greater percentage of 2-bedroom+ units mean the units to be served by the additional parking are significantly more likely to be occupied by families. Surrounding new developments are not comparable in terms of units sizes or mixes making the development to be served a unique product type in the market and one targeted toward families. Families have an increased demand for parking given the need to drive to and from school, sports, appointments, and other activities.

Evidence of this increased demand by families and/or occupants of larger units such as the ones that the additional parking would serve is found in data from the MLS. Since 2019, a review of condo sales in San Francisco indicates that the average size of a condo unit sold without parking was 732 square feet while the average size of a condo sold with parking was 1,293 square feet. In terms of the number of units sold, 68 were without parking while 256 were with parking, indicating that in the larger condo unit market, off-street parking is in high demand with almost 400% more units sold with than without parking. Here, the units that the seven (7) additional parking spaces would serve are at their smallest larger than the average unit size of a condo sold with parking indicating that the demand for the parking being proposed would be very high.

This demand theoretically could be satisfied by utilizing the variety of transit lines surrounding the project site including the soon to be completed Van Ness BRT. Families, however, especially

those with non-school age children have schedules that fall outside the traditional peak transit hours and visit areas of the City not traditionally accessed by high volume transit lines making utilization of transit more challenging. For example, the No. 49 bus does run frequently on a north/south route from the Mission District on Van Ness Avenue to Fisherman's Wharf, but this would not a line frequently accessed by families. As a result, families look for residential units with parking spaces to provide them with the flexibility that they need to meet their transportation demands.

Where large units do not have parking, they sit vacant longer as the group most likely to occupy those units, families, are unlikely to do so because of the lack of parking. Evidence in support of this is the applicant's recent experience marketing for-sale units at 3620 Cesar Chavez a 24-unit project. In reviewing sales data, when parking was not available to be added to a unit, the unit took four (4) to five (5) months longer to sell. Today, almost 10 months after the first sale in the project, four (4) units remain unsold including three (3) 2-bedroom, 2-bath units over 1,000 square feet. There is no more parking available, making these larger units less desirable for families limiting the demand and resulting in them sitting on the market longer. This trend exists in other projects as well and indicates that there is a high demand for parking for larger units.

Carpool or other alternate means of transportation are also an option for families that could reduce parking demand, but the ability to utilize carpools varies over time and depends extensively on the age of the children in the family and the ability to identify other families in the area heading to the same location.

Finally, on-street parking is not a viable option. On-street parking along the Van Ness Avenue corridor and along Union Street is very challenging. There are numerous older, smaller flats without parking or with limited parking and retail uses as well as restricted parking area and curb cuts limiting on-street parking access. The Van Ness BRT project also removed significant street parking, including approximately 24 parking spaces along the block from Union Street to Filbert Street, which is the block immediately around the development to be served by the seven (7) additional parking spaces. Private off-street parking lots in the vicinity have disappeared and been replaced with new residential developments thereby eliminating options for other off-street parking.

(iv) Demonstration that the apparent demand for additional parking cannot be satisfied by the provision by the applicant of one or more car-share parking spaces in addition to those that may already be required by Section [166](#) of this Code;

Criteria met. The project has less than 50 units and is not required by Section 166 to provide car share parking. A search of Zipcar indicates at least nine (9) car shares in a three (3) block radius of the Project.

(v) The absence of potential detrimental effects of the proposed parking upon the surrounding area, especially through unnecessary demolition of sound structures, contribution to traffic congestion, or disruption of or conflict with transit services, walking, and cycling; and

Criteria met: The addition of seven (7) parking spaces at 2525 Van Ness Avenue will not have a detrimental effect on the surrounding area. The number of spaces proposed is de minimis and will utilize the curb cut currently proposed and can be accommodated in mechanical vehicle stackers installed in the area already planned for off-street parking without additional excavation. A circulation memorandum prepared by CHS in 2017 for the adjacent project at 1501 Union Street, which is 100 feet away from 2525 Van Ness, concluded the minor additional parking for that project's 31 parking spaces would not contribute to traffic congestion and would not result in significant cumulative effects related to transit, pedestrian and bicycle safety and circulation. Similarly, the seven (7) additional parking spaces would not cause detrimental would not contribute to traffic congestion, or disrupt or conflict with transit, walking and cycling. Failure to provide the additional parking could create a detrimental effect by increasing the number of vehicles searching for street parking in an area already burdened by limited on- and off-street parking.

