



SAN FRANCISCO PLANNING DEPARTMENT

Executive Summary Large Project Authorization

HEARING DATE: MARCH 3, 2016

Date: February 22, 2016
Case No.: **2015-000453ENXSHD**
Project Address: **88 ARKANSAS STREET**
Zoning: UMU (Urban Mixed-Use) Zoning District
48-X Height and Bulk District
Block/Lot: 3953/002 and 3953/002A
Project Sponsor: Julie Heinzler, Martin Building Company
14 Mint Plaza, 5th Floor
San Francisco, CA 94103
Staff Contact: Kate Conner – (415) 575-6914
kate.conner@sfgov.org

Recommendation: **Approval with Conditions**

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PROJECT DESCRIPTION

The Project includes demolition of the two existing two-story industrial warehouse buildings on the subject lots (measuring 25,560 square feet), and new construction of a five-story residential building (approximately 118,684 gsf), approximately 48 feet tall with 127 dwelling units, approximately 3,118 gross square feet of retail/restaurant uses, up to 98 off-street parking spaces, 128 Class 1 bicycle parking spaces and 12 Class 2 bicycle parking spaces, and a combination of private and common open space. The Project includes a dwelling unit mix of approximately 25 studio apartments, 51 one-bedroom apartments, 48 two-bedroom apartments, and six three-bedroom units. The proposed project includes common open space (approximately 8,921 square feet) and private open space (approximately 1,379 square feet). Common open space will be provided by two courtyards on the ground floor, a solarium on the fifth floor, and a roof deck. An approximately 31-foot-wide parklet will be located on 17th Street near the southwest corner of 17th and Arkansas Streets, which will require a separate application process subsequent to this entitlement. The proposed project is also seeking Leadership in Energy and Environmental Design (LEED) for Homes Platinum certification from the U.S. Green Building Council.

SITE DESCRIPTION AND PRESENT USE

The Project is located on an approximately 30,000 square foot rectangular site that is comprised of two adjacent lots: 88 Arkansas Street/1500 17th Street (Lot 002) and 1530 17th Street (Lot 002A). The lots are located on the block bounded by 17th Street to the south, 16th Street to the north, Arkansas Street to the east, and Wisconsin Street to the west in the Potrero Hill neighborhood. Lot 002A is developed with an approximately 24-foot-tall, two-story industrial building constructed in 1923 that is currently vacant. Lot 002 is developed with an approximately 24-foot-tall, two-story industrial building constructed in 1906

and a surface parking lot providing 13 spaces. Access to the parking lot is via an approximately 24-foot-wide curb cut off of Arkansas Street. Currently, 88 Arkansas Street is occupied by an interior design staging company. Collectively, the two existing buildings provide approximately 25,560 gsf of industrial and storage space.

SURROUNDING PROPERTIES AND NEIGHBORHOOD

The Project Site is located in the UMU (Urban Mixed Use) Zoning District directly north of Jackson Playground along a mixed-use corridor within the Showplace Square/ Potrero Hill Area Plan. The UMU District is intended to promote a vibrant mix of uses while maintaining the characteristics of this formerly industrially-zoned area. It is also intended to serve as a buffer between residential districts and PDR districts in the Eastern Neighborhoods. Within the UMU, allowed uses include production, distribution, and repair uses such as light manufacturing, home and business services, arts activities, warehouse, and wholesaling. Additional permitted uses include retail, educational facilities, and nighttime entertainment. Housing is also permitted, but is subject to higher affordability requirements. Family-sized dwelling units are encouraged.

The immediate neighborhood includes: one-to-two story tall, warehouse properties across Arkansas Street; residential properties and a bar/restaurant use to the south of 17th Street and on the eastern side of Arkansas Street; and, various one-to-two-story tall, commercial and industrial properties to the north. The subject lot is located within five blocks of the Showplace Square Heavy Timber and Steel-frame Brick Warehouse and Factory Historic District, which is located four blocks west and one block north of the Project Site. Other zoning districts in the vicinity of the Project Site include: P (Public); RH-3 (Residential, House, Three-Family); PDR-1-D (Production, Distribution and Repair-Design), and, PDR-1-G (Production, Distribution and Repair-General).

ENVIRONMENTAL REVIEW

Pursuant to the Guidelines of the State Secretary of Resources for the implementation of the California Environmental Quality Act (CEQA), on January 14, 2016, the Planning Department of the City and County of San Francisco determined that the proposed application was exempt from further environmental review under Section 15183 of the CEQA Guidelines and California Public Resources Code Section 21083.3. The Project is consistent with the adopted zoning controls in the Eastern Neighborhoods Area Plan and was encompassed within the analysis contained in the Eastern Neighborhoods Area Plan Final EIR. Since the Final EIR was finalized, there have been no substantial changes to the Eastern Neighborhoods Area Plan and no substantial changes in circumstances that would require major revisions to the Final EIR due to the involvement of new significant environmental effects or an increase in the severity of previously identified significant impacts, and there is no new information of substantial importance that would change the conclusions set forth in the Final EIR.

HEARING NOTIFICATION

TYPE	REQUIRED PERIOD	REQUIRED NOTICE DATE	ACTUAL NOTICE DATE	ACTUAL PERIOD
Classified News Ad	20 days	February 12, 2016	February 10, 2016	20 days
Posted Notice	20 days	February 12, 2016	February 12, 2016	20 days
Mailed Notice	10 days	February 23, 2016	February 19, 2016	10 days

The proposal requires a Section 312 Neighborhood notification, which was conducted in conjunction with the required hearing notification for the Large Project Authorization.

PUBLIC COMMENT

As of February 16, 2016, the Department has received three pieces of correspondence in opposition to the Project and seven letters in support of the Project.

ISSUES AND OTHER CONSIDERATIONS

- Large Project Authorization Exceptions: As part of the Large Project Authorization (LPA), the Commission may grant exceptions from certain Planning Code requirements for projects that exhibit outstanding overall design and are complementary to the design and values of the surrounding area. The proposed project requests exceptions from 1) rear yard (Planning Code Section 134); 2) dwelling unit exposure (Planning Code Section 140); 3) off-street loading (Planning Code Section 152.1); and, 4) dwelling unit mix (Planning Code Section 207.6). Department staff is generally supportive of the proposed exceptions given the overall project's outstanding and unique design.
- Shadow Analysis: Per Planning Code Section 295, the Commission must grant authorization to new construction projects that will cast shade or shadow upon any property under the jurisdiction of the Recreation and Park Commission. The Project was reviewed by the Recreation and Park Commission on December 21, 2016, who recommended that the additional shadow cast by the Project would not be adverse to the use of the nearby Jackson Playground. The Commission must adopt a motion that the additional shadow cast by the Project on Jackson Playground would not be adverse to its use.
- Inclusionary Affordable Housing: The Project has elected to participate in the Priority Processing Program and has elected the on-site affordable housing alternative, identified in Planning Code Section 415.6. The project site is located within the UMU Zoning District, which requires 14.4% of the total number of units to be designated as part of the inclusionary affordable housing program. However, by participating in the Priority Processing Program, the Project Sponsor has committed to providing 20% of the units as on-site affordable housing units. The Project contains 127 dwelling units and the Project Sponsor will fulfill this requirement by providing the 25 affordable units on-site, which will be available for rent. As part of the project, the Project Sponsor has entered into a Costa-Hawkins Agreement with the City. A copy of this agreement will be provided at the Planning Commission Hearing.

- Development Impact Fees: The Project would be subject to the following development impact fees, which are estimated as follows:

FEE TYPE	PLANNING CODE SECTION/FEE	AMOUNT
Eastern Neighborhoods Impact Fee (3,118 sq ft – Tier 1; Change in Use from PDR to Non-Residential)	423 (@ \$3.82)	\$11,911
Eastern Neighborhoods Impact Fee (22,442 sq ft – Tier 1; Change in Use from PDR to Residential)	423 (@ \$6.37)	\$142,956
Eastern Neighborhoods Impact Fee (93,124 sq ft – Tier 1; New Residential)	423 (@ \$10.19)	\$948,934
Transportation Sustainability Fee (TSF) (3,118 sq ft – Change in Use from PDR to Retail – TIDF Rates)	411A (@ \$7.49)	\$23,354
Transportation Sustainability Fee (TSF) (22,442 sq ft – Change in Use from PDR to Residential)	411A (@ \$0)	\$0
Transportation Sustainability Fee (TSF) (72,637 sq ft – New Residential (First 99 Units) – Discount of 50% for EE filed prior to 07/21/15)	411A (@ \$3.87)	\$281,105
Transportation Sustainability Fee (TSF) (20,487 sq ft – New Residential (Additional 28 Units) – Discount of 50% for EE filed prior to 07/21/15)	411A (@ \$4.37)	\$89,528
Child Care In-Lieu Fee (3,118 sq ft – New Commercial)	414 (@.34)	\$1,060
Child Care In-Lieu Fee (115,566 sq ft - New Residential)	414 (@1.26)	\$145,613
	TOTAL	\$1,644,461

Please note that these and other impact fees are subject to change between Planning Commission approval and approval of the associated Building Permit Application, include annual indexing of impact fees by the City Controller's Office.

REQUIRED COMMISSION ACTION

In order for the project to proceed, the Commission must grant a Large Project Authorization pursuant to Planning Code Section 329 to allow the new construction of a five-story residential building with approximately 118,684 gross square feet with 127 dwelling units, and to allow exceptions to the requirements for rear yard (Planning Code Section 134), dwelling unit exposure (Planning Code Section 140), off-street loading (Planning Code Section 152.1), and dwelling unit mix (Planning Code Section 207.6).

In addition, with a recommendation from the General Manager of the Recreation and Park Department and in consultation with the Recreation and Park Commission, the Planning Commission must adopt a motion that finds additional shadow cast by the Project on Jackson Playground would not be adverse to the use of the park, pursuant to Planning Code 295.

BASIS FOR RECOMMENDATION

The Department believes this project is approvable for the following reasons:

- The Project complies with the applicable requirements of the Planning Code.
- The Project is consistent with the objectives and policies of the General Plan.
- The Project is located in zoning districts where residential and ground floor retail uses are principally permitted.
- The Project is consistent with and respects the existing neighborhood character, and provides an appropriate massing and scale for a corner lot.
- The Project adds 127 new dwelling units to the City's housing stock, including 25 on-site affordable housing units for rent.
- The Project produces a new mixed-use development with new ground floor retail space and significant site updates, including a new corner bulb-out at 17th and Arkansas Street, sidewalk planters, street trees and site furnishings, as well as private and common open space.
- The Project includes new shadow on Jackson Playground, which is not adverse to the use of the park.
- The Project complies with the First Source Hiring Program.
- The Project will fully utilize the Eastern Neighborhoods Area Plan controls, and will pay the appropriate development impact fees, including the Transportation Sustainability Fee.

RECOMMENDATION:	Approval with Conditions
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Attachments:

Draft Motion-Large Project Authorization
Draft Motion-Shadow Findings
Recreation and Park Commission Resolution
Parcel Map
Sanborn Map
Zoning Map
Height Map
Aerial Photo
Site Photo
Major Projects within .25 Miles
Community Plan Exemption
Affordable Housing Affidavit
First Source Hiring Affidavit

Anti-Discriminatory Housing Affidavit

Public Correspondence

- Letter from the Project Sponsor dated: February 18, 2016
- Exhibit A from Project Sponsor dated: February 18, 2016
- Letter from 2Gen Equity dated: February 1, 2016
- Letter from Potrero Boosters Neighborhood Association dated: January 24, 2016
- Letter from Potrero Boosters Neighborhood Association, Development Committee Comments and recommendations dated: June 1, 2015
- Letter from Del Popolo dated: January 25, 2016
- Letter from Friends of Mint Plaza dated: February 16, 2016
- Letter from Livable City dated: January 7, 2016
- San Francisco Housing Action Coalition (SFHAC) Project Report Card dated: February 10, 2016
- Letter from San Francisco Housing Action Coalition (SFHAC) dated February 18, 2016
- Letter from Terry Kelley-Farias dated February 14, 2016

Architectural Drawings

Attachment Checklist

- | | |
|---|--|
| <input checked="" type="checkbox"/> Executive Summary | <input checked="" type="checkbox"/> Project Sponsor Submittal |
| <input checked="" type="checkbox"/> Draft Motion | Drawings: <u>Existing Conditions</u> |
| <input checked="" type="checkbox"/> Environmental Determination | <input checked="" type="checkbox"/> Check for legibility |
| <input checked="" type="checkbox"/> Zoning District Map | Drawings: <u>Proposed Project</u> |
| <input checked="" type="checkbox"/> Height & Bulk Map | <input checked="" type="checkbox"/> Check for legibility |
| <input checked="" type="checkbox"/> Parcel Map | <input type="checkbox"/> Health Dept. Review of RF levels |
| <input checked="" type="checkbox"/> Sanborn Map | <input type="checkbox"/> RF Report |
| <input checked="" type="checkbox"/> Aerial Photo | <input type="checkbox"/> Community Meeting Notice |
| <input checked="" type="checkbox"/> Context Photos | <input checked="" type="checkbox"/> Inclusionary Affordable Housing Program:
Affidavit for Compliance |
| <input checked="" type="checkbox"/> Site Photos | |

Exhibits above marked with an "X" are included in this packet

KC
Planner's Initials

RS: C:\Users\rsucre\Desktop\ExecutiveSummary_88 Arkansas.doc



SAN FRANCISCO PLANNING DEPARTMENT

Subject to: (Select only if applicable)

- | | |
|---|---|
| <input checked="" type="checkbox"/> Affordable Housing (Sec. 415) | <input checked="" type="checkbox"/> First Source Hiring (Admin. Code) |
| <input type="checkbox"/> Jobs Housing Linkage Program (Sec. 413) | <input checked="" type="checkbox"/> Child Care Requirement (Sec. 414) |
| <input type="checkbox"/> Downtown Park Fee (Sec. 412) | <input checked="" type="checkbox"/> Other (EN Impact Fees) |

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Planning Commission Draft Motion

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ADOPTING FINDINGS RELATING TO A LARGE PROJECT AUTHORIZATION PURSUANT TO PLANNING CODE SECTION 329, TO ALLOW EXCEPTIONS TO 1) REAR YARD PURSUANT TO PLANNING CODE SECTION 134, 2) DWELLING UNIT EXPOSURE PURSUANT TO PLANNING CODE SECTION 140, 3) OFF-STREET LOADING PURSUANT TO PLANNING CODE SECTION 152.1, AND, 4) DWELLING UNIT MIX PURSUANT TO PLANNING CODE SECTION 207.6, TO ALLOW CONSTRUCTION OF A NEW FIVE-STORY RESIDENTIAL BUILDING (APPROXIMATELY 118,684 GSF) WITH 127 DWELLING UNITS (CONSISTING OF 25 STUDIOS, 48 1-BEDROOM UNITS, 48 2-BEDROOM UNITS, AND 6 3-BEDROOM UNITS), LOCATED AT 88 ARKANSAS STREET, LOTS 002 AND 002A IN ASSESSOR'S BLOCK 3953, WITHIN THE UMU (URBAN MIXED-USE) ZONING DISTRICT, THE SHOWPLACE SQUARE/POTRERO HILL AREA PLAN, AND A 48-X HEIGHT AND BULK DISTRICT.

PREAMBLE

On April 1, 2015, Julie Heinzler, Martin Building Company (hereinafter "Project Sponsor") filed Application No. **2015-000453ENXSHD** (hereinafter "Application") with the Planning Department (hereinafter "Department") for a Large Project Authorization to construct a new five-story residential building with 127 dwelling units at 88 Arkansas Street (Block 3953 Lots 002 and 002A) in San Francisco, California.

The environmental effects of the Project were determined by the San Francisco Planning Department to have been fully reviewed under the Eastern Neighborhoods Area Plan Environmental Impact Report (hereinafter "EIR"). The EIR was prepared, circulated for public review and comment, and, at a public hearing on August 7, 2008, by Motion No. 17661, certified by the Commission as complying with the California Environmental Quality Act (Cal. Pub. Res. Code Section 21000 et seq., (hereinafter "CEQA"). The Commission has reviewed the Final EIR, which has been available for this Commissions review as well as public review.

The Eastern Neighborhoods EIR is a Program EIR. Pursuant to CEQA Guideline 15168(c)(2), if the lead agency finds that no new effects could occur or no new mitigation measures would be required of a proposed project, the agency may approve the project as being within the scope of the project covered by the program EIR, and no additional or new environmental review is required. In approving the Eastern Neighborhoods Plan, the Commission adopted CEQA Findings in its Motion No. 17661 and hereby incorporates such Findings by reference.

Additionally, State CEQA Guidelines Section 15183 provides a streamlined environmental review for projects that are consistent with the development density established by existing zoning, community plan or general plan policies for which an EIR was certified, except as might be necessary to examine whether there are project-specific effects which are peculiar to the project or its site. Section 15183 specifies that examination of environmental effects shall be limited to those effects that (a) are peculiar to the project or parcel on which the project would be located, (b) were not analyzed as significant effects in a prior EIR on the zoning action, general plan or community plan with which the project is consistent, (c) are potentially significant off-site and cumulative impacts which were not discussed in the underlying EIR, or (d) are previously identified in the EIR, but which are determined to have a more severe adverse impact than that discussed in the underlying EIR. Section 15183(c) specifies that if an impact is not peculiar to the parcel or to the proposed project, then an EIR need not be prepared for that project solely on the basis of that impact.

On January 14, 2016, the Department determined that the proposed application did not require further environmental review under Section 15183 of the CEQA Guidelines and Public Resources Code Section 21083.3. The Project is consistent with the adopted zoning controls in the Eastern Neighborhoods Area Plan and was encompassed within the analysis contained in the Eastern Neighborhoods Final EIR. Since the Eastern Neighborhoods Final EIR was certified, there have been no substantial changes to the Eastern Neighborhoods Area Plan and no substantial changes in circumstances that would require major revisions to the Final EIR due to the involvement of new significant environmental effects or an increase in the severity of previously identified significant impacts, and there is no new information of substantial importance that would change the conclusions set forth in the Final EIR. The file for this Project, including the Eastern Neighborhoods Final EIR and the Community Plan Exemption certificate, is available for review at the San Francisco Planning Department, 1650 Mission Street, Suite 400, San Francisco, California.

Planning Department staff prepared a Mitigation Monitoring and Reporting Program (MMRP) setting forth mitigation measures that were identified in the Eastern Neighborhoods Plan EIR that are applicable

to the project. These mitigation measures are set forth in their entirety in the MMRP attached to the draft Motion as Exhibit C.

On March 3, 2016, the Commission conducted a duly noticed public hearing at a regularly scheduled meeting on Large Project Authorization Application Case No. 2015-000453ENXSHD.

The Commission has heard and considered the testimony presented to it at the public hearing and has further considered written materials and oral testimony presented on behalf of the applicant, Department staff, and other interested parties.

MOVED, that the Commission hereby authorizes the Large Project Authorization requested in Application No. 2015-000453ENXSHD, subject to the conditions contained in "EXHIBIT A" of this motion, based on the following findings:

FINDINGS

Having reviewed the materials identified in the preamble above, and having heard all testimony and arguments, this Commission finds, concludes, and determines as follows:

1. The above recitals are accurate and constitute findings of this Commission.
2. **Site Description and Present Use.** The Project is located on an approximately 30,000 square foot rectangular site that is comprised of two adjacent lots: 88 Arkansas Street/1500 17th Street (Lot 002) and 1530 17th Street (Lot 002A). The lots are located on the block bounded by 17th Street to the south, 16th Street to the north, Arkansas Street to the east, and Wisconsin Street to the west in the Potrero Hill neighborhood. Lot 002A is developed with an approximately 24-foot-tall, two-story industrial building constructed in 1923 that is currently vacant. Lot 002 is developed with an approximately 24-foot-tall, two-story industrial building constructed in 1906 and a surface parking lot providing 13 spaces. Access to the parking lot is via an approximately 24-foot-wide curb cut off of Arkansas Street. Currently, 88 Arkansas Street is occupied by an interior design staging company. Collectively, the two existing buildings provide approximately 25,560 gsf of industrial and storage space.
3. **Surrounding Properties and Neighborhood.** The Project Site is located in the UMU (Urban Mixed Use) Zoning District directly north of Jackson Playground along a mixed-use corridor within the Showplace Square/ Potrero Hill Area Plan. The UMU District is intended to promote a vibrant mix of uses while maintaining the characteristics of this formerly industrially-zoned area. It is also intended to serve as a buffer between residential districts and PDR districts in the Eastern Neighborhoods. Within the UMU, allowed uses include production, distribution, and repair uses such as light manufacturing, home and business services, arts activities, warehouse, and wholesaling. Additional permitted uses include retail, educational facilities, and nighttime entertainment. Housing is also permitted, but is subject to higher affordability requirements. Family-sized dwelling units are encouraged.

The immediate neighborhood includes: one-to-two story tall, warehouse properties across Arkansas Street; residential properties and a bar/restaurant use to the south of 17th Street and on the eastern side of Arkansas Street; and, various one-to-two-story tall, commercial and industrial properties to the north. The subject lot is located within five blocks of the Showplace Square Heavy Timber and Steel-frame Brick Warehouse and Factory Historic District, which is located four blocks west and one block north of the Project Site. Other zoning districts in the vicinity of the Project Site include: P (Public); RH-3 (Residential, House, Three-Family); PDR-1-D (Production, Distribution and Repair-Design), and, PDR-1-G (Production, Distribution and Repair-General).

4. **Project Description.** The Project includes demolition of the two existing two-story industrial warehouse buildings on the subject lots (measuring 25,560 square feet), and new construction of a five-story residential building (approximately 118,684 gsf), approximately 48 feet tall with 127 dwelling units, approximately 3,118 gross square feet of retail/restaurant uses, up to 98 off-street parking spaces, 128 Class 1 bicycle parking spaces and 12 Class 2 bicycle parking spaces, and a combination of private and common open space. The Project includes a dwelling unit mix of approximately 25 studio apartments, 51 one-bedroom apartments, 48 two-bedroom apartments, and six three-bedroom units. The proposed project includes common open space (approximately 8,921 square feet) and private open space (approximately 1,379 square feet). Common open space will be provided by two courtyards on the ground floor, a solarium on the fifth floor, and a roof deck. An approximately 31-foot-wide parklet will be located on 17th Street near the southwest corner of 17th and Arkansas Streets, which will require a separate application process subsequent to this entitlement. The proposed project is also seeking Leadership in Energy and Environmental Design (LEED) for Homes Platinum certification from the U.S. Green Building Council.
5. **Public Comment.** As of February 16, 2016, the Department has received three pieces of correspondence in opposition to the Project and seven letters in support of the Project.
6. **Planning Code Compliance:** The Commission finds that the Project is consistent with the relevant provisions of the Planning Code in the following manner:
 - A. **Permitted Uses in UMU Zoning Districts.** Planning Code Section 843.20 states that residential use is principally permitted use within the UMU Zoning District. In addition, Planning Code Section 843.45 states that a retail use is principally permitted use within the UMU Zoning District.

The Project would construct a new residential development within the UMU Zoning District with a ground floor café use; therefore, the Project complies with Planning Code Section 843.20 and Planning Code Section 843.45.

- B. **Rear Yard.** Planning Code Section 134 requires a minimum rear yard equal to 25 percent of the total lot depth of the lot to be provided at every residential level. The total lot depth is 150 feet and the length is 200 feet. Therefore, the Project would have to provide a rear yard, which measures approximately 7,500 sf.

The Project contains dwellings at the ground floor and contains two courtyards: a smaller courtyard along the northern property line and a southern courtyard located along the western property line. The southern courtyard is 3,581 sf in area and the northern courtyard is 1,901 sf in area. The two courtyards total 5,482 sf in area which is 2,018 sf deficient from the required amount.

A comparable, but not necessarily equal amount of square footage as would be created in a Code conforming rear yard is provided elsewhere within the development. For the proposed 127 dwelling units, the Project is required to provide 10,160 sf of open space. The Project includes a combination of courtyards, patios, porches, balconies, an artificial turf area, a solarium, and a roof deck to meet the open space requirements. The total common open space being provided in the Project is 9,530 sf and with the 669 sf of private open space, the total amount of open space being provided is 10,199 sf, which exceeds the 10,160 sf open space requirement for 127 dwelling units and the required 7,500 sf of open space, which would have been provided through the required rear yard. Since the Project does not provide a rear yard against the rear lot line, the Project is seeking an exception of the rear yard requirement as part of the Large Project Authorization (See Below).

A conforming rear yard would have been opposite of Arkansas Street and would result in an inconsistent street wall along 17th Street. By providing an exception to the rear yard requirement, the Project appropriately holds the street wall and results in a more favorable design.

- C. **Useable Open Space.** Planning Code Section 135 requires a minimum of 80 sf of open space per dwelling unit, if not publically accessible, or 54 sf of open space per dwelling unit, if publically accessible. Private useable open space shall have a minimum horizontal dimension of six feet and a minimum area of 36 sf is located on a deck, balcony, porch or roof, and shall have a minimum horizontal dimension of 10 feet and a minimum area of 100 sf if located on open ground, a terrace or the surface of an inner or outer court. Common useable open space shall be at least 15 feet in every horizontal dimension and shall be a minimum of 300 sf. Further, inner courts may be credited as common useable open space if the enclosed space is not less than 20 feet in every horizontal dimension and 400 sf in area, and if the height of the walls and projections above the court on at least three sides is such that no point on any such wall or projection is higher than one foot for each foot that such point is horizontally distant from the opposite side of the clear space in the court.

For the proposed 127 dwelling units, the Project is required to provide 10,160 sf of open space. The Project includes a combination of courtyards, patios, porches, balconies, an artificial turf area, a solarium, and a roof deck to meet the open space requirements. The Project contains two courtyards at the ground floor. The northern courtyard contains open space and three patios which qualify for private open space for three units. Each of these private patios is 80 sf in area. The remainder of the northern courtyard does not contribute to the required open space. The southern courtyard contains 3,514 sf of open space which qualifies as common open space.

There are private porches at the ground floor for five units, each totaling 39 sf in area. There are also private balconies on the second floor for six units, each totaling 39 sf in area. In total, between the private patios, porches, and balconies, there is a total of 669 sf of private open space available in the Project.

Common open space is also being provided at the fifth floor through the addition of a solarium. Pursuant to Planning Code Section 135, the area of a totally or partially enclosed solarium may be credited as common usable open space if the space is not less than 15 feet in every horizontal dimension and 300 square feet in area; and if such area is exposed to the sun through openings or clear glazing on not less than 30 percent of its perimeter and 30 percent of its overhead area. Approximately 1,236 sf of area is dedicated to the solarium use on the fifth floor.

The Project includes two separate open spaces at the roof level: a roof deck and an artificial turf area. The roof deck is 3,260 sf in area and the artificial turf area is 1,140 sf in area. The total common open space being provided in the Project is 9,530 sf and with the 669 sf of private open space, the total amount of open space being provided is 10,199 sf, which exceeds the 10,160 sf requirement for 127 dwelling units.

- D. Streetscape and Pedestrian Improvements.** Planning Code Section 138.1 requires a streetscape plan, which includes elements from the Better Streets Plan.

The Project includes the new construction of a five-story residential building on a lot with approximately 200 feet of frontage along Arkansas Street and 150 feet of frontage along 17th Street. The Project includes streetscape elements, including a bulb out at the intersection of 17th Street and Arkansas Street, bicycle parking racks, sidewalk planters, street trees, site furnishings, and the widening of Arkansas Street sidewalks by eleven feet to incorporate a planting strip and sidewalk furnishings. Although it would be ideal to widen the sidewalks of 17th Street to 15 feet is not in conjunction with future SFMTA plans for widening the street.

Therefore, the Project complies with Planning Code Section 138.1.

- E. Bird Safety.** Planning Code Section 139 outlines the standards for bird-safe buildings, including the requirements for location-related and feature-related hazards.

The subject lot is not located in close proximity to an Urban Bird Refuge. The Project meets the requirements of feature-related standards and does not include any unbroken glazed segments 24- sf and larger in size; therefore, the Project complies with Planning Code Section 139. Any unbroken glazed segments that meet these requirements would have to be treated appropriately.

- F. Dwelling Unit Exposure.** Planning Code Section 140 requires that at least one room of all dwelling units face onto a public street, rear yard or other open area that meets minimum requirements for area and horizontal dimensions. To meet exposure requirements, a public street, public alley, side yard or rear yard must be at least 25 ft in width, or an open area

(inner court) must be no less than 25 ft in every horizontal dimension for the floor at which the dwelling unit is located.

The Project fronts on both 17th Street and Arkansas Street. Units facing these streets meet exposure requirements. In addition, units facing the southern courtyard also meet exposure requirements because the southern courtyard is 45'-9" wide and 65'-9" deep. For a five-story building that contains units on the ground floor, the courtyard must be 40'-0" wide and 40'-0" deep to meet exposure requirements at all levels. Some of the units facing the northern courtyard will require an exception from the exposure requirement. The northern courtyard is only 25'-0" wide and is 71'-3" deep. There are four units on each floor that face this courtyard. All four units on the first three floors require the modification because of the width of the area. The units on the fourth and fifth floors meet the requirement aside from the studio unit which faces an articulated portion of the building, thereby not meeting the exposure requirement. Therefore, the Project is seeking an exception of the dwelling unit exposure requirements for 14 dwelling units as part of the Large Project Authorization (See Below).

- G. Street Frontage in Mixed Use Districts.** Planning Code Section 145.1 requires off-street parking at street grade on a development lot to be set back at least 25 feet on the ground floor; that no more than one-third of the width or 20 feet, whichever is less, of any given street frontage of a new structure parallel to and facing a street shall be devoted to parking and loading ingress or egress; that space for active uses be provided within the first 25 feet of building depth on the ground floor; that non-residential uses have a minimum floor-to-floor height of 14 feet; that the floors of street-fronting interior spaces housing non-residential active uses and lobbies be as close as possible to the level of the adjacent sidewalk at the principal entrance to these spaces; and that frontages with active uses that are not residential or PDR be fenestrated with transparent windows and doorways for no less than 60 percent of the street frontage at the ground level. Ground floor non-residential uses in UMU Districts shall have a minimum floor-to-floor height of 17 feet, as measured from grade.

The Project meets the requirements of Planning Code Section 145.1. At grade, the off-street parking is setback by more than 25-ft from the street. The Project has one 12-foot wide garage entrance to the off-street parking located off of Arkansas Street. The Project features the appropriate amount of active use with the ground floor walk-up dwelling units, which provide direct, individual pedestrian access to a public sidewalk. Finally, the Project features appropriate street-facing ground level spaces, as well as the ground level transparency and fenestration requirements. The building lobby and leasing office are well below the 40-foot maximum frontage. The retail space has a ceiling height of at least 17 feet, thereby meeting this requirement.

- H. Off-Street Parking.** Planning Section 151.1 of the Planning Code allows off-street parking at a maximum ratio of .75 per dwelling unit. For dwelling units in the UMU Zoning District with at least 2 bedrooms and at least 1,000 square feet of occupied floor area, off-street parking is permitted at 1 car for each dwelling unit for these dwelling units per the criteria under 151.1(g).

The Project includes 127 dwelling units, five of which have at least two bedrooms and are at least 1,000 square feet in size; therefore, the Project is permitted five off-street parking spaces for these dwelling units and 92 spaces for the remaining units. The Project is required to provide one car share space. Therefore, the Project is permitted a total of 98 off-street parking spaces.

Currently, the Project provides 98 off-street parking spaces via a combination of mechanical lifts and single spaces. Of these 98 off-street parking spaces, there are 82 stacker spaces, two handicap parking spaces, as well as one car-share parking space and 13 standard spaces. Therefore, the Project complies with Planning Code Section 151.1.

- I. **Off-Street Freight Loading.** Planning Section 152.1 of the Planning Code requires one off-street freight loading space for apartment use between 100,001 - 200,000 gsf.

The Project includes approximately 111,694 square feet of apartment use, thus at least one off-street freight loading space is required. The Project does not possess any off-street freight loading parking spaces; however, the Project is proposing a 45-foot loading zone on Arkansas Street. Therefore, the Project is seeking an exception of this requirement as part of the Large Project Authorization (See Below).

- J. **Bicycle Parking.** Planning Section 155.2 of the Planning Code requires at least 100 Class 1 bicycle parking spaces plus one Class 1 bicycle parking space for every four dwelling units and one Class 2 bicycle parking spaces for every 20 dwelling units. A café use requires a minimum of two spaces and one Class 2 space for every 750 sq. ft. of occupied floor area.

The Project includes 127 dwelling units; therefore, the Project is required to provide 107 Class 1 bicycle parking spaces and 6 Class 2 bicycle parking spaces to satisfy the residential requirement. The Project will provide 127 Class 1 spaces and 6 Class 2 spaces for the residential component and provide 1 additional Class 1 space and 6 additional Class 2 bicycle parking spaces for the café component. The café has 2,916 sf of occupied floor area; therefore, four spaces are required. The total bicycle parking required is 107 Class 1 spaces and 10 Class 2 spaces. The Project is providing 128 Class 1 spaces and 12 Class 2 spaces; therefore the Project is exceeding the requirement and complies with Planning Code Section 155.2.

- K. **Car Share Requirements.** Planning Code Section 166 requires one car-share parking space, plus one for every residential project with 50-200 dwelling units.

Since the Project includes 127 dwelling units, it is required to provide a minimum of one car-share parking space. The Project provides one car-share parking space. Therefore, the proposed project complies with Planning Code Section 166.

- L. **Unbundled Parking.** Planning Code Section 167 requires that all off-street parking spaces accessory to residential uses in new structures of 10 dwelling units or more be leased or sold separately from the rental or purchase fees for dwelling units for the life of the dwelling units.

The Project is providing off-street parking that is accessory to the dwelling units. These spaces will be unbundled and sold and/or leased separately from the dwelling units; therefore, the Project meets this requirement.

- M. **Dwelling Unit Mix.** Planning Code Section 207.6 requires that no less than 40 percent of the total number of proposed dwelling units contain at least two bedrooms, or no less than 30 percent of the total number of proposed dwelling units contain at least three bedrooms.

For the 127 dwelling units, the Project is required to provide at least 51 two-bedroom units or 38 three-bedroom units. The Project provides 25 studios, 51 one-bedroom units, 41 two-bedroom units, and 10 three-bedroom units. The Project is utilizing a nested bedroom layout for the two and three bedroom units. Therefore, the Project is seeking an exception of this requirement as part of the Large Project Authorization (See Below).

- N. **Horizontal Mass Reduction.** Planning Code Section 270.1 outlines the requirements for horizontal mass reduction on large lots within the Eastern Neighborhoods Mixed Use Districts. For projects with street frontage greater than 200-ft in length, one or more mass reduction breaks must be incorporated to reduce the horizontal scale of the building into discrete sections not more than 200-ft in length. Specifically, the mass reduction must 1) be not less than 30-ft in width; 2) be not less than 60-ft in depth from the street-facing building façade; 3) extend up to the sky from a level not higher than 25-ft above grade or the third story, whichever is lower; and, 4) result in discrete building sections with a maximum plan length along the street frontage not greater than 200-ft.

Given that the frontage is only 200 feet along Arkansas Street, the Project is not required to provide horizontal mass breaks; therefore, the Project meets this requirement.

- O. **Mid-Block Alley.** Planning Code Section 270.2 outlines the requirements for mid-block alleys on large lots within the Eastern Neighborhoods Mixed Use Districts. This requirement applies to all new construction on parcels that have one or more street frontages of over 200 linear feet on a block face longer than 400-ft between intersections. On lots with frontage greater than 300-ft, the project shall provide a publicly-accessible mid-block alley for the entire depth of the property, generally located toward the middle of the subject block face, perpendicular to the subject frontage and connecting to any existing streets and alleys.

Given that the frontage is only 200 feet along Arkansas Street, the Project is not required to provide a mid-block alley; therefore, the Project meets this requirement.

