



SAN FRANCISCO PLANNING DEPARTMENT

Executive Summary Conditional Use

HEARING DATE: JUNE 22, 2017

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San Francisco,
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Date: June 12, 2017
Case No.: **2015-007917CUA**
Project Address: **2675 GEARY BOULEVARD**
Zoning: NC-3 (Moderate-Scale Neighborhood Commercial) Zoning District
40-X and 80-D Height and Bulk District
City Center Special Sign District
Block/Lots: 1094/001
Project Sponsor: Mark Loper
Reuben, Junius & Rose, LLP
1 Bush Street, Suite 600
San Francisco, CA 94104
Staff Contact: Christopher May – (415) 575-9087
christopher.may@sfgov.org
Recommendation: **Approval with Conditions**

PROJECT DESCRIPTION

The project proposes three separate improvements to the existing City Center retail complex located at the southeast corner of Geary Boulevard and Masonic Avenue.

The first portion of the project proposes the elimination of sixteen (16) parking spaces in parking lot 'D', which is located on top of the existing four-story retail building on the northwest corner of the site, and the construction of 1- and 2- story horizontal additions totaling approximately 7,530 square feet to the existing two-story "Crow's Nest" retail building.

The second portion of the project proposes the construction of a new two-story retail building totaling approximately 22,072 square feet in parking lot 'F'. The terraced topography allows for the second-floor retail spaces to be accessed from parking lot 'E'. This building would replace fifty-eight (58) off-street parking spaces in parking lot 'F' and three (3) in parking lot 'E'. A proposed landscaped pedestrian walkway connecting O'Farrell Street through the site to Geary Boulevard on the opposite side will formalize and improve a route currently used by many neighborhood residents seeking a short cut through the parking area to the bus stop on Geary Boulevard.

The third portion of the project proposes the construction of a new one-story retail building totaling approximately 3,608 square feet on the northeast corner of Masonic Avenue and O'Farrell Street in parking lot 'A'. This retail building would replace nineteen (19) surface parking spaces with active commercial uses and a more landscaped pedestrian realm. The project also proposes to expand parking lot 'B' by constructing an elevated parking deck above parking lot 'A' and over the proposed new retail building. The expanded parking deck would increase the number of parking spaces in parking lot 'B'.

from eighty-one (81) to one hundred and seventy-one (171) and would provide direct access to the second floor of the main retail building currently occupied by Target.

The subject property currently has 640 parking spaces spread across six terraced parking levels. As a result of the proposed conversion of parking space in lots 'A', 'D' and 'F' for the additional retail uses, the amount of off-street parking would be reduced to 634 parking spaces. The total gross square footage of retail uses requires a minimum of 721 off-street parking spaces. As such, the Planned Unit Development (PUD) process pursuant to Planning Code Section 304 would allow for a modification to the off-street parking requirements of Planning Code Section 151.

While the project is not required to provide any new bicycle parking spaces, the project sponsor nevertheless proposes an additional twenty-three (23) Class 1 bicycle parking space in secure, weather-protected bicycle lockers located in parking lots 'B', 'C', 'D' and 'F' as well as an additional sixty-six (66) Class 2 bicycle parking spaces along the Masonic Avenue and O'Farrell Street sidewalks. The project also includes streetscape improvements including twelve (12) new street trees along the O'Farrell Street frontage and a large landscaped bulb-out at the intersection of O'Farrell Street and Anzavista Avenue to improve pedestrian safety next to the Raoul Wallenburg Traditional High School.

SITE DESCRIPTION AND PRESENT USE

The subject property occupies an entire block bound by Geary Boulevard to the north, Masonic Avenue to the west, O'Farrell Street to the south and Lyon Street to the east, Block 1094, Lot 001, and is approximately 288,297 square feet, or 6.6 acres, in size. The property is located within the NC-3 (Moderate-Scale Neighborhood Commercial) Zoning District, the City Center Special Sign District and the 40-X and 80-D Height and Bulk Districts. The site slopes downward from the Masonic Avenue frontage to corner of Geary Boulevard and Lyon Street, and is developed with a four-story retail building constructed in 1961 located on the northwest portion of the site surrounded by five terraced surface parking areas across the remainder of the site. There is one curb cut providing vehicular access to the site on the Masonic Avenue frontage and five curb cuts providing access on the O'Farrell Street frontage. Half of the Geary Boulevard and the entire Lyon Street frontages are occupied by retaining walls which maintain an elevated grade for surface parking lots 'E' and 'F'.

SURROUNDING PROPERTIES AND NEIGHBORHOOD

The project site is located on the southeast corner of Geary Boulevard and Masonic Avenue in the Western Addition neighborhood where it abuts the Inner Richmond and Presidio Heights neighborhoods to the west. The immediate area contains a mix of apartment buildings, and commercial and public utility buildings on large lots with varied heights. Directly across Masonic Avenue and to the west of the subject lot is a three-story building containing commercial uses on the ground floor with two floors of residential uses above. Directly across Geary Boulevard and to the north of the subject lot is a two-story building which occupies the entire block and is owned by the SFMTA for vehicle storage and office uses. Across the intersection, on the northwest corner of Geary Boulevard and Masonic Avenue is a vacant one-story commercial building, formerly d.b.a. The Lucky Penny, which is proposed to be demolished and redeveloped with an eight-story mixed use building. Directly to the south of the subject property, across O'Farrell Street, are two religious institutions – the one-story Epiphany Center and the three-story Mt. St. Joseph-St. Elizabeth Church building containing religious institutional uses and transitional housing for

women and children, as well as the Raoul Wallenburg Traditional High School. Further to the east are several multi-family buildings which have their rear yards abutting O'Farrell Street. Across Lyon Street and to the east is the Kaiser Permanente San Francisco Medical Center, which occupies the entire block. The central portion of Geary Boulevard, which abuts the north side of the subject property, includes two lanes of vehicular traffic in both directions tunneled beneath Masonic Avenue. The outer portions of Geary Boulevard include one lane of vehicular traffic and curbside parking, and are located at grade and form two separate intersections with Masonic Avenue.

ENVIRONMENTAL REVIEW

The Project was determined to be exempt from the California Environmental Quality Act ("CEQA") as a Class 32 categorical exemption.

HEARING NOTIFICATION

TYPE	REQUIRED PERIOD	REQUIRED NOTICE DATE	ACTUAL NOTICE DATE	ACTUAL PERIOD
Classified News Ad	20 days	June 2, 2017	May 31, 2017	22 days
Posted Notice	20 days	June 2, 2017	June 2, 2017	20 days
Mailed Notice	20 days	June 2, 2017	June 2, 2017	20 days

PUBLIC COMMENT/COMMUNITY OUTREACH

The Department has not received any public comment as of June 12, 2017, in advance of the Planning Commission hearing on June 22, 2017 with regards to the project.

ISSUES AND OTHER CONSIDERATIONS

- Development of Large Lots. The Project proposes the development of a 288,297 square-foot lot. Planning Code Section 121.1 states that new construction or significant enlargement of existing buildings on lots in excess of 10,000 square feet in the NC-3 (Moderate Scale Neighborhood Commercial) District shall be permitted only as Conditional Uses.
- Use Size Limits. The Project proposes approximately 33,210 square feet of additional non-residential uses. Per Planning Code Section 121.2, non-residential uses greater than 5,999 square feet shall be permitted only as Conditional Uses.
- Bulk. The project proposes a building length of 132 feet and a diagonal dimension of 141 feet above 40 feet in height. Planning Code Section 270 states that in the D Bulk District, the maximum length of a building is 110 feet with a maximum diagonal dimension of 140 feet above 40 feet.
- Off-Street Parking. The Project proposes a total of 634 off-street parking spaces, while Planning Code Section 151 requires a minimum of 721 off-street parking spaces, representing a deficiency of 87 parking spaces. Section 304 of the Planning Code allows for a modification of this requirement through the Planned Unit Development (PUD) process.

REQUIRED COMMISSION ACTION

In order for the project to proceed, the Commission must grant Conditional Use Authorization pursuant to Planning Code Sections 121.1, 121.2, 271, 303 and 304, to permit the construction of 1- and 2- story horizontal additions totaling 7,530 square feet to the existing 2-story “Crow’s Nest” retail building in parking lot ‘D’, a new 2-story retail building totaling 22,072 square feet in parking lot ‘F’ and a new 1-story retail building totaling 3,608 square feet on the northeast corner of Masonic Avenue and O’Farrell Street in parking lot ‘A’ within an NC-3 (Moderate-Scale Neighborhood Commercial) Zoning District, the City Center Special Sign District and the 40-X and 80-D Height and Bulk Districts.

BASIS FOR RECOMMENDATION

- The project will replace underutilized surface parking areas with new retail spaces which will spread activity across the site and better distribute the supply of parking.
- The project will provide a total of ninety-eight (98) bicycle parking spaces that will accommodate both employees and patrons of the retail center.
- The project will result in pedestrian improvements including an upgraded pedestrian pathway through the site from O’Farrell Street to Geary Boulevard.
- The project will improve the streetscape on this prominent location by adding street-facing ground-floor retail space with high-quality building materials, and will screen several surface parking areas with landscaping.
- The project would add commercial spaces of differing sizes, which will be appealing to a wide range of retailers or personal service tenants.
- The project will create new jobs for unskilled and semi-skilled workers and will increase the City’s sales tax base.
- The project will not displace any existing commercial tenants, nor would it prevent the district from achieving optimal diversity in the types of goods and services available in the surrounding neighborhood commercial district.

RECOMMENDATION:	Approval with Conditions
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Attachments:

Block Book Map

Sanborn Map

Zoning Map

Aerial Photographs

Site Photographs

Project Sponsor Submittal, including:

- Reduced Plans

Attachment Checklist

- | | |
|---|--|
| <input checked="" type="checkbox"/> Executive Summary | <input checked="" type="checkbox"/> Project sponsor submittal |
| <input checked="" type="checkbox"/> Draft Motion | Drawings: <u>Existing Conditions</u> |
| <input type="checkbox"/> Environmental Determination | <input checked="" type="checkbox"/> Check for legibility |
| <input checked="" type="checkbox"/> Zoning District Map | Drawings: <u>Proposed Project</u> |
| <input checked="" type="checkbox"/> Height & Bulk Map | <input checked="" type="checkbox"/> Check for legibility |
| <input checked="" type="checkbox"/> Block Map | 3-D Renderings (new construction or significant addition) |
| <input checked="" type="checkbox"/> Sanborn Map | <input checked="" type="checkbox"/> Check for legibility |
| <input checked="" type="checkbox"/> Aerial Photo | <input type="checkbox"/> Wireless Telecommunications Materials |
| <input checked="" type="checkbox"/> Site Photos | <input type="checkbox"/> Health Dept. review of RF levels |
| <input type="checkbox"/> Public Correspondence | <input type="checkbox"/> RF Report |
| <input type="checkbox"/> Previously-Approved Plans | <input type="checkbox"/> Community Meeting Notice |
| | <input type="checkbox"/> Housing Documents |
| | <input type="checkbox"/> Inclusionary Affordable Housing Program: Affidavit for Compliance |

Exhibits above marked with an "X" are included in this packet

CM
Planner's Initials



SAN FRANCISCO PLANNING DEPARTMENT

Subject to: (Select only if applicable)

- | | |
|--|---|
| <input type="checkbox"/> Affordable Housing (Sec. 415) | <input checked="" type="checkbox"/> First Source Hiring (Admin. Code) |
| <input type="checkbox"/> Jobs Housing Linkage Program (Sec. 413) | <input type="checkbox"/> Child Care Requirement (Sec. 414) |
| <input type="checkbox"/> Downtown Park Fee (Sec. 412) | <input checked="" type="checkbox"/> Other |

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Planning Commission Draft Motion

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ADOPTING FINDINGS RELATING TO THE APPROVAL OF CONDITIONAL USE AUTHORIZATION PURSUANT TO PLANNING CODE SECTIONS 121.1, 121.2, 271, 303 AND 304, TO PERMIT THE CONSTRUCTION OF 1- AND 2- STORY HORIZONTAL ADDITIONS TOTALING APPROXIMATELY 7,530 SQUARE FEET TO THE EXISTING 2-STORY “CROW’S NEST” RETAIL BUILDING IN PARKING LOT ‘D’, A NEW 2-STORY RETAIL BUILDING TOTALING APPROXIMATELY 22,072 SQUARE FEET IN PARKING LOT ‘F’ AND A NEW 1-STORY RETAIL BUILDING TOTALING APPROXIMATELY 3,608 SQUARE FEET ON THE NORTHEAST CORNER OF MASONIC AVENUE AND O’FARRELL STREET IN PARKING LOT ‘A’ WITHIN AN NC-3 (MODERATE-SCALE NEIGHBORHOOD COMMERCIAL) ZONING DISTRICT, THE CITY CENTER SPECIAL SIGN DISTRICT AND THE 40-X AND 80-D HEIGHT AND BULK DISTRICTS. THE PROJECT ALSO PROPOSES TO EXPAND PARKING LOT ‘B’ BY CONSTRUCTING AN ELEVATED PARKING DECK ABOVE PARKING LOT ‘A’ AND THE PROPOSED NEW RETAIL BUILDING AT THE CORNER OF MASONIC AVENUE AND O’FARRELL STREET. THE PLANNED UNIT DEVELOPMENT (PUD) PROCESS PURSUANT TO PLANNING CODE SECTION 304 WOULD ALLOW FOR A MODIFICATION TO THE OFF-STREET PARKING REQUIREMENTS OF PLANNING CODE SECTION 151.

PREAMBLE

On April 21, 2016, Mark Loper (hereinafter "Project Sponsor") filed an application with the Planning Department (hereinafter "Department") for Conditional Use Authorization under Planning Code Sections 121.1, 121.2, 271, 303 and 304, to permit the construction of 1- and 2- story horizontal additions totaling approximately 7,530 square feet to the existing 2-story "Crow's Nest" retail building in parking lot 'D', a new 2-story retail building totaling approximately 22,072 square feet in parking lot 'F' and a new 1-story retail building totaling approximately 3,608 square feet on the northeast corner of Masonic Avenue and O'Farrell Street in parking lot 'A' within an NC-3 (Moderate-Scale Neighborhood Commercial) Zoning District, the City Center Special Sign District and the 40-X and 80-D Height and Bulk Districts. The project also proposes to expand parking lot 'B' by constructing an elevated parking deck above parking lot 'A' and the proposed new retail building at the corner of Masonic Avenue and O'Farrell Street. The Planned Unit Development (PUD) process pursuant to Planning Code Section 304 would allow for a modification to the off-street parking requirements of Planning Code Section 151.

On June 22, 2017, the San Francisco Planning Commission (hereinafter "Commission") conducted a duly noticed public hearing at a regularly scheduled meeting on Conditional Use Application No. 2015-007917CUA.

The Project is exempt from the California Environmental Quality Act ("CEQA") as a Class 32 categorical exemption.

The Commission has heard and considered the testimony presented to it at the public hearing and has further considered written materials and oral testimony presented on behalf of the applicant, Department staff, and other interested parties.

MOVED, that the Commission hereby authorizes the Conditional Use requested in Application No. 2015-007917CUA, subject to the conditions contained in "EXHIBIT A" of this motion, based on the following findings:

FINDINGS

Having reviewed the materials identified in the preamble above, and having heard all testimony and arguments, this Commission finds, concludes, and determines as follows:

1. The above recitals are accurate and constitute findings of this Commission.
2. **Site Description and Present Use.** The subject property occupies an entire block bound by Geary Boulevard to the north, Masonic Avenue to the west, O'Farrell Street to the south and Lyon Street to the east, Block 1094, Lot 001, and is approximately 288,297 square feet, or 6.6 acres, in size. The property is located within the NC-3 (Moderate-Scale Neighborhood Commercial) Zoning District, the City Center Special Sign District and the 40-X and 80-D Height and Bulk Districts. The site slopes downward from the Masonic Avenue frontage to corner of Geary Boulevard and Lyon Street, and is developed with a four-story retail building constructed in 1961 located on the northwest portion of the site surrounded by five terraced surface parking areas across the remainder of the site. There is one curb cut providing vehicular access to the site on the Masonic Avenue frontage and five curb cuts providing access on the O'Farrell Street frontage.

Half of the Geary Boulevard and the entire Lyon Street frontages are occupied by retaining walls which maintain an elevated grade for surface parking lots 'E' and 'F'.

3. **Surrounding Properties and Neighborhood.** The project site is located on the southeast corner of Geary Boulevard and Masonic Avenue in the Western Addition neighborhood where it abuts the Inner Richmond and Presidio Heights neighborhoods to the west. The immediate area contains a mix of apartment buildings, and commercial and public utility buildings on large lots with varied heights. Directly across Masonic Avenue and to the west of the subject lot is a three-story building containing commercial uses on the ground floor with two floors of residential uses above. Directly across Geary Boulevard and to the north of the subject lot is a two-story building which occupies the entire block and is owned by the SFMTA for vehicle storage and office uses. Across the intersection, on the northwest corner of Geary Boulevard and Masonic Avenue is a vacant one-story commercial building, formerly d.b.a. The Lucky Penny, which is proposed to be demolished and redeveloped with an eight-story mixed use building. Directly to the south of the subject property, across O'Farrell Street, are two religious institutions – the one-story Epiphany Center and the three-story Mt. St. Joseph-St. Elizabeth Church building containing religious institutional uses and transitional housing for women and children, as well as the Raoul Wallenburg Traditional High School. Further to the east are several multi-family buildings which have their rear yards abutting O'Farrell Street. Across Lyon Street and to the east is the Kaiser Permanente San Francisco Medical Center, which occupies the entire block. The central portion of Geary Boulevard, which abuts the north side of the subject property, includes two lanes of vehicular traffic in both directions tunneled beneath Masonic Avenue. The outer portions of Geary Boulevard include one lane of vehicular traffic and curbside parking, and are located at grade and form two separate intersections with Masonic Avenue.
4. **Project Description.** The project proposes the elimination of sixteen (16) parking spaces in parking lot 'D', which is located on top of the existing four-story retail building on the northwest corner of the site, and the construction of 1- and 2- story horizontal additions totaling approximately 7,530 square feet to the existing two-story "crow's nest" retail building.

A new two-story retail building totaling approximately 22,072 square feet is proposed in parking lot 'F'. The terraced topography allows for the second-floor retail spaces to be accessed from parking lot 'E'. This building would replace fifty-eight (58) off-street parking spaces in parking lot 'F' and three (3) in parking lot 'E'. The proposed retail building would be set back approximately 25 feet from the O'Farrell Street lot line, with an enclosed solid waste enclosure and loading spaced screened by a large landscaped buffer and an outdoor seating area located along the street edge. A landscaped pedestrian walkway connecting O'Farrell Street through the site to Geary Boulevard on the opposite side will formalize and improve a route currently used by many neighborhood residents seeking a short cut through the parking area to the bus stop on Geary Boulevard.

A new one-story retail building totaling approximately 3,608 square feet is proposed to be constructed on the northeast corner of Masonic Avenue and O'Farrell Street in parking lot 'A'. This retail building would replace nineteen (19) surface parking spaces with active commercial uses and a more landscaped pedestrian realm. The project also proposes to expand parking lot

'B' by constructing an elevated parking deck above parking lot 'A' and over the proposed new retail building. The expanded parking deck would increase the number of parking spaces in parking lot 'B' from eighty-one (81) to one hundred and seventy-one (171) and would provide direct access to the second floor of the main retail building currently occupied by Target.

The subject property currently has 640 parking spaces spread across six terraced parking levels. As a result of the proposed conversion of parking space in lots 'A', 'D' and 'F' for the additional retail uses, the amount of off-street parking would be reduced to 634 parking spaces. The total gross square footage of retail uses requires a minimum of 721 off-street parking spaces. As such, the Planned Unit Development (PUD) process pursuant to Planning Code Section 304 would allow for a modification to the off-street parking requirements of Planning Code Section 151.

The subject property currently has nine (9) Class 1 bicycle parking spaces located in parking lots 'B' and 'C' and has twenty-four Class 2 bicycle parking spaces located in parking lots 'A', 'C', 'E' and 'F'. While the project is not required to provide any new bicycle parking spaces, because the proposed addition amounts to an increase in gross floor area less than 20% of the existing gross floor area, the project sponsor nevertheless proposes an additional twenty-three (23) Class 1 bicycle parking space in secure, weather-protected bicycle lockers located in parking lots 'B', 'C', 'D' and 'F' as well as an additional sixty-six (66) Class 2 bicycle parking spaces along the Masonic Avenue and O'Farrell Street sidewalks, in accordance with the Better Streets Plan guidelines. The project also includes streetscape improvements including twelve (12) new street trees along the O'Farrell Street frontage and a large landscaped bulb-out at the intersection of O'Farrell Street and Anzavista Avenue to improve pedestrian safety next to the Raoul Wallenburg Traditional High School.

5. **Public Comment.** As of June 12, 2017, the Department had not received any public comment in advance of the Planning Commission hearing on June 22, 2017.
6. **Planning Code Compliance:** The Commission finds that the Project is consistent with the relevant provisions of the Planning Code in the following manner:
 - A. **Development of Large Lots.** Planning Code Section 121.1 states that in order to promote, protect, and maintain a scale of development which is appropriate to each district and compatible with adjacent buildings, new construction or significant enlargement of existing buildings on lots in excess of 10,000 square feet in the NC-3 (Moderate Scale Neighborhood Commercial) District shall be permitted only as conditional uses.

The Project proposes the development of a 288,297 square-foot lot and therefore requires Conditional Use Authorization. The additional required findings are listed below under Subsection 7.

- B. **Use Size Limits.** Per Planning Code Section 121.2, non-residential uses greater than 5,999 square feet shall be permitted only as conditional uses.

The Project proposes approximately 33,210 square feet of additional non-residential uses and therefore requires Conditional Use Authorization. The additional required findings are listed below under Subsection 8.

- C. **Floor Area Ratio.** Planning Code Section 124 states that in the NC-3 Zoning District, the basic floor area ratio limit shall be 3.6 to 1.

The subject property is approximately 288,297 square feet in size. As such, the maximum permitted basic floor area ratio would allow for a total of 1,037,869 square feet. The total proposed gross floor area of the Project is 241,297 square feet and therefore complies with this requirement.

- D. **Street Frontage in Neighborhood-Commercial Districts.** Section 145.1 of the Planning Code requires that, with the exception of space allowed for parking and loading access, building egress, and access to mechanical systems, space for active uses shall be provided within the first 25 feet of building depth on the ground floor and 15 feet on floors above from any facade facing a street at least 30 feet in width. The floors of street-fronting interior spaces housing non-residential active uses and lobbies shall be as close as possible to the level of the adjacent sidewalk at the principal entrance to these spaces. Frontages with active uses must be fenestrated with transparent windows and doorways for no less than 60 percent of the street frontage at the ground level and allow visibility to the inside of the building. The use of dark or mirrored glass shall not count towards the required transparent area. No more than one-third of the width or 20 feet, whichever is less, of any given street frontage shall be devoted to parking ingress or egress.

The existing Crow's Nest building within parking lot 'D' is elevated above the existing four-story retail building and has no street frontage. The proposed new retail buildings in parking lots 'A' and 'F' would both have ceiling heights exceeding 14 feet and would have transparent glazing across the majority of their street-facing facades.

- E. **Off-Street Parking.** Planning Section 151 of the Planning Code requires a minimum of one (1) off-street parking space for every 500 square feet of retail uses for the first 20,000 square feet plus one (1) space for every 250 square feet for the portion of retail uses exceeding 20,000 square feet. The Planning Code also requires a minimum of one (1) off-street parking space for every 1,000 square feet of occupied floor area for service and repair uses, and a minimum of one (1) off-street parking space for every 200 square feet of occupied floor area for restaurant uses.

The Project proposes a total of 182,919 square feet of occupied floor area for retail uses, 2,874 square feet of occupied floor area for service and repair uses, and 5,240 square feet of occupied floor area for restaurant uses. As such, a minimum of 721 off-street parking spaces are required for the proposed uses within the three buildings. The project proposes a total of 634 off-street parking spaces, representing a deficiency of 87 parking spaces, which will be modified through the Planned Unit Development process. The criteria and limitations pursuant to Planning Code Section 304 are listed below under Subsection 11.

- F. **Bicycle Parking.** Planning Code Section 155.2 requires the provision of Class 1 and Class 2 bicycle parking spaces when a change of occupancy or increase in intensity of use is proposed which would increase the gross square footage by more than 20 percent.

The project proposes to increase the gross square footage by less than 20 percent of the existing gross floor area, and therefore no additional bicycle parking spaces are required. Nevertheless, the project sponsor proposes to increase the number of Class 1 bicycle parking spaces from nine (9) to thirty-two (32) and increase the number of Class 2 bicycle parking spaces from thirty-six (36) to sixty-six (66).

- G. **Transportation Demand Management (TDM) Plan.** Pursuant to Planning Code Section 169 and the TDM Program Standards, the Project shall finalize a TDM Plan prior to Planning Department approval of the first Building Permit or Site Permit. As currently proposed, the Project must achieve a target of 13 points.

The Project submitted a completed Environmental Evaluation Application prior to September 4, 2016. Therefore, the Project must only achieve 50% of the point target established in the TDM Program Standards, resulting in a required target of 6.5 points. As currently proposed, the Project will achieve 12 points through the following TDM measures:

- *Parking Supply*
- *Bicycle Parking (Option A)*

- H. **Building Height.** Per Planning Code Section 260, the maximum height limit for the subject property is 80 feet for the northwest portion of the site which is currently occupied by a four-story retail building, and 40 feet for the remainder of the site.

The Crow's Nest portion of the project proposes a building height of approximately 71 feet, the retail building in parking lot 'A' proposes a building height of approximately 23 feet and the retail building in parking lot 'F' proposes a building height of approximately 39 feet, all of which would be compliant with these requirements.

- I. **Bulk.** Planning Code Section 270 states that in the D Bulk District, the maximum length of a building is 110 feet with a maximum diagonal dimension of 140 feet above 40 feet.

The Crow's Nest portion of the project proposes a building length of 132 feet and a diagonal dimension of 141 feet above 40 feet in height. As the Project exceeds both the maximum permitted building length and diagonal dimension, a bulk exception is being sought. The additional required findings are listed below under Subsection 9.

- J. **Shadow.** Planning Code Section 295 restricts net new shadow, cast by structures exceeding a height of 40 feet, upon property under the jurisdiction of the Recreation and Park Commission. Any project in excess of 40 feet in height and found to cast net new shadow must be found by the Planning Commission, with comment from the General Manager of the Recreation and Parks Department, in consultation with the Recreation and Park Commission, to have no adverse impact upon the property under the jurisdiction of the Recreation and Park Commission.

Based upon a shadow analysis, the Project does not cast any net new shadow upon property under the jurisdiction of the Recreation and Parks Commission.

- K. **Neighborhood Notification.** Planning Code Section 312 requires notification to all owners and occupants within 150 feet of the subject property when new construction is proposed in the Neighborhood Commercial District.

The proposal is located in the NC-3 (Moderate Scale Neighborhood Commercial) District and proposes the construction of two retail buildings and an expansion to an existing commercial building. This new construction requires Neighborhood Notification per Planning Code Section 312. Accordingly, notification was conducted in conjunction with the noticing for the Conditional Use Authorization.

- L. **Transportation Sustainability Fee.** Planning Code Section 411A is applicable to any development project that proposes additions of 800 gross square feet or more to an existing non-residential use.

The Project proposes the addition of approximately 33,210 square feet of retail uses and is therefore subject to the Transportation Sustainability Fee. These fees must be paid prior to the issuance of the first construction document.

- M. **First Source Hiring.** The Project is subject to the requirements of the First Source Hiring Program as they apply to any building permit application for a commercial activity over 25,000 square feet in floor area and involving new construction, an addition, or alteration (Section 83.4(m) of the Administrative Code), and the Project Sponsor shall comply with the requirements of this Program as to all construction work and on-going employment required for the Project. Prior to the issuance of any building permit to construct or a First Addendum to the Site Permit, the Project Sponsor shall have a First Source Hiring Construction and Employment Program approved by the First Source Hiring Administrator, and evidenced in writing. In the event that both the Director of Planning and the First Source Hiring Administrator agree, the approval of the Employment Program may be delayed as needed.

Prior to issuance of a building permit, the Project Sponsor will submit a First Source Hiring Affidavit and will execute a First Source Hiring Memorandum of Understanding and a First Source Hiring Agreement with the City's First Source Hiring Administration.

7. **Development of Large Lots.** In addition to the criteria of Section [303](#)(c) of the Planning Code, as it pertains to the development of large lots, the City Planning Commission shall consider the extent to which the following criteria are met:

- A. The mass and facade of the proposed structure are compatible with the existing scale of the district.

The scale of development along Geary Boulevard is mixed, with an eclectic architectural character and many blocks featuring large building footprints with no prevailing style establishing a dominant visual pattern. The project's proposed massing and facades are in keeping with the existing character of the City Center site are compatible and the existing scale of the surrounding neighborhood commercial district.

- B. The facade of the proposed structure is compatible with design features of adjacent facades that contribute to the positive visual quality of the district.

The City Center itself is primarily a four-story stand-alone shopping center located at the northwest portion of the site, at the intersection of Geary Boulevard and Masonic Avenue. The expansion to the Crow's Nest building extends the first and second stories with an expansive glass window consistent with the existing ground floor footprint, improving the usability of this as a leasable space as well as its perception from the parking lot and surrounding neighborhoods. The retail building located in parking lot 'F' utilizes the existing topography on the site and the 17-foot retaining wall separating parking lots 'E' and 'F' to create an elegantly framed, two-story structure with direct ground floor access to both levels. The upper level has direct access from parking lot 'E' and the lower level from parking lot 'F'. The upper floor of the building makes a suitable complement to the recently completed shops across the parking lot and utilizes a lower floor-to-floor height than the ground floor to relate to the scale of existing retail on site and the adjacent buildings across the street backing onto O'Farrell Street. A network of landscaped and outdoor seating areas respond to the existing curvature of the lot and provide a more welcoming experience to site users, while doubling as stormwater infiltration and remediation. A new midblock pedestrian walkway will be created adjacent to the upper floor retail uses fronting parking lot 'E' with a new public stair connection to provide a new formalized path of travel for neighbors crossing the site to gain access to Geary Boulevard bus stop. The retail building in parking lot 'A' will add an active use at the corner of Masonic Avenue and O'Farrell Street, and will provide landscaping along both frontages in areas that are not occupied by pedestrian and vehicle entrances onto the site or the building itself. In addition, the parking deck expanding parking lot 'B' will feature screening and greening requirements consistent with the Planning Code, ensuring adequate landscaping along the second story as viewed from the pedestrian realm. Both the ground floor retail space and landscaping, and the expanded parking area's screening and greening features, will improve the district's visual quality. All three buildings that comprise the Project will be designed with materials that are similar in style to the existing materials on the City Center—which was significantly re clad as part of a comprehensive upgrade to the City Center completed in 2013. Attention has been given to complement the existing building's fenestration.

8. **Non-Residential Use Size Limits.** Planning Code Section 121.2 provides that the Planning Commission shall consider the extent to which the following criteria are met:

- A. The intensity of activity in the district is not such that allowing the larger use will be likely to foreclose the location of other needed neighborhood-serving uses in the area.

In the NC-3 zoning district within which the subject property is located, Conditional Use Authorization is required for all projects proposing a non-residential use exceeding 6,000 square feet. Geary Boulevard is a somewhat more prominent and heavily-trafficked street than the streets most other NC districts are focused around, particularly in and around the intersection with Masonic Avenue. Many of the uses and buildings in the area tend to match the large scale of the streets. The Kaiser Permanente San Francisco Medical Center occupies the entire block east of the subject property on Geary Boulevard, while on the north side of Geary Boulevard, there are several four and five-story residential buildings, as well as a seven-story self-storage building, and a two-story building which occupies the entire block and is owned by the SFMTA for vehicle storage and office uses. The project

proposes an expansion representing approximately 13.5% of the existing 208,087 square-foot City Center that is unlikely to cause the same sort of disruption to existing retailers as one located a typical Neighborhood Commercial corridor, such as the nearby Fillmore or Divisadero Street corridors. The proposed project would not preclude other smaller-scale neighborhood serving commercial uses from continuing to operate along these streets.

- B. The proposed use will serve the neighborhood, in whole or in significant part, and the nature of the use requires a larger site in order to function.

Although City Center is a unique property in that it was designed in an auto-oriented era, it nevertheless contains a number of smaller retailers that complement larger anchor tenants such as Target. The proposed additions to City Center will create at least three spaces that could be occupied by a variety of different tenants. The two proposed retail buildings at the corner of Masonic Avenue and O'Farrell Street and within parking lot 'F' adjacent to O'Farrell Street are ideal locations for smaller, stand-alone retailers offering goods and services for local residents as well as those who travel from beyond the immediate neighborhood.

- C. The building in which the use is to be located is designed in discrete elements which respect the scale of development in the district.

Each of the three portions of the project are meant to have a minimal visual impact on the surrounding district, despite the subject site itself being a stand-alone commercial complex occupying its entire block. The expansion to the Crow's Nest building increases the usability of this space while having a minimal impact as perceived from the parking lot or surrounding neighborhoods. The proposed retail building in parking lot 'F' is set back from the O'Farrell Street property line, allowing for generous landscaping and creates the effect of being a single-story building as viewed from a majority of the property, reducing its apparent massing. The proposed retail building along the Masonic Avenue frontage will significantly improve the public realm by adding an active use along a significant portion of the Masonic Avenue frontage, with a modern transparent design surrounded by landscaping. The proposed rooftop parking area above will feature screening and greening as required by the Planning Code to conceal the parking from the public realm.

9. **Bulk Exception Findings.** Planning Code Section 271 establishes criteria to allow exceptions to the Bulk limit with Conditional Use Approval. On balance, the project does comply with said criteria in that:

- a. The appearance of the bulk in the building, structure or development shall be reduced by means of at least one and preferably a combination of the following factors, so as to produce the impression of an aggregate of parts rather than a single building mass.
- i. Major variations in the planes of wall surfaces, in either depth or direction, that significantly alter the mass;

There are a number of features on the proposed addition to the Crow's Nest building that add variation to the planes of the wall surfaces. A ground floor protrusion facing parking lot 'D' provides depth at the pedestrian level. Offsetting rows of windows between the ground and second

story break up the massing while the second story includes a canopy wrapping around the windowed portion of the building. Paneling on the eastern façade of the building moderates bulk, as do two vertical bands of darker panels on this façade that contrast with the primarily white palate on this façade.

- ii. Significant differences in the heights of various portions of the building, structure or development that divide the mass into distinct elements;

The project will maintain the existing vertical non-usable architectural element that projects from the Crow's Nest building. This feature provides a significant difference in height within the Crow's Nest structure itself. The larger City Center site contains buildings of different height, consistent with its split height district. The Crow's Nest and the large retail structure it sits on provide a sense of verticality on the lot, while the remaining existing and proposed retail spaces either are or will be located below the site's off-street parking. The parking lots themselves generally terrace downwards to the east and west away from a high point approximately at the middle of O'Farrell Street, consistent with site topography.

- iii. Differences in materials, colors or scales of the facades that produce separate major elements;

The Crow's Nest building features different materials, colors, and scales. The ground floor would be a dark slate grey color, and the second story would be white. As noted above, its windows will be offsetting, with ground floor windows pushed to the center of the building while the windows on the second story would wrap around the southern two-thirds of the floor. The windows are designed using discrete panels and do not feature large unbroken glazed segments.

- iv. Compensation for those portions of the building, structure or development that may exceed the bulk limits by corresponding reduction of other portions below the maximum bulk permitted; and

The northwest and northeast corners of the ground floor of the Crow's Nest building are notched inward to compensate for the portion of the building which exceeds the bulk limit. An ADA-accessible ramp and Class 2 bicycle parking spaces are located within these notched areas.

- v. In cases where two or more buildings, structures or towers are contained within a single development, a wide separation between such buildings, structures or towers.

The existing and proposed structures on the property are separated. The largest structure on the site containing the majority of the retailers is located at the intersection of Geary Boulevard and Masonic Avenue. The proposed two new retail buildings are separated from one other by terraced parking areas.

- b. In every case the building, structure or development shall be made compatible with the character and development of the surrounding area by means of all of the following factors:

- i. A silhouette harmonious with natural land-forms and building patterns, including the patterns produced by height limits;

The Crow's Nest building and the large retail structure it sits on provide a sense of verticality on the site. The two proposed retail buildings will generally follow the downwards sloping topography.

- ii. Either maintenance of an overall height similar to that of surrounding development or a sensitive transition, where appropriate, to development of a dissimilar character;

Conditions in the area are atypical for neighborhood commercial districts, which are generally characterized by small- to mid-sized businesses, often located on the ground floor of mixed-use buildings with residences above. The area surrounding the property and the subject property itself are auto-oriented in their scale and design. At the northern edge of the property, Geary Boulevard is a nearly 200-foot wide recessed expressway with exit ramps providing access to Masonic Avenue, which itself is approximately 200-feet wide at its intersection with Geary Boulevard. Many of the uses and buildings in the area tend to match the large scale of the streets. A mid-rise Kaiser Permanente medical center occupies several blocks of Geary Boulevard to the west of the property. On the north side of Geary Boulevard, there are several four and five-story residential buildings, as well as a seven-story self-storage building, and a single-story restaurant. A Trader Joe's grocery is located just north of Geary Boulevard on Masonic Avenue. The project reflects this somewhat unique setting. It is most dense and tall at the northeast corner fronting Geary Boulevard and Masonic Avenue. The project will not adversely affect the urban form of the surrounding neighborhood, or impact important views or sightlines across the property. The horizontal expansion to the Crow's Nest building increases the usability of this space while having a minimal impact as perceived from the parking lot or surrounding neighborhoods. The third story of the Crow's Nest would be eliminated as part of the project, removing 1,340 square feet of space. The lower level of the proposed retail building in parking lot 'F' would be concealed behind a retaining wall maintaining the grade for parking lot 'E' while the proposed retail building in parking lot 'A' would be nestled beneath the parking deck expanding parking lot 'B'.

- iii. Use of materials, colors and scales either similar to or harmonizing with those of nearby development; and

The scale of the proposed improvements to the Crow's Nest building is in harmony with the existing building as it maintains the existing height and length. Overhanging canopies, building materials of contrasting colors are all utilized to complement the existing development and provide interest in the building facades. The proposed cement plaster and high efficiency fenestration on the facades blend in with the existing concrete forms and storefront entrances of the City Center.

- iv. Preservation or enhancement of the pedestrian environment by maintenance of pleasant scale and visual interest.

The project will provide a landscaped pedestrian walkway from O'Farrell Street, past the upper floor of the retail building fronting parking lot 'E', and onto Geary Boulevard directly adjacent to

a MUNI bus stop. This mid-block pathway should benefit pedestrian circulation throughout the property and ease access to and from the bus stop. The project also will improve portions of O'Farrell Street adjacent to the Property, consistent with the Better Streets Plan. These improvements include a large bulb-out with landscaping features at the intersection of O'Farrell Street and Anzavista Avenue.

10. **Planning Code Section 303** establishes criteria for the Planning Commission to consider when reviewing applications for Conditional Use approval. On balance, the project complies with said criteria in that:

- A. The proposed use or feature, at the size and intensity contemplated and at the proposed location, will provide a development that is necessary or desirable for, and compatible with, the neighborhood or the community.

As currently configured, inactivity in portions of the parking lots farthest from the main retail building creates an opportunity for increased crime. The two retail buildings in parking lots 'A' and 'F' will replace portions of existing surface parking with more active and pedestrian-friendly uses fronting on Masonic Avenue and O'Farrell Street with outdoor seating and landscape improvements. Enlivening these areas with ground-floor retail and a corresponding increase in retail shoppers in the parking lots should deter these kinds of activities. The retail building in parking lot 'A' will add a stand-alone active use on this prominent street corner and will also add landscaping features both at the ground level and on the upper story to screen the expanded parking deck, significantly improving the public realm. The Crow's Nest expansion will add viable commercial space in an underutilized portion of the City Center. Overall, the Project is necessary and desirable in that it adds a number of retail spaces of different sizes and in various locations throughout the site. As part of the Project, and in consultation with residents in the Anza Vista neighborhood, the Project will involve improvements to a pedestrian through-way from O'Farrell Street, past the retail building in parking lot 'F', and onto Geary Boulevard. The Project also will improve portions of O'Farrell Street itself adjacent to the property, including a large bulb-out with landscaping features at the intersection of O'Farrell and Anzavista Avenue. The proposed project should offer additional services and retail outlets for local residents and will support the presence of the existing large-scale retailers currently operating on the site.

- B. The proposed project will not be detrimental to the health, safety, convenience or general welfare of persons residing or working in the vicinity. There are no features of the project that could be detrimental to the health, safety or convenience of those residing or working the area, in that:

- i. Nature of proposed site, including its size and shape, and the proposed size, shape and arrangement of structures;

The City Center has operated as a retail shopping center for approximately 50 years. The Project will not adversely affect the urban form of the surrounding neighborhood, or impact important public views or sightlines across the property. The expansion to the Crow's Nest increases the usability of this space while having a minimal impact as perceived from the parking lot or

surrounding neighborhoods, as it will be no taller than it is currently. The retail building in parking lot 'F' will be situated such that it will create the effect of being a single-story building as viewed from a majority of the properties across O'Farrell Street, reducing its apparent massing. The retail building in parking lot 'A' adds an active use within a building well below the maximum permitted building height with additional landscaping at this street corner.

- ii. The accessibility and traffic patterns for persons and vehicles, the type and volume of such traffic, and the adequacy of proposed off-street parking and loading;

The subject property is well-served by public transit, on-site parking, and bicycle spaces. A number of MUNI bus lines run directly in front of the site or nearby, including the 38-Geary, 33-Masonic, 1-California, 2-Clement, 4-Sutter, and 31-Balboa. In addition, the subject site abuts the Geary Boulevard transit corridor, which is proposed to be redesigned for a bus rapid transit (BRT) line in the near future. The Property currently has 640 parking spaces, 5 off-street freight loading spaces, and approximately 47 bicycle parking spaces. The Project will relocate and replace all eliminated parking spaces except for six, reducing the total from 640 spaces to 634. Parking will be rearranged to maximize efficiency throughout City Center and obviate the need for retail customers to leave the City Center to look for parking in surrounding neighborhoods. Currently, a disproportionate amount of off-street parking is located in the eastern portion of the site in parking lots 'E' and 'F' which remains underutilized even during peak retail shopping hours due to its distance from the primary retail node at the northwest corner of the site. The Project would locate additional retail space within parking lot 'F' and add active storefronts along parking lots 'E' and 'F', which should balance the distribution of vehicles parking across the entire site. The project also proposes to improve pedestrian circulation throughout the site and will include a formal pedestrian through-way from O'Farrell Street, past the retail building along parking lot 'E', and onto Geary Boulevard to improve neighborhood access to MUNI and other transit options on Geary Boulevard. The project also will improve portions of O'Farrell Street adjacent to the subject property including a large landscaped bulb-out at the intersection of O'Farrell and Anzavista Avenue.

- iii. The safeguards afforded to prevent noxious or offensive emissions such as noise, glare, dust and odor;

The Project will implement necessary safeguards to prevent noxious or offensive emissions such as noise, glare, dust and odor.

- iv. Treatment given, as appropriate, to such aspects as landscaping, screening, open spaces, parking and loading areas, service areas, lighting and signs;

The project proposes a number of street improvements along a portion O'Farrell Street consistent with the Better Streets Plan. These improvements include a large bulb-out with landscaping features at the intersection of O'Farrell and Anzavista Avenue. The project will also provide a pathway for pedestrians to cross the subject property from O'Farrell Street, past the upper level of the proposed retail building fronting parking lot 'E', through to Geary Boulevard at Presidio Avenue, providing a pedestrian-friendly mid-block walkway through the site. The retail building within parking lot 'A' will introduce an active use at the corner of Masonic Avenue and O'Farrell

Street, and will provide landscaping along areas that are not occupied by pedestrian and vehicle entrances onto the site. In addition, the parking deck expanding parking lot 'B' will feature screening and greening requirements consistent with the Planning Code, ensuring adequate landscaping along this area as viewed from the pedestrian realm.

- C. That the use as proposed will comply with the applicable provisions of the Planning Code and will not adversely affect the General Plan.

The project complies with all relevant requirements and standards of the Planning Code. Where the project deviates from the standard provisions of the Planning Code, the project sponsor is requesting the necessary exceptions and variances as required by the Planning Code. The project is consistent with objectives and policies of the General Plan as detailed below.

- D. That the use as proposed would provide development that is in conformity with the purpose of the applicable Neighborhood-Commercial District.

The proposed project is consistent with the stated purposes of NC-3 District in that it proposes moderate-scale commercial uses that are compatible with other uses nearby.

11. **Planning Code Section 304(d)** establishes criteria and limitations for the authorization of Planned Unit Developments (PUDs) over and above those applicable to Conditional Uses in general and contained in Section 303(c) and elsewhere in the Code. In cases of outstanding overall design, Projects may merit modification of certain Code requirements. PUDs must also:

- A. Affirmatively promote applicable objectives and policies of the General Plan;

As is further explained in Subsection 9, the Project is, on balance, consistent with the objectives and policies of the General Plan.

- B. Provide off street parking adequate for the occupancy proposed.

The project's existing occupied floor area requires a minimum of 604 off-street parking spaces pursuant to Planning Code Section 151. With the proposed additional retail floor area in the three portions of the project, the parking requirement would increase to a minimum of 721 spaces. The project currently has 640 off-street parking spaces, and the proposed project would result in a total of 63 - a loss of six spaces. Because the Project would result in a total deficit of 87 parking spaces required by the Planning Code, a modification to the minimum accessory parking standards pursuant to Section 304 is necessary. The project offers a significant amount of parking, much of which does not approach capacity at any time of week, including on weekends. Parking lots 'E' and 'F' are particularly underutilized, and the project would replace a portion of the parking in these lots with new retail space, but still leaving a total of 180 spaces (93 in parking lot 'E', and 87 in parking lot 'F') to service existing retailers and the proposed new retail uses. The Project would relocate and replace parking so more will be closer to the main retail portion of project, maximizing parking efficiency and obviating the need for retail customers to leave the subject property to look for parking in surrounding neighborhoods. The Crow's Nest expansion would eliminate 16 spaces in parking lot 'D', leaving 107 spaces. The retail building in parking lot 'A' would eliminate 19 spaces, leaving 59 spaces. In total, the Project would eliminate 96 existing stalls and would relocate and replace all but six of these stalls

in a story of parking above parking lot 'A', as an extension of parking lot 'B'. This expansion area would have direct access into the second story of the Target tenant space, reorienting parking on the site in a location where retail customers can more easily access the majority of the retail spaces, including Target, Best Buy, and ULTA. Additionally, reflecting current City policy to de-emphasize vehicle trips and encourage alternate modes of transportation, the project would increase the amount of Class II bicycle parking by 30 spaces, from 36 to 66, and would increase the amount of Class I spaces by 23, from nine to 32. It would continue to be extremely well-served by numerous MUNI transit lines, providing many options for retail shoppers to travel by methods other than private auto to the site.

- C. Provide open space usable by the occupants and, where appropriate, by the general public, at least equal to the open spaces required by the Code.

The two new retail buildings located in parking lots 'A' and 'F' would provide outdoor seating areas accessible to the public as well as a landscaped pedestrian walkway providing public access through the site to connect the O'Farrell Street frontage to the Geary Boulevard frontage.

- D. Be limited in dwelling unit density to less than the density that would be allowed by Article 2 of this Code for a District permitting a greater density, so that the PUD will not be substantially equivalent to a reclassification of property.

The project does not propose any residential uses.

- E. In R Districts, include commercial uses only to the extent that such uses are necessary to serve residents of the immediate vicinity, subject to the limitations for NC-1 (Neighborhood Commercial Cluster) Districts under the Code.

The subject property is located in the NC-3 Zoning district.

- F. Under no circumstances be excepted from any height limit established by Article 2.5 of this Code, unless such exception is explicitly authorized by the terms of this Code. In the absence of such an explicit authorization, exceptions from the provisions of this Code with respect to height shall be confined to minor deviations from the provisions for measurement of height in Sections 260 and 261 of this Code, and no such deviation shall depart from the purposes or intent of those sections.

The two new retail buildings and the addition to the existing Crow's Nest building would not exceed any height limitations established by the Planning Code.

- G. In NC Districts, be limited in gross floor area to that allowed under the floor area ratio limit permitted for the district in Section 124 and Article 7 of the Planning Code.

The subject property is approximately 288,297 square feet in size. As such, the maximum permitted basic floor area ratio would allow for a total of 1,037,869 square feet. The total proposed gross floor area of the Project is 241,297 square feet and therefore complies with this requirement.