(vi) Accommodating excess accessory parking does not degrade the overall urban design quality of the project proposal nor diminish the quality and viability of existing or planned streetscape enhancements.

Criteria met. The additional seven (7) parking spaces can be accommodated in below grade parking already planned for 2525 Van Ness Avenue through the addition of mechanical vehicular stackers. Access to the additional seven (7) parking space would be through the same curb cut as the 2525 Van Ness project and would not degrade the urban design quality of the project or diminish the quality or viability of planned streetscape enhancements.

ATTACHMENT 2



235 Montgomery St., Ste. 760, San Francisco, CA 94104
tel: 415.392.4520 • fax: 415.392.0485
sfchamber.com • twitter: @sf_chamber

July 19, 2021

President Joel Koppel
San Francisco Planning Commission
49 South Van Ness, 14th Floor
San Francisco, CA 94103

RE: 2016-002728CUA-02: 2525 Van Ness Avenue

Dear President Koppel and Commissioners:

The San Francisco Chamber of Commerce strongly urges you to approve the proposal before you to grant the Conditional Use authorization to increase parking at 2525 Van Ness Avenue from 0.5 spaces per unit to 0.75 spaces per unit or 7 parking spaces.

This additional parking is needed to avoid congestion and competition for street parking in the neighborhood and makes sense given the units in the project are large with six 3-bedroom units and 20 2-bedroom units. Every unit also has at least two bathrooms. These are family size units and if we want to attract and keep families in San Francisco we need projects like 2525 Van Ness Avenue.

The current project only includes 14 parking spaces – a 0.5 to 1 parking ratio. Increasing the parking count by 7 cars by adding stackers in the garage raises the parking ratio to 0.75 to 1 and makes them more attractive to families who may find more barriers to using alternative modes of transportation.

In 2020, San Francisco faced unprecedented economic and other challenges. We need more family housing to keep working people in San Francisco. Please approve the Conditional Use authorization for 2525 Van Ness Avenue.

Sincerely,

Somiah Handy
Small Business Manager, SF Chamber of Commerce

San Francisco Planning Commission
49 South Van Ness
Ste 1400
San Francisco, CA 94103
Attn: Christopher May

RE: 2016-002728CUA-02

Dear President Koppel, Vice President Moore & Commissioners:

I'm writing to support the above referenced CUA related to increasing the number of off-street parking spaces provided by the 28-unit Project at 2525 Van Ness Avenue.

The Project provides spacious family size condominiums, including six (6) 3-Bedroom units, twenty (20) 2-Bedroom units and two (2) 1-Bedroom units with dens. Additional off-street parking will encourage a higher quality of life for multigenerational families living there.

The previously approved project only provides parking for 14 cars. Increasing the parking count by 7 cars via the addition of a puzzle stacker will help ensure adequate parking is provided for residents and families while reducing the demand for on-street parking and reducing congestion related to residents circling the block while looking for street parking.

The Van Ness BRT project has resulted in the loss of on-street parking fronting the project site. In fact, (24) on-street parking spaces have been eliminated on Van Ness between Filbert and Union Street; the additional parking spaces requested by this CUA will help offset that loss.

Please approve 2016-002728CUA-02.

Sincerely,

Chang Lee

(Name)

96301

(Zip code)

San Francisco Planning Commission
49 South Van Ness
Ste 1400
San Francisco, CA 94103
Attn: Christopher May

RE: 2016-002728CUA-02

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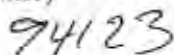
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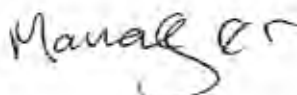
Sincerely,



(Name)



(Zip code)



San Francisco Planning Commission
49 South Van Ness
Ste 1400
San Francisco, CA 94103
Attn: Christopher May

RE: 2016-002728CUA-02

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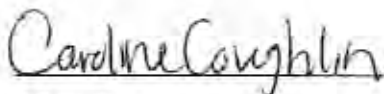
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Please approve 2016-002728CUA-02.

Sincerely,



(Name)

94133

(Zip code)

San Francisco Planning Commission
49 South Van Ness
Ste 1400
San Francisco, CA 94103
Attn: Christopher May

RE: 2016-002728CUA-02

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Please approve 2016-002728CUA-02.

Sincerely,

Ryan Restup 

(Name)

94123

(Zip code)

San Francisco Planning Commission
49 South Van Ness
Ste 1400
San Francisco, CA 94103
Attn: Christopher May

RE: 2016-002728CUA-02

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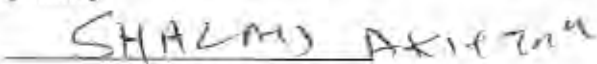
Please approve 2016-002728CUA-02.