- P. **Shadow.** Planning Code Section 295 restricts net new shadow, cast by structures exceeding a height of 40 feet, upon property under the jurisdiction of the Recreation and Park Commission. Any project in excess of 40 feet in height and found to cast net new shadow must be found by the Planning Commission, with comment from the General Manager of the Recreation and Parks Department, in consultation with the Recreation and Park Commission,

to have no adverse impact upon the property under the jurisdiction of the Recreation and Park Commission.

Based upon a detail shadow analysis, the Project would cast new shadow upon Jackson Playground which is a property under the jurisdiction of the Recreation and Parks Commission. Based upon the recommendation of the General Manager of the Recreation and Parks Department, in consultation with Recreation and Park Commission, the net new shadow would not be adverse to the use of Jackson Playground. The Commission has adopted findings regarding the impact of shadow on Jackson Playground, as documented in Motion No. XXXXX.

- Q. Inclusionary Affordable Housing Program.** Planning Code Section 415 sets forth the requirements and procedures for the Inclusionary Affordable Housing Program. Under Planning Code Section 415.3, the current percentage requirements apply to projects that consist of ten or more units, where the first application (EE or BPA) was applied for on or after July 18, 2006. Pursuant to Planning Code Section 415.5, 415.6 and 419 (because the Project is located in the UMU District) the Inclusionary Affordable Housing Program Requirement for the On-site Affordable Housing Alternative is to provide 14.4% of the proposed dwelling units as affordable.

The Project Sponsor has demonstrated that it is eligible for the On-Site Affordable Housing Alternative under Planning Code Section 415.5 and 415.6, and has submitted a 'Affidavit of Compliance with the Inclusionary Affordable Housing Program: Planning Code Section 415,' to satisfy the requirements of the Inclusionary Affordable Housing Program by providing the affordable housing on-site instead of through payment of the Affordable Housing Fee. In order for the Project Sponsor to be eligible for the On-Site Affordable Housing Alternative, the Project Sponsor must submit an 'Affidavit of Compliance with the Inclusionary Affordable Housing Program: Planning Code Section 415,' to the Planning Department stating that any affordable units designated as on-site units shall be sold as ownership units and will remain as ownership units for the life of the project or submit to the Department a contract demonstrating that the project's on- or off-site units are not subject to the Costa Hawkins Rental Housing Act, California Civil Code Section 1954.50 because, under Section 1954.52(b), the Project Sponsor has entered into an agreement with a public entity in consideration for a direct financial contribution or any other form of assistance specified in California Government Code Sections 65915 et seq. and submits an Affidavit of such to the Department. All such contracts entered into with the City and County of San Francisco must be reviewed and approved by the City Attorney's Office. The Project Sponsor has indicated the intention to enter into an agreement with the City to qualify for a waiver from the Costa-Hawkins Rental Housing Act based upon the proposed density bonus and concessions provided by the City and approved herein. The Project Sponsor submitted such Affidavit on February 10, 2016 and a draft of the Costa Hawkins agreement. The Project must execute the Costa Hawkins agreement prior to Planning Commission approval or must revert to payment of the Affordable Housing Fee. The EE application was submitted on January 14, 2015. Pursuant to Planning Code Section 415.3, 415.6, and 419, the on-site requirement is 14.4%.

The Project elected to participate in the Priority Processing program and submitted an application for the program on December 22, 2014 and committed to provide 20% of the units as Affordable Units on-

site. The Project thus qualifies for Priority Processing. Planning Director Bulletin #2 Planning Department Priority Application Processing Guidelines provides Priority Processing for Projects providing 20% of the proposed dwelling units as affordable to qualifying households which must meet or exceed the standards set forth in Planning Code Section 415. Therefore, as a condition or approval and a condition of receiving priority processing, 20% or 25 units (5 studios, 10 one-bedroom, 8 two-bedroom, and 2 three-bedroom) of the 127 units provided will be affordable units restricted under the provisions of Planning Code Section 415 et seq.. If the Project becomes ineligible to meet its Inclusionary Affordable Housing Program obligation through the On-site Affordable Housing Alternative, it must pay the Affordable Housing Fee with interest, if applicable.

- R. **Eastern Neighborhood Infrastructure Impact Fees.** Planning Code Section 423 is applicable to any development project within the UMU (Urban Mixed Use) Zoning District that results in the addition of gross square feet of non-residential and residential floor area.

The Project includes approximately 111,694 gross square feet of new residential development and 3,118 gross square feet of non-residential development. These uses are subject to Eastern Neighborhood Infrastructure Impact Fees, as outlined in Planning Code Section 423. These fees must be paid prior to the issuance of the first construction document.

7. **Large Project Authorization in Eastern Neighborhoods Mixed Use District.** Planning Code Section 329(c) lists nine aspects of design review in which a project must comply; the Planning Commission finds that the project is compliant with these nine aspects as follows:

- A. Overall building mass and scale.

The Project's mass and scale are appropriate for a large lot and the surrounding context, which includes small and large industrial buildings that create varied context along Arkansas Street. The Project expresses one distinct mass along Arkansas Street, which is articulated by different architectural designs. The Project is consistent with the mass and scale of nearby industrial properties. Thus, the Project is appropriate and consistent with the mass and scale of the surrounding neighborhood.

- B. Architectural treatments, facade design and building materials:

The Project's architectural treatments, façade design and building materials include exposed concrete, steel, aluminum, and glass which reference the predominantly industrial character of the neighborhood. Overall, the Project offers a high quality architectural treatment, which provides for unique and expressive architectural design that is consistent and compatible with the surrounding industrial character found in the neighborhood.

- C. The design of lower floors, including building setback areas, commercial space, townhouses, entries, utilities, and the design and siting of rear yards, parking and loading access;

Along the lower floors, the Project provides walk-up dwelling units with individual pedestrian access. These dwelling units provide activity along the street. The lower floors are further enhanced by the publically-accessible retail space on 17th Street and streetscape improvements along Arkansas Street.

- D. The provision of required open space, both on- and off-site. In the case of off-site publicly accessible open space, the design, location, access, size, and equivalence in quality with that otherwise required on-site;

The Project provides the required open space for the 127 dwelling units through private porches, patios, and balconies, in addition to common open space satisfied through a solarium, roof deck, courtyard, and artificial turf area. In total, the Project provides a total of 10,199 sf of useable open space, thus exceeding the required amount for the dwelling units.

- E. The provision of mid-block alleys and pathways on frontages between 200 and 300 linear feet per the criteria of Section 270, and the design of mid-block alleys and pathways as required by and pursuant to the criteria set forth in Section 270.2;

The Project is not required to provide a mid-block alley.

- F. Streetscape and other public improvements, including tree planting, street furniture, and lighting.

In compliance with Planning Code Section 138.1, the Project provides 5 new street trees along the street frontages on Arkansas and 17th Streets in addition to the 12 existing trees which will remain, and would pay an in-lieu fee any street trees not provided due to proximity towards underground utilities, etc. In addition, the Project includes streetscape elements, including an extended bulb out at the corner of Arkansas and 17th Streets. The Project includes streetscape elements, bicycle parking racks, sidewalk planters, street trees, site furnishings, and the widening of Arkansas Street sidewalks by eleven feet to incorporate a planting strip and sidewalk furnishings. Although it would be ideal to widen 17th Street to 15 feet is not in conjunction with future SFMTA plans for widening the street. The Commission finds that these improvements would improve the public realm.

- G. Circulation, including streets, alleys and mid-block pedestrian pathways;

The Project provides ample circulation in and around the Project Site, including two courtyards and a publically accessible retail space. Automobile access is limited to the one opening at the northern corner of the Arkansas Street frontage.

- H. Bulk limits;

The Project is within an 'X' Bulk District, which does not restrict bulk.

- I. Other changes necessary to bring a project into conformance with any relevant design guidelines, Area Plan or Element of the General Plan;

The Project, on balance, meets the Objectives and Policies of the General Plan. See Below.

8. **Large Project Authorization Exceptions.** Proposed Planning Code Section 329 allows exceptions for Large Projects in the Eastern Neighborhoods Mixed Use Districts:

- A. Exception for rear yards, pursuant to the requirements of Section 134(f);

(f) Modification of Requirements in the Eastern Neighborhoods Mixed Use Districts. The rear yard requirement in Eastern Neighborhoods Mixed Use Districts may be modified or waived by the Planning Commission pursuant to Section 329. The rear yard requirement in Eastern Neighborhoods Mixed Use Districts may be modified by the Zoning Administrator pursuant to the procedures set forth in Section 307(h) for other projects, provided that:

- (1) A comparable, but not necessarily equal amount of square footage as would be created in a code conforming rear yard is provided elsewhere within the development;

The Project provides for a comparable amount of open space, in lieu of the required rear yard. Overall, the project site is 30,000 sf in size, and would be required to provide a rear yard measuring 7,500 sf. The Project includes a combination of courtyards, patios, porches, balconies, an artificial turf area, a solarium, and a roof deck to meet the open space requirements. The total common open space being provided in the Project is 9,530 sf and with the 669 sf of private open space, the total amount of open space being provided is 10,199 sf, which exceeds the 10,160 sf open space requirement for 127 dwelling units and the required 7,500 sf of open space, which would have been provided through the required rear yard.

- (2) The proposed new or expanding structure will not significantly impede the access to light and air from adjacent properties or adversely affect the interior block open space formed by the rear yards of adjacent properties; and

The Project does not impede access to light and air for the adjacent properties. The Project is not located adjacent to any residential use. The subject block does not possess a pattern of mid-block open space.

- (3) The modification request is not combined with any other residential open space modification or exposure variance for the project, except exposure modifications in designated landmark buildings under Section 307(h)(1).

The Project is not seeking an exception to the open space requirements; however, the Project is seeking an exception to the exposure requirements for 14 of the 127 dwelling units. The Project does meet the overall intent of these requirements and is seeking an exception to the dimensional requirements of the Planning Code, as allowed under Planning Code Section 329(d) (11).

- B. Exception from satisfaction of loading requirements per Section 152.1 pursuant to the criteria contained therein.

For projects in the Eastern Neighborhoods Mixed Use Districts that are subject to Section 329, the Planning Commission may waive these requirements per the procedures of Section 329 if it finds that the design of the project, particularly ground floor frontages, would be improved and that such loading could be sufficiently accommodated on adjacent streets and alleys.

The Project includes approximately 111,694 square feet of apartment use, thus at least one off-street freight loading space is required. The Project does not possess any off-street freight loading parking spaces; however, the Project is proposing a 45-foot loading zone on Arkansas Street. Further, by providing for on-street loading, the Project has reduced the overall size and scale of the garage opening, and has limited automobile access to one opening on Arkansas Street.

- C. Exception from satisfaction of the required minimum dwelling unit mix requirements.

Planning Code Section 207.6 requires that no less than 40 percent of the total number of proposed dwelling units contain at least two bedrooms, or no less than 30 percent of the total number of proposed dwelling units contain at least three bedrooms. For the 127 dwelling units, the Project is required to provide at least 51 two-bedroom or larger units or 38 three-bedroom units. The Project provides 25 studios, 51 one-bedroom units, 41 two-bedroom units, and 10 three-bedroom units. The Project is using a nested bedroom layout for the two and three bedroom units. Of the 41 two bedrooms and 10 three bedroom units, 38 will have at least one bedroom that does not include an exterior window.

All 41 two-bedroom units are considered two-bedroom units pursuant to the Building Code, which allows for nested bedroom layouts. There is an interpretation of the Planning Code that requires bedrooms subject to dwelling unit mix requirements in the Eastern Neighborhoods Plan Area to have exterior windows. However, given more recent changes to the Building Code and the desire to treat all zoning districts with dwelling unit mix requirements equitably, the Department is conducting comprehensive review of this interpretation, which may result in a future revision. The Department researched other jurisdictions and consulted with the Department of Building Inspection, Mayor's Office of Housing and Community Development and the Office of Community Investment and Infrastructure regarding the definition of a bedroom. Research from other jurisdictions and consultation with other City agencies has shown consistent support of the use of nested bedroom layouts.

- D. Where not specified elsewhere in Planning Code Section 329(d), modification of other Code requirements which could otherwise be modified as a Planned Unit Development (as set forth in Section 304), irrespective of the zoning district in which the property is located;

In addition to the modification of the requirements for rear yard, off-street loading and dwelling unit mix requirements, the Project is seeking modifications of the requirements for exposure (Planning Code Section 140).

The Project fronts on both 17th Street and Arkansas Street. Units facing these streets meet exposure requirements. In addition, units facing the southern courtyard also meet exposure requirements because the southern courtyard is 45'-9" wide and 65'-9" deep. For a five-story building that contains units on the ground floor, the courtyard must be 40'-0" wide and 40'-0" deep to meet exposure requirements. Some of the units facing the northern courtyard will require an exception from the exposure requirement. The northern courtyard is only 25'-0" wide and is 71'-3" deep. There are four units on each floor that face this courtyard. All four units on the first three floors require the modification because of the width of the area. The units on the fourth and fifth floors meet the requirement aside from the studio unit which faces an articulated portion of the building, thereby not meeting the exposure requirement. Therefore, the Project is seeking an exception of the dwelling unit exposure requirements for 14 dwelling units as part of the Large Project Authorization. Although these dwelling units do not face onto a code-complying open space, the provided open space still affords sufficient access to light and air. Given the overall design and composition of the Project, the Commission is in support of this modification, due to the Project's high quality of design and amount of open space/open areas.

8. **General Plan Compliance.** The Project is, on balance, consistent with the following Objectives and Policies of the General Plan:

HOUSING

OBJECTIVES AND POLICIES

OBJECTIVE 1

IDENTIFY AND MAKE AVAILABLE FOR DEVELOPMENT ADEQUATE SITES TO MEET THE CITY'S HOUSING NEEDS, ESPECIALLY PERMANENTLY AFFORDABLE HOUSING.

Policy 1.1

Plan for the full range of housing needs in the City and County of San Francisco, especially affordable housing.

The Project is a higher density residential development in an underutilized, transitioning industrial and residential area. The Project site is an ideal infill site that is currently occupied by an industrial use. The Project Site was rezoned to UMU as part of a long range planning goal to create a cohesive, higher density residential and mixed-use neighborhood. The surrounding neighborhood features a wide variety of zoning, including: P (Public); RH-3 (Residential, House, Three-Family); PDR-1-D (Production, Distribution and Repair-Design), and, PDR-1-G (Production, Distribution and Repair-General). The Project will provide 25 affordable units on-site, which will provide immediate opportunities for affordable housing in this area.

OBJECTIVE 4

FOSTER A HOUSING STOCK THAT MEETS THE NEEDS OF ALL RESIDENTS ACROSS LIFECYCLES.

Policy 4.4

Encourage sufficient and suitable rental housing opportunities, emphasizing permanently affordable rental units wherever possible.

The Project meets the affordable housing requirements for the UMU Zoning District by providing 25 affordable units on-site. The Project will provide 20% of the units as affordable. The Project applied and qualifies is providing 20% of the units as Affordable Units. 25 units (5 studios, 10 one-bedroom, 8 two-bedroom, and 2 three-bedroom) of the 127 units provided will be affordable units.

OBJECTIVE 11

SUPPORT AND RESPECT THE DIVERSE AND DISTINCT CHARACTER OF SAN FRANCISCO'S NEIGHBORHOODS.

Policy 11.1

Promote the construction and rehabilitation of well-designed housing that emphasizes beauty, flexibility, and innovative design, and respects existing neighborhood character.

Policy 11.2

Ensure implementation of accepted design standards in project approvals.

Policy 11.3

Ensure growth is accommodated without substantially and adversely impacting existing residential neighborhood character.

Policy 11.4

Continue to utilize zoning districts which conform to a generalized residential land use and density plan and the General Plan.

Policy 11.6

Foster a sense of community through architectural design, using features that promote community interaction.

Policy 11.8

Consider a neighborhood's character when integrating new uses, and minimize disruption caused by expansion of institutions into residential areas.

The architecture of this Project responds to the site's location as a transition between industrial zones and the contemporary and traditional architecture of residential zones. The Project's facades provide a unique expression not commonly found within the surrounding area, while providing for a material palette that draws from the surrounding industrial context. The exterior is designed with modern materials including glass veneer, metal wall panels, and aluminum windows.

RECREATION AND OPEN SPACE ELEMENT

OBJECTIVES AND POLICIES

OBJECTIVE 1:

ENSURE A WELL-MAINTAINED HIGHLY UTILIZED AND INTEGRATED OPEN SPACE SYSTEM.

Policy 1.11:

Encourage private recreational facilities on private land that provide a community benefit, particularly to low and moderate-income residents.

OBJECTIVE 2:

INCREASE RECREATION AND OPEN SPACE TO MEET THE LONG-TERM NEEDS OF THE CITY AND BAY REGION.

Policy 2.3:

Provide recreational programs that are responsive to community needs and changing demographics.

Policy 2.8

Consider repurposing underutilized City-owned properties as open space and recreational facilities.

Policy 2.11:

Assure that privately developed residential open spaces are useable, beautiful and environmentally sustainable.

The Project will create private and common open space areas in a new residential development through private porches, patios and balconies, two courtyards, a solarium, roof deck, and artificial turf area. In addition, the Project is proposing a parklet on 17th Street which would provide a new publically-accessible space across the street from Jackson Playground.

TRANSPORTATION ELEMENT

OBJECTIVES AND POLICIES

OBJECTIVE 24:

IMPROVE THE AMBIENCE OF THE PEDESTRIAN ENVIRONMENT.

Policy 24.2:

Maintain and expand the planting of street trees and the infrastructure to support them.

Policy 24.3:

Install pedestrian-serving street furniture where appropriate.

Policy 24.4:

Preserve pedestrian-oriented building frontages.

The Project will install new street trees along both Arkansas and 17th Streets. Further, the Project will provide new site furnishings, amenities, and planting strips on both frontages. Frontages are designed with active spaces oriented at the pedestrian level.

OBJECTIVE 28:

PROVIDE SECURE AND CONVENIENT PARKING FACILITIES FOR BICYCLES.

Policy 28.1:

Provide secure bicycle parking in new governmental, commercial, and residential developments.

Policy 28.3:

Provide parking facilities which are safe, secure, and convenient.

The Project includes 140 bicycle parking spaces in secure, convenient locations, which meets the requirements specified in the Planning Code.

OBJECTIVE 34:

RELATE THE AMOUNT OF PARKING IN RESIDENTIAL AREAS AND NEIGHBORHOOD COMMERCIAL DISTRICTS TO THE CAPACITY OF THE CITY'S STREET SYSTEM AND LAND USE PATTERNS.

Policy 34.1:

Regulate off-street parking in new housing so as to guarantee needed spaces without requiring excesses and to encourage low auto ownership in neighborhoods that are well served by transit and are convenient to neighborhood shopping.

Policy 34.3:

Permit minimal or reduced off-street parking supply for new buildings in residential and commercial areas adjacent to transit centers and along transit preferential streets.

Policy 34.5:

Minimize the construction of new curb cuts in areas where on-street parking is in short supply and locate them in a manner such that they retain or minimally diminish the number of existing on-street parking spaces.

The Project provides 98 off-street parking spaces, which is the maximum permitted amount of off-street parking. The parking spaces are accessed by one access door measuring 12-ft wide. Parking is adequate for the project and complies with maximums prescribed by the Planning Code.

URBAN DESIGN ELEMENT

OBJECTIVES AND POLICIES

OBJECTIVE 1:

EMPHASIS OF THE CHARACTERISTIC PATTERN WHICH GIVES TO THE CITY AND ITS NEIGHBORHOODS AN IMAGE, A SENSE OF PURPOSE, AND A MEANS OF ORIENTATION.

Policy 1.7:

Recognize the natural boundaries of districts, and promote connections between districts.

OBJECTIVE 2:

CONSERVATION OF RESOURCES WHICH PROVIDE A SENSE OF NATURE, CONTINUITY WITH THE PAST, AND FREEDOM FROM OVERCROWDING.

Policy 2.6:

Respect the character of older development nearby in the design of new buildings.

The Project is located within the Potrero Hill neighborhood, which is characterized by the mix of residential and industrial uses. As such, the Project provides expressive street façades, which respond to the form, scale and material palette of the existing neighborhood, while also providing a new contemporary architectural vocabulary.

OBJECTIVE 4:

IMPROVEMENT OF THE NEIGHBORHOOD ENVIRONMENT TO INCREASE PERSONAL SAFETY, COMFORT, PRIDE AND OPPORTUNITY.

Policy 4.5:

Design walkways and parking facilities to minimize danger to pedestrians.

Policy 4.13:

Improve pedestrian areas by providing human scale and interest.

Although the project site has two street frontages, it only provides one vehicular access points for the entire project, limiting conflicts with pedestrians and bicyclists. Numerous street trees will be planted on Arkansas and 17th Streets, and a publically-accessible dog park will be constructed along 20th Street. Ample frontages, common and private open spaces, and ground floor dwelling units with direct accessing to street will be provided. Along the project site, the pedestrian experience will be greatly improved.

In addition, the Project includes streetscape elements, including an extended bulb out at the corner of Arkansas and 17th Streets. The Project includes streetscape elements, bicycle parking racks, sidewalk planters, street trees, site furnishings, and the widening of Arkansas Street sidewalks by eleven feet to incorporate a planting strip and sidewalk furnishings. Although it would be ideal to widen 17th Street to 15 feet is not in conjunction with future SFMTA plans for widening the street.

SHOWPLACE SQUARE/POTRERO AREA PLAN

OBJECTIVES AND POLICIES

Land Use

OBJECTIVE 1.2

IN AREAS OF SHOWPLACE/POTRERO WHERE HOUSING AND MIXED USE IS ENCOURAGED, MAXIMIZE DEVELOPMENT POTENTIAL IN KEEPING WITH NEIGHBORHOOD CHARACTER.

Policy 1.2.1

Ensure that in-fill housing development is compatible with its surroundings.

Policy 1.2.2

In general, where residential development is permitted, control residential density through building height and bulk guidelines and bedroom mix requirements.

Housing

OBJECTIVE 2.1

ENSURE THAT A SIGNIFICANT PERCENTAGE OF NEW HOUSING CREATED IN THE SHOWPLACE / POTRERO AREA IS AFFORDABLE TO PEOPLE WITH A WIDE RANGE OF INCOMES.

Policy 2.1.1

Require developers in some formally industrial areas to contribute towards the City's very low, low, moderate and middle income needs as identified in the Housing Element of the General Plan.

OBJECTIVE 2.3

REQUIRE THAT A SIGNIFICANT NUMBER OF UNITS IN NEW DEVELOPMENTS HAVE TWO OR MORE BEDROOMS EXCEPT SENIOR HOUSING AND SRO DEVELOPMENTS UNLESS ALL BELOW MARKET RATE UNITS ARE TWO OR MORE BEDROOM UNITS.

Policy 2.3.2

Prioritize the development of affordable family housing, both rental and ownership, particularly along transit corridors and adjacent to community amenities.

Policy 2.3.3

Require that a significant number of units in new developments have two or more bedrooms, except Senior Housing and SRO developments.

Policy 2.3.6

Establish an impact fee to be allocated towards an Eastern Neighborhoods Public Benefit Fund to mitigate the impacts of new development on transit, pedestrian, bicycle, and street improvements, park and recreational facilities, and community facilities such as libraries, child care and other neighborhood services in the area.

OBJECTIVE 2.5

PROMOTE HEALTH THROUGH RESIDENTIAL DEVELOPMENT DESIGN AND LOCATION

Policy 2.5.3

Require new development to meet minimum levels of “green” construction.

Built Form

OBJECTIVE 3.1

PROMOTE AN URBAN FORM THAT REFLECTS SHOWPLACE SQUARE AND POTRERO HILL’S DISTINCTIVE PLACE IN THE CITY’S LARGER FORM AND STRENGTHENS ITS PHYSICAL FABRIC AND CHARACTER.

Policy 3.1.6

New buildings should epitomize the best in contemporary architecture, but should do so with full awareness of, and respect for, the height, mass, articulation and materials of the best of the older buildings that surrounds them.

OBJECTIVE 3.2

PROMOTE AN URBAN FORM AND ARCHITECTURAL CHARACTER THAT SUPPORTS WALKING AND SUSTAINS A DIVERSE, ACTIVE AND SAFE PUBLIC REALM.

Policy 3.2.1

Require high quality design of street-facing building exteriors.

The Project is a new larger-scale residential development. The Project provides the appropriate use encouraged by the Area Plan for this location. In addition, the Project is located within the prescribed height and bulk guidelines. The Project introduces a contemporary architectural vocabulary, which is sensitive to the prevailing scale and neighborhood fabric. The Project provides for a high quality designed exterior, which features a variety of materials, colors and textures, including glass veneer, exposed concrete, and aluminum windows. The Project will also pay the appropriate development impact fees, including the Eastern Neighborhoods Impact Fees.

INTERIM CONTROLS: ADDITIONAL DESIGN STANDARDS FOR LPAS WITHIN THE SHOWPLACE SQUARE, POTRERO HILL, AND CENTRAL WATERFRONT AREA PLANS.

Approvals for a Large Project Authorizations under Planning Code 329 in the Showplace Square, Potrero Hill, or Central Waterfront Area Plans must conform to the existing provisions of the Planning Code and must also demonstrate the following:

1. An awareness of urban patterns, and harmonizes visual and physical relationships between existing buildings, streets, open spaces, natural features, and view corridors;
2. An awareness of neighborhood scale and materials, and renders building facades with texture, detail, and depth; and
3. A modulation of buildings vertically and horizontally, with rooftops and facades designed to be seen from multiple vantage points.

The Project is modulated through articulated private balconies, the residential entrance on Arkansas Street and a prominent base complete with active ground level uses, walk-up residences, and a warmer palate of materials. The glass veneer adds transparency and harmonizes with the visual and physical relationship with Jackson Playground. The exposed concrete, steel, aluminum, and glass materials reference the existing industrial context. Residential scale is accomplished by providing regular twenty-five foot vertical building modules. The Project is adjacent to industrial buildings and will hold the corner with the proposed 48-foot height. By utilizing materials found on the industrial buildings in the area and by providing an activated residential ground floor, the Project provides a transition between lower-scale industrial buildings and the three to four- floor residential buildings further south on Arkansas Street.

9. **Planning Code Section 101.1(b)** establishes eight priority-planning policies and requires review of permits for consistency with said policies. On balance, the project does comply with said policies in that:

- A. That existing neighborhood-serving retail uses be preserved and enhanced and future opportunities for resident employment in and ownership of such businesses be enhanced.

The Project would add new residents, visitors, and employees to the neighborhood, which would assist in strengthening nearby retail uses.

- B. That existing housing and neighborhood character be conserved and protected in order to preserve the cultural and economic diversity of our neighborhoods.

No housing exists on the Project Site. The Project will provide up to 127 new dwelling units, thus resulting in a significant increase in the neighborhood housing stock. The Project is expressive in design, and relates to the scale and form of the surrounding neighborhood by providing relationships to the mid-to-large-scale industrial properties. For these reasons, the proposed Project would protect and preserve the cultural and economic diversity of the neighborhood.

- C. That the City's supply of affordable housing be preserved and enhanced.

The Project will not displace any affordable housing because there is currently no housing on the site. The Project will comply with the City's Inclusionary Housing Program by providing 25 affordable units on-site.

- D. That commuter traffic not impede MUNI transit service or overburden our streets or neighborhood parking.

The Project Site is well-served by public transportation. The Project is located on the 10 and 22 bus lines and one block from the 19. In addition the Project is within nine blocks of the MUNI T-Line Station. The Project is located within walking distance to the 22nd Street Caltrain Station. Future residents would be afforded close proximity to bus or rail transit. The Project also provides sufficient off-street parking and sufficient bicycle parking for residents and their guests.

- E. That a diverse economic base be maintained by protecting our industrial and service sectors from displacement due to commercial office development, and that future opportunities for resident employment and ownership in these sectors be enhanced.

The Project is consistent with the Showplace Square / Potrero Hill Area Plan, which provides for a balance between industrial and residential development. The Project does not include commercial office development, and provides new opportunities for housing, which is a top priority for the City.

- F. That the City achieve the greatest possible preparedness to protect against injury and loss of life in an earthquake.

The Project will be designed and will be constructed to conform to the structural and seismic safety requirements of the Building Code. This Project will not impact the property's ability to withstand an earthquake.

- G. That landmarks and historic buildings be preserved.

There are no landmarks or historic buildings on the project site. The Project would not impact the nearby Showplace Square Heavy Timber and Steel-frame Brick Warehouse and Factory Historic District.

- H. That our parks and open space and their access to sunlight and vistas be protected from development.

The Project will cast additional shadow on the nearby Jackson Playground and will have an effect on a property managed and owned by the Recreation and Parks Commission. As noted in Planning Commission Motion No. XXXXX, the additional shadow cast by the Project would not be adverse to the usability of Jackson Playground.

9. **First Source Hiring.** The Project is subject to the requirements of the First Source Hiring Program as they apply to permits for residential development (Section 83.4(m) of the Administrative Code), and the Project Sponsor shall comply with the requirements of this Program as to all construction work and on-going employment required for the Project. Prior to the issuance of any building permit to construct or a First Addendum to the Site Permit, the Project Sponsor shall have a First Source Hiring Construction and Employment Program approved by the First Source Hiring Administrator, and evidenced in writing. In the event that both the Director of Planning and the First Source Hiring Administrator agree, the approval of the Employment Program may be delayed as needed.

The Project Sponsor submitted a First Source Hiring Affidavit and prior to issuance of a building permit will execute a First Source Hiring Memorandum of Understanding and a First Source Hiring Agreement with the City's First Source Hiring Administration.

10. The Project is consistent with and would promote the general and specific purposes of the Code provided under Section 101.1(b) in that, as designed, the Project would contribute to the character and stability of the neighborhood and would constitute a beneficial development.

11. The Commission hereby finds that approval of the Large Project Authorization would promote the health, safety and welfare of the City.

DECISION

That based upon the Record, the submissions by the Applicant, the staff of the Department and other interested parties, the oral testimony presented to this Commission at the public hearings, and all other written materials submitted by all parties, the Commission hereby **APPROVES Large Project Authorization Application No. 2015-000453~~ENX~~SHD** under Planning Code Section 329 to allow the new construction of a five-story residential building with 127 dwelling units, and an exception to the requirements for: 1) rear yard (Planning Code Section 134); 2) dwelling unit exposure (Planning Code Section 140); 3) off-street loading (Planning Code Section 152.1); and, 4) dwelling unit mix requirements (Planning Code Section 207.6) within the UMU (Urban Mixed-Use) Zoning District, and a 48-X Height and Bulk District. The project is subject to the following conditions attached hereto as "EXHIBIT A" in general conformance with plans on file, dated December 22, 2015, and stamped "EXHIBIT B", which is incorporated herein by reference as though fully set forth, and adopts the MMRP, attached as "Exhibit C", as conditions of approval, incorporated herein as part of this Motion as though fully set forth herein.

APPEAL AND EFFECTIVE DATE OF MOTION: Any aggrieved person may appeal this Section 329 Large Project Authorization to the Board of Appeals within fifteen (15) days after the date of this Motion. The effective date of this Motion shall be the date of adoption of this Motion if not appealed (after the 15-day period has expired) OR the date of the decision of the Board of Appeals if appealed to the Board of Appeals. For further information, please contact the Board of Appeals at (415) 575-6880, 1660 Mission, Room 3036, San Francisco, CA 94103.

Protest of Fee or Exaction: You may protest any fee or exaction subject to Government Code Section 66000 that is imposed as a condition of approval by following the procedures set forth in Government Code Section 66020. The protest must satisfy the requirements of Government Code Section 66020(a) and must be filed within 90 days of the date of the first approval or conditional approval of the development referencing the challenged fee or exaction. For purposes of Government Code Section 66020, the date of imposition of the fee shall be the date of the earliest discretionary approval by the City of the subject development.

If the City has not previously given Notice of an earlier discretionary approval of the project, the Planning Commission's adoption of this Motion, Resolution, Discretionary Review Action or the Zoning Administrator's Variance Decision Letter constitutes the approval or conditional approval of the development and the City hereby gives **NOTICE** that the 90-day protest period under Government Code Section 66020 has begun. If the City has already given Notice that the 90-day approval period has begun for the subject development, then this document does not re-commence the 90-day approval period.

I hereby certify that the Planning Commission ADOPTED the foregoing Motion on March 3, 2016.

Jonas P. Ionin
Commission Secretary

AYES:

Draft Motion
March 3, 2016

CASE NO. 2015-000453~~ENX~~SHD
88 Arkansas Street

NAYS:

ABSENT:

ADOPTED: March 3, 2016

EXHIBIT A

AUTHORIZATION

This authorization is for a Large Project Authorization to allow for the new construction of a five-story residential building with 127 dwelling units, and an exception to the requirements for rear yard, dwelling unit exposure, off-street loading, and dwelling unit mix requirements, located at 88 Arkansas Street, Lots 002 and 002A in Assessor's Block 3953, pursuant to Planning Code Section 329 within the UMU (Urban Mixed-Use) Zoning District, and a 48-X Height and Bulk District; in general conformance with plans, dated December 1, 2014, and stamped "EXHIBIT B" included in the docket for Case No. 2015-000453~~ENX~~SHD and subject to conditions of approval reviewed and approved by the Commission on March 3, 2016 under Motion No. XXXXXX. This authorization and the conditions contained herein run with the property and not with a particular Project Sponsor, business, or operator.

RECORDATION OF CONDITIONS OF APPROVAL

Prior to the issuance of the building permit or commencement of use for the Project the Zoning Administrator shall approve and order the recordation of a Notice in the Official Records of the Recorder of the City and County of San Francisco for the subject property. This Notice shall state that the project is subject to the conditions of approval contained herein and reviewed and approved by the Planning Commission on March 3, 2016 under Motion No. XXXXXX.

PRINTING OF CONDITIONS OF APPROVAL ON PLANS

The conditions of approval under the 'Exhibit A' of this Planning Commission Motion No. XXXXXX shall be reproduced on the Index Sheet of construction plans submitted with the Site or Building permit application for the Project. The Index Sheet of the construction plans shall reference to the Office Development Authorization and any subsequent amendments or modifications.

SEVERABILITY

The Project shall comply with all applicable City codes and requirements. If any clause, sentence, section or any part of these conditions of approval is for any reason held to be invalid, such invalidity shall not affect or impair other remaining clauses, sentences, or sections of these conditions. This decision conveys no right to construct, or to receive a building permit. "Project Sponsor" shall include any subsequent responsible party.

CHANGES AND MODIFICATIONS

Changes to the approved plans may be approved administratively by the Zoning Administrator. Significant changes and modifications of conditions shall require Planning Commission approval of a new authorization.

Conditions of Approval, Compliance, Monitoring, and Reporting

PERFORMANCE

1. **Validity.** The authorization and right vested by virtue of this action is valid for three (3) years from the effective date of the Motion. The Department of Building Inspection shall have issued a Building Permit or Site Permit to construct the project and/or commence the approved use within this three-year period.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

2. **Expiration and Renewal.** Should a Building or Site Permit be sought after the three (3) year period has lapsed, the project sponsor must seek a renewal of this Authorization by filing an application for an amendment to the original Authorization or a new application for Authorization. Should the project sponsor decline to so file, and decline to withdraw the permit application, the Commission shall conduct a public hearing in order to consider the revocation of the Authorization. Should the Commission not revoke the Authorization following the closure of the public hearing, the Commission shall determine the extension of time for the continued validity of the Authorization.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

3. **Diligent Pursuit.** Once a site or Building Permit has been issued, construction must commence within the timeframe required by the Department of Building Inspection and be continued diligently to completion. Failure to do so shall be grounds for the Commission to consider revoking the approval if more than three (3) years have passed since this Authorization was approved.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

4. **Extension.** All time limits in the preceding three paragraphs may be extended at the discretion of the Zoning Administrator where implementation of the project is delayed by a public agency, an appeal or a legal challenge and only by the length of time for which such public agency, appeal or challenge has caused delay.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

5. **Conformity with Current Law.** No application for Building Permit, Site Permit, or other entitlement shall be approved unless it complies with all applicable provisions of City Codes in effect at the time of such approval.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

6. **Mitigation Measures.** Mitigation measures described in the MMRP attached as Exhibit C are necessary to avoid potential significant effects of the proposed project and have been agreed to by the project sponsor. Their implementation is a condition of project approval.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

7. **Additional Project Authorization.** The Project Sponsor must obtain an authorization under Planning Code Section 295 for a project which would cast shadow upon a property under the jurisdiction of the Recreation and Parks Commission, and satisfy all the conditions thereof. The conditions set forth below are additional conditions required in connection with the Project. If these conditions overlap with any other requirement imposed on the Project, the more restrictive or protective condition or requirement, as determined by the Zoning Administrator, shall apply.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

DESIGN – COMPLIANCE AT PLAN STAGE

8. **Final Materials.** The Project Sponsor shall continue to work with Planning Department on the building design. Final materials, glazing, color, texture, landscaping, and detailing shall be subject to Department staff review and approval. The architectural addenda shall be reviewed and approved by the Planning Department prior to issuance.