- H. In NC Districts, not violate the use limitations by story set forth in Article 7 of the Planning Code.

The project proposes retail uses that are permitted on the 1st story, 2nd story, and 3rd stories and above pursuant to Article 7 of the Planning Code.

- I. In RTO and NCT Districts, include the extension of adjacent alleys or streets onto or through the site, and/or the creation of new publicly-accessible streets or alleys through the site as appropriate, in order to break down the scale of the site, continue the surrounding existing pattern of block size, streets and alleys, and foster beneficial pedestrian and vehicular circulation.

The subject property is located in the NC-3 Zoning district.

- J. Provide street trees as per the requirements of Section 138.1 of the Code.

The project proposes streetscape improvements including twelve (12) new street trees along the O'Farrell Street frontage and a large landscaped bulb-out at the intersection of O'Farrell Street and Anzavista Avenue, in accordance with the Better Streets Plan guidelines.

- K. Provide landscaping and permeable surfaces in any required setbacks in accordance with Section 132 (g) and (h).

The project will provide landscaping along areas that are not occupied by pedestrian and vehicle entrances onto the site between the two proposed retail buildings in parking lots 'A' and 'F'.

12. **General Plan Compliance.** The Project is, on balance, consistent with the following Objectives and Policies of the General Plan:

COMMERCE AND INDUSTRY ELEMENT

Objectives and Policies

OBJECTIVE 1:

MANAGE ECONOMIC GROWTH AND CHANGE TO ENSURE ENHANCEMENT OF THE TOTAL CITY LIVING AND WORKING ENVIRONMENT.

Policy 1.1:

Encourage development which provides substantial net benefits and minimizes undesirable consequences.

Policy 1.3:

Locate commercial and industrial activities according to a generalized commercial and industrial land use plan.

The project proposes to add approximately 33,210 square feet of commercial space in a location that has historically been and is currently occupied by a large number and range of retailers. The Project will allow future tenants to provide desirable goods or services to the neighborhood, and will increase employment opportunities for neighborhood residents.

OBJECTIVE 2:

MAINTAIN AND ENHANCE A SOUND AND DIVERSE ECONOMIC BASE AND FISCAL STRUCTURE FOR THE CITY.

Policy 2.1:

Seek to retain existing commercial and industrial activity and to attract new such activity to the city.

The project would add two discrete commercial spaces of differing sizes and would expand one existing space, which will be appealing to a wide range of retailers or personal service tenants.

OBJECTIVE 3:

PROVIDE EXPANDED EMPLOYMENT OPPORTUNITIES FOR CITY RESIDENTS, PARTICULARLY THE UNEMPLOYED AND ECONOMICALLY DISADVANTAGED.

Policy 3.1:

Promote the attraction, retention and expansion of commercial and industrial firms which provide employment improvement opportunities for unskilled and semi-skilled workers.

The project will create new jobs for unskilled and semi-skilled workers and will increase the City's sales tax base.

OBJECTIVE 6:

MAINTAIN AND STRENGTHEN VIABLE NEIGHBORHOOD COMMERCIAL AREAS EASILY ACCESSIBLE TO CITY RESIDENTS.

Policy 6.1:

Ensure and encourage the retention and provision of neighborhood-serving goods and services in the city's neighborhood commercial districts, while recognizing and encouraging diversity among the districts.

The project will not displace any existing commercial tenants, nor would it prevent the district from achieving optimal diversity in the types of goods and services available in the surrounding neighborhood commercial district. It will add two discrete spaces of differing sizes, and will expand one existing space, which will appeal to a wide range of commercial tenants.

URBAN DESIGN ELEMENT

Objectives and Policies

OBJECTIVE 3:

MODERATION OF MAJOR NEW DEVELOPMENT TO COMPLEMENT THE CITY PATTERN, THE RESOURCES TO BE CONSERVED, AND THE NEIGHBORHOOD ENVIRONMENT.

Policy 3.1:

Promote harmony in the visual relationships and transitions between new and older buildings.

Policy 3.3:

Promote efforts to achieve high quality of design for buildings to be constructed at prominent locations.

The project will improve the streetscape on this prominent location by adding street-facing ground-floor retail space with high-quality building materials, and will screen several surface parking areas with landscaping.

TRANSPORTATION ELEMENT

Objectives and Policies

OBJECTIVE 2:

USE THE TRANSPORTATION SYSTEM AS A MEANS FOR GUIDING DEVELOPMENT AND IMPROVING THE ENVIRONMENT.

Policy 2.1:

Use rapid transit and other transportation improvements in the city and region as the catalyst for desirable development, and coordinate new facilities with public and private development.

The project proposes to intensify the retail sales and services uses on the subject property within walking distance of the 38-Geary, 33-Masonic, 1-California, 2-Clement, 4-Sutter, and 31-Balboa MUNI bus lines. In addition, the subject site abuts the Geary Boulevard transit corridor, which is proposed to be redesigned for a bus rapid transit (BRT) line in the near future. The project would reduce the number of off-street parking spaces by a total of 6 spaces, which will encourage transit usage amongst visitors to the site.

OBJECTIVE 11:

ESTABLISH PUBLIC TRANSIT AS THE PRIMARY MODE OF TRANSPORTATION IN SAN FRANCISCO AND AS A MEANS THROUGH WHICH TO GUIDE FUTURE DEVELOPMENT AND IMPROVE REGIONAL MOBILITY AND AIR QUALITY.

Policy 11.3:

Encourage development that efficiently coordinates land use with transit service, requiring that developers address transit concerns as well as mitigate traffic problems.

The project is located within a neighborhood rich with public transportation and, with a reduction in the number of off-street parking spaces, some visitors of the retail center are expected to rely on public transit, cycling or walking to access the retail center.

OBJECTIVE 28:

PROVIDE SECURE AND CONVENIENT PARKING FACILITIES FOR BICYCLES.

Policy 28.1:

Provide secure bicycle parking in new governmental, commercial, and residential developments.

Policy 28.3:

Provide parking facilities which are safe, secure, and convenient.

The project will provide a total of thirty-two (32) Class 1 bicycle parking spaces within secure and convenient spaces as well as sixty-six (66) Class 2 bicycle parking spaces along the Masonic Avenue and O'Farrell Street frontages that will accommodate both employees and patrons of the retail center.

13. **Planning Code Section 101.1(b)** establishes eight priority-planning policies and requires review of permits for consistency with said policies. On balance, the project complies with said policies in that:

- A. That existing neighborhood-serving retail uses be preserved and enhanced and future opportunities for resident employment in and ownership of such businesses be enhanced.

The City Center has historically been occupied by retailers that serve a clientele from the immediate neighborhood and citywide. No neighborhood-serving retail uses will be displaced by the project. Rather, the project will create additional employment opportunities for the community, new and improved locations for businesses to open or expand, and more options for customers to access neighborhood-serving retailers.

- B. That existing housing and neighborhood character be conserved and protected in order to preserve the cultural and economic diversity of our neighborhoods.

The project will not displace existing housing or adversely affect the neighborhood character. The project will result in pedestrian and bicycle-friendly improvements to O'Farrell Street and Masonic Avenue and an upgraded pedestrian pathway through the site from O'Farrell Street to Geary Boulevard.

- C. That the City's supply of affordable housing be preserved and enhanced.

The Project will not affect the City's supply of affordable housing.

- D. That commuter traffic not impede MUNI transit service or overburden our streets or neighborhood parking.

The project is well-served by public transit, 634 proposed on-site parking spaces and a total of 98 bicycle spaces. A number of MUNI bus lines run directly in front of the site or nearby, including the 38-Geary, 33-Masonic, 1-California, 2-Clement, 4-Sutter, and 31-Balboa.

- E. That a diverse economic base be maintained by protecting our industrial and service sectors from displacement due to commercial office development, and that future opportunities for resident employment and ownership in these sectors be enhanced.

The Project will not displace any industrial or service uses or activities.

- F. That the City achieve the greatest possible preparedness to protect against injury and loss of life in an earthquake.

The Project will conform to the structural and seismic requirements of the San Francisco Building Code, meeting this requirement.

- G. That landmarks and historic buildings be preserved.

A landmark or historic building does not occupy the Project site.

- H. That our parks and open space and their access to sunlight and vistas be protected from development.

The project will have no negative impact on existing parks and open spaces. The Project does not have an impact on open spaces.

14. The Project is consistent with and would promote the general and specific purposes of the Code provided under Section 101.1(b) in that, as designed, the Project would contribute to the character and stability of the neighborhood and would constitute a beneficial development.
15. The Commission hereby finds that approval of the Conditional Use authorization would promote the health, safety and welfare of the City.

DECISION

That based upon the Record, the submissions by the Applicant, the staff of the Department and other interested parties, the oral testimony presented to this Commission at the public hearings, and all other written materials submitted by all parties, the Commission hereby **APPROVES Conditional Use Application No. 2015-007917CUA** subject to the following conditions attached hereto as "EXHIBIT A" in general conformance with plans on file, dated May 30, 2017, and stamped "EXHIBIT B", which is incorporated herein by reference as though fully set forth.

APPEAL AND EFFECTIVE DATE OF MOTION: Any aggrieved person may appeal this Conditional Use Authorization to the Board of Supervisors within thirty (30) days after the date of this Motion No. XXXXX. The effective date of this Motion shall be the date of this Motion if not appealed (After the 30-day period has expired) OR the date of the decision of the Board of Supervisors if appealed to the Board of Supervisors. For further information, please contact the Board of Supervisors at (415) 554-5184, City Hall, Room 244, 1 Dr. Carlton B. Goodlett Place, San Francisco, CA 94102.

Protest of Fee or Exaction: You may protest any fee or exaction subject to Government Code Section 66000 that is imposed as a condition of approval by following the procedures set forth in Government Code Section 66020. The protest must satisfy the requirements of Government Code Section 66020(a) and must be filed within 90 days of the date of the first approval or conditional approval of the development referencing the challenged fee or exaction. For purposes of Government Code Section 66020, the date of imposition of the fee shall be the date of the earliest discretionary approval by the City of the subject development.

If the City has not previously given Notice of an earlier discretionary approval of the project, the Planning Commission's adoption of this Motion, Resolution, Discretionary Review Action or the Zoning Administrator's Variance Decision Letter constitutes the approval or conditional approval of the development and the City hereby gives **NOTICE** that the 90-day protest period under Government Code Section 66020 has begun. If the City has already given Notice that the 90-day approval period has begun for the subject development, then this document does not re-commence the 90-day approval period.

I hereby certify that the Planning Commission ADOPTED the foregoing Motion on June 22, 2017.

Jonas P. Ionin
Commission Secretary

AYES:

NAYS:

ABSENT:

ADOPTED: June 22, 2017

EXHIBIT A

AUTHORIZATION

This authorization is to permit the construction of 1- and 2- story horizontal additions totaling 7,530 square feet to the existing 2-story "Crow's Nest" retail building in parking lot 'D', a new 2-story retail building totaling 22,072 square feet in parking lot 'F' and a new 1-story retail building totaling 3,608 square feet on the northeast corner of Masonic Avenue and O'Farrell Street in parking lot 'A', located at 2675 Geary Boulevard, Block 1094, Lot 001, pursuant to Planning Code Sections 121.1, 121.2, 270, 303 and 304 within the NC-3 (Moderate-Scale Neighborhood Commercial) Zoning District, the City Center Special Sign District and the 40-X and 80-D Height and Bulk Districts; in general conformance with plans, dated May 30, 2017, and stamped "EXHIBIT B" included in the docket for Case No. 2015-007917CUA and subject to conditions of approval reviewed and approved by the Commission on June 22, 2017 under Motion No XXXXXX. This authorization and the conditions contained herein run with the property and not with a particular Project Sponsor, business, or operator.

RECORDATION OF CONDITIONS OF APPROVAL

Prior to the issuance of the building permit or commencement of use for the Project the Zoning Administrator shall approve and order the recordation of a Notice in the Official Records of the Recorder of the City and County of San Francisco for the subject property. This Notice shall state that the project is subject to the conditions of approval contained herein and reviewed and approved by the Planning Commission on July 7, 2016 under Motion No XXXXXX.

PRINTING OF CONDITIONS OF APPROVAL ON PLANS

The conditions of approval under the 'Exhibit A' of this Planning Commission Motion No. XXXXXX shall be reproduced on the Index Sheet of construction plans submitted with the site or building permit application for the Project. The Index Sheet of the construction plans shall reference to the Conditional Use authorization and any subsequent amendments or modifications.

SEVERABILITY

The Project shall comply with all applicable City codes and requirements. If any clause, sentence, section or any part of these conditions of approval is for any reason held to be invalid, such invalidity shall not affect or impair other remaining clauses, sentences, or sections of these conditions. This decision conveys no right to construct, or to receive a building permit. "Project Sponsor" shall include any subsequent responsible party.

CHANGES AND MODIFICATIONS

Changes to the approved plans may be approved administratively by the Zoning Administrator. Significant changes and modifications of conditions shall require Planning Commission approval of a new Conditional Use authorization.

Conditions of Approval, Compliance, Monitoring, and Reporting

PERFORMANCE

1. **Validity.** The authorization and right vested by virtue of this action is valid for three (3) years from the effective date of the Motion. The Department of Building Inspection shall have issued a Building Permit or Site Permit to construct the project and/or commence the approved use within this three-year period.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

2. **Expiration and Renewal.** Should a Building or Site Permit be sought after the three (3) year period has lapsed, the project sponsor must seek a renewal of this Authorization by filing an application for an amendment to the original Authorization or a new application for Authorization. Should the project sponsor decline to so file, and decline to withdraw the permit application, the Commission shall conduct a public hearing in order to consider the revocation of the Authorization. Should the Commission not revoke the Authorization following the closure of the public hearing, the Commission shall determine the extension of time for the continued validity of the Authorization.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

3. **Diligent pursuit.** Once a site or Building Permit has been issued, construction must commence within the timeframe required by the Department of Building Inspection and be continued diligently to completion. Failure to do so shall be grounds for the Commission to consider revoking the approval if more than three (3) years have passed since this Authorization was approved.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

4. **Extension.** All time limits in the preceding three paragraphs may be extended at the discretion of the Zoning Administrator where implementation of the project is delayed by a public agency, an appeal or a legal challenge and only by the length of time for which such public agency, appeal or challenge has caused delay.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

5. **Conformity with Current Law.** No application for Building Permit, Site Permit, or other entitlement shall be approved unless it complies with all applicable provisions of City Codes in effect at the time of such approval.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

DESIGN – COMPLIANCE AT PLAN STAGE

6. **Streetscape Plan.** Pursuant to Planning Code Section 138.1, the Project Sponsor shall continue to work with Planning Department staff, in consultation with other City agencies, to refine the design and programming of the Streetscape Plan so that the plan generally meets the standards of the Better Streets Plan and all applicable City standards. The Project Sponsor shall complete final design of all required street improvements, including procurement of relevant City permits, prior to issuance of first architectural addenda, and shall complete construction of all required street improvements prior to issuance of first temporary certificate of occupancy.

For information about compliance, contact the Case Planner, Planning Department at 415-575-9087, www.sf-planning.org

7. **Street Trees.** Pursuant to Planning Code Section 138.1, the Project Sponsor shall submit a site plan to the Planning Department prior to Planning approval of the building permit application indicating that street trees, at a ratio of one street tree of an approved species for every 20 feet of street frontage along public or private streets bounding the Project, with any remaining fraction of 10 feet or more of frontage requiring an extra tree, shall be provided. The street trees shall be evenly spaced along the street frontage except where proposed driveways or other street obstructions do not permit. The exact location, size and species of tree shall be as approved by the Department of Public Works (DPW). In any case in which DPW cannot grant approval for installation of a tree in the public right-of-way, on the basis of inadequate sidewalk width, interference with utilities or other reasons regarding the public welfare, and where installation of such tree on the lot itself is also impractical, the requirements of this Section 428 may be modified or waived by the Zoning Administrator to the extent necessary.

For information about compliance, contact the Case Planner, Planning Department at 415-575-9087, www.sf-planning.org

PARKING AND TRAFFIC

8. **Bicycle Parking.** Pursuant to Planning Code Sections 155.1, 155.4, and 155.5, the Project shall provide no fewer than 98 bicycle parking spaces (32 Class 1 spaces and 66 Class 2 spaces for the commercial portion of the Project).
For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org
9. **Parking Minimum.** Pursuant to Planning Code Section 151, the Project shall provide no fewer than 634 off-street parking spaces.
For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org
10. **Managing Traffic During Construction.** The Project Sponsor and construction contractor(s) shall coordinate with the Traffic Engineering and Transit Divisions of the San Francisco Municipal Transportation Agency (SFMTA), the Police Department, the Fire Department, the Planning Department, and other construction contractor(s) for any concurrent nearby Projects to manage traffic congestion and pedestrian circulation effects during construction of the Project.
For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

PROVISIONS

11. **First Source Hiring.** The Project shall adhere to the requirements of the First Source Hiring Construction and End-Use Employment Program approved by the First Source Hiring Administrator, pursuant to Section 83.4(m) of the Administrative Code. The Project Sponsor shall comply with the requirements of this Program regarding construction work and on-going employment required for the Project.

For information about compliance, contact the First Source Hiring Manager at 415-581-2335, www.onestopSF.org

12. **Transportation Sustainability Fee.** The Project is subject to the Transportation Sustainability Fee (TSF), as applicable, pursuant to Planning Code Section 411A.

For information about compliance, contact the Case Planner, Planning Department at 415-575-9087, www.sf-planning.org

MONITORING - AFTER ENTITLEMENT

13. **Enforcement.** Violation of any of the Planning Department conditions of approval contained in this Motion or of any other provisions of Planning Code applicable to this Project shall be subject to the enforcement procedures and administrative penalties set forth under Planning Code Section 176 or Section 176.1. The Planning Department may also refer the violation complaints to other city departments and agencies for appropriate enforcement action under their jurisdiction. *For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org*

14. **Revocation due to Violation of Conditions.** Should implementation of this Project result in complaints from interested property owners, residents, or commercial lessees which are not resolved by the Project Sponsor and found to be in violation of the Planning Code and/or the specific conditions of approval for the Project as set forth in Exhibit A of this Motion, the Zoning Administrator shall refer such complaints to the Commission, after which it may hold a public hearing on the matter to consider revocation of this authorization. *For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org*

OPERATION

15. **Garbage, Recycling, and Composting Receptacles.** Garbage, recycling, and compost containers shall be kept within the premises and hidden from public view, and placed outside only when being serviced by the disposal company. Trash shall be contained and disposed of pursuant to garbage and recycling receptacles guidelines set forth by the Department of Public Works.
For information about compliance, contact Bureau of Street Use and Mapping, Department of Public Works at 415-554-.5810, <http://sfdpw.org>
16. **Sidewalk Maintenance.** The Project Sponsor shall maintain the main entrance to the building and all sidewalks abutting the subject property in a clean and sanitary condition in compliance with the Department of Public Works Streets and Sidewalk Maintenance Standards.
For information about compliance, contact Bureau of Street Use and Mapping, Department of Public Works, 415-695-2017, <http://sfdpw.org>
17. **Community Liaison.** Prior to issuance of a building permit to construct the project and implement the approved use, the Project Sponsor shall appoint a community liaison officer to deal with the issues of concern to owners and occupants of nearby properties. The Project Sponsor shall provide the Zoning Administrator with written notice of the name, business address, and telephone number of the community liaison. Should the contact information change, the Zoning Administrator shall be made aware of such change. The community liaison shall report to the Zoning Administrator what issues, if any, are of concern to the community and what issues have not been resolved by the Project Sponsor.
For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org
18. **Lighting.** All Project lighting shall be directed onto the Project site and immediately surrounding sidewalk area only, and designed and managed so as not to be a nuisance to adjacent residents. Nighttime lighting shall be the minimum necessary to ensure safety, but shall in no case be directed so as to constitute a nuisance to any surrounding property.
For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org



SAN FRANCISCO PLANNING DEPARTMENT

Certificate of Determination Exemption from Environmental Review

Case No.: 2015-007917ENV
Project Address: 2675 Geary Boulevard
Zoning: NC-3 (Moderate-Scale Neighborhood Commercial) Zoning District
40-X/80-D Height and Bulk District
Block/Lot: 1094/001
Lot Size: 288,297 square feet
Project Sponsor: Mark Loper, Reuben, Junius & Rose LLP, (415) 567-9000
Staff Contact: Don Lewis, (415) 575-9168
don.lewis@sfgov.org

1650 Mission St.
Suite 400
San Francisco,
CA 94103-2479

Reception:
415.558.6378

Fax:
415.558.6409

Planning
Information:
415.558.6377

PROJECT DESCRIPTION:

The project site is located on a large irregularly-shaped lot located at the southeast corner of Geary Boulevard and Masonic Avenue, on the block bounded by O'Farrell Street to the south, Lyon Street to the east, Geary Boulevard to the north, and Masonic Street to the west in the Western Addition neighborhood. The project site is occupied by a shopping center (City Center) and includes an existing three-story building approximately 208,090 square feet in size with 640 vehicular parking spaces located in six surface parking lots (lots A-F) that are accessed at different grades. The shopping center was constructed in 1951, contains a mixture of small- and large-scale retail businesses (including Target and Best Buy), and includes a prominent tower element that rises up to approximately 120 feet in height. The proposed project would include the construction of two new buildings (the Masonic Retail building and the Retail Pad building) and the horizontal addition to the existing three-story Crow's Nest retail building. The project would add 33,210 square feet of retail space, increasing the amount of retail space on the project site from 208,090 to 241,300 square feet.

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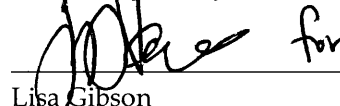
EXEMPT STATUS:

Categorical Exemption, Class 32 (State CEQA Guidelines section 15332)

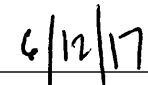
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DETERMINATION:

I do hereby certify that the above determination has been made pursuant to state and local requirements.



Lisa Gibson
Environmental Review Officer



Date

cc: Mark Loper, Project Sponsor; Christopher May, Current Planner; Supervisor Mark Farrell, District 2 (via Clerk of the Board); Distribution List; Virna Byrd, M.D.F.

PROJECT DESCRIPTION (continued):

The proposed 17-foot-tall, one-story, 3,610-square-foot Masonic Retail building would front on Masonic Avenue within the existing footprint of lot A at the west side of the project site. The proposed 36-foot-tall, two-story, 22,070-square-foot Retail Pad building would be located at the east side of the project site within lot F. The building would include approximately 900 square feet of outdoor space at level 1 and 980 square feet of outdoor space at level 2.

The proposed Crow's Nest building improvements would include the horizontal expansion of the ground floor and second floor and removal of the 1,340-square-foot third story. The proposed improvements would add approximately 7,530 square feet of retail space. With implementation of the project, the Crow's Nest building would be two stories, 36 feet tall, and 13,470 square feet in size.

Construction of the proposed buildings and the horizontal addition to the existing building would result in the removal of 96 parking stalls. The proposed project would include the construction of a new parking deck on top of lot A that would add 90 stalls to lot B. The project site currently has a total of 640 vehicle parking spaces within lots A-F.¹ The proposed project would reduce parking spaces in lots A, D, E, and F, and would increase parking spaces in lot B.² With implementation of the project, there would be an overall reduction of six parking spaces for a total of 634. The project site contains five existing commercial loading spaces, and the project would add one 25-foot-long loading space near the proposed Retail Pad building.

The project site contains nine existing class I bicycle spaces and 36 existing class II bicycle parking spaces, and the proposed project would add 23 class I bicycle spaces and 30 class II bicycle spaces. With implementation of the project, the project site would contain 32 class I bicycle spaces and 66 class II bicycle spaces. The proposed project would include streetscape improvements at the intersection of Anzavista Avenue/O'Farrell Street and along O'Farrell Street between the entrances of lots C and E. Streetscape improvements would include bulb-outs, new Americans with Disabilities Act (ADA) accessible curb ramps, new high visibility crosswalks, and landscaping.

During the approximately 20-month construction period, the proposed project would require excavation of approximately seven feet below the existing ground surface and 1,990 cubic yards of soil would be removed from the project site. The proposed improvements would be supported by spread footings with drilled piers; piling driving is not required. No diesel back-up generators are proposed.

Project Approvals

The proposed project would require the following approvals:

- **Conditional Use Authorization** (*Planning Commission*). The proposed project would require conditional use authorization for (1) development of a lot in excess of 10,000 square feet, (2) to permit non-residential uses in excess of 6,000 square feet in a Neighborhood Commercial district,

¹ The project site currently contains 78 parking spaces in lot A, 81 parking spaces in lot B, 117 parking spaces in lot C, 123 parking spaces in lot D, 96 parking spaces in lot E, and 145 parking spaces in Lot F.

² There would be a reduction of 19 stalls in lot A, an increase of 90 stalls in lot B, no change in lot C, a reduction of 16 stalls in lot D, a reduction of three stalls in lot E, and a reduction of 58 stalls in lot F.

and (3) for a bulk limit exception in a non-C-3 (Downtown) district. The project site qualifies for a planned unit development (PUD) since the subject lot is greater than one half acre. As part of the PUD, the project sponsor is seeking a modification to minimum accessory parking standards. The approval of the conditional use/PUD authorization would be the approval action for the project. The approval action date establishes the start of the 30-day appeal period for this CEQA exemption determination pursuant to section 31.04(h) of the San Francisco Administrative Code.

- **Building Permit** (*Department of Building Inspection*) (DBI). The project would require approval from DBI for the proposed construction.

EXEMPT STATUS (continued):

CEQA Guidelines section 15332, or Class 32, provides an exemption from environmental review for in-fill development projects that meet the following conditions. As discussed below, the proposed project satisfies the terms of the Class 32 exemption.

- a) *The project is consistent with applicable general plan designations and policies as well as with applicable zoning designations.*

The San Francisco General Plan establishes objectives and policies to guide land use decisions related to the physical development of San Francisco and is composed of ten elements, each of which addresses a particular topic that applies citywide: air quality; arts; commerce and industry; community facilities; community safety; environmental protection; housing; recreation and open spaces; transportation; and urban design. The proposed project would be consistent with the San Francisco General Plan and with applicable zoning designations and policies. The project site is located within the Moderate-Scale Neighborhood Commercial (NC-3) zoning district, where the existing retail use is permitted.

Planning Code section 121.1 states that in order to promote, protect, and maintain a scale of development which is appropriate to each district and compatible with adjacent buildings, new construction or significant enlargement of existing buildings on lots in excess of 10,000 square feet in a Neighborhood Commercial (NC) district shall be permitted only as conditional uses. Since the project site is over 10,000 square feet in size and is located in the NC-3 district, the project requires conditional use authorization. Pursuant to Planning Code section 121.2, non-residential uses greater than 6,000 gross square feet in a NC district require conditional use authorization. Since the project involves increasing the amount of retail floor area by more than 6,000 gross square feet, the project requires a conditional use authorization. Conditional use authorization is also required per section 217(c) for a bulk limit exception in a non-C-3 (Downtown) district. The maximum permitted length above 40 feet is 110 feet with a maximum permitted diagonal of 140 feet. The proposed horizontal addition to the Crow's Nest building would result in a length of 132 feet with a diagonal of 141 feet. Therefore, the proposed project would require a bulk exception. The new buildings proposed would comply with the 40-X and 80-D height and bulk district.

Planning Code section 152 requires one off-street loading space in the NC-3 district for a commercial use between 10,001 and 60,000 gross square feet in size. Since the project proposes the addition of 33,210 gross square feet of commercial use, the project would be required to provide one loading space. The project would install a new loading space at the proposed Retail Pad building to fulfill these requirements.

Pursuant to Planning Code section 155.2, the proposed project would be required to provide four class I bicycle spaces and 20 class II bicycle spaces. Furthermore, section 155.2 would require the project to provide no less than five percent of the automobile parking spaces, or no less than 32 total bicycle parking spaces. The proposed project would provide an additional 23 class I bicycle spaces and 30 class II bicycle spaces, which exceeds these requirements.

Developments of lots greater than one half acre qualify for authorization as a planned unit development (PUD) pursuant to section 304 of the Planning Code. The objective of the PUD process is to allow well-reasoned modifications to certain code provisions for sites of considerable size that are developed as integrated units and designed to produce a desirable development which will benefit the occupants, the neighborhood, and the city as a whole. Pursuant to Planning Code section 151, the City Center's existing off-street parking requirement is 608 spaces, and with the proposed project, the parking requirement would increase to 721 spaces. City Center currently has 640 off-street parking spaces, and implementation of the project would remove six spaces for a total of 634. Because the project would result in a deficit of 12 percent from the minimum off-street parking (87 spaces), a modification to minimum accessory parking standards pursuant to section 304 is necessary. Since the project site measures 6.6 acres (288,300 square feet) in size, the project sponsor is seeking a parking modification through the PUD process. The proposed project would be consistent with applicable general plan zoning designations.

b) The development occurs within city limits on a site of less than five acres surrounded by urban uses.

The approximately 33,310-square-foot (0.76 acres) project site is located within a developed area of San Francisco. Surrounding uses near the project site include residential, commercial, office, medical, and institutional uses. Therefore, the proposed project would be properly characterized as an in-fill development of less than five acres, surrounded by urban uses.

c) The project site has no habitat for endangered, rare or threatened species.

The project site, which is occupied by an existing shopping center with surface parking lots, is within a developed urban area of San Francisco with no significant riparian corridors, estuaries, marshes, wetlands, or any other potential wildlife habitat that might contain endangered, rare, or threatened species. Thus, the project site has no value as habitat for rare, threatened, or endangered species.

d) Approval of the project would not result in any significant effects relating to traffic, noise, air quality, or water quality.

Transportation

On March 3, 2016, in anticipation of the future certification of revised CEQA Guidelines pursuant to Senate Bill 743, the San Francisco Planning Commission adopted State Office of Planning and Research's recommendation in the *Revised Proposal on Updates to the CEQA Guidelines on Evaluating Transportation Impacts in CEQA*³ to use the Vehicle Miles Traveled (VMT) metric instead of automobile delay to evaluate the transportation impacts of projects (Resolution 19579). The VMT metric does not apply to the analysis of impacts on non-automobile modes of travel such as riding transit, walking, and bicycling. Accordingly,

³ This document is available online at: https://www.opr.ca.gov/s_sb743.php.

this certificate of determination does not contain a separate discussion of automobile delay (i.e., traffic) impacts. Instead, a VMT and induced automobile travel impact analysis is provided.

Vehicle Miles Traveled (VMT) Analysis

Many factors affect travel behavior. These factors include density, diversity of land uses, design of the transportation network, access to regional destinations, distance to high-quality transit, development scale, demographics, and transportation demand management. Typically, low-density development at great distance from other land uses, located in areas with poor access to non-private vehicular modes of travel, generate more automobile travel compared to development located in urban areas, where a higher density, mix of land uses, and travel options other than private vehicles are available.

Given these travel behavior factors, San Francisco has a lower VMT ratio than the nine-county San Francisco Bay Area region. In addition, some areas of the City have lower VMT ratios than other areas of the City. These areas of the City can be expressed geographically through transportation analysis zones. Transportation analysis zones are used in transportation planning models for transportation analysis and other planning purposes. The zones vary in size from single city blocks in the downtown core, multiple blocks in outer neighborhoods, to even larger zones in historically industrial areas like the Hunters Point Shipyard.

The San Francisco County Transportation Authority (Transportation Authority) uses the San Francisco Chained Activity Model Process (SF-CHAMP) to estimate VMT by private automobiles and taxis for different land use types. Travel behavior in SF-CHAMP is calibrated based on observed behavior from the California Household Travel Survey 2010-2012 Census data regarding automobile ownership rates and county-to-county worker flows, and observed vehicle counts and transit boardings. SF-CHAMP uses a synthetic population, which is a set of individual actors that represents the Bay Area's actual population, who make simulated travel decisions for a complete day. For retail uses, the Transportation Authority uses trip-based analysis, which counts VMT from individual trips to and from the project (as opposed to entire chain of trips). A trip-based approach, as opposed to a tour-based approach, is necessary for retail projects because a tour is likely to consist of trips stopping in multiple locations, and the summarizing of tour VMT to each location would over-estimate VMT.^{4,5}

A project would have a significant effect on the environment if it would cause substantial additional VMT. The State Office of Planning and Research's (OPR) *Revised Proposal on Updates to the CEQA Guidelines on Evaluating Transportation Impacts in CEQA* ("proposed transportation impact guidelines") recommends screening criteria to identify types, characteristics, or locations of projects that would not result in significant impacts to VMT. If a project meets one of the three screening criteria provided (Map-Based Screening, Small Projects, and Proximity to Transit Stations), then it is presumed that VMT impacts would be less than significant for the project and a detailed VMT analysis is not required. Map-Based Screening is used to determine if a project site is located within a transportation analysis zone that

⁴ To state another way: a tour-based assessment of VMT at a retail site would consider the VMT for all trips in the tour, for any tour with a stop at the retail site. If a single tour stops at two retail locations, for example, a coffee shop on the way to work and a restaurant on the way back home, then both retail locations would be allotted the total tour VMT. A trip-based approach allows us to apportion all retail-related VMT to retail sites without double-counting.

⁵ San Francisco Planning Department, Executive Summary: Resolution Modifying Transportation Impact Analysis, Appendix F, Attachment A, March 3, 2016.

exhibits low levels of VMT; Small Projects are projects that would generate fewer than 100 vehicle trips per day; and the Proximity to Transit Stations criterion includes projects that are within a half mile of an existing major transit stop, have a floor area ratio of greater than or equal to 0.75, vehicle parking that is less than or equal to that required or allowed by the planning code without conditional use authorization, and are consistent with the applicable Sustainable Communities Strategy.

The existing average daily work-related VMT per retail employee is 7.4 for TAZ 663, which is 50 percent below the existing average daily work-related VMT per retail employee of 14.9. Future 2040 average daily work-related VMT per retail employee is 7.2 for TAZ 663, which is approximately 51 percent below the future 2040 regional average daily work-related VMT per retail employee of 14.6. Therefore, the project would not cause substantial additional VMT, and impacts would be less-than-significant.⁶

Induced Automobile Travel Analysis

A project would have a significant effect on the environment if it would substantially induce additional automobile travel by increasing physical roadway capacity in congested areas (i.e., by adding new mixed-flow lanes) or by adding new roadways to the network. OPR's proposed transportation impact guidelines includes a list of transportation project types that would not likely lead to a substantial or measureable increase in VMT. If a project fits within the general types of projects (including combinations of types), then it is presumed that VMT impacts would be less than significant and a detailed VMT analysis is not required.

The proposed project is not a transportation project. However, the proposed project would include changes within the public right-of-way. Streetscape improvements are proposed to be implemented at the intersection of Anzavista Avenue/O'Farrell Street and along O'Farrell Street between the entrance to lots C and E. Improvements to aid in pedestrian circulation and safe pedestrian access at the O'Farrell Street/Anzavista Avenue intersection would include bulb-outs, new ADA accessible curb ramps, new high visibility crosswalks, and landscaping. These features fit within the general types of projects that would not substantially induce automobile travel.⁷ Therefore, the proposed project would not substantially induce automobile travel and impacts would be less-than-significant.

Trip Generation

The proposed project would involve the addition of 32,210 square feet of retail space to the existing shopping center. The project would result in an overall reduction of six parking spaces (from 640 to 634), the addition of one loading space near the proposed Retail Pad building, and the installation of 23 class I bicycle spaces and 30 class II bicycle spaces on the project site. The project sponsor has agreed to implement several improvement measures that would reduce less-than-significant transportation and circulation impacts (see Attachment A).

The proposed project would generate an estimated 8,383 net new person trips (inbound and outbound) on a weekday daily basis, consisting of 4,850 person trips by auto (2,515 vehicle trips accounting for vehicle occupancy data for this Census Tract), 1,541 transit trips, 1,565 walk trips and 427 trips by other

⁶ San Francisco Planning Department, *Eligibility Checklist: CEQA Section 21099 – Modernization of Transportation Analysis for 2675 Geary Boulevard*, April 5, 2016. This document (and all other documents cited in this report, unless otherwise noted), is available for review at 1650 Mission Street, Suite 400, San Francisco, CA, as part of Case No. 2015-007917ENV.

⁷ *Ibid.*

modes, which includes bicycle trips.⁸ During the p.m. peak hour, the proposed project would generate an estimated 1,000 person trips, consisting of 570 person trips by auto (293 vehicle trips accounting for vehicle occupancy data for this Census Tract), 198 transit trips, 178 walk trips and 54 trips by other modes. In addition, the project would generate approximately 37 daily truck trips, which is equivalent to a loading demand of 1.72 trucks during the average loading hour and a loading demand of 2.15 trucks during the peak loading demand.

Transit

The project site is located within a quarter mile of several local transit lines including Muni lines 2-Clement, 31-Balboa, 31BX-Balboa B Express, 38-Geary, 38BX-Geary B Express, 38R-Geary Rapid, and 43-Masonic. The proposed project would generate approximately 198 weekday p.m. peak hour transit trips. Transit trips associated with the proposed project would not result in any of the screenlines or corridors to exceed the San Francisco Municipal Transportation Authority's (SFMTA) 85 percent capacity utilization threshold. The project would contribute trips to two transit lines (Fulton/Hayes and Third Street sub-corridors) which are already operating above this capacity utilization threshold. However, the project's contribution to these lines would be less than 0.5 percent, which is less than SFMTA's five percent threshold.⁹ Therefore, the proposed project's impact on Muni transit capacity would be less than significant and the project would not result in any new significant transit impacts or contribute considerably to any cumulative transit impacts.

Notably, the Geary Bus Rapid Transit Project, which involves improvements that would enhance reliability and travel time for transit users along the Geary corridor, is currently under review and is pending approval by the San Francisco County Transportation Authority Board. This project includes the following components: the installation of dedicated lanes exclusively for bus traffic; installation of improved bus stations with good quality lighting; implementation of pedestrian infrastructure enhancements near bus stations to reduce crossing distances; introduction of buses with enhanced boarding capabilities, including features such as low floors and all-door boarding; and modification of signal timing along the corridor to improve travel time for buses. Construction is currently planned to begin in late 2017, with service starting in 2021.¹⁰ Transit service to the project site is expected to be enhanced with these improvements.

Bicycle

The proposed project would generate approximately 54 bicycle trips during the weekday p.m. peak hour. The project site currently contains nine class I bicycle space and 36 class II bicycle spaces, and the project would add 23 class I bicycle spaces and 30 class II bicycle spaces. There are three major bicycle routes in the project area, including bicycle routes on Geary Boulevard, Masonic Avenue, and Presidio Boulevard. The increase of bicycle trips generated by the proposed project would be accommodated by the existing bicycle network, and the amount of proposed bicycle spaces would meet the demand. The proposed project would not create potentially hazardous conditions for bicyclists and the proposed project would have a less than significant impact on bicyclists. To reduce this less-than-significant impact, the project

⁸ Stantec Consulting Services, 2675 Geary Boulevard Transportation Impact Study, June 2017.

⁹ *Ibid.*

¹⁰ San Francisco County Transportation Authority. 2016. <http://www.sfcta.org/geary-corridor-bus-rapid-transit-about>. Accessed January 3, 2017.

sponsor has agreed to implement Improvement Measure TR-2: Queue Abatement to enhance bicycle safety by reducing vehicle queues in the public right-of-way.¹¹

Pedestrians

The proposed project would generate approximately 178 pedestrian trips during a typical weekday p.m. peak hour. Pedestrians would be able to access the site from entrances on Geary Boulevard, Masonic Avenue, and O'Farrell Street. Americans with Disabilities Act (ADA) conforming curb ramps are provided at the Geary Boulevard/Masonic Avenue, Geary Boulevard/Presidio Avenue, and Masonic Avenue/O'Farrell Street intersections. The Geary Boulevard/Lyon Street, O'Farrell/Lyon Street, and O'Farrell/Anzavista Avenue intersections currently have one or more non-compliant curb ramps. As part of this project the O'Farrell/Anzavista Avenue intersection would undergo pedestrian improvement modifications including curb bulb-outs, new ADA accessible ramps, and new high visibility crosswalks.¹² The increase in daily pedestrian person-trips generated by the project would not substantially overcrowd sidewalks in the project vicinity or otherwise interfere with pedestrian accessibility to the site and adjoining areas. The proposed project is expected to have a less-than-significant impact on pedestrians. To reduce this less-than-significant impact, the project sponsor has agreed to implement Improvement Measure TR-2: Queue Abatement to enhance pedestrian safety by reducing vehicle queues in the public right-of-way.

Loading

The project site currently has five off-street loading spaces located on the east side of the project site in lot E. To accommodate the general commercial loading demand of the proposed addition of 32,210 square feet of retail use, one new 25-foot-long off-street loading space would be provided near the Retail Pad building on lot F. The project is expected to generate approximately 37 daily delivery/service trips, which corresponds to a loading demand of about two trucks during the peak hour. At an average loading rate of 25 minutes per truck, the loading space could serve about 2.4 trucks per hour arriving consecutively. However, if the project provided loading space is in use, the loading activity could take place at one of the five existing loading spaces on site. The combined retail loading demand would be expected to be accommodated by the existing and proposed off-street loading spaces, and loading impacts are expected to be less than significant.

Although not anticipated, in the event that no space is available, the double-parking of large trucks along O'Farrell Street could result in some traffic impacts (e.g., exacerbating traffic congestion and blocking travel lanes) as well as contributing to adverse effects to traffic and public safety for users of this adjacent roadway. To reduce the less-than-significant loading impact, the project sponsor has agreed to implement Improvement Measure TR-3: Limited Delivery Time, which would enforce appropriate loading procedures to avoid any blockages along O'Farrell Street during loading activities and reduce any potential conflicts between delivery vehicles and other users of adjacent roadway (e.g., transit vehicles, bicyclists) and pedestrians walking along these adjacent streets. This improvement measure would also require the project sponsor to enforce strict truck size regulations for use of the off-street service vehicle parking spaces and to restrict deliveries and trucks trips to the project site during peak hours (generally 7:00 a.m. to 9:00 a.m. and 4:00 p.m. to 6:00 p.m.).

¹¹ A vehicle queue is defined as one or more vehicles (destined to the parking facility) blocking any portion of any public street, alley or sidewalk for a consecutive period of three minutes or longer on a daily or weekly basis.

¹² These proposed improvements would undergo an SFMTA approval process before implementation.

Construction

Construction traffic would not substantially affect transportation and circulation conditions. Construction workers who drive to the project site would cause a small temporary increase in traffic volume on surrounding roadways. As the parking lots are sufficiently sized, all construction vehicles related to the proposed project would be stored on site, and would not likely impact the available parking and travel lanes on nearby streets. Due to the temporary and limited duration, construction-related traffic impacts would not be considered significant. To further reduce this less-than-significant impact, the project sponsor has agreed to implement Improvement Measure TR-4: Construction Management. Improvement Measure TR-4 would require the sponsor to create a Construction Management Plan (CMP) to reduce construction-related vehicle-trips through transportation demand management programs. The CMP would require the following: coordination of construction activities with all other ongoing projects in the area; consultation with surrounding community to assist coordination of construction traffic management strategies as they relate to the needs of other users adjacent to the project site; development of a public information plan to provide adjacent residents and businesses with regularly-updated information regarding project construction activities, peak construction vehicle activities, lane closures, and to provide construction management contact to log and address community concerns.

In light of the above, the proposed project would not result in individually or cumulatively significant transportation and circulation impacts.

Noise

An approximate doubling of traffic volumes in the project area would be necessary to produce an increase in ambient noise levels noticeable to most people. The proposed project would not cause a doubling in traffic volumes with the addition of 33,210 square feet of commercial use within the existing 208,090-square-foot shopping center. The project's increase to the existing traffic volumes would not cause a noticeable increase in the ambient noise level in the project vicinity. The noise generated by the proposed project's increase in retail space on the project site would be considered common and generally acceptable in an urban area, and would not be considered a significant impact.

Noise is regulated by the San Francisco Noise Ordinance (Noise Ordinance), which is codified in Article 29 of the San Francisco Police Code. Article 29 establishes property line and other limits for fixed noise sources and also regulates construction noise. Under section 2909(b), fixed noise sources (e.g. mechanical equipment) from commercial properties are limited to 8 dBA¹³ above ambient levels and section 2909(d) also establishes that such noise not exceed an interior daytime (7 a.m. to 10 p.m.) noise limit of 55 dBA or nighttime noise limit (10 p.m. to 7 a.m.) of 45 dBA at the nearest residential receptor. The requirements of the Noise Ordinance are designed to prevent sleep disturbance, protect public health, and prevent the acoustical environment from progressive deterioration.

During project construction, all diesel and gasoline-powered engines would be equipped with noise-arresting mufflers. Delivery truck trips and construction equipment would generate noise that that may be considered an annoyance by occupants of nearby properties. Construction noise is also regulated by the Noise Ordinance. Section 2907 of the Police Code requires that noise levels from individual pieces of

¹³ The standard method used to quantify environmental noise involves evaluating the sound with an adjustment to reflect the fact that human hearing is less sensitive to low-frequency sound than to mid-and high-frequent sound. This measurement adjustment is called "a" weighting, and the data are reported in A-weighted decibel (dBA). A -10dB (decibel) increase in noise level is generally perceived to be twice as loud.

construction equipment, other than impact tools, not exceed 80 A-weighted dBA at a distance of 100 feet from the source. Impact tools (such as jackhammers and impact wrenches) must have both intake and exhaust muffled to the satisfaction of the Director of Public Works. Section 2908 of the Police Code prohibits construction work between 8:00 p.m. and 7:00 a.m. if the construction noise would exceed the ambient noise level by 5 dBA at the project property line, unless a special permit is authorized by the Director of Public Works. Construction noise impacts related to the project would be temporary and intermittent in nature.

In light of the above, the proposed project would not result in individually or cumulatively significant noise impacts.

Air Quality

In accordance with the state and federal Clean Air Acts, air pollutant standards are identified for the following six criteria air pollutants: ozone, carbon monoxide (CO), particulate matter (PM), nitrogen dioxide (NO₂), sulfur dioxide (SO₂) and lead. These air pollutants are termed criteria air pollutants because they are regulated by developing specific public health- and welfare-based criteria as the basis for setting permissible levels. The Bay Area Air Quality Management District (BAAQMD) in its CEQA Air Quality Guidelines (May 2011), has developed screening criteria to determine if projects would violate an air quality standard, contribute substantially to an air quality violation, or result in a cumulatively considerable net increase in criteria air pollutants within the San Francisco Bay Area Air Basin. If a proposed project meets the screening criteria, then the project would result in less-than-significant criteria air pollutant impacts. A project that exceeds the screening criteria may require a detailed air quality assessment to determine whether criteria air pollutant emissions would exceed significance thresholds. The proposed project, which would construct 33,210 square feet of retail space to an existing shopping center, would not exceed criteria air pollutant screening levels for operation or construction.¹⁴

In addition to criteria air pollutants, individual projects may emit toxic air contaminants (TACs). TACs collectively refer to a diverse group of air pollutants that are capable of causing chronic (i.e., of long-duration) and acute (i.e., severe but short-term) adverse effects to human health, including carcinogenic effects. In response to growing concerns of TACs and their human health effects, the San Francisco Board of Supervisors approved a series of amendments to the San Francisco Building and Health Codes, generally referred to as the Enhanced Ventilation Requirements for Urban Infill Sensitive Use Developments or Health Code, Article 38 (Ordinance 224-14, effective December 8, 2014)(Article 38). The purpose of Article 38 is to protect the public health and welfare by establishing an Air Pollutant Exposure Zone and imposing an enhanced ventilation requirement for all urban infill sensitive use development within the Air Pollutant Exposure Zone. Projects within the Air Pollutant Exposure Zone require special consideration to determine whether the project's activities would expose sensitive receptors to substantial air pollutant concentrations or add emissions to areas already adversely affected by poor air quality.

The proposed project is not within an Air Pollutant Exposure Zone. Therefore, the proposed project would not result in a significant impact with respect to siting new sensitive receptors in areas with substantial levels of air pollution. The proposed project would require construction activities for approximately 20 months. However, construction emissions would be temporary and variable in nature

¹⁴ Bay Area Air Quality Management District, CEQA Air Quality Guidelines, Updated May 2011. Table 3-1. Criteria air pollutant screening sizes for a Regional Shopping Center is 99,000 square feet for operational and 277,000 square feet for construction.

and would not be expected to expose sensitive receptors to substantial air pollutants. Furthermore, the proposed project would be subject to, and comply with, California regulations limiting idling to no more than five minutes,¹⁵ which would further reduce nearby sensitive receptors' exposure to temporary and variable TAC emissions. Therefore, construction period TAC emissions would not result in a significant impact with respect to exposing sensitive receptors to substantial levels of air pollution. In conclusion, the proposed project would not result in significant air quality impacts.