Sincerely,



FENZI UMO

(Name)



SHALOM AXILTZIN

(Zip code)

94123

San Francisco Planning Commission
49 South Van Ness
Ste 1400
San Francisco, CA 94103
Attn: Christopher May

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Please approve 2016-002728CUA-02.

Sincerely,



(Name)

94123

(Zip code)

San Francisco Planning Commission
49 South Van Ness
Ste 1400
San Francisco, CA 94103
Attn: Christopher May

RE: 2016-002728CUA-02

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Please approve 2016-002728CUA-02.

Sincerely,

San. Gebre

(Name)

94123

(Zip code)

San Francisco Planning Commission
49 South Van Ness
Ste 1400
San Francisco, CA 94103
Attn: Christopher May

RE: 2016-002728CUA-02

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Please approve 2016-002728CUA-02.

Sincerely,

Linda Tan

(Name)

94123

(Zip code)

San Francisco Planning Commission
49 South Van Ness
Ste 1400
San Francisco, CA 94103
Attn: Christopher May

RE: 2016-002728CUA-02

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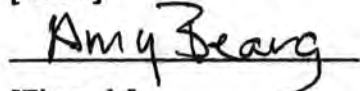
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Please approve 2016-002728CUA-02.

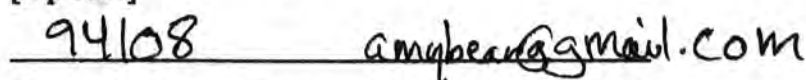
Sincerely,



[Name]



[Zip code]



[Optional - Email / Phone used for project updates]

San Francisco Planning Commission
49 South Van Ness
Ste 1400
San Francisco, CA 94103
Attn: Christopher May

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Please approve 2016-002728CUA-02.

Sincerely,

Anya Patel

[Name]

94109

[Zip code]

anya21patel@gmail.com

[Optional - Email / Phone used for project updates]

San Francisco Planning Commission
49 South Van Ness
Ste 1400
San Francisco, CA 94103
Attn: Christopher May

RE: 2016-002728CUA-02

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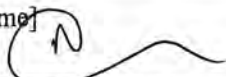
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By supporting this CUA you will help reduce demand for on-street parking around the Project which will minimize congestion related to vehicles circling the block looking for street parking.

Please approve 2016-002728CUA-02.

Sincerely,

NOAH PAR

[Name] 

[Zip code] 94123

[Optional - Email / Phone used for project updates]

San Francisco Planning Commission
49 South Van Ness
Ste 1400
San Francisco, CA 94103
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Please approve 2016-002728CUA-02.

Sincerely,

ELIZABETH GARTNER

[Name]

94123

[Zip code]

[Optional - Email / Phone used for project updates]

San Francisco Planning Commission
49 South Van Ness
Ste 1400
San Francisco, CA 94103
Attn: Christopher May

RE: 2016-002728CUA-02

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Please approve 2016-002728CUA-02.

Sincerely,

FADI BERBER

[Name]

94109

[Zip code]

[Optional - Email / Phone used for project updates]

San Francisco Planning Commission
49 South Van Ness
Ste 1400
San Francisco, CA 94103
Attn: Christopher May

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FARZAD Arjmand

[Name]

94123

[Zip code]

[Optional - Email / Phone used for project updates]

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Sincerely,

JACKSON Daecher

[Name]

94123

[Zip code]

JackDaecher@yahoo.com

[Optional - Email / Phone used for project updates]

San Francisco Planning Commission
49 South Van Ness
Ste 1400
San Francisco, CA 94103
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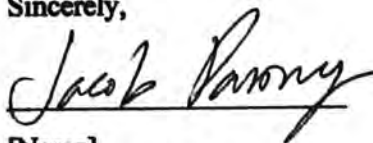
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Please approve 2016-002728CUA-02.

Sincerely,



[Name]

94123

[Zip code]

[Optional - Email / Phone used for project updates]

San Francisco Planning Commission
49 South Van Ness
Ste 1400
San Francisco, CA 94103
Attn: Christopher May

RE: 2016-002728CUA-02

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Please approve 2016-002728CUA-02.