For information about compliance, contact the Case Planner, Planning Department at 415-558-6378, www.sf-planning.org

9. **Garbage, Composting and Recycling Storage.** Space for the collection and storage of garbage, composting, and recycling shall be provided within enclosed areas on the property and clearly labeled and illustrated on the architectural addenda. Space for the collection and storage of recyclable and compostable materials that meets the size, location, accessibility and other standards specified by the San Francisco Recycling Program shall be provided at the ground level of the buildings.

For information about compliance, contact the Case Planner, Planning Department at 415-558-6378, www.sf-planning.org

10. **Rooftop Mechanical Equipment.** Pursuant to Planning Code 141, the Project Sponsor shall submit a roof plan to the Planning Department prior to Planning approval of the building permit application for each building. Rooftop mechanical equipment, if any is proposed as part of the Project, is required to be screened so as not to be visible from any point at or below the roof level of the subject building.

For information about compliance, contact the Case Planner, Planning Department at 415-558-6378, www.sf-planning.org

11. **Streetscape Plan.** Pursuant to Planning Code Section 138.1, the Project Sponsor shall continue to work with Planning Department staff, in consultation with other City agencies, to refine the design and programming of the Streetscape Plan so that the plan generally meets the standards of the Better Streets Plan and all applicable City standards. The Project Sponsor shall complete final

design of all required street improvements, including procurement of relevant City permits, prior to issuance of first architectural addenda, and shall complete construction of all required street improvements prior to issuance of first temporary certificate of occupancy.

For information about compliance, contact the Case Planner, Planning Department at 415-558-6378, www.sf-planning.org

PARKING AND TRAFFIC

12. **Unbundled Parking.** All off-street parking spaces shall be made available to Project residents only as a separate “add-on” option for purchase or rent and shall not be bundled with any Project dwelling unit for the life of the dwelling units. The required parking spaces may be made available to residents within a quarter mile of the project. All affordable dwelling units pursuant to Planning Code Section 415 shall have equal access to use of the parking as the market rate units, with parking spaces priced commensurate with the affordability of the dwelling unit. Each unit within the Project shall have the first right of refusal to rent or purchase a parking space until the number of residential parking spaces are no longer available. No conditions may be placed on the purchase or rental of dwelling units, nor may homeowner’s rules be established, which prevent or preclude the separation of parking spaces from dwelling units.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

13. **Parking Maximum.** Pursuant to Planning Code Section 151.1, the Project shall provide no more than 98 off-street parking spaces for the 127 dwelling units contained therein.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

14. **Car Share.** Pursuant to Planning Code Section 166, no fewer than one (1) car share space shall be made available, at no cost, to a certified car share organization for the purposes of providing car share services for its service subscribers.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

15. **Bicycle Parking.** Pursuant to Planning Code Sections 155.1, 155.4, and 155.5, the Project shall provide no fewer than **107** Class 1 bicycle parking spaces and **10** Class 2 bicycle parking spaces.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

16. **Managing Traffic During Construction.** The Project Sponsor and construction contractor(s) shall coordinate with the Traffic Engineering and Transit Divisions of the San Francisco Municipal Transportation Agency (SFMTA), the Police Department, the Fire Department, the Planning Department, and other construction contractor(s) for any concurrent nearby Projects to manage traffic congestion and pedestrian circulation effects during construction of the Project.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

PROVISIONS

17. **First Source Hiring.** The Project shall adhere to the requirements of the First Source Hiring Construction and End-Use Employment Program approved by the First Source Hiring Administrator, pursuant to Section 83.4(m) of the Administrative Code. The Project Sponsor shall comply with the requirements of this Program regarding construction work and on-going employment required for the Project.
For information about compliance, contact the First Source Hiring Manager at 415-581-2335, www.onestopSF.org
18. **Eastern Neighborhoods Infrastructure Impact Fee.** Pursuant to Planning Code Section 423 (formerly 327), the Project Sponsor shall comply with the Eastern Neighborhoods Public Benefit Fund provisions through payment of an Impact Fee pursuant to Article 4.
For information about compliance, contact the Case Planner, Planning Department at 415-558-6378, www.sf-planning.org
19. **Transportation Sustainability Fee (TSF).** Pursuant to Planning Code Section 411A, the Project Sponsor shall comply with the Transportation Sustainability Fee provisions through payment of an Impact Fee pursuant to Article 4.
For information about compliance, contact the Case Planner, Planning Department at 415-558-6378, www.sf-planning.org
20. **Child Care In-Lieu Fee.** Pursuant to Planning Code Section 414, the Project Sponsor shall comply with the Child Care In-Lieu Fee provisions through payment of an Impact Fee pursuant to Article 4.
For information about compliance, contact the Case Planner, Planning Department at 415-558-6378, www.sf-planning.org

MONITORING

21. **Enforcement.** Violation of any of the Planning Department conditions of approval contained in this Motion or of any other provisions of Planning Code applicable to this Project shall be subject to the enforcement procedures and administrative penalties set forth under Planning Code Section 176 or Section 176.1. The Planning Department may also refer the violation complaints to other city departments and agencies for appropriate enforcement action under their jurisdiction.
For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org
22. **Revocation Due to Violation of Conditions.** Should implementation of this Project result in complaints from interested property owners, residents, or commercial lessees which are not resolved by the Project Sponsor and found to be in violation of the Planning Code and/or the specific conditions of approval for the Project as set forth in Exhibit A of this Motion, the Zoning Administrator shall refer such complaints to the Commission, after which it may hold a public hearing on the matter to consider revocation of this authorization.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

OPERATION

23. **Garbage, Recycling, and Composting Receptacles.** Garbage, recycling, and compost containers shall be kept within the premises and hidden from public view, and placed outside only when being serviced by the disposal company. Trash shall be contained and disposed of pursuant to garbage and recycling receptacles guidelines set forth by the Department of Public Works.

For information about compliance, contact Bureau of Street Use and Mapping, Department of Public Works at 415-554-.5810, <http://sfdpw.org>

24. **Sidewalk Maintenance.** The Project Sponsor shall maintain the main entrance to the building and all sidewalks abutting the subject property in a clean and sanitary condition in compliance with the Department of Public Works Streets and Sidewalk Maintenance Standards.

For information about compliance, contact Bureau of Street Use and Mapping, Department of Public Works, 415-695-2017, <http://sfdpw.org>

25. **Community Liaison.** Prior to issuance of a building permit to construct the project and implement the approved use, the Project Sponsor shall appoint a community liaison officer to deal with the issues of concern to owners and occupants of nearby properties. The Project Sponsor shall provide the Zoning Administrator with written notice of the name, business address, and telephone number of the community liaison. Should the contact information change, the Zoning Administrator shall be made aware of such change. The community liaison shall report to the Zoning Administrator what issues, if any, are of concern to the community and what issues have not been resolved by the Project Sponsor.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

26. **Lighting.** All Project lighting shall be directed onto the Project site and immediately surrounding sidewalk area only, and designed and managed so as not to be a nuisance to adjacent residents. Nighttime lighting shall be the minimum necessary to ensure safety, but shall in no case be directed so as to constitute a nuisance to any surrounding property.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

INCLUSIONARY HOUSING

27. **Number of Required Units.** Pursuant to Planning Code Section 415.6, the Project is required to provide 14.4% of the proposed dwelling units as affordable to qualifying households. To meet the threshold for Priority Processing, the Project has elected to provide 20% of the units as Affordable Units in order to receive Priority Processing under Planning Director Bulletin #2 Planning Department Priority Application Processing Guidelines, which provides Priority Processing for Projects providing 20% of the proposed dwelling units as affordable to qualifying households which must meet or exceed the standards set forth in Planning Code Section 415. Therefore, as a

condition of approval and a condition of receiving Priority Processing, 20% of the units will be provided as affordable. The Project contains 127 units; therefore, 25 affordable units will be provided. The Project Sponsor will fulfill this requirement by providing the 25 affordable units on-site. If the number of market-rate units change, the number of required affordable units shall be modified accordingly with written approval from Planning Department staff in consultation with the Mayor's Office of Housing and Community Development ("MOHCD").

For information about compliance, contact the Case Planner, Planning Department at 415-558-6378, www.sf-planning.org or the Mayor's Office of Housing and Community Development at 415-701-5500, www.sf-moh.org.

28. **Unit Mix.** The Project contains 25 studios, 51 one-bedroom, 41 two-bedroom, and 10 three-bedroom units; therefore, the required affordable unit mix is 5 studios, 10 one-bedroom, 8 two-bedroom, and 2 three-bedroom units. If the market-rate unit mix changes, the affordable unit mix will be modified accordingly with written approval from Planning Department staff in consultation with MOHCD.

For information about compliance, contact the Case Planner, Planning Department at 415-558-6378, www.sf-planning.org or the Mayor's Office of Housing and Community Development at 415-701-5500, www.sf-moh.org.

29. **Unit Location.** The affordable units shall be designated on a reduced set of plans recorded as a Notice of Special Restrictions on the property prior to the issuance of the first construction permit.

For information about compliance, contact the Case Planner, Planning Department at 415-558-6378, www.sf-planning.org or the Mayor's Office of Housing and Community Development at 415-701-5500, www.sf-moh.org.

30. **Phasing.** If any building permit is issued for partial phasing of the Project, the Project Sponsor shall have designated not less than twenty percent (20%) of the each phase's total number of dwelling units as on-site affordable units.

For information about compliance, contact the Case Planner, Planning Department at 415-558-6378, www.sf-planning.org or the Mayor's Office of Housing and Community Development at 415-701-5500, www.sf-moh.org.

31. **Duration.** Under Planning Code Section 415.8, all units constructed pursuant to Section 415.6 or 419, must remain affordable to qualifying households for the life of the project.

For information about compliance, contact the Case Planner, Planning Department at 415-558-6378, www.sf-planning.org or the Mayor's Office of Housing and Community Development at 415-701-5500, www.sf-moh.org.

32. **Other Conditions.** The Project is subject to the requirements of the Inclusionary Affordable Housing Program under Section 415 et seq. of the Planning Code and the terms of the City and County of San Francisco Inclusionary Affordable Housing Program Monitoring and Procedures Manual ("Procedures Manual"). The Procedures Manual, as amended from time to time, is

incorporated herein by reference, as published and adopted by the Planning Commission, and as required by Planning Code Section 415. Terms used in these conditions of approval and not otherwise defined shall have the meanings set forth in the Procedures Manual. A copy of the Procedures Manual can be obtained at the Mayor's Office of Housing ("MOH") at 1 South Van Ness Avenue or on the Planning Department or Mayor's Office of Housing's websites, including on the internet at: <http://sf-planning.org/Modules/ShowDocument.aspx?documentid=4451>. As provided in the Inclusionary Affordable Housing Program, the applicable Procedures Manual is the manual in effect at the time the subject units are made available for sale or rent.

- a. The affordable unit(s) shall be designated on the building plans prior to the issuance of the first construction permit by the Department of Building Inspection ("DBI"). The affordable unit(s) shall (1) reflect the unit size mix in number of bedrooms of the market rate units, (2) be constructed, completed, ready for occupancy and marketed no later than the market rate units, and (3) be evenly distributed throughout the building; and (4) be of comparable overall quality, construction and exterior appearance as the market rate units in the principal project. The interior features in affordable units should be generally the same as those of the market units in the principal project, but need not be the same make, model or type of such item as long they are of good and new quality and are consistent with then-current standards for new housing. Other specific standards for on-site units are outlined in the Procedures Manual.
- b. If the units in the building are offered for rent, the affordable unit(s) shall be rented to qualifying households, as defined in the Procedures Manual, whose gross annual income, adjusted for household size, does not exceed an average fifty-five (55) percent of Area Median Income under the income table called "Maximum Income by Household Size derived from the Unadjusted Area Median Income for HUD Metro Fair Market Rent Area that contains San Francisco." The initial and subsequent rent level of such units shall be calculated according to the Procedures Manual. Limitations on (i) occupancy; (ii) lease changes; (iii) subleasing, and; are set forth in the Inclusionary Affordable Housing Program and the Procedures Manual.
- c. The Project Sponsor is responsible for following the marketing, reporting, and monitoring requirements and procedures as set forth in the Procedures Manual. MOHCD shall be responsible for overseeing and monitoring the marketing of affordable units. The Project Sponsor must contact MOHCD at least six months prior to the beginning of marketing for any unit in the building.
- d. Required parking spaces shall be made available to renters of affordable units according to the Procedures Manual.
- e. Prior to the issuance of the first construction permit by DBI for the Project, the Project Sponsor shall record a Notice of Special Restriction on the property that contains these conditions of approval and a reduced set of plans that identify the affordable units satisfying the requirements of this approval. The Project Sponsor shall promptly provide

a copy of the recorded Notice of Special Restriction to the Department and to MOHCD or its successor.

- f. The Project Sponsor has demonstrated that it is eligible for the On-site Affordable Housing Alternative under Planning Code Section 415.6 instead of payment of the Affordable Housing Fee, and has submitted the Affidavit of Compliance with the Inclusionary Affordable Housing Program: Planning Code Section 415 to the Planning Department stating the intention to enter into an agreement with the City to qualify for a waiver from the Costa-Hawkins Rental Housing Act based upon the proposed density bonus and concessions (as defined in California Government Code Section 65915 et seq.) provided herein. The Project Sponsor has executed the Costa Hawkins agreement and will record a Memorandum of Agreement prior to issuance of the first construction document or must revert payment of the Affordable Housing Fee.
- g. If the Project Sponsor fails to comply with the Inclusionary Affordable Housing Program requirement, the Director of DBI shall deny any and all site or building permits or certificates of occupancy for the development project until the Planning Department notifies the Director of compliance. A Project Sponsor's failure to comply with the requirements of Planning Code Section 415 et seq. shall constitute cause for the City to record a lien against the development project and to pursue any and all available remedies at law.
- h. If the Project becomes ineligible at any time for the On-site Affordable Housing Alternative, the Project Sponsor or its successor shall pay the Affordable Housing Fee prior to issuance of the first construction permit. If the Project becomes ineligible after issuance of its first construction permit, the Project Sponsor shall notify the Department and MOHCD and pay interest on the Affordable Housing Fee and penalties, if applicable.

For information about compliance, contact the Case Planner, Planning Department at 415-558-6378, www.sf-planning.org or the Mayor's Office of Housing and Community Development at 415-701-5500, www.sf-moh.org.



SAN FRANCISCO PLANNING DEPARTMENT

Subject to: (Select only if applicable)

- | | |
|---|---|
| <input checked="" type="checkbox"/> Affordable Housing (Sec. 415) | <input checked="" type="checkbox"/> First Source Hiring (Admin. Code) |
| <input type="checkbox"/> Jobs Housing Linkage Program (Sec. 413) | <input type="checkbox"/> Child Care Requirement (Sec. 414) |
| <input type="checkbox"/> Downtown Park Fee (Sec. 412) | <input checked="" type="checkbox"/> Other |

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Planning Commission Motion No. XXXXX

HEARING DATE: MARCH 3, 2016

Date: February 22, 2016
Case No.: **2015-000453ENXSHD**
Project Address: **88 ARKANSAS STREET**
Zoning: UMU (Urban Mixed-Use) Zoning District
48-X Height and Bulk District
Block/Lot: 3953/002 and 3953/002A
Project Sponsor: Julie Heinzler, Martin Building Company
14 Mint Plaza, 5th Floor
San Francisco, CA 94103
Staff Contact: Kate Conner – (415) 575-6914
kate.conner@sfgov.org

ADOPTING FINDINGS, WITH THE RECOMMENDATION OF THE GENERAL MANAGER OF THE RECREATION AND PARK DEPARTMENT, IN CONSULTATION WITH THE RECREATION AND PARK COMMISSION, THAT NET NEW SHADOW ON JACKSON PLAYGROUND BY THE PROPOSED PROJECT AT 88 ARKANSAS STREET WOULD NOT BE ADVERSE TO THE USE OF JACKSON PLAYGROUND.

PREAMBLE

Under Planning Code Section ("Section") 295, a building permit application for a project exceeding a height of 40 feet cannot be approved if there is any shadow impact on a property under the jurisdiction of the Recreation and Park Department, unless the Planning Commission, upon recommendation from the General Manager of the Recreation and Park Department, in consultation with the Recreation and Park Commission, makes a determination that the shadow impact will not be significant or adverse.

On February 7, 1989, the Recreation and Park Commission and the Planning Commission adopted criteria establishing absolute cumulative limits for additional shadows on fourteen parks throughout San Francisco (Planning Commission Resolution No. 11595).

Jackson Playground is located on Lot 001 in Assessor's Block 3981, is generally bounded by 17th Street to the north, Arkansas Street to the east, Mariposa Street to the south, and Carolina Street to the west. Jackson Playground measures approximately 191,999 square feet and is characterized by two baseball diamonds, a small recreation/restroom building, a playground, a basketball court, a tennis court, a Learning Garden, and a picnic area. The neighborhood immediately surrounding Jackson Playground is characterized by one- and two-story buildings, typically of non-residential use. The neighborhood

encompassing Jackson Playground is part of the Eastern Neighborhoods Area Plan and is envisioned, generally, for increased building heights and residential density.

On an annual basis, the Theoretically Available Annual Sunlight ("TAAS") on Jackson Playground (with no adjacent structures present) is approximately 714,508,800 square-foot-hours of sunlight. Existing structures in the area cast shadows on Jackson Playground that total approximately 46,306,396 square-foot hours, or approximately 6.481 percent of the TAAS.

On March 25, 2015 Julie Heinzler (hereinafter "Project Sponsor") filed an application with the Planning Department (hereinafter "Department") for a Shadow Impact Study and on April 1, 2015, the Project Sponsor filed an application with the Department for a Large Project Authorization on the property at 88 Arkansas Street, west side of Arkansas Street between 16th Street and 17th Street; Lots 002 and 002A in Assessor's Block 3953, (hereinafter "Subject Property") to construct a five-story mixed-use building (hereinafter "the Project") at this site. The Project is located within an UMU (Urban Mixed Use) Zoning District a 48-X Height and Bulk District.

A technical memorandum, prepared by Prevision Design, was finalized on January 13, 2016, analyzing the potential shadow impacts of the Project to properties under the jurisdiction of the Recreation and Parks Department (Case No. 2015-000453ENXSHD). The memorandum concluded that the Project would cast approximately 19,308 square-foot-hours of new shadow on Jackson Playground, equal to approximately 0.003 percent of the theoretically available annual sunlight ("TAAS") on Jackson Playground.

On January 21, 2016, the Recreation and Park Commission conducted a duly noticed public hearing at a regularly scheduled meeting and recommended that the Planning Commission find that the shadows cast by the Project on Jackson Playground will not be adverse to the use of Jackson Playground.

The Planning Commission has reviewed and considered reports, studies, plans and other documents pertaining to the Project.

The Planning Commission has heard and considered the testimony presented at the public hearing and has further considered the written materials and oral testimony presented on behalf of the Project Sponsor, Department staff, and other interested parties.

FINDINGS

Having reviewed the materials identified in the recitals above, and having heard all testimony and arguments, this Commission finds, concludes, and determines as follows:

1. The foregoing recitals are accurate, and also constitute findings of this Commission.
2. The additional shadow cast by the Project, while numerically significant, would not be adverse, and is not expected to interfere with the use of the Park, for the following reasons:
 - a. The proposed project would reduce the annual available insolation by about 0.003 percent (a reduction of 19,308 square foot hours of sunlight). This results in a total shadow load of 46,325,704 square foot hours and a reduction of the available insolation to 6.484 percent.

- b. Although the additional shadow cast by the proposed project has a numerically significant effect, the magnitude of the additional shadow is well below one tenth of one percent, and amounts to a reasonable and extremely small loss of sunlight for a park in an area of slated for increased building heights and residential density.
 - c. The net new shadow cast upon Jackson Playground from the Project occurs within the first 12 minutes and the last 20 minutes of time for which Proposition K is concerned.
 - d. The net new shadow cast is relatively small in area in comparison to the size of Jackson Playground and at its greatest extent never exceeds 0.021 percent of the area of Jackson Playground. The average duration of the net new shadow is 15 minutes and never exceeds 27 minutes.
3. A determination by the Planning Commission and the Recreation and Park Commission to allocate net new shadow to the Project does not constitute an approval of the Project.

DECISION

That based upon the Record, the submissions by the Project Sponsor, the staff of the Planning Department, the recommendation of the General Manager of the Recreation and Park Department, in consultation with the Recreation and Park Commission, and other interested parties, the oral testimony presented to the Planning Commission at the public hearing, and all other written materials submitted by all parties, the Planning Commission hereby DETERMINES, under Shadow Analysis Application No. **2015-000453ENXSHD**, that the net new shadow cast by the Project on Jackson Playground will not be adverse to the use of Jackson Playground.

I hereby certify that the foregoing Motion was ADOPTED by the Planning Commission at its regular meeting on March 3, 2016.

Jonas P. Ionin
Commission Secretary

AYES:

NAYES:

ABSENT:

ADOPTED: March 3, 2016

RECREATION AND PARK COMMISSION
City and County of San Francisco
Resolution No. 1601-003

RESOLUTION RECOMMENDING TO THE PLANNING COMMISSION THAT THE NEW SHADOW CAST BY THE PROPOSED PROJECT AT 88 ARKANSAS STREET WILL NOT HAVE AN ADVERSE IMPACT ON THE USE OF JACKSON PLAYGROUND, AS REQUIRED BY PLANNING CODE SECTION 295 (THE SUNLIGHT ORDINANCE).

WHEREAS, Under Planning Code Section 295, a building permit application for a project exceeding a height of 40 feet cannot be approved by the Planning Commission if there is any adverse shadow impact on a property under the jurisdiction of the Recreation and Park Commission, unless the Planning Commission, upon recommendation from the General Manager of the Recreation and Park Department in consultation with the Recreation and Park Commission, makes a determination that the shadow impact will not be significant; and

WHEREAS, The Recreation and Park Commission has jurisdiction over real property located on lot 001 of Assessor's Block 3981 known as Jackson Playground ("Park"); and

WHEREAS, Martin Building Company ("Project Sponsor") proposes to construct one five-story, 48-foot tall mixed-use buildings, containing 127 residential units and ground floor commercial space ("Project"); and

WHEREAS, PreVision Design analyzed the new shadow cast by the proposed Project on the Park and determined that the Theoretical Annual Available Sunlight ("TAAS") for the Park is 714,508,800 shadow-foot hours (sfh). The amount of shadow currently cast on the Park by existing buildings constitutes 6.481% of the TAAS for the Park. The additional shadow cast by the Project would constitute 0.003% of TAAS, bringing the total annual shading of the Park as a percentage of TAAS to 6.484%; and

WHEREAS, The Planning Department is responsible for conducting environmental review for the project. The Department completed a Community Plan Exemption (CPE) Checklist to evaluate whether the environmental impacts of the proposed project were addressed in the Programmatic Environmental Impact Report (PEIR) for the Eastern Neighborhoods Rezoning and Area Plans; and

WHEREAS, The Planning Department determined that the proposed project would not result in new, significant environmental effects, or effects of greater severity than were already analyzed and disclosed in the Eastern Neighborhoods PEIR; and

WHEREAS, the Project will provide public benefits to the city, including 25 on-site below-market-rate units and contributions of approximately \$700,000-800,000 in Eastern Neighborhoods Impact Fees; and

WHEREAS, There are other proposed development projects in the vicinity of Jackson Playground that have undergone design review with the Planning Department such that their final form and massing have been reasonably established. The cumulative shading increase from these projects would bring the annual shading total to 6.951% of TAAS, an increase of 0.470%. The shading from the Project would at no time overlap with shading from these cumulative projects; and

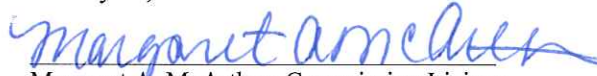
WHEREAS, The Commission finds that the additional shadow cast by the Project will not have a significant adverse impact on the use of Jackson Playground for the following reasons: (1) all of the new shadow cast by the Project would be cast in areas with existing mature tree cover; (2) the shadows would be cast in the early morning and early evening, when use of the park, particularly in the areas of new shadow, is lower relative to other times of the day; and (3) the duration of the new shadow is brief, averaging 15 minutes, and occurs for 69 days of the year; now therefore, be it

RESOLVED, That this Commission does recommend that the Planning Commission find that the shadow cast by the proposed project at 88 Arkansas will not have a significant adverse impact on the use of Jackson Playground Park, pursuant to Planning Code Section 295 (the Sunlight Ordinance).

Adopted by the following vote:

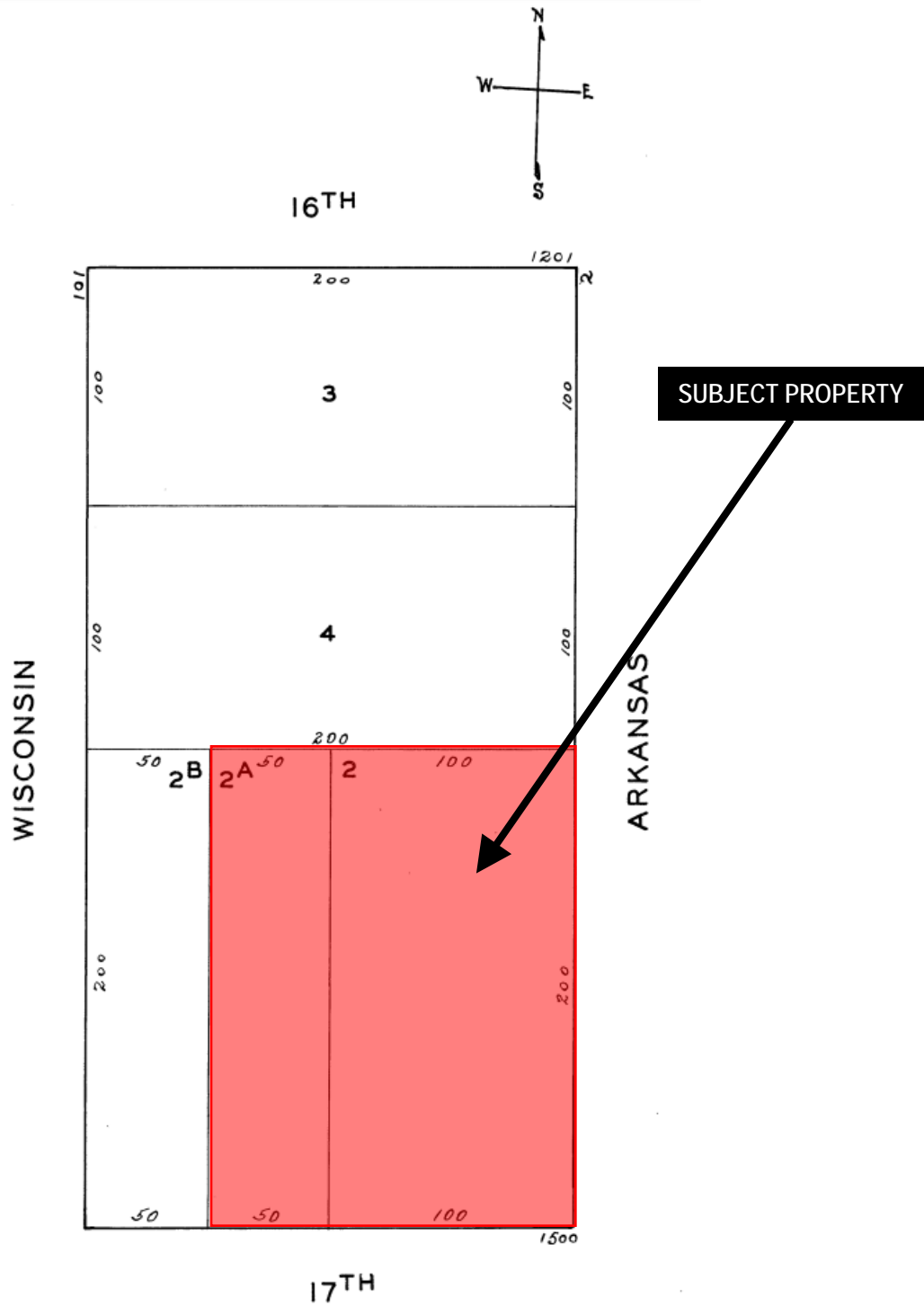
Ayes	5
Noes	0
Absent	1

I hereby certify that the foregoing resolution was
adopted by the Recreation and Park meeting of
January 21, 2016.

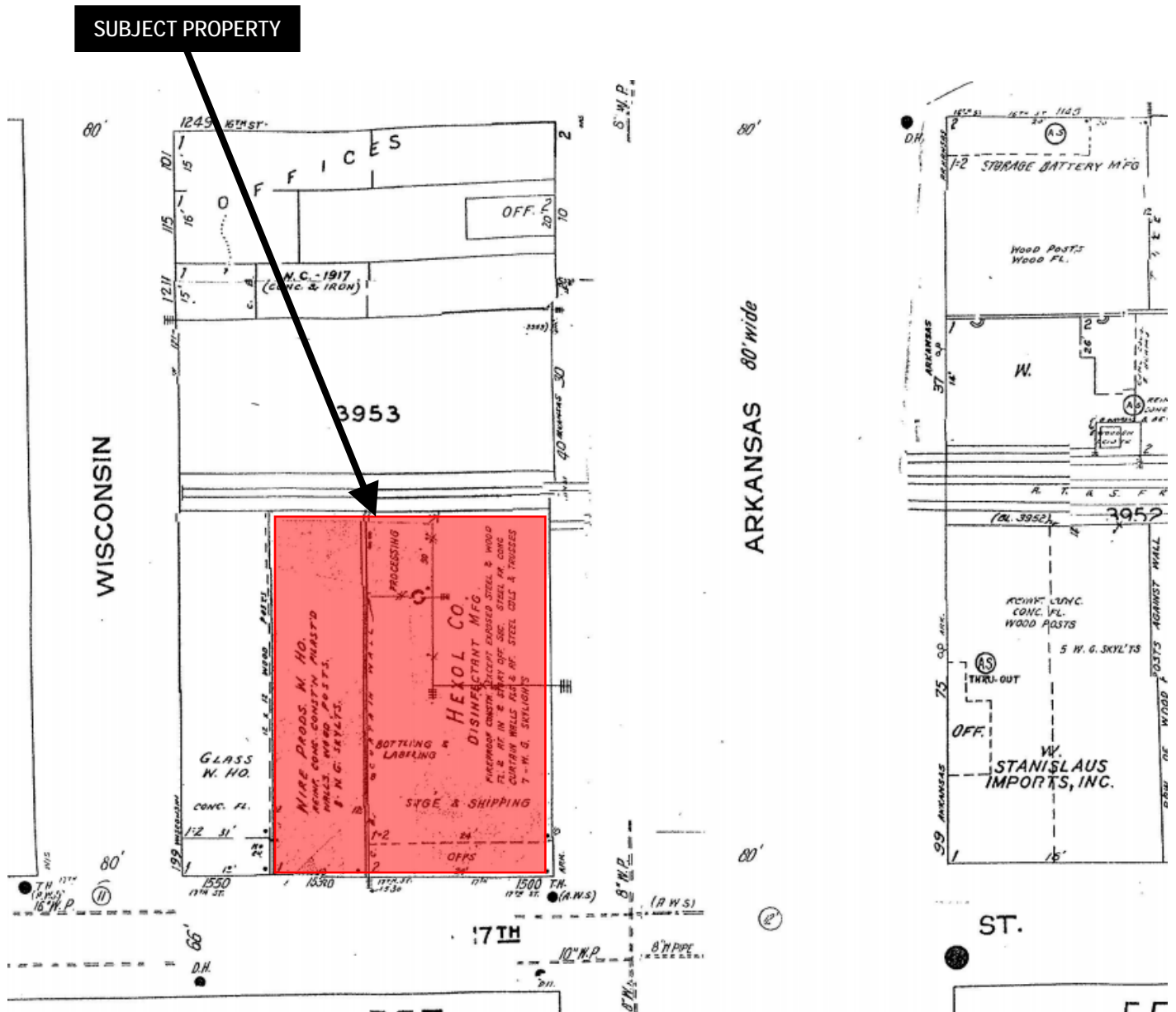


Margaret A. McArthur, Commission Liaison

Parcel Map



Sanborn Map*



**The Sanborn Maps in San Francisco have not been updated since 1998, and this map may not accurately reflect existing conditions.*



SAN FRANCISCO
PLANNING DEPARTMENT

Large Project Authorization Hearing
Case Number 2015-000453ENXSHD
 88 Arkansas Street
 3953/002 and 002A

Aerial Photo



SUBJECT PROPERTY



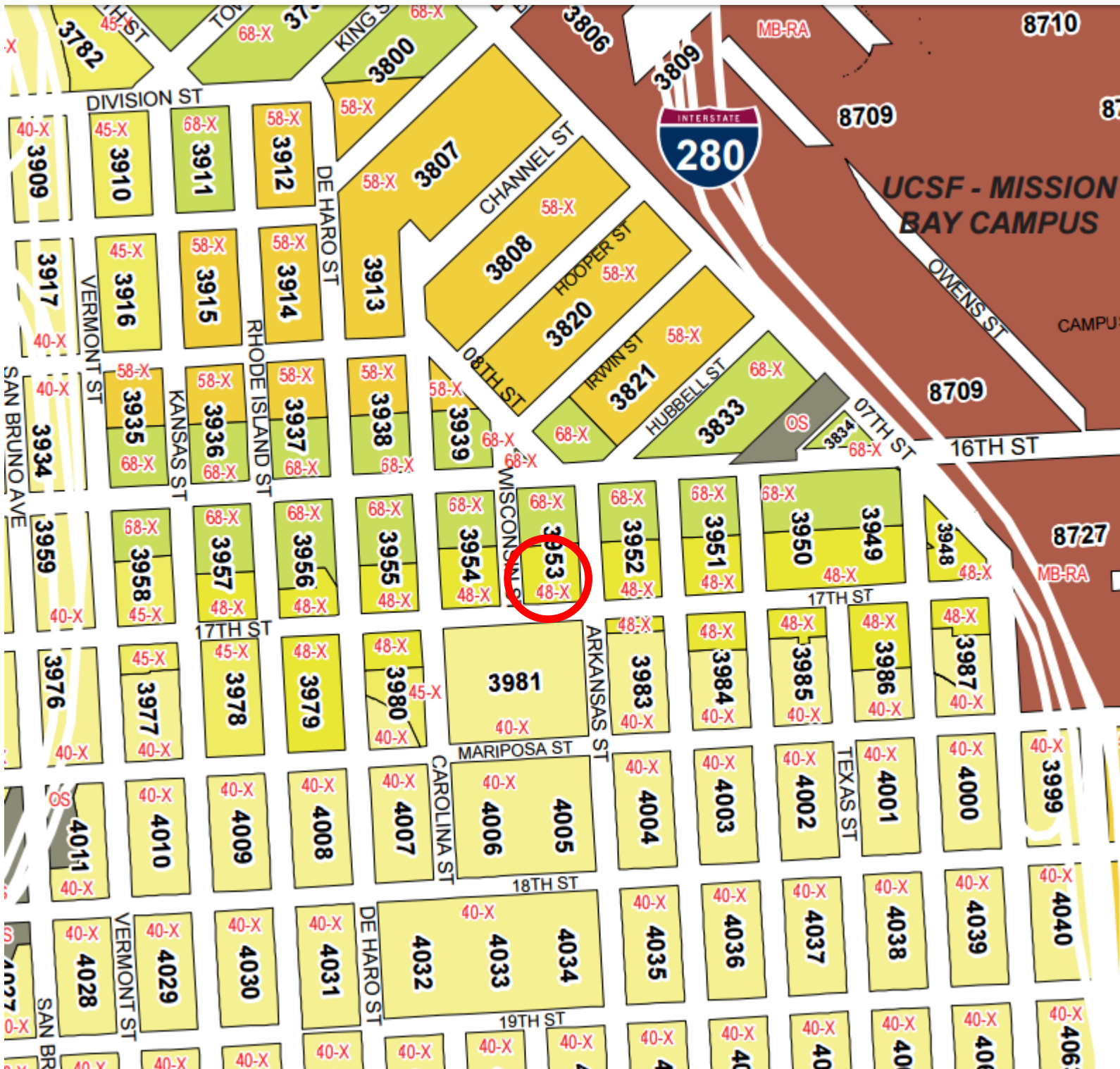
Large Project Authorization Hearing
Case Number 2015-000453 ENXSHD
88 Arkansas Street
3953/002 and 002A