Water Quality

The proposed project would not generate wastewater or result in wastewater discharges that would have the potential to degrade water quality or contaminate a public water supply. Project-related wastewater and stormwater would flow to San Francisco's combined sewer system and would be treated to standards contained in San Francisco's National Pollutant Discharge Elimination System Permit for the Southeast Water Pollution Control Plant prior to discharge. In 2013, the San Francisco Public Utilities Commission (SFPUC) adopted the Construction Site Runoff Ordinance (Public Works Code, Ordinance 260-13) which requires all construction sites, regardless of size to implement Best Management Practices (BMPs) to prevent construction site runoff discharges into the combined or separate sewer systems. Therefore, the proposed project would not result in significant water quality impacts.

e) The site can be adequately served by all required utilities and public services.

The project site is located in a dense urban area where all public services and facilities are available. The proposed project would be connected to existing water, electric, gas, and wastewater services. Prior to receiving a building permit, the project would be reviewed by the Department of Building Inspection, the San Francisco Fire Department, the San Francisco Public Utilities Commission, and Public Works to ensure compliance with city and state regulations concerning building standards, fire protection, sewer connections, and hydrology. Therefore, the proposed project would be adequately served by all required utilities and public services.

DISCUSSION OF ENVIRONMENTAL ISSUES:

CEQA Guidelines section 15300.2 establishes exceptions to the application of a categorical exemption for a project. None of the established exceptions applies to the proposed project.

CEQA Guidelines section 15300.2, subdivision (b), provides that a categorical exemption shall not be used where the cumulative impact of successive projects of the same type in the same place, over time, is significant. There are no cumulative projects in the vicinity that could combine with the proposed project to result in significant cumulative effects on the environment. Therefore, there is no possibility of a significant cumulative effect on the environment due to the proposed project.

CEQA Guidelines section 15300.2, subdivision (c), provides that a categorical exemption shall not be used for an activity where there is a reasonable possibility that the activity will have a significant effect on the environment due to unusual circumstances. As discussed above, the proposed project would not have a significant effect on traffic, noise, air quality and water quality. In addition, the proposed project would

¹⁵ California Code of Regulations, Title 13, Division 3, § 2485 (on-road) and § 2449(d)(2) (off-road).

not have a significant effect on the environment due to unusual circumstances for other environmental topics, including hazardous materials which is discussed below.

CEQA Guidelines section 15300.2, subdivision (e), provides that a categorical exemption shall not be used for a project located on a site which is included on any list compiled pursuant to section 65962.5 of the Government Code. The project site is not located on such a list.

OTHER ENVIRONMENTAL TOPICS:

Hazardous Materials

Article 22A of the Health Code, also known as the Maher Ordinance, is administered and overseen by the Department of Public Health (DPH). The Maher area includes properties throughout the City where there is potential to encounter hazardous materials, primarily industrial zoning districts, sites with industrial uses or underground storage tanks, sites with historic bay fill, and sites in close proximity to freeways or underground storage tanks. The over-arching goal of the Maher Ordinance is to protect public health and safety by requiring appropriate handling, treatment, disposal and when necessary, mitigation of contaminated soils that are encountered in the building construction process. Projects that disturb 50 cubic yards or more of soil that are located on sites with potentially hazardous soil or groundwater are subject to this ordinance.

The proposed project is located within the Article 22A (Maher) area and would involve excavation up to approximately six feet below ground surface and approximately 1,990 cubic yards of soil disturbance. Therefore, the project is subject to the Maher Ordinance. In compliance with the Maher Ordinance, the project sponsor submitted a Maher Application and a Phase I Environmental Site Assessment (ESA)¹⁶ to DPH.¹⁷ According to the Phase I ESA, the project site was used as a cemetery from the late 1800's until circa 1940 when the graves were relocated to make way for the existing shopping center, which was constructed in 1951 as a Sears Roebuck & Company retail store. In 1992, the existing building was renovated and partitioned into various tenant spaces.

The Sears Roebuck & Company operated an automotive service center at the southeast side of the building. The service center utilized five hydraulic lifts, eleven hoists, and seven underground storage tanks (USTs). In 1992, five hydraulic lifts, eleven hoists, and three 120-gallon USTs were removed from the project site. On April 5, 1995, DPH issued a Notice of Completion for removal of the USTs associated with the hydraulic lifts. A report prepared by SCS Engineers in 1993 indicated that low levels of residual oil contamination was present in the UST cavities during removal. The DPH apparently did not require the remaining impacted soil to be removed, as stated by the consultant. The excavation was backfilled and closed, but a closure letter was not issued.

In order to obtain regulatory closure, a subsurface investigation was required by the DPH. SECOR completed this investigation in February 1998, which included the drilling and sampling of seven soil borings. Petroleum hydrocarbon (oil) contamination up to 5,295 milligrams per kilogram (mg/kg) was detected in the former UST cavities. This contamination was not associated with any other petroleum

¹⁶ Stantec Consulting Services, *Phase I Environmental Site Assessment, City Center, 2675 Geary Boulevard, San Francisco, CA*, March 30, 2012.

¹⁷ Martita Lee Weden, San Francisco Department of Public Health, letter to Brian Bacharach, property owner, February 24, 2017.

hydrocarbons (gasoline or diesel fuels), heavy metals, or other contaminants of concern at considerable concentrations. Outside of the UST cavity, oil contamination appeared very minor with peak concentrations of 35 mg/kg of oil. This indicates that the residual oil contamination appeared to be confined to the former UST cavity area, which measures approximately 20 feet by 60 feet with an average depth of 13 feet. Groundwater was not encountered at the project site or reported to be affected by the detected oil impact.

Since the project site is located in the Maher area and the proposed project would require more than 50 cubic yards of soil disturbance, the proposed project would be required to remediate potential soil contamination described above in accordance with Article 22A of the Health Code. Therefore, the proposed project would not result in a significant hazard to the public or the environment through the release of hazardous materials.

Naturally Occurring Asbestos

A geotechnical investigation was prepared for the proposed project and determined that the project site is underlain by serpentinite bedrock.¹⁸ The proposed project would involve up to approximately six feet of excavation and would include drilled piers that would extend up to approximately 35 feet below ground surface. Therefore, project construction could potentially disturb serpentinite bedrock. Serpentinite commonly contains naturally occurring chrysotile asbestos (NOA) or tremolite-actinolite, a fibrous mineral that can be hazardous to human health if airborne emissions are inhaled. In the absence of proper controls, NOA could become airborne during excavation and handling of excavated materials. On-site workers and the public could be exposed to airborne asbestos unless appropriate control measures are implemented. Although the California Air Resources Board (ARB) has not identified a safe exposure level for asbestos in residential areas, exposure to low levels of asbestos for short periods of time poses minimal risk.¹⁹ To address health concerns from exposure to NOA, ARB enacted an Asbestos Airborne Toxic Control Measure (ATCM) for Construction, Grading, Quarrying, and Surface Mining Operations in July 2001. The requirements established by the Asbestos ATCM are contained in California Code of Regulations (CCR) Title 17, section 93105²⁰ and are enforced by the BAAQMD.

The Asbestos ATCM requires construction activities in areas where NOA is likely to be found to employ best available dust control measures. Additionally, the San Francisco Board of Supervisors approved the Construction Dust Control Ordinance in 2008 to reduce fugitive dust generated during construction activities. The requirements for dust control as identified in the Construction Dust Control Ordinance are as effective as the dust control measures identified in the Asbestos ATCM. Thus, the measures required in compliance with the Construction Dust Control Ordinance would protect the workers themselves as well as the public from fugitive dust that may also contain asbestos. The project sponsor would be required to comply with the Construction Dust Control Ordinance, which would ensure that significant exposure to NOA would not occur. Therefore, the proposed project would not result in a hazard to the public or environment from exposure to NOA.

¹⁸ Langan Treadwell Rollo, *Geotechnical Investigation, 2675 Geary Boulevard, San Francisco, CA*, October 16, 2015.

¹⁹ California Air Resources Board, Fact Sheet #1 Health Information on Asbestos, 2002. Available online at: <http://www.arb.ca.gov/toxics/Asbestos/1health.pdf>. Accessed May 24, 2017.

²⁰ California Air Resources Board, Operations, July 29, 2002.

Public Notice and Comment. On April 5, 2016, the planning department mailed a "Notification of Project Receiving Environmental Review" to community organizations, tenants of properties adjacent to the project site, and those persons who own property within 300 feet of the project site. Three members of the public provided the following comments: the reduction of off-street parking on the project site would exacerbate the shortage of on-street parking spaces on nearby streets; the project would block views; and the project would add to neighborhood congestion.

In accordance with CEQA section 21099 – Modernization of Transportation Analysis for Transit Oriented Projects – aesthetics and parking shall not be considered in determining if a project has the potential to result in significant environmental effects, provided the project meets all of the following three criteria: a) the project is in a transit priority area; b) the project is on an infill site; and c) the project is residential, mixed-use residential, or an employment center. The project site is an infill site located in a transit priority area, and meets the definition of an employment center. Since the proposed project meets each of the above three criteria, this certificate does not consider aesthetics or parking in determining the significance of project impacts under CEQA.²¹ As discussed above, a transportation impact study was prepared for the proposed project which determined that the proposed project would not result in a significant transportation impact.

Conclusion. The proposed project satisfies the criteria for exemption under the above-cited classification(s). In addition, none of the CEQA Guidelines section 15300.2 exceptions to the use of a categorical exemption applies to the proposed project. For the above reasons, the proposed project is appropriately exempt from environmental review.

²¹ San Francisco Planning Department. *Eligibility Checklist: CEQA Section 21099 – Modernization of Transportation Analysis for 2675 Geary Boulevard*, May 25, 2017.

2675 Geary Boulevard LP is currently planning an expansion to the existing facilities at 2675 Geary Boulevard in San Francisco (the "Property"). The expansion includes three additions to the City Center shopping plaza: (1) tenant improvement and an addition to an existing commercial pad located in the center of the Property, facing parking lot D ("Lot D"); (2) new construction of a two-story building in an existing parking lot on the Property's east, within parking lot F ("Lot F") with retail spaces fronting both parking lot E ("Lot E") and Lot F; and (3) a 3,608 square foot, stand-alone retail building along Masonic Avenue at O'Farrell Street within parking lot A ("Lot A") and an extension of parking lot B ("Lot B") over the footprint of Lot A and the Masonic Avenue retail building. In total, the Project will increase retail square footage in the City Center by 33,210 square feet, from 208,087 square feet to 241,297 square feet. Parking would decrease from 640 to 634 stalls.

Although the project would have less than significant transportation related impacts, the project sponsor has agreed to implement the following improvement measures to further reduce the less than significant impacts to traffic and circulation described below.

Recommended Improvement Measures

Improvement Measure 1: Implement Transportation Demand Management Measures

The sponsor has committed to implementing the following TDM Program options:

- **City Access for Data Collection:** As part of an ongoing effort to quantify the efficacy of TDM measures in general, City staff may need to access the project site (including the garage) to perform trip counts, and/or intercept surveys and/or other types of data collection. Any on-site activity would require the project sponsor or property management approval and be coordinated through the TDM Coordinator. The project sponsor or a contracted transportation brokerage service (e.g. TMA) would be responsible for administering periodic tenant surveys as part of an ongoing program monitoring effort.
- **Provide Bicycle Parking:** Increase the number of on-site secured bicycle parking beyond Planning Code requirements and/or provide additional bicycle facilities in the public right-of-way in on public right-of-way locations adjacent to or within a quarter mile of the project site (e.g., sidewalks, on-street parking spaces). The Project would provide 53 additional bicycle parking spaces. As it is required to provide 32 parking spaces, the Project is providing 21 more bicycle parking spaces than required.

Improvement Measure 2: Queue Abatement

It is the responsibility of the owner / operator of any off-street parking facility with more than 20 parking spaces (excluding loading and CarShare spaces) to ensure that recurring vehicle queues do not occur on the public right-of-way. A vehicle queue is defined as one or more vehicles (destined to the parking facility) blocking any portion of any public street, alley or sidewalk for a consecutive period of three minutes or longer on a daily or weekly basis.

If recurring queuing occurs, the owner / operator of the parking facility shall employ abatement methods as needed to abate the queue. Appropriate abatement methods will vary depending on the characteristics and causes of the recurring queue, as well as the characteristics of the parking facility, the street(s) to which the facility connects, and the associated land uses (if applicable).

Suggested abatement methods include but are not limited to the following: redesign of facility to improve vehicle circulation and/or on-site queue capacity; employment of parking attendants; installation of LOT FULL signs with active management by parking attendants; use of valet parking or other space-efficient parking techniques; use of off-site parking facilities or shared parking with nearby uses; use of parking occupancy sensors and signage directing drivers to available spaces; travel demand management strategies; and/or parking demand management strategies such as parking time limits, paid parking, time-of-day parking surcharge, or validated parking.

Improvement Measure 3: Limited Delivery Time

The project sponsor would restrict deliveries and trucks trips to the project site during peak hours (generally 7:00 a.m. to 9:00 a.m. and 4:00 p.m. to 6:00 p.m.).

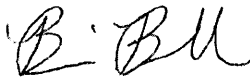
Improvement Measure 4: Construction Management Plan

The Construction Management Plan (CMP) would address transportation-related circulation, access, staging, and hours for deliveries and should include, but not be limited to, the following additional measures:

- Identify ways to reduce construction worker vehicle-trips through transportation demand management programs and methods to manage construction worker parking demands, including encouraging and rewarding alternate modes of transportation (i.e. transit, walk, bicycle, etc.), carpooling, or providing shuttle service from nearby off-street parking facility.
- Identify ways to consolidate truck delivery trips, minimizing delivery trips.

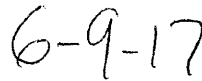
- Require coordination of construction activities with all other ongoing projects in the area, including but not limited to the Geary BRT project, the Masonic Avenue Streetscape Projects, and the projects listed in Table 17 of the Transportation Impact Study.
- Require consultation with surrounding community, including business and property owners near the project site to assist coordination of construction traffic management strategies as they relate to the needs of other users adjacent to the project site.
- Develop a public information plan to provide adjacent residents and businesses with regularly-updated information regarding project construction activities and duration, peak construction vehicle activities, (e.g. concrete pours), lane closures, and provide construction management contact to log and address community concerns.

2675 Geary Boulevard LP commits to implementing these measures at 2675 Geary Boulevard as part of carrying out the project described above.



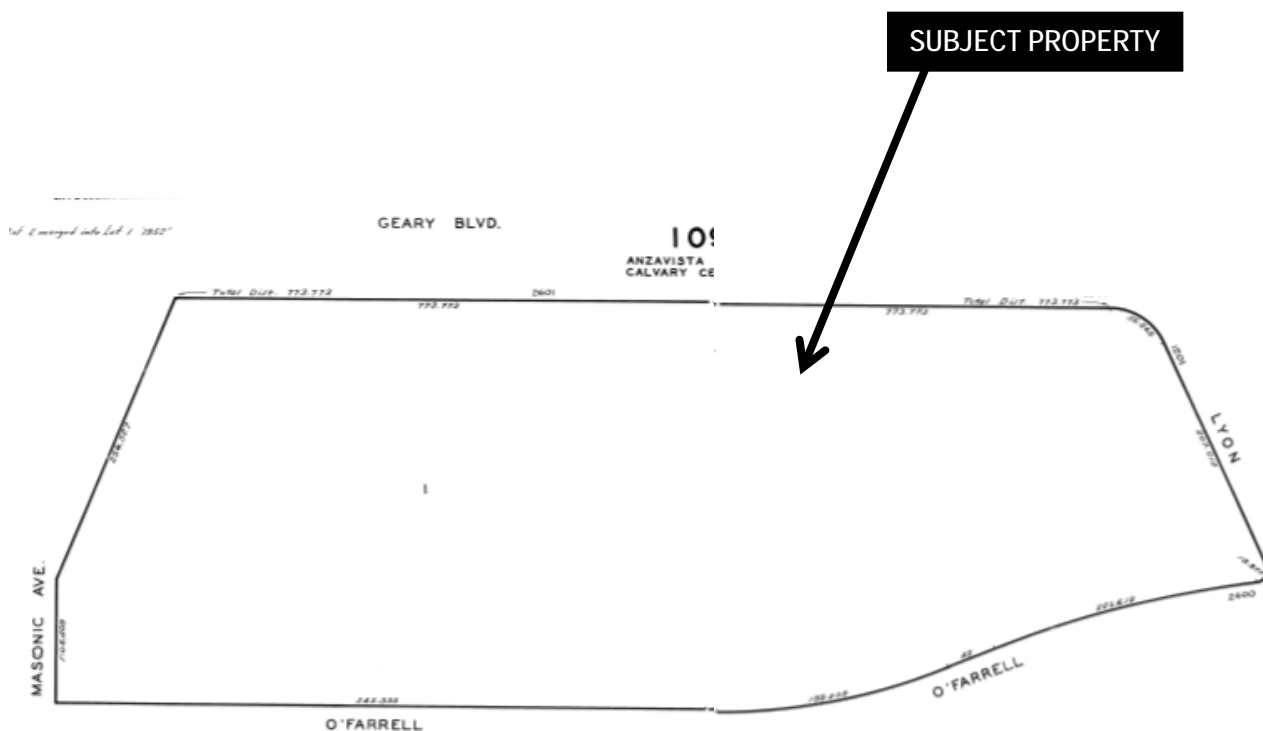
Brian Bacharach, Director

Acadia Realty Trust

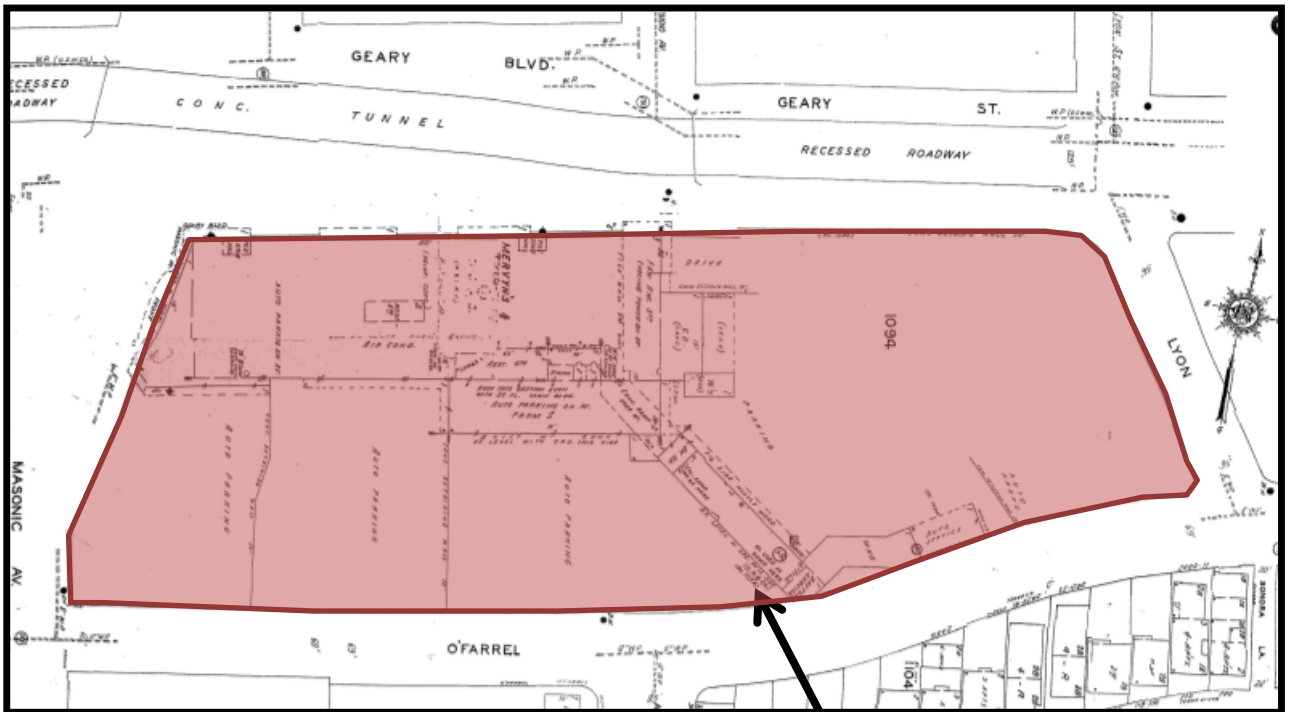


Date

Block Book Map



Sanborn Map*



SUBJECT PROPERTY

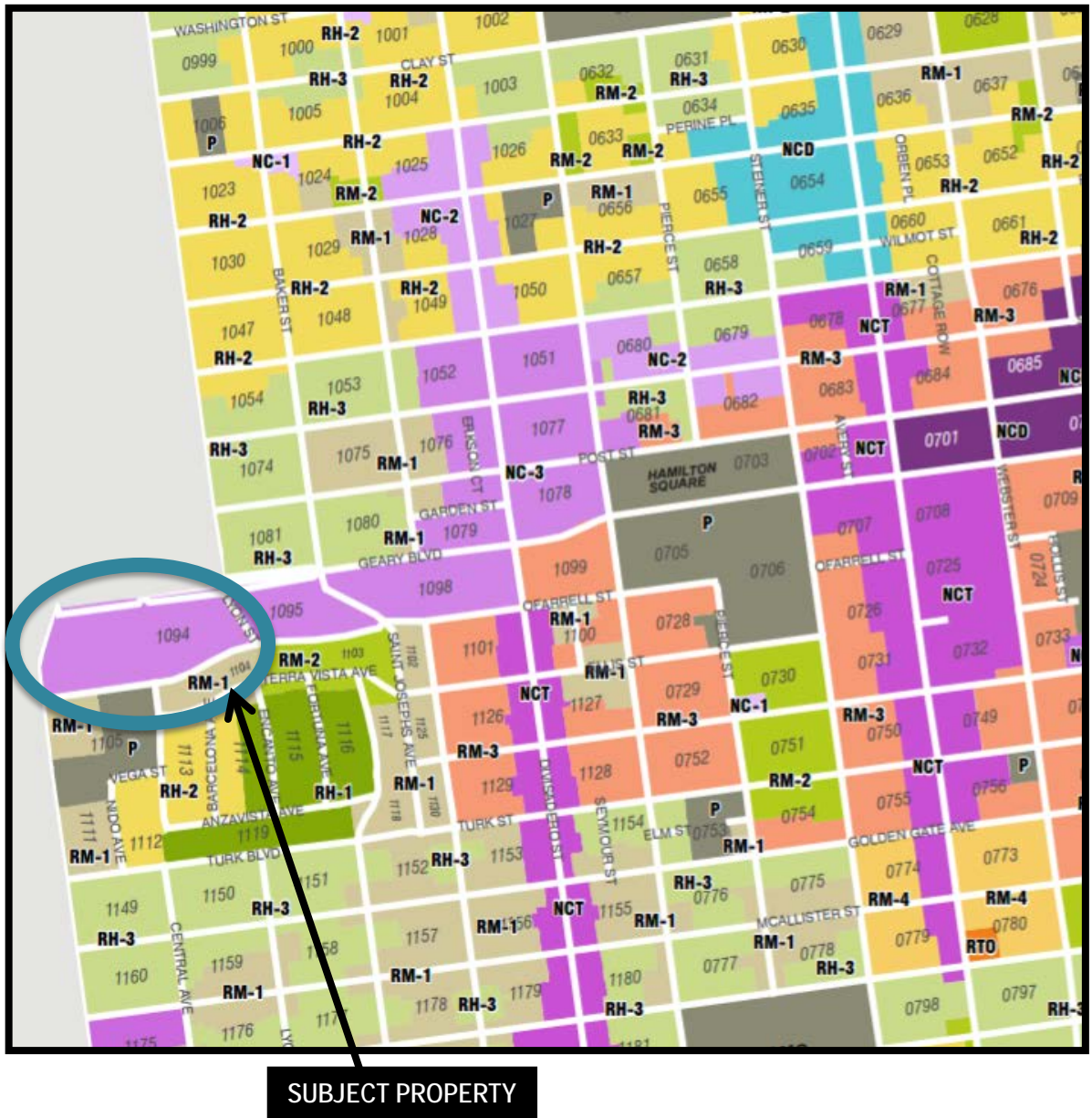
*The Sanborn Maps in San Francisco have not been updated since 1998, and this map may not accurately reflect existing conditions.

SAN FRANCISCO
PLANNING DEPARTMENT



Conditional Use Hearing
Case Number 2015-007917CUA
2675 Geary Boulevard
Block 1094 Lot 001

Zoning Map

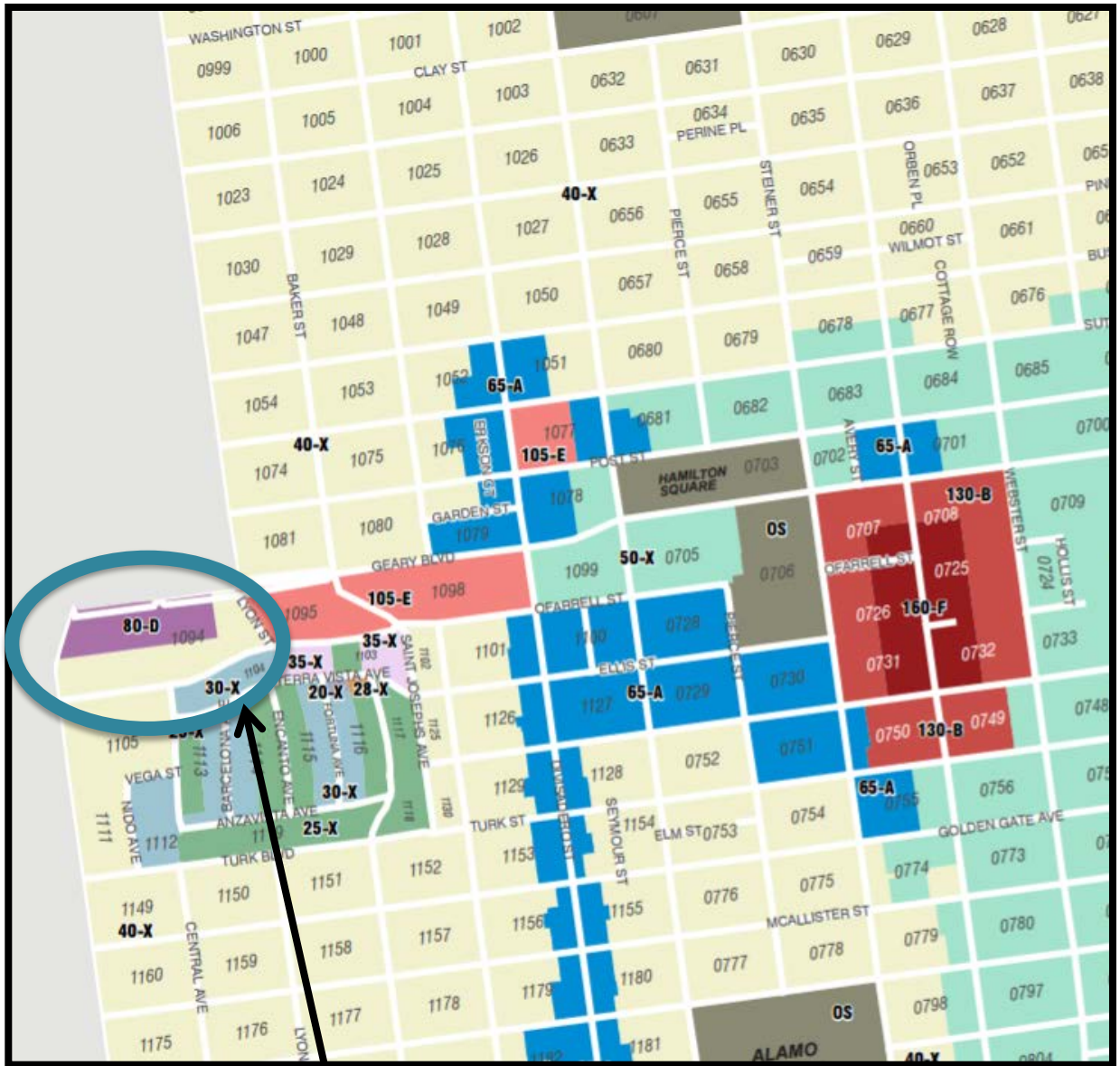


SAN FRANCISCO
PLANNING DEPARTMENT



Conditional Use Hearing
Case Number 2015-007917CUA
2675 Geary Boulevard
Block 1094 Lot 001

Height & Bulk Map



SUBJECT PROPERTY

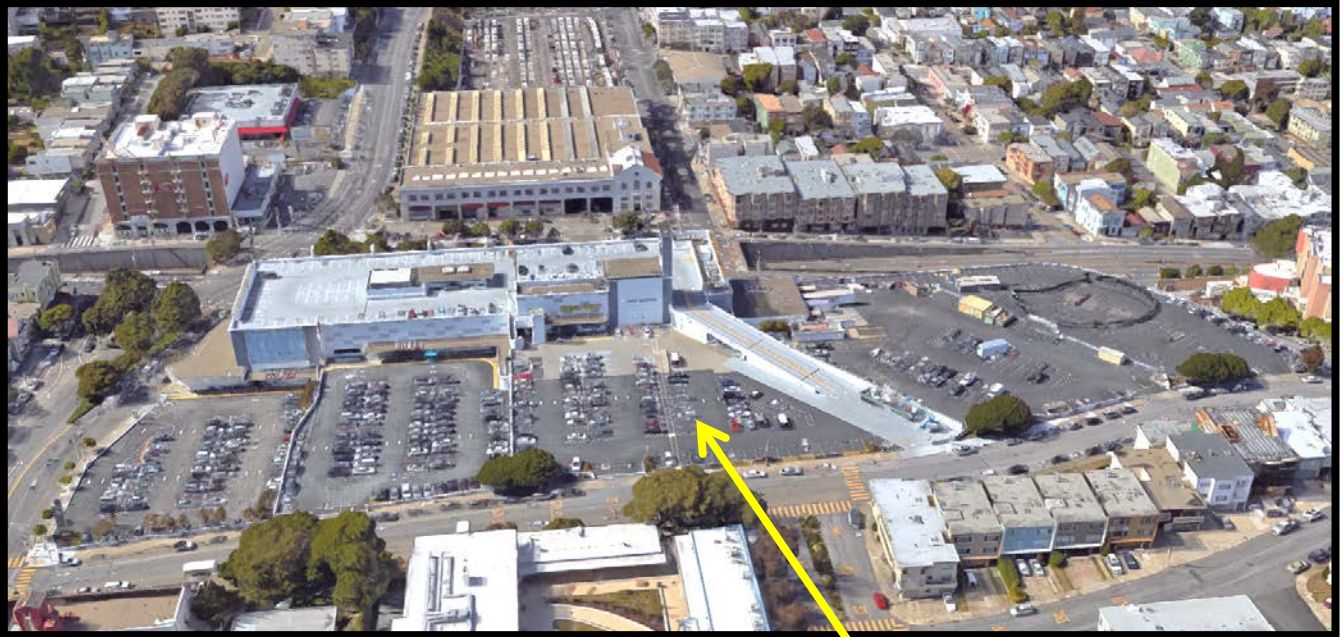
SAN FRANCISCO
PLANNING DEPARTMENT



Conditional Use Hearing
Case Number 2015-007917CUA
2675 Geary Boulevard
Block 1094 Lot 001

Aerial Photo

(looking north)



SUBJECT PROPERTY

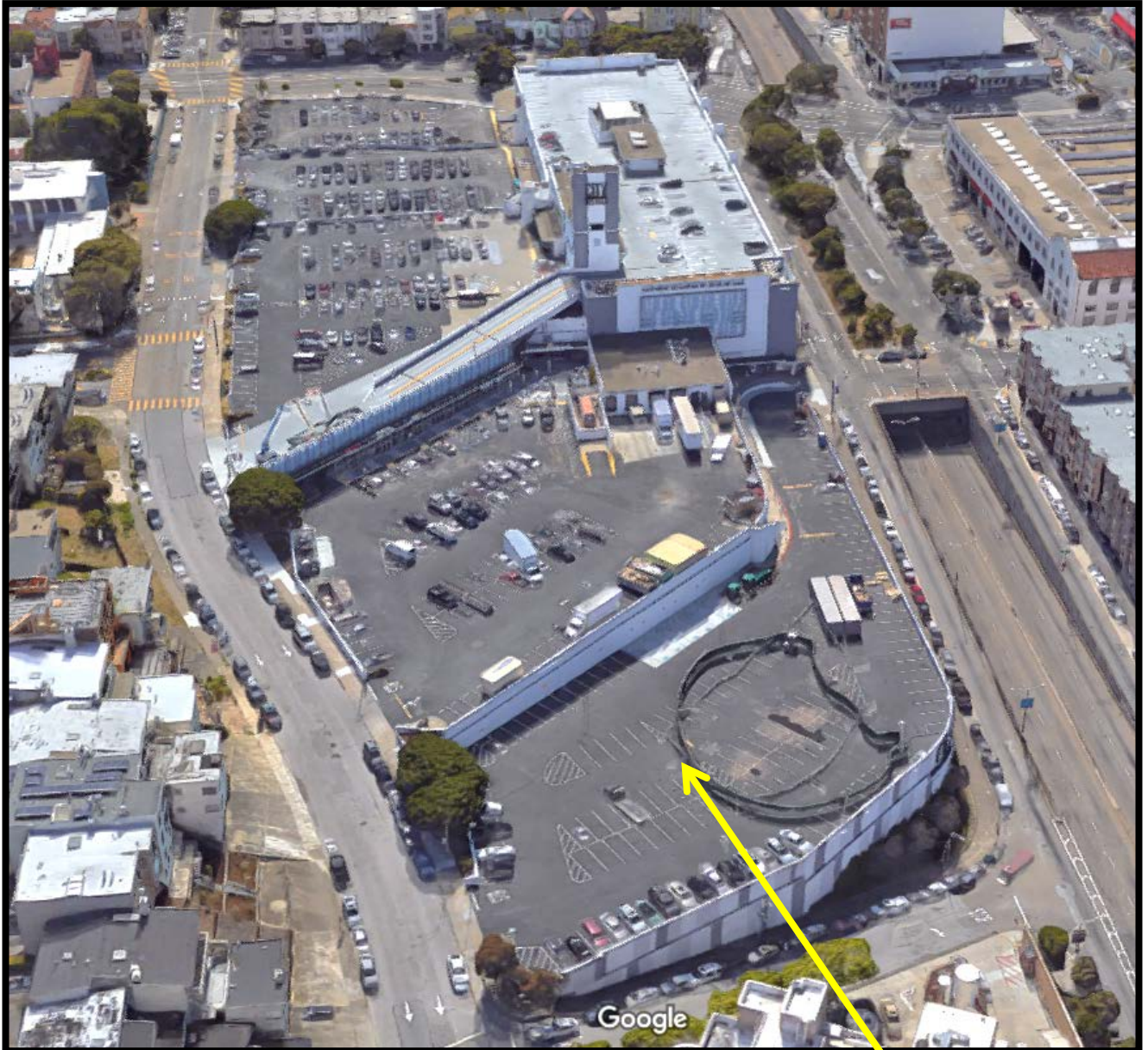
SAN FRANCISCO
PLANNING DEPARTMENT



Conditional Use Hearing
Case Number 2015-007917CUA
2675 Geary Boulevard
Block 1094 Lot 001

Aerial Photo

(looking west)



SUBJECT PROPERTY

SAN FRANCISCO
PLANNING DEPARTMENT



Conditional Use Hearing
Case Number 2015-007917CUA
2675 Geary Boulevard
Block 1094 Lot 001

Aerial Photo

(looking south)



SUBJECT PROPERTY

SAN FRANCISCO
PLANNING DEPARTMENT



Conditional Use Hearing
Case Number 2015-007917CUA
2675 Geary Boulevard
Block 1094 Lot 001

Aerial Photo

(looking east)



SUBJECT PROPERTY

SAN FRANCISCO
PLANNING DEPARTMENT



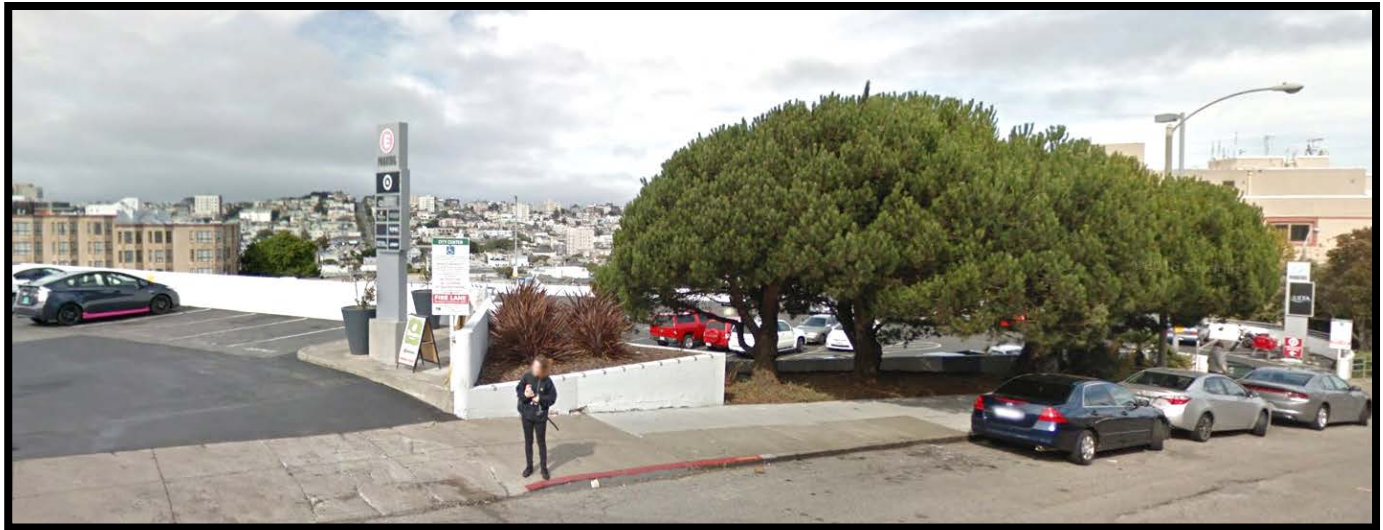
Conditional Use Hearing
Case Number 2015-007917CUA
2675 Geary Boulevard
Block 1094 Lot 001

Site Photo



Parking Lot 'A' (northeast corner of Masonic Ave & O'Farrell St)

Site Photo



Parking Lots 'E' and 'F' (northwest corner of O'Farrell St & Lyon St)

REUBEN, JUNIUS & ROSE, LLP

June 9, 2017

Delivered Via Email (christopher.may@sfgov.org)

Rich Hillis, President
San Francisco Planning Commission
1650 Mission Street, Suite 400
San Francisco, CA 94013

Re: City Center – 2675 Geary Boulevard
Case No.: 2015-007917
Our File No.: 8855.01

Dear President Hillis and Commissioners,

Our office represents 2675 Geary Boulevard LP, an affiliated entity of Acadia Realty Trust (“Acadia”), owner of the City Center shopping plaza (“City Center”) located at 2675 Geary Boulevard, on the block bound by Geary, Masonic Avenue, O’Farrell Street, and Lyon Street (the “Property”).

Acadia proposes three interrelated improvements at the Property (collectively, the “Project”) that would add approximately 33,000 square feet in total to the existing 208,000 square foot City Center: (1) new construction of a two-story, 22,000 square foot “Retail Pad” building in the easternmost two parking lots on the site; (2) tenant improvements and a 7,530 square foot horizontal addition to an existing commercial pad located in the center of the property, aka the Crow’s Nest; and (3) new construction of a stand-alone 3,600 square foot retail building at the corner of Masonic and O’Farrell and an extension of an existing parking lot over the footprint of this retail building and the parking lot below it—relocating and replacing all but six parking spaces the new construction would eliminate.

A. Benefits of Project

The Project represents a net benefit for the site and the neighborhood, improving pedestrian walkability, vehicle movement and efficiency, and urban form, while also adding a range of new tenant spaces consistent with the City Center’s unique positioning as a retail destination in the heart of the city. The benefits of the Project include:

James A. Reuben | Andrew J. Junius | Kevin H. Rose | Daniel A. Frattin | John Kevlin
Tuija I. Catalano | Jay F. Drake | Matthew D. Visick | Lindsay M. Petrone | Sheryl Reuben¹
Thomas Tunny | David Silverman | Melinda A. Sarjapur | Mark H. Loper | Jody Knight
Chloe V. Angelis | Corie A. Edwards | Coryn E. Millslagle | Jared Eigerman^{2,3} | John McInerney III²

1. Also admitted in New York 2. Of Counsel 3. Also admitted in Massachusetts

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- **Pedestrian-Friendly Improvements.** A primary component of the Project is increasing the pedestrian experience both around and through the site. This includes streetscape improvements along portions of O'Farrell Street consistent with the Better Streets Plan, a new landscaped "mid-block" walking path extending across the middle of the site linking O'Farrell Street to Geary, and a stand-alone retail building surrounded by landscaping at the corner of Masonic and O'Farrell instead of the existing surface parking area. The mid-block walking path is expected to be frequently used by Anza Vista neighborhood residents, linking this neighborhood to Geary Boulevard and providing more convenient access to the planned Geary Corridor Bus Rapid Transit.
- **Neighborhood Engagement.** Throughout the nearly two year planning and entitlement process, Acadia has maintained an open door policy, with consistent updates and meetings with neighborhood groups, including the Anza Vista Neighborhood Association. To date neighbors have been supportive of the Project and Acadia's efforts to involve them in the planning process.
- **Reorienting Parking.** Parking will be rearranged to maximize efficiency throughout the City Center and minimize the potential of retail customers to leave the site and look for parking in surrounding neighborhoods. Currently, a disproportionate amount of off-street parking is located in the eastern portion of the site, which remains underutilized even during peak retail shopping hours. The Project would relocate and replace all but six stalls in a new story of above-ground parking on the western portion of the site above the stand-alone retail building fronting Masonic and O'Farrell. City Center currently has 640 off-street stalls, and the Project would result in 634, more than adequate for existing and future retail shoppers.
- **Careful Additions.** The Project's three components are each designed to minimize impacts to existing tenants and neighbors, while creating space that will be attractive to a wide range of retailers and service providers. Going from east to west, the two-story Retail Pad will add an active use within an underutilized portion of the site, creating the effect of being a single story building as viewed from a majority of the Property. Outdoor seating and the pedestrian-friendly walking path will line it. The expansion to the Crow's Nest increases usability of this space while having a minimal impact as perceived from the parking lot or surrounding neighborhoods. Additionally, a modernization of the façade will bring a much needed upgrade to the look of the building. The Masonic Retail building will add a stand-alone active use on this prominent street corner instead of a surface parking lot, with landscaping both on street level and the upper story, significantly improving the pedestrian experience. Each building has been designed to accommodate the needs of modern day retail, while also incorporating the design criteria Planning Department staff requested, as well as neighborhood feedback.

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B. Background: Project Context and History of the City Center Shopping Plaza

City Center is located along a three mile Geary Boulevard commercial corridor that stretches from the Western addition to the Outer Richmond. Commercial and institutional uses are located on main streets in the project vicinity—including City Center, the University of San Francisco, Kaiser Permanente Medical Center, and the Laurel Heights Shopping Center.

Conditions in the area are atypical for neighborhood commercial districts, which are generally characterized by small- to mid-sized businesses, often located in mixed use buildings. Neighborhood commercial streets usually tend to be pedestrian-oriented with continuous retail frontages at the ground floor. In contrast, the area surrounding the Property is auto-oriented in its scale and design.

The City Center has operated as a shopping center for approximately 50 years. It is a four-level, stand-alone shopping center containing approximately 208,000 square feet of primarily retail space. It was built in 1961 and used as a Sears department store until the 1990s. After Sears vacated, City Center's retail space was subdivided and initially reoccupied by several national retailers, including Mervyns, Toys-R-Us, the Good Guys, and Office Depot. The Good guys left the property in 2005, Toys-R-Us was replaced by Best Buy in 2007, and Mervyn's vacated an approximately 90,000 square foot space in December of 2008.

In 2011, at the height of the great recession, this Commission authorized a comprehensive repositioning of the City Center, with Target as an anchor tenant and smaller spaces located within the central portion of the site. These improvements were completed in October 2013. In 2015 and 2016, this Commission approved several retailers to move into those smaller spaces.

The Project in many respects represents a second phase of the City Center's modern redevelopment, adding a range of spaces appealing to a diversity of potential tenants, and positioning the site to continue to provide viable brick and mortar retail and service spaces into the future. Just as important, the Project takes significant steps to increase the pedestrian experience around and through the City Center, while still acknowledging that the site is a heavily-trafficked auto-oriented retail destination that is unique for San Francisco by maintaining a sufficient amount of on-site parking.

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C. Detailed Project Description and Site Plan

The Retail Pad. The two-story approximately 22,072 square foot stand alone “Retail Pad” building is located in the only area on the City Center property where long-term leases with current tenants allow a new stand-alone building (the Project sponsor has authorization to construct the Masonic Avenue building): parking lot F, with frontage on lot E. Design of the Retail Pad utilizes existing topography to create “ground floor” access on both levels. A 10,139 square foot lower floor would front Lot F, and an 11,933 square foot upper floor would front Lot E. The Retail Pad will be visible and inviting to pedestrians walking along Geary Street, and makes a suitable complement to existing retail shops located across the parking lot. Although the ground floor of Lot F is below street grade (and therefore not visible to passing pedestrians) a network of landscaped and outdoor seating areas line the building’s floorplate to provide a more welcoming experience to site’s users.

The Crow’s Nest. The Crow’s Nest, located on the northern portion of the site within Lot D, involves a tenant improvement and 7,530 square feet horizontal addition in an existing three-story space. The project would increase the size of the ground floor space by approximately 3,965 square feet, from 3,260 square feet to 6,955 square feet, and would expand the second story of the building over the enlarged ground floor. 1,340 square feet of the third floor will be eliminated. The Crow’s Nest expansion would push the envelope of the building partially into existing Lot D. 20 bicycle parking spaces would be added in this lot.

Masonic Retail. The stand-alone “Masonic Retail” building at the corner of Masonic Avenue and O’Farrell Street will introduce an active use on the site at this prominent street corner and replace a portion of a surface parking lot. The Masonic Retail space will be approximately 3,608 square feet in size, appropriate for a wide range of retailers. The facade along both Masonic and O’Farrell will feature transparent glass, and the roof will include screening and greening features consistent with Planning Code requirements for above-grade parking structures. The landscaped plaza area in front of the retail activates Masonic Avenue by providing usable outdoor space.

Parking and Circulation. The Project would not add any additional parking. Instead, it would replace and rearrange parking so more will be closer to the main retail portion of City Center, maximizing parking efficiency and lowering the potential for retail shoppers to leave the Property and instead enter the Anza Vista neighborhood during peak shopping periods. Currently, a disproportionate amount of off-street parking is located in the eastern portion of the site in Lots E and F. It remains underutilized even during peak retail shopping hours because it is still somewhat distant from the majority of the retail options on the site.