Sincerely,

Juan Lored

[Name]

94133

[Zip code]

[Optional - Email / Phone used for project updates]

San Francisco Planning Commission
49 South Van Ness
Ste 1400
San Francisco, CA 94103
Attn: Christopher May

RE: 2016-002728CUA-02

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Please approve 2016-002728CUA-02.

Sincerely,



[Name]

94118

[Zip code]

[Optional - Email / Phone used for project updates]

San Francisco Planning Commission
49 South Van Ness
Ste 1400
San Francisco, CA 94103
Attn: Christopher May

RE: 2016-002728CUA-02

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Sincerely,



[Name]

94109

[Zip code]

[Optional - Email / Phone used for project updates]

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Sincerely,

Wenqian Zhu

[Name]

94109

[Zip code]

wqianzhu@gmail.com

[Optional - Email / Phone used for project updates]

San Francisco Planning Commission
49 South Van Ness
Ste 1400
San Francisco, CA 94103
Attn: Christopher May

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Please approve 2016-002728CUA-02.

Sincerely,

Olivia Davis 

[Name]

94109

[Zip code]

olivialpsmile@gmail.com

[Optional - Email / Phone used for project updates]

San Francisco Planning Commission
49 South Van Ness
Ste 1400
San Francisco, CA 94103
Attn: Christopher May

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Please approve 2016-002728CUA-02.

Sincerely,

MICHAEL KIM

(Name)

94123

(Zip code)

Pacific Heights Inn
mykim72@gmail.com

San Francisco Planning Commission
49 South Van Ness
Ste 1400
San Francisco, CA 94103
Attn: Christopher May

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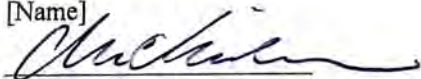
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Please approve 2016-002728CUA-02.

Sincerely,

CHI LAM

[Name]



[Zip code]

LL328@hotmail.com

[Optional - Email / Phone used for project updates]

San Francisco Planning Commission
49 South Van Ness
Ste 1400
San Francisco, CA 94103
Attn: Christopher May

RE: 2016-002728CUA-02

Dear President Koppel, Vice President Moore & Commissioners:

I'm writing to support the above referenced CUA related to increasing the number of off-street parking spaces provided by the 28-unit Project at 2525 Van Ness Avenue.

The Project provides spacious family size condominiums, including six (6) 3-Bedroom, twenty (20) 2-Bedroom and two (2) 1-Bedroom units. Approving additional off-street parking will encourage multigenerational families to live in the neighborhood.

The reconfiguration of Van Ness Avenue has resulted in a significant loss of on-street parking. In fact, twenty-four (24) on-street parking spaces have been eliminated on Van Ness between Filbert and Union Street alone.

By approving this CUA you will reduce demand for on-street parking around the project, while minimizing congestion related to vehicles circling the block looking for street parking.

Please approve 2016-002728CUA-02.

Sincerely,

Pongpattana
[Name]
94109

[Zip code]
PONGPATTANA@HOTMAIL.COM

[Optional - Email / Phone used for project updates]

San Francisco Planning Commission
49 South Van Ness
Ste 1400
San Francisco, CA 94103
Attn: Christopher May

RE: 2016-002728CUA-02

Dear President Koppel, Vice President Moore & Commissioners:

I'm writing to express my support for the above referenced CUA related to increasing the number of off-street parking spaces provided by the 28-unit Project at 2525 Van Ness Avenue.

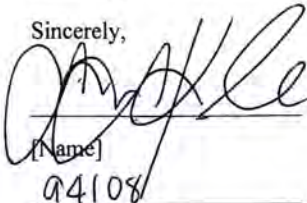
The Project provides spacious family size condominiums, including six (6) 3-Bedroom, twenty (20) 2-Bedroom and two (2) 1-Bedroom units. Approving seven (7) additional off-street parking will encourage multigenerational families to live in the neighborhood.

The reconfiguration of Van Ness Avenue has resulted in a significant loss of on-street parking. Allowing the project to provide 25% additional garage capacity will help offset that loss.

By supporting this CUA you will help reduce demand for on-street parking around the Project which will minimize congestion related to vehicles circling the block looking for street parking.

Please approve 2016-002728CUA-02.

Sincerely,



[Name]

94108
[Zip code]

[Optional - Email / Phone used for project updates]

San Francisco Planning Commission
49 South Van Ness
Ste 1400
San Francisco, CA 94103
Attn: Christopher May

RE: 2016-002728CUA-02

Dear President Koppel, Vice President Moore & Commissioners:

I'm writing to support the above referenced CUA related to increasing the number of off-street parking spaces provided by the 28-unit Project at 2525 Van Ness Avenue.