Zoning Map



Large Project Authorization Hearing
 Case Number 2015-000453ENXSHD
 88 Arkansas Street
 3953/002 and 002A

Height Map



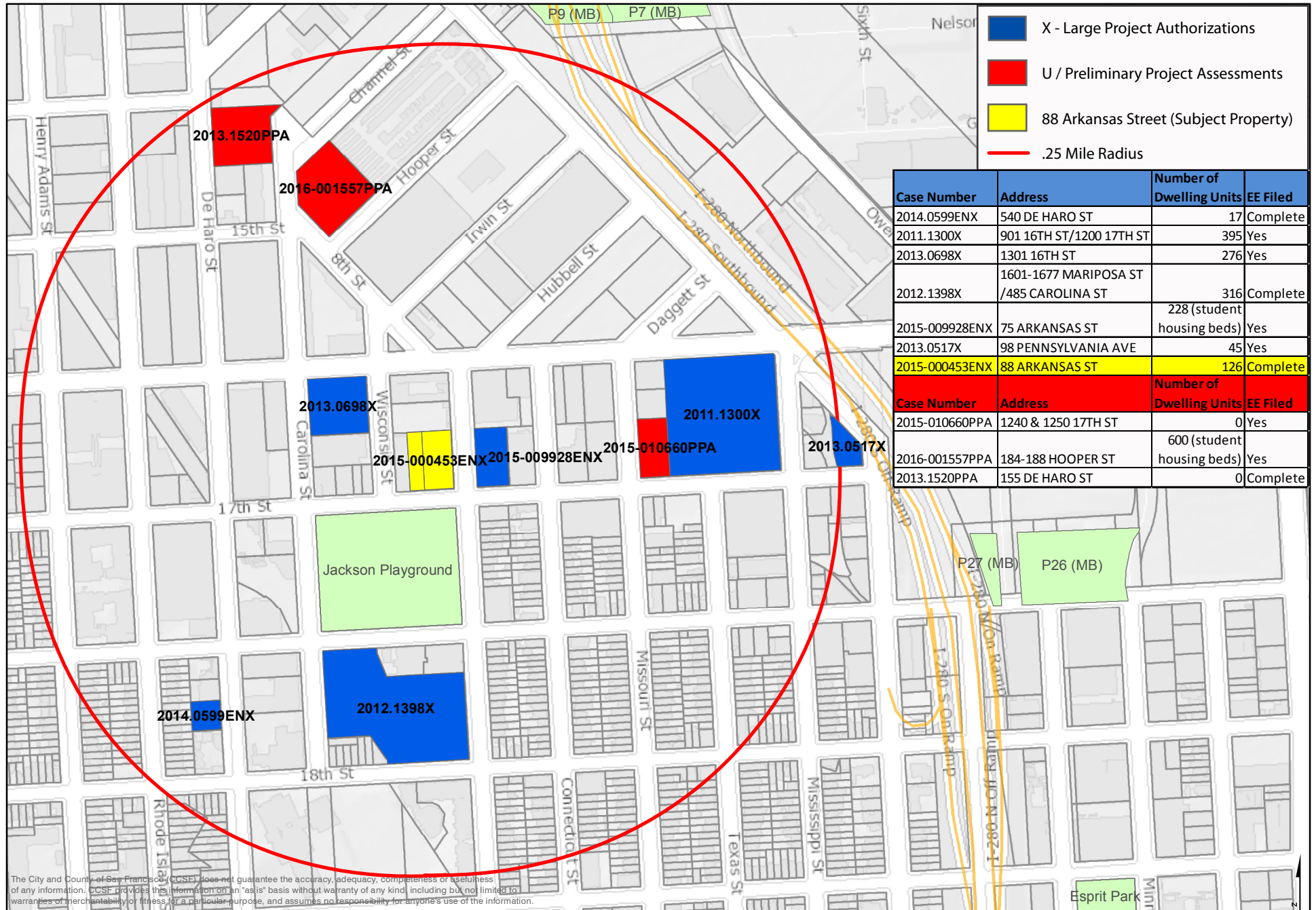
Large Project Authorization Hearing
 Case Number 2015-000453ENXSHD
 88 Arkansas Street
 3953/002 and 002A

Site Photo



Large Project Authorization Hearing
Case Number 2015-000453 ENXSHD
88 Arkansas Street
3953/002 and 002A

Major Projects within .25 Mile Radius of 88 Arkansas Street



The City and County of San Francisco (CCSF) does not guarantee the accuracy, adequacy, completeness or usefulness of any information. CCSF provides this information on an "as is" basis without warranty of any kind, including but not limited to warranties of merchantability or fitness for a particular purpose, and assumes no responsibility for anyone's use of the information.

0 190 380 760 Feet



SAN FRANCISCO PLANNING DEPARTMENT

Certificate of Determination EXEMPTION FROM ENVIRONMENTAL REVIEW

Case No.: 2015-000453ENV
Project Address: 88 Arkansas Street
Zoning: UMU (Urban Mixed-Use) Zoning District
48-X Height & Bulk District
Block/Lot: 3953/002 & 002A
Lot Size: 29,998 square feet
Plan Area: Eastern Neighborhoods Area Plan (Showplace Square/Potrero Hill)
Project Sponsor: Julie Heinzler, Martin Building Company, (415) 348-4644
Staff Contact: Jenny Delumo – (415) 575-9146, Jenny.Delumo@sfgov.org

1650 Mission St.
Suite 400
San Francisco,
CA 94103-2479

Reception:
415.558.6378

Fax:
415.558.6409

Planning
Information:
415.558.6377

PROJECT DESCRIPTION

The project site is an approximately 30,000-square-foot (sf) area located in the Potrero Hill neighborhood and within the Showplace Square/Potrero Hill subarea of the Eastern Neighborhoods Area Plan. The project site is comprised of two adjacent lots: 88 Arkansas Street/1500 17th Street (Lot 002) and 1530 17th Street (Lot 002A). The lots are located on Assessor's Block 3953, which is bounded by 17th Street to the south, 16th Street to the north, Arkansas Street to the east, and Wisconsin Street to the west. The project site currently contains two industrial buildings. Lot 002A is developed with an approximately 9,485-gross-square-foot (gsf), 19-foot-tall, one-story-with-mezzanine industrial building. The building was constructed in 1923 and is currently vacant. Lot 002 is developed with an approximately 16,075-gsf, 24-foot-tall, two-story industrial building and a 4,440-sf surface parking lot, which provides space for approximately 13 vehicles. The rearmost portion of the building is approximately 31 feet tall. The building was constructed in 1906. Approximately 6,000 sf of the building on Lot 002 is currently occupied by Volta Industries. Collectively, the buildings provide approximately 25,560 gsf of Production, Distribution, and Repair (PDR) space.

(Continued on next page.)

EXEMPT STATUS

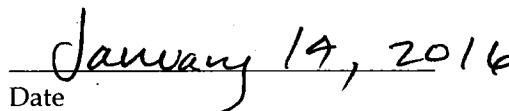
Exempt per Section 15183 of the California Environmental Quality Act (CEQA) Guidelines and California Public Resources Code Section 21083.3

DETERMINATION

I do hereby certify that the above determination has been made pursuant to State and Local requirements.


SARAH B. JONES

Environmental Review Officer


Date January 19, 2016

cc: Julie Heinzler, Project Sponsor; Supervisor Malia Cohen, District 10; Kate Conner, Current Planning Division; Virna Byrd, M.D.F.; Exemption/Exclusion File

PROJECT DESCRIPTION (continued)

Vehicular access to the buildings on the project site is provided by three curbs cuts: two curb cuts along the 17th Street frontage (approximately 14 feet and 18 feet wide) and one curb cut along the Arkansas Street frontage (approximately 38 feet wide). An approximately 42-foot-wide curb cut, located on Arkansas Street, is inactive and has been painted white. It currently serves as two, perpendicular on-street parking spaces. Access to the surface parking lot is provided via an approximately 38-foot-wide curb cut off Arkansas Street. Approximately four parallel parking spaces are located along the 17th Street frontage and approximately 15 perpendicular parking spaces are provided along the Arkansas Street frontage. There are no on-street loading spaces.

The proposed project would demolish the two, one-story-over-basement buildings and surface parking lot on the project site. The sponsor proposes to construct an approximately 143,990-gsf, five-story mixed-use building. The proposed building would be approximately 48 feet tall (up to 65 feet tall with staircase and elevator penthouses) and include approximately 111,695 gsf of residential space and 3,275 gsf of retail space. An approximately 29,020-gsf, below-grade parking garage would provide off-street parking for about 98 vehicles, including one car share space and two Americans with Disabilities Act (ADA) - accessible spaces. The project would provide 127 Class I bicycle parking spaces and 1 Class II bicycle parking space in the garage, which would be accessed via an approximately 12-foot-wide curb cut on Arkansas Street that would be created by reducing an existing, roughly 42-foot-wide curb cut. Approximately 12 Class II bicycle spaces would be placed on the southwest corner of the project site.

The residential component of the proposed project would include approximately 25 studio apartments, 51 one-bedroom apartments, 41 two-bedroom apartments, and 10 three-bedroom apartments, for a total of 127 residential dwelling units. Twenty-five of the dwelling units (twenty percent) would be designated affordable units. Two entrances to the residential portion of the building would be located along the Arkansas Street frontage. On the ground floor, five of the residential units fronting Arkansas Street would have direct street access. The retail component of the proposed project would be located on the ground floor with pedestrian access provided via three entrances along the 17th Street frontage. Proposed open space would be comprised of private balconies, two interior courtyards on the ground floor, an approximately 1,295-sf solarium on the fifth floor, and an approximately 2,515-sf artificial turf area and 2,315-sf deck on the roof. The approximately 185-sf north courtyard would be located in the northwest corner of the project site and the approximately 2,790-sf south courtyard would be located along the western perimeter of the project site.

A new bulb-out extension is proposed for the northwest corner of the project site at the 17th Street and Arkansas Street intersection. The bulb-out would not extend into traffic lanes. A fire hydrant exists at this location, and may need to be relocated in order to accommodate the proposed bulb-out. The project also proposes to widen the portion of the sidewalk along Arkansas Street not included in the bulb-out to approximately 24 feet wide. Roughly six on-street parallel parking spaces would be provided on 17th Street and four on-street parallel parking spaces would be provided on Arkansas Street, one of the spaces on Arkansas Street would be ADA accessible. On-street loading space would be provided by an approximately 45-foot-long loading zone located between the parallel parking spaces and garage entrance on Arkansas Street. The on-street parking spaces that would be located along 17th Street would be created by removing a 14-foot-long curb cut and an 18-foot-long curb cut, for a total of 32 feet of restored curb length. The on-street parking and loading spaces that would be located along Arkansas Street would be created by reducing a 42-foot-long curb cut to 24 feet in length.

Approximately 18,000 cubic yards of excavation, to a maximum depth of approximately 20 feet below grade, is proposed in order to accommodate the garage. Construction activities would last approximately 19 months. The project site includes approximately eight street trees along the Arkansas Street frontage and four street trees along the 17th Street frontage. The proposed project would retain all of the existing street trees on the project site and provide an additional three street trees on 17th Street and two street trees on Arkansas Street. The proposed project is also seeking Leadership in Energy and Environmental Design (LEED)® for Homes Platinum certification from the U.S. Green Building Council.

Project Approval

The proposed project would require approval of a Large Project Authorization (LPA) by the Planning Commission, pursuant to *Planning Code* Section 329. LPAs are required for new construction greater than 25,000 gross square feet in Urban Mixed Use Zoning (UMU) Districts. The LPA would also authorize project-specific modifications to the following *Planning Code* requirements:

- Rear Yard (*Planning Code* Section 134)
- Exposure (*Planning Code* Section 140)
- Loading (*Planning Code* Section 152.1)
- Dwelling Unit Mix (*Planning Code* Section 207.6; Interpretation of *Planning Code* Section 102.29)

Other approvals that would be required in order to implement the proposed project include the following:

- **Department of Building Inspection (DBI).** Approval of demolition, grading, building and occupancy permits for demolition of the existing structures and new construction.
- **Department of Public Health (DPH).** Approval of a Site Mitigation Plan pursuant to the Maher Ordinance prior to the commencement of any excavation work, and approval of a Soil Mitigation Plan and Dust Control Plan prior to construction-period activities.
- **San Francisco Fire Department (SFFD).** Approval of any changes to the location of existing fire hydrants and water valves.
- **San Francisco Municipal Transportation Agency (SFMTA).** Approval of all proposed changes in curb cuts and parking zones pursuant to the SFMTA Color Curb Program. Coordination with the SFMTA Interdepartmental Staff Committee on Traffic and Transportation to coordinate temporary construction-related changes to the transportation network.
- **San Francisco Public Works Department (Public Works), Bureau of Streets and Mapping.** Approval of modifications to public sidewalks, street trees, curb cuts, and bulb out extensions.
- **San Francisco Public Utilities Commission (SFPUC).** Approval of a stormwater control plan and an erosion and sediment control plan prior to commencing construction.

Approval of the Large Project Authorization by the Planning Commission would be the Approval Action for the project. The Approval Action date establishes the start of the 30-day appeal period for this CEQA exemption determination pursuant to Section 31.04(h) of the San Francisco Administrative Code.

COMMUNITY PLAN EXEMPTION OVERVIEW

California Public Resources Code Section 21083.3 and CEQA Guidelines Section 15183 provide an exemption from environmental review for projects that are consistent with the development density established by existing zoning, community plan or general plan policies for which an Environmental Impact Report (EIR) was certified, except as might be necessary to examine whether there are project-specific significant effects which are peculiar to the project or its site. Section 15183 specifies that examination of environmental effects shall be limited to those effects that: a) are peculiar to the project or parcel on which the project would be located; b) were not analyzed as significant effects in a prior EIR on the zoning action, general plan or community plan with which the project is consistent; c) are potentially significant off-site and cumulative impacts that were not discussed in the underlying EIR; or d) are previously identified in the EIR, but which, as a result of substantial new information that was not known at the time that the EIR was certified, are determined to have a more severe adverse impact than that discussed in the underlying EIR. Section 15183(c) specifies that if an impact is not peculiar to the parcel or to the proposed project, then an EIR need not be prepared for the project solely on the basis of that impact.

This determination evaluates the potential project-specific environmental effects of the 88 Arkansas Street project described above, and incorporates by reference information contained in the Programmatic EIR for the Eastern Neighborhoods Rezoning and Area Plans (PEIR)¹. Project-specific studies were prepared for the proposed project to determine if the project would result in any significant environmental impacts that were not identified in the Eastern Neighborhoods PEIR.

After several years of analysis, community outreach, and public review, the Eastern Neighborhoods PEIR was adopted in December 2008. The Eastern Neighborhoods PEIR was adopted in part to support housing development in some areas previously zoned to allow industrial uses, while preserving an adequate supply of space for existing and future production, distribution, and repair (PDR) employment and businesses. The Eastern Neighborhoods PEIR also included changes to existing height and bulk districts in some areas, including the project site at 88 Arkansas Street.

The Planning Commission held public hearings to consider the various aspects of the proposed Eastern Neighborhoods Rezoning and Area Plans and related Planning Code and Zoning Map amendments. On August 7, 2008, the Planning Commission certified the Eastern Neighborhoods PEIR by Motion 17659 and adopted the Preferred Project for final recommendation to the Board of Supervisors.^{2,3}

In December 2008, after further public hearings, the Board of Supervisors approved and the Mayor signed the Eastern Neighborhoods Rezoning and Planning Code amendments. New zoning districts include districts that would permit PDR uses in combination with commercial uses; districts mixing residential and commercial uses and residential and PDR uses; and new residential-only districts. The districts replaced existing industrial, commercial, residential single-use, and mixed-use districts.

The Eastern Neighborhoods PEIR is a comprehensive programmatic document that presents an analysis of the environmental effects of implementation of the Eastern Neighborhoods Rezoning and Area Plans, as well as the potential impacts under several proposed alternative scenarios. The Eastern Neighborhoods

¹ Planning Department Case No. 2004.0160E and State Clearinghouse No. 2005032048. This material is available for review at the Planning Department, 1650 Mission Street, Suite 400, under the aforementioned Case No..

² San Francisco Planning Department. Eastern Neighborhoods Rezoning and Area Plans Final Environmental Impact Report (FEIR), Planning Department Case No. 2004.0160E, certified August 7, 2008. Available online at: <http://www.sf-planning.org/index.aspx?page=1893>, accessed August 17, 2012.

³ San Francisco Planning Department. San Francisco Planning Commission Motion 17659, August 7, 2008. Available online at: <http://www.sf-planning.org/Modules/ShowDocument.aspx?documentid=1268>, accessed August 17, 2012.

Draft EIR evaluated three rezoning alternatives, two community-proposed alternatives which focused largely on the Mission District, and a "No Project" alternative. The alternative selected, or the Preferred Project, represents a combination of Options B and C. The Planning Commission adopted the Preferred Project after fully considering the environmental effects of the Preferred Project and the various scenarios discussed in the PEIR. The Eastern Neighborhoods PEIR estimated that implementation of the Eastern Neighborhoods Plan could result in approximately 7,400 to 9,900 net dwelling units and 3,200,000 to 6,600,000 square feet of net non-residential space (excluding PDR loss) built in the Plan Area throughout the lifetime of the Plan (year 2025).

A major issue of discussion in the Eastern Neighborhoods rezoning process was the degree to which existing industrially-zoned land would be rezoned to primarily residential and mixed-use districts, thus reducing the availability of land traditionally used for PDR employment and businesses. Among other topics, the Eastern Neighborhoods PEIR assesses the significance of the cumulative land use effects of the rezoning by analyzing its effects on the City's ability to meet its future PDR space needs as well as its ability to meet its housing needs as expressed in the City's General Plan.

As a result of the Eastern Neighborhoods rezoning process, the project site has been rezoned to UMU (Urban Mixed Use) District. The UMU District is intended to promote a vibrant mix of uses while maintaining the characteristics of this formerly industrially-zoned area. It is also intended to serve as a buffer between residential districts and PDR districts in the Eastern Neighborhoods. The proposed project and its relation to PDR land supply and cumulative land use effects is discussed further in the Community Plan Exemption (CPE) Checklist, under Land Use. The 88 Arkansas Street site, which is located in the Potrero Hill neighborhood of the Eastern Neighborhoods, was designated as a site allowing buildings up to 48 feet in height.

Individual projects that could occur in the future under the Eastern Neighborhoods Rezoning and Area Plans will undergo project-level environmental evaluation to determine if they would result in further impacts specific to the development proposal, the site, and the time of development and to assess whether additional environmental review would be required. This determination concludes that the proposed project at 88 Arkansas Street is consistent with and was encompassed within the analysis in the Eastern Neighborhoods PEIR, including the Eastern Neighborhoods PEIR development projections. This determination also finds that the Eastern Neighborhoods PEIR adequately anticipated and described the impacts of the proposed 88 Arkansas Street project, and identified the mitigation measures applicable to the 88 Arkansas Street project. The proposed project is also consistent with the zoning controls and the provisions of the Planning Code applicable to the project site.⁴⁵ Therefore, no further CEQA evaluation for the 88 Arkansas Street project is required. In sum, the Eastern Neighborhoods PEIR and this Certificate of Exemption for the proposed project comprise the full and complete CEQA evaluation necessary for the proposed project.

PROJECT SETTING

The project site vicinity is characterized by a mix of industrial, PDR, educational, retail, office, and open spaces uses. The subject block, along with the blocks to the east and west of the project site between 16th and 17th Streets, is zoned UMU (Urban Mixed Use). The block north of the project site on the north site of

⁴ Adam Varat, San Francisco Planning Department, Community Plan Exemption Eligibility Determination, Citywide Planning and Policy Analysis, 88 Arkansas Street, June 10, 2015. This document is available for review at the San Francisco Planning Department, 1650 Mission Street, Suite 400 as part of Case File No. 2015-000453ENV.

⁵ Jeff Joslin, San Francisco Planning Department, Community Plan Exemption Eligibility Determination, Current Planning Analysis, 88 Arkansas Street, December 30, 2015. This document is available for review at the San Francisco Planning Department, 1650 Mission Street, Suite 400 as part of Case File No. 2015-000453ENV.

16th Street is zoned PDR-1-D (Production, Distribution, and Repair – 1 Design). The block directly south of the project site is occupied by Jackson Playground and Recreation Center and is zoned P (Public). The majority of the block southwest of the project site is zoned RH-3 (Residential-House, Three Family). RH-2 (Residential-House, Two Family) districts are also located in the site vicinity. The project site, along with adjacent lots to the east and west of the site, are within a 48-X height and bulk district. The project vicinity includes 68-X height and bulk districts (on lots north of the project site) and 40-X height and bulk districts (on lots south of the project site).

As previously mentioned, the subject block is bounded by 16th, 17th, Arkansas, and Wisconsin Streets. Running east/west, 16th and 17th Streets are two-lane, two-way streets with parallel parking on both sides. 16th and 17th Streets are part of Citywide Bicycle Route 40. Running north/south, Arkansas and Wisconsin Streets are unstriped two-lane, two-way streets with parallel parking on the east side of each street and perpendicular parking on the west side of each street. Arkansas and Wisconsin Streets are not part of the Citywide Bicycle Network. With regards to the subject block, the sidewalks along 16th and 17th Streets are approximately 10 feet wide and the sidewalks along Arkansas and Wisconsin Streets are approximately 15 feet wide. Roughly 379 on-street parking spaces are located within the area bounded by 16th Street to the north, 17th Street to the south, Arkansas Street to the east, and Carolina Street to the west, with additional on-street parking provided throughout the project site vicinity. The site vicinity does not contain any city-owned or public off-street parking facilities. The off-street parking facilities in the vicinity of the project site primarily serve employees and patrons of private businesses.

The low-density scale of development in the project site vicinity includes one- to three-story-tall residential and commercial buildings. An approximately 15-foot-tall, one-story building is located on the southwest corner of the block just west of the project site and the one-story portion of a commercial building on the southeast corner of the subject block is also approximately 15 feet tall; however, the remainder of the buildings on the subject block and in the site vicinity are approximately 20 to 40 feet tall. On the subject block, retail and warehouse buildings sit adjacent to the portion of the project site fronting 17th Street. One-story office and warehouse buildings front Arkansas Street and Wisconsin Street, including a retail store and warehouse space with frontage on Wisconsin and 16th Streets. A surface parking lot, with frontage on Wisconsin Street and Arkansas Street, is located in the middle of the subject block. An abandoned rail spur is located between the parking lot and the project site. As previously mentioned, Jackson Playground and Recreation Center is located on the south side of 17th Street across from the project site. A one-story industrial building sits opposite the project site's Arkansas Street frontage. A one-story auto body shop and one-story warehouse are located on the west side of Wisconsin Street. Other uses in the area include Live Oak School (one block south of the project site), Anchor Steam Brewery (one block southwest), Thee Parkside bar and music venue (one block west), and The Connecticut Yankee restaurant (one block east).

POTENTIAL ENVIRONMENTAL EFFECTS

The Eastern Neighborhoods PEIR included analyses of environmental issues including: land use; plans and policies; visual quality and urban design; population, housing, business activity, and employment (growth inducement); transportation; noise; air quality; parks, recreation and open space; shadow; archeological resources; historic architectural resources; hazards; and other issues not addressed in the previously issued initial study for the Eastern Neighborhoods Rezoning and Area Plans. The proposed 88 Arkansas Street project is in conformance with the height, use and density for the site described in the Eastern Neighborhoods PEIR and would represent a small part of the growth that was forecast for the Eastern Neighborhoods plan areas. Thus, the plan analyzed in the Eastern Neighborhoods PEIR considered the incremental impacts of the proposed 88 Arkansas Street project. As a result, the proposed

project would not result in any new or substantially more severe impacts than were identified in the Eastern Neighborhoods PEIR.

Significant and unavoidable impacts were identified in the Eastern Neighborhoods PEIR for the following topics: land use, historic architectural resources, transportation and circulation, and shadow. The proposed project would not contribute to significant and unavoidable impacts on transportation, historic resources, and shadow. In regards to significant and unavoidable transportation impacts related to traffic and transit, the proposed project would not considerably contribute to project-specific and cumulative traffic and transit impacts identified in the Eastern Neighborhoods PEIR. The existing buildings on the project site were not found to be historic resources, nor is the project site located in or adjacent to an identified historic district. As such, the proposed project would not result in a significant adverse impact on historic resources, and therefore would not contribute to any significant and unavoidable impacts to historic resources. The Eastern Neighborhoods PEIR determined that projects developed in the Potrero Hill neighborhood on lots adjacent to the Jackson Playground and Recreation Center (Jackson Playground) could result in significant and unavoidable shadow impacts. However, shadow on Jackson Playground generated by the proposed project would not be expected to substantially affect the use and enjoyment of the park as the shade would primarily occur in the early morning and would be of short duration. Therefore, the proposed project would not considerably contribute to shadow impacts identified in the Eastern Neighborhoods PEIR. The proposed project would contribute to significant and unavoidable impacts on land use. The existing buildings on the project site are zoned UMU, which allows for PDR uses. Approximately 6,000 sf of the building on Lot 002 is currently occupied by Volta Industries. The building on Lot 002A has been vacant since 2014, but was previously occupied by the Flynn and Enslow Wire Products and Screening Company. Thus the proposed project would remove approximately 25,600 gsf of existing PDR use. In addition, the PEIR considers the presence of PDR businesses and activities and how they may operate as PDR clusters. The roughly 25,560 sf of industrial use on the project site, combined with uses in the PDR-1-D district located north of the project site, may form a PDR cluster. The existing PDR business at the project site would be required to relocate and may not be able to relocate near other similar PDR businesses. This effect of the project, combined with the loss of approximately 25,560 sf of PDR space would contribute to the significant and unavoidable land use impacts identified in the PEIR.

The Eastern Neighborhoods PEIR identified feasible mitigation measures to address significant impacts related to noise, air quality, archeological resources, historical resources, hazardous materials, and transportation. **Table 1** below lists the mitigation measures identified in the Eastern Neighborhoods PEIR and states whether each measure would apply to the proposed project.

Table 1 – Eastern Neighborhoods PEIR Mitigation Measures

Mitigation Measure	Applicability	Compliance
F. Noise		
F-1: Construction Noise (Pile Driving)	Not Applicable: pile driving is not required or proposed.	Not applicable.
F-2: Construction Noise	Applicable: temporary construction noise from use of heavy equipment.	The project sponsor has agreed to develop and implement a set of construction noise attenuation measures.
F-3: Interior Noise Levels	Not Applicable: the project is	Not applicable.

Mitigation Measure	Applicability	Compliance
	subject to Title 24 interior noise standards.	
F-4: Siting of Noise-Sensitive Uses	Applicable: project includes noise-sensitive uses located along streets where existing noise levels exceed 60 dBA (Ldn).	The project sponsor has conducted and submitted a detailed analysis of noise reduction requirements to meet Title 24 interior noise levels. The project sponsor shall submit building plans consistent with the recommendations included in the noise analysis.
F-5: Siting of Noise-Generating Uses	Not Applicable: no noise-generating uses.	Not applicable.
F-6: Open Space in Noisy Environments	Applicable: the project would include shared courtyards, a solarium, and a roof deck.	The project sponsor has conducted and submitted a detailed acoustical analysis for proposed open spaces and determined that the open spaces meet the requirements of Mitigation Measure F-6 in the PEIR. No further mitigation is required to comply with this measure. .
G. Air Quality		
G-1: Construction Air Quality	Not applicable: the project sponsor could be required to comply with the San Francisco Dust Control Ordinance.	Not applicable.
G-2: Air Quality for Sensitive Land Uses	Not Applicable: the project site is not in the Air Pollutant Exposure Zone.	Not Applicable.
G-3: Siting of Uses that Emit DPM	Not Applicable: the proposed residential uses are not uses that would emit substantial levels of DPM.	Not Applicable.
G-4: Siting of Uses that Emit other TACs	Not Applicable: the proposed project would not emit substantial levels of other TACs.	Not Applicable.
J. Archeological Resources		

Mitigation Measure	Applicability	Compliance
	mitigation by the San Francisco Municipal Transportation Agency.	
E-3: Enhanced Funding	Not Applicable: plan level mitigation by the San Francisco Municipal Transportation Agency and the San Francisco Transit Authority.	Not Applicable.
E-4: Intelligent Traffic Management	Not Applicable: plan level mitigation by the San Francisco Municipal Transportation Agency & Planning Department.	Not Applicable.
E-5: Enhanced Transit Funding	Not Applicable: plan level mitigation by the San Francisco Municipal Transportation Agency.	Not Applicable.
E-6: Transit Corridor Improvements	Not Applicable: plan level mitigation by the San Francisco Municipal Transportation Agency.	Not Applicable.
E-7: Transit Accessibility	Not Applicable: plan level mitigation by the San Francisco Municipal Transportation Agency.	Not Applicable.
E-8: Muni Storage and Maintenance	Not Applicable: plan level mitigation by the San Francisco Municipal Transportation Agency.	Not Applicable.
E-9: Rider Improvements	Not Applicable: plan level mitigation by the San Francisco Municipal Transportation Agency.	Not Applicable.
E-10: Transit Enhancement	Not Applicable: plan level mitigation by the San Francisco Municipal Transportation Agency.	Not Applicable.
E-11: Transportation Demand Management	Not Applicable: plan level mitigation by the San Francisco Municipal Transportation Agency.	Not Applicable.

Mitigation Measure	Applicability	Compliance
J-1: Properties with Previous Studies	Not Applicable: the project site was not evaluated in any previous studies.	Not applicable.
J-2: Properties with no Previous Studies	Applicable: the project site is located in an area with no previous studies.	The project sponsor has agreed to implement the Planning Department's Standard Mitigation Measure #1 (Accidental Discovery) in compliance with this mitigation measure.
J-3: Mission Dolores Archeological District	Not Applicable: the project site is not located within the Mission Dolores Archeological District.	Not Applicable.
K. Historical Resources		
K-1: Interim Procedures for Permit Review in the Eastern Neighborhoods Plan area	Not Applicable: plan-level mitigation completed by Planning Department.	Not Applicable.
K-2: Amendments to Article 10 of the Planning Code Pertaining to Vertical Additions in the South End Historic District (East SoMa)	Not Applicable: plan-level mitigation completed by Planning Commission.	Not Applicable.
K-3: Amendments to Article 10 of the Planning Code Pertaining to Alterations and Infill Development in the Dogpatch Historic District (Central Waterfront)	Not Applicable: plan-level mitigation completed by Planning Commission.	Not Applicable.
L. Hazardous Materials		
L-1: Hazardous Building Materials	Applicable: the project involves the demolition of existing buildings.	The project sponsor has agreed to remove and properly dispose of any hazardous building materials in accordance with applicable federal, state, and local laws prior to demolishing the existing buildings.
E. Transportation		
E-1: Traffic Signal Installation	Not Applicable: plan level mitigation by the San Francisco Municipal Transportation Agency.	Not Applicable.
E-2: Intelligent Traffic Management	Not Applicable: plan level	Not Applicable.

Please see the attached Mitigation Monitoring and Reporting Program (MMRP) for the complete text of the applicable mitigation measures. With implementation of these mitigation measures the proposed project would not result in significant impacts beyond those analyzed in the Eastern Neighborhoods PEIR.

PUBLIC NOTICE AND COMMENT

A "Notification of Project Receiving Environmental Review" was mailed on April 30, 2015 to adjacent occupants and owners of properties within 300 feet of the project site. Overall, concerns and issues raised by the public in response to the notice were taken into consideration and incorporated in the environmental review as appropriate for CEQA analysis. Comments were received from individuals and a neighborhood association. Concerns related to physical environmental effects include the methodology for analyzing potential cumulative conditions in the Transportation Impact Study, the potential presence of subsurface contamination and serpentine soil, excavation activities (soil excavation and materials transport), and potential impacts to vistas. Concerns regarding the analysis of cumulative conditions is addressed the Transportation section of the CPE Checklist. In regards to the potential presence of serpentine soil and subsurface contamination on the project site, the project is subject to Health Code Article 22A, which requires soil remediation for subsurface contamination. The project site is not located on serpentine soil. This is further discussed in the Hazards and Hazardous Materials section of the CPE Checklist. The Hazards and Hazardous Materials section also includes a discussion of the protocol for excavation activities, which is dictated by DPH pursuant to Health Code Article 22A. In regards to potential impacts to vistas, pursuant to Public Resources Code 21099(d) visual effects, analyzed under the topic of Aesthetics in CEQA, are not to be considered significant CEQA impacts for mixed-use residential development projects on in-fill sites in a transit priority area. The proposed project meets the criteria, as discussed on page 20 of the CPE Checklist. The proposed project would not result in significant adverse environmental impacts associated with the issues identified by the public beyond those identified in the Eastern Neighborhoods PEIR.

CONCLUSION

As summarized above and further discussed in the CPE Checklist⁶:

1. The proposed project is consistent with the development density established for the project site in the Eastern Neighborhoods Rezoning and Area Plans;
2. The proposed project would not result in effects on the environment that are peculiar to the project or the project site that were not identified as significant effects in the Eastern Neighborhoods PEIR;
3. The proposed project would not result in potentially significant off-site or cumulative impacts that were not identified in the Eastern Neighborhoods PEIR;
4. The proposed project would not result in significant effects, which, as a result of substantial new information that was not known at the time the Eastern Neighborhoods PEIR was certified, would be more severe than were already analyzed and disclosed in the PEIR; and

⁶ The CPE Checklist is available for review at the Planning Department, 1650 Mission Street, Suite 400, San Francisco, in Case File No. 2015-000453ENV.

5. The project sponsor will undertake feasible mitigation measures specified in the Eastern Neighborhoods PEIR to mitigate project-related significant impacts.

Therefore, the proposed project is exempt from further environmental review pursuant to Public Resources Code Section 21083.3 and CEQA Guidelines Section 15183.