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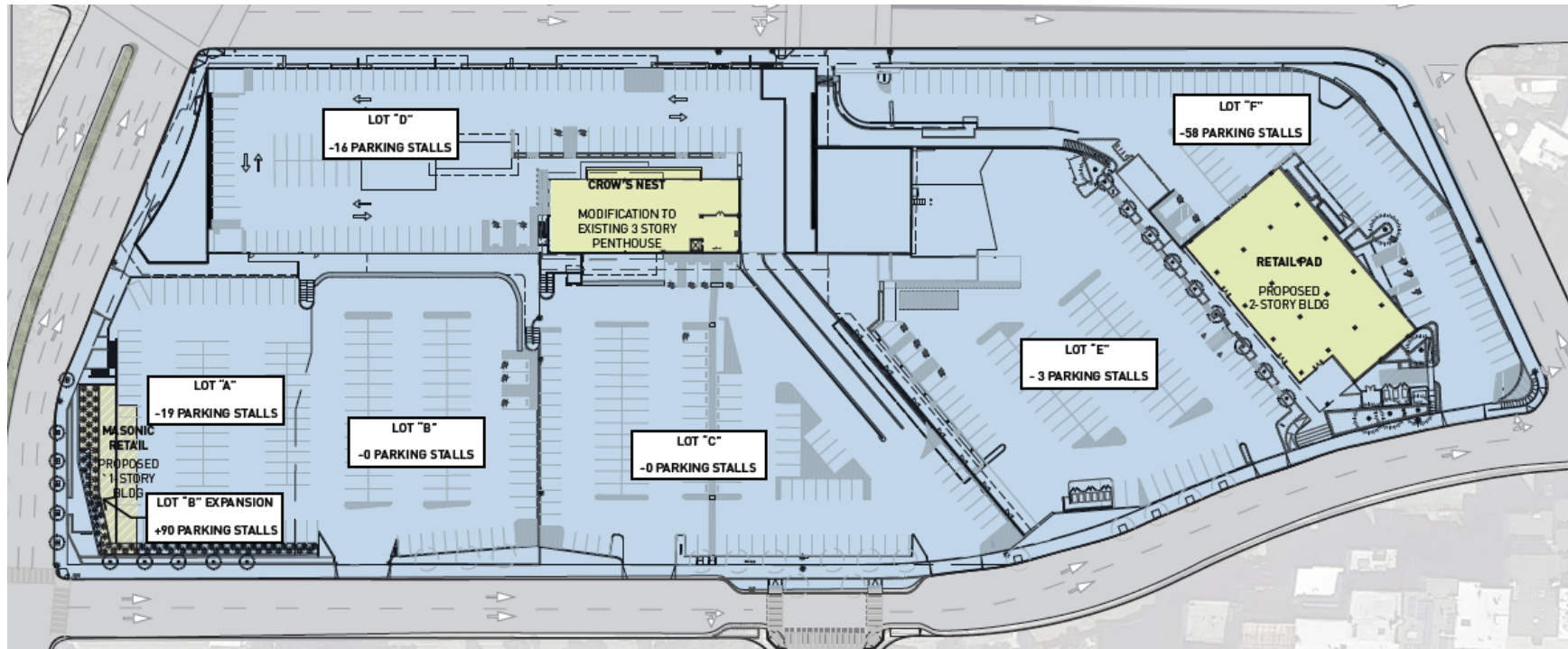
Each of the three project components would eliminate stalls in the lots where the buildings would be located. The Project would relocate and replace these stalls in a story of parking above Lot A, extending Lot B westward to the Masonic Avenue property line. This expansion area would have direct access into the second story of the Target tenant space, reorienting parking on the site in a location where retail customers can more easily access the majority of the retail spaces, including the spaces currently occupied by Target, Best Buy, and ULTA. At the same time, adequate parking will remain in Lots E and F, with a combined total of 180 spaces serving the businesses operating out of the proposed Retail Pad building. The expanded Lot B will feature screening and greening requirements consistent with the Planning Code, ensuring adequate landscaping along this area as viewed from the pedestrian realm, both at the ground level and on the second story.

A chart showing the reorientation of parking:

	Existing	Proposed	Net Change
Lot A (Masonic Retail)	78	59	- 19
Lot B (Above Masonic Retail)	81	171	+ 90
Lot C	117	117	0
Lot D (Crow's Nest)	123	107	-16
Lot E (Retail Pad)	96	93	-3
Lot F (Retail Pad)	145	87	-58
<i>Totals</i>	640 stalls	634 stalls	-6 stalls

Finally, a site plan showing each parking lot and the location of each component of the Project is located on the following page.

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Source: project plans, pg. 9

D. Neighborhood Outreach and Support

Acadia has made community engagement and outreach a central feature of its redevelopment of City Center, since acquiring the site in March of 2015. Throughout the nearly two-year planning and entitlement process, Acadia has maintained an open-door policy, with consistent updates and meetings with neighborhood groups, including the Anza Vista Neighborhood Association. At least six group and individual meetings have been conducted.

In consultation with neighbors, Acadia is providing a pedestrian through-way from O'Farrell Street, past the proposed Retail Pad building along Lot E, and onto Geary Boulevard directly adjacent to a MUNI stop. This "mid-block pathway" should benefit pedestrian circulation throughout the Property and ease access to and from the bus stop. The Project also will improve portions of O'Farrell Street adjacent to the Property, consistent with the Better Streets Plan. These improvements include a large bulb-out with landscaping features at the intersection of O'Farrell and Anza Vista Avenue. Finally, per requests from the neighbors immediately adjacent to the property, the height of the Retail Pad building was reduced by a few feet to protect the views of those homeowners.

To date neighbors have been supportive of the Project and Acadia's efforts to involve them in the planning process. Acadia hopes to have a formal letter of support from the neighborhood by the Planning Commission hearing date.

E. Conclusion

The Project is consistent with the City Center's unique destination for both neighborhood residents and San Franciscans from around the city, and will provide upgraded facilities and streetscape and pedestrian enhancements. Although the overall parking count will be reduced by six spaces from 640 to 634, its reorientation will more efficiently utilize existing parking throughout the site. We respectfully ask that you approve this project.

Sincerely,

REUBEN, JUNIUS & ROSE, LLP


Mark Loper

cc: Chris May, San Francisco Planning Department
Brian Bacharach, Acadia Realty

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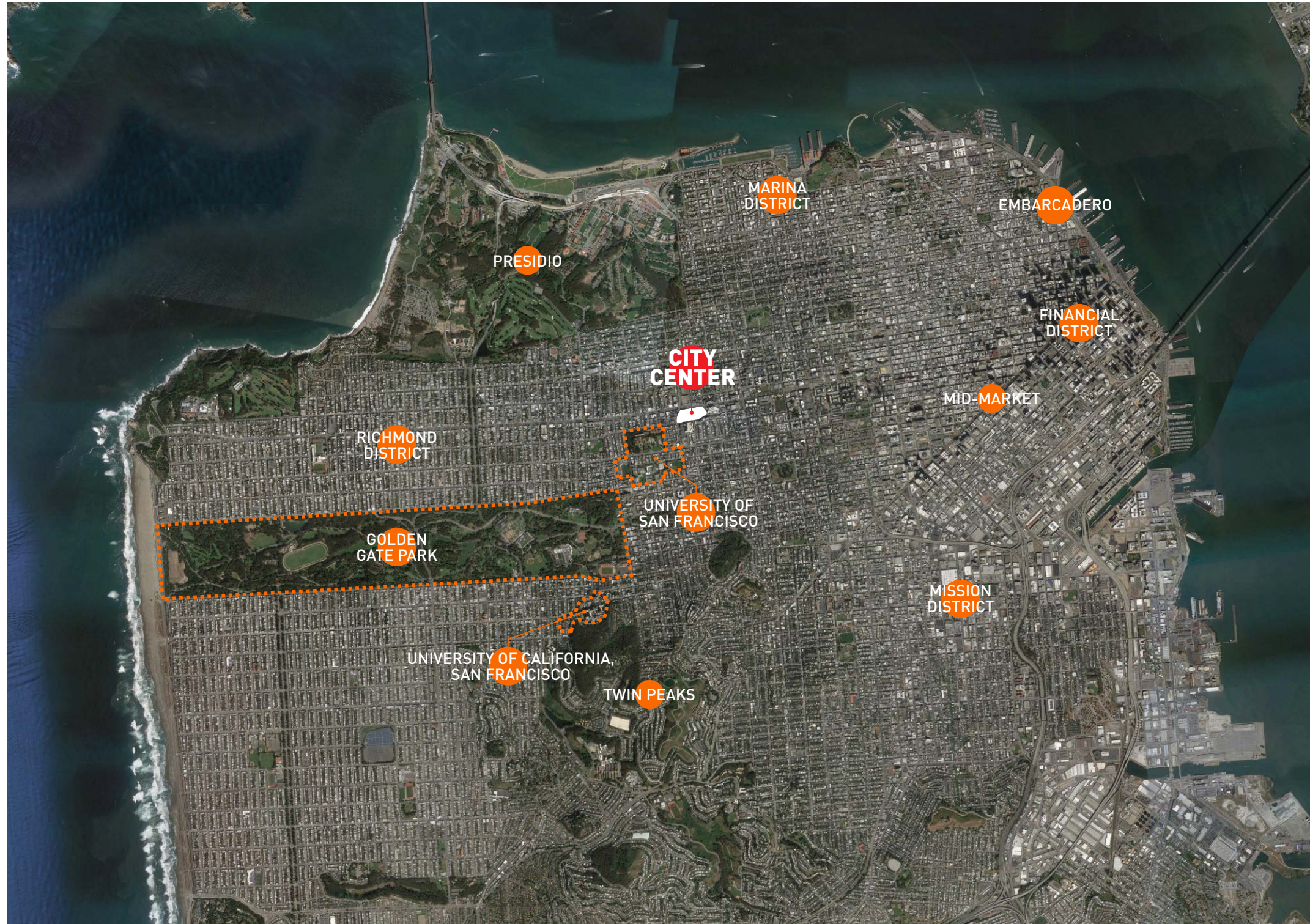
MASONIC RETAIL

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VICINITY MAP



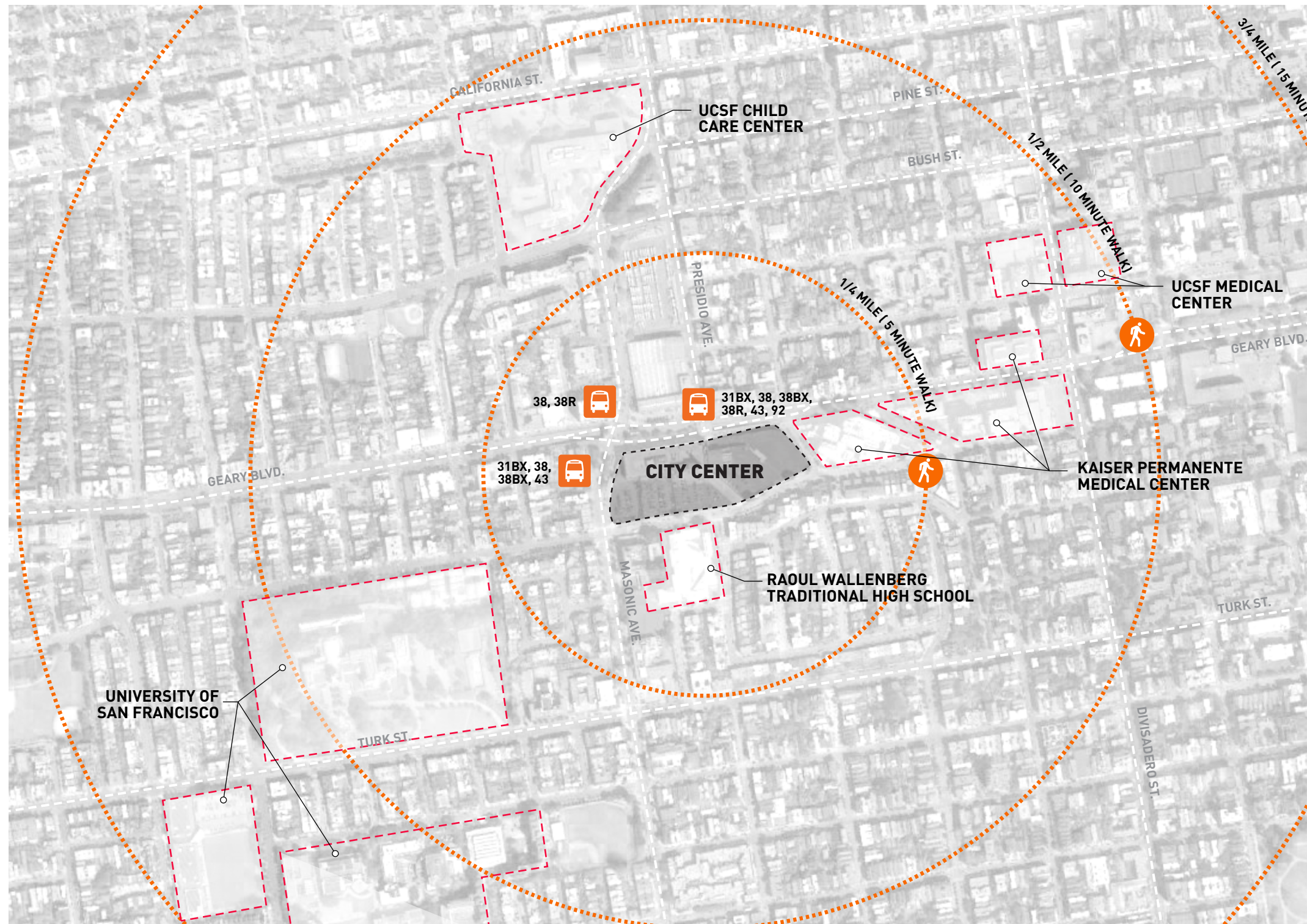
LOCATION MAP

WALKABILITY

The project is located within a 15-minute walk (.75 mile radius) of the University of San Francisco, Raoul Wallenberg Traditional High School, and multiple medical facilities.

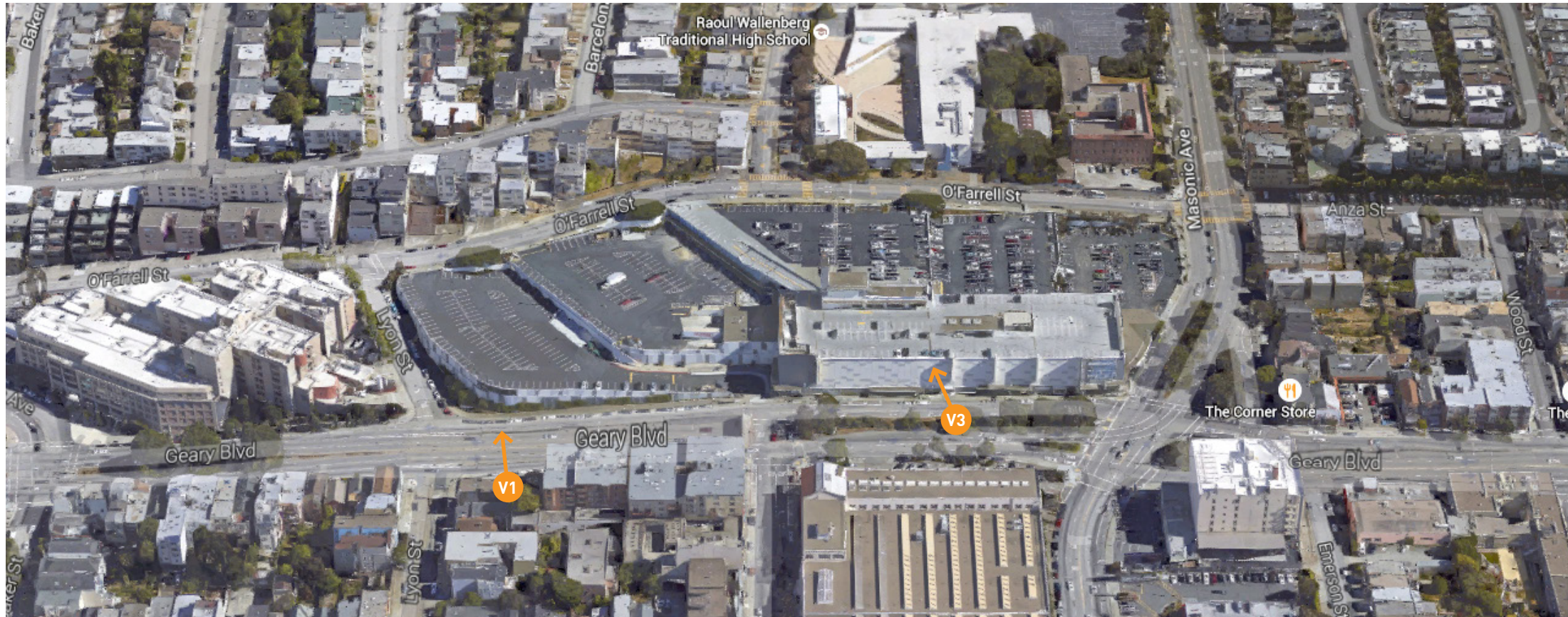
TRANSIT

The project is close to transit stops with bus lines (31BX, 38, 38BX, 38R, 43, 92) located within a short walk of the project.

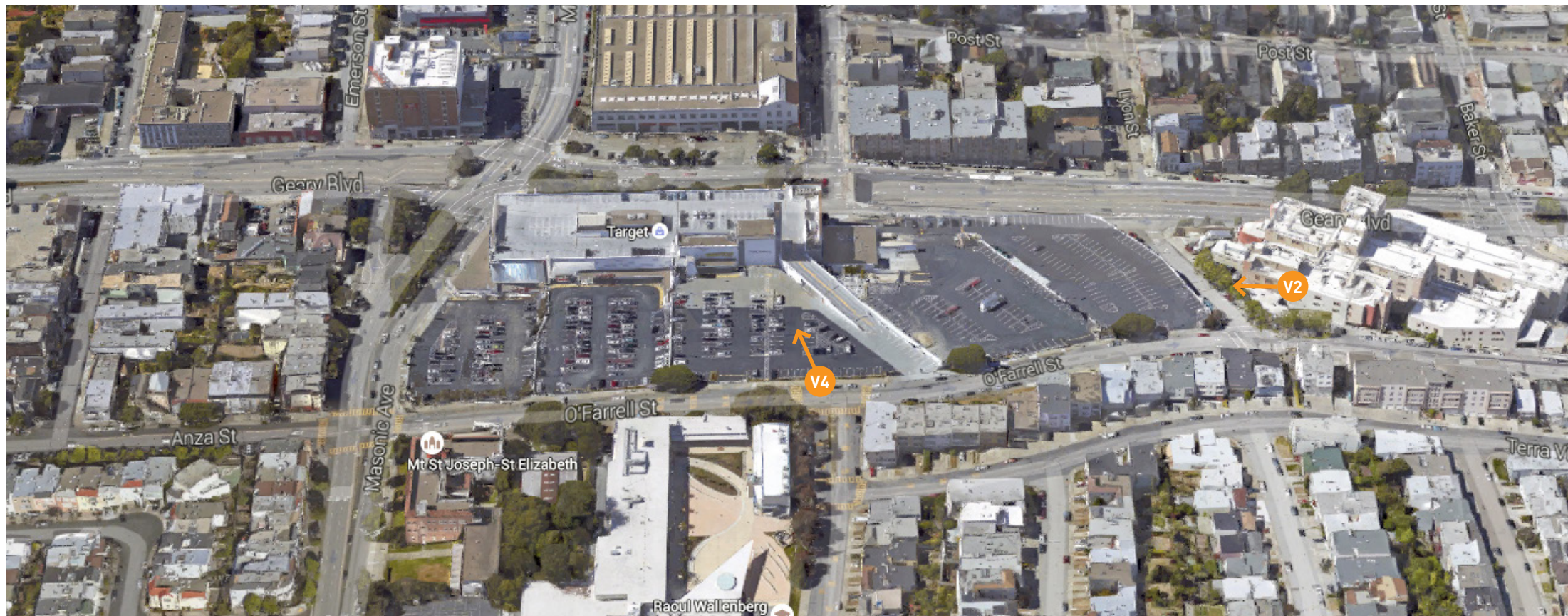


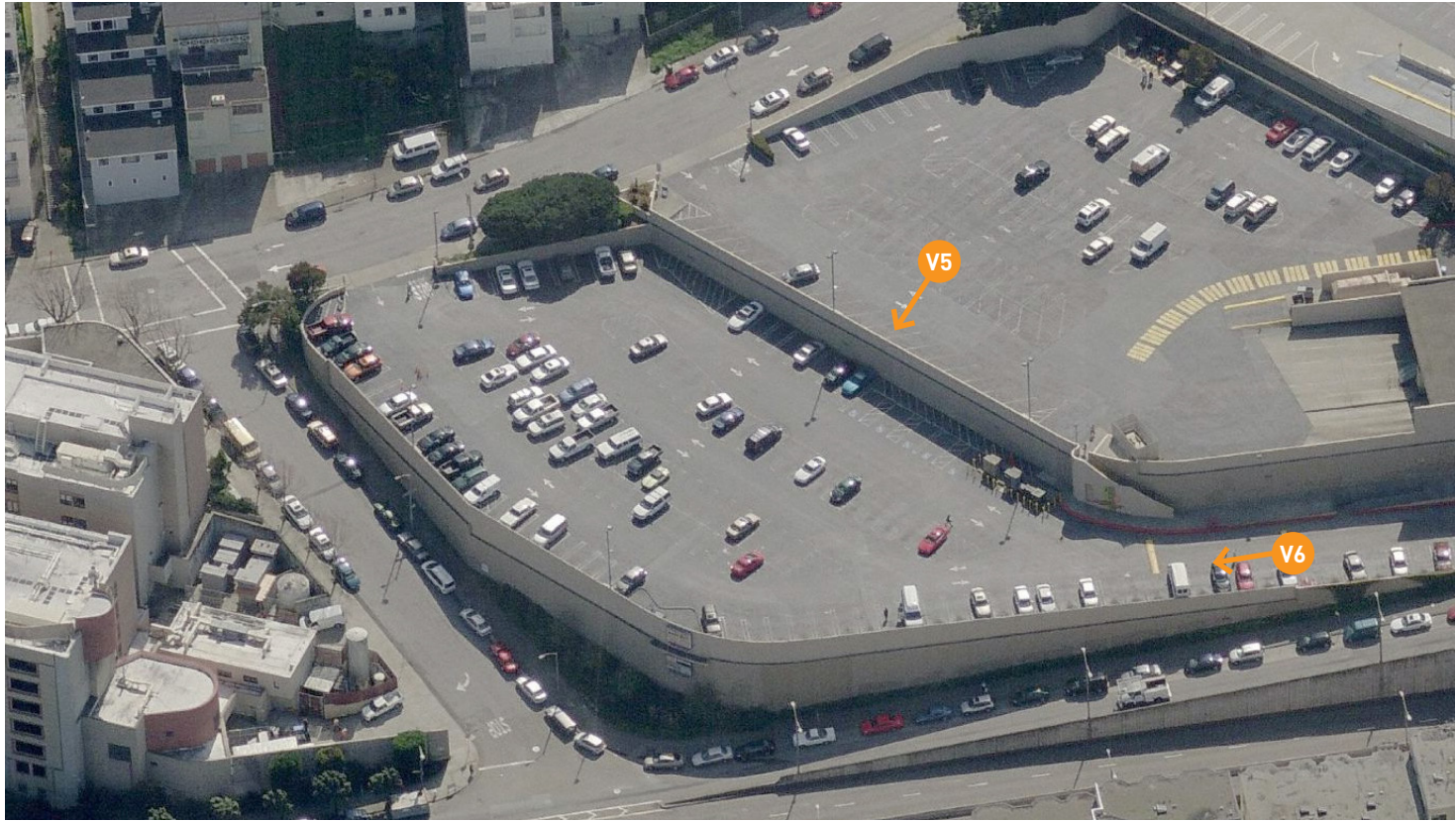
EXISTING CONDITIONS

AERIAL LOOKING TOWARD O'FARRELL STREET

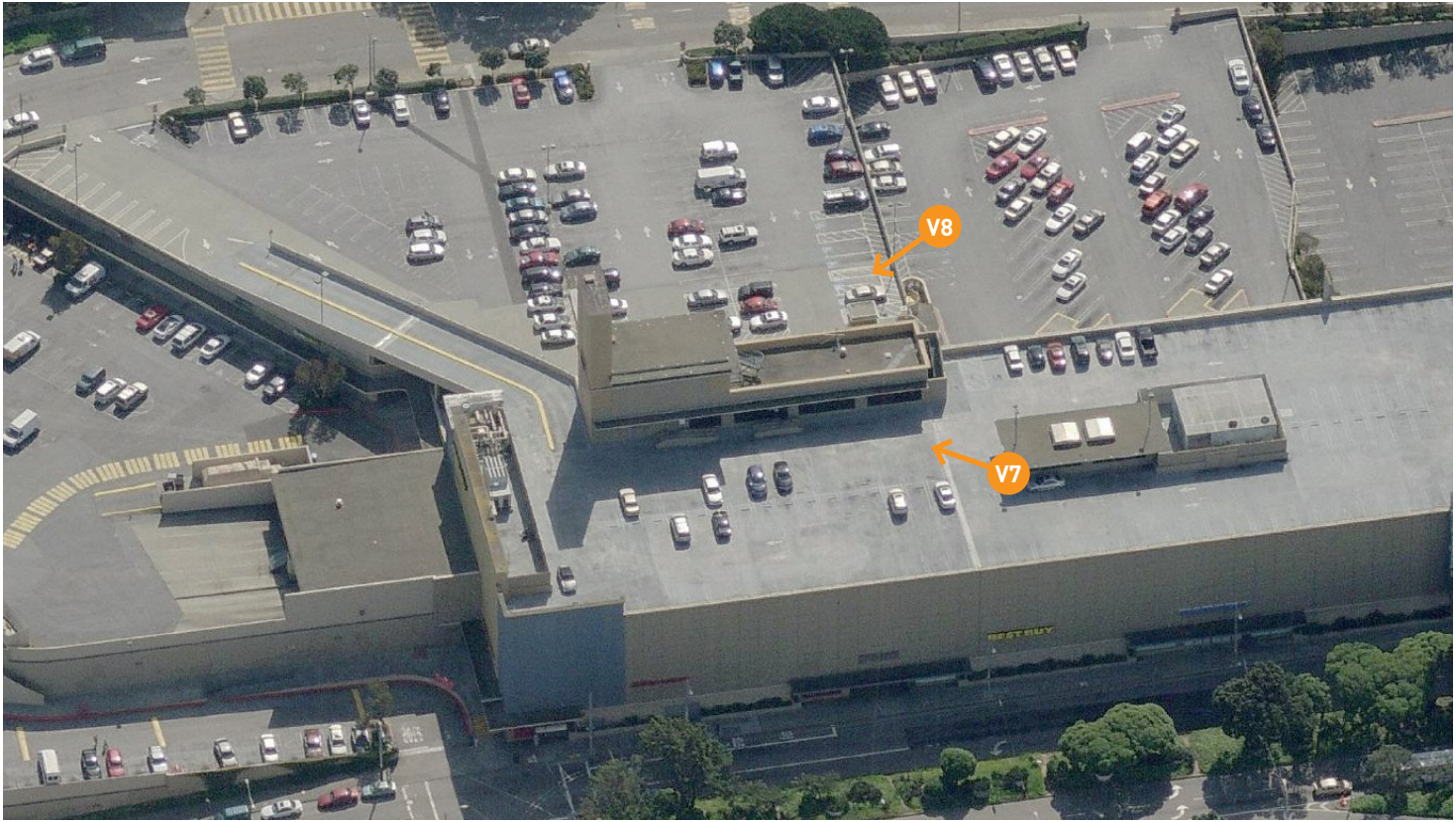


AERIAL LOOKING TOWARD GEARY BLVD.

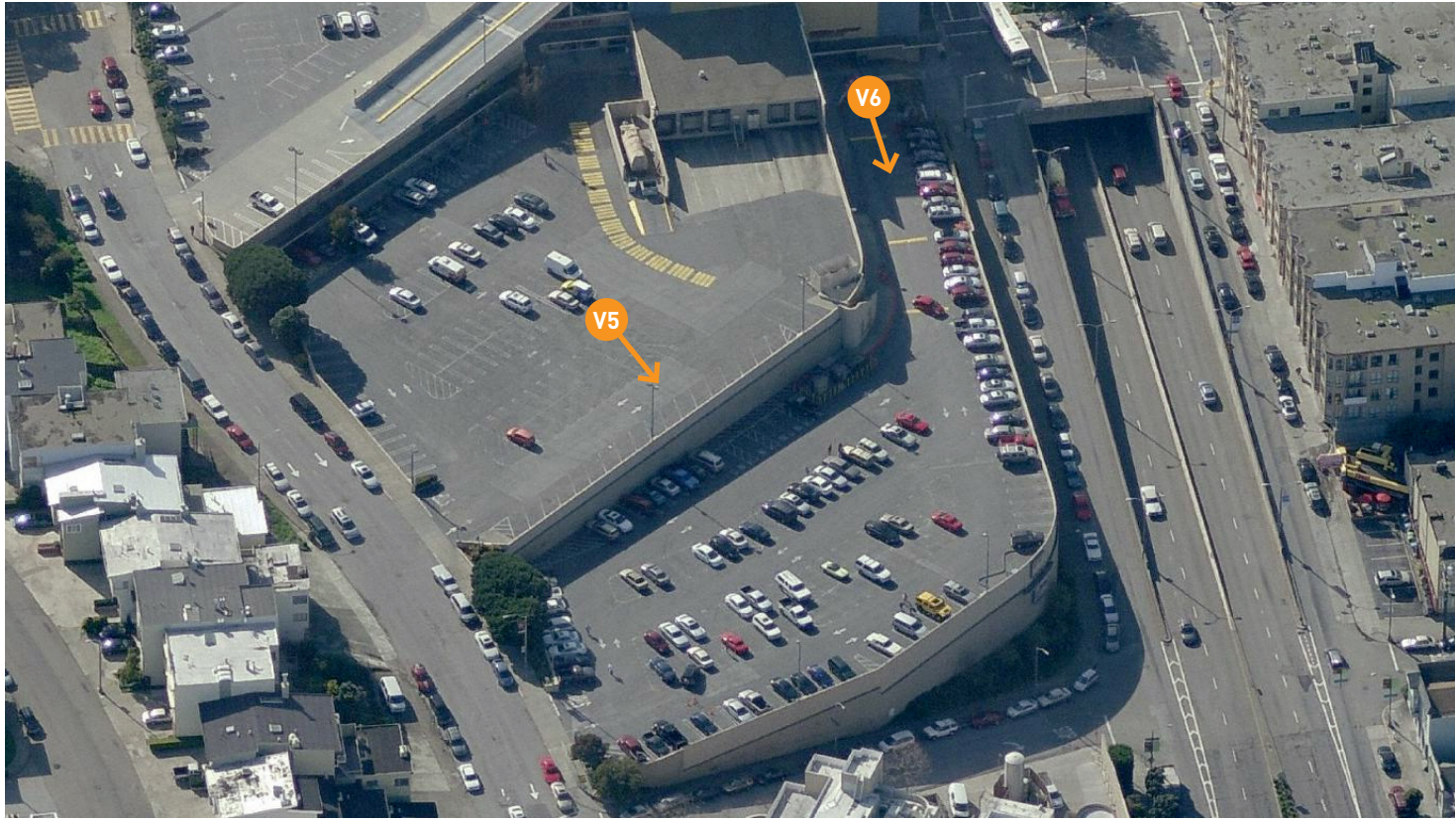




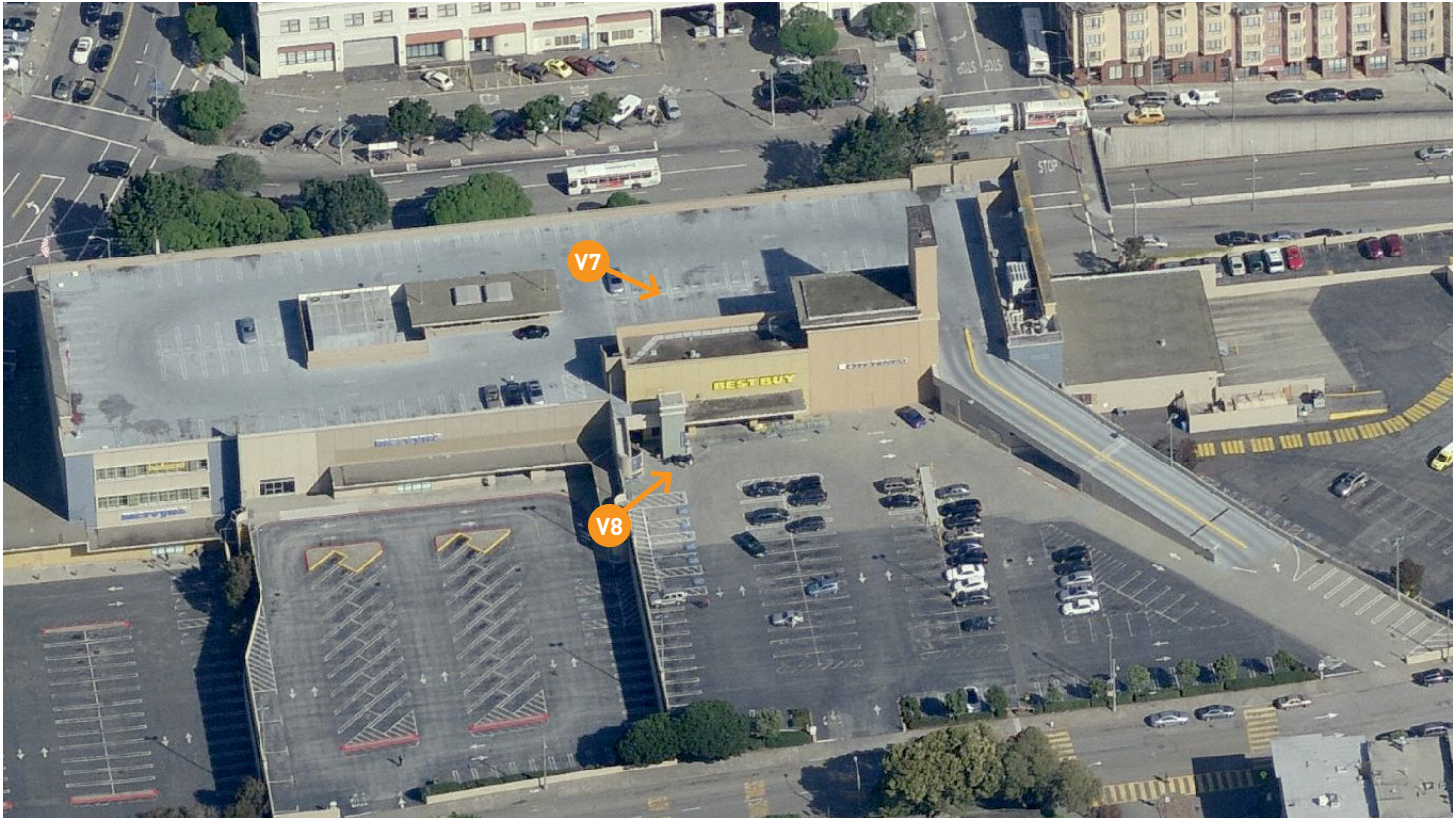
VIEW 1 EXISTING LOT F



VIEW 3 EXISTING CROWS NEST



VIEW 2 EXISTING LOT F



VIEW 4 EXISTING CROWS NEST



VIEW 5 EXISTING LOT F



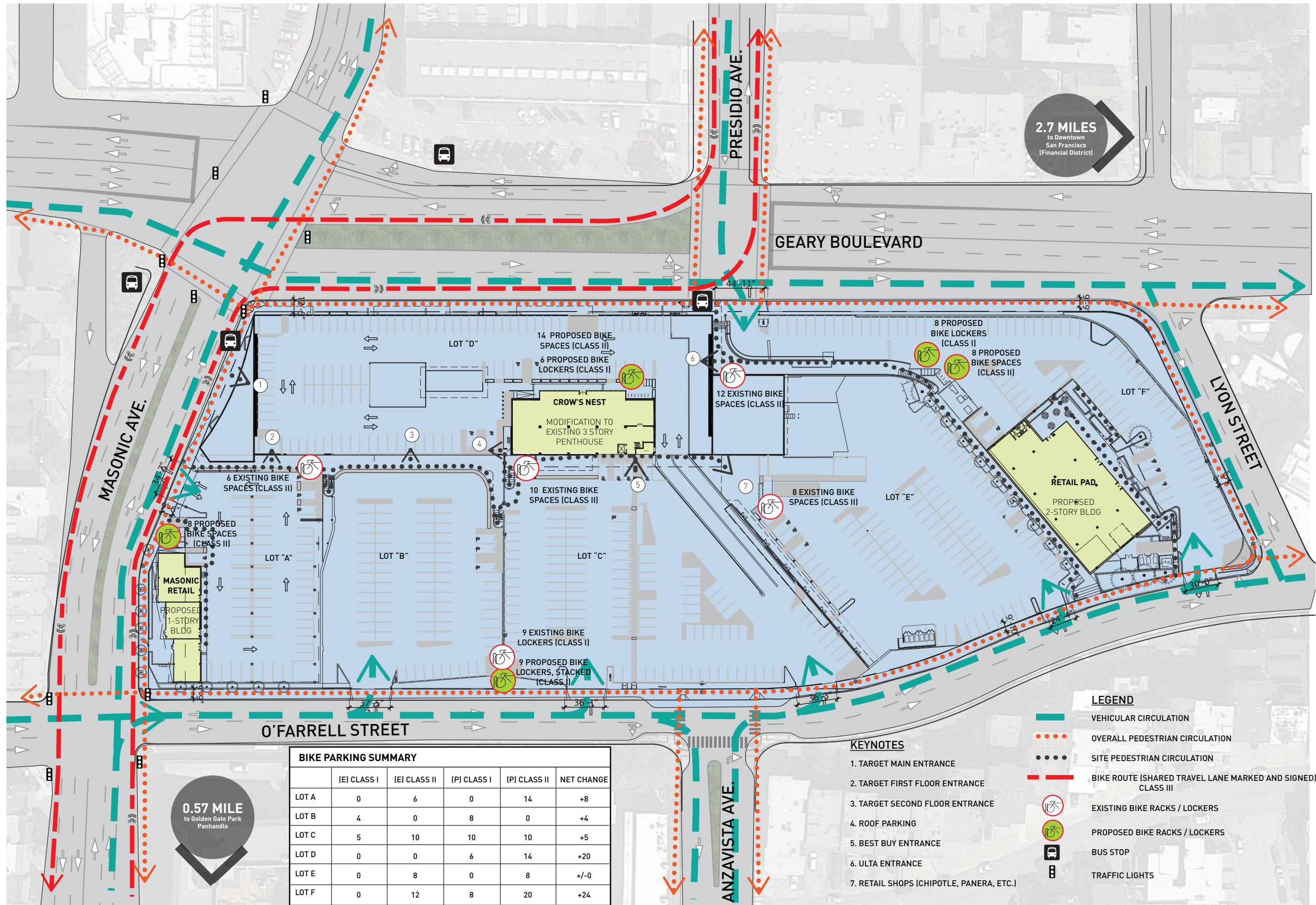
VIEW 7 EXISTING CROWS NEST

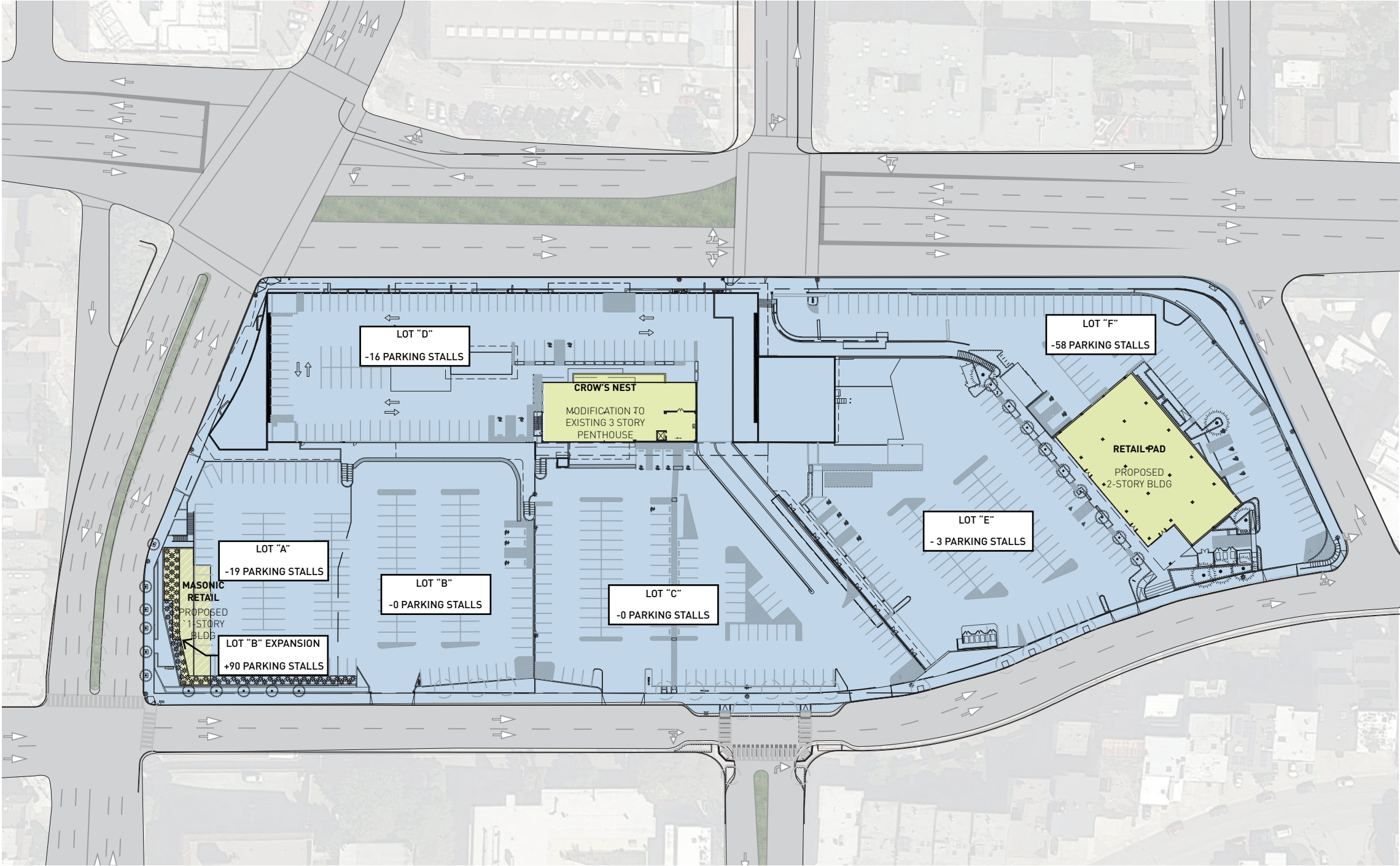


VIEW 6 EXISTING LOT F

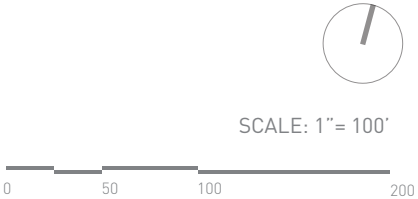


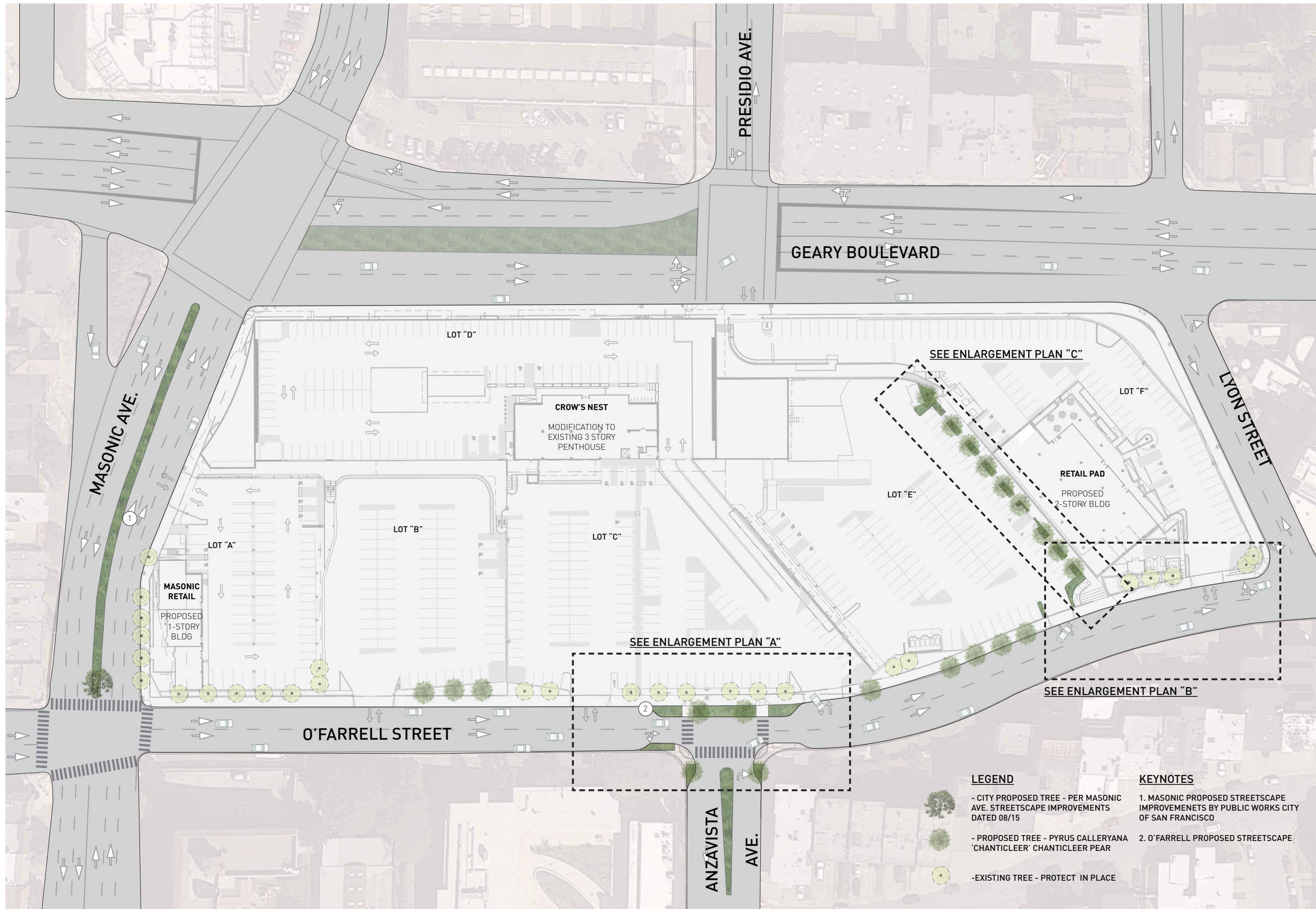
VIEW 8 EXISTING CROWS NEST

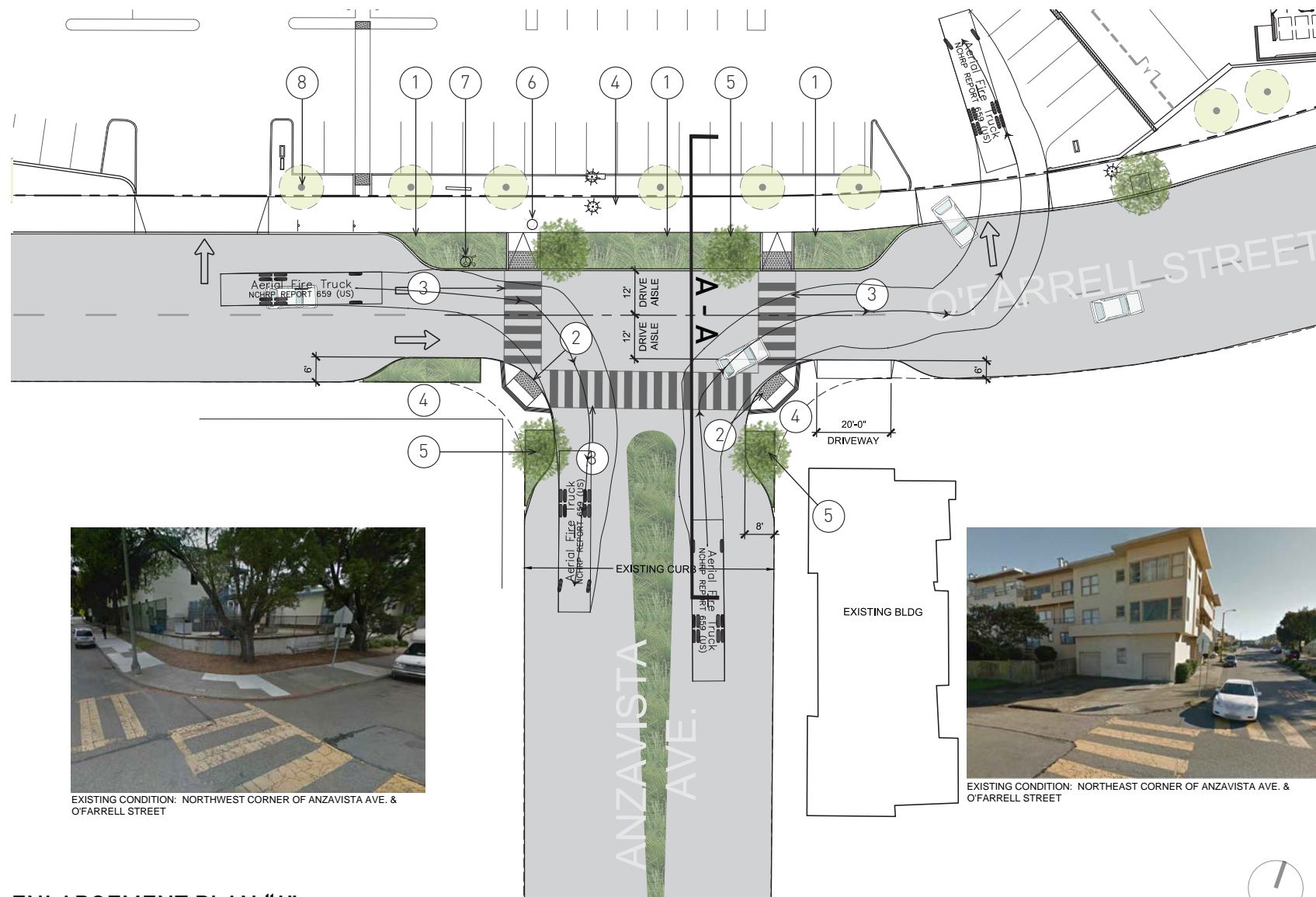




	EXISTING	PROPOSED	NET CHANGE
LOT A	78 STALLS	59 STALLS	-19 STALLS
LOT B	81 STALLS	171 STALLS	+90 STALLS
LOT C	117 STALLS	117 STALLS	+0 STALLS
LOT D	123 STALLS	107 STALLS	-16 STALLS
LOT E	96 STALLS	93 STALLS	-3 STALLS
LOT F	145 STALLS	87 STALLS	-58 STALLS
TOTAL	640 STALLS	634 STALLS	-6 STALLS