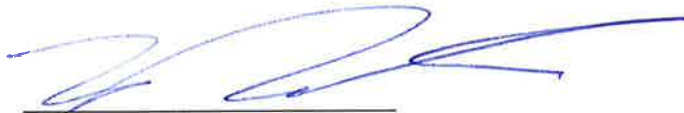
The Project provides spacious family size condominiums, including six (6) 3-Bedroom units, twenty (20) 2-Bedroom units and two (2) 1-Bedroom units with dens. Additional off-street parking will encourage a higher quality of life for multigenerational families living there.

The previously approved project only provides parking for 14 cars. Increasing the parking count by 7 cars via the addition of a puzzle stacker will help ensure adequate parking is provided for residents and families while reducing the demand for on-street parking and reducing congestion related to residents circling the block while looking for street parking.

The Van Ness BRT project has resulted in the loss of on-street parking fronting the project site. In fact, (24) on-street parking spaces have been eliminated on Van Ness between Filbert and Union Street; the additional parking spaces requested by this CUA will help offset that loss.

Please approve 2016-002728CUA-02.

Sincerely,



(Name)

94109 - Owner The Silver Platter

(Zip code)

Approve More Off-Street Parking at 2525 Van Ness

 **SAN FRANCISCO PLANNING COMMISSION**

RE: 2016-002728CUA-02

Dear President Koppel, Vice President Moore & Commissioners:

I'm writing to urge you to approve the Conditional Use authorization to increase parking at 2525 Van Ness Avenue from 0.5 spaces per unit to 0.75 spaces per unit or 7 parking spaces. This additional parking is needed to avoid congestion and competition for street parking in the neighborhood and makes sense given the units in the project are large – ranging from 1,100 to over 1,900 square feet (almost double (2X) the size of a typical SF unit) with six (6) 3-bedroom units and 20 2-bedroom units. Every unit also has at least two (2) bathrooms. These are family size units and if we want to attract and keep families in San Francisco we need projects like 2525 Van Ness Avenue and we need to let them have more parking.

The current project only includes 14 cars – a 0.5 to 1 parking ratio. Increasing the parking count by 7 cars by adding stackers in the garage raises the parking ratio to 0.75 to 1 and makes them more attractive to families. The additional parking will also reduce demand for on-street parking in the immediate vicinity of the project while reducing congestion related to vehicles circling the block looking for street parking.

Please approve the Conditional Use authorization for 2525 Van Ness. It is good for families and good for the neighborhood.

41 Signatures Collected



Only 9 more until our goal of 50

SIGN THIS PETITION

First Name

Last Name

Email *

Zip/Postal Code *

Not in the US?