Attachment A: MITIGATION MEASURES ADOPTED AS CONDITIONS OF APPROVAL				
MITIGATION MEASURES ADOPTED AS CONDITIONS OF APPROVAL	Responsibility for Implementation	Mitigation Schedule	Monitoring/Report Responsibility	Status/Date Completed
MITIGATION MEASURES FROM THE EASTERN NEIGHBORHOODS AREA PLAN EIR				
<p>PMM 1: Properties with No Previous Studies (Mitigation Measure J-2 of the Eastern Neighborhoods PEIR) The following mitigation measure is required to avoid any potential adverse effect from the proposed project on accidentally discovered buried or submerged historical resources as defined in CEQA Guidelines Section 15064.5(a) and (c). The project sponsor shall distribute the Planning Department archeological resource "ALERT" sheet to the project prime contractor; to any project subcontractor (including demolition, excavation, grading, foundation, pile driving, etc. firms); or utilities firm involved in soils disturbing activities within the project site. Prior to any soils disturbing activities being undertaken each contractor is responsible for ensuring that the "ALERT" sheet is circulated to all field personnel including, machine operators, field crew, pile drivers, supervisory personnel, etc. The project sponsor shall provide the Environmental Review Officer (ERO) with a signed affidavit from the responsible parties (prime contractor, subcontractor(s), and utilities firm) to the ERO confirming that all field personnel have received copies of the Alert Sheet.</p> <p>Should any indication of an archeological resource be encountered during any soils disturbing activity of the project, the project Head Foreman and/or project sponsor shall immediately notify the ERO and shall immediately suspend any soils disturbing activities in the vicinity of the discovery until the ERO has determined what additional measures should be undertaken.</p> <p>If the ERO determines that an archeological resource may be present within the project site, the project sponsor shall retain the services of an archaeological consultant from the pool of qualified archaeological consultants maintained by the Planning Department archaeologist. The</p>	<p>Project sponsor, contractor, Planning Department's archeologist or qualified archaeological consultant, and Planning Department's Environmental Review Officer.</p>	<p>Prior to issuance of any permit for soil-disturbing activities and during construction.</p>	<p>Sponsor, contractor(s), sponsor's archeologist (if applicable), Environmental Review Officer.</p>	<p>Considered complete upon ERO's approval of FARR, if required. Otherwise considered complete upon submittal of signed affidavit and completion of construction activities.</p>

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MITIGATION MEASURES ADOPTED AS CONDITIONS OF APPROVAL	Responsibility for Implementation	Mitigation Schedule	Monitoring/Report Responsibility	Status/Date Completed
<p>archeological consultant shall advise the ERO as to whether the discovery is an archeological resource, retains sufficient integrity, and is of potential scientific/historical/cultural significance. If an archeological resource is present, the archeological consultant shall identify and evaluate the archeological resource. The archeological consultant shall make a recommendation as to what action, if any, is warranted. Based on this information, the ERO may require, if warranted, specific additional measures to be implemented by the project sponsor.</p> <p>Measures might include: preservation in situ of the archeological resource; an archaeological monitoring program; or an archeological testing program. If an archeological monitoring program or archeological testing program is required, it shall be consistent with the Environmental Planning (EP) division guidelines for such programs. The ERO may also require that the project sponsor immediately implement a site security program if the archeological resource is at risk from vandalism, looting, or other damaging actions.</p> <p>The project archeological consultant shall submit a Final Archeological Resources Report (FARR) to the ERO that evaluates the historical significance of any discovered archeological resource and describing the archeological and historical research methods employed in the archeological monitoring/data recovery program(s) undertaken. Information that may put at risk any archeological resource shall be provided in a separate removable insert within the final report.</p> <p>Copies of the Draft FARR shall be sent to the ERO for review and approval. Once approved by the ERO, copies of the FARR shall be distributed as follows: California Archaeological Site Survey Northwest Information Center (NWIC) shall receive one (1) copy and</p>				

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MITIGATION MEASURES ADOPTED AS CONDITIONS OF APPROVAL	Responsibility for Implementation	Mitigation Schedule	Monitoring/Report Responsibility	Status/Date Completed
the ERO shall receive a copy of the transmittal of the FARR to the NWIC. The Environmental Planning division of the Planning Department shall receive one bound copy, one unbound copy and one unlocked, searchable PDF copy on CD; three copies of the FARR along with copies of any formal site recordation forms (CA DPR 523 series) and/or documentation for nomination to the National Register of Historic Places/California Register of Historical Resources. In instances of high public interest or interpretive value, the ERO may require a different final report content, format, and distribution than that presented above.				
PMM 2: Construction Noise (Mitigation Measure F-2 of the Eastern Neighborhoods PEIR). The project sponsor shall develop a set of site-specific noise attenuation measures under the supervision of a qualified acoustical consultant. Prior to commencing construction, a plan for such measures shall be submitted to the Department of Building Inspection (DBI) to ensure that maximum feasible noise attenuation will be achieved. These attenuation measures shall include as many of the following control strategies as feasible: <ul style="list-style-type: none"> Erect temporary plywood noise barriers around the construction site, particularly where a site adjoins noise-sensitive uses; Utilize noise control blankets on the building structure as the building is erected to reduce noise emission from the site; Evaluate the feasibility of noise control at the receivers by temporarily improving the noise reduction capability of adjacent buildings housing sensitive uses; Monitor the effectiveness of noise attenuation measures by taking noise measurements; and 	Project sponsor, contractor(s).	Prior to and during construction activities.	Project sponsor, contractor(s).	Considered complete upon receipt of final monitoring report at completion of construction.

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<ul style="list-style-type: none"> Post signs on-site pertaining to permitted construction days and hours and complaint procedures and who to notify in the event of a problem, with telephone numbers listed. <p>PMM 3: Siting of Noise-Sensitive Uses (Mitigation Measure F-4 of the Eastern Neighborhoods PEIR). To reduce potential conflicts between existing noise-generating uses and new sensitive receptors, for new development including noise-sensitive uses, the project sponsor was required to provide an analysis that included, at a minimum, a site survey to identify potential noise-generating uses within 900 feet of, and that have a direct line-of-sight to, the project site, and including at least one 24-hour noise measurement (with maximum noise level readings taken at least every 15 minutes), prior to the first project approval action. The analysis which was prepared by persons qualified in acoustical analysis and/or engineering, demonstrated with reasonable certainty that Title 24 standards, where applicable, can be met, and that there are no particular circumstances about the proposed project site that appear to warrant heightened concern about noise levels in the vicinity. The report concluded that if its recommendations are incorporated into the design and construction of the proposed building, the project would achieve compliance with Title 24. Recommendations include the following: (1) on the ground floor, exterior windows and doors should have an STC rating of 32 along Arkansas Street, an STC rating of 35 along 17th Street, and an STC rating of 28 for all other locations; (2) on the second floor, exterior windows and doors should have an STC rating of 32 along Arkansas Street, an STC rating of 35 on the northeast corner of the project site, and an STC rating of 28 for all other locations; (3) on the third through fifth floors exterior windows and doors should have an STC rating of 32 along Arkansas Street, an STC rating of 35 along 17th Street, an STC rating of 32 on the eastern half of the northern façade of the building, an STC rating of 30 on the a portion of the western half of the northern</p>	Project sponsor, contractor(s).	Prior to project approval.	San Francisco Planning Department, San Francisco Department of Building Inspection.	Considered complete upon finalization of the noise study and incorporation of acoustical requirements into final construction drawing set.

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façade of the building, an STC rating of 38 on the southwest and southeast corners of the building, an STC rating of 35 on the northwest and northeast corners of the building, and an STC rating of 28 for all other locations. Overall, the proposed project would achieve compliance with the Title 24 standard of DNL 45 dBA for interior noise by installing exterior windows and doors with STC ratings between 28 and 38, depending on the location. The STC ratings shall be incorporated into the final construction drawings.				
PMM 4: Open Space in Noisy environments (Mitigation Measure F-6 of the Eastern Neighborhoods PEIR). The project sponsor shall ensure that open space required under the Planning Code for such uses be protected, to the maximum feasible extent, from existing ambient noise levels that could prove annoying or disruptive to users of the open space. Implementation of this measure could involve, among other things, site design that uses the building itself to shield on-site open space from the greatest noise sources, construction of noise barriers between noise sources and open space, and appropriate use of both common and private open space in multi-family dwellings, and implementation would also be undertaken consistent with other principles of urban design. This mitigation measure has been complied with as part of this environmental review process. No further actions are required to comply with Mitigation Measure F-6 of the Eastern Neighborhoods PEIR.	Project sponsor, architect.	Design measures to be incorporated into project design and included in the building permit.	San Francisco Planning Department, San Francisco Department of Building Inspection.	Considered complete upon approval of final construction drawing set.

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MITIGATION MEASURES ADOPTED AS CONDITIONS OF APPROVAL	Responsibility for Implementation	Mitigation Schedule	Monitoring/Report Responsibility	Status/Date Completed
PMM 5: Hazardous Building Materials (Mitigation Measure L-1 of the Eastern Neighborhoods PEIR). The project sponsor shall ensure that any equipment containing PCBs or DEPH, such as fluorescent light ballasts, are removed and properly disposed of according to applicable federal, state, and local laws prior to the start of renovation, and that any fluorescent light tubes, which could contain mercury, are similarly removed and properly disposed of. Any other hazardous materials identified, either before or during work, shall be abated according to applicable federal, state, and local laws.	Project sponsor, contractor(s).	Prior to demolition of structures.	Project sponsor, contractor(s), DPH, various federal and state agencies.	Considered complete when equipment containing PCBs or DEHP or other hazardous materials is properly disposed.

Attachment B: IMPROVEMENT MEASURES ADOPTED AS CONDITIONS OF APPROVAL				
IMPROVEMENT MEASURES ADOPTED AS CONDITIONS OF APPROVAL	Responsibility for Implementation	Mitigation Schedule	Monitoring/Report Responsibility	Status/Date Completed
<p>Transportation and Circulation</p> <p>Project Improvement Measure 1: Implementation of Transportation Demand Management Strategies. The project sponsor would implement a Transportation Demand Management (TDM) program to encourage the use of alternative forms of transportation for trips to and from the project vicinity, such as transit, bicycle, rideshare, and walking. Components of the improvement measure are as follows:</p> <p>IM-1a TDM Coordinator: The project sponsor would identify a TDM Coordinator for the project site who would be responsible for the implementation and ongoing operation of all other TDM measures included in the proposed Project. The TDM Coordinator should be the single point of contact for all transportation-related questions from residents and City staff. The project sponsor may use an existing transportation management association (e.g., the Transportation Management Association of San Francisco) to supply the TDM Coordinator, or the TDM Coordinator may be an existing staff member (e.g., property manager). In either case, the TDM Coordinator does not have to work full-time at the Project site. The TDM coordinator would be the single point of contact for all transportation-related questions from building occupants and City staff. The TDM Coordinator should provide TDM training to other building staff about the transportation amenities and options available in the project vicinity.</p> <p>IM-1b Transportation and Trip Planning Information Move-In</p>	Project sponsor, property owner, TDM Coordinator.	Following project occupancy.	San Francisco Planning Department, San Francisco Municipal Transportation Agency, San Francisco Department of Public Works, and/or Bay Area Bike Share.	Following project occupancy.

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<p>Packet: Provide a transportation insert for the move-in packet that includes information on transit service (local and regional, schedules and fares), information on where transit passes may be purchased, information on the 511 Regional Rideshare Program, and nearby bike and car share programs, and information on where to find additional web-based alternative transportation materials. The move-in packet should be continuously updated as transportation options change, and the packet should be provided to each new building occupant. Muni maps as well as San Francisco Bicycle and Pedestrian maps should be provided upon request.</p>				
<p>IM-1c City Access to Data Collection: As part of an ongoing effort to quantify the efficacy of TDM measures, City staff may need to access the project site (including the garage) to perform trip counts, and/or intercept surveys and/or other types of data collection. All on-site activities shall be coordinated through the TDM Coordinator. The project sponsor assures future access to the site by City staff. Providing access to existing developments for data collection purposes is also encouraged.</p>				
<p>IM-1d Bicycle Parking: Increase the number of on-site secured bicycle parking beyond Planning Code requirements and/or provide additional bicycle facilities in the public right-of-way adjacent to or within a quarter mile of the project site (e.g., sidewalks, on-street parking spaces).</p>				
<p>IM-1e Car-Share Parking: Provide optional car share spaces as described in Planning Code Section 166(g).</p>				

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IM-1f Transit Pass: Offer free or subsidized Muni passes to tenants. For example, the project sponsor may offer a 50 percent subsidy for one Muni monthly pass for new residents (one per household), and employees for up to one year. The recipient would be responsible for the remainder of the costs associated with the Muni monthly pass.					
Project Improvement Measure 2: Pedestrian Safety Improvements. Install audible and visual warning devices to alert pedestrians of vehicles exiting the parking garage.	Project sponsor, construction contractor	Prior to project occupancy.	San Francisco Planning Department, San Francisco Department of Building Inspection,	Prior to project occupancy.	
Project Improvement Measure 3: On-Street Loading Management. IM-3a Schedule and coordinate loading activities through building management to ensure that trucks can be accommodated in the curbside loading spaces. All regularly scheduled activities requiring use of the loading space (e.g., building supply deliveries) should be coordinated directly with building management. IM-3b Trucks should be discouraged from parking illegally or obstructing vehicle, transit, bicycle, or pedestrian traffic flow along any of the streets adjacent to the project site.	Project sponsor, property owner, building management.	Following project occupancy.	San Francisco Municipal Transportation Agency.	Following project occupancy.	
Project Improvement Measure 4: Construction-Related Traffic Management. IM-4a Limit hours of construction-related traffic, including, but not	Project sponsor, construction contractor.	Prior to initiation of construction and during construction	Sustainable Streets Division of the San Francisco Municipal Transportation Agency,	Complete upon completion of construction.	

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<p>limited to, truck movements, to avoid the weekday AM and PM peak hours (7:00 a.m. to 9:00 a.m. and 4:00 p.m. to 6:00 p.m.; or other times, if approved by SFMTA).</p> <p>IM4b Construction contractor(s) should coordinate construction activities with other potential projects that may be constructed in the vicinity of the project site.</p>		activities.	San Francisco Fire Department, Muni, and the Planning Department.	
<p>Project Improvement Measure 5: Off-Street Parking Traffic Enhancements</p> <p>IM-5a Install a traffic signal on both the inside and outside of the garage opening to indicate the presence of approaching vehicles from the opposite direction. The traffic signal would operate on sensors/detectors, and alternate with two beacons, with one indicating a solid red signal or marked with "STOP" and another indicating a solid green signal or marked with "GO." The signals would be installed facing outward at the garage opening outside of the security gate, and at the interior of the garage facing inward into the garage ramp, and facing inward at the interior of the garage on the basement level, at the entry to the garage ramp.</p> <p>IM-5b The owner/operator of any off-street parking facility, as determined by the Planning Director, with more than 20 parking spaces (excluding loading and car-share spaces) could ensure that recurring vehicle queues¹ do not occur on the public right of way. If a recurring queue occurs, the parking facility's owner/operator could implement abatement measures as needed to abate the queue.</p>	Project sponsor, property owner, parking garage operator.	Following project occupancy.	Planning Director or designee.	Following project occupancy.

¹ Vehicle Queue: When one or more vehicles blocking any portion of a public street, alley or sidewalk for a consecutive period of three minutes or longer on a daily or weekly basis.

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<p>Suggested abatement methods include but are not limited to the following: redesign of facility layout to improve vehicle circulation and/or on-site queue capacity; employment of parking attendants; installation of LOT FULL signs with active management by parking attendants; use of valet parking or other space-efficient parking techniques; use of off-site parking facilities or shared parking with nearby uses; use of parking occupancy sensors and signage directing drivers to available spaces; travel demand management strategies such as those discussed in IM-1, customer shuttles or delivery services; and/or parking demand management strategies such as parking time limits, paid parking or validated parking.</p> <p>If the Planning Director, or his or her designee, suspects that a recurring queue is present, the Department may notify the property owner in writing. Upon request, the owner/operator could hire a qualified transportation consultant to evaluate the conditions at the site for no less than seven days. The consultant could prepare a monitoring report to be submitted to the Planning Department for review. If the Planning Department determines that a recurring queue does exist, the facility owner/operator could abate the queue.</p>				



SAN FRANCISCO PLANNING DEPARTMENT

Agreement to Implement Mitigation Measure(s)

Case No.: 2015-000453ENV
Project Title: 88 Arkansas Street
BPA Nos.: 201511021443
Zoning: UMU (Urban Mixed-Use) Zoning District
48-X Height & Bulk District
Block/Lot: 3953/002 & 002A
Lot Size: 29,998 square feet
Plan Area: Eastern Neighborhoods Area Plan (Potrero Hill)
Project Sponsor: Julie Heinzler, Martin Building Company, (415) 348-4644
Lead Agency: San Francisco Planning Department
Staff Contact: Jenny Delumo – (415) 575-9146, Jenny.Delumo@sfgov.org

1650 Mission St.
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MITIGATION MEASURES

Project Mitigation Measure 1: Properties with No Previous Studies (Implementing Eastern Neighborhoods PEIR Mitigation Measure J-2)

The following mitigation measure is required to avoid any potential adverse effect from the proposed project on accidentally discovered buried or submerged historical resources as defined in CEQA Guidelines Section 15064.5(a) and (c). The project sponsor shall distribute the Planning Department archeological resource "ALERT" sheet to the project prime contractor; to any project subcontractor (including demolition, excavation, grading, foundation, pile driving, etc. firms); or utilities firm involved in soils disturbing activities within the project site. Prior to any soils disturbing activities being undertaken each contractor is responsible for ensuring that the "ALERT" sheet is circulated to all field personnel including, machine operators, field crew, pile drivers, supervisory personnel, etc. The project sponsor shall provide the Environmental Review Officer (ERO) with a signed affidavit from the responsible parties (prime contractor, subcontractor(s), and utilities firm) to the ERO confirming that all field personnel have received copies of the Alert Sheet.

Should any indication of an archeological resource be encountered during any soils disturbing activity of the project, the project Head Foreman and/or project sponsor shall immediately notify the ERO and shall immediately suspend any soils disturbing activities in the vicinity of the discovery until the ERO has determined what additional measures should be undertaken.

If the ERO determines that an archeological resource may be present within the project site, the project sponsor shall retain the services of an archaeological consultant from the pool of qualified archaeological consultants maintained by the Planning Department archaeologist. The archeological consultant shall advise the ERO as to whether the discovery is an archeological resource, retains sufficient integrity, and is of potential scientific/historical/cultural significance. If an archeological resource is present, the archeological consultant shall identify and evaluate the archeological resource. The archeological consultant shall make a recommendation as to what

action, if any, is warranted. Based on this information, the ERO may require, if warranted, specific additional measures to be implemented by the project sponsor.

Measures might include: preservation in situ of the archeological resource; an archaeological monitoring program; or an archeological testing program. If an archeological monitoring program or archeological testing program is required, it shall be consistent with the Environmental Planning (EP) division guidelines for such programs. The ERO may also require that the project sponsor immediately implement a site security program if the archeological resource is at risk from vandalism, looting, or other damaging actions.

The project archeological consultant shall submit a Final Archeological Resources Report (FARR) to the ERO that evaluates the historical significance of any discovered archeological resource and describing the archeological and historical research methods employed in the archeological monitoring/data recovery program(s) undertaken. Information that may put at risk any archeological resource shall be provided in a separate removable insert within the final report.

Copies of the Draft FARR shall be sent to the ERO for review and approval. Once approved by the ERO, copies of the FARR shall be distributed as follows: California Archaeological Site Survey Northwest Information Center (NWIC) shall receive one (1) copy and the ERO shall receive a copy of the transmittal of the FARR to the NWIC. The Environmental Planning division of the Planning Department shall receive one bound copy, one unbound copy, and one unlocked, searchable PDF copy on CD; three copies of the FARR along with copies of any formal site recordation forms (CA DPR 523 series) and/or documentation for nomination to the National Register of Historic Places/California Register of Historical Resources. In instances of high public interest or interpretive value, the ERO may require a different final report content, format, and distribution than that presented above.

Project Mitigation Measure 2: Construction Noise (Implementing Eastern Neighborhoods PEIR Mitigation Measure F-2)

The project sponsor shall develop a set of site-specific noise attenuation measures under the supervision of a qualified acoustical consultant. Prior to commencing construction, a plan for such measures shall be submitted to the Department of Building Inspection to ensure that maximum feasible noise attenuation will be achieved. These attenuation measures shall include as many of the following control strategies as feasible:

- Erect temporary plywood noise barriers around the construction site, particularly where a site adjoins noise-sensitive uses;
- Utilize noise control blankets on the building structure as the building is erected to reduce noise emission from the site;
- Evaluate the feasibility of noise control at the receivers by temporarily improving the noise reduction capability of adjacent buildings housing sensitive uses;
- Monitor the effectiveness of noise attenuation measures by taking noise measurements; and
- Post signs on-site pertaining to permitted construction days and hours and complaint procedures and who to notify in the event of a problem, with telephone numbers listed.

Project Measure 3: Siting of Noise-Sensitive Uses (Implementing Eastern Neighborhoods PEIR Mitigation Measure F-4)

To reduce potential conflicts between existing noise-generating uses and new sensitive receptors, for new development including noise-sensitive uses, the project sponsor was required to provide an analysis that included, at a minimum, a site survey to identify potential noise-generating uses within 900 feet of, and that have a direct line-of-sight to, the project site, and including at least one 24-hour noise measurement (with maximum noise level readings taken at least every 15 minutes), prior to the first project approval action. The analysis which was prepared by persons qualified in acoustical analysis and/or engineering, demonstrated with reasonable certainty that Title 24 standards, where applicable, can be met, and that there are no particular circumstances about the proposed project site that appear to warrant heightened concern about noise levels in the vicinity. The report concluded that if its recommendations are incorporated into the design and construction of the proposed building, the project would achieve compliance with Title 24. Recommendations include the following: (1) on the ground floor, exterior windows and doors should have an STC rating of 32 along Arkansas Street, an STC rating of 35 along 17th Street, and an STC rating of 28 for all other locations; (2) on the second floor, exterior windows and doors should have an STC rating of 32 along Arkansas Street, an STC rating of 35 on the northeast corner of the project site, and an STC rating of 28 for all other locations; (3) on the third through fifth floors exterior windows and doors should have an STC rating of 32 along Arkansas Street, an STC rating of 35 along 17th Street, an STC rating of 32 on the eastern half of the northern façade of the building, an STC rating of 30 on the a portion of the western half of the northern façade of the building, an STC rating of 38 on the southwest and southeast corners of the building, an STC rating of 35 on the northwest and northeast corners of the building, and an STC rating of 28 for all other locations. Overall, the proposed project would achieve compliance with the Title 24 standard of DNL 45 dBA for interior noise by installing exterior windows and doors with STC ratings between 28 and 38, depending on the location. The STC ratings shall be incorporated into the final construction drawings.

Project Measure 4: Open Space in Noisy Environments (Implementing Eastern Neighborhoods PEIR Mitigation Measure F-6)

The project sponsor shall ensure that open space required under the Planning Code for such uses be protected, to the maximum feasible extent, from existing ambient noise levels that could prove annoying or disruptive to users of the open space. Implementation of this measure could involve, among other things, site design that uses the building itself to shield on-site open space from the greatest noise sources, construction of noise barriers between noise sources and open space, and appropriate use of both common and private open space in multi-family dwellings, and implementation would also be undertaken consistent with other principles of urban design. This mitigation measure has been complied with as part of this environmental review process. No further actions are required to comply with Mitigation Measure F-6 of the Eastern Neighborhoods PEIR.

Project Mitigation Measure 5: Hazardous Building Materials (Implementation of Eastern Neighborhoods PEIR Mitigation Measure L-1)

The project sponsor shall ensure that any equipment containing PCBs or DEPH, such as fluorescent light ballasts, are removed and properly disposed of according to applicable federal, state, and local laws prior to the start of renovation, and that any fluorescent light tubes, which could contain mercury, are similarly removed and properly disposed of. Any other hazardous materials identified, either before or during work, shall be abated according to applicable federal, state, and local laws.

IMPROVEMENT MEASURES**Project Improvement Measure 1: Implementation of Transportation Demand Management Strategies**

The project sponsor would implement a Transportation Demand Management (TDM) program to encourage the use of alternative forms of transportation for trips to and from the project vicinity, such as transit, bicycle, rideshare, and walking. Components of the improvement measure are as follows:

IM-1a TDM Coordinator: The project sponsor would identify a TDM Coordinator for the project site who would be responsible for the implementation and ongoing operation of all other TDM measures included in the proposed Project. The TDM Coordinator should be the single point of contact for all transportation-related questions from residents and City staff. The project sponsor may use an existing transportation management association (e.g., the Transportation Management Association of San Francisco) to supply the TDM Coordinator, or the TDM Coordinator may be an existing staff member (e.g., property manager). In either case, the TDM Coordinator does not have to work full-time at the Project site. The TDM coordinator would be the single point of contact for all transportation-related questions from building occupants and City staff. The TDM Coordinator should provide TDM training to other building staff about the transportation amenities and options available in the project vicinity.

IM-1b Transportation and Trip Planning Information Move-In Packet: Provide a transportation insert for the move-in packet that includes information on transit service (local and regional, schedules and fares), information on where transit passes may be purchased, information on the 511 Regional Rideshare Program, and nearby bike and car share programs, and information on where to find additional web-based alternative transportation materials. The move-in packet should be continuously updated as transportation options change, and the packet should be provided to each new building occupant. Muni maps as well as San Francisco Bicycle and Pedestrian maps should be provided upon request.

- IM-1c City Access to Data Collection:** As part of an ongoing effort to quantify the efficacy of TDM measures, City staff may need to access the project site (including the garage) to perform trip counts, and/or intercept surveys and/or other types of data collection. All on-site activities shall be coordinated through the TDM Coordinator. The project sponsor assures future access to the site by City staff. Providing access to existing developments for data collection purposes is also encouraged.
- IM-1d Bicycle Parking:** Increase the number of on-site secured bicycle parking beyond Planning Code requirements and/or provide additional bicycle facilities in the public right-of-way adjacent to or within a quarter mile of the project site (e.g., sidewalks, on-street parking spaces).
- IM-1e Car-Share Parking:** Provide optional car share spaces as described in Planning Code Section 166(g).
- IM-1f Transit Pass:** Offer free or subsidized Muni passes to tenants. For example, the project sponsor may offer a 50 percent subsidy for one Muni monthly pass for new residents (one per household), and employees for up to one year. The recipient would be responsible for the remainder of the costs associated with the Muni monthly pass.

Project Improvement Measure 2: Pedestrian Safety Improvements

Install audible and visual warning devices to alert pedestrians of vehicles exiting the parking garage.

Project Improvement Measure 3: On-Street Loading Management

- IM-3a** Schedule and coordinate loading activities through building management to ensure that trucks can be accommodated in the curbside loading spaces. All regularly scheduled activities requiring use of the loading space (e.g., building supply deliveries) should be coordinated directly with building management.
- IM-3b** Trucks should be discouraged from parking illegally or obstructing vehicle, transit, bicycle, or pedestrian traffic flow along any of the streets adjacent to the project site.

Project Improvement Measure 4: Construction-Related Traffic Management

- IM-4a** Limit hours of construction-related traffic, including, but not limited to, truck movements, to avoid the weekday AM and PM peak hours (7:00 a.m. to 9:00 a.m. and 4:00 p.m. to 6:00 p.m.; or other times, if approved by SFMTA).
- IM-4b** Construction contractor(s) should coordinate construction activities with other potential projects that may be constructed in the vicinity of the project site.

Project Improvement Measure 5: Off-Street Parking Traffic Enhancements

IM-5a Install a traffic signal on both the inside and outside of the garage opening to indicate the presence of approaching vehicles from the opposite direction. The traffic signal would operate on sensors/detectors, and alternate with two beacons, with one indicating a solid red signal or marked with "STOP" and another indicating a solid green signal or marked with "GO." The signals would be installed facing outward at the garage opening outside of the security gate, and at the interior of the garage facing inward into the garage ramp, and facing inward at the interior of the garage on the basement level, at the entry to the garage ramp.

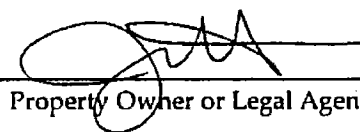
IM-5b The owner/operator of any off-street parking facility, as determined by the Planning Director, with more than 20 parking spaces (excluding loading and car-share spaces) could ensure that recurring vehicle queues¹ do not occur on the public right of way. If a recurring queue occurs, the parking facility's owner/operator could implement abatement measures as needed to abate the queue.

Suggested abatement methods include but are not limited to the following: redesign of facility layout to improve vehicle circulation and/or on-site queue capacity; employment of parking attendants; installation of LOT FULL signs with active management by parking attendants; use of valet parking or other space-efficient parking techniques; use of off-site parking facilities or shared parking with nearby uses; use of parking occupancy sensors and signage directing drivers to available spaces; travel demand management strategies such those discussed in IM-1, customer shuttles or delivery services; and/or parking demand management strategies such as parking time limits, paid parking or validated parking.


If the Planning Director, or his or her designee, suspects that a recurring queue is present, the

Department may notify the property owner in writing. Upon request, the owner/operator could hire a qualified transportation consultant to evaluate the conditions at the site for no less than seven days. The consultant could prepare a monitoring report to be submitted to the Planning Department for review. If the Planning Department determines that a recurring queue does exist, the facility owner/operator could abate the queue.

_____ I agree to implement the above mitigation measure(s) as a condition of project approval.



Property Owner or Legal Agent Signature



Date

¹ Vehicle Queue: When one or more vehicles blocking any portion of a public street, alley or sidewalk for a consecutive period of three minutes or longer on a daily or weekly basis.



SAN FRANCISCO PLANNING DEPARTMENT

Community Plan Exemption Checklist

Case No.: 2015-000453ENV
Project Address: 88 Arkansas Street
Zoning: UMU (Urban Mixed-Use) Zoning District
48-X Height & Bulk District
Block/Lot: 3953/002 & 002A
Lot Size: 29,998 square feet (combined for two lots)
Plan Area: Eastern Neighborhoods Area Plan (Showplace Square/Potrero Hill)
Project Sponsor: Julie Heinzler, Martin Building Company, (415) 348-4644
Staff Contact: Jenny Delumo – (415) 575-9146, Jenny.Delumo@sfgov.org

1650 Mission St.
Suite 400
San Francisco,
CA 94103-2479

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415.558.6378

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Planning
Information:
415.558.6377

PROJECT DESCRIPTION

Project Site

The project site is an approximately 30,000-square-foot (sf) area located in the Potrero Hill neighborhood and within the Showplace Square/Potrero Hill subarea of the Eastern Neighborhoods Area Plan (see **Figure 1**). The project site is comprised of two adjacent lots: 88 Arkansas Street/1500 17th Street (Lot 002) and 1530 17th Street (Lot 002A). The lots are located on Assessor's Block 3953, which is bounded by 17th Street to the south, 16th Street to the north, Arkansas Street to the east, and Wisconsin Street to the west. The project site currently contains two industrial buildings (see **Figure 2**). Lot 002A is developed with an approximately 9,485-gross-square-foot (gsf), 19-foot-tall, one-story-with-mezzanine industrial building. The building was constructed in 1923 and is currently vacant. Lot 002 is developed with an approximately 16,075-gsf, 24-foot-tall, two-story industrial building and a 4,440-sf surface parking lot, which provides space for approximately 13 vehicles. The rearmost portion of the building is approximately 31 feet tall. The building was constructed in 1906. Approximately 6,000 sf of the building on Lot 002 is currently occupied by Volta Industries. Collectively, the buildings provide approximately 25,560 gsf of Production, Distribution, and Repair (PDR) space.

Vehicular access to the buildings on the project site is provided by three curbs cuts: two curb cuts along the 17th Street frontage (approximately 14 feet and 18 feet wide) and one curb cut along the Arkansas Street frontage (approximately 38 feet wide). An approximately 42-foot-wide curb cut, located on Arkansas Street, is inactive and has been painted white. It currently serves as two, perpendicular on-street parking spaces. Access to the surface parking lot is provided via an approximately 38-foot-wide curb cut off Arkansas Street. Approximately four parallel parking spaces are located along the 17th Street frontage and approximately 15 perpendicular parking spaces are provided along the Arkansas Street frontage. There are no on-street loading spaces.

Project Characteristics

The proposed project would demolish the two, one-story-over-basement buildings and surface parking lot on the project site. The sponsor proposes to construct an approximately 143,990-gsf, five-story mixed-use building. The proposed building would be approximately 48 feet tall (up to 65 feet tall with staircase and

elevator penthouses) and include approximately 111,695 gsf of residential space and 3,275 gsf of retail space. An approximately 29,020-gsf, below-grade parking garage would provide off-street parking for about 98 vehicles, including one car share space and two Americans with Disabilities Act (ADA) - accessible spaces. The project would provide 127 Class I bicycle parking spaces and 1 Class II bicycle parking space in the garage, which would be accessed via an approximately 12-foot-wide curb cut on Arkansas Street that would be created by reducing an existing, roughly 42-foot-wide curb cut. Approximately 12 Class II bicycle spaces would be placed on the southwest corner of the project site. **Table 1**, below, lists the proposed project features.

Table 1 – Proposed Project Features

Use	88 Arkansas Street (Existing)	88 Arkansas Street (Proposed)
PDR	25,560 sf	0
Residential	0	111,695 sf (127 units)
Office	0	0
Retail	0	3,275 sf
Parking	4,440 sf (surface)	29,020 sf (subterranean)
Total	30,000 sf	143,990 sf
Number of buildings	2	1
Number of stories	1-2	5
Height to Roofline(s)	24 feet	48 feet
Parking Spaces	13	98
Bicycle Parking Spaces	0	140

The residential component of the proposed project would include approximately 25 studio apartments, 51 one-bedroom apartments, 41 two-bedroom apartments, and 10 three-bedroom apartments, for a total of 127 residential dwelling units. Twenty-five of the dwelling units (twenty percent) would be designated affordable units (see **Table 2**). Two entrances to the residential portion of the building would be located along the Arkansas Street frontage. On the ground floor, five of the residential units fronting Arkansas Street would have direct street access. The retail component of the proposed project would be located on the ground floor with pedestrian access provided via three entrances along the 17th Street frontage. Proposed open space would be comprised of private balconies, two interior courtyards on the ground floor, an approximately 1,295-sf solarium on the fifth floor, and an approximately 2,515-sf artificial turf area and 2,315-sf deck on the roof. The approximately 185-sf north courtyard would be located in the northwest corner of the project site and the approximately 2,790-sf south courtyard would be located along the western perimeter of the project site (see **Figure 4**).

Table 2 – Proposed Residential Dwelling Unit Mix

Unit Type	Market Rate Units	Affordable Units	Total Units
Studio	20	5	25
1 Bedroom	41	10	51
2 Bedroom	33	8	41
3 Bedroom	8	2	10
Total	102	25	127

A new bulb-out extension is proposed for the northwest corner of the project site at the 17th Street and Arkansas Street intersection. The bulb-out would not extend into traffic lanes. A fire hydrant exists at this location, and may need to be relocated in order to accommodate the proposed bulb-out. The project also proposes to widen the portion of the sidewalk along Arkansas Street not included in the bulb-out to approximately 24 feet wide. Roughly six on-street parallel parking spaces would be provided on 17th Street and four on-street parallel parking spaces would be provided on Arkansas Street, one of the spaces on Arkansas Street would be ADA accessible. On-street loading space would be provided by an approximately 45-foot-long loading zone located between the parallel parking spaces and garage entrance on Arkansas Street. The on-street parking spaces that would be located along 17th Street would be created by removing a 14-foot-long curb cut and an 18-foot-long curb cut, for a total of 32 feet of restored curb length. The on-street parking and loading spaces that would be located along Arkansas Street would be created by reducing a 42-foot-long curb cut to 24 feet in length.

The project site includes approximately eight street trees along the Arkansas Street frontage and four street trees along the 17th Street frontage. The proposed project would retain all of the existing street trees on the project site and provide an additional three street trees on 17th Street and two street trees on Arkansas Street. The proposed project is also seeking Leadership in Energy and Environmental Design (LEED)® for Homes Platinum certification from the U.S. Green Building Council.

Project Construction

On-site construction work would consist of abatement and demolition of the existing structures, excavation and subgrade work (including subsurface treatment, if required by the Department of Public Health (DPH)), installation of the foundation, construction of the superstructure, exterior wall construction and finishes, and interior construction and finishes. Project construction is anticipated to last approximately 19 months.