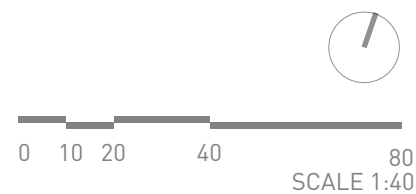
ENLARGEMENT PLAN "A"

KEYNOTES

1. BIO FILTRATION SYSTEM
2. ADA CURB RAMP
3. UNCONTROLLED INTERSECTION W/ CONTINENTAL CROSSWALK MARKERS
4. ADJACENT SIDEWALK
5. O'FARRELL PROPOSED STREETSCENE
6. EXISTING FIRE HYDRANT TO BE RELOCATED
7. NEW FIRE HYDRANT LOCATION
8. EXISTING LANDSCAPE

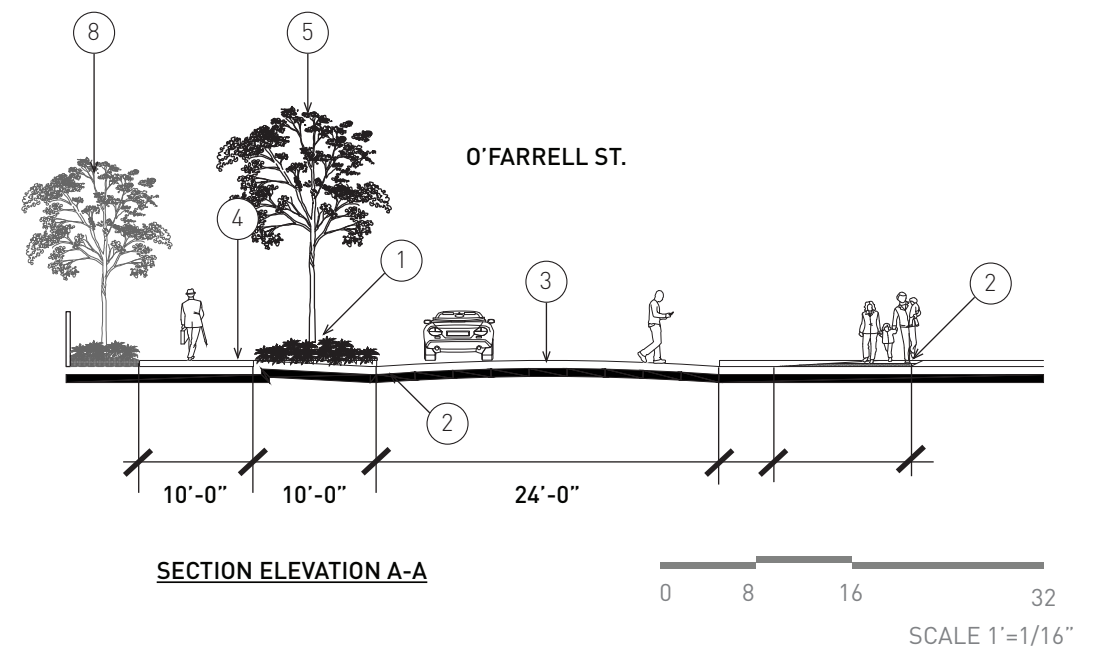
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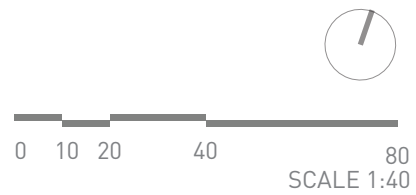
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KEYNOTES

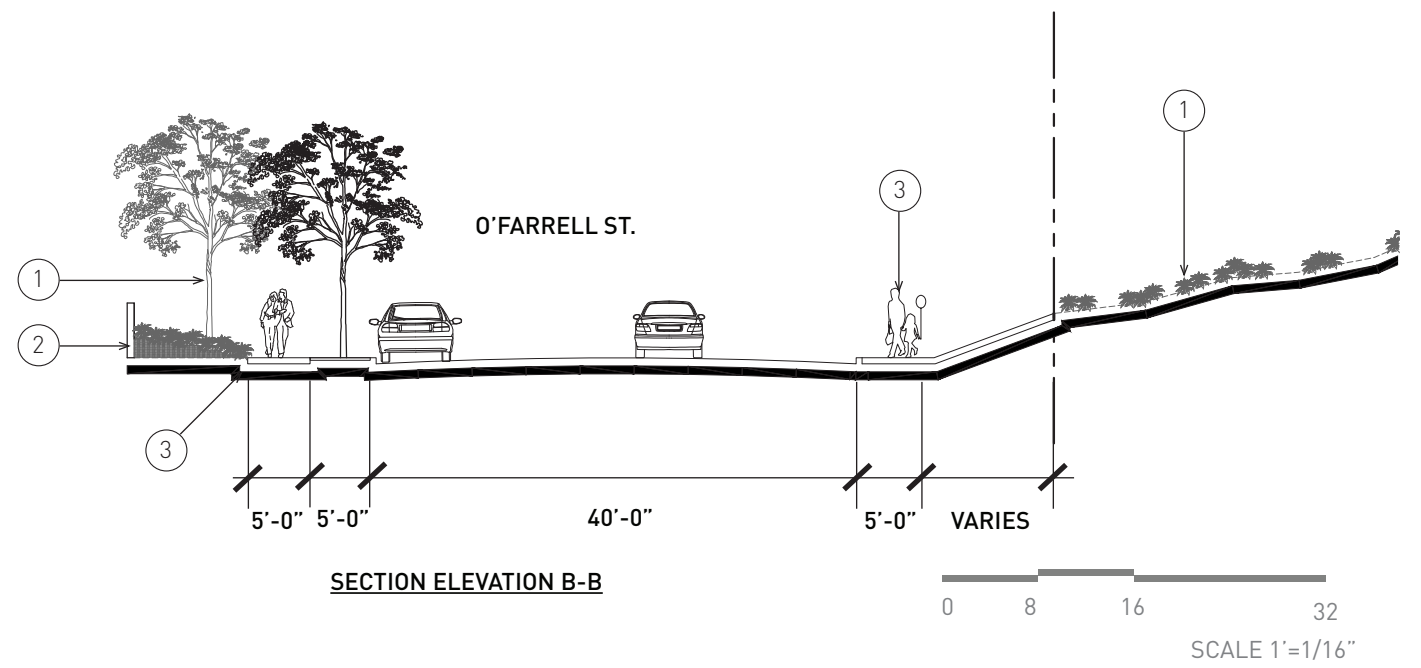
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2. ADA CURB RAMP
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4. ADJACENT SIDEWALK
5. O'FARRELL PROPOSED STREETSCENE
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7. NEW FIRE HYDRANT LOCATION
8. EXISTING LANDSCAPE

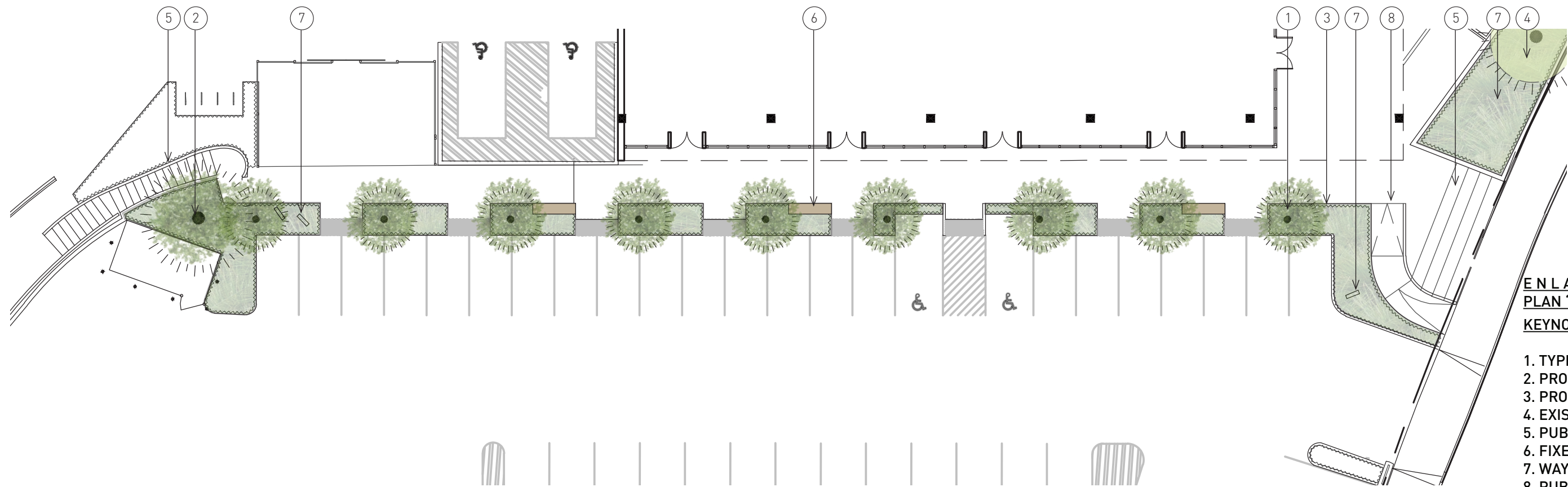




KEYNOTES

- 1. EXISTING LANDSCAPE
- 2. EXISTING WALL
- 3. ADJACENT SIDEWALK





**ENLARGEMENT
PLAN "C"**
KEYNOTES

1. TYPICAL PROPOSED TREE
2. PROPOSED SPECIMEN TREE
3. PROPOSED CURB PLANTERS
4. EXISTING TREE
5. PUBLIC PEDESTRIAN STAIR
6. FIXED SEATING
7. WAYFINDING COMPONENT
8. PUBLIC PEDESTRIAN RAMP

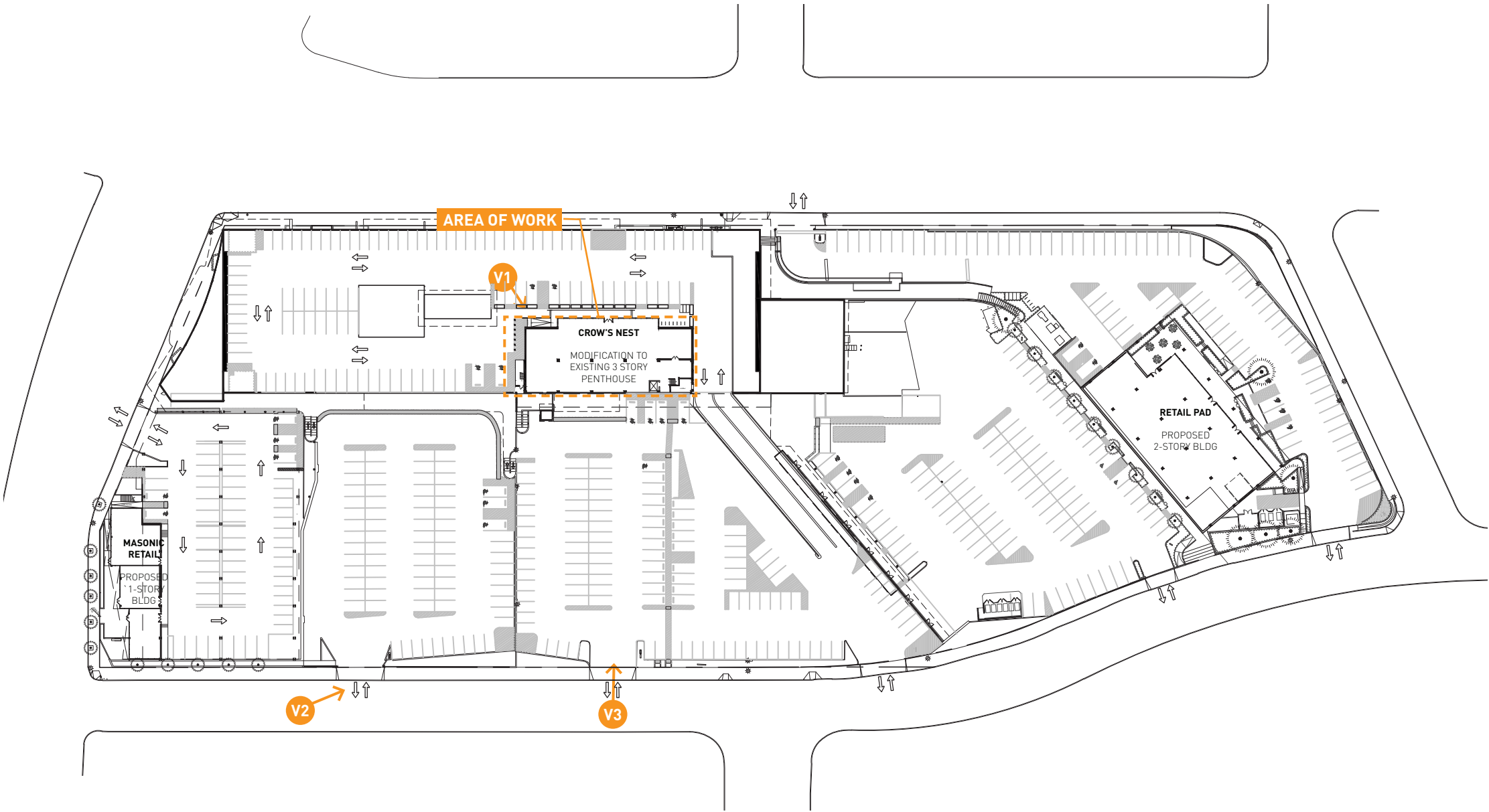
CROWS NEST PROJECT INFO

PROJECT DESCRIPTION

BUILDING AND FACADE IMPROVEMENT TO EXISTING 3 STORY 5,940 SF COMMERCIAL PAD BUILDING AND ADDITION OF 7, 530 SF OF GLA.

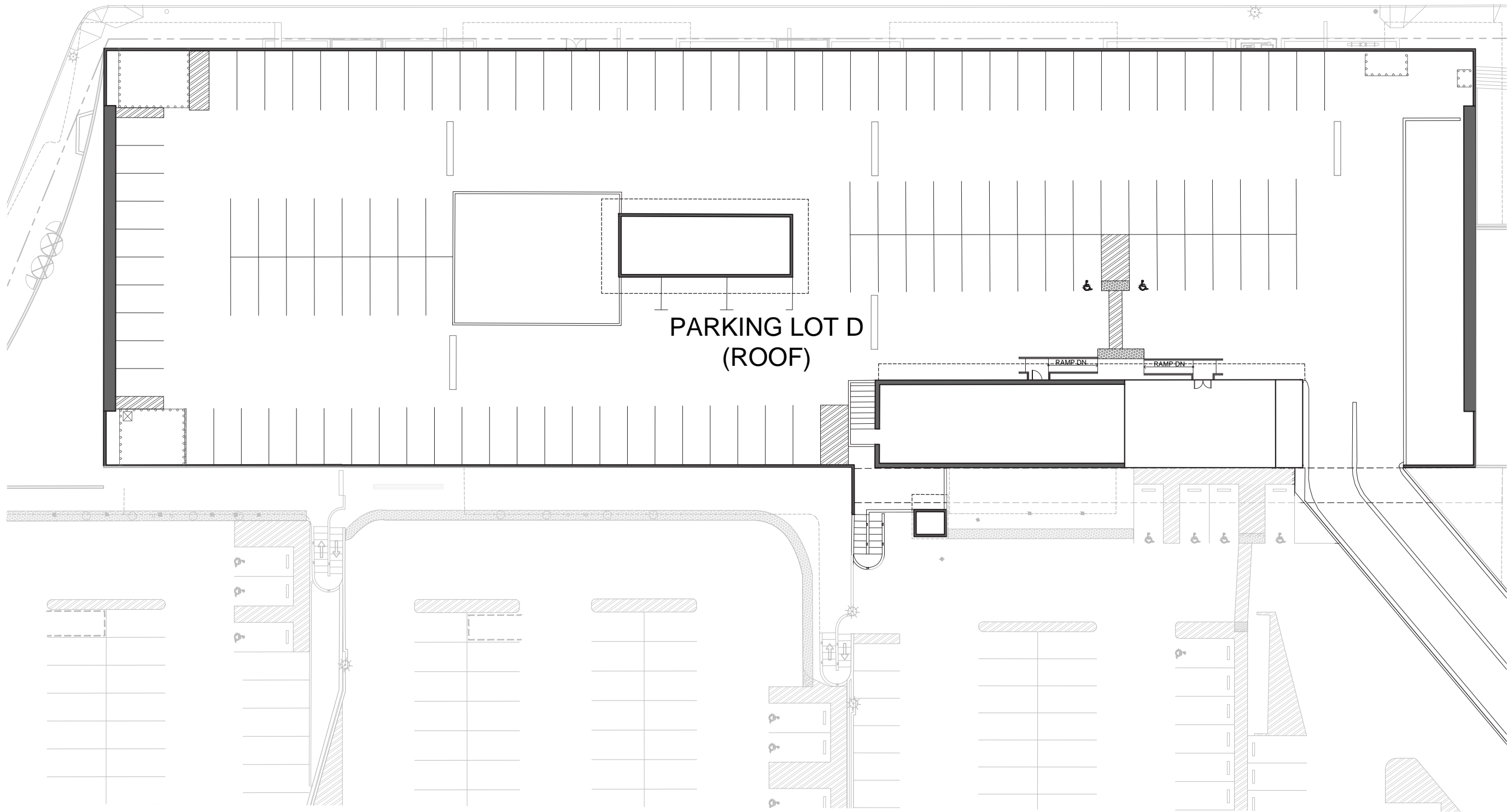
PROJECT SUMMARY

	EXISTING	PROPOSED	DIFFERENCE
LEVEL 1	3,260 SF	6,955 SF	+3,965 SF
LEVEL 2	1,340 SF	6,515 SF	+5,175 SF
LEVEL 3	1,340 SF	0 SF	-1,340 SF
TOTAL	5,940 SF	13,470 SF	+7,530 SF
CAR PARKING (LOT D)	123 STALLS	107 STALLS	-16 STALLS
BIKE PARKING (LOT D)	0 SPACES	20 SPACES	+20 SPACES

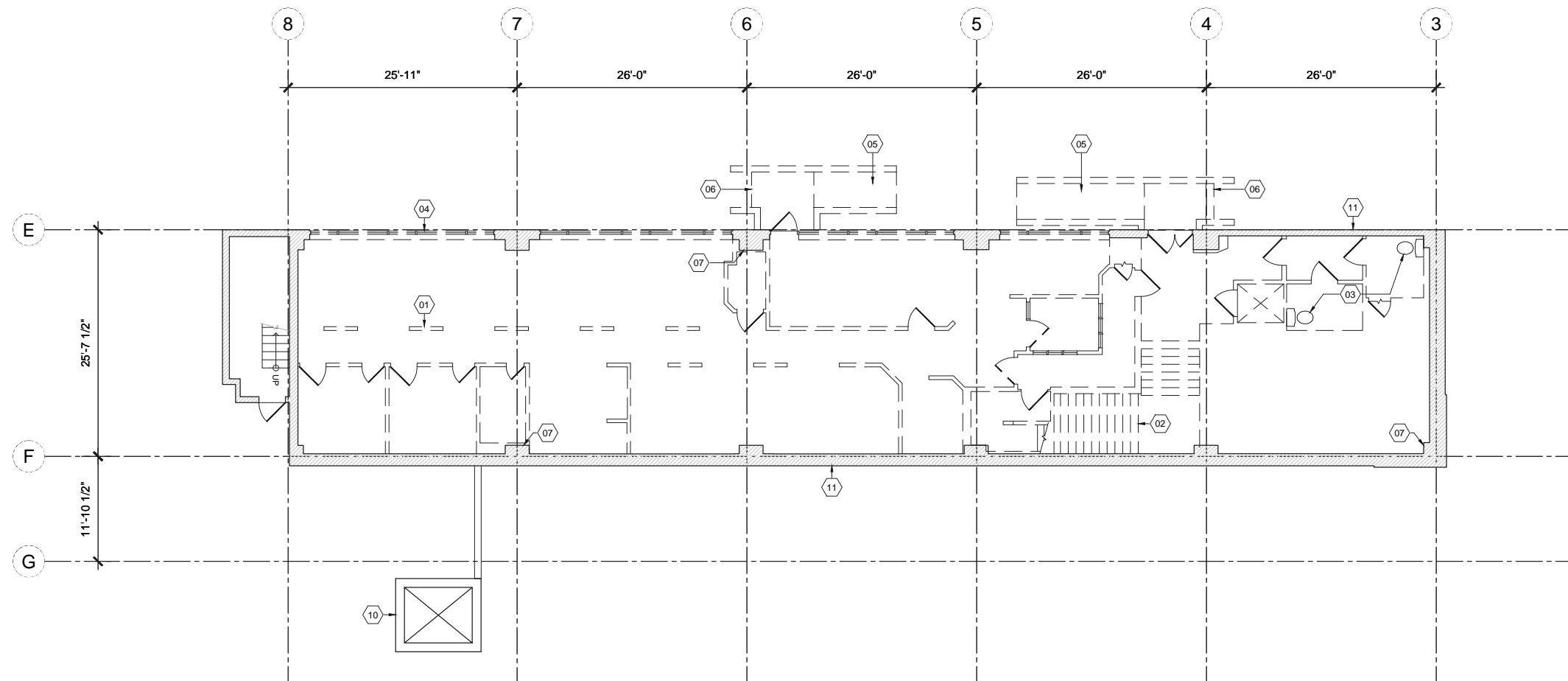


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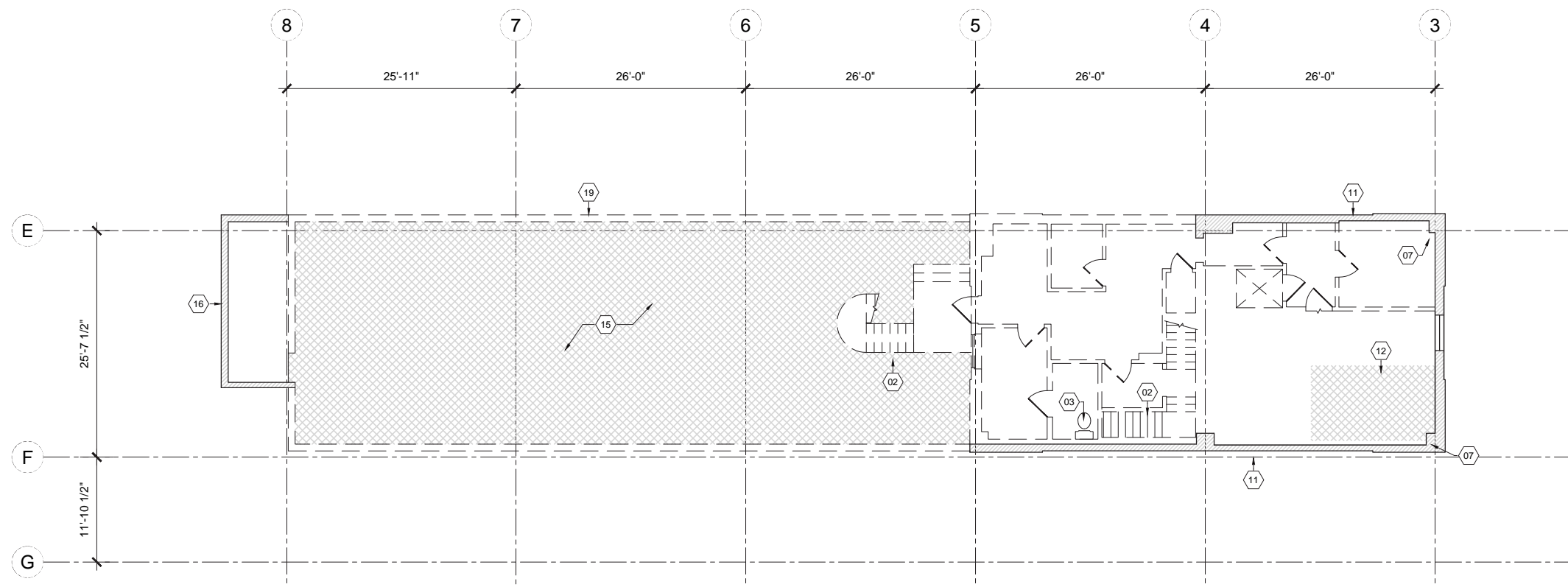




PARKING LOT D
(ROOF)



LEVEL 1 DEMOLITION FLOOR PLAN



LEVEL 2 DEMOLITION FLOOR PLAN

DEMO PLAN KEYNOTES

- 01 DEMO EXISTING INTERIOR STUD FRAMED WALL
- 02 DEMO EXISTING STAIR
- 03 REMOVE EXISTING PLUMBING
- 04 (E) STOREFRONT TO REMAIN
- 05 DEMO EXISTING RAMP AND CONC. SLAB
- 06 DEMO EXISTING STAIR AND CONC. SLAB
- 07 (E) CONCRETE COLUMNS TO REMAIN
- 08 (E) CONCRETE BEAMS TO REMAIN
- 09 DEMO EXISTING EXTERIOR WALL WHERE (N) STOREFRONT OCCURS
- 10 (E) ELEVATOR TO BE RETROFITTED
- 11 (E) CONCRETE WALL TO REMAIN
- 12 DEMO EXISTING FLOOR
- 13 DEMO EXISTING CLERESTORY WINDOW
- 14 DEMO EXISTING CONCRETE COLUMN
- 15 DEMO EXISTING ROOF
- 16 REMOVE EXISTING CONC. GUARDRAIL
- 17 DEMO EXISTING SLAB
- 18 EXISTING SLAB TO REMAIN
- 19 DEMO EXISTING PARAPET

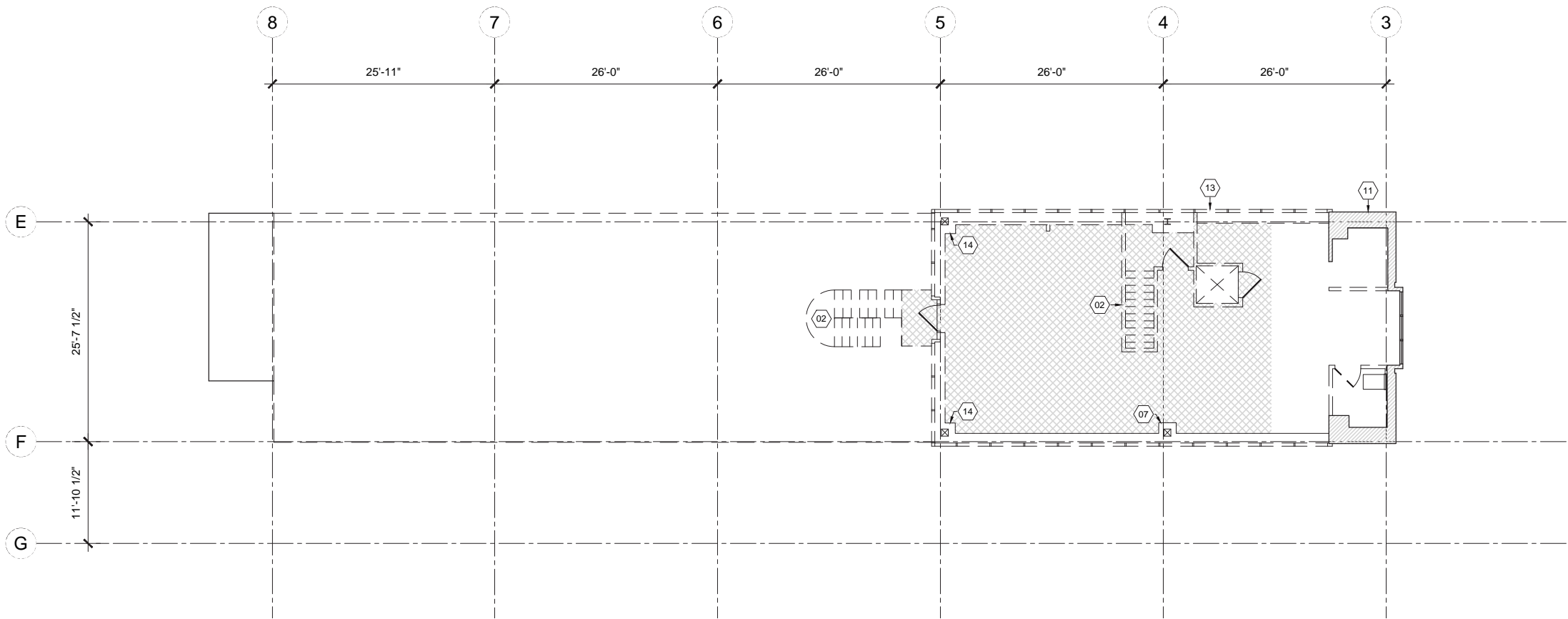


SCALE: 1/16"=1'

DEMOLITION PLAN (FOR REFERENCE ONLY) | LEVEL 1 + 2

CITY CENTER CROW'S NEST

05.30.2017



LEVEL 3 DEMOLITION FLOOR PLAN

DEMO PLAN KEYNOTES

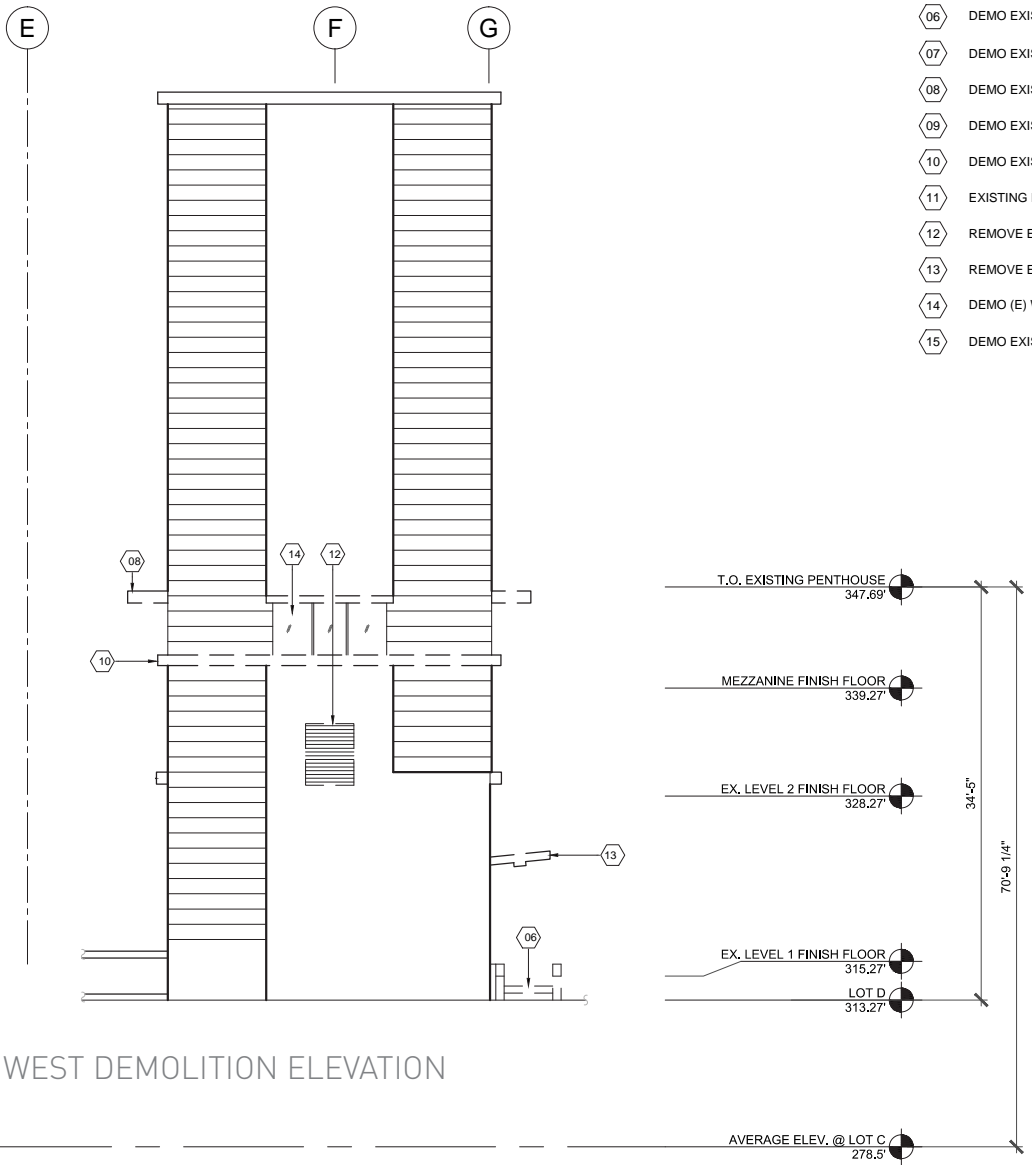
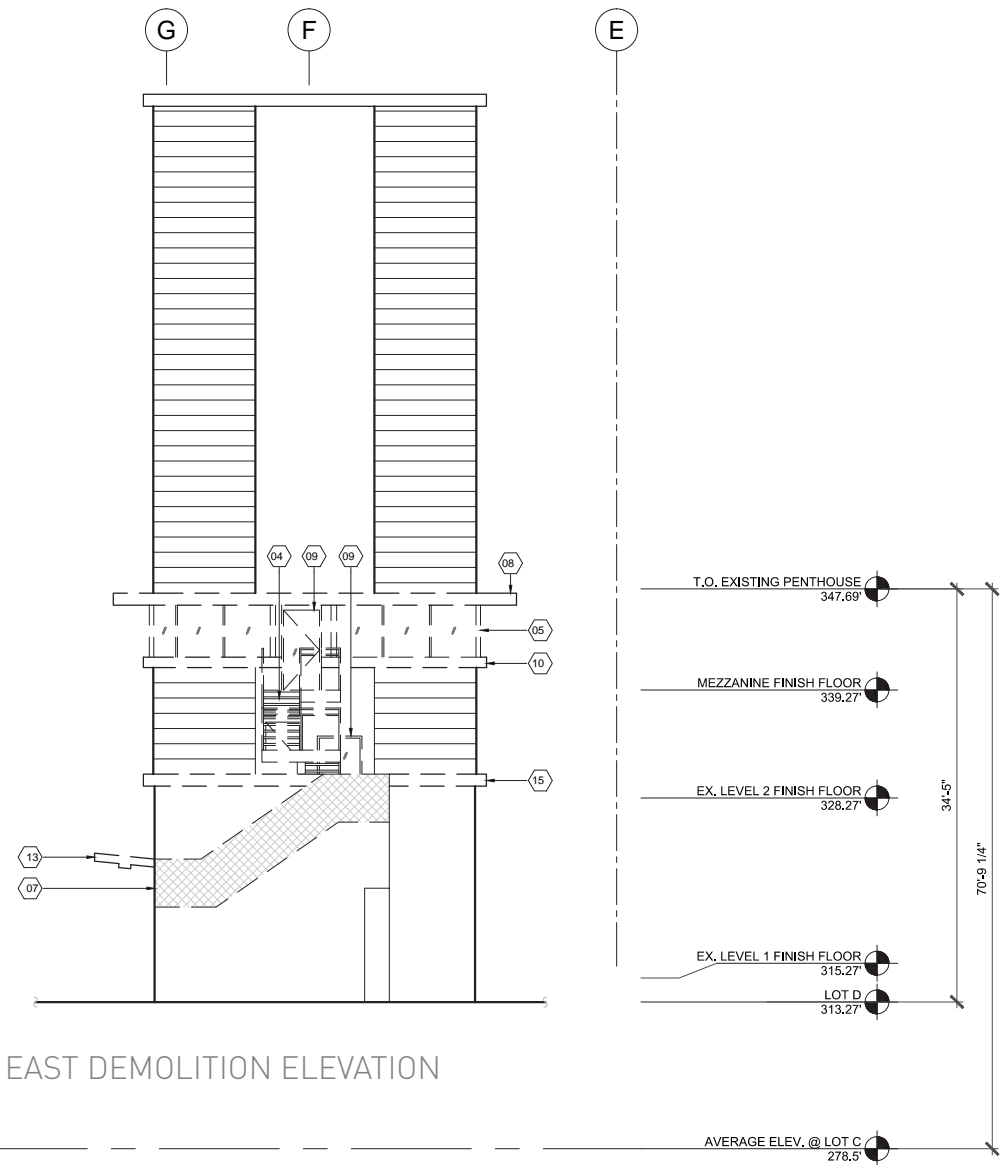
- 01 DEMO EXISTING INTERIOR STUD FRAMED WALL
- 02 DEMO EXISTING STAIR
- 03 REMOVE EXISTING PLUMBING
- 04 (E) STOREFRONT TO REMAIN
- 05 DEMO EXISTING RAMP AND CONC. SLAB
- 06 DEMO EXISTING STAIR AND CONC. SLAB
- 07 (E) CONCRETE COLUMNS TO REMAIN
- 08 (E) CONCRETE BEAMS TO REMAIN
- 09 DEMO EXISTING EXTERIOR WALL WHERE (N) STOREFRONT OCCURS
- 10 (E) ELEVATOR TO BE RETROFITTED
- 11 (E) CONCRETE WALL TO REMAIN
- 12 DEMO EXISTING FLOOR
- 13 DEMO EXISTING CLERESTORY WINDOW
- 14 DEMO EXISTING CONCRETE COLUMN
- 15 DEMO EXISTING ROOF
- 16 REMOVE EXISTING CONC. GUARDRAIL
- 17 DEMO EXISTING SLAB
- 18 EXISTING SLAB TO REMAIN
- 19 DEMO EXISTING PARAPET



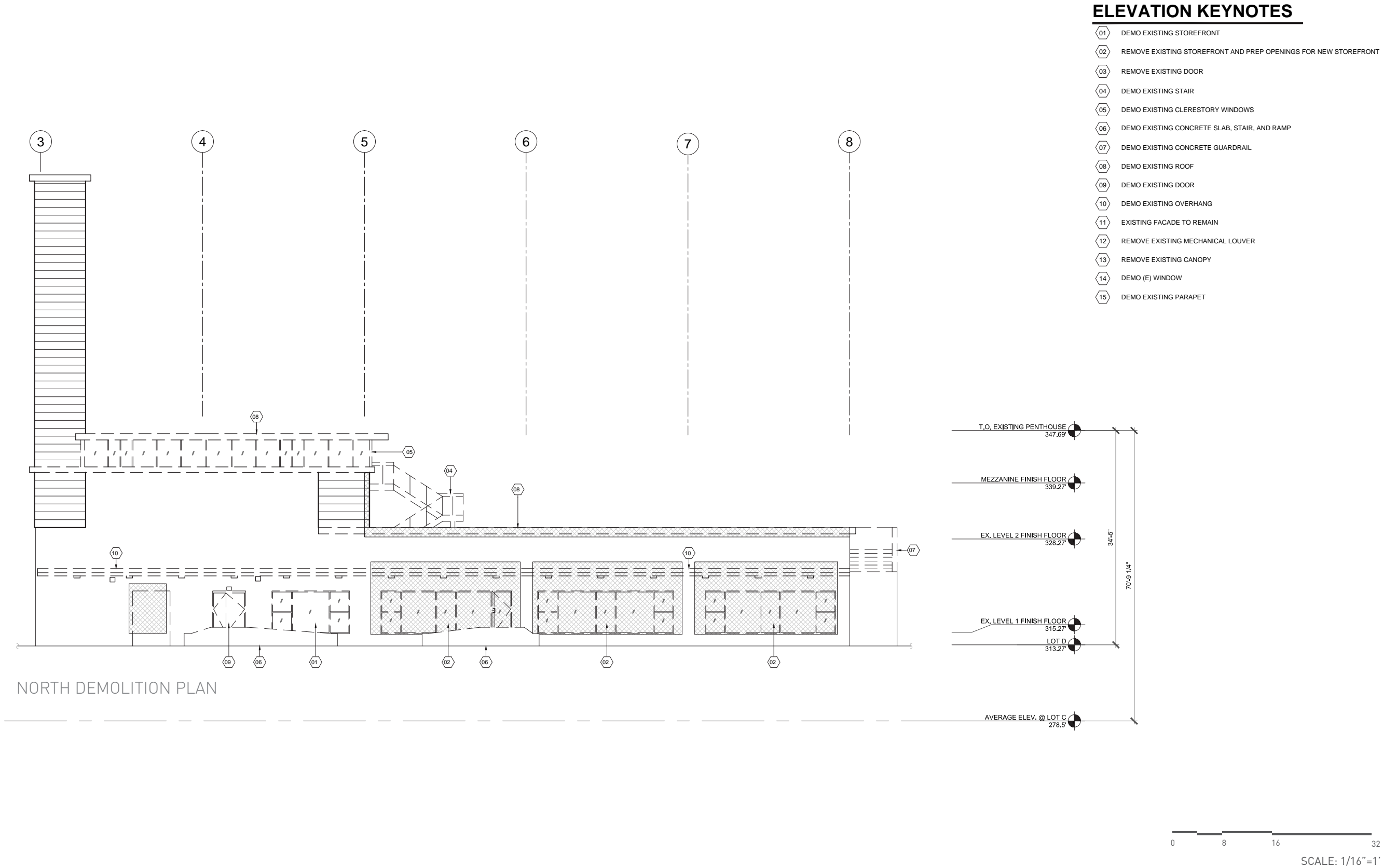
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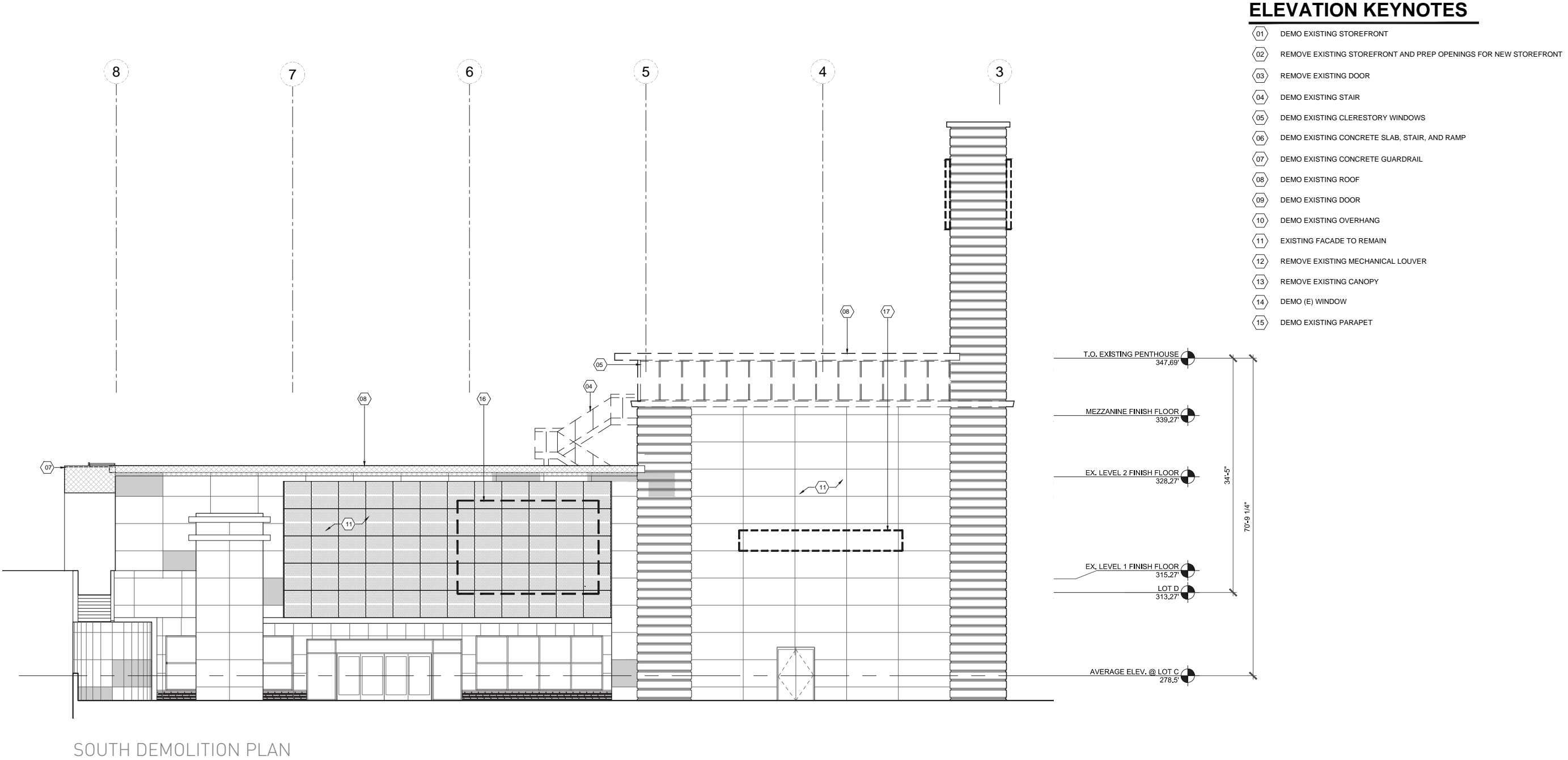
ELEVATION KEYNOTES

- 01 DEMO EXISTING STOREFRONT
- 02 REMOVE EXISTING STOREFRONT AND PREP OPENINGS FOR NEW STOREFRONT
- 03 REMOVE EXISTING DOOR
- 04 DEMO EXISTING STAIR
- 05 DEMO EXISTING CLERESTORY WINDOWS
- 06 DEMO EXISTING CONCRETE SLAB, STAIR, AND RAMP
- 07 DEMO EXISTING CONCRETE GUARDRAIL
- 08 DEMO EXISTING ROOF
- 09 DEMO EXISTING DOOR
- 10 DEMO EXISTING OVERHANG
- 11 EXISTING FACADE TO REMAIN
- 12 REMOVE EXISTING MECHANICAL LOUVER
- 13 REMOVE EXISTING CANOPY
- 14 DEMO (E) WINDOW
- 15 DEMO EXISTING PARAPET