Comments

First name	Last name	Email	Address	City	State	State Abbr	Zip code	Country	Comments	Referrer Code	Timestamp (EST)
Jennifer	Bailey	jennynbailey@gmail.com		San Francisco	California	CA	94109	US			2021-07-10 12:11:27 EST
Nicolas	Hills	nhills@umich.edu		San Francisco	California	CA	94116	US			2021-07-13 12:20:18 EST
Tammy	Tang	tex.tang@gmail.com		San Francisco	California	CA	94118	US	We need more viable housing options for famili		2021-07-13 17:52:18 EST
Leo	Tovey	ltovey.lt@gmail.com		San Francisco	California	CA	94123	US			2021-07-13 18:06:01 EST
Elizabeth	Parks	lizbetparks@gmail.com		San Francisco	California	CA	94133	US			2021-07-13 18:06:36 EST
Domenic	Tringali	dtringali33@yahoo.com		San Francisco	California	CA	94109	US	Please approve 2016-002728CUA-02! 7 more sp		2021-07-13 18:10:09 EST
Emerald	Xu	xemerald512@gmail.com		San Francisco	California	CA	94109	US			2021-07-13 18:10:28 EST
Alanna	Kretchmer	alannadswartz@gmail.com		San Francisco	California	CA	94123	US			2021-07-13 18:10:53 EST
Jiashun	Xu	emerald@marchcapitalfund.com		San Francisco	California	CA	94109	US			2021-07-13 18:11:07 EST
Jiashun	Xu	xujiashun@sre.com.cn		San Francisco	California	CA	94109	US			2021-07-13 18:12:11 EST
Emerald	Xu	mandrake0512@hotmail.com		San Francisco	California	CA	94109	US			2021-07-13 18:13:16 EST
Kelsi	Gallegos	kgallegos07@yahoo.com		San Francisco	California	CA	94118	US			2021-07-13 18:22:04 EST
Sam	Berlin	samantha.berlin1@gmail.com		San Francisco	California	CA	94118	US			2021-07-13 18:29:03 EST
Audrey	Higuera	a.higuera13@gmail.com		San Francisco	California	CA	94109	US			2021-07-13 18:35:53 EST
David	Beam	dbeam16@alumni.stanford.edu		San Francisco	California	CA	94109	US			2021-07-13 18:36:36 EST
Rachel	Hart	hartrn18@gmail.com		San Francisco	California	CA	94115	US			2021-07-13 18:39:31 EST
Rachel	Hart	hartrn18@gmail.com		San Francisco	California	CA	94115	US			2021-07-13 18:39:31 EST
Joe	Abbott	josephaabbott@gmail.com		San Francisco	California	CA	94123	US			2021-07-13 18:48:45 EST
Frank	Lavin	frankrlavin@aol.com		Novato	California	CA	94949	US			2021-07-13 19:24:23 EST
Pali	Khalsa	palikkhalsa@gmail.com		San Francisco	California	CA	94118	US			2021-07-13 19:39:47 EST
Gonzalo	Teso	gonzotersen@msn.com		San Francisco	California	CA	94109	US			2021-07-13 20:09:44 EST
Marta	Benjumea	martabenjumeaadelavega@gmail.com		San Francisco	California	CA	94109	US			2021-07-13 20:11:58 EST
Esther	Hong	estherhong455@gmail.com		San Francisco	California	CA	94109	US	We need more parking ALL over SF		2021-07-13 20:33:09 EST
Leland	Evans	lelandevans5050@gmail.com		San Francisco	California	CA	94109	US			2021-07-13 20:34:11 EST
Noemi	Hong	noemihongg@gmail.com		San Francisco	California	CA	94109	US	I can never find parking!! Yes! More parking sp		2021-07-13 20:36:16 EST
Enrique	Asmar	easmar@stanford.edu		San Francisco	California	CA	94109	US	More Parking for SF!! This project will be great ;		2021-07-13 21:24:46 EST
Luis	Varela Eleta	luisfervarela55@gmail.com		San Francisco	California	CA	94109	US	Please add more parking if possible, this would		2021-07-13 21:33:24 EST
Reynaldo	Farias Zorrill	reynaldofarias96@gmail.com		San Francisco	California	CA	94109	US	Please approve more parking if possible		2021-07-13 21:35:30 EST
Alvaro	Bravo	siman.alvaro@icloud.com		San Francisco	California	CA	94109	US			2021-07-13 21:36:12 EST
Montserrat	Pladevall	montserrat.pladevall@gmail.com		San Francisco	California	CA	94109	US			2021-07-13 21:59:42 EST
Maria	Traboulsi	mtraboulsi20@gmail.com		San Francisco	California	CA	94108	US			2021-07-13 22:06:52 EST
Oriana	Delgado	oriana089@hotmail.com		San Francisco	California	CA	94109	US			2021-07-13 22:53:34 EST
Carolina	Gonzalez	carolinamgonzalez03@gmial.com		San Francisco	California	CA	94109	US			2021-07-13 23:18:15 EST
Stuart	Abrams	stuartabrams92@gmail.com		San Francisco	California	CA	94109	US			2021-07-14 10:57:52 EST
Katie	Wimer	ktwimer@gmail.com		San Francisco	California	CA	94109	US	I support the approval of additional parking for		2021-07-14 12:35:44 EST
Blas	Rodriguez	blasandresrv@hotmail.com		San Francisco	California	CA	94109	US			2021-07-14 12:58:08 EST
Megan	Calkins	mcalkins13@gmail.com		San Francisco	California	CA	94109	US			2021-07-16 11:42:07 EST
Emily	Richardson	ejrich610@gmail.com		San Francisco	California	CA	94109	US			2021-07-16 11:48:40 EST
Alexa	Rodrigues	alexagigliotti@yahoo.com		San Francisco	California	CA	94109	US			2021-07-16 11:57:11 EST
Emily	Knight	knight.emily.ann@gmail.com		San Francisco	California	CA	94109	US			2021-07-16 12:11:21 EST
Jarrett	Antone	jarrett.berkeley@gmail.com		San Francisco	California	CA	94109	US			2021-07-16 13:45:14 EST