Abatement and demolition of the existing buildings on the project site would be completed in approximately one month. Following demolition, the project site would be excavated to a maximum depth of roughly 20 feet below grade, resulting in approximately 18,000 cubic yards of soil disturbance. The project sponsor proposes to export all of the excavated soil in one phase. Creation of temporary slopes and shoring would also take place during this phase, which is expected to last approximately three months.

Due to the presence of heterogeneous fill and weak marsh deposits on the site, the project sponsor proposes to support the building using a mat slab foundation. Installation of the foundation is expected to last approximately two months.

The structure of the proposed building would be constructed over the course of approximately five months. The last month of this phase would overlap with the first month of the exterior finishing phase, which would take approximately four months to complete. Towards the second month of exterior finishing, the contractor would begin constructing the building's interiors. It is expected that the building's interiors would be installed within approximately seven months.

Figure 1 – Project Site Location



Figure 2 – Existing Site Plan

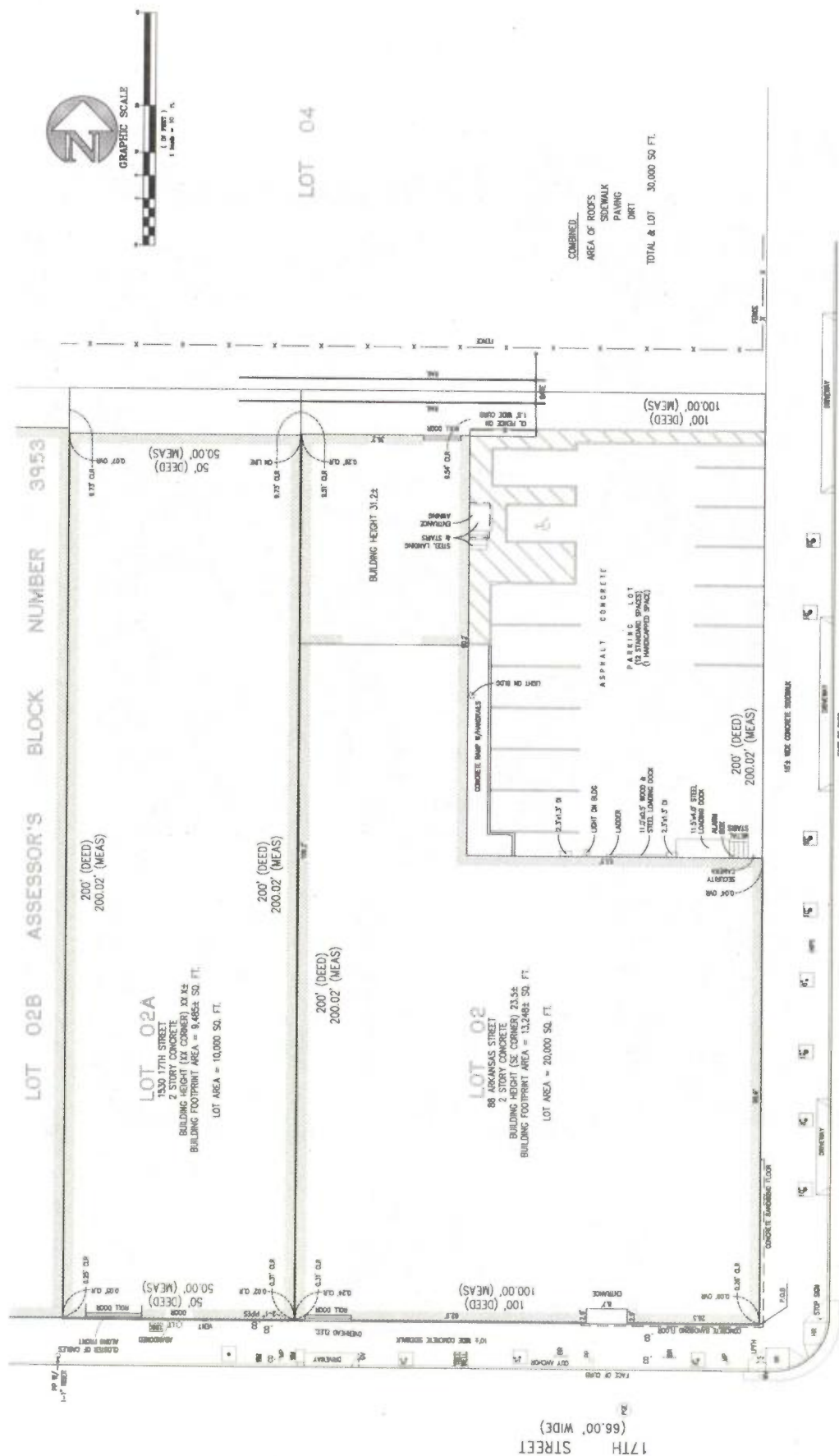


Figure 3 – Proposed Project Site Plan



[illegible]

Figure 5 – Proposed Basement Plan

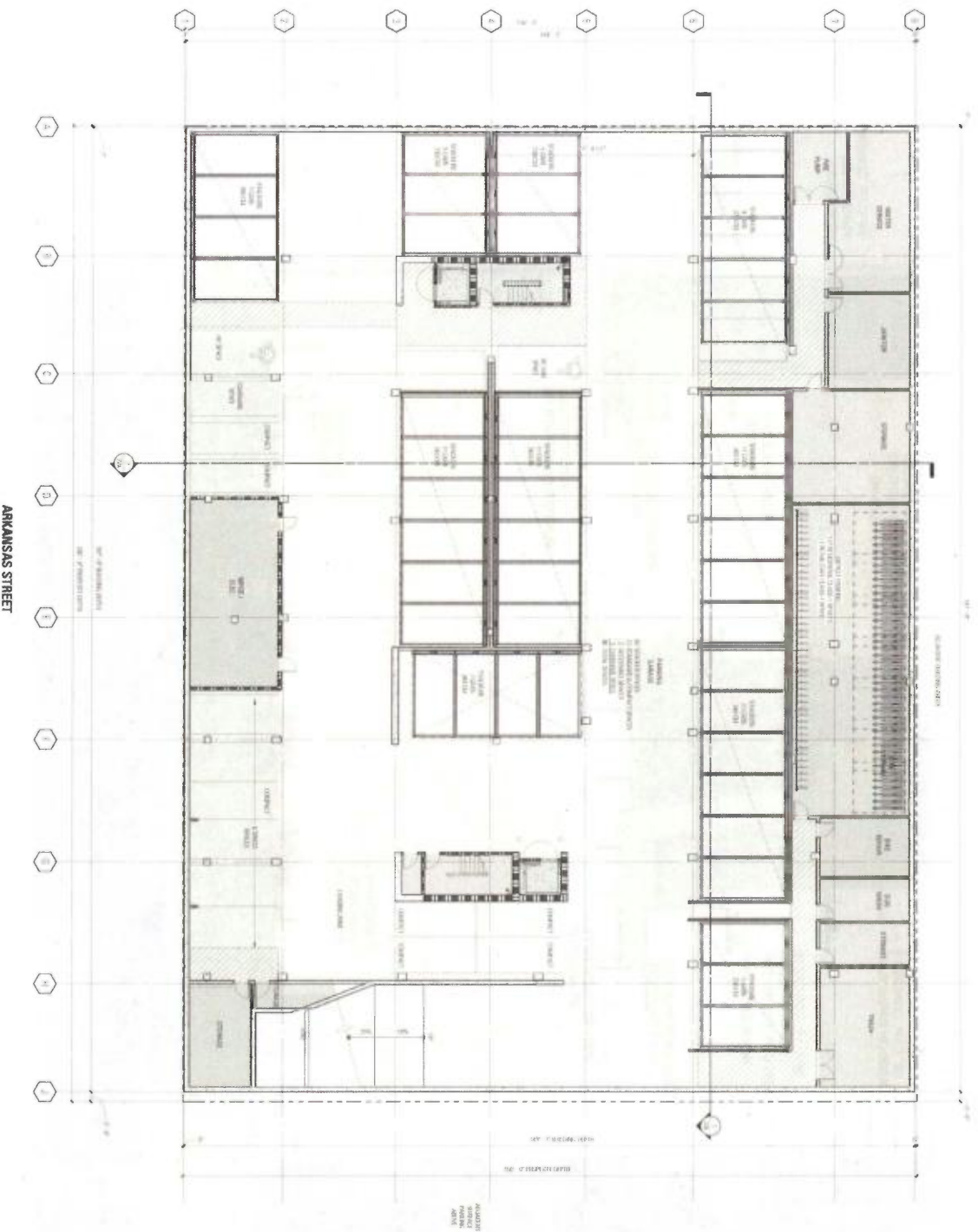


Figure 6 – Proposed Ground Floor



Figure 7 – Proposed Second Floor



Figure 8 – Proposed Third and Fourth Floors



Figure 9 – Proposed Fifth Floor



Figure 10 – Proposed Roof Plan

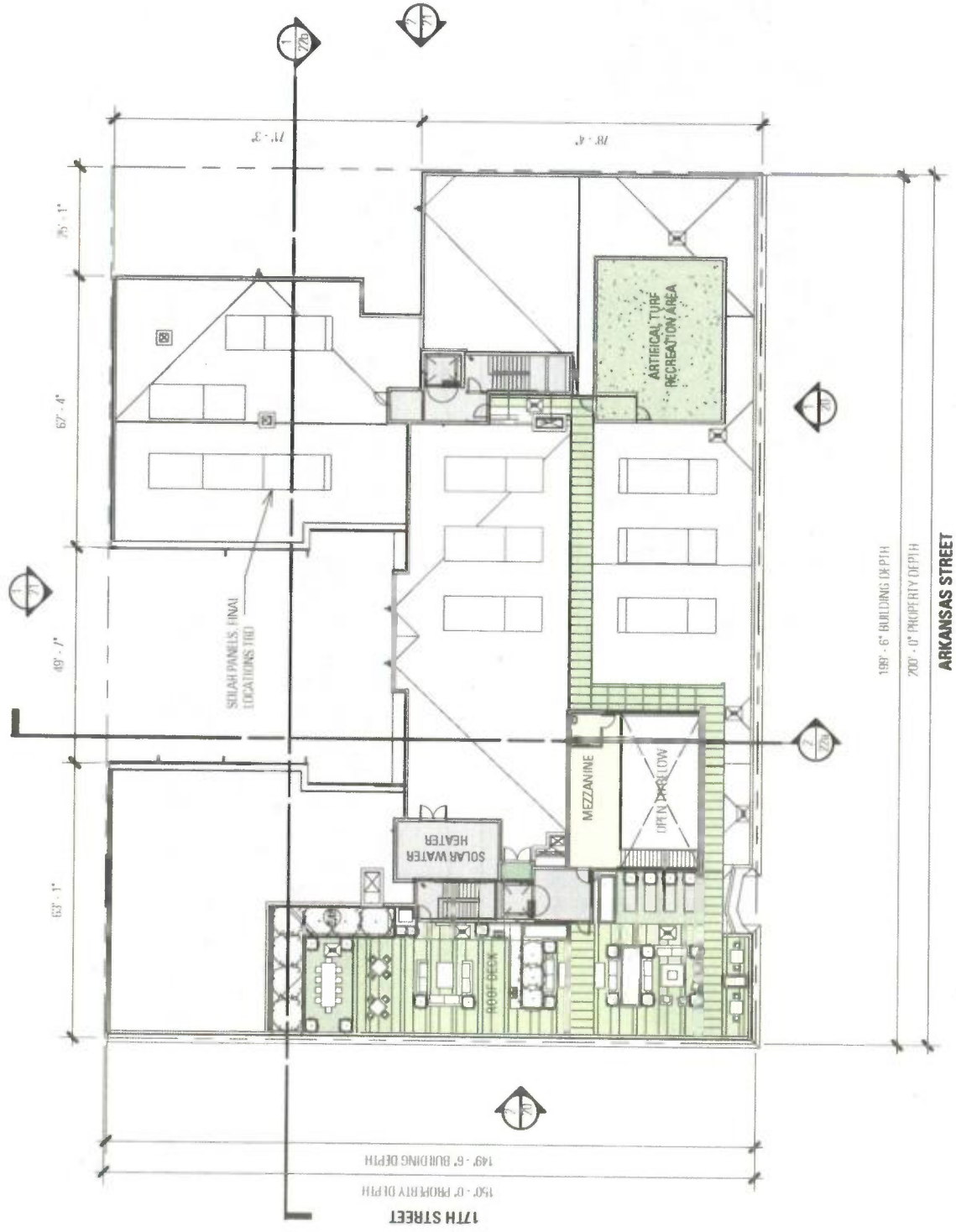
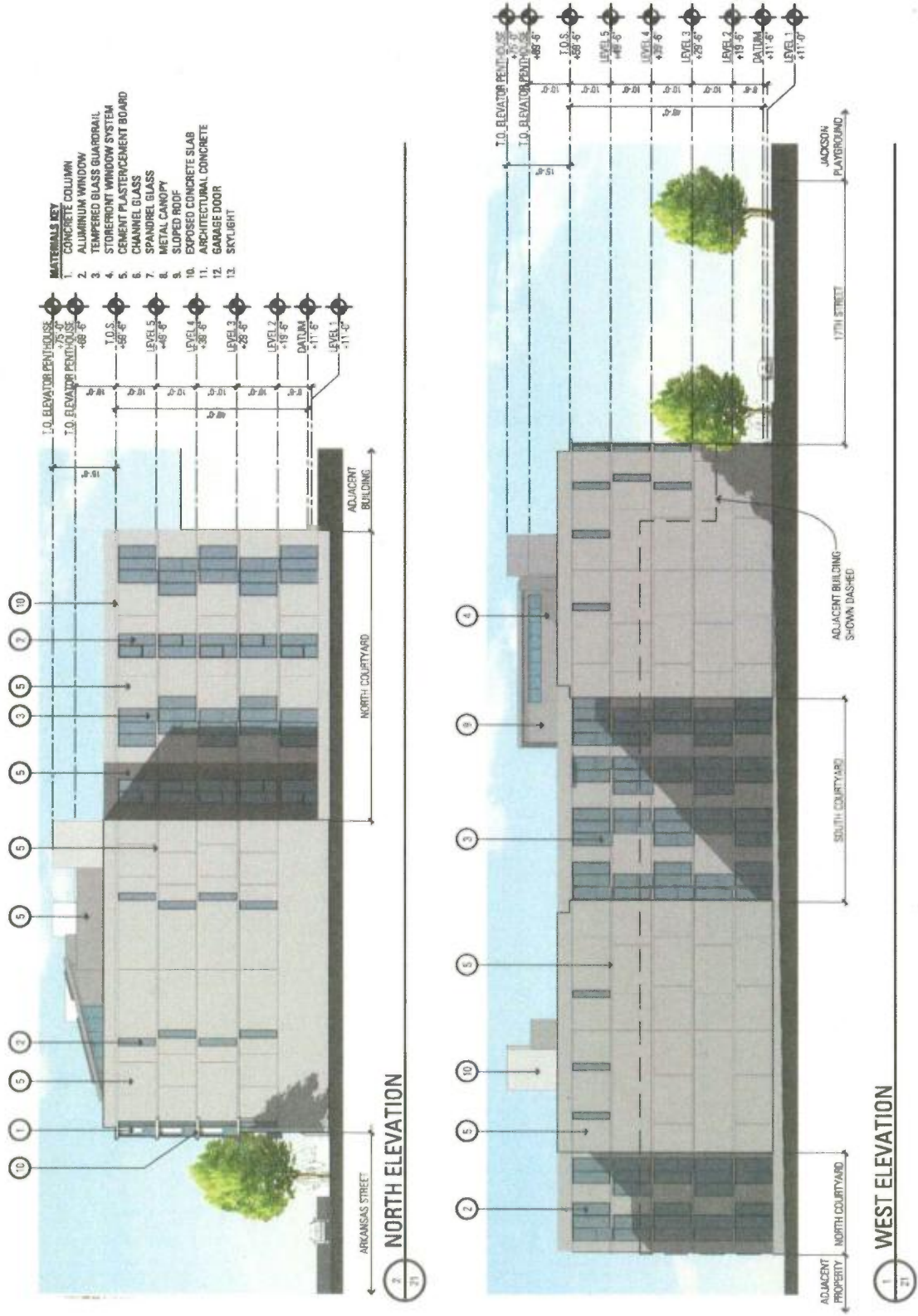




Figure 12 – Proposed North/South and East/West Sections



Project Setting

The project site vicinity is characterized by a mix of industrial, PDR, educational, retail, office, and open spaces uses. The subject block, along with the blocks to the east and west of the project site between 16th and 17th Streets, is zoned UMU (Urban Mixed Use). The block north of the project site on the north side of 16th Street is zoned PDR-1-D (Production, Distribution, and Repair – 1 Design). The block directly south of the project site is occupied by Jackson Playground and Recreation Center and is zoned P (Public). The majority of the block southwest of the project site is zoned RH-3 (Residential-House, Three Family). RH-2 (Residential-House, Two Family) districts are also located in the site vicinity. The project site, along with adjacent lots to the east and west of the site, are within a 48-X height and bulk district. The project vicinity includes 68-X height and bulk districts (on lots north of the project site) and 40-X height and bulk districts (on lots south of the project site).

The low-density scale of development in the project site vicinity includes one- to three-story-tall residential and commercial buildings. An approximately 15-foot-tall, one-story entertainment venue is located on the southwest corner of the block just west of the project site and the one-story portion of a commercial building on the southeast corner of the subject block is approximately 15 feet tall; however, the remainder of the buildings on the subject block and in the site vicinity are approximately 20 to 40 feet tall. On the subject block, retail and warehouse buildings are adjacent to the portion of the project site fronting 17th Street. One-story office and warehouse buildings front Arkansas and Wisconsin Streets, including a retail store and warehouse space with frontage on Wisconsin and 16th Streets. A surface parking lot, with frontage on Wisconsin Street and Arkansas Street, is located in the middle of the subject block. An abandoned rail spur is located between the parking lot and the project site. A one-story industrial building is located across from the project site on the east side of Arkansas Street. A one-story auto body shop and one-story warehouse are located on the west side of Wisconsin Street. Other uses in the area include Live Oak School (one block south of the project site), Anchor Steam Brewery (one block southwest), Thee Parkside bar and music venue (one block west), and The Connecticut Yankee restaurant (one block east).

As previously mentioned, the subject block is bounded by 16th, 17th, Arkansas, and Wisconsin Streets. Running east/west, 16th and 17th Streets are two-lane, two-way streets with parallel parking on both sides. 16th and 17th Streets are part of Citywide Bicycle Route 40. Running north/south, Arkansas and Wisconsin Streets are unstriped two-lane, two-way streets with parallel parking on the east side of each street and perpendicular parking on the west side of each street. Arkansas and Wisconsin Streets are not part of the Citywide Bicycle Network. With regards to the subject block, the sidewalks along 16th and 17th Streets are approximately 10 feet wide and the sidewalks along Arkansas and Wisconsin Streets are approximately 15 feet wide. Roughly 379 on-street parking spaces are located within the area bounded by 16th Street to the north, 17th Street to the south, Arkansas Street to the east, and Carolina Street to the west with additional on-street parking provided throughout the project site vicinity. The site vicinity does not contain any city-owned or public off-street parking facilities. The off-street parking facilities in the vicinity of the project site primarily serve employees and patrons of private businesses.

Required Approvals

The proposed 88 Arkansas Street project would require the following approvals:

Actions by the Planning Commission

- **Large Project Authorization.** Pursuant to Planning Code Section 329, the proposed project requires a Large Project Authorization (LPA). LPAs are required for new construction greater than 25,000 gross square feet in Urban Mixed Use (UMU) Zoning Districts. The LPA would also authorize project-specific modifications to the following Planning Code requirements:
 - Rear Yard (Planning Code Section 134)
 - Exposure (Planning Code Section 140)
 - Loading (Planning Code Section 152.1)
 - Dwelling Unit Mix (Planning Code Section 207.6; Interpretation of Planning Code Section 102.29)

Actions by other City Departments

- **Department of Building Inspection (DBI).** Approval of demolition, grading, building and occupancy permits for demolition of the existing structures and new construction.
- **Department of Public Health (DPH).** Approval of a Site Mitigation Plan pursuant to the Maher Ordinance prior to the commencement of any excavation work, and approval of a Soil Mitigation Plan and Dust Control Plan prior to construction-period activities.
- **San Francisco Fire Department (SFFD).** Approval of any changes to the location of existing fire hydrants and water valves.
- **San Francisco Municipal Transportation Agency (SFMTA).** Approval of all proposed changes in curb cuts and parking zones pursuant to the SFMTA Color Curb Program. Coordination with the SFMTA Interdepartmental Staff Committee on Traffic and Transportation to coordinate temporary construction-related changes to the transportation network.
- **San Francisco Public Works Department (Public Works), Bureau of Streets and Mapping.** Approval of modifications to public sidewalks, street trees, curb cuts, and bulb out extensions.
- **San Francisco Public Utilities Commission (SFPUC).** Approval of a stormwater control plan and an erosion and sediment control plan prior to commencing construction.

Approval of the Large Project Authorization by the Planning Commission would be the Approval Action for the project. The Approval Action date establishes the start of the 30-day appeal period for this CEQA exemption determination pursuant to Section 31.04(h) of the San Francisco Administrative Code.

EVALUATION OF ENVIRONMENTAL EFFECTS

This Community Plan Exemption (CPE) Checklist evaluates whether the environmental impacts of the proposed project are addressed in the Programmatic Environmental Impact Report for the Eastern

Neighborhoods Rezoning and Area Plans (Eastern Neighborhoods PEIR).¹ The CPE Checklist indicates whether the proposed project would result in significant impacts that: (1) are peculiar to the project or project site; (2) were not identified as significant project-level, cumulative, or off-site effects in the PEIR; or (3) are previously identified significant effects, which as a result of substantial new information that was not known at the time that the Eastern Neighborhoods PEIR was certified, are determined to have a more severe adverse impact than discussed in the PEIR. Such impacts, if any, will be evaluated in a project-specific Mitigated Negative Declaration or Environmental Impact Report. If no such impacts are identified, the proposed project is exempt from further environmental review in accordance with Public Resources Code Section 21083.3 and CEQA Guidelines Section 15183.

Mitigation measures identified in the PEIR are discussed under each topic area, and measures that are applicable to the proposed project are provided under the Mitigation Measures Section at the end of this checklist.

The Eastern Neighborhoods PEIR identified significant impacts related to land use, transportation, cultural resources, shadow, noise, air quality, and hazardous materials. Additionally, the PEIR identified significant cumulative impacts related to land use, transportation, and cultural resources. Mitigation measures were identified for the above impacts and reduced all impacts to less-than-significant except for those related to land use (cumulative impacts on PDR use), transportation (program-level and cumulative traffic impacts at nine intersections; program-level and cumulative transit impacts on seven Muni lines), cultural resources (cumulative impacts from demolition of historical resources), and shadow (program-level impacts on parks).

The proposed project would include construction of an approximately 48-foot-tall, 142,635-gsf mixed-use building. The proposed building would encompass approximately 111,620 gsf of residential space (127 dwelling units), 3,275 gsf of commercial space, and a 27,740-gsf below-grade parking garage with space for 98 vehicles and 107 bicycles. 25 of the proposed dwelling units (or 20 percent) would be below market rate. As discussed below in this checklist, the proposed project would not result in new, significant environmental effects, or effects of greater severity than were already analyzed and disclosed in the Eastern Neighborhoods PEIR.

CHANGES IN THE REGULATORY ENVIRONMENT

Since the certification of the Eastern Neighborhoods PEIR in 2008, several new policies, regulations, statutes, and funding measures have been adopted, passed, or are underway that affect the physical environment and/or environmental review methodology for projects in the Eastern Neighborhoods plan areas. As discussed in each topic area referenced below, these policies, regulations, statutes, and funding measures have or will implement mitigation measures or further reduce less-than-significant impacts identified in the PEIR. These include:

- State statute regulating Aesthetics and Parking Impacts for Transit Priority Infill, effective January 2014 (see associated heading below);

¹ San Francisco Planning Department, Eastern Neighborhoods Rezoning and Area Plans Final Environmental Impact Report (PEIR), Planning Department Case No. 2004.0160E, State Clearinghouse No. 2005032048, certified August 7, 2008. This material and subsequent materials are available for review at the Planning Department, 1650 Mission Street, Suite 400, under the aforementioned Case No. and/or Case No. 2015-000453ENV.

- San Francisco Bicycle Plan update adoption in June 2009, Better Streets Plan adoption in 2010, Transit Effectiveness Project (aka "Muni Forward") adoption in March 2014, Vision Zero adoption by various City agencies in 2014, Proposition A and B passage in November 2014, and the Transportation Sustainability Program process (see Checklist section "Transportation");
- San Francisco ordinance establishing Noise Regulations Related to Residential Uses Near Places of Entertainment effective June 2015 (see Checklist section "Noise");
- San Francisco ordinances establishing Construction Dust Control, effective July 2008, and Enhanced Ventilation Required for Urban Infill Sensitive Use Developments, amended December 2014 (see Checklist section "Air Quality");
- San Francisco Clean and Safe Parks Bond passage in November 2012 and San Francisco Recreation and Open Space Element of the General Plan adoption in April 2014 (see Checklist section "Recreation");
- Urban Water Management Plan adoption in 2011 and Sewer System Improvement Program process (see Checklist section "Utilities and Service Systems"); and
- Article 22A of the Health Code amended August 2013 (see Checklist section "Hazardous Materials").

CHANGES IN THE PHYSICAL ENVIRONMENT

Since the certification of the Eastern Neighborhoods PEIR in 2008, as evidenced by the volume of development applications submitted to the Planning Department since 2012, the pace of development activity has increased in the Eastern Neighborhoods Plan areas. The Eastern Neighborhoods PEIR projected that implementation of the Eastern Neighborhoods Plan could result in a substantial amount of growth within the Eastern Neighborhoods Plan area, resulting in an increase of approximately 7,400 to 9,900 net dwelling units and 3,200,000 to 6,600,000 square feet of net non-residential space (excluding PDR loss) throughout the lifetime of the Plan (year 2025).² The growth projected in the Eastern Neighborhoods PEIR was based on a soft site analysis (i.e., assumptions regarding the potential for a site to be developed through the year 2025) and not based upon the created capacity of the rezoning options (i.e., the total potential for development that would be created indefinitely).³

² Tables 12 through 16 of the Eastern Neighborhoods Draft EIR and Table C&R-2 in the Comments and Responses show projected net growth based on proposed rezoning scenarios. A baseline for existing conditions in the year 2000 was included to provide context for the scenario figures for parcels affected by the rezoning, not projected growth totals from a baseline of the year 2000. Estimates of projected growth were based on parcels that were to be rezoned and did not include parcels that were recently developed (i.e., parcels with projects completed between 2000 and March 2006) or have proposed projects in the pipeline (i.e., projects under construction, projects approved or entitled by the Planning Department, or projects under review by the Planning Department or Department of Building Inspection). Development pipeline figures for each Plan Area were presented separately in Tables 5, 7, 9, and 11 in the Draft EIR. Environmental impact assessments for these pipeline projects were considered separately from the Eastern Neighborhoods rezoning effort.

³ San Francisco Planning Department, Community Planning in the Eastern Neighborhoods, Rezoning Options Workbook, Draft, February 2003.

As of July 31, 2015, projects containing 8,559 dwelling units and 2,231,595 square feet of non-residential space (excluding PDR loss) have completed or are proposed to complete environmental review⁴ within the Eastern Neighborhoods Plan area. These estimates include projects that have completed environmental review (4,885 dwelling units and 1,472,688 square feet of non-residential space) and foreseeable projects, including the proposed project (3,647 dwelling units and 758,907 square feet of non-residential space). Foreseeable projects are those projects for which environmental evaluation applications have been submitted to the San Francisco Planning Department. Of the 4,885 dwelling units that have completed environmental review, building permits have been issued for 3,710 dwelling units, or approximately 76 percent of those units (information is not available regarding building permits for non-residential square footage). An issued building permit means the buildings containing those dwelling units are currently under construction or open for occupancy.

Within the Showplace Square/Potrero Hill subarea, the Eastern Neighborhoods PEIR projected that implementation of the Eastern Neighborhoods Plan could result in an increase of approximately 2,300 to 3,900 net dwelling units and 1,500,000 to 1,700,000 square feet net non-residential space (excluding PDR loss) through the year 2025. As of July 31, 2015, projects containing 3,266 dwelling units and 865,849 square feet of non-residential space (excluding PDR loss) have completed or are proposed to complete environmental review within the Showplace Square/Potrero Hill subarea. These estimates include projects that have completed environmental review (1,822 dwelling units and 621,768 square feet of non-residential space) and foreseeable projects, including the proposed project (1,444 dwelling units and 244,081 square feet of non-residential space). Of the 1,822 dwelling units that have completed environmental review, building permits have been issued for 1,105 dwelling units, or approximately 61 percent of those units.

Growth that has occurred within the Plan area since adoption of the Eastern Neighborhoods PEIR has been planned for and the effects of that growth were anticipated and considered in the Eastern Neighborhoods PEIR. The reasonably foreseeable growth in the residential land use category is approaching the projections within the Eastern Neighborhoods PEIR and the non-residential reasonably foreseeable growth is between approximately 34 and 69 percent of the non-residential projections in the Eastern Neighborhoods PEIR. The Eastern Neighborhoods PEIR utilized the growth projections to analyze the physical environmental impacts associated with that growth for the following environmental impact topics: Land Use; Population, Housing, Business Activity, and Employment; Transportation; Noise; Air Quality; Parks, Recreation, and Open Space; Utilities/Public Services; and Water. The analysis took into account the overall growth in the Eastern Neighborhoods and did not necessarily analyze in isolation the impacts of growth in one land use category, although each land use category may have differing severities of effects. In summary, projects proposed within the Eastern Neighborhoods Plan Area have not exceeded the overall growth that was projected in the Eastern Neighborhoods PEIR; therefore foreseeable growth within the Plan Areas do not present substantial new information that was not known at the time of the PEIR and would not result in new significant environmental impacts or substantially more severe adverse impacts than discussed in the PEIR.

⁴ For this and the Land Use and Land Use Planning section, environmental review is defined as projects that have or are relying on the growth projections and analysis in the Eastern Neighborhoods PEIR for environmental review (i.e., Community Plan Exemptions or Focused Mitigated Negative Declarations and Focused Environmental Impact Reports with an attached Community Plan Exemption Checklist).

AESTHETICS AND PARKING IMPACTS FOR TRANSIT PRIORITY INFILL DEVELOPMENT

Public Resources Code Section 21099(d), effective January 1, 2014, provides that, "aesthetics and parking impacts of a residential, mixed-use residential, or employment center project on an infill site located within a transit priority area shall not be considered significant impacts on the environment." Accordingly, aesthetics and parking are no longer to be considered in determining if a project has the potential to result in significant environmental effects for projects that meet all of the following three criteria:

- a) The project is in a transit priority area;
- b) The project is on an infill site; and
- c) The project is residential, mixed-use residential, or an employment center.

The proposed project meets the Public Resources Code Section 21099 because:

1. The proposed project would include the construction of a mixed-use residential and commercial building, thus meeting the criterion that the proposed project consists of residential, mixed-use residential, or employment center uses;
2. The project site is developed with two industrial buildings and bounded by fully developed lots serving industrial and commercial uses, thus meeting the criterion that the proposed project is located on an infill site; and
3. The proposed project is located within one-half mile of an existing major transit stop, thus meeting the criterion that the proposed project is located in a transit priority area.

The proposed project meets each of the above three criteria, thus this CPE Checklist does not consider aesthetics or parking in determining the significance of project impacts under CEQA. Project elevations are included in the project description.

Topics:	<i>Significant Impact Peculiar to Project or Project Site</i>	<i>Significant Impact not Identified in PEIR</i>	<i>Significant Impact due to Substantial New Information</i>	<i>No Significant Impact not Previously Identified in PEIR</i>
1. LAND USE AND LAND USE PLANNING— Would the project:				
a) Physically divide an established community?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Conflict with any applicable land use plan, policy, or regulation of an agency with jurisdiction over the project (including, but not limited to the general plan, specific plan, local coastal program, or zoning ordinance) adopted for the purpose of avoiding or mitigating an environmental effect?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Have a substantial impact upon the existing character of the vicinity?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

The Eastern Neighborhoods PEIR analyzed a range of potential rezoning options and considered the effects of losing between approximately 520,000 to 4,930,000 square feet of PDR space in the Plan Areas throughout the lifetime of the Plan (year 2025). This was compared to an estimated loss of approximately 4,620,000 square feet of PDR space in the Plan Area under the No Project scenario. Within the Showplace Square/Potrero Hill subarea, the Eastern Neighborhoods PEIR considered the effects of losing up to approximately 991,000 square feet of PDR space through the year 2025. The Eastern Neighborhoods PEIR determined that adoption of the Area Plans would result in an unavoidable significant impact on land use character due to the cumulative loss of PDR space. This impact was addressed in a Statement of Overriding Considerations with CEQA Findings and adopted as part of the Eastern Neighborhoods Rezoning and Areas Plans approval on January 19, 2009.

As of July 31, 2015, projects containing the removal of 1,748,422 net square feet of PDR space have completed or are proposed to complete environmental review within the Eastern Neighborhoods Plan area. These estimates include projects that have completed environmental review (796,446 square feet of PDR space loss) and foreseeable projects, including the proposed project (951,976 square feet of PDR space loss). Foreseeable projects are those projects for which environmental evaluation applications have been submitted to the San Francisco Planning Department. As of July 31, 2015, projects containing the removal of approximately 803,193 net square feet of PDR space have completed or are proposed to complete environmental review within the Showplace Square/Potrero Hill subarea. These estimates include projects that have completed environmental review (307,970 square feet of PDR space loss) and foreseeable projects, including the proposed project (495,223 square feet of PDR space loss).

The Eastern Neighborhoods Areas include PDR clusters where similar types of PDR-related businesses are located near one another in order to capitalize on their shared proximity to customers, transportation, labor, and infrastructure. By forming in clusters, PDR businesses are also able to share resources and information. One of the objectives of the Eastern Neighborhoods Area Plans, as discussed in the PEIR, was to encourage new housing development while preserving a sufficient supply of land for PDR businesses. Thus, the PEIR found that in order to achieve this objective a key element of the Plan would be establishing districts that would encourage transitional development patterns between business and employment districts (e.g., PDR and commercial districts) and predominately residential neighborhoods. Transitions between PDR districts and residential area would be achieved through Urban Mixed Use (UMU) and Mixed-Use, Residential (MUR) districts, which allow some PDR uses in combination with commercial, residential, and mixed uses. The PEIR found that this development pattern would reduce PDR displacement and minimize the secondary economic effects related to increases in land values that

occur through the conversion of specific sites to non-industrial uses, undermining the economic viability of existing adjacent industrial clusters. However, the PEIR determined that implementation of the Area Plans would likely result in the eventual displacement of some existing PDR businesses and employment. The PEIR noted that the area between 16th and Mariposa Streets, which include the project site, is predominately characterized by a mix of land uses that included Light PDR, PDR warehouses, dining and entertainment, and some residential uses. The project site was previously zoned M-2 (Heavy Industrial) with a Height and Bulk designation of 40-X. As part of the rezoning process that occurred subsequent to certification of the Eastern Neighborhoods Rezoning and Area Plans PEIR, the site was zoned UMU with a Height and Bulk designation of 48-X.