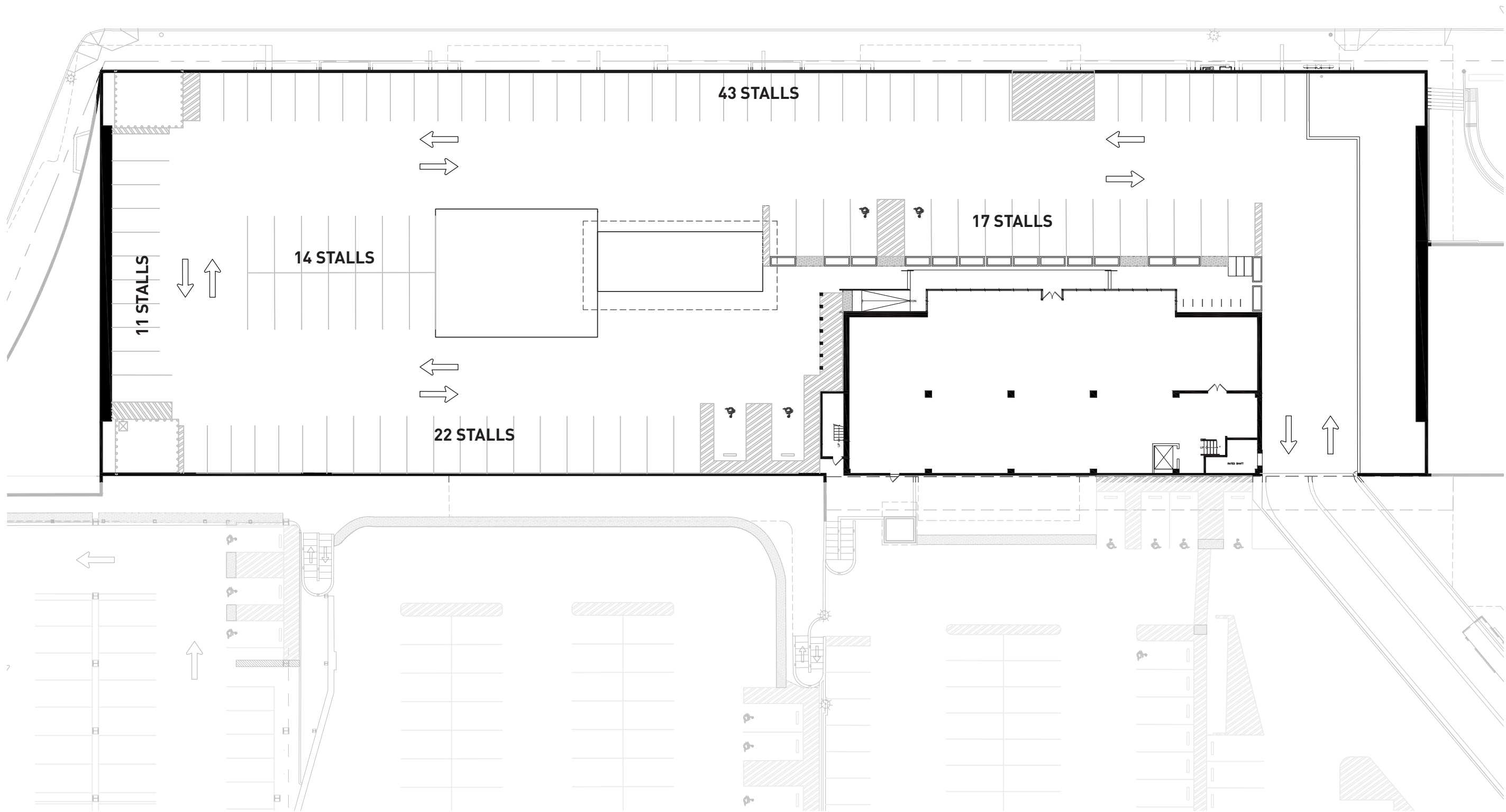


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SOUTH DEMOLITION PLAN



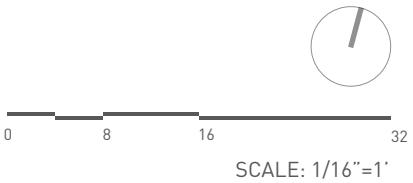
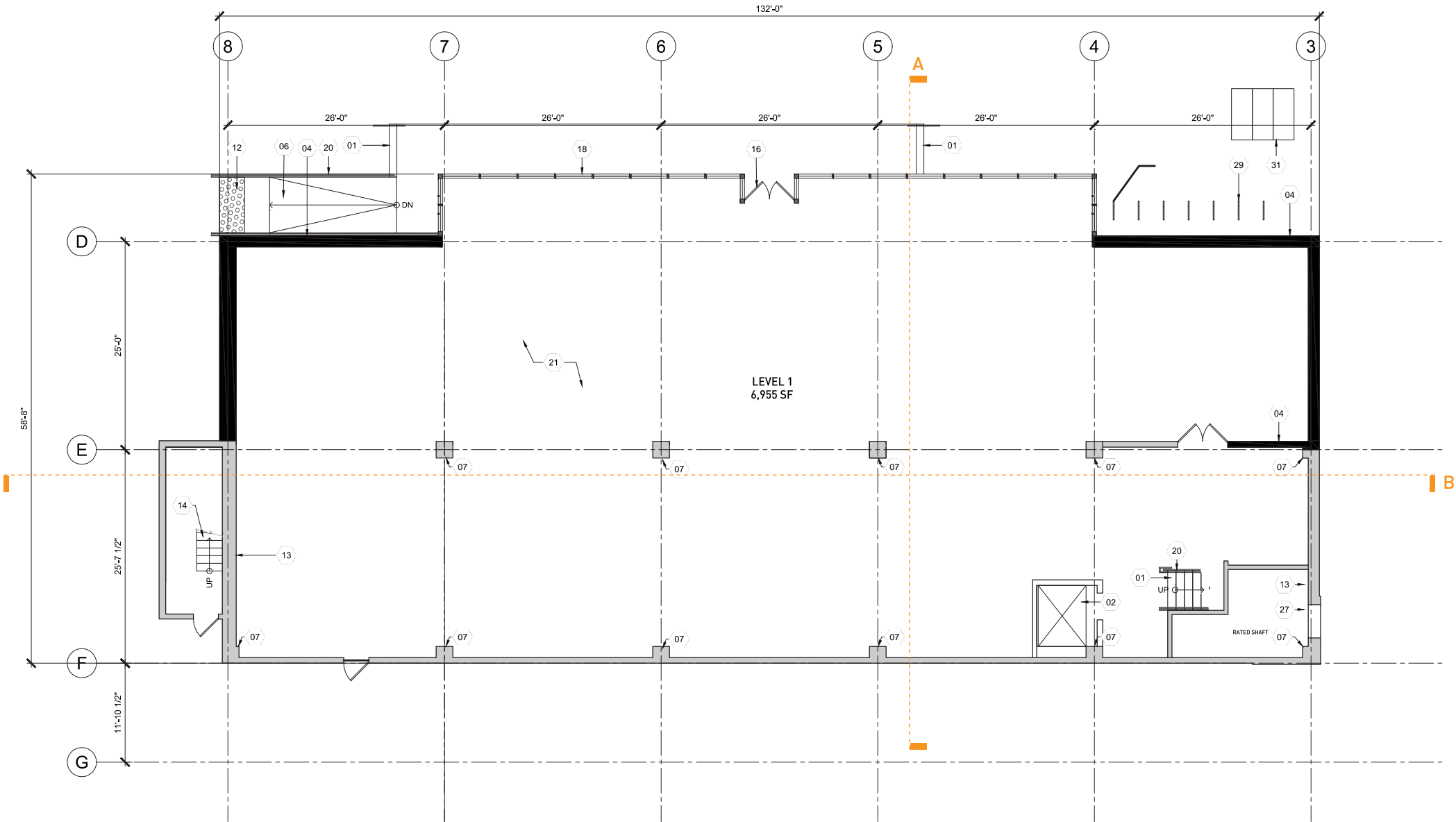
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PROPOSED SITE PLAN
CITY CENTER CROWS NEST
08.20.2018

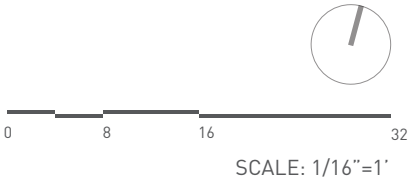
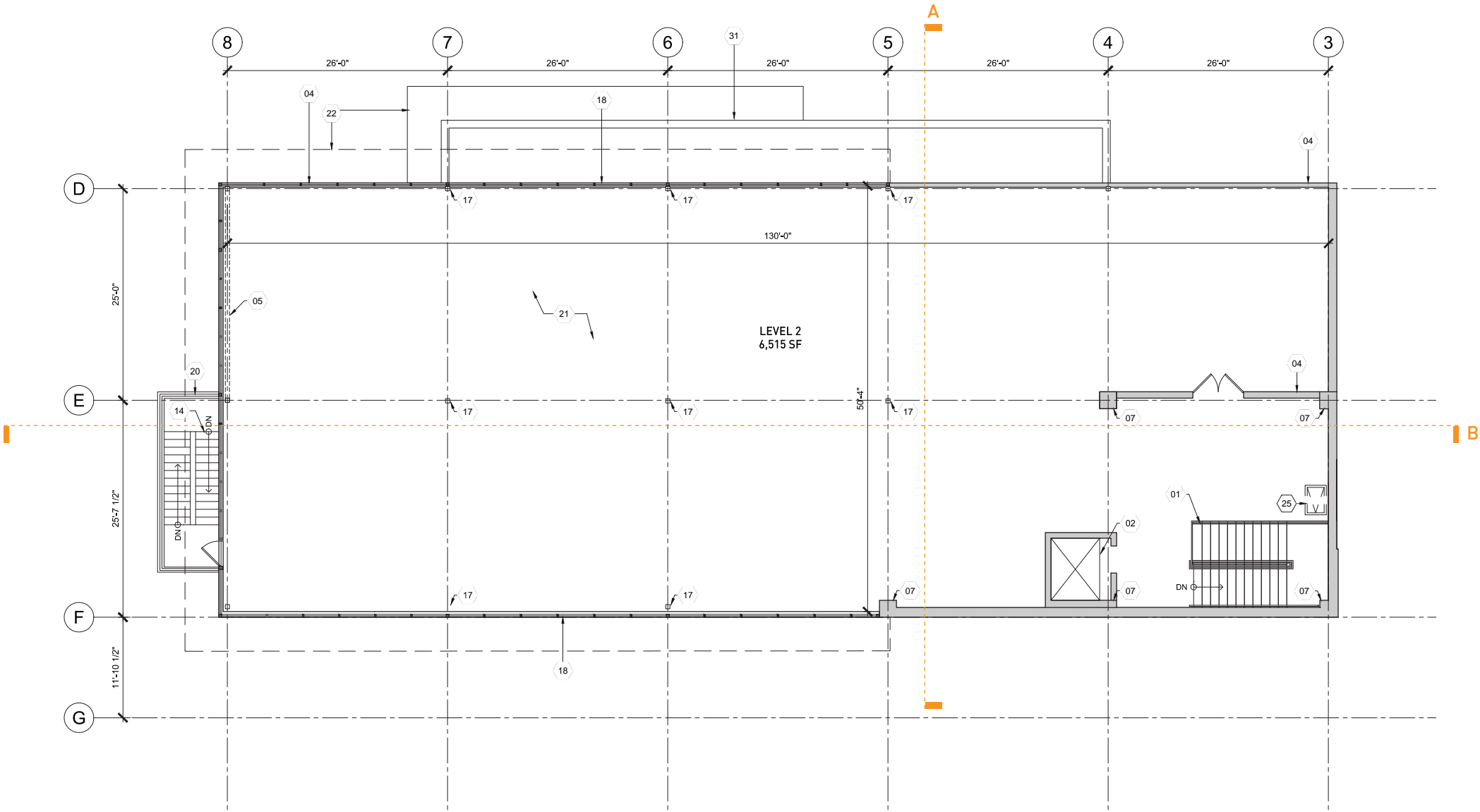
FLOOR PLAN KEYNOTES

- 01 (N) STAIR
- 02 (N) ELEVATOR
- 03 (E) DOOR
- 04 SHEAR WALL
- 05 (N) BRACED FRAME
- 06 (N) RAMP
- 07 (E) CONCRETE COLUMN
- 08 (E) CONCRETE BEAM
- 09 NOT USED
- 10 NOT USED
- 11 NOT USED
- 12 (N) TRUNCATED DOME SURFACE
- 13 (E) CONCRETE WALL W/ 4" SHOTCRETE ADD'L REBAR
- 14 (E) STAIRS
- 15 (E) TOWER
- 16 (N) 6'-0" X 8'-0" ALUMINUM DOOR & FRAME
- 17 (N) HOLLOW STRUCTURAL SECTION COLUMN
- 18 (N) ALUMINUM STOREFRONT WINDOW; SEE ELEVATIONS
- 19 (E) MECHANICAL LEDGE
- 20 (N) BRUSHED STAINLESS STEEL CABLE GUARDRAIL AND HANDRAIL
- 21 CONCRETE SLAB TO REMAIN AND TO BE EXPANDED PER EXPANDED FOOTPRINT
- 22 (N) OVERHANG
- 23 (N) MECHANICAL LOUVRE
- 24 (N) T.P.O. ROOF O/ RIGID INSULATION
- 25 (N) HATCH TO PROVIDE ACCESS TO MECHANICAL LEDGE
- 26 (N) ROOF HATCH
- 27 RELOCATED ELEVATOR EXHAUST FROM FREIGHT ELEVATORS BELOW
- 28 NOT USED
- 29 (N) CLASS II BIKE PARKING STALLS
- 30 (N) PARAPET
- 31 (N) PARAPET BELOW
- 32 (N) CLASS I BIKE LOCKERS, STACKED



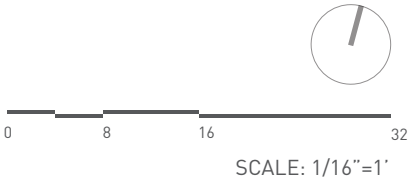
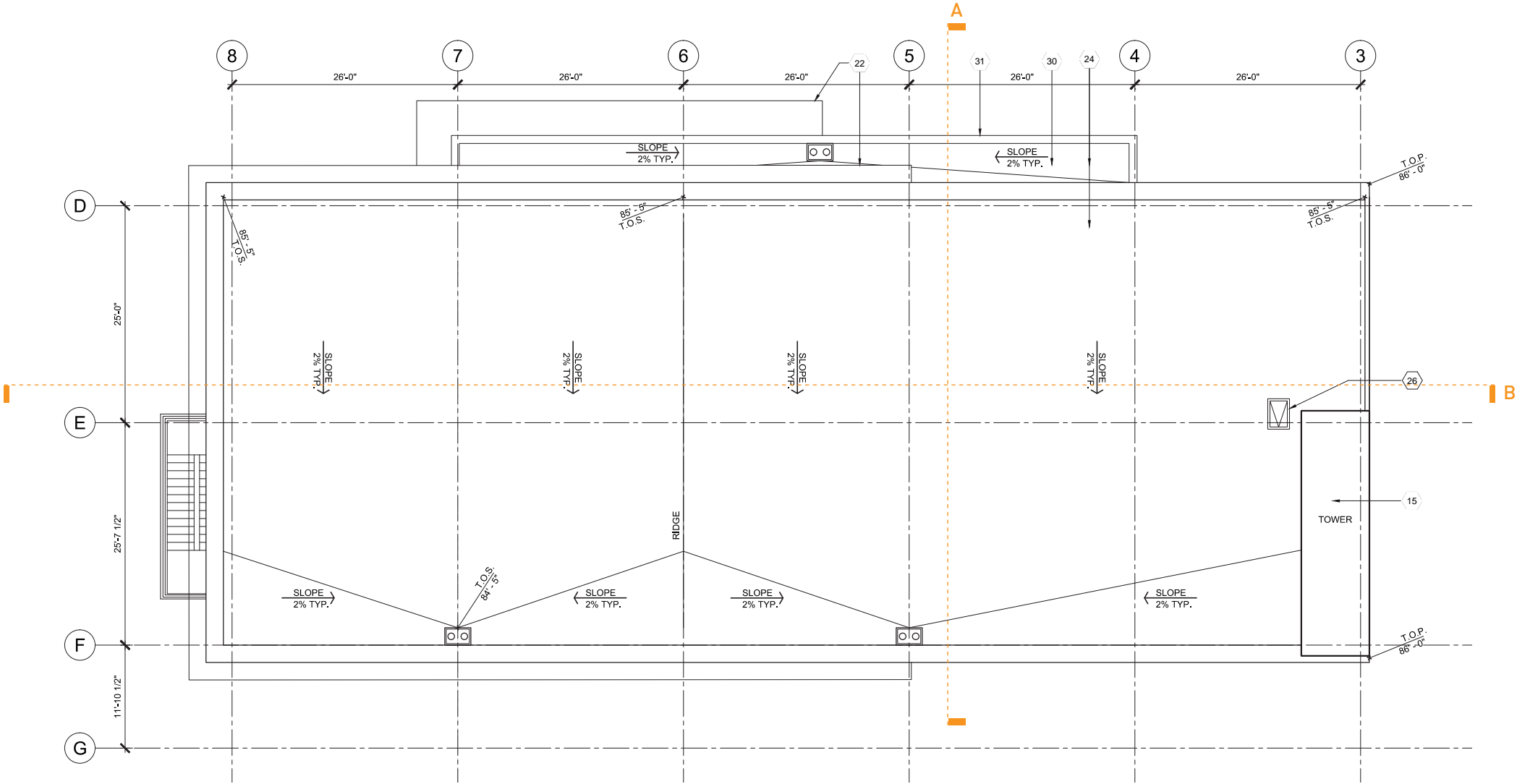
FLOOR PLAN KEYNOTES

- 01 (N) STAIR
- 02 (N) ELEVATOR
- 03 (E) DOOR
- 04 SHEAR WALL
- 05 (N) BRACED FRAME
- 06 (N) RAMP
- 07 (E) CONCRETE COLUMN
- 08 (E) CONCRETE BEAM
- 09 NOT USED
- 10 NOT USED
- 11 NOT USED
- 12 (N) TRUNCATED DOME SURFACE
- 13 (E) CONCRETE WALL W/ 4" SHOTCRETE ADD'L REBAR
- 14 (E) STAIRS
- 15 (E) TOWER
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- 17 (N) HOLLOW STRUCTURAL SECTION COLUMN
- 18 (N) ALUMINUM STOREFRONT WINDOW; SEE ELEVATIONS
- 19 (E) MECHANICAL LEDGE
- 20 (N) BRUSHED STAINLESS STEEL CABLE GUARDRAIL AND HANDRAIL
- 21 CONCRETE SLAB TO REMAIN AND TO BE EXPANDED PER EXPANDED FOOTPRINT
- 22 (N) OVERHANG
- 23 (N) MECHANICAL LOUVRE
- 24 (N) T.P.O. ROOF O/ RIGID INSULATION
- 25 (N) HATCH TO PROVIDE ACCESS TO MECHANICAL LEDGE
- 26 (N) ROOF HATCH
- 27 RELOCATED ELEVATOR EXHAUST FROM FREIGHT ELEVATORS BELOW
- 28 NOT USED
- 29 (N) CLASS II BIKE PARKING STALLS
- 30 (N) PARAPET
- 31 (N) PARAPET BELOW
- 32 (N) CLASS I BIKE LOCKERS, STACKED



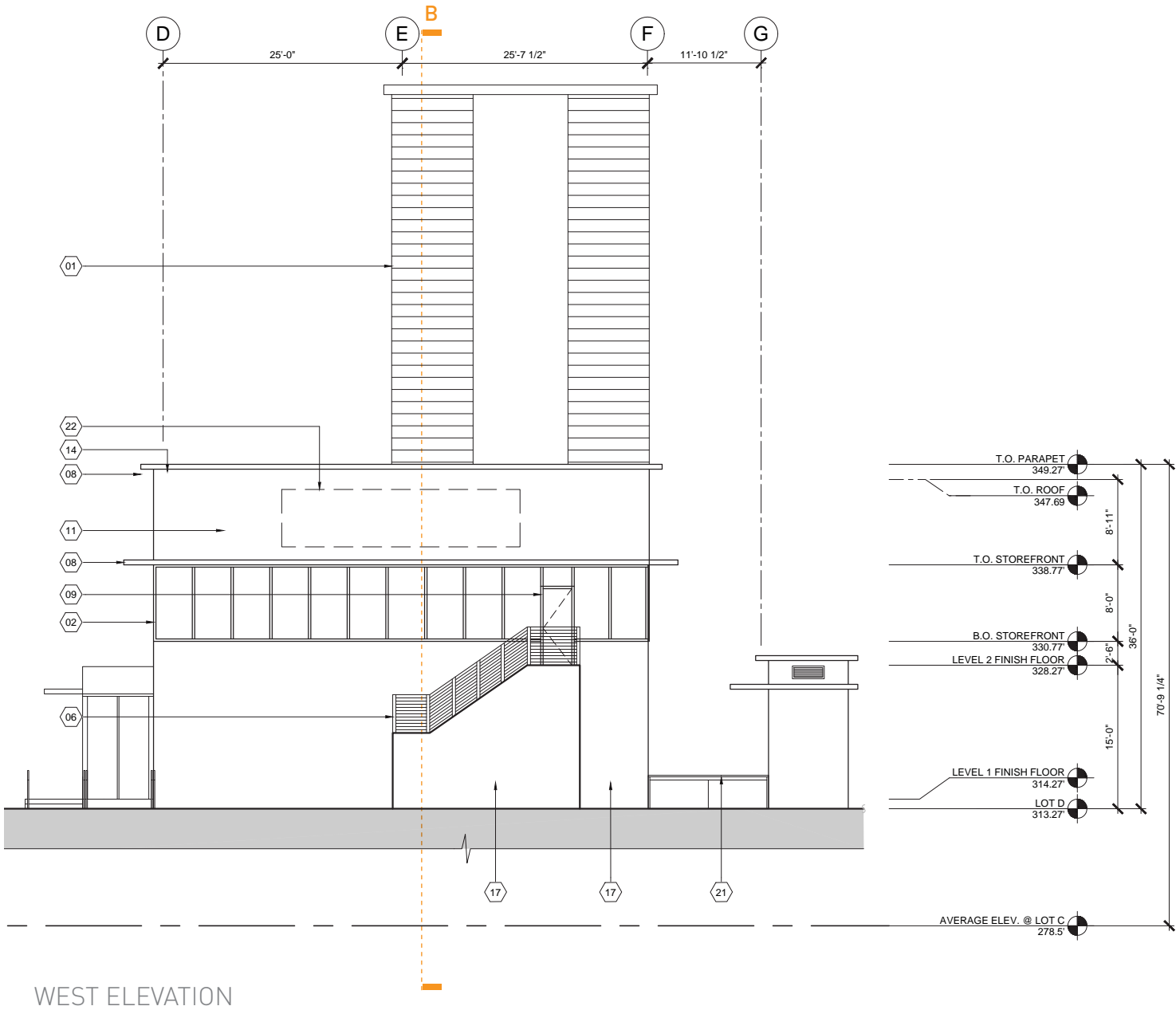
FLOOR PLAN KEYNOTES

- 01 (N) STAIR
- 02 (N) ELEVATOR
- 03 (E) DOOR
- 04 SHEAR WALL
- 05 (N) BRACED FRAME
- 06 (N) RAMP
- 07 (E) CONCRETE COLUMN
- 08 (E) CONCRETE BEAM
- 09 NOT USED
- 10 NOT USED
- 11 NOT USED
- 12 (N) TRUNCATED DOME SURFACE
- 13 (E) CONCRETE WALL W/ 4" SHOTCRETE ADD'L REBAR
- 14 (E) STAIRS
- 15 (E) TOWER
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- 17 (N) HOLLOW STRUCTURAL SECTION COLUMN
- 18 (N) ALUMINUM STOREFRONT WINDOW; SEE ELEVATIONS
- 19 (E) MECHANICAL LEDGE
- 20 (N) BRUSHED STAINLESS STEEL CABLE GUARDRAIL AND HANDRAIL
- 21 CONCRETE SLAB TO REMAIN AND TO BE EXPANDED PER EXPANDED FOOTPRINT
- 22 (N) OVERHANG
- 23 (N) MECHANICAL LOUVRE
- 24 (N) T.P.O. ROOF O/ RIGID INSULATION
- 25 (N) HATCH TO PROVIDE ACCESS TO MECHANICAL LEDGE
- 26 (N) ROOF HATCH
- 27 RELOCATED ELEVATOR EXHAUST FROM FREIGHT ELEVATORS BELOW
- 28 NOT USED
- 29 (N) CLASS II BIKE PARKING STALLS
- 30 (N) PARAPET
- 31 (N) PARAPET BELOW
- 32 (N) CLASS I BIKE LOCKERS, STACKED

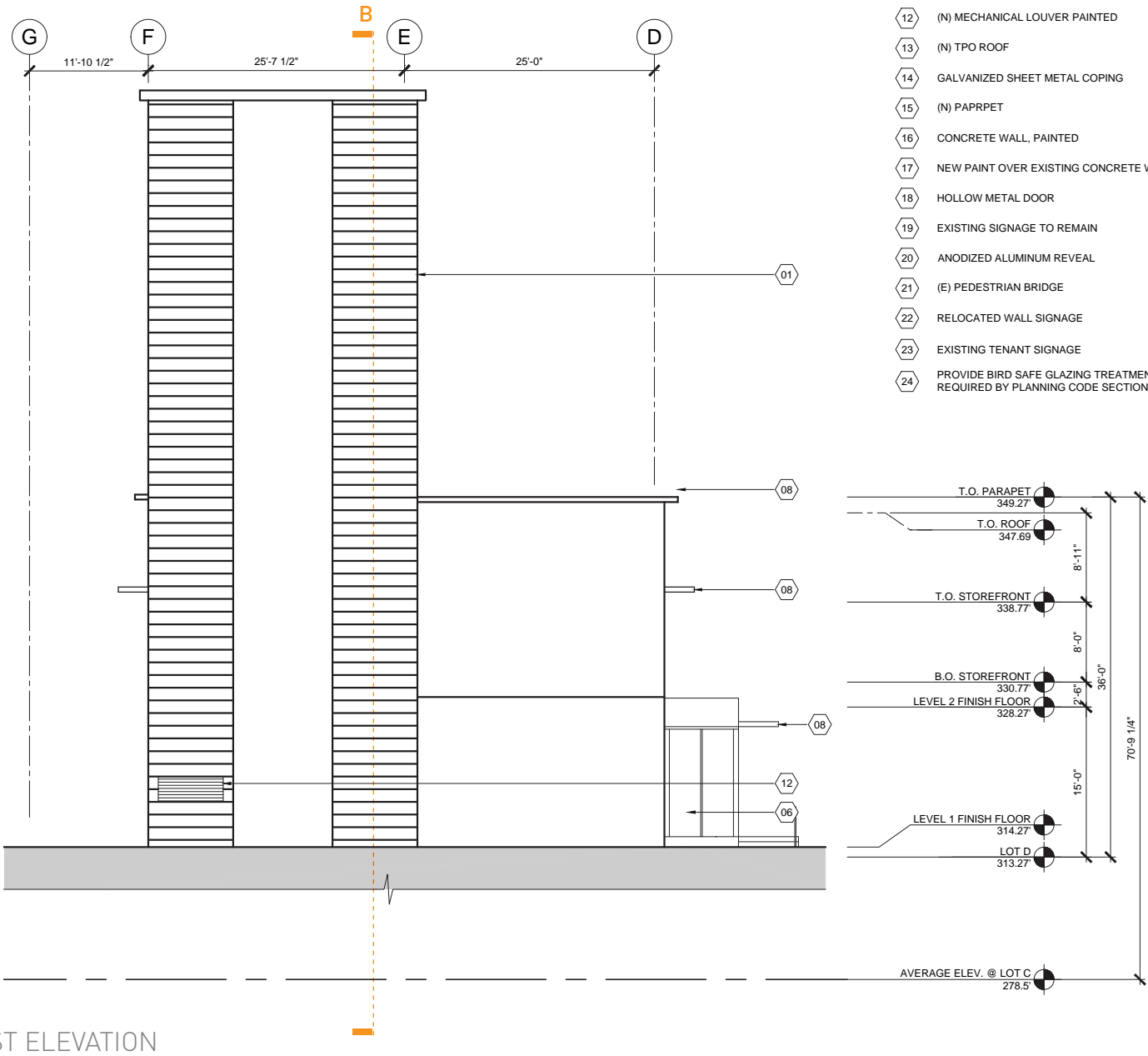


ELEVATION KEYNOTES

- 01 (E) TOWER
- 02 (N) ANODIZED ALUMINUM STOREFRONT
- 03 (N) CONCRETE RAMP
- 04 (N) CONCRETE STAIR
- 05 (N) HOLLOW STRUCTURAL SECTION CANOPY
- 06 (N) BRUSHED STAINLESS STEEL CABLE GUARDRAIL
- 07 (E) CONCRETE WITH NEW PAINT
- 08 (N) OVERHANG
- 09 3' x 8' ANODIZED ALUMINUM DOOR
- 10 FUTURE SIGNAGE
- 11 INTEGRAL COLOR EXTERIOR CEMENT PLASTER
- 12 (N) MECHANICAL LOUVER PAINTED
- 13 (N) TPO ROOF
- 14 GALVANIZED SHEET METAL COPING
- 15 (N) PAPRPET
- 16 CONCRETE WALL, PAINTED
- 17 NEW PAINT OVER EXISTING CONCRETE WALL
- 18 HOLLOW METAL DOOR
- 19 EXISTING SIGNAGE TO REMAIN
- 20 ANODIZED ALUMINUM REVEAL
- 21 (E) PEDESTRIAN BRIDGE
- 22 RELOCATED WALL SIGNAGE
- 23 EXISTING TENANT SIGNAGE
- 24 PROVIDE BIRD SAFE GLAZING TREATMENT WHERE REQUIRED BY PLANNING CODE SECTION 139, TYP.



WEST ELEVATION

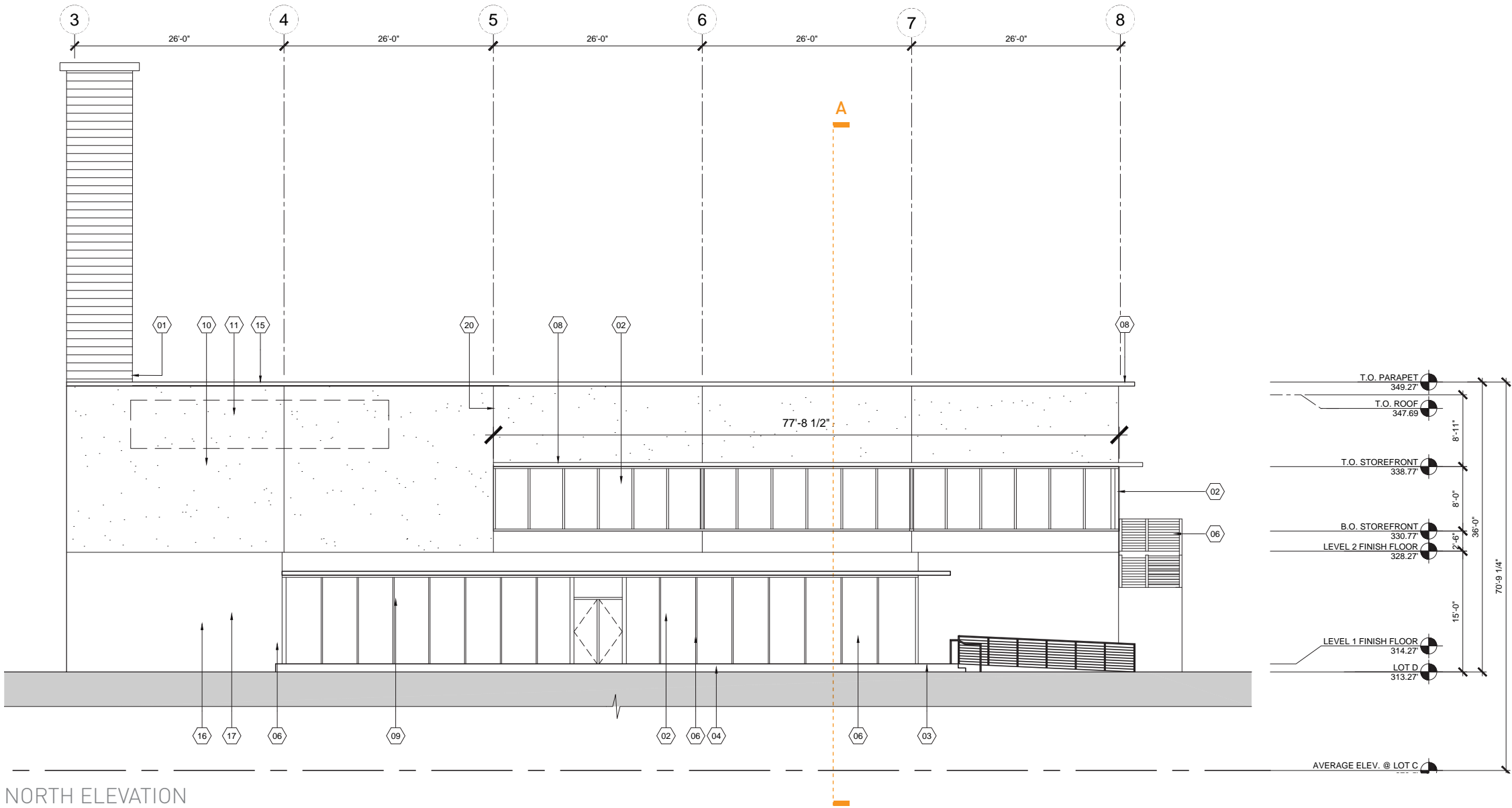


EAST ELEVATION



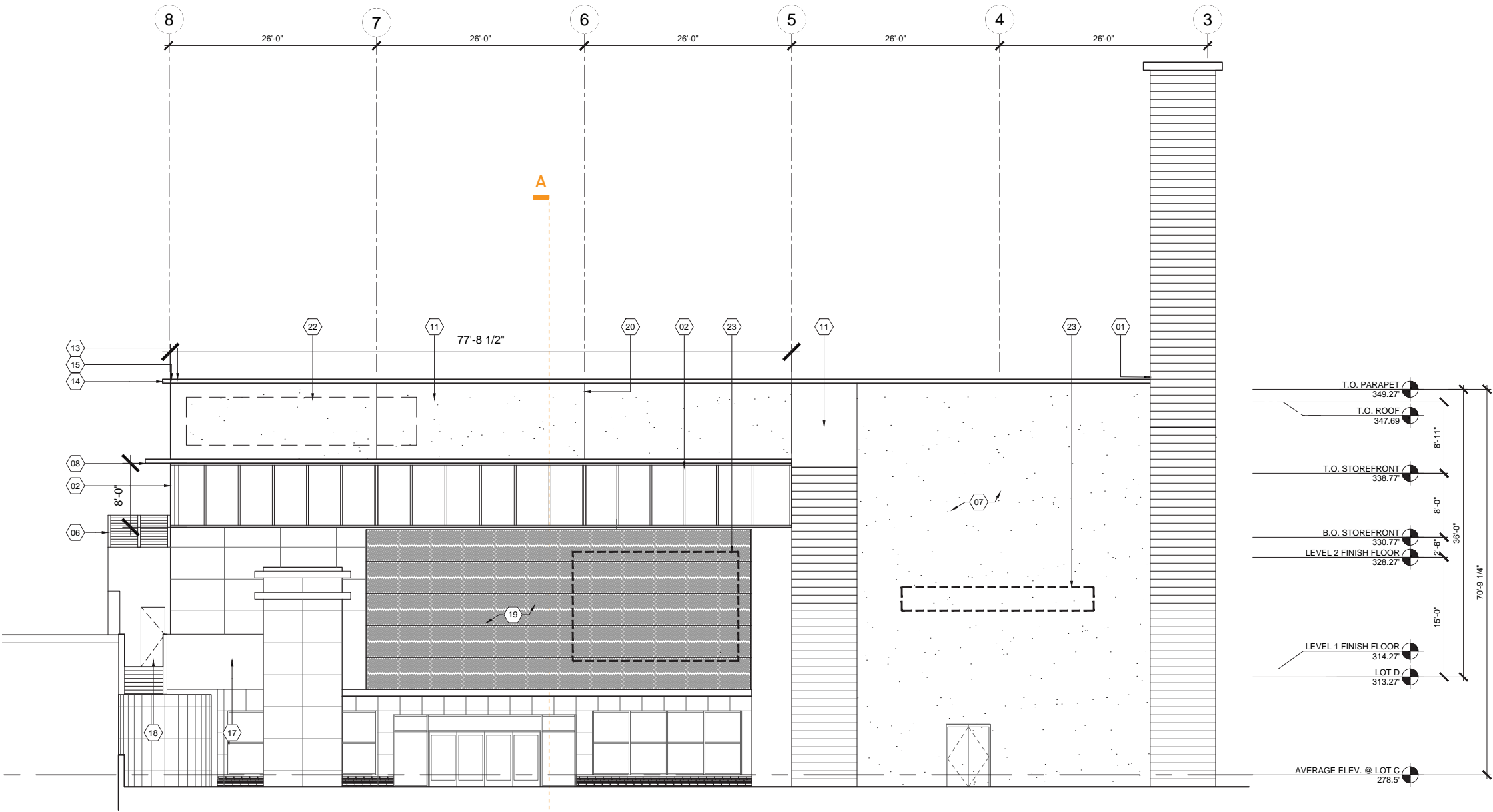
ELEVATION KEYNOTES

- 01 (E) TOWER
- 02 (N) ANODIZED ALUMINUM STOREFRONT
- 03 (N) CONCRETE RAMP
- 04 (N) CONCRETE STAIR
- 05 (N) HOLLOW STRUCTURAL SECTION CANOPY
- 06 (N) BRUSHED STAINLESS STEEL CABLE GUARDRAIL
- 07 (E) CONCRETE WITH NEW PAINT
- 08 (N) OVERHANG
- 09 3' x 8' ANODIZED ALUMINUM DOOR
- 10 FUTURE SIGNAGE
- 11 INTEGRAL COLOR EXTERIOR CEMENT PLASTER
- 12 (N) MECHANICAL LOUVER PAINTED
- 13 (N) TPO ROOF
- 14 GALVANIZED SHEET METAL COPING
- 15 (N) PAPRPET
- 16 CONCRETE WALL, PAINTED
- 17 NEW PAINT OVER EXISTING CONCRETE WALL
- 18 HOLLOW METAL DOOR
- 19 EXISTING SIGNAGE TO REMAIN
- 20 ANODIZED ALUMINUM REVEAL
- 21 (E) PEDESTRIAN BRIDGE
- 22 RELOCATED WALL SIGNAGE
- 23 EXISTING TENANT SIGNAGE
- 24 PROVIDE BIRD SAFE GLAZING TREATMENT WHERE REQUIRED BY PLANNING CODE SECTION 139, TYP.



ELEVATION KEYNOTES

- 01 (E) TOWER
- 02 (N) ANODIZED ALUMINUM STOREFRONT
- 03 (N) CONCRETE RAMP
- 04 (N) CONCRETE STAIR
- 05 (N) HOLLOW STRUCTURAL SECTION CANOPY
- 06 (N) BRUSHED STAINLESS STEEL CABLE GUARDRAIL
- 07 (E) CONCRETE WITH NEW PAINT
- 08 (N) OVERHANG
- 09 3' x 8' ANODIZED ALUMINUM DOOR
- 10 FUTURE SIGNAGE
- 11 INTEGRAL COLOR EXTERIOR CEMENT PLASTER
- 12 (N) MECHANICAL LOUVER PAINTED
- 13 (N) TPO ROOF
- 14 GALVANIZED SHEET METAL COPING
- 15 (N) PAPRPET
- 16 CONCRETE WALL, PAINTED
- 17 NEW PAINT OVER EXISTING CONCRETE WALL
- 18 HOLLOW METAL DOOR
- 19 EXISTING SIGNAGE TO REMAIN
- 20 ANODIZED ALUMINUM REVEAL
- 21 (E) PEDESTRIAN BRIDGE
- 22 RELOCATED WALL SIGNAGE
- 23 EXISTING TENANT SIGNAGE
- 24 PROVIDE BIRD SAFE GLAZING TREATMENT WHERE REQUIRED BY PLANNING CODE SECTION 139, TYP.

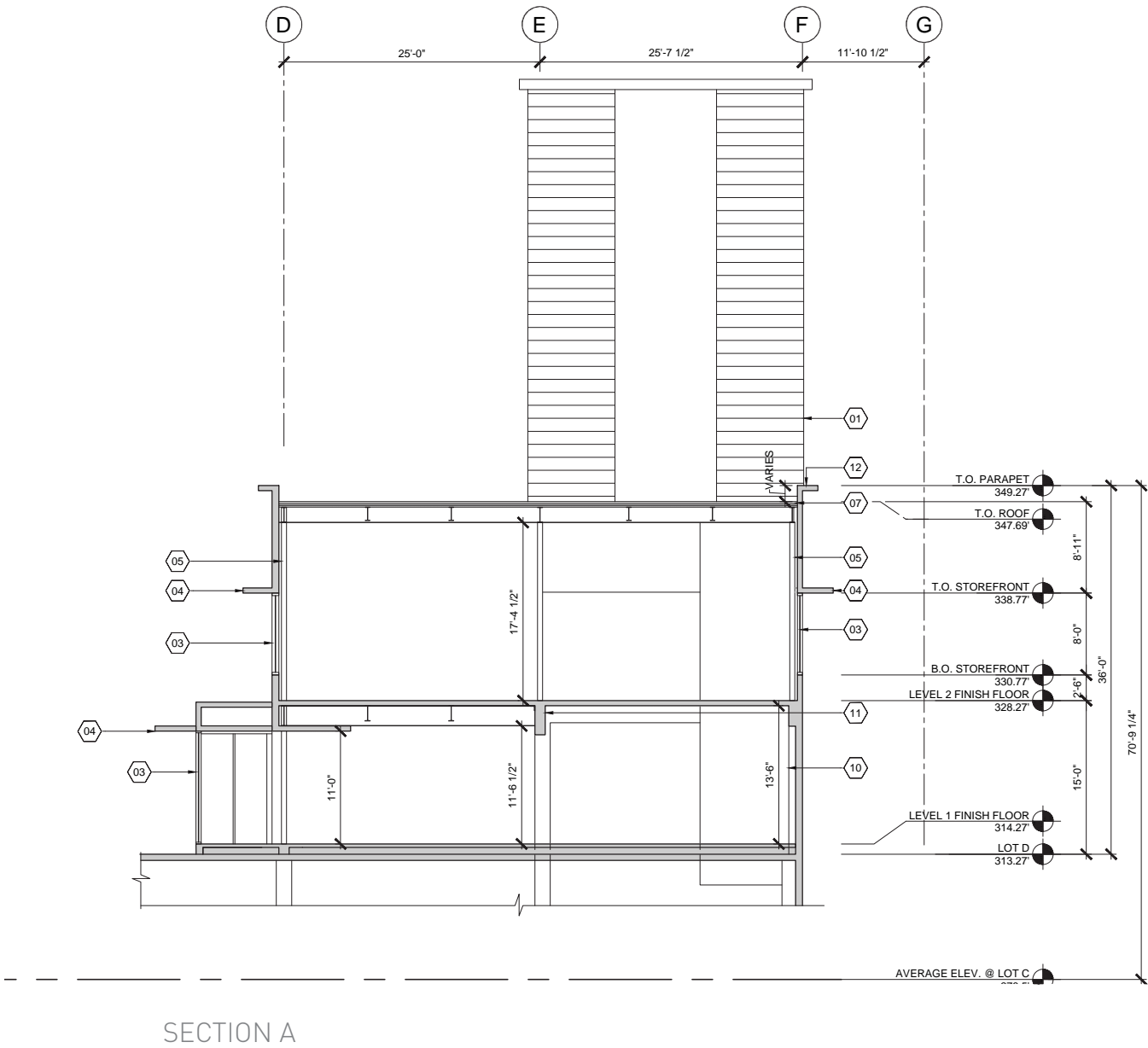


SOUTH ELEVATION



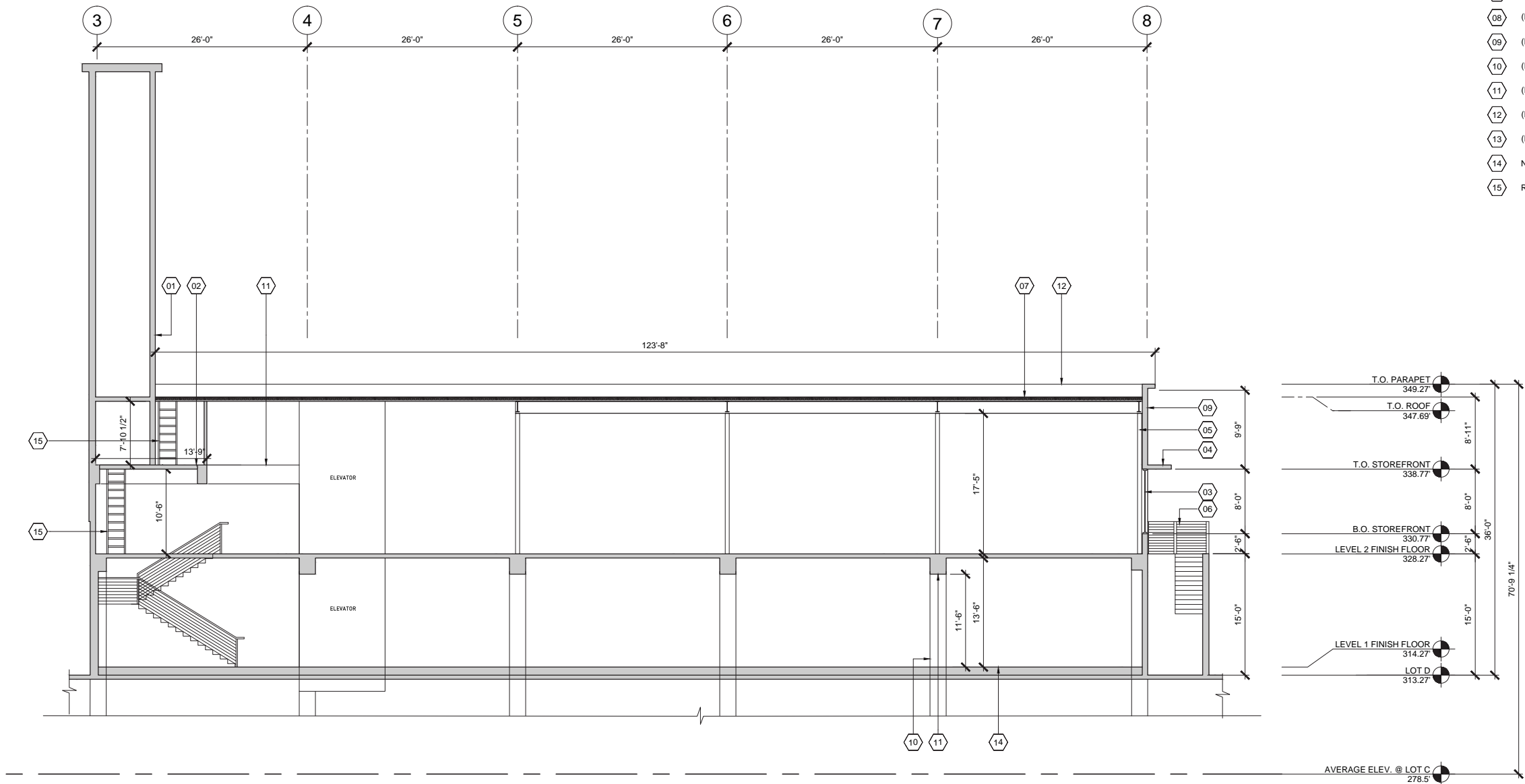
SECTION KEYNOTES

- 01 (E) TOWER
- 02 (E) MECHANICAL LEDGE
- 03 (N) STOREFRONT
- 04 (N) OVERHANG
- 05 (N) H.S.S. COLUMN
- 06 (N) BRUSHED STAINLESS STEEL CABLE GUARDRAIL
- 07 (N) T.P.O. ROOF
- 08 (N) COPING
- 09 (N) WALL
- 10 (E) CONCRETE COLUMN
- 11 (E) CONCRETE BEAM
- 12 (N) PARAPET
- 13 (N) ELEVATOR
- 14 NEW FLOOR
- 15 ROOF ACCESS

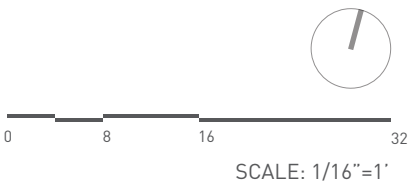
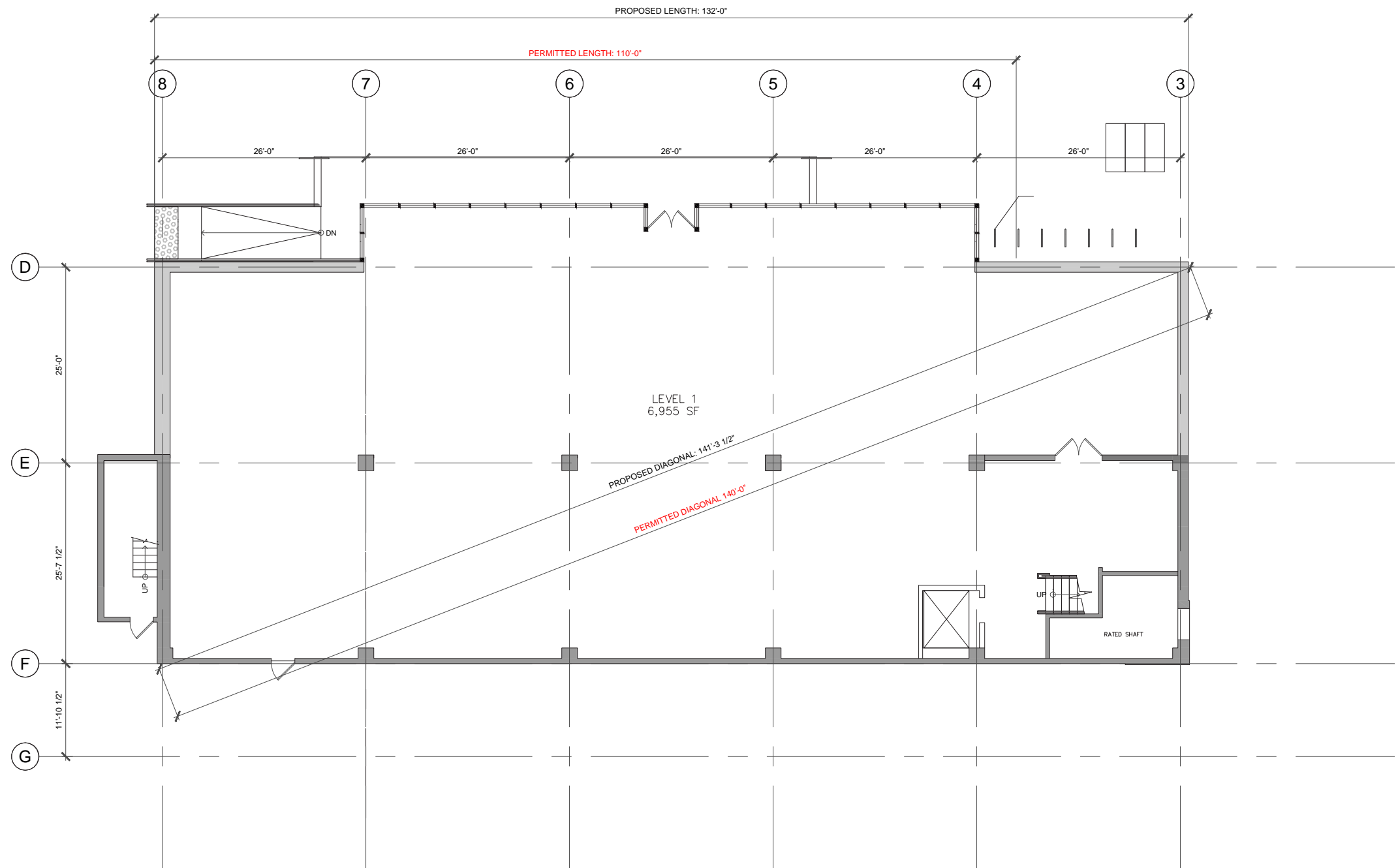


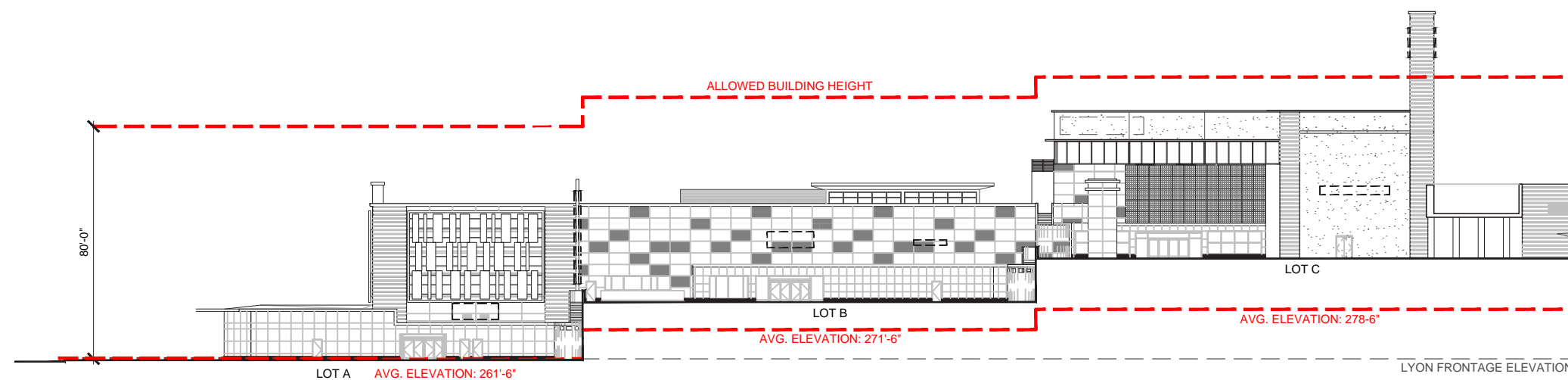
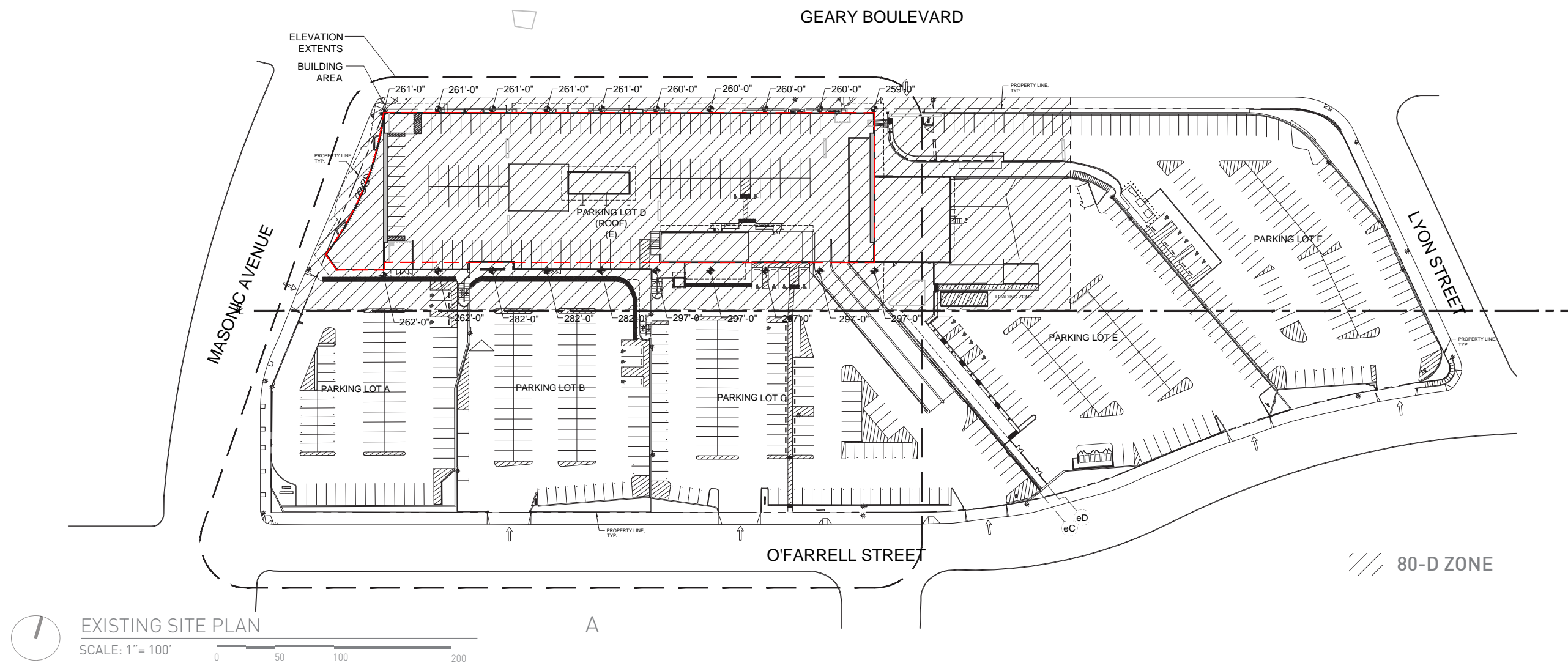
SECTION KEYNOTES

- 01 (E) TOWER
- 02 (E) MECHANICAL LEDGE
- 03 (N) STOREFRONT
- 04 (N) OVERHANG
- 05 (N) H.S.S. COLUMN
- 06 (N) BRUSHED STAINLESS STEEL CABLE GUARDRAIL
- 07 (N) T.P.O. ROOF
- 08 (N) COPING
- 09 (N) WALL
- 10 (E) CONCRETE COLUMN
- 11 (E) CONCRETE BEAM
- 12 (N) PARAPET
- 13 (N) ELEVATOR
- 14 NEW FLOOR
- 15 ROOF ACCESS



SCALE: 1/16"=1'





A

BUILDING HEIGHT DIAGRAM TAKEN AS UPWARD SLOPING LOT WITH STREET FRONTAGE MASONIC AVENUE

SCALE: 1" = 50'

0 25 50 100







VIEW 3 LOT C ENTRANCE
CITY CENTER CROW'S NEST
05.30.2017

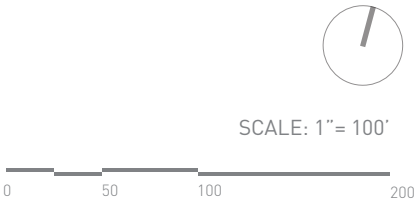
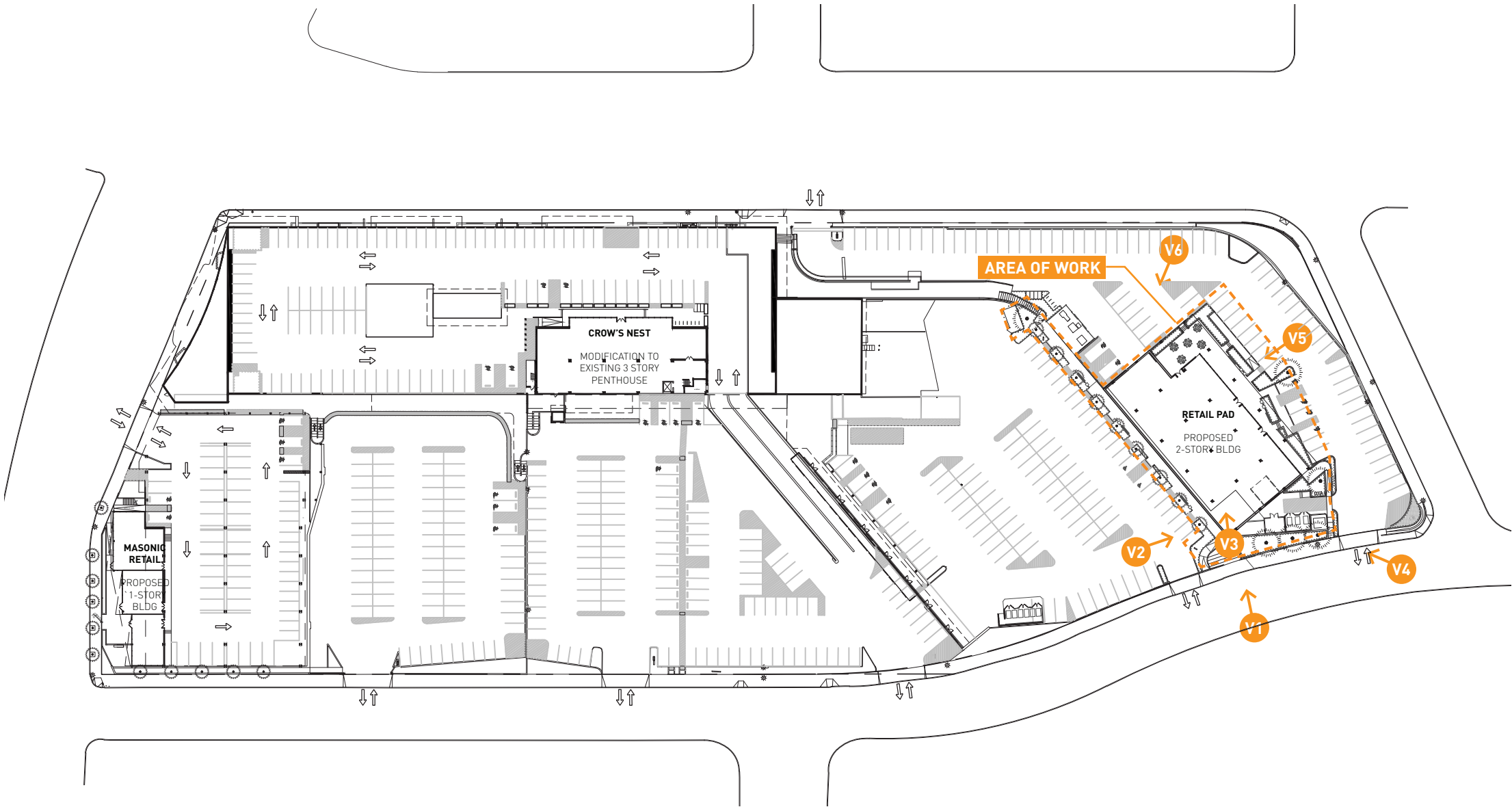
RETAIL PAD PROJECT INFO

PROJECT DESCRIPTION

NEW TWO STORY RETAIL BUILDING ON EXISTING PARKING LOT.

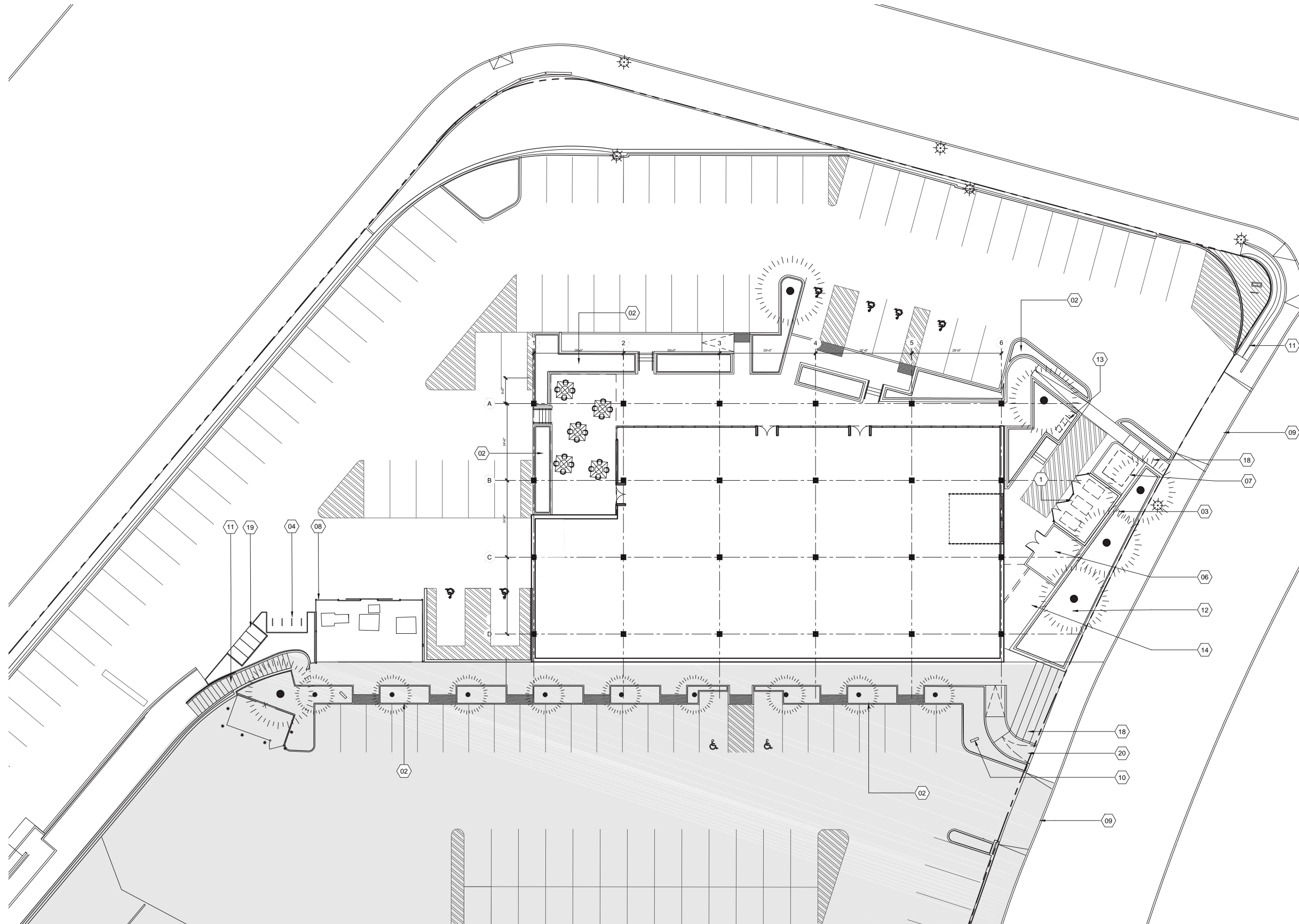
PROJECT SUMMARY

	EXISTING	PROPOSED	DIFFERENCE
LEVEL 1	0 SF	10,139 SF	+10,139 SF
LEVEL 2	0 SF	11,933 SF	+11,933 SF
TOTAL	0 SF	22,072 SF	+22,072 SF
CAR PARKING LOT E	96 STALLS	93 STALLS	-3 STALLS
CAR PARKING LOT F	145 STALLS	87 STALLS	-58 SPACES
BIKE PARKING LOT F	12 SPACES	28 SPACES	+16 SPACES



SITE PLAN KEYNOTES

- 01 COVERED TRASH ENCLOSURE
- 02 LANDSCAPED INFILTRATION PLANTER, TYP.
- 03 OUTDOOR SEATING AREA
- 04 (N) CLASS II BIKE PARKING
- 05 (N) PUBLIC RIGHT OF WAY
- 06 SCREENED UTILITIES
- 07 NEW TRANSFORMER
- 08 GAS METERS AND ELECTRICAL POINT OF CONNECTION
- 09 EXISTING SITE ENTRY
- 10 RELOCATED WAYFINDING GRAPHICS
- 11 (E) PUBLIC STAIR
- 12 GEARY CROSSING SIGNAGE
- 13 (N) BACKFLOW PREVENTER
- 14 (N) UTILITIES ROOM
- 15 FIXED SEATING
- 16 GUARDRAIL
- 17 (N) WAYFINDING GRAPHIC
- 18 (N) PUBLIC STAIR
- 19 (N) CLASS I BIKE PARKING
- 20 (N) PUBLIC ACCESSIBLE RAMP

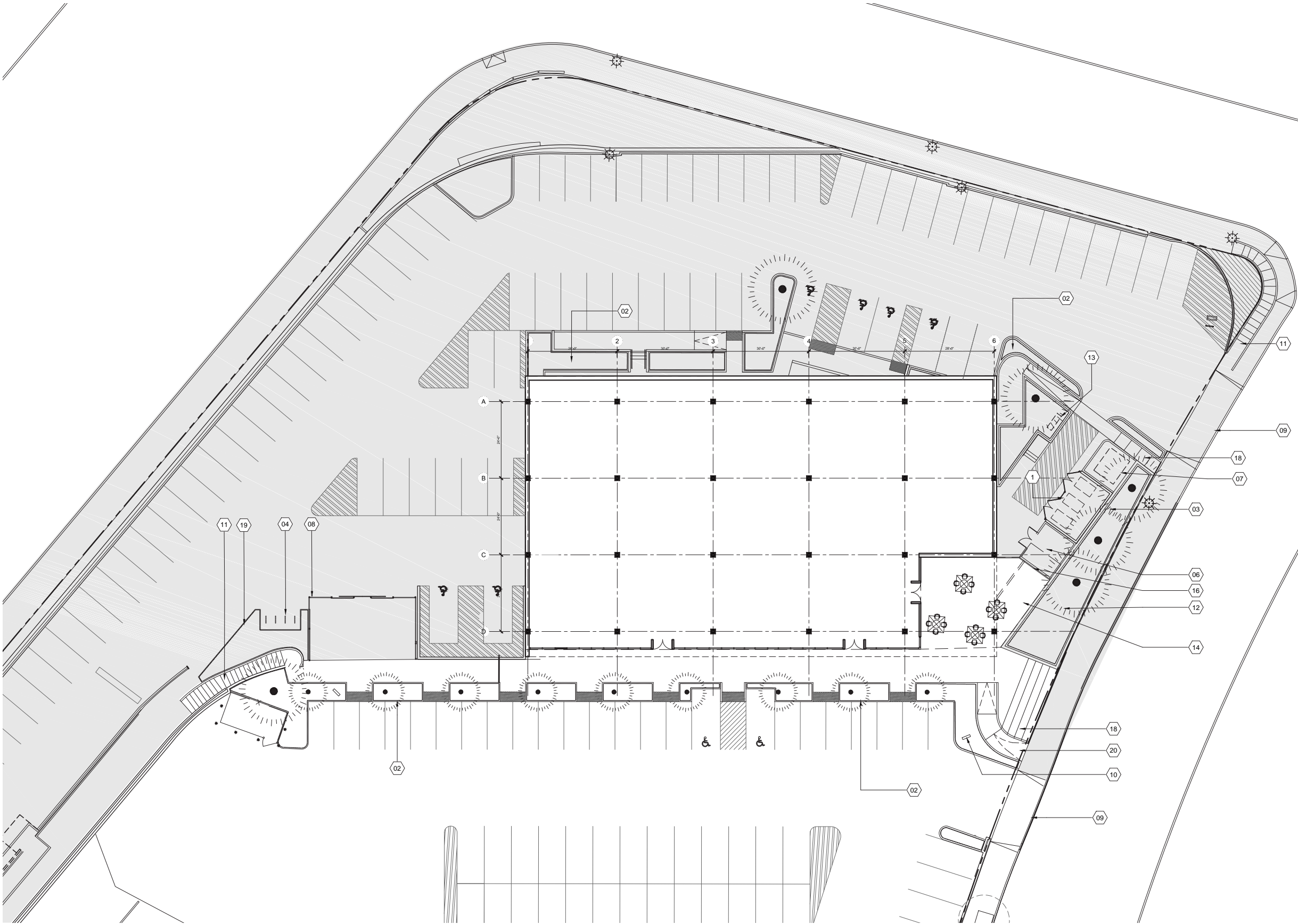


SCALE: 1/32"=1'



SITE PLAN KEYNOTES

- 01 COVERED TRASH ENCLOSURE
- 02 LANDSCAPED INFILTRATION PLANTER, TYP.
- 03 OUTDOOR SEATING AREA
- 04 (N) CLASS II BIKE PARKING
- 05 (N) PUBLIC RIGHT OF WAY
- 06 SCREENED UTILITIES
- 07 NEW TRANSFORMER
- 08 GAS METERS AND ELECTRICAL POINT OF CONNECTION
- 09 EXISTING SITE ENTRY
- 10 RELOCATED WAYFINDING GRAPHICS
- 11 (E) PUBLIC STAIR
- 12 GEARY CROSSING SIGNAGE
- 13 (N) BACKFLOW PREVENTER
- 14 (N) UTILITIES ROOM
- 15 FIXED SEATING
- 16 GUARDRAIL
- 17 (N) WAYFINDING GRAPHIC
- 18 (N) PUBLIC STAIR
- 19 (N) CLASS I BIKE PARKING
- 20 (N) PUBLIC ACCESSIBLE RAMP

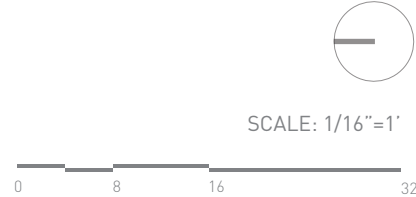
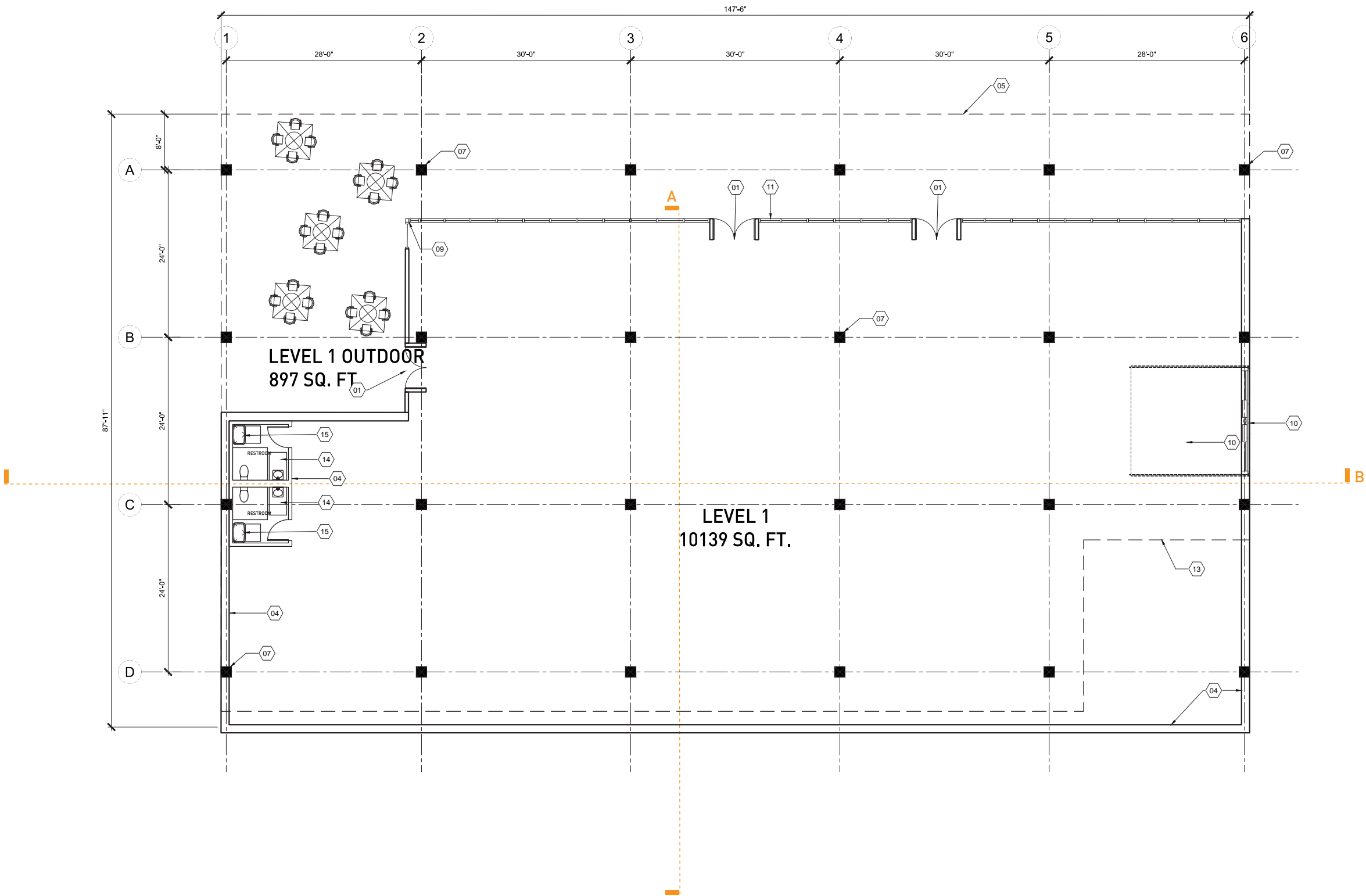


SCALE: 1/32"=1'



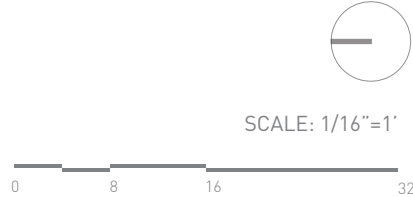
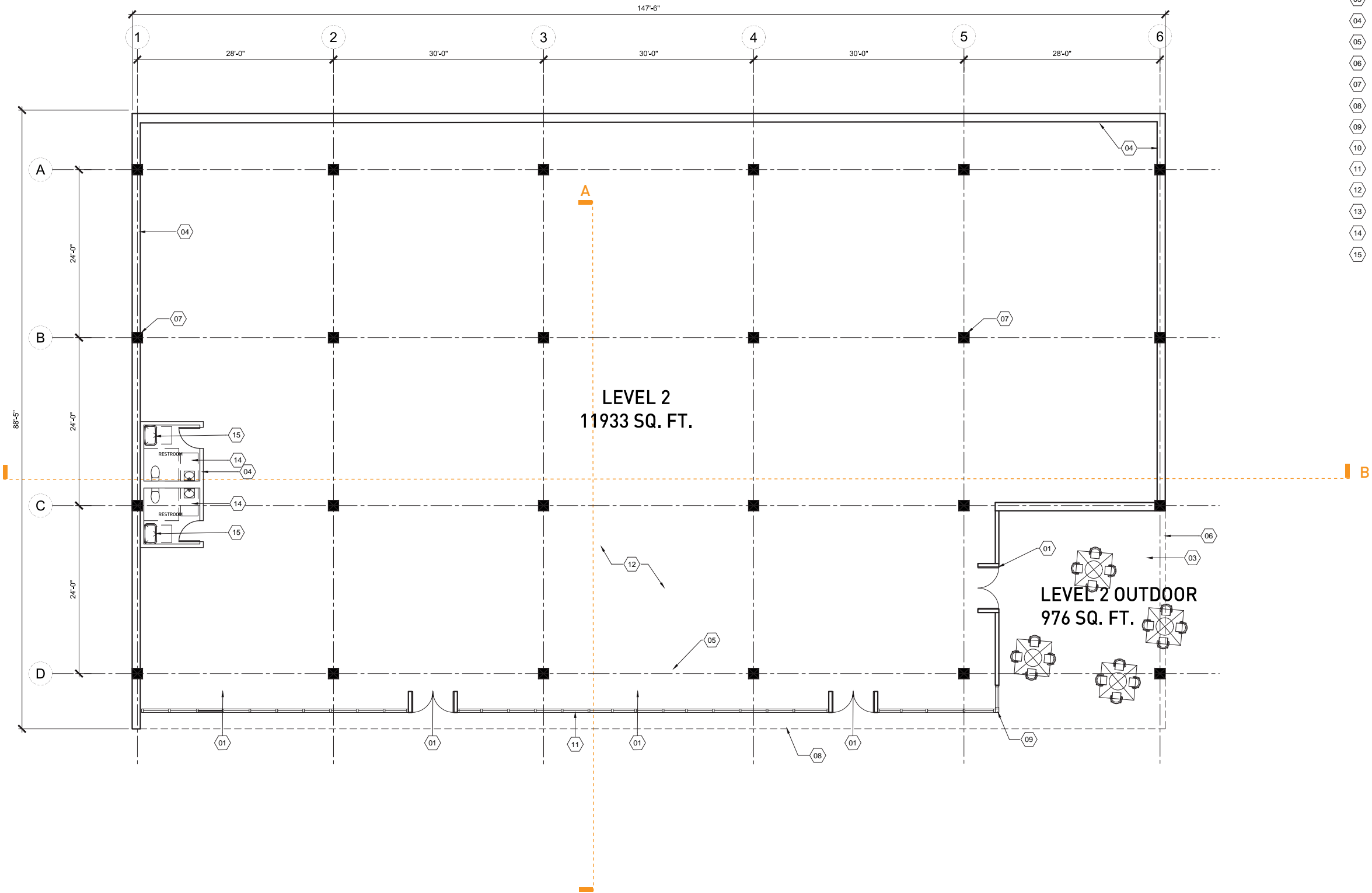
FLOOR PLAN KEYNOTES

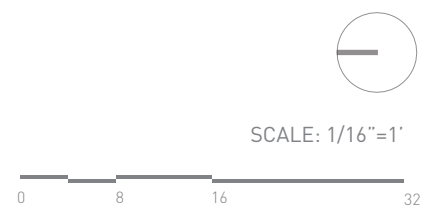
- 01 6' x 8' ALUMINUM DOOR AND FRAME
- 02 NOT USED
- 03 PATIO W/ GLASS GUARDRAIL
- 04 STUD WALL TYP.
- 05 OVERHANG OF FLOOR ABOVE
- 06 TRELLIS ABOVE
- 07 COLUMN, TYP.
- 08 SIGNAGE CANOPY
- 09 HSS POST
- 10 20 FT ROLL UP DOOR
- 11 ALUMINUM STOREFRONT
- 12 6" CONCRETE SLAB ON GRADE
- 13 LINE OF FLOOR ABOVE
- 14 ACCESSIBLE RESTROOM
- 15 ACCESSIBLE BABY CHANGING STATION



FLOOR PLAN KEYNOTES

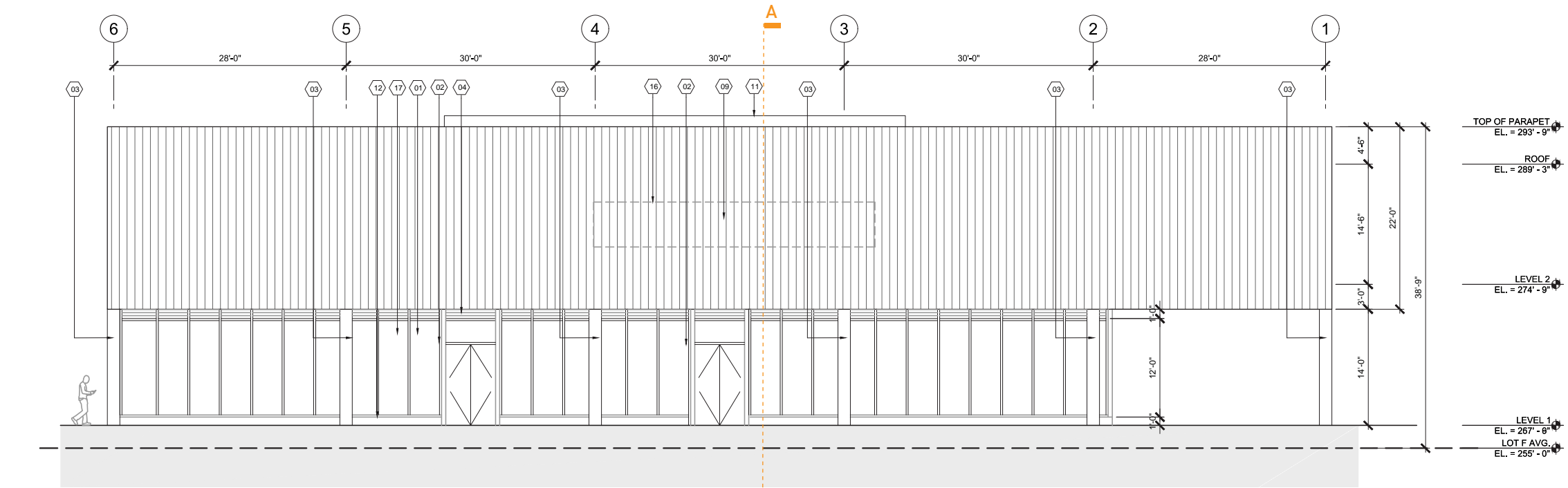
- 01 6' x 8' ALUMINUM DOOR AND FRAME
- 02 NOT USED
- 03 PATIO W/ GLASS GUARDRAIL
- 04 STUD WALL TYP.
- 05 OVERHANG OF FLOOR ABOVE
- 06 TRELLIS ABOVE
- 07 COLUMN, TYP.
- 08 SIGNAGE CANOPY
- 09 HSS POST
- 10 20 FT ROLL UP DOOR
- 11 ALUMINUM STOREFRONT
- 12 6" CONCRETE SLAB ON GRADE
- 13 LINE OF FLOOR ABOVE
- 14 ACCESSIBLE RESTROOM
- 15 ACCESSIBLE BABY CHANGING STATION



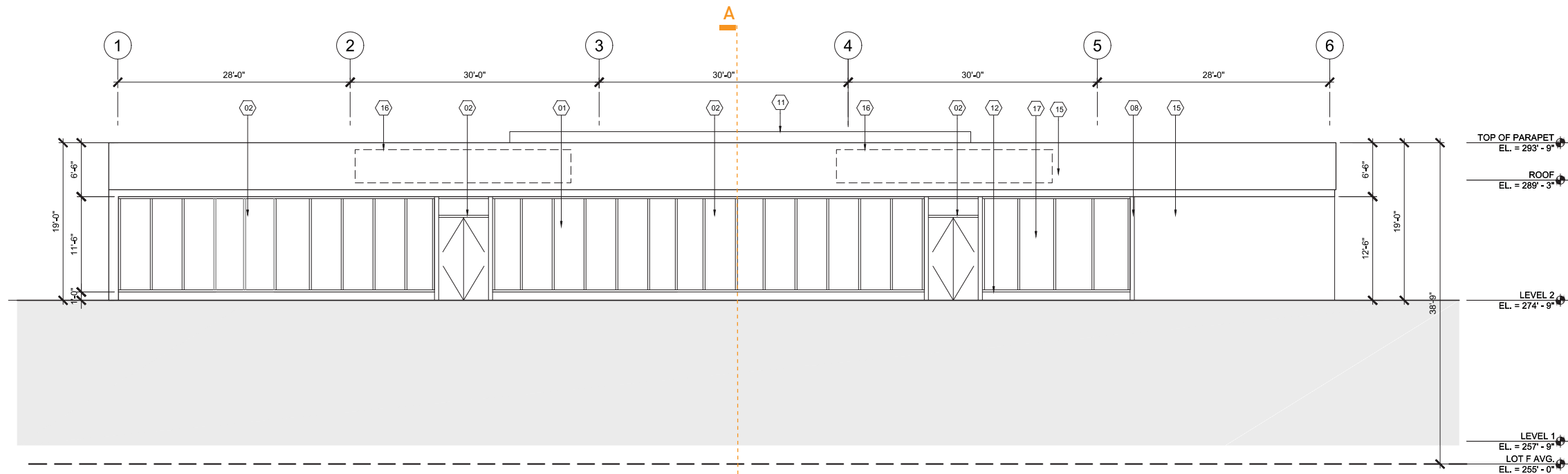


ELEVATION KEYNOTES

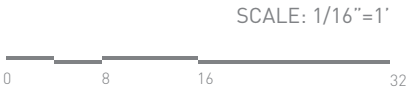
- 01 ANODIZED ALUMINUM STOREFRONT W/ CLEAR INSULATED GLAZING UNITS
- 02 RECESSED ENTRY; 6'-0" x 8'-0" ANODIZED ALUMINUM DOOR AND FRAME
- 03 COLUMN PER STRUCTURAL
- 04 MECHANICAL LOUVERS
- 05 EIFS APPLIED IN VARYING DEPTHS
- 06 CONCRETE WALKWAY
- 07 (E) RETAINING WALL TO REMAIN
- 08 HSS POST
- 09 ALUMINUM SLAT SCREEN OVER EIFS FACADE; ONE SIDE OF SLATS TO BE COLOI FOR VARIED LOOK
- 10 NOT USED
- 11 MECHANICAL ROOF SCREEN BEYOND
- 12 12" CONCRETE CURB, TYP. AT STOREFRONT
- 13 ROLL UP DOOR
- 14 LANDSCAPE BUFFER WITH 6" CURB
- 15 EIFS
- 16 FUTURE SIGNAGE
- 17 PROVIDE BIRD SAFE GLAZING TREATMENT WHERE REQUIRED BY PLANNING CODE SECTION 139, TYP.

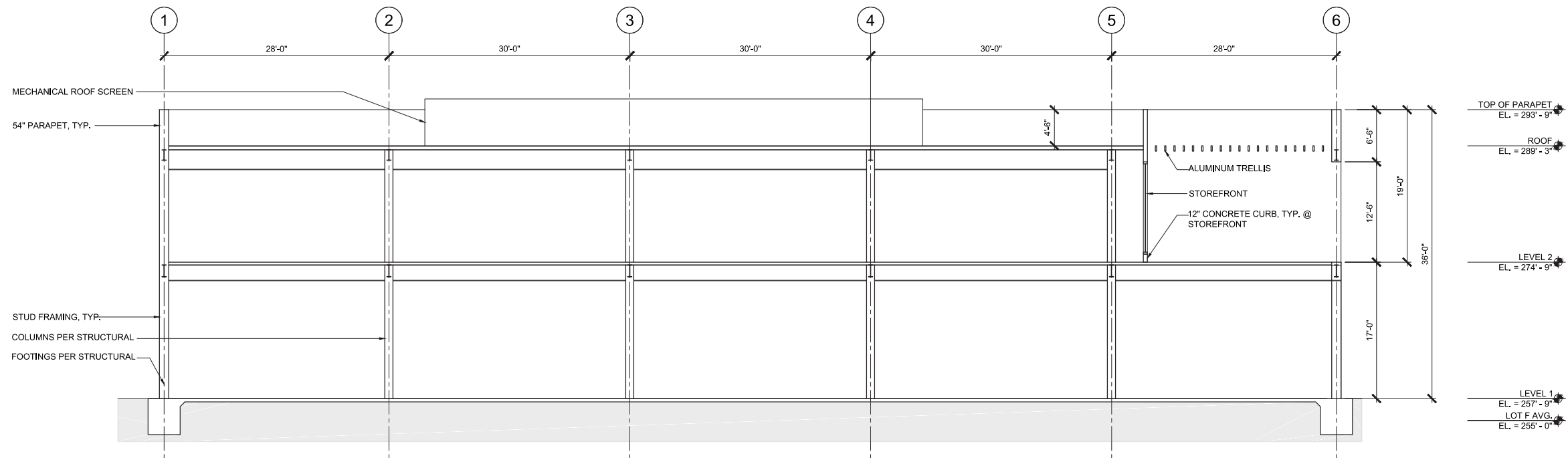
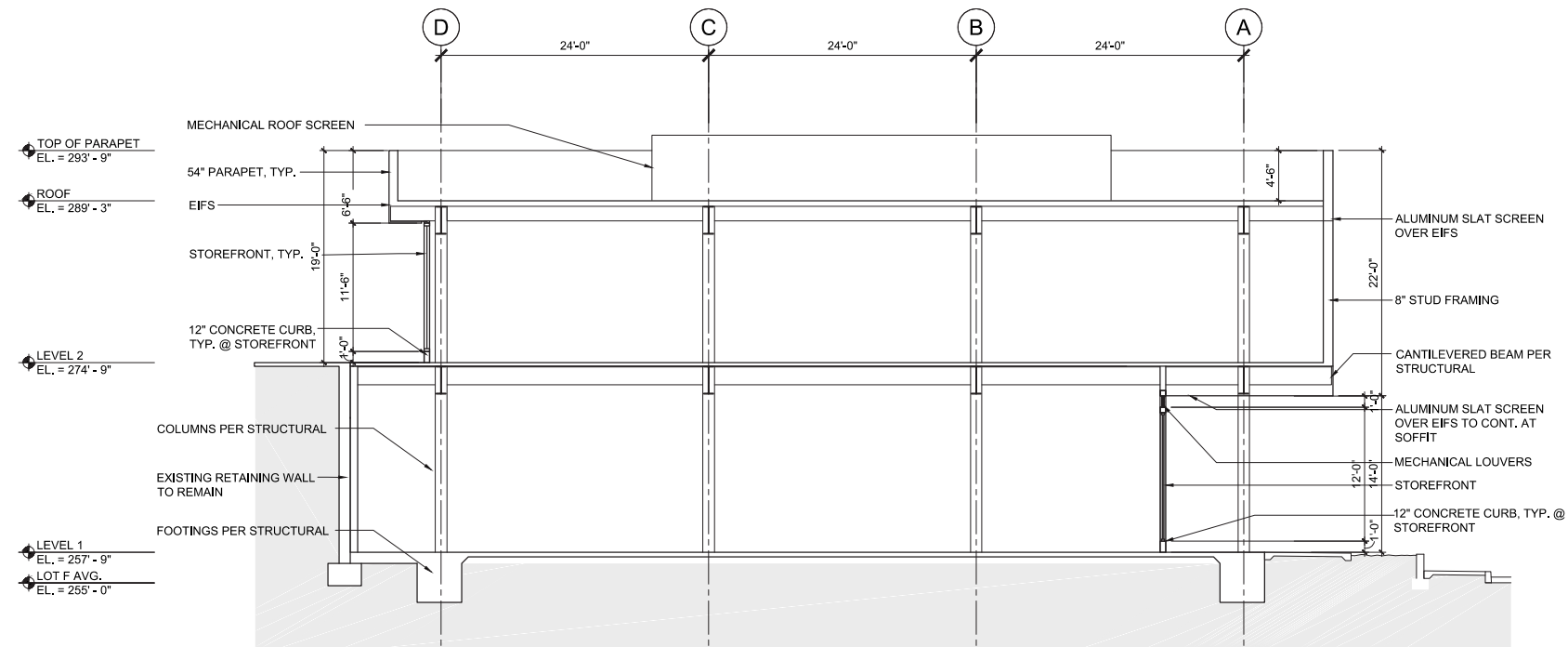


EAST ELEVATION



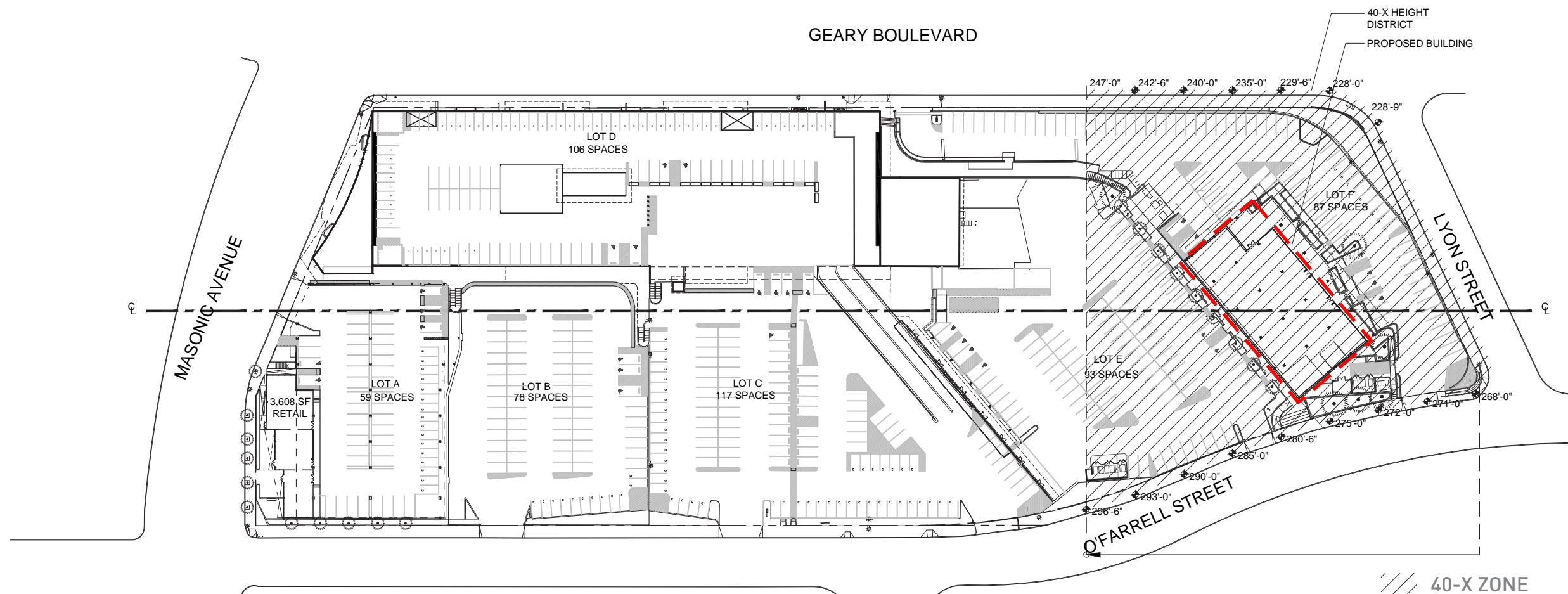
WEST ELEVATION



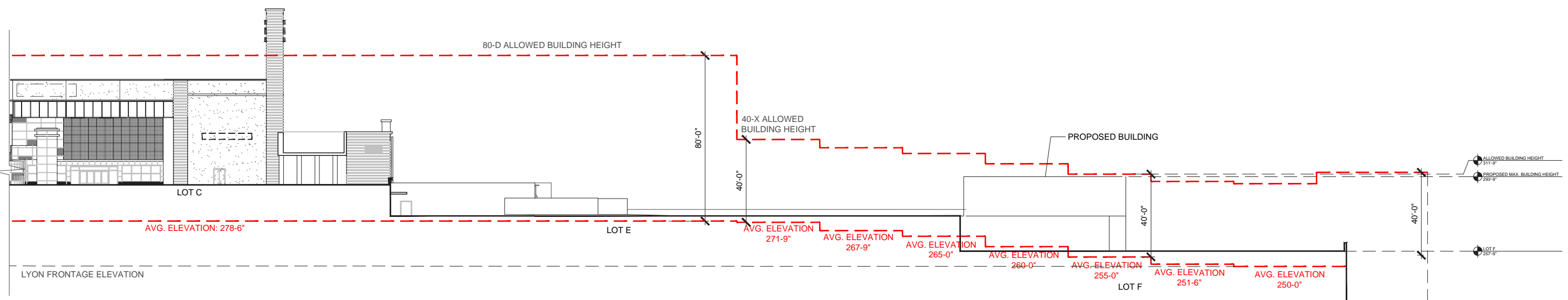


SCALE: 1/16"=1'





I SITE PLAN
SCALE: 1" = 100'



A BUILDING HEIGHT DIAGRAM TAKEN AS UPWARD SLOPING LOT WITH STREET FRONTAGE LYON STREET
SCALE: 1" = 50'



VIEW 1 GEARY BLVD LEVEL 2 ENTRANCE
CITY CENTER RETAIL PAD
05.30.2017



VIEW 2 LOT E PARKING LOT @ LEVEL 2
CITY CENTER RETAIL PAD
05.30.2017



VIEW 3 LEVEL 2 OUTDOOR SPACE
CITY CENTER RETAIL PAD
05.30.2017



VIEW 4 GEARY BLVD / LOT F ENTRANCE @ LEVEL 1
CITY CENTER RETAIL PAD
05.30.2017



VIEW 5 LEVEL 1 FROM LOT F
CITY CENTER RETAIL PAD
05.30.2017



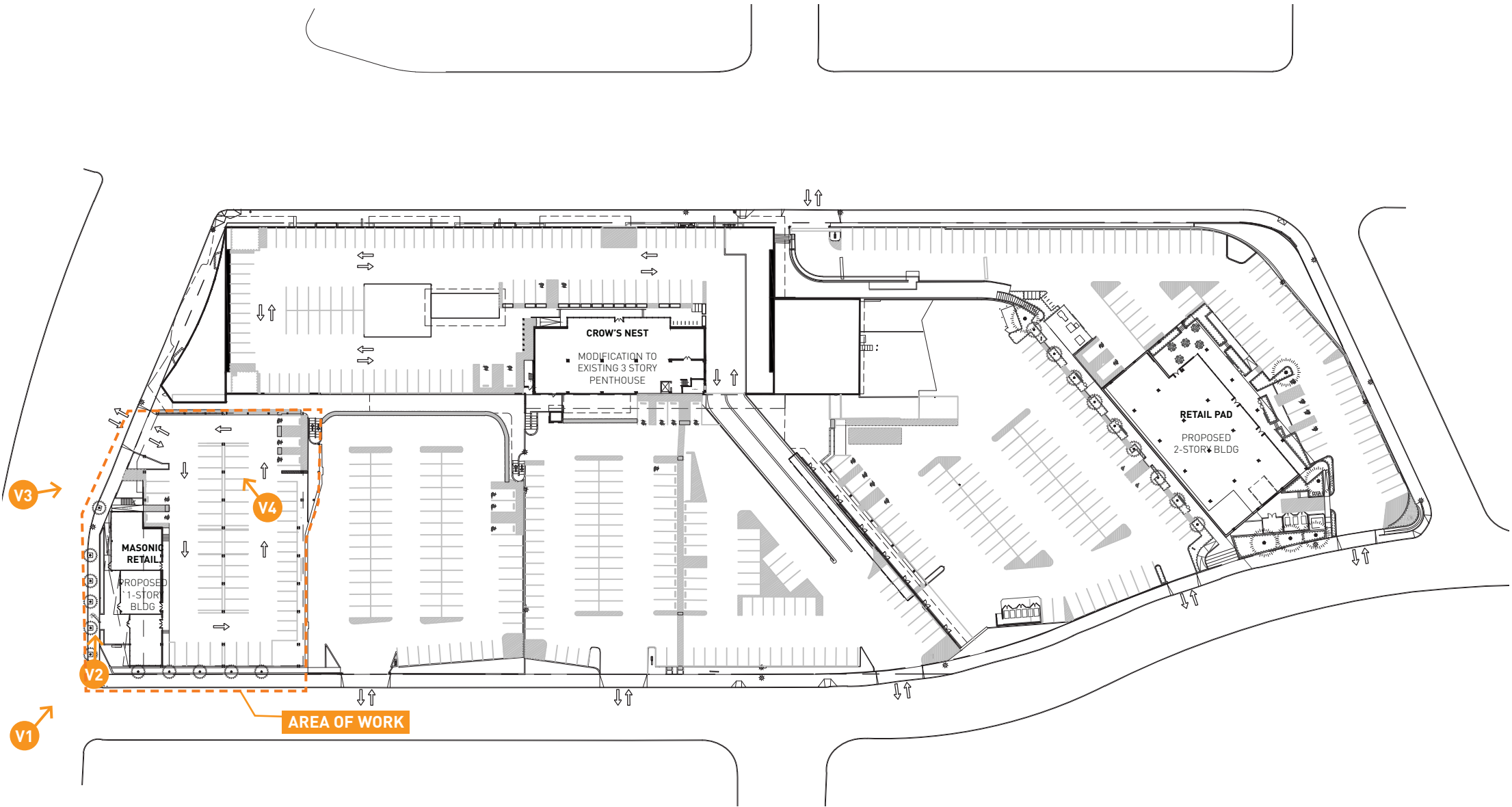
MASONIC RETAIL PROJECT INFO

PROJECT DESCRIPTION

NEW RETAIL AND NEW PARKING GARAGE / DECK ON TOP OF EXISTING PARKING LOT.