As discussed in the Project Description section, the project site is developed with two industrial buildings. The building on Lot 002 provides approximately 16,075 sf of PDR space and the building on Lot 002A provides approximately 9,485 sf of PDR space. Thus, demolition of the existing buildings and development of the proposed project would result in the net loss of approximately 25,560 square feet of PDR building space. The PEIR considers the presence of PDR businesses and activities and how they may operate as PDR clusters. The roughly 25,560 sf of industrial use on the project site, combined with uses in the PDR-1-D district located north of the project site, may form a PDR cluster. PDR uses at the project site would have to relocate and may not be able to relocate near other similar PDR uses, thus potentially reducing the influence of this PDR cluster in the site vicinity and contributing to the significant land use impact identified in the Eastern Neighborhoods PEIR. Thus, the proposed project would contribute considerably to the significant cumulative land use impact related to loss of PDR uses that was identified in the Eastern Neighborhoods PEIR.

The proposed project is within the development density as envisioned for the site under the Eastern Neighborhoods PEIR. While the proposed loss of 25,560 gross square feet of existing PDR uses represents a considerable contribution to the cumulative loss of PDR space analyzed in the Eastern Neighborhoods PEIR, it would not result in significant impacts that were not previously identified or a more severe adverse impact than analyzed in the PEIR.

The Eastern Neighborhoods PEIR determined that implementation of the Area Plans would not create any new physical barriers in the Eastern Neighborhoods because the rezoning and Area Plans do not provide for any new major roadways, such as freeways that would disrupt or divide the project area or individual neighborhoods or subareas.

The Citywide Planning and Current Planning Divisions of the Planning Department determined that the proposed project is permitted in the UMU District and is consistent with the bulk, density, and land uses as envisioned in the Showplace Square/Potrero Hill Area Plan. The proposed project would be located in the 16th-17th Street Corridor, which is intended to encourage development of new housing mixed with smaller neighborhood-serving retail and remaining PDR uses. The proposed project was determined to be consistent with the development density envisioned in the Showplace Square/Potrero Hill Plan Area.^{5,6}

⁵ Adam Varat, San Francisco Planning Department, Community Plan Exemption Eligibility Determination, Citywide Planning and Policy Analysis, 88 Arkansas Street, June 10, 2015. This document is available for review at the San Francisco Planning Department, 1650 Mission Street, Suite 400 as part of Case File No. 2015-000453ENV.

⁶ Jeff Joslin, San Francisco Planning Department, Community Plan Exemption Eligibility Determination, Current Planning Analysis, 88 Arkansas Street, December 30, 2015. This document is available for review at the San Francisco Planning Department, 1650 Mission Street, Suite 400 as part of Case File No. 2015-000453ENV.

Because the proposed project is consistent with the development density established in the Eastern Neighborhoods Rezoning and Area Plans, implementation of the proposed project would not result in significant impacts that were not identified in the Eastern Neighborhoods PEIR related to land use and land use planning, and no mitigation measures are necessary.

Topics:	<i>Significant Impact Peculiar to Project or Project Site</i>	<i>Significant Impact not Identified in PEIR</i>	<i>Significant Impact due to Substantial New Information</i>	<i>No Significant Impact not Previously Identified in PEIR</i>
2. POPULATION AND HOUSING— Would the project:				
a) Induce substantial population growth in an area, either directly (for example, by proposing new homes and businesses) or indirectly (for example, through extension of roads or other infrastructure)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Displace substantial numbers of existing housing units or create demand for additional housing, necessitating the construction of replacement housing?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Displace substantial numbers of people, necessitating the construction of replacement housing elsewhere?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

One of the objectives of the Eastern Neighborhoods Area Plans is to identify appropriate locations for housing in the City's industrially zoned land to meet the citywide demand for additional housing. The PEIR concluded that an increase in population in the Plan Areas is expected to occur as a secondary effect of the proposed rezoning and that any population increase would not, in itself, result in adverse physical effects, but would serve to advance key City policy objectives, such as providing housing in appropriate locations next to Downtown and other employment generators and furthering the City's Transit First policies. It was anticipated that the rezoning would result in an increase in both housing development and population in all of the Area Plan neighborhoods. The Eastern Neighborhoods PEIR determined that the anticipated increase in population and density would not result in significant adverse physical effects on the environment. No mitigation measures were identified in the PEIR.

The project site is currently developed with two industrial buildings: (1) 88 Arkansas Street/1500 17th Street (Lot 002) is partially (approximately 6,000 sf) occupied by Volta Industries; (2) 1530 17th Street (Lot 002A) was formerly occupied by Flynn and Enslow Wire Products and Screening Company, but has since been vacant. Thus, the proposed project would not displace any housing. The proposed mixed-used development would include approximately 127 net dwelling units, 25 of which would be designated below market rate units. Thus, the proposed project would add to San Francisco's affordable housing stock and overall housing inventory. In addition to the residential space proposed, the project would include approximately 3,275 gsf of net commercial space. The project sponsor anticipates the space would be occupied by a café or a similar neighborhood-serving retail use. As stated in the "Changes in the Physical Environment" section above, these direct effects of the proposed project on population and housing are within the scope of the population growth anticipated under the Eastern Neighborhoods Rezoning and Area Plans and evaluated in the Eastern Neighborhoods PEIR.

For the above reasons, the proposed project would not result in significant impacts on population and housing that were not identified in the Eastern Neighborhoods PEIR.

<i>Topics:</i>	<i>Significant Impact Peculiar to Project or Project Site</i>	<i>Significant Impact not Identified in PEIR</i>	<i>Significant Impact due to Substantial New Information</i>	<i>No Significant Impact not Previously Identified in PEIR</i>
3. CULTURAL AND PALEONTOLOGICAL RESOURCES—Would the project:				
a) Cause a substantial adverse change in the significance of a historical resource as defined in §15064.5, including those resources listed in Article 10 or Article 11 of the San Francisco Planning Code?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Cause a substantial adverse change in the significance of an archaeological resource pursuant to §15064.5?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Directly or indirectly destroy a unique paleontological resource or site or unique geologic feature?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Disturb any human remains, including those interred outside of formal cemeteries?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Historic Architectural Resources

Pursuant to CEQA Guidelines Sections 15064.5(a)(1) and 15064.5(a)(2), historical resources are buildings or structures that are listed, or are eligible for listing, in the California Register of Historical Resources or are identified in a local register of historical resources, such as Articles 10 and 11 of the San Francisco Planning Code.

The Eastern Neighborhoods PEIR determined that future development facilitated through the changes in use districts and height limits under the Eastern Neighborhoods Area Plans could have substantial adverse changes on the significance of both individual historical resources and on historical districts within the Plan Areas. The PEIR determined that approximately 32 percent of the known or potential historical resources in the Plan Areas could potentially be affected under the preferred alternative. The Eastern Neighborhoods PEIR found this impact to be significant and unavoidable. This impact was addressed in a Statement of Overriding Considerations with findings and adopted as part of the Eastern Neighborhoods Rezoning and Area Plans approval on January 19, 2009.

The project site is developed with two buildings which were constructed in 1906 (Lot 002) and 1923 (Lot 002A). The buildings on the project site were evaluated in the Showplace Square/Northeast Mission Historic District Survey and were rated "6Z", which means the buildings were found ineligible for inclusion in the National Register of Historic Places, the California Register of Historic Resources, or local designation through survey evaluation. As such, the project site does not contain any historical structures, sites or architectural features. Nor is the project site located within or adjacent to any identified historic districts. Therefore, the proposed project would not contribute to the significant historic resource impact identified in the Eastern Neighborhoods PEIR, and no historic resource mitigation measures would apply to the proposed project.

For these reasons, the proposed project would not result in significant impacts on historic architectural resources that were not identified in the Eastern Neighborhoods PEIR.

Archeological Resources

The Eastern Neighborhoods PEIR determined that implementation of the Area Plan could result in significant impacts on archeological resources and identified three mitigation measures that would reduce these potential impacts to a less than significant level. Eastern Neighborhoods PEIR Mitigation Measure J-1 applies to properties for which a final archeological research design and treatment plan is on file at the Northwest Information Center and the Planning Department. Mitigation Measure J-2 applies to properties for which no archeological assessment report has been prepared or for which the archeological documentation is incomplete or inadequate to serve as an evaluation of potential effects on archeological resources under CEQA. Mitigation Measure J-3, which applies to properties in the Mission Dolores Archeological District, requires that a specific archeological testing program be conducted by a qualified archeological consultant with expertise in California prehistoric and urban historical archeology.

The proposed project would excavate to a maximum depth of approximately 20 feet below grade, resulting in roughly 18,000 cubic yards of soils disturbance. Excavation is proposed in order to construct the one-level subterranean parking garage, which would also provide space for loading activities, Class I bicycle parking, and trash, electrical and building service space. The project site has not been the subject of a prior archeological study. Thus, the proposed project is subject to Mitigation Measure J-2, which is required for properties with no previous archeological studies. In accordance with Mitigation Measure J-2, a Preliminary Archeological (PAR) assessment was conducted by the Planning Department's staff archeologists. Based on the PAR, the Planning Department determined that standard Archeological Mitigation Measure I (Accidental Discovery) would apply to the proposed project.⁷ The PAR and mitigation requirements are consistent with Mitigation Measure J-2 of the Eastern Neighborhoods PEIR; the implementation of which would reduce impacts related to archeological resources to a less-than-significant level. The project sponsor has agreed to implement Mitigation Measure J-2, including the requirements of the Planning Department's first standard Archeological Mitigation Measure, as **Project Mitigation Measure 1**. The full text of Project Mitigation Measure 1 is provided in the Mitigation Measures section below.

For these reasons, the proposed project would not result in significant impacts on archeological resources that were not identified in the Eastern Neighborhoods PEIR.

⁷ Randall Dean, Staff Archeologist, San Francisco Planning Department, *Preliminary Archeological Review (PAR) for 88 Arkansas Street*. This document is available for review at the San Francisco Planning Department, 1650 Mission Street, Suite 400 as part of Case File No. 2015-000453ENV.

<i>Topics:</i>	<i>Significant Impact Peculiar to Project or Project Site</i>	<i>Significant Impact not Identified in PEIR</i>	<i>Significant Impact due to Substantial New Information</i>	<i>No Significant Impact not Previously Identified in PEIR</i>
4. TRANSPORTATION AND CIRCULATION— Would the project:				
a) Conflict with an applicable plan, ordinance or policy establishing measures of effectiveness for the performance of the circulation system, taking into account all modes of transportation including mass transit and non-motorized travel and relevant components of the circulation system, including but not limited to intersections, streets, highways and freeways, pedestrian and bicycle paths, and mass transit?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Conflict with an applicable congestion management program, including but not limited to level of service standards and travel demand measures, or other standards established by the county congestion management agency for designated roads or highways?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Result in a change in air traffic patterns, including either an increase in traffic levels, obstructions to flight, or a change in location, that results in substantial safety risks?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Substantially increase hazards due to a design feature (e.g., sharp curves or dangerous intersections) or incompatible uses?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Result in inadequate emergency access?	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) Conflict with adopted policies, plans, or programs regarding public transit, bicycle, or pedestrian facilities, or otherwise decrease the performance or safety of such facilities?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

The Eastern Neighborhoods PEIR anticipated that growth resulting from the zoning changes would not result in significant impacts related to pedestrians, bicyclists, loading, emergency access, or construction. As the proposed project is within the development projected under the Eastern Neighborhoods Rezoning and Area Plans, there would be no additional impacts on pedestrians, bicyclists, loading, emergency access, or construction beyond those analyzed in the Eastern Neighborhoods PEIR.

However, the Eastern Neighborhoods PEIR anticipated that growth resulting from the zoning changes could result in significant impacts on traffic and transit ridership, and identified 11 transportation mitigation measures, which are described further below in the Traffic and Transit sub-sections. Even with mitigation, however, it was anticipated that the significant adverse cumulative traffic impacts and the cumulative impacts on transit lines could not be fully mitigated. Thus, these impacts were found to be significant and unavoidable.

The projected traffic conditions and cumulative effects of project buildout analyzed in the Eastern Neighborhoods PEIR were based on a 2025 horizon year. Projected traffic conditions and cumulative project buildout have been or will soon be realized. In order to provide a conservative transportation analysis of the proposed project at 88 Arkansas Street, the Planning Department determined that year 2040 was an appropriate horizon year for projected growth and cumulative conditions traffic analysis. The following transportation analysis reflects the updated traffic and transit demand forecasts. Therefore, the cumulative year used in the transportation analysis is year 2040, which is beyond the date (year 2025) analyzed in the Eastern Neighborhoods PEIR.

In order to analyze Existing Conditions and Cumulative Conditions under a 2040 horizon, growth rates were determined based on the San Francisco-Chained Activity Modeling Process (SF-CHAMP) Model 4.3 (the most recent version available at the time of this analysis), which is the official travel forecasting model used for San Francisco. The growth rates were applied to the analysis of traffic volumes at each of the eight study intersections (see **Table 3** in the Traffic sub-section below) and directional Muni and regional transit lines (see **Table 5** in the Transit sub-section below). With regards to the traffic analysis, Existing Conditions at the eight study intersections were determined using the methodology provided in the 2000 Highway Capacity Manual (HCM). The HCM prescribes the use of quantitative and qualitative measures to estimate traffic conditions, including capacity and Level of Service (LOS),⁸ among other performance measures. 2040 Cumulative Condition traffic volumes at each intersection were projected by applying the annual growth rate (up to year 2040) to the traffic volumes calculated for Existing Conditions. Ridership data for Muni transit lines collected during Fall 2013 (the most recent information available at the time of this analysis) was obtained for the analysis of transit operations under Existing, Existing plus Project, and 2040 Cumulative Conditions. Ridership data for regional transit was obtained from the San Francisco Municipal Transportation Agency's (SFMTA) Transit Effectiveness Project Final Environmental Impact Report (TEP FEIR)⁹ for the analysis of regional transit operations under Existing, Existing plus Project, and 2040 Cumulative Conditions. The data was aggregated by direction (i.e., north, south, east, west) into screenlines that were developed in relation to the location of the project site. The screenlines were used to determine capacity utilization and the number of trips the proposed project would contribute to capacity utilization under Existing Conditions and 2040 Cumulative Conditions.

The project site is not located within an airport land use plan area, or in the vicinity of a private airstrip. Therefore, the Community Plan Exemption Checklist topic 4c is not applicable.

Trip Generation

The proposed project would include the demolition of the existing structures on the project site and construction of an approximately 143,990-gsf mixed use building, which would be comprised of approximately 111,695 gsf of residential space, 3,275 gsf of retail space, and a 29,020-gsf, subterranean garage. The project would provide approximately 127 dwelling units, 98 vehicle parking spaces, and 127 Class I bicycle spaces and 13 Class II bicycle spaces.

Trip generation of the proposed project was calculated using information in the 2002 *Transportation Impacts Analysis Guidelines for Environmental Review* (SF Guidelines) developed by the San Francisco Planning Department.¹⁰ The proposed project would generate an estimated 2,951 daily person trips (inbound and outbound) on a weekday daily basis, consisting of approximately 251 person trips by auto, 87 transit trips, 91 walk trips and 24 trips by other modes. Accounting for vehicle occupancy for this Census Tract, the proposed project would generate an estimated 141 PM peak hour vehicle trips. In order to provide a conservative analysis of project's contribution to Existing and Cumulative Conditions, the estimated daily trips used in the analysis was 3,048, which is greater than the final trip generation calculated for the proposed project.

⁸ LOS is a measure of performance based on average seconds of delay per vehicle at an intersection.

⁹ San Francisco Planning Department, Transit Effectiveness Project Final Environmental Impact Report, Planning Department Case No. 2011.0558E, State Clearinghouse No. 2011112030, certified March 27, 2014. This material is available for review at the Planning Department, 1650 Mission Street, Suite 400, as part of Case No. 2015-000453ENV.

¹⁰ AECOM, 88 Arkansas Street Final Transportation Impact Study, January 2016. This document is available for review at the San Francisco Planning Department, 1650 Mission Street, Suite 400 as part of Case File No. 2015-000453ENV.

Traffic

Mitigation Measures E-1 through E-4 in the Eastern Neighborhoods PEIR were adopted as part of the Plan with uncertain feasibility to address significant traffic impacts. These measures are not applicable to the proposed project, as they are plan-level mitigations to be implemented by City and County agencies. Since certification of the PEIR, SFMTA has been engaged in public outreach regarding some of the parking-related measures identified in Mitigation Measures E-2 and E-4: Intelligent Traffic Management, although they have not been implemented. Measures that have been implemented include traffic signal installation at Rhode Island/16th streets as identified in Mitigation Measure E-1 and enhanced funding as identified in Mitigation Measure E-3 through San Francisco propositions A and B passed in November 2014. Proposition A authorized the City to borrow \$500 million through issuing general obligation bonds in order to meet some of the transportation infrastructure needs of the City. These funds are allocated for constructing transit-only lanes and separated bikeways, installing new boarding islands and escalators at Muni/BART stops, installing sidewalk curb bulb-outs, raised crosswalks, median islands, and bicycle parking and upgrading Muni maintenance facilities, among various other improvements. Proposition B, which also passed in November 2014, amends the City Charter to increase the amount the City provided to the SFMTA based on the City's population, with such funds to be used to improve Muni service and street safety. Some of this funding may be applied to transportation projects within the Eastern Neighborhoods Plan area.

The proposed project's vehicle trips would travel through the intersections surrounding the project block. Intersection operating conditions are characterized by the concept of Level of Service (LOS), which ranges from A to F and provides a description of an intersection's performance based on traffic volumes, intersection capacity, and vehicle delays. LOS A represents free flow conditions, with little or no delay, while LOS F represents congested conditions, with extremely long delays; LOS D (moderately high delays) is considered the lowest acceptable level in San Francisco. The intersections near the project site (within approximately 800 feet) include Arkansas Street and 16th Street, Arkansas Street and 17th Street, Arkansas Street and Mariposa Street, De Haro Street and 16th Street, Carolina Street and 17th Street, Pennsylvania Avenue and Mariposa Street, the I-280 northbound off-ramp and Mariposa Street, and the I-280 southbound on-ramp and Mariposa Street. **Table 3** provides existing, existing plus project, and cumulative LOS data gathered for these intersections during weekday PM peak hour, per the Transportation Impact Study conducted for the project at 88 Arkansas Street.¹¹

¹¹ AECOM, 88 Arkansas Street Final Transportation Impact Study, January 2016. This document is available for review at the San Francisco Planning Department, 1650 Mission Street, Suite 400 as part of Case File No. 2015-000453ENV.

Table 3 – Weekday PM Peak Hour Level of Service (LOS)

<u>Intersection</u>	<u>Control</u>	<u>Existing Conditions</u> (2008)		<u>Existing Plus Project Conditions</u>		2040 Cumulative Conditions	
		LOS	Delay ¹	LOS	Delay	LOS	Delay
Arkansas Street/16th Street	One-way Stop	D	30.1	D	32.9	F	>50.0
Arkansas Street/17th Street	All-Way Stop	B	12.9	B	14.0	F	>50.0
Arkansas Street/Mariposa Street	All-Way Stop	B	10.7	B	11.2	F	>50.0
De Haro Street/16th Street	Signalized	B	17.7	B	18.7	D	29.1
Carolina Street/17th Street	Two-Way Stop	C	22.4	C	23.5	F	>50.0
Pennsylvania Avenue/Mariposa Street	Two-Way Stop	F	>50.0	F	>50.0	F	>50.0
I-280 Northbound Off-Ramp/Mariposa Street	Signalized	C	27.6	C	27.8	F	>50.0
I-280 Southbound On-Ramp/Mariposa Street	One-Way Stop	D	32.4	D	35.0	E	45.5
¹ Delay = Seconds per vehicle							

Sources: AECOM, 2016

The proposed project would generate an estimated 141 new PM peak-hour vehicle trips that could travel through surrounding intersections. As shown in **Table 3**, seven of the eight study intersections would continue to operate acceptably under Existing Plus Project conditions during the weekday PM peak hour. New weekday PM peak-hour vehicle trips would not substantially increase traffic volumes at these intersections and would not substantially increase average delay that would cause intersections that currently operate at acceptable LOS to deteriorate to unacceptable LOS; impacts to those intersections would be less-than-significant. One intersection, Pennsylvania Avenue and Mariposa Street currently operates as LOS F and would continue to do so under Existing Plus Project conditions. The proposed project's contributions to LOS F critical movements at this intersection were further analyzed.

During the weekday PM peak hour at the Pennsylvania Avenue and Mariposa Street intersection, the proposed project would add one vehicle trip to the critical southbound left-turn movement. This project-generated contribution would represent two percent of the total weekday PM peak hour volume to this critical movement. The proposed project would add zero vehicles to the critical southbound through and southbound right-turn movements, resulting in a project-generated contribution of zero percent of the total weekday PM peak hour volume for those critical movements. The proposed project's contribution to these failing critical movements would be minimal (less than five percent); therefore, the proposed project would have a less-than-significant impact on the intersection of Pennsylvania Avenue and Mariposa Street under Existing Plus Project conditions.

The Eastern Neighborhood PEIR analyzed cumulative transportation impacts in the Plan Areas for year 2025 conditions and, for each of the rezoning options, identified significant and unavoidable cumulative (2025) impacts relating to weekday PM peak hour traffic conditions, with the Preferred Project having

significant impacts at nine intersections. The project site is not located near any of the nine intersections. Therefore, the proposed project would not contribute trips at these intersections and would not contribute to those impacts. General mitigation measures were proposed for the entire Area Plan. These include intelligent traffic management, enhanced transportation funding, and parking management to discourage driving. Even with mitigation, however, cumulative impacts at these eight intersections were found to be significant and unavoidable and a Statement of Overriding Considerations related to the significant and unavoidable cumulative traffic impacts was adopted as part of the PEIR Certification and project approval.

While the PEIR used 2025 as the horizon year for the Cumulative Conditions analysis of Plan implementation, as previously discussed, the Cumulative Conditions horizon year was extended to 2040 in order to provide a conservative analysis of potential transportation impacts for the project at 88 Arkansas Street. There are several projects proposed for development in the project site vicinity,¹² including, but not limited to, 1301 16th Street, 100 Hooper Street, 901 16th Street/1200 17th Street, 580 De Haro Street, 1601-1677 Mariposa Street, and 153 Arkansas Street. These projects were considered in the Cumulative Conditions analysis of the proposed project. Other projects in the project site vicinity are accounted for in the estimated growth projections used in the 2040 Cumulative Conditions analysis for the proposed project. As shown on **Table 3**, during the weekday PM peak hour seven of the eight study intersections would deteriorate to an unacceptable LOS under Cumulative Conditions. Thus, the proposed project's contributions to LOS E and LOS F critical movements at these seven intersections (see **Table 4**) were further analyzed.

¹² Proposed projects include 1301 16th Street (Case No. 2013.0698E), 100 Hooper Street (Case No. 2012.0203E), 901 16th Street/1200 17th Street (Case No. 2011.1300E), 580 De Haro Street (Case No. 2013.1671E), 1601-1677 Mariposa Street (Case No. 2012.1398E), and 153 Arkansas Street (Case No. 2014.1246ENV).

Table 4 – Weekday PM Peak Hour Critical Movements and Level of Service (LOS) under 2040 Cumulative Conditions						
Intersection ¹	Critical Lane Group				Project Contribution	
	Movement/Lane Group ²	Volume	LOS	Delay ³	Trips	Percent of Total Volume
Arkansas Street/16th Street	NBL	93		>50.0		
	NBR	19	F		0	0
Arkansas Street/17th Street	NBL	178		>50.0		
	NBT	148				
	NBR	20	F		10	2.9
Arkansas Street/Mariposa Street	EBL	31		>50.0		
	EBT	234				
	EBR	28	F		5	1.7
Carolina Street/17th Street	NBL	36		>50.0		
	NBT	38				
	NBR	31	F		5	4.8
Pennsylvania Avenue/Mariposa Street	SBL	54		>50.0		
	SBT	4				
	SBR	4	F		1	1.6
I-280 Northbound Off-Ramp/Mariposa Street	NBL	703			9	1.3
	NBT	566			-	-
	NBR	465	F	>50.0	0	0
I-280 Southbound On-Ramp/Mariposa Street	WBL	1,620			0	0
	WBT	1,059	E	45.5	9	0.8
¹ The DeHaro Street/16th Street intersection was not included as the LOS for this intersection under 2040 Cumulative Conditions would continue to operate at an acceptable level (LOS D) ² NBL = northbound left, NBT = northbound through, NBR = northbound right, EBL = eastbound left, EBT = eastbound through, EBR = eastbound right, SBL = southbound left, SBT = southbound through, SBR = southbound right. ³ Delay = Seconds per vehicle						

- *Arkansas Street/16th Street.* During the weekday PM peak hour at the Arkansas Street and 16th Street intersection, the proposed project would add zero vehicle trips to the critical northbound left-turn and northbound right-turn movements. This represents a project-generated contribution of zero percent of the total weekday PM peak hour volume for these critical movements.

- *Arkansas Street/17th Street.* During the weekday PM peak hour at the Arkansas Street and 17th Street intersection, the proposed project would add 10 vehicles to the critical northbound left turn, through, and right-turn movements. This represents a project-generated contribution of 2.9 percent of the total weekday PM peak hour volume for these critical movements.
- *Arkansas Street/Mariposa Street.* During the weekday PM peak hour at the Arkansas Street and Mariposa Street intersection, the proposed project would add five vehicles to the critical eastbound right-turn, through, and left-turn movements. This represents a project-generated contribution of 1.7 percent of the total weekday PM peak hour volume for these critical movements.
- *Carolina Street/17th Street.* During the weekday PM peak hour at the Carolina Street and 17th Street intersection, the proposed project would add five vehicles to the critical northbound left-turn, through, and right-turn movements. This represents a project-generated contribution of 4.8 percent of the total weekday PM peak hour volume for these critical movements.
- *Pennsylvania Avenue/Mariposa Street.* During the weekday PM peak hour at the Pennsylvania Avenue and Mariposa Street intersection, the proposed project would add one vehicle trip to the critical southbound left-turn, through, and right-turn movements. This represents a project-generated contribution of 1.6 percent of the total weekday PM peak hour volume for these critical movements.
- *I-280 Northbound Off-Ramp/Mariposa Street.* During the weekday PM peak hour at the I-280 Northbound Off-Ramp and Mariposa Street intersection, the proposed project would add nine vehicle trips to the critical northbound left-turn and through movements. This represents a project-generated contribution of 1.3 percent of the total weekday PM peak hour volume for these critical movements.
- *I-280 Southbound On-Ramp/Mariposa Street.* During the weekday PM peak hour at the I-280 Southbound On-Ramp and Mariposa Street intersection, the proposed project would add zero vehicle trips to the critical westbound left-turn movement. This represents zero percent of the total weekday PM peak hour volume for this critical movement. The proposed project would contribute nine vehicle trips to the westbound through movement, representing 0.8 percent of the total weekday PM peak hour volume for this movement. However, the westbound through lane was not found to be a critical movement.

The proposed project would contribute less than five percent of the additional traffic volume projected for each of these seven intersections under 2040 Cumulative Conditions. Therefore, the proposed project would have a less-than-significant impact on these intersections.

The proposed project would not contribute considerably to LOS delay conditions as its contribution of an estimated 141 PM peak-hour vehicle trips would not be a substantial proportion of the overall traffic volume or the new vehicle trips generated by Eastern Neighborhoods' Plan projects. The proposed project would also not contribute considerably to 2040 cumulative traffic conditions. Thus, the proposed project would not have any significant cumulative traffic impacts.

While traffic impacts would not be significant, implementation of a Transportation Demand Management (TDM) program would further reduce project-generated traffic. A TDM program would encourage residents and employees who travel to and from the project site to use alternative means of transportation such as public transit, biking, and walking. Components of a TDM program may include an on-site TDM coordinator, dissemination of transportation and trip planning information, and free or subsidized transit

passes, among other measures. The TDM program is included as **Project Improvement Measure 1 Implementation of Transportation Demand Management Strategies** (full text provided in the Improvement Measures section below).

For the above reasons, the proposed project would not result in significant impacts on traffic that were not identified in the Eastern Neighborhoods PEIR.

Transit

Mitigation Measures E-5 through E-11 in the Eastern Neighborhoods PEIR were adopted as part of the Plan with uncertain feasibility to address significant transit impacts. These measures are not applicable to the proposed project, as they are plan-level mitigations to be implemented by City and County agencies. In compliance with a portion of Mitigation Measure E-5: Enhanced Transit Funding, the City adopted impact fees for development in Eastern Neighborhoods that goes towards funding transit and complete streets. In addition, the City is currently conducting outreach regarding Mitigation Measures E-5: Enhanced Transit Funding and Mitigation Measure E-11: Transportation Demand Management as part of the Transportation Sustainability Program.¹³ In compliance with all or portions of Mitigation Measure E-6: Transit Corridor Improvements, Mitigation Measure E-7: Transit Accessibility, Mitigation Measure E-9: Rider Improvements, and Mitigation Measure E-10: Transit Enhancement, the SFMTA is implementing the Transit Effectiveness Project (TEP), which was approved by the SFMTA Board of Directors in March 2014. The TEP (now called Muni Forward) includes system-wide review, evaluation, and recommendations to improve service and increase transportation efficiency. Examples of transit priority and pedestrian safety improvements within the Eastern Neighborhoods Plan area as part of Muni Forward include the 14 Mission Rapid Transit Project, the 22 Fillmore Extension along 16th Street to Mission Bay (expected construction between 2017 and 2020), and the Travel Time Reduction Project on Route 9 San Bruno (initiation in 2015). In addition, Muni Forward includes service improvements to various routes with the Eastern Neighborhoods Plan area; for instance the implemented new Route 55 on 16th Street.

Mitigation Measure E-7 also identifies implementing recommendations of the Bicycle Plan and Better Streets Plan. As part of the San Francisco Bicycle Plan, adopted in 2009, a series of minor, near-term, and long-term bicycle facility improvements are planned within the Eastern Neighborhoods, including along 2nd Street, 5th Street, 17th Street, Townsend Street, Illinois Street, and Cesar Chavez Boulevard. The San Francisco Better Streets Plan, adopted in 2010, describes a vision for the future of San Francisco's pedestrian realm and calls for streets that work for all users. The Better Streets Plan requirements were codified in Section 138.1 of the Planning Code and new projects constructed in the Eastern Neighborhoods Plan area are subject to varying requirements, dependent on project size. Another effort which addresses transit accessibility, Vision Zero, was adopted by various City agencies in 2014. Vision Zero focuses on building better and safer streets through education, evaluation, enforcement, and engineering. The goal is to eliminate all traffic fatalities by 2024. Vision Zero projects within the Eastern Neighborhoods Plan area include pedestrian intersection treatments along Mission Street from 18th to 23rd streets, the Potrero Avenue Streetscape Project from Division to Cesar Chavez streets, and the Howard Street Pilot Project, which includes pedestrian intersection treatments from 4th to 6th streets.

The project site is located within a quarter mile of several local transit lines including Muni lines 9 (San Bruno), 9R (San Bruno Rapid), 10 (Townsend), 19 (Polk), 22 (Fillmore), 33 (Stanyan), 55 (16th Street), and T

¹³ Available at: <http://tsp.sfplanning.org>

(Third Street). The proposed project would be expected to generate 87 transit trips during the PM peak hour. Of those 87 trips, 73 would utilize Muni transit lines with the remaining 14 using regional transit options. As shown on **Tables 5 and 6**, due to the wide availability of nearby transit, the addition of 87 PM peak hour transit trips would be accommodated by existing capacity, and Muni and regional capacity utilization would be below the standard established by SFMTA (85 percent) or regional operators.¹⁴ In addition, while the proposed project would result in increase use of existing transit stops along 17th Street and in the project site vicinity, none of the bus stops in the site vicinity have reached maximum load point for any of the Muni lines that utilize those stops. As such, the proposed project would not result in unacceptable levels of transit service or cause a substantial increase in delays or operating costs such that significant adverse impacts in transit service could result.

Table 5 – Weekday PM Peak Hour Directional Muni Line Capacity, Existing and Existing Plus Project

Direction	Existing			Project Trips ¹	Existing Plus Project	
	Ridership	Ridership Capacity	Utilization		Ridership Capacity	Utilization
Northbound	761	1,071	71%	21	782	73%
Southbound	703	1,071	66%	10	713	67%
Eastbound	429	725	59%	18	447	62%
Westbound	386	725	53%	24	410	57%
¹ Some regional transit trips would utilize Muni to reach regional transit provider. Those trips are accounted for in the analysis of the 73 Muni trips.						

Table 6 – Weekday PM Peak Hour Directional Regional Transit Capacity, Existing and Existing Plus Project

Direction	Existing			Project Trips	Existing Plus Project	
	Ridership	Ridership Capacity	Utilization		Ridership Capacity	Utilization
East bay	22,777	27,591	83%	10	27,591	83%
North Bay	2,352	4,776	49%	5	4,776	49%
South Bay	13,200	18,330	72%	8	18,330	72%

Each of the rezoning options in the Eastern Neighborhoods PEIR identified significant and unavoidable cumulative impacts relating to increases in transit ridership on Muni lines, with the Preferred Project

¹⁴ Some regional transit trips would need to utilize Muni in order to reach a regional transit provider. Those trips are accounted for in the analysis of the 73 Muni trips.

having significant impacts on seven lines. Of those lines, the project site is located within a quarter-mile of Muni lines 9 (San Bruno), 22 (Fillmore), and 33 (Stanyan). Under 2040 Cumulative Conditions, as shown on **Table 7**, the estimated 73 project-generated Muni trips constitute a minor contribution (less than three percent) to the utilization of Muni capacity. **Table 8** shows directional regional transit capacity utilization under 2040 Cumulative Conditions. The projected 10 additional trips in the East Bay direction, five trips in the North Bay direction and six trips in the South Bay direction are not anticipated to substantially change the estimated capacity utilization along these lines. Thus, the estimated 23 project-generated regional transit trips constitute a minor contribution to the utilization of regional transit capacity.

Table 7 – Weekday PM Peak Hour Directional Muni Line Capacity, Cumulative Conditions				
Direction	2040 Cumulative Conditions		Project Contribution	
	Ridership	Capacity Utilization	Trips	Percentage of Ridership Volume
Northbound	1,111	148%	21	1.4%
Southbound	1,540	72%	10	0.5%
Eastbound	983	98%	18	1.8%
Westbound	866	98%	24	2.7%

Table 8 – Weekday PM Peak Hour Directional Regional Transit Capacity, Cumulative Conditions			
Direction	2040 Cumulative Conditions		Project Contribution
	Ridership	Capacity Utilization	Trips
East bay	42,702	84%	10
North Bay	3,689	77%	5
South Bay	13,208	72%	8

Overall, the proposed project would not contribute considerably to Existing Conditions or Cumulative Conditions as its minor contribution of 87 PM peak hour transit trips would not be a substantial proportion of the overall additional transit volume generated by Eastern Neighborhood projects.

For the above reasons, the proposed project would not result in significant impacts that were not identified in the Eastern Neighborhoods PEIR related to transit and would not contribute considerably to cumulative transit impacts that were identified in the Eastern Neighborhoods PEIR.

Pedestrians

During a typical weekday evening peak hour, the proposed project would generate 91 PM peak-hour pedestrian trips. The 91 new pedestrian trips could be accommodated on the existing sidewalks and crosswalks in the site vicinity, and would not result in a substantial adverse impact on pedestrian

circulation in the site vicinity. Thus, project-generated pedestrian trips would not substantially overcrowd the current pedestrian conditions along 16th, 17th, Arkansas, or Wisconsin Streets.

Pedestrian access to the residential portion of the proposed project would be provided on the Arkansas Street frontage near the 17th Street and Arkansas Street intersection. Street-level entrance to the retail space would be located on 17th Street. The project proposes sidewalk alterations that could improve pedestrian access and safety. Proposed alterations include a new bulb-out extension at the northwest corner of 17th and Arkansas Streets, widening the sidewalk on Arkansas Street along the building frontage from roughly 15 feet wide to 24 feet wide, removing two existing curb cuts totaling approximately 30 feet in length, and reducing a 20-foot wide curb cut to 12 feet wide. These proposed activities would improve pedestrian safety when crossing 17th Street to access transit stops, increase pedestrian visibility of vehicles, and reduce the potential for conflicts between vehicles accessing the garage and pedestrians, bicyclists, and other traffic.