PROJECT SUMMARY

	EXISTING	PROPOSED	DIFFERENCE
LEVEL 1 RETAIL	0 SF	3,608 SF	+3,608 SF
LEVEL 2 RETAIL	0 SF	0 SF	0 SF
TOTAL	0 SF	3,608 SF	+3,608 SF
LOT A, LEVEL 1 PARKING LOT	27,456 SF	21,491 SF	-5,965 SF
LOT A, LEVEL 2 PARKING LOT	0 SF	22,490 SF	+22,490 SF
TOTAL	27,456 SF	43,981 SF	+16,525 SF
CAR PARKING (LOT A)	78 STALLS	59 STALLS	-19 STALLS
CAR PARKING (LOT B)	81 STALLS	171 STALLS	+90 STALLS
TOTAL	159 STALLS	233 STALLS	+74 STALLS
BIKE PARKING (LOT A+B)	10 SPACES	22 SPACES	+12 SPACES



SCALE: 1" = 100'



FLOOR PLAN KEYNOTES

- 01 (E) STAIR TO LOT B
- 02 (N) CLEAR ANODIZED STOREFRONT
- 03 (E) LANDSCAPING
- 04 (N) PAINTED CMU LANDSCAPING AREA; PROVIDE WITH SEPARATELY METERED IRRIGATION AND PROVIDE CONNECTION TO STORM DRAIN.
- 05 (N) STEEL PAN STAIR W/ PRECAST CONCRETE TREADS AND PERFORATED METAL RISERS
- 06 (N) POTENTIAL TENANT DEMISING WALL
- 07 (E) RETAINING WALL
- 08 PARKING DECK/SOFFIT ABOVE
- 09 (E) PARKING ENTRY / EXIT
- 10 (N) ACCESSIBLE PARKING
- 11 (N) SHEAR WALL OR STEEL CROSS BRACING PER STRUCTURAL; PROVIDE MINIMUM 3-HR RATING
- 12 RELOCATE TRUNCATED DOME SURFACE TO ACCOMMODATE NEW COLUMN LAYOUT
- 13 (E) PYLON SIGN TO REMAIN
- 14 PROVIDE NEW GARAGE LIGHTING @ 28'-0" O.C. PER MANUFACTURER RECOMMENDATIONS. PROVIDE WITH EMERGENCY BACKUP AS REQ'D BY CODE. AMERLUX CHAPERONE G300 LED 48W
- 15 ACCESSIBLE RAMP UNDER 5% SLOPE WITHOUT HANDRAILS
- 16 METAL GUARDRAIL
- 17 (E) PARKING LOT SURFACE TO REMAIN; REPAIR/PATCH AS NEEDED
- 18 CONCRETE STAIRS W/ HANDRAIL
- 19 (N) TRUNCATED DOMES
- 20 PROVIDE 6" SQUARE RECESSED LED LIGHTING @ 48" O.C. @ STOREFRONT SOFFIT
- 21 PROVIDE ACCOMMODATIONS FOR 10 FUTURE E.V. CHARGING STATIONS; REFER TO ELECTRICAL
- 22 EXISTING BOLLARDS TO REMAIN
- 23 NEW PAINTED METAL SCREEN WALL W/ WEATHER RESISTANT FINISH; ATTACH TO STRUCTURE
- 24 PROVIDE MINIMUM 2-HR RATING AT FLOOR-CEILING ASSEMBLY
- 25 COLUMNS PER STRUCTURAL; PROVIDE MINIMUM 3-HR RATED ASSEMBLY AT ALL PRIMARY STRUCTURAL ELEMENTS
- 26 (N) STAIR ABOVE FROM LOT B TO LOT C
- 27 (N) ELEVATED CONCRETE WALK

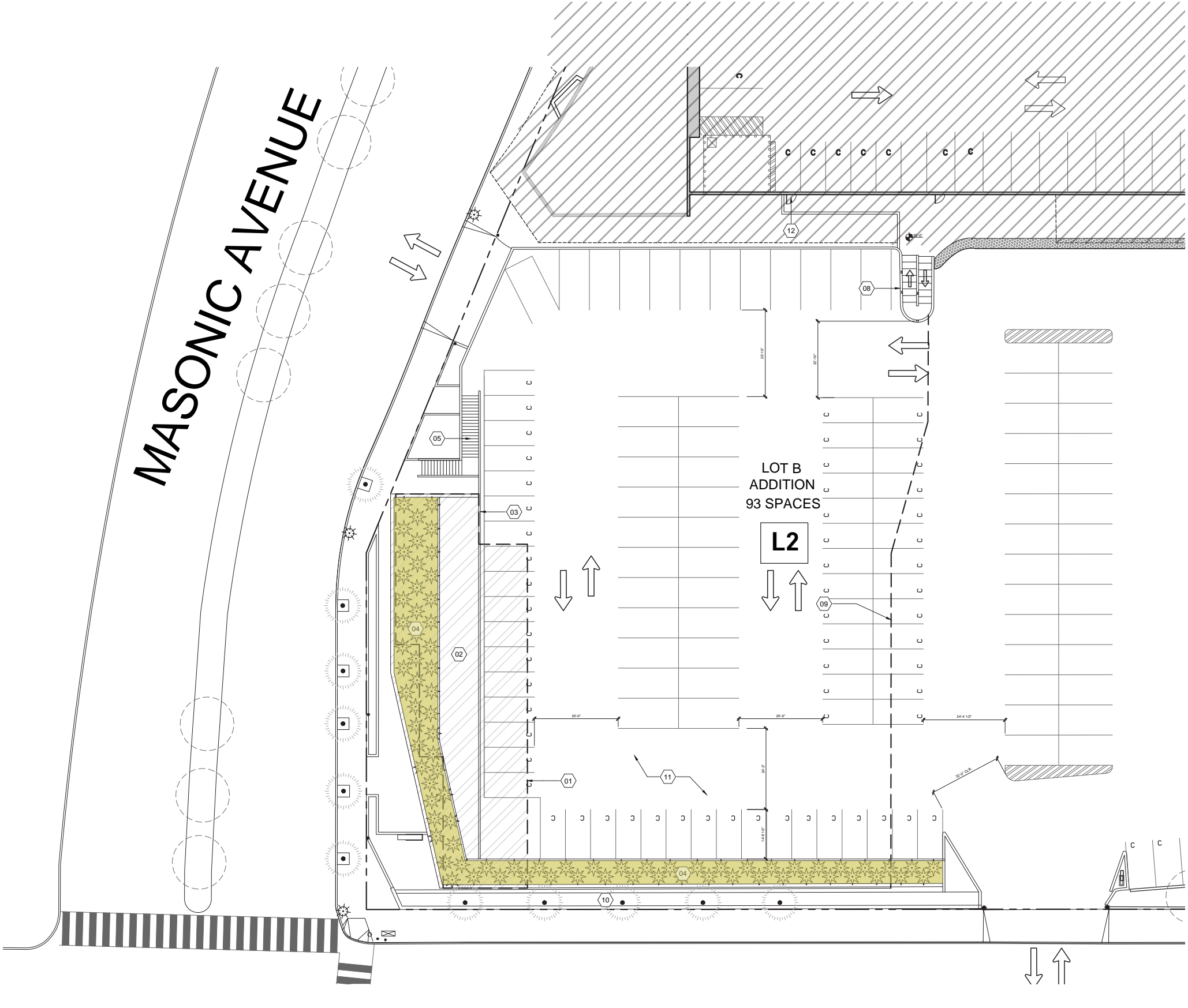
PLAN LEGEND

- PROPERTY LINE
- NOT A PART
- TRUNCATED DOMES
- SHEAR WALL (PER STRUCTURAL)
- POTENTIAL INTERIOR WALL
- EXISTING LANDSCAPING TO REMAIN
- NEW LANDSCAPING



SCALE: 1/32"=1'

0 16 32 64



FLOOR PLAN KEYNOTES

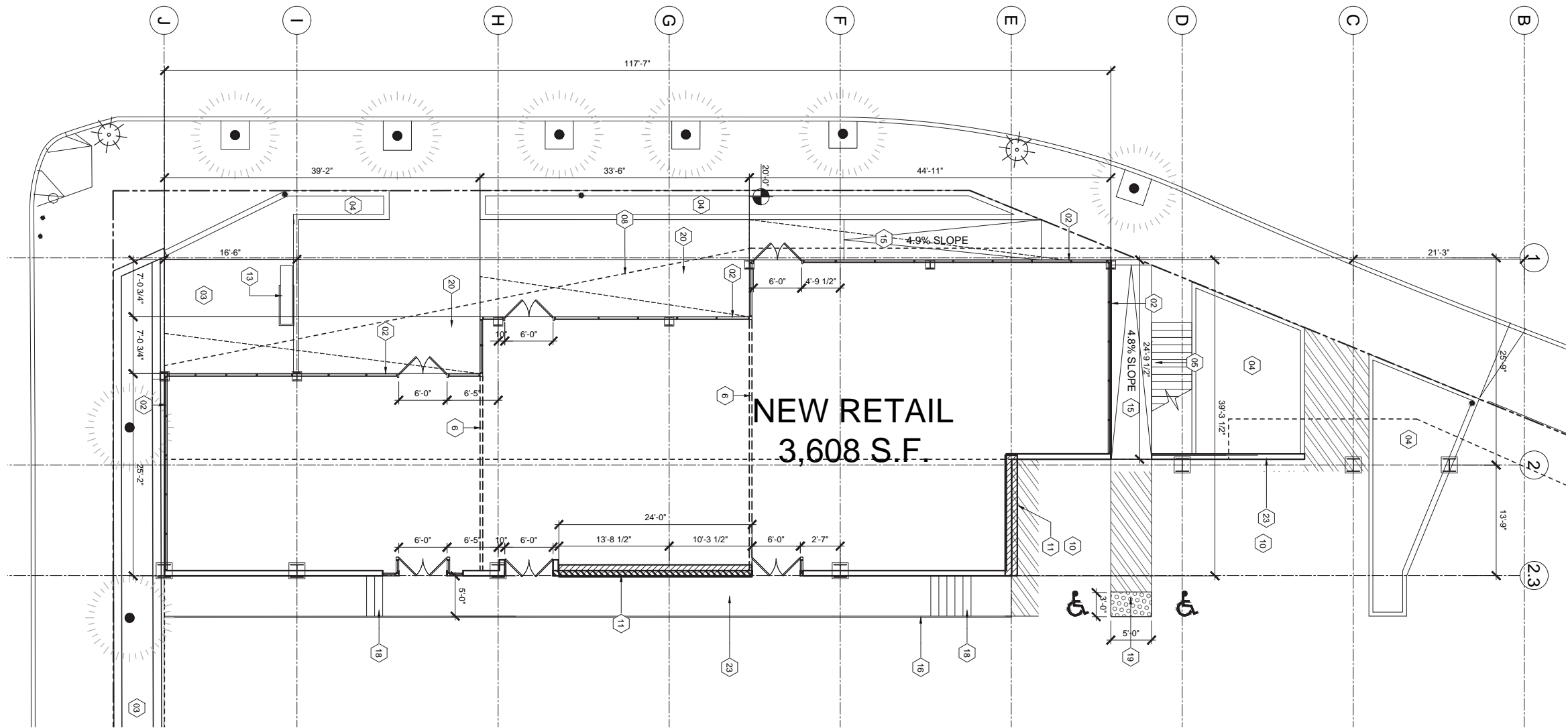
- 01 (N) RETAIL BELOW
- 02 ZONE FOR NEW 6" RAISED CONCRETE EQUIPMENT PADS TO ACCOMMODATE FUTURE EQUIPMENT AND MECHANICAL
- 03 (N) 48" CMU SEPARATION WALL W/ 16/30 STUCCO FINISH
- 04 (N) CMU LANDSCAPING AREA; PROVIDE WITH SEPARATELY METERED IRRIGATION AND HOT MOP WATERPROOFING; PROVIDE 2" DIA. DRAIN AND CONNECTION TO STORM DRAIN
- 05 (N) STAIR
- 06 NOT USED
- 07 NOT USED
- 08 (E) STAIR FROM LOT A TO LOT B
- 09 (E) RETAINING WALL BELOW; REMOVE EXISTING PARAPET WALL TO EXTEND PARKING DECK
- 10 (E) LANDSCAPING BELOW TO REMAIN
- 11 PROVIDE (N) SITE LIGHTING; TYPE AND SPACING TO MATCH EXISTING
- 12 TARGET ENTRANCE

PLAN LEGEND

- PROPERTY LINE
- [Hatched Box] NOT A PART
- [Diagonal Line Box] AREA OF DEPRESSED SLAB; 4" TOPPING SLAB O/ WATERPROOFING
- [Dotted Box] TRUNCATED DOMES
- [Hatched Box] SHEAR WALL (PER STRUCTURAL)
- [Double Line Box] POTENTIAL INTERIOR WALL
- [Green Box] EXISTING LANDSCAPING TO REMAIN
- [Yellow Box] NEW LANDSCAPING

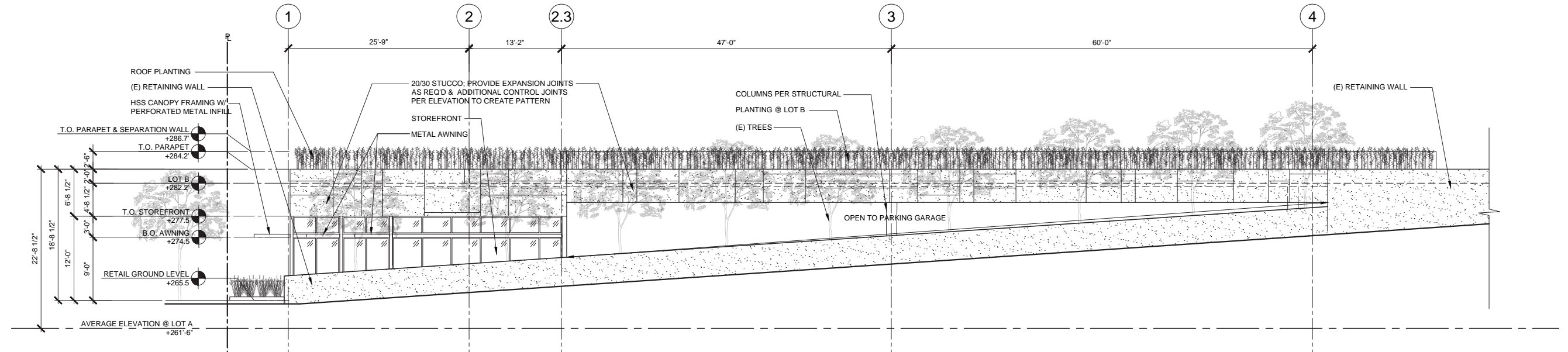


- 01 (E) STAIR TO LOT B
- 02 (N) CLEAR ANODIZED STOREFRONT
- 03 (E) LANDSCAPING
- 04 (N) PAINTED CMU LANDSCAPING AREA; PROVIDE WITH SEPARATELY METERED IRRIGATION AND PROVIDE CONNECTION TO STORM DRAIN.
- 05 (N) STEEL PAN STAIR W/ PRECAST CONCRETE TREADS AND PERFORATED METAL RISERS
- 06 (N) POTENTIAL TENANT DEMISING WALL
- 07 (E) RETAINING WALL
- 08 PARKING DECK/SOFFIT ABOVE
- 09 (E) PARKING ENTRY / EXIT
- 10 (N) ACCESSIBLE PARKING
- 11 (N) SHEAR WALL OR STEEL CROSS BRACING PER STRUCTURAL; PROVIDE MINIMUM 3-HR RATING
- 12 RELOCATE TRUNCATED DOME SURFACE TO ACCOMMODATE NEW COLUMN LAYOUT
- 13 (E) PYLON SIGN TO REMAIN
- 14 PROVIDE NEW GARAGE LIGHTING @ 28'-0" O.C. PER MANUFACTURER RECOMMENDATIONS. PROVIDE WITH EMERGENCY BACKUP AS REQ'D BY CODE. AMERLUX CHAPERONE G300 LED 48W
- 15 ACCESSIBLE RAMP UNDER 5% SLOPE WITHOUT HANDRAILS
- 16 METAL GUARDRAIL
- 17 (E) PARKING LOT SURFACE TO REMAIN; REPAIR/PATCH AS NEEDED
- 18 CONCRETE STAIRS W/ HANDRAIL
- 19 (N) TRUNCATED DOMES
- 20 PROVIDE 6" SQUARE RECESSED LED LIGHTING @ 48" O.C. @ STOREFRONT SOFFIT
- 21 PROVIDE ACCOMMODATIONS FOR 10 FUTURE E.V. CHARGING STATIONS; REFER TO ELECTRICAL
- 22 EXISTING BOLLARDS TO REMAIN
- 23 NEW PAINTED METAL SCREEN WALL W/ WEATHER RESISTANT FINISH; ATTACH TO STRUCTURE
- 24 PROVIDE MINIMUM 2-HR RATING AT FLOOR-CEILING ASSEMBLY
- 25 COLUMNS PER STRUCTURAL; PROVIDE MINIMUM 3-HR RATED ASSEMBLY AT ALL PRIMARY STRUCTURAL ELEMENTS
- 26 (N) STAIR ABOVE FROM LOT B TO LOT C
- 27 (N) ELEVATED CONCRETE WALK

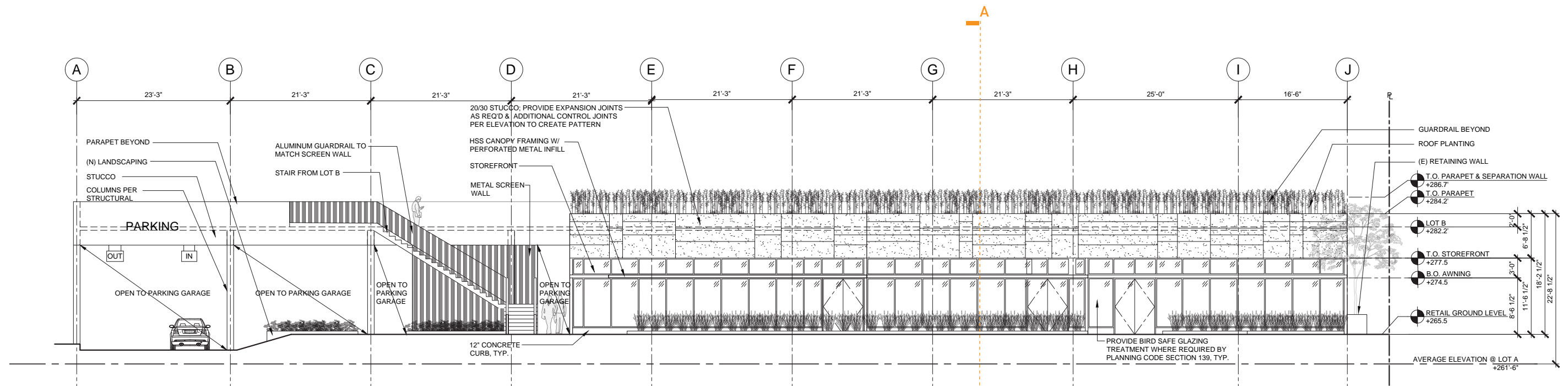


SCALE: 1/16" = 1'

0 8 16 32



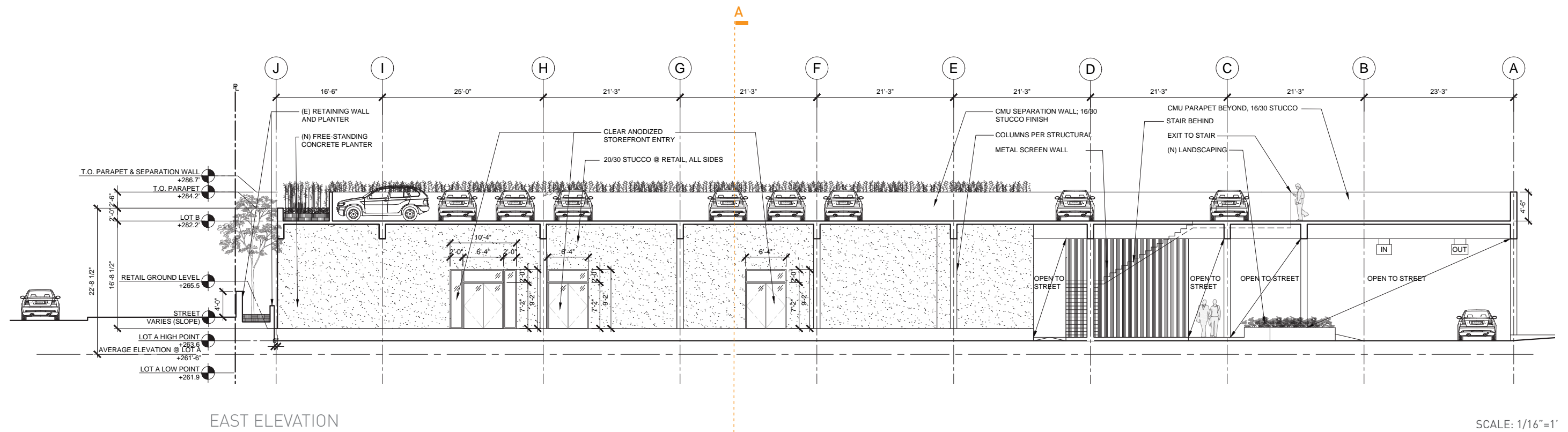
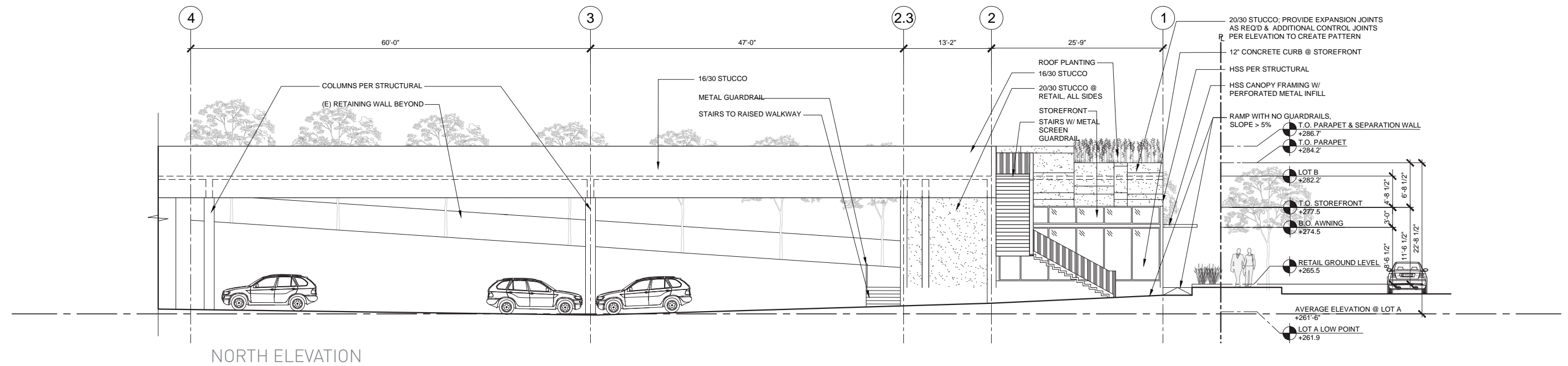
SOUTH ELEVATION



WEST ELEVATION

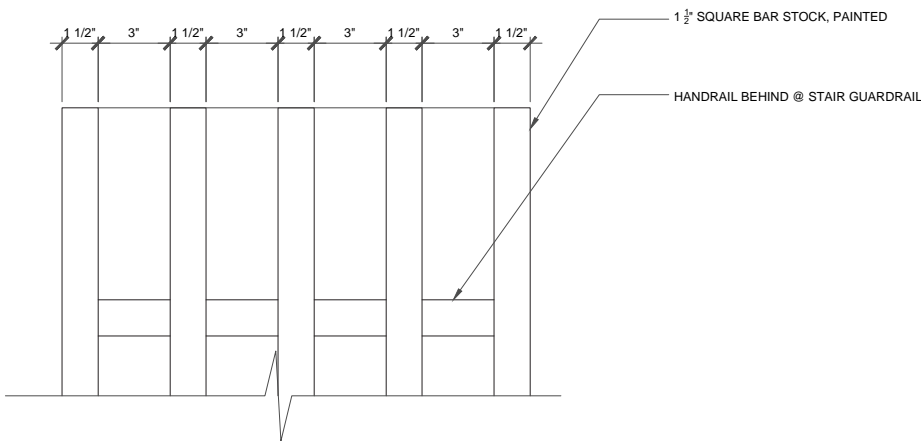
SCALE: 1/16"=1'



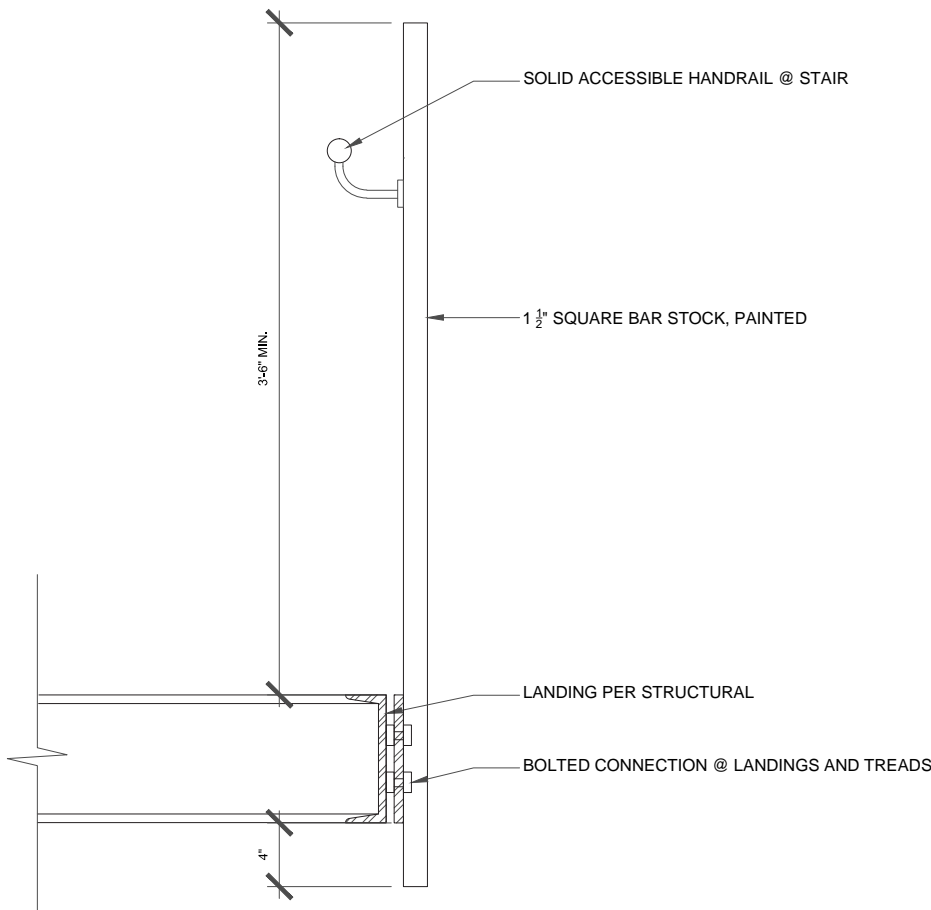


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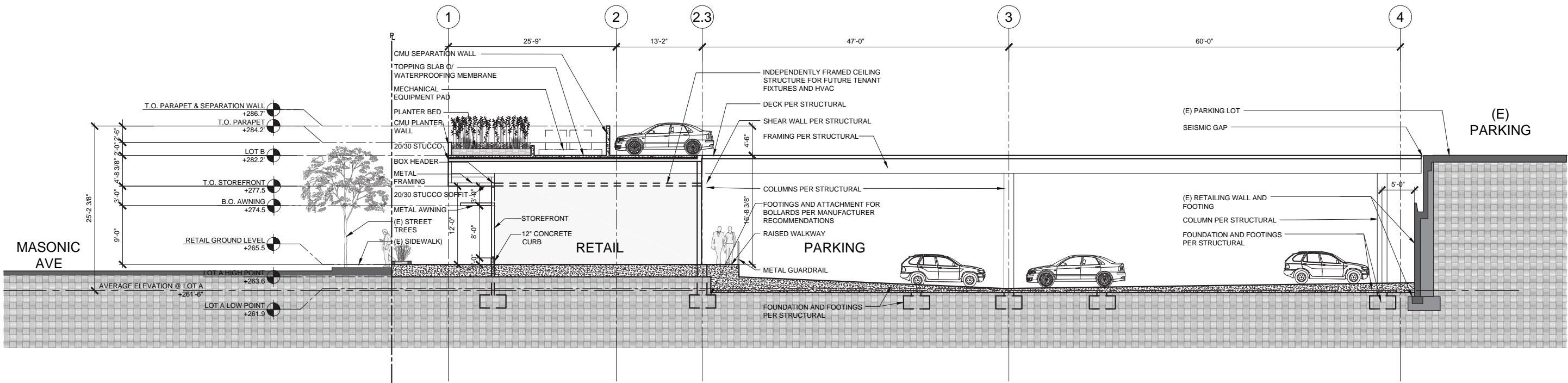
0 8 16 32



TYPICAL ELEVATION @ METAL SCREEN AND GUARDRAIL
SCALE: 1 1/2"=1'



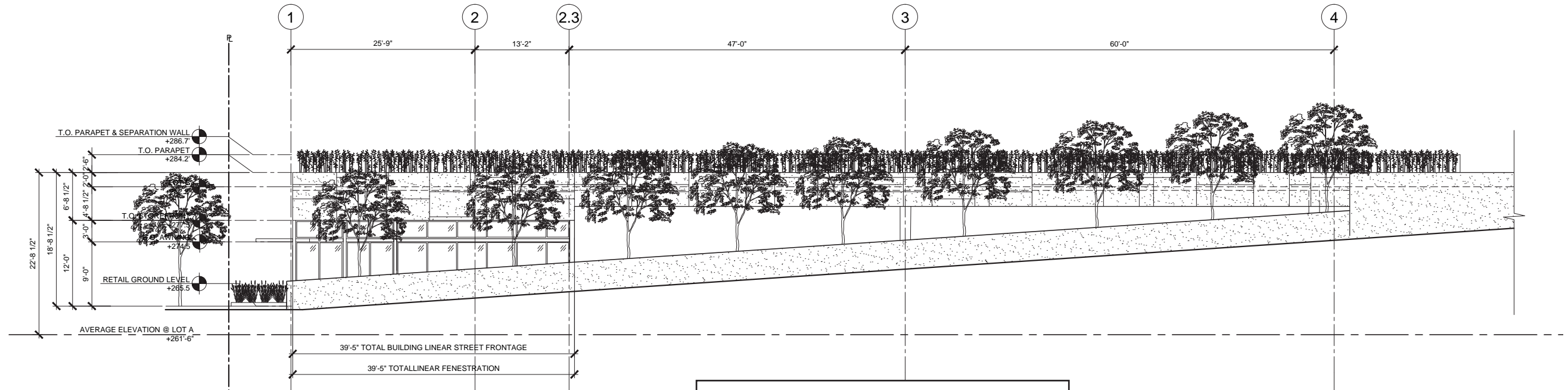
HANDRAIL CONNECTION AT METAL SCREEN GUARDRAIL
SCALE: 1"=1'



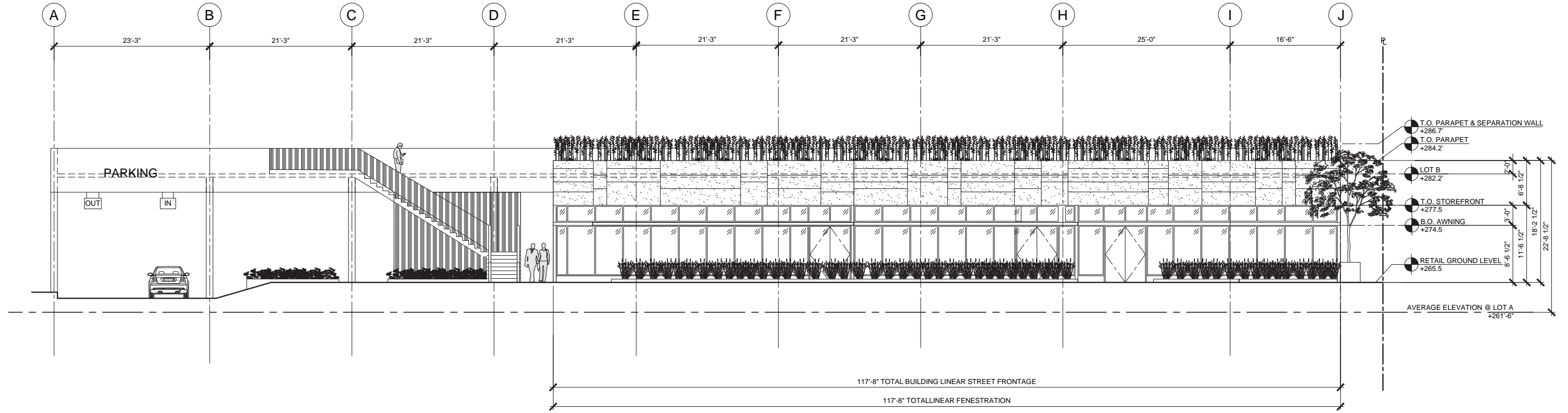
SECTION A

SCALE: 1/16"=1'





SOUTH ELEVATION (O'FARRELL STREET)



WEST ELEVATION (MASONIC AVENUE)



VIEW 1 FROM CORNER OF GEARY BLVD & MASONIC AVE
 CITY CENTER MASONIC RETAIL
 05.30.2017



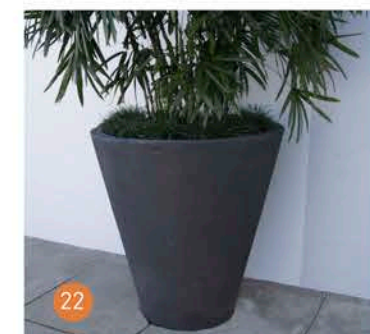
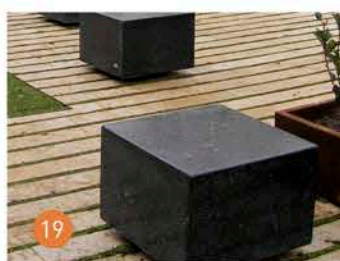
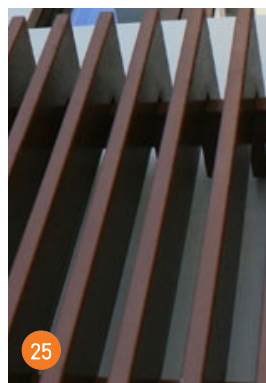
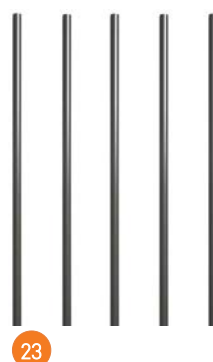
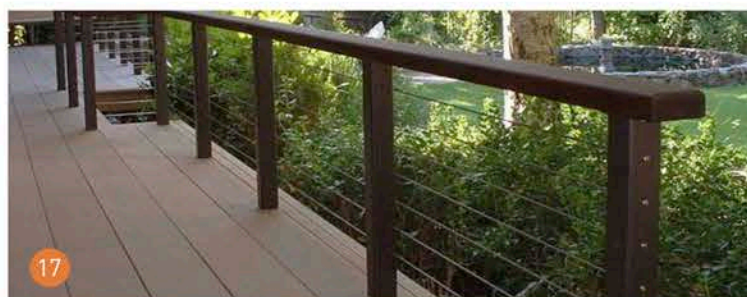
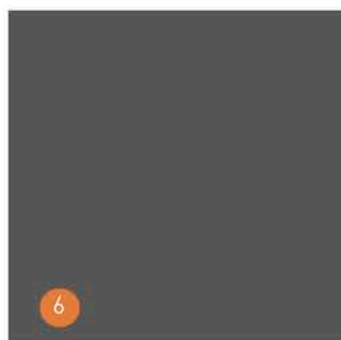
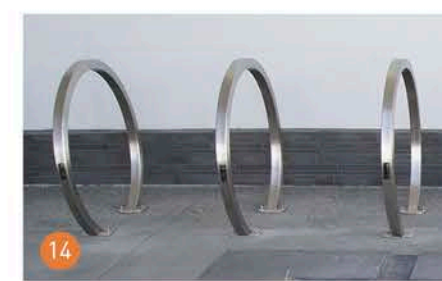
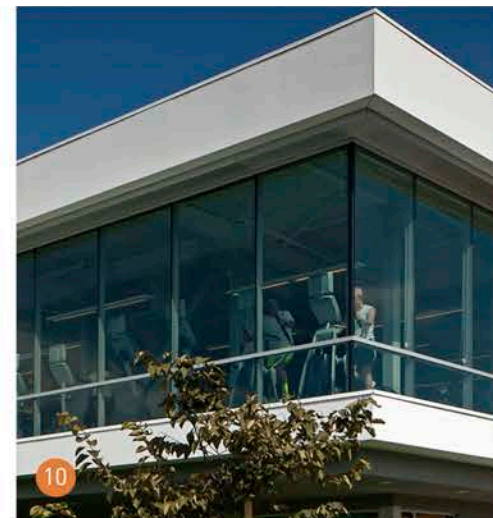
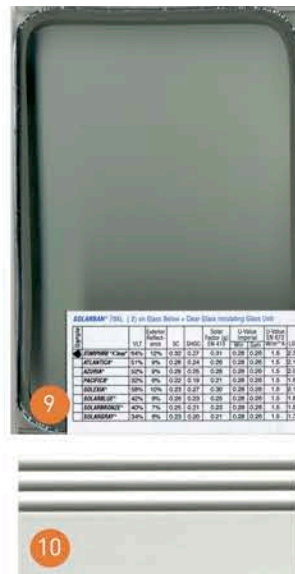
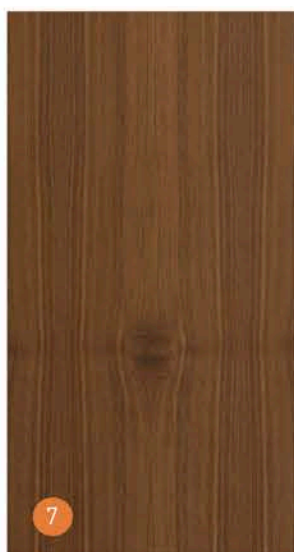
VIEW 2 FROM MASONIC AVE SIDEWALK
CITY CENTER MASONIC RETAIL
05.30.2017



VIEW 3 FROM MASONIC AVENUE @ GARAGE ENTRANCE
CITY CENTER MASONIC RETAIL
05.30.2017



VIEW 4 FROM LOT A
CITY CENTER MASONIC RETAIL
05.30.2017



MATERIALS

- 1 DUNN EDWARDS - DROPLETS
- 2 DUNN EDWARDS - GRAY PEARL
- 3 DUNN EDWARDS - WALRUS
- 4 DUNN EDWARDS - CASTLEROCKI
- 5 DUNN EDWARDS - CHARCOAL SMUDGE
- 6 DUNN EDWARDS - JET
- 7 EXTERIOR TEAK PANELING
- 8 16/20 PAINTED CEMENT PLASTER
- 9 PERFORMANCE GLAZING @ STOREFRONT
CLEAR 1" INSULATED UNIT
- 10 CLEAR ANODIZED ALUMINUM STOREFRONT
- 11 COOL ZATIQUE - METAL ACCENTS AND CANOPY
- 12 CLEAR ANODIZED ROLL UP SECTIONAL DOOR
- 13 BICYCLE LOCKER
- 14 TUBE STEEL BICYCLE RACK
- 15 TRASH RECEPTACLE
- 16 CUSTOM MURAL/DECAL
- 17 CABLE RAIL
- 18 SACKED & SEALED CONCRETE
- 19 FIXED BENCHES
- 20 LOOSE SEATING
- 21 GLASS GUARDRAIL
- 22 CONCRETE PLANTERS
- 23 VERTICAL METAL BAR STOCKSCREEN
- 24 EIFS - WHITE
- 25 ALUMINUM SLAT SCREEN

MATERIALS PALETTE

CITY CENTER CROW'S NEST + RETAIL PAD + MASONIC RETAIL

05.30.2017

TENANT/SHOPPING CENTER GROSS SQUARE FOOTAGE				
	Suite Number	Approx. Existing GSF	Approx. Proposed GSF	Approx. Expansion GSF
City Target	200	119,000	119,000	0
Best Buy	300	55,311	55,311	0
ULTA Beauty	100	10,194	10,194	0
Sleep Number	B100	3,390	3,390	0
Panera Bread	E106	4,000	4,000	0
Chipotle	E105	2,100	2,100	0
Vacant	E104	1,374	1,374	0
Subway	E103	1,060	1,060	0
European Wax Center	E102	1,277	1,277	0
GNC	E101	1,264	1,264	0
Best Buy Auto Install	E100	1,987	1,987	0
Crows Nest	400	5,940	13,470	7,530
Retail Pad	TBD	0	22,072	22,072
Retail Shed	TBD	1,190	1,190	0
Masonic Retail	TBD	0	3,608	3,608
TOTAL		208,087	241,297	33,210
FAR		0.72 to 1	0.84 to 1	
ALLOWED FAR		3.6 to 1	3.6 to 1	

PARKING												
Lot	Primary Accessible Tenants (1)	Car Parking Spaces		Accessible Parking Spaces			Class 1 Bike Parking Spaces			Class 2 Bike Parking Spaces		
		Existing	Proposed	Existing	Proposed	Required	Existing	Proposed	Required	Existing	Proposed	Required
Parking Lot A	City Target	78	59	3	5		0	0		6	14	0
	(N) Masonic Retail											
Parking Lot B	City Target	81	171	3	3		4	8		0	0	0
	Sleep Number											
Parking Lot C	Best Buy	117	117	5	5		5	10		10	10	0
	Retail Shed											
Parking Lot D	City Target Overflow	123	107	2	4		0	6		0	14	0
	City Dental											
Parking Lot E	City Target	96	93	4	6		0	0		8	8	0
	Panera Bread											
	Chipotle											
	Vacant											
	Subway											
	European Wax Center											
	GNC											
Parking Lot F	ULTA Beauty / Retail Pad	145	87	5	6		0	8		12	20	2
TOTAL		640	634	22	29	14	9	32	27	36	66	36
PARKING RATIO		2.6 per 1000 SF										

LOADING ANALYSIS					
Use	Loading Requirement		Existing Loading	Required Loading - Existing GSF	Proposed Loading - Proposed GSF
Retail stores, wholesaling, manufacturing, live/work units in newly constructed structures, and all other uses primarily engaged in the handling of goods.	SF	Loading	5	5	6
	0-10,000 SF	0			5
	10,001-60,000 SF	1			
	60,001-100,000 SF	2			
	Over 100,000 SF	3 + 1 for every 80,000 SF			

NOTES
(1) - All parking is non-exclusive and available to all tenants.

ESTIMATED OCCUPIED SQUARE FOOTAGE* AND PARKING ANALYSIS					
Use	Parking Requirement	Approx. Existing Occupied SF	Existing Parking Required	Approx. Proposed Occupied SF	Proposed Parking Requirement
Retail	1 : 500 (First 20,000 SF)	20,000	40	20,000	40
	1 : 250 (SF over 20,000 SF)	134,690	539	162,919	652
Service/Repair	1 : 1,000	2,874	3	2,874	3
Restaurant	1 : 200	5,240	26	5,240	26
TOTAL		162,804	608	191,032	721

*Occupied square footage calculated by deducting approximate back of house loss factor from gross square footage