The proposed project is also subject to the Better Streets Plan. The proposed streetscape improvements would comply with the Better Streets Plan requirements, would improve the pedestrian realm adjacent to the project site and promote pedestrian safety and comfort, and would allow for adequate public space and maneuverability for safe pedestrian passage along the sidewalk areas. In addition, the proposed project would not include any features that would potentially increase hazards to pedestrians. The proposed project would not include sidewalk narrowing, roadway widening, or removal of a center median; conditions that can adversely affect pedestrians. As such, the proposed project would not cause a hazard to pedestrians or otherwise interfere with pedestrian accessibility to the project site and adjoining areas. Pedestrian activity may increase as a result of the proposed project, but not to a degree that would result in substantial overcrowding on public sidewalks. Thus, the proposed project would not result in significant impacts on pedestrian conditions.

While pedestrian impacts would be less-than-significant, installation of audible and visual warning devices would further reduce potential conflicts between pedestrians and vehicles accessing the garage.

Project Improvement Measure 2 Pedestrian Safety Improvements calls for the installation of audible/visual warning devices (full text provided in the Improvement Measures section below).

For the above reasons, the proposed project would not result in significant impacts on pedestrian conditions that were not identified in the Eastern Neighborhoods PEIR.

Bicycles

The proposed project would include approximately 127 Class I and one Class II bicycle parking spaces¹⁵ within the basement-level parking garage and 12 Class II bicycle parking spaces¹⁶ on the southwest corner of the project site. In order to access the garage-level bicycle parking spaces, riders would either use the elevators or stairs located in the residential lobby or ride their bikes down the garage ramp.

Several Citywide Bicycle Routes are located in the vicinity of the project site. Bicycle Routes are classified as Class 1, 2, or 3. Class 1 routes are dedicated bike paths for bicyclists and pedestrians which do not allow

¹⁵ Per Planning Code Section 155.1, Class I bicycle spaces are in secure, weather-protected facilities intended for use as long-term, overnight, and work-day bicycle storage by dwelling unit residents, non-residential occupants, and employees.

¹⁶ Per Planning Code Section 155.1, Class II bicycle spaces are bicycle racks located in a publicly-accessible, highly visible location intended for transient or short-term use by visitors, guests, and patrons of the building.

motorized travel. Class 2 routes are striped bike lanes located between vehicle lanes and curbs/street-side parking. Class 3 routes are unmarked paths where bicyclists often must share travel lanes with motorized vehicles. While Class 3 routes do not feature markings or striping, they often include signage. Bicycle Routes within the site vicinity include Routes 7 (Class 3), 23 (Class 2), 25 (Class 2), 40 (Class 2), and 123 (Class 3). These routes provide direct connectivity to several neighborhoods in the city, including Downtown, South of Market (SoMa), Noe Valley, Mission Bay/Dogpatch, and the Central Waterfront.

The proposed project is anticipated to generate approximately 24 bicycle trips during a typical weekday evening peak period. Observations taken for preparation of the Transportation Impact Study indicate there is a moderate level of bicycle activity (137 cyclists) during the PM peak period, primarily along 16th Street and other streets with designated bicycle routes. The Eastern Neighborhoods PEIR found that Plan implementation would result in a five to twenty percent increase in bicycle trips in the Showplace Square/Potrero Hill Plan Area. The estimated 24 project-generated bicycle trips are not anticipated to substantially contribute to bicycle activity in the area or result in significant cumulative bicycle impacts.

Due to the moderate use of existing bicycle network facilities it is anticipated that the project-generated bicycle trips could be accommodated by the existing bicycle network within the project site vicinity. Further, the proposed project would not generate new trips in the project vicinity in an amount such that bicycle facilities and circulation would be adversely impacted. Thus, the proposed project would not result in significant impacts on bicycle conditions.

Loading

Based on the Transportation Guidelines, the proposed project's residential uses are estimated to generate approximately 4 trips by delivery and service vehicles per day. Pursuant to Planning Code Section 152.1, one off-street freight loading zone is required for the residential portion of the proposed project. The project proposes to provide on-street and off-street loading facilities. Subject to review and approval by SFMTA, on-street loading would consist of an approximately 45-foot-long "yellow curb" loading zone located just south of the proposed garage entrance on Arkansas Street. The off-street loading space would be primarily used for commercial and freight loading and trash/recycling activities, but could be used for passenger loading and move-in/move-out activities when not utilized by delivery and service vehicles. In order to create space for the on-street loading zone, the proposed project would remove approximately five on-street perpendicular parking spaces. An approximately 12-foot-wide and 25-foot long off-street loading space would be located in the garage. The loading space, along with the garage driveway and ramp, would have a maximum vertical clearance of 12 feet. Vehicles taller 12 feet would be able to use the on-street loading zone. The proposed loading spaces would conform to Planning Code requirements for the number of required loading spaces and their dimensions. The project would generate an estimated demand of less than one space during average and peak-period loading times, and this supply would meet the loading demand estimated for the project. Thus, loading impacts would not be significant.

While loading impacts were found to be less than significant, if building management schedules and coordinates loading activities and discourages trucks from illegally parking or obstructing traffic flow in the site vicinity, loading impacts could be further reduced. **Project Improvement Measure 3 On-Street Loading Management** calls for building management to coordinate loading activities (full text provided in the Improvement Measures section below).

Construction

Proposed construction activities would last approximately 19 months, including demolition, excavation, and building construction. Construction activities would likely occur weekdays from 7:00 a.m. to 3:30 p.m., with Saturday construction taking place, on an as-needed basis, from 8:00 a.m. to 4:00 p.m. All construction work would be conducted in accordance with the San Francisco Noise Ordinance (see Noise section below).

Construction of the proposed project would require roughly 30 to 100 construction workers per day, depending on the construction phase. The general contractor would be responsible for all phases of construction, and would be required to follow SFMTA's *Regulations for Working in San Francisco Street* (the Blue Book). The project is not expected to require off-site staging, or closure/ relocation of travel lanes, or transit facilities. However, closure of the portions on the sidewalks adjacent to the northwest corner of the project site (at the Arkansas Street and 17th Street intersection) may be required in order to install the proposed bulb-out. Should any closure or relocation of sidewalks, travel lanes, and transit facilities be required, the work must be coordinated with SFMTA's Interdepartmental Staff Committee on Traffic and Transportation (ISCOTT) and a public meeting would be held. The general contractor would also be required to develop a construction management plan for review and approval by SFMTA's Transportation Advisory Committee, which consists of representatives of City departments including SFMTA, San Francisco Public Works (Public Works), the San Francisco Fire Department (SFFD), the San Francisco Police Department (SFPD), the Department of Public Health (DPH), the Port of San Francisco, and the Taxi Commission. SFMTA would have to be reimbursed for any installation or removal of temporary signage or striping. In addition, prior to commencing with construction activities, the general contractor would coordinate construction activities with Muni's Street Operations and Special Events Office to reduce any impacts on transit operations in the site vicinity.

There would be a flow of construction-related traffic to and from the project site throughout the construction period. Construction workers and construction vehicles would need to access the site throughout the construction period. During the most active phase of construction, an estimated 80 construction worker trips and 15 construction vehicle trips could occur over the course of one day. Construction workers would be able to use nearby transit lines to reach the project site, and the project sponsor has indicated that arrangements would likely be made for construction workers to access off-street parking. Thus, construction activities would temporarily increase traffic volume, but the additional trips would not substantially affect traffic conditions. Moreover, construction-related impacts, generally, would not be considered significant due to their temporary and limited duration.

Construction of other development projects in the area may also occur at the same time as construction of the proposed project. If the construction of other development projects in the site vicinity takes place simultaneous to the construction of the proposed project, the overlap in construction schedules could potentially increase traffic levels due to employee ingress and egress, excavation, and the delivery of construction materials via trucks. Depending on the proximity of the sites to each other and the project site, as well as the uncertainty about construction schedules, construction activities could cause disruptions to traffic and to travel by transit, pedestrians, and bicycles. Should this occur, coordination of construction activities with other projects would reduce potential construction-generated traffic impacts. Limiting construction-related traffic to non-peak hours would further reduce potential construction

impacts. These measures are included as **Project Improvement Measure 4 Construction-Related Traffic Management** (full text provided in the Improvement Measures section below).

Emergency Vehicle Access

Emergency vehicle access to the project site would be provided by the residential entrances on Arkansas Street, retail entrances on 17th Street, or through the parking garage ramp. Development of the proposed project would not reduce or eliminate the travel lanes on Arkansas and 17th Streets, and emergency access to the project site would remain unchanged from existing conditions. In addition, a fire station is located within a half mile of the project site on the northeast corner of 16th and Vermont Streets.

A fire hydrant and water valves exists at the location of the proposed bulb-out extension on the northwest corner of the 17th Street/Arkansas Street intersection. Thus, the bulb-out would be subject to SFMTA and SFFD approval. However, the proposed bulb-out is not anticipated to impede emergency vehicle maneuvering or access to existing fire suppression infrastructure. Therefore, project-related impacts on emergency access would be less than significant.

Parking

As discussed on page 21 the proposed project meets the criteria for a Transit Priority Infill development and thus this CPE checklist does not consider the adequacy of parking in determining the significance of project impacts under CEQA.¹⁷ However, the following discussion is provided for informational purposes only.

The project site is located in a UMU district where, pursuant to Section 151.1 of the Planning Code, the proposed project would not be required to provide any off-street parking spaces. The project sponsor proposes to provide approximately 98 parking spaces in a one-level-below-grade parking garage. The proposed parking garage would be accessed via an approximately 12-foot-wide curb cut on Arkansas Street that would be created by reducing an existing, roughly 42-foot-wide curb cut. The entrance would be located roughly 180 feet north of the Arkansas Street/17th Street intersection along the eastern frontage of the building. The 12-foot-wide garage entrance would provide space for one vehicle at a time; however, the ramp would be able to accommodate one vehicle traveling in each direction. The garage would accommodate the proposed 98 parking spaces via a combination of eight self-service mechanical parking system arrays (82 parking spaces) and 16 individually-accessible parking spaces (six standard, seven compact, one car share, and two ADA spaces). The parking garage would provide sufficient circulation space to accommodate the estimated parking/car retrieval volume during peak periods. However, installation of a traffic signal on both the inside and outside of the garage opening would reduce any potential conflicts between vehicles approaching the garage entrance from opposite directions. Implementation of a vehicle queue abatement program would also address potential traffic congestion near the garage entrance. These measures are included as **Project Improvement Measure 5 Off-Street Parking Traffic Enhancements** (full text provided in the Improvement Measures section below).

The parking demand for the proposed new residential and retail uses on the project site was calculated using the methodology provided in the San Francisco Transportation Impact Analysis Guidelines for

¹⁷ San Francisco Planning Department. Transit-Oriented Infill Project Eligibility Checklist for 88 Arkansas Street, September 29, 2015. This document is available for review at the San Francisco Planning Department, 1650 Mission Street, Suite 400 as part of Case File No. 2015-000453ENV.

Environmental Review (the Transportation Guidelines). Per the Transportation Guidelines, on an average weekday, the proposed project is estimated to generate a parking demand of 134 parking spaces during the midday peak period and 165 parking spaces during the evening peak period. Thus, as proposed, the project would result in an unmet parking demand ranging from 36 spaces to 67 spaces during the midday and evening peak periods, respectively.

The proposed on-site parking spaces are not 'bundled' with the residential units. Residents would have the option to rent or purchase a parking space, but one would not be automatically provided with the residential unit. For this reason, project-generated project demand could result in residents without assigned parking seeking parking spaces near the proposed building. In addition, while seeking a parking space, residents may temporarily block private driveways or otherwise obstruct traffic and vehicle accessibility near the project site. However, the provision of off-street parking is not a requirement for the development of the project pursuant to Planning Code Section 151.1.

The proposed project would result in a parking shortfall. However, the unmet parking demand could be accommodated through existing on-street parking and alternative transportation modes. Approximately 379 on-street parking spaces are located within the area bounded by 16th, 17th, Arkansas, and Carolina Streets. Based on field observation, approximately 97 percent of those parking spaces (367) are occupied during the weekday mid-day peak period and 64 percent are occupied during the weekday PM peak period, leaving roughly 12 to 138 parking spaces available over the course of a day. The site is also well served by public transit and bicycle facilities, as discussed in the Transit and Bicycles sub-sections. In addition, implementation of **Project Improvement Measure 1 Implementation of Transportation Demand Management Strategies** (noted in the Traffic sub-section) would further reduce any conflicts arising from a parking shortfall.

While any unmet demand for parking spaces could be accommodated by existing transportation infrastructure, parking conditions are not static, as parking supply and demand varies from day to day, from day to night, from month to month, etc. Hence, the availability of parking spaces (or lack thereof) is not a permanent physical condition, but changes over time as people change their modes and patterns of travel. The absence of a ready supply of parking spaces, combined with available alternatives to auto travel (e.g., transit service, taxis, bicycles or travel by foot) and a relatively dense pattern of urban development, induces many drivers to seek and find alternative parking facilities, shift to other modes of travel, or change their overall travel habits. The secondary effects of drivers searching for parking is typically offset by a reduction in vehicle trips due to others who are aware of constrained parking conditions in a given area, and thus choose to reach their destination by other modes. Thus, the proposed project would not result in a substantial parking shortfall that would create hazardous traffic conditions or overtax the capacity of the surrounding transportation system.

Based on the foregoing, the proposed project would not result in significant transportation impacts, and implementation of the Project Improvement Measures identified above would further reduce these less-than-significant impacts.

Topics:	Significant Impact Peculiar to Project or Project Site	Significant Impact not Identified in PEIR	Significant Impact due to Substantial New Information	No Significant Impact not Previously Identified in PEIR
5. NOISE—Would the project:				
a) Result in exposure of persons to or generation of noise levels in excess of standards established in the local general plan or noise ordinance, or applicable standards of other agencies?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Result in exposure of persons to or generation of excessive groundborne vibration or groundborne noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Result in a substantial permanent increase in ambient noise levels in the project vicinity above levels existing without the project?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Result in a substantial temporary or periodic increase in ambient noise levels in the project vicinity above levels existing without the project?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) For a project located within an airport land use plan area, or, where such a plan has not been adopted, in an area within two miles of a public airport or public use airport, would the project expose people residing or working in the area to excessive noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) For a project located in the vicinity of a private airstrip, would the project expose people residing or working in the project area to excessive noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
g) Be substantially affected by existing noise levels?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

The Eastern Neighborhoods PEIR determined that implementation of the Eastern Neighborhoods Area Plans and Rezoning would result in significant noise impacts during construction activities and due to conflicts between noise-sensitive uses in proximity to noisy uses such as PDR, retail, entertainment, cultural/institutional/educational uses, and office uses. The Eastern Neighborhoods PEIR also determined that incremental increases in traffic-related noise attributable to implementation of the Eastern Neighborhoods Area Plans and Rezoning would be less than significant. The Eastern Neighborhoods PEIR therefore identified six noise mitigation measures that would reduce noise impacts from construction and noisy land uses to less-than-significant levels.

Eastern Neighborhoods PEIR Mitigation Measures F-1 and F-2 relate to construction noise. Mitigation Measure F-1 addresses individual projects that include pile-driving, and Mitigation Measure F-2 addresses individual projects that include particularly noisy construction procedures (including pile-driving). The proposed project would require excavation in order to construct a one-level-below-grade garage that would cover the entire footprint of the project site. According to the geotechnical investigation prepared for the proposed project, due to the presence of fill material and "new bay mud" on the site, the proposed buildings can be accommodated with a mat slab foundation or pier/pile foundation.¹⁸ The project sponsor intends to install a mat slab foundation and does not anticipate using pile drivers. Therefore, Mitigation Measure F-1 does not apply to the proposed project. While the project sponsor has confirmed that pile

¹⁸ H. Allen Gruen, *Geotechnical Investigation, Planned Development at 88 Arkansas and 1530 17th Streets, San Francisco, California*, February 21, 2015. This document is available for review at the San Francisco Planning Department, 1650 Mission Street, Suite 400 as part of Case File No. 2015-000453ENV.

driving would not be used for the proposed project,¹⁹ the project could involve noisy construction activities in close proximity to residential uses. The closest sensitive receptors are located approximately 110 feet southeast from the project site. Construction equipment would include use of a dozer, excavator, soldier pile rig, tie back drill, mobile crane, and rollers. Therefore, Eastern Neighborhoods Mitigation Measure F-2 applies to the project as and has been included as **Project Mitigation Measure 2 Construction Noise**. Project Mitigation Measure 2 requires the identification and implementation of site-specific noise attenuation measures and is described in detail in the Mitigation Measures section below.

In addition, all construction activities for the proposed project (approximately 19 months) would be subject to and would comply with the San Francisco Noise Ordinance (Article 29 of the San Francisco Police Code) (Noise Ordinance). Construction noise is regulated by the Noise Ordinance. The Noise Ordinance requires that construction work be conducted in the following manner: (1) noise levels of construction equipment, other than impact tools, must not exceed 80 dBA at a distance of 100 feet from the source (the equipment generating the noise); (2) impact tools must have intake and exhaust mufflers that are approved by the Director of Public Works or the Director of the Department of Building Inspection (DBI) to best accomplish maximum noise reduction; and (3) if the noise from the construction work would exceed the ambient noise levels at the site property line by 5 dBA, the work must not be conducted between 8:00 p.m. and 7:00 a.m. unless the Director of Public Works authorizes a special permit for conducting the work during that period.

DBI is responsible for enforcing the Noise Ordinance for private construction projects during normal business hours (8:00 a.m. to 5:00 p.m.). The Police Department is responsible for enforcing the Noise Ordinance during all other hours. Nonetheless, during the construction period for the proposed project of approximately 19 months, occupants of the nearby properties could be disturbed by construction noise. Times may occur when noise could interfere with indoor activities in nearby residences and other businesses near the project site. The increase in noise in the project area during project construction would not be considered a significant impact of the proposed project, because the construction noise would be temporary, intermittent, and restricted in occurrence and level, as the contractor would be required to comply with the Noise Ordinance and Eastern Neighborhoods PEIR Mitigation Measure F-2, which would reduce construction noise impacts to a less than significant level.

Eastern Neighborhoods PEIR Mitigation Measures F-3 and F-4 require that a detailed analysis of noise reduction requirements be conducted for new development that includes noise-sensitive uses located along streets with noise levels above 60 dBA (Ldn)²⁰ or near existing noise-generating uses. Since certification of the PEIR, San Francisco adopted Noise Regulations Relating to Residential Uses Near Places of Entertainment (Ordinance 70-15, Administrative Code Section 116, effective June 19, 2015) (Places of Entertainment Ordinance). The intent of the regulations is to address conflicts between residential uses and noise-generating uses in noise critical areas, such as in proximity to highways, country roads, city streets, railroads, rapid transit lines, airports, nighttime entertainment venues or industrial areas. Residential structures to be located where the day-night average sound level (Ldn) or community noise equivalent level (CNEL) exceeds 60 dBA shall require an acoustical analysis with the application of a building permit showing that the proposed design will limit exterior noise to 45 decibels in any habitable room. Furthermore, the regulations require the Planning Department and Planning Commission to

¹⁹ Julie Heinzler, Martin Building Company, Project Sponsor. Email to Jenny Delumo, San Francisco Planning Department, regarding 88 Arkansas Street, February 10, 2015. This document is available for review at the San Francisco Planning Department, 1650 Mission Street, Suite 400 as part of Case File No. 2015-000453ENV.

²⁰ A day-night average sound level.

consider the compatibility of uses when approving residential uses adjacent to or near existing permitted places of entertainment and take all reasonably available means through the City's design review and approval processes to ensure that the design of such new residential development projects take into account the needs and interests of both the places of entertainment and the future residents of the new development.

The project site is located within a 300-foot radius of a Place of Entertainment,²¹ and is therefore subject to the Places of Entertainment Ordinance. The Planning Department will consider the compatibility of uses when approving residential uses near existing permitted Places of Entertainment and, if provided, any comments from the Entertainment Commission during the permit review period. At the time of project approval a Notice of Special Restrictions (NSR) for the project site must be recorded with the Assessor. The NSR would state the restrictions prescribed by the ordinance and any other conditions the Planning Department or Planning Commission places on the property. For projects that applied for a permit before the ordinance became effective and for which a first construction document has not been issued, such as the proposed project, the sponsor shall be subject to implementation of the Places of Entertainment Ordinance per Section 116.11 of the Administrative Code. Notice was provided to the project sponsor and the Entertainment Commission about the applicability of the Places of Entertainment ordinance to the proposed project.²²

In compliance with the ordinance, the project sponsor would be required to provide a copy of Section 116 and a written disclosure to future residential tenants of the proposed building that the site is located near an existing Place of Entertainment, and that there is the potential for noise generation from their operation. A copy of the disclosure notice and an affidavit of disclosure must be provided to the San Francisco Entertainment Commission.

The regulations and procedures set forth by the San Francisco Noise Regulations Relating to Residential Uses Near Places of Entertainment are consistent with the provisions of PEIR Mitigation Measures F-3 and F-4. The proposed project would introduce noise-sensitive uses (residential dwelling units) to an area where ambient noise levels exceed 60 dBA (Ldn), therefore Mitigation Measure F-4 applies to the project. In accordance with PEIR Mitigation Measure F-4, the project sponsor has conducted an environmental noise study demonstrating that the proposed project can feasibly attain acceptable interior noise levels.²³ The noise study prepared for the project at 88 Arkansas Street included a noise survey conducted at four locations between March 9, 2015 and March 11, 2015. A sound meter was placed on each of the streets that bound the subject block: Arkansas Street, 17th Street, Wisconsin Street, and 16th Street in order to obtain long-term continuous noise measurements. Based on the investigation, traffic-related noise is the primary source of noise affecting the noise environment on the project site and in the site vicinity. Measured noise levels were found to be 68 dBA (Ldn) on Arkansas Street, 68 dBA (Ldn) on 17th Street, 72 dBA (Ldn) on Wisconsin Street, and 76 dBA (Ldn) on 16th Street. No live events took place at the entertainment venue located on the block adjacent to the project site during this timeframe, and thus noise from that venue is not accounted for in this analysis. It is estimated that the anticipated future increase in traffic volume in the site vicinity would result in a 1 dBA increase in ambient noise over ten years. The noise study

²¹ Thee Parkside (1600 17th Street); Bottom of the Hill (1233 17th Street)

²² Kate Connor, San Francisco Planning Department, *Email to Project Sponsor, Entertainment Commission, and Environmental Planner regarding 88 Arkansas Street, December 21, 2015*. This document is available for review at the San Francisco Planning Department, 1650 Mission Street, Suite 400 as part of Case File No. 2015-000453ENV.

²³ Charles M. Salter Associates, Inc., *88 Arkansas Street residences, San Francisco, CA, May 13, 2015*. This document is available for review at the San Francisco Planning Department, 1650 Mission Street, Suite 400 as part of Case File No. 2015-000453ENV.

concluded that the proposed project would be able to achieve compliance with Title 24, Part II of the California Code of Regulations (Title 24) interior noise standards provided the project sponsor incorporates window and building assemblies that meet the minimum Sound Transmission Class (STC)²⁴ ratings as identified in the report and installs a ventilation or air-conditioning system that does not compromise sound attenuation. Incorporation of the recommendations provided in the report would be consistent with the requirements of Mitigation Measure F-4 of the Eastern Neighborhoods PEIR. The project sponsor has agreed to implement the recommendations for noise attenuation as **Project Mitigation Measure 3 Sitting of Noise-Sensitive Uses** (full text provided in the Mitigation Measure section below).

Eastern Neighborhoods PEIR Mitigation Measure F-5 addresses impacts related to individual projects that include new noise-generating uses that would be expected to generate noise levels in excess of ambient noise in the proposed project site vicinity. The proposed project does not include noise-generating land uses. While the proposed project includes commercial space on the ground floor, it is not anticipated that use of the commercial space would generate noise above existing ambient noise levels in the project site vicinity. Therefore, Mitigation Measure F-5 is not applicable.

Mitigation Measure F-6 addresses impacts from existing ambient noise levels on open space required under the Planning Code for new development that includes noise sensitive uses. The proposed project at 88 Arkansas Street would provide open space via two ground-floor courtyards, a fifth-floor solarium, and a shared deck and recreation area on the roof level. As such, Mitigation Measure F-6 is applicable to the proposed project, and has been incorporated into the project as **Project Mitigation Measure 4 Open Space in Noisy Environments**, which is described in the Mitigation Measure Section below. The noise study prepared for the proposed project assessed noise levels at the proposed outdoor spaces. The noise study estimated that, as proposed, sound levels would be 53 dBA (Ldn) at the north courtyard, 53 dBA (Ldn) at the south courtyard, and 64 dBA (Ldn) at the roof level. The proposed solarium would be open to the sky, but surrounded by the fifth floor dwelling units, and therefore it is not anticipated this space would need additional acoustical shielding. The report concludes that the proposed building design would provide the open spaces with adequate acoustical shielding from existing ambient noise. Therefore, the requirements of Eastern Neighborhoods PEIR Mitigation Measure F-6 have been complied with as part of this environmental review process.

The project site is not located within an airport land use plan area, within two miles of a public airport, or in the vicinity of a private airstrip. Therefore, topic 12e and f from the CEQA Guidelines, Appendix G is not applicable.

For the above reasons, the proposed project would not result in significant noise impacts that were not identified in the Eastern Neighborhoods PEIR.

²⁴ A figure used to rate how well a building partition attenuates sound. STC ratings typically address the isolation of speech sounds and do not low-frequency noises, such as music.

<i>Topics:</i>	<i>Significant Impact Peculiar to Project or Project Site</i>	<i>Significant Impact not Identified in PEIR</i>	<i>Significant Impact due to Substantial New Information</i>	<i>No Significant Impact not Previously Identified in PEIR</i>
6. AIR QUALITY—Would the project:				
a) Conflict with or obstruct implementation of the applicable air quality plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Violate any air quality standard or contribute substantially to an existing or projected air quality violation?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Result in a cumulatively considerable net increase of any criteria pollutant for which the project region is non-attainment under an applicable federal, state, or regional ambient air quality standard (including releasing emissions which exceed quantitative thresholds for ozone precursors)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Expose sensitive receptors to substantial pollutant concentrations?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Create objectionable odors affecting a substantial number of people?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

The Eastern Neighborhoods PEIR identified potentially significant air quality impacts resulting from construction activities and impacts to sensitive land uses²⁵ as a result of exposure to elevated levels of diesel particulate matter (DPM) and other toxic air contaminants (TACs). The Eastern Neighborhoods PEIR identified four mitigation measures that would reduce these air quality impacts to less-than-significant levels and stated that with implementation of identified mitigation measures, the Area Plan would be consistent with the Bay Area 2005 Ozone Strategy, the applicable air quality plan at that time. All other air quality impacts were found to be less than significant.

Eastern Neighborhoods PEIR Mitigation Measure G-1 addresses air quality impacts during construction, PEIR Mitigation Measure G-2 addresses the siting of sensitive land uses near sources of TACs and PEIR Mitigation Measures G-3 and G-4 address proposed uses that would emit DPM and other TACs.

Construction Dust Control

Eastern Neighborhoods PEIR Mitigation Measure G-1 Construction Air Quality requires individual projects involving construction activities to include dust control measures and to maintain and operate construction equipment so as to minimize exhaust emissions of particulates and other pollutants. The San Francisco Board of Supervisors subsequently approved a series of amendments to the San Francisco Building and Health Codes, generally referred to as the Construction Dust Control Ordinance (Ordinance 176-08, effective July 30, 2008). The intent of the Construction Dust Control Ordinance is to reduce the quantity of fugitive dust generated during site preparation, demolition, and construction work in order to protect the health of the general public and of on-site workers, minimize public nuisance complaints, and to avoid orders to stop work by DBI. Project-related construction activities would result in construction dust, primarily from ground-disturbing activities. For projects over one half-acre, such as the proposed project, the Dust Control Ordinance requires that the project sponsor submit a Dust Control Plan for

²⁵ The Bay Area Air Quality Management District (BAAQMD) considers sensitive receptors as: children, adults or seniors occupying or residing in: 1) residential dwellings, including apartments, houses, condominiums, 2) schools, colleges, and universities, 3) daycares, 4) hospitals, and 5) senior care facilities. BAAQMD, Recommended Methods for Screening and Modeling Local Risks and Hazards, May 2011, page 12.

approval by the San Francisco Department of Public Health. DBI will not issue a building permit without written notification from the Director of Public Health that the applicant has a site-specific Dust Control Plan, unless the Director waives the requirement. The site-specific Dust Control Plan would require the project sponsor to implement additional dust control measures such as installation of dust curtains and windbreaks and to provide independent third-party inspections and monitoring, provide a public complaint hotline, and suspend construction during high wind conditions.

The regulations and procedures set forth by the San Francisco Dust Control Ordinance would ensure that construction dust impacts would not be significant. These requirements supersede the dust control provisions of PEIR Mitigation Measure G-1. Therefore, the portion of PEIR Mitigation Measure G-1 Construction Air Quality that addresses dust control is no longer applicable to the proposed project.

Criteria Air Pollutants

In accordance with the state and federal Clean Air Acts, air pollutant standards are identified for the following six criteria air pollutants: ozone, carbon monoxide (CO), particulate matter (PM), nitrogen dioxide (NO₂), sulfur dioxide (SO₂), and lead. These air pollutants are termed criteria air pollutants because they are regulated by developing specific public health- and welfare-based criteria as the basis for setting permissible levels. In general, the San Francisco Bay Area Air Basin (SFBAAB) experiences low concentrations of most pollutants when compared to federal or state standards. The SFBAAB is designated as either in attainment or unclassified for most criteria pollutants with the exception of ozone, PM_{2.5}, and PM₁₀, for which these pollutants are designated as non-attainment for either the state or federal standards. By its very nature, regional air pollution is largely a cumulative impact in that no single project is sufficient in size to, by itself, result in non-attainment of air quality standards. Instead, a project's individual emissions contribute to existing cumulative air quality impacts. If a project's contribution to cumulative air quality impacts is considerable, then the project's impact on air quality would be considered significant.

While the Eastern Neighborhoods PEIR determined that at a program-level the Eastern Neighborhoods Rezoning and Area Plans would not result in significant regional air quality impacts, the PEIR states that "Individual development projects undertaken in the future pursuant to the new zoning and area plans would be subject to a significance determination based on the Bay Area Air Quality Management District's (BAAQMD) quantitative thresholds for individual projects."²⁶ The BAAQMD prepared updated 2011 BAAQMD CEQA Air Quality Guidelines (Air Quality Guidelines),²⁷ which provided new methodologies for analyzing air quality impacts. The Air Quality Guidelines also provide thresholds of significance for those criteria air pollutants that the SFBAAB is in non-attainment. These thresholds of significance are used by the City to help assess whether a project's criteria air pollutant emissions would violate an air quality standard, contribute to an existing or projected air quality violation, or result in a cumulatively considerable net increase in criteria air pollutants.

The Air Quality Guidelines also provide screening criteria for determining whether a land use development project has the potential to exceed thresholds of significance. Projects that exceed the screening criteria require further air quality assessment in order to further analyze project-related criteria air pollutant emissions. Projects that meet the screening criteria are found to be less than significant with

²⁶ San Francisco Planning Department, Eastern Neighborhood's Rezoning and Area Plans Final Environmental Impact Report. See page 346. Available online at: <http://www.sf-planning.org/Modules/ShowDocument.aspx?documentid=4003>. Accessed June 4, 2014.

²⁷ Bay Area Air Quality Management District, CEQA Air Quality Guidelines, updated May 2011. See pp. 3-2 to 3-3..

respect to project-generated criteria air pollutant emissions. The proposed 127 dwelling units and 3,275 gsf of retail space meets the Air Quality Guidelines screening criteria for construction and operations.²⁸ However, approximately 18,000 cubic yards of soil would be excavated and exported off site. This exceeds the BAAQMD's screening criteria that states that construction-related activities should not include extensive material transport (i.e., greater than 10,000 cubic yards of soil import/export) requiring a considerable amount of haul truck activity. Thus, a project-specific analysis of construction emissions is provided below.

Construction

Construction activities from the proposed project would result in the emission of criteria air pollutants from equipment exhaust, construction-related vehicular activity, and construction worker automobile trips. In addition, construction of the proposed project, which would occur over approximately 19 months, would result in approximately 18,000 cubic yards of soil excavation and export. As previously discussed, the Air Quality Guidelines prescribe that construction-related activities should not include extensive material transport requiring a considerable amount of haul truck activity. As the proposed project would exceed the material transport screening criterion, construction-related criteria air pollutants generated by the proposed project were quantified using the California Emissions Estimator Model (CalEEMod) and provided within an Air Quality Analysis Memo for 88 Arkansas Street.²⁹ The model was developed, including default data (e.g., emission factors, meteorology, etc.) in collaboration with California air districts' staff. Default assumptions were used where project-specific information was unknown. Emissions were converted from tons/year to lbs/day using the estimated construction duration of 479 working days. As shown in **Table 4**, unmitigated project construction emissions would be below the threshold of significance for all criteria air pollutants. As such, no additional mitigation measures are required, and construction-related air quality impacts would not be significant.

Table 4: Daily Project Construction Emissions

	Pollutant Emissions (Average Pounds per Day)			
	ROG	NOx	Exhaust PM ₁₀	Exhaust PM _{2.5}
Unmitigated Project Emissions	26.06	41.93	1.56	0.59
Significance Threshold	54.0	54.0	82.0	54.0

Source: BAAQMD, 2011; Planning Department

Operation

The proposed 127 dwelling units and 3,275 gsf of retail space meets the Air Quality Guidelines screening criteria for operations. Therefore, the proposed project would not result in significant impacts with respect to operational criteria air pollutant emissions. For these reasons, implementation of the proposed project would not result in either project-level or cumulative significant impacts that were not identified in the

²⁸ The screening criteria level for an "Apartment, mid-rise" is 494 dwelling units for operations and 240 dwelling units for construction. The screening criteria level for a "Fast food restaurant without a drive through" is 8,000 sf for operations and 277,000 sf for construction. The project sponsor anticipates the retail space would be used as a café, and this land use category best reflects that use and is one of the most restrictive uses for a small retail space.

²⁹ San Francisco Planning Department, *Air Quality Analysis Memo for 88 Arkansas Street*, December 2, 2015. This document is available for review at the San Francisco Planning Department, 1650 Mission Street, Suite 400 as part of Case File No. 2015-000453ENV.

Eastern Neighborhoods PEIR related to violations of air quality standards or substantial increases in non-attainment criteria air pollutants.

Health Risk

Since certification of the PEIR, the San Francisco Board of Supervisors approved a series of amendments to the San Francisco Building and Health Codes, generally referred to as the Enhanced Ventilation Required for Urban Infill Sensitive Use Developments or Health Code, Article 38 (Ordinance 224-14, effective December 8, 2014)(Article 38). The purpose of Article 38 is to protect the public health and welfare by establishing an Air Pollutant Exposure Zone and imposing an enhanced ventilation requirement for all urban infill sensitive use development within the Air Pollutant Exposure Zone. The Air Pollutant Exposure Zone as defined in Article 38 are areas that, based on modeling of all known air pollutant sources, exceed health protective standards for cumulative PM_{2.5} concentration, cumulative excess cancer risk, and incorporates health vulnerability factors and proximity to freeways. Projects within the Air Pollutant Exposure Zone require special consideration to determine whether the project's activities would expose sensitive receptors to substantial air pollutant concentrations or add emissions to areas already adversely affected by poor air quality.

Construction

The project site is not located within an identified Air Pollutant Exposure Zone. Therefore, the ambient health risk to sensitive receptors from air pollutants is not considered substantial and the remainder of Mitigation Measure G-1 that requires the minimization of construction exhaust emissions is not applicable to the proposed project.

Siting Sensitive Land Uses

The proposed project would include development of 127 residential dwelling units and is considered a sensitive land use for purposes of air quality evaluation. As discussed above, the ambient health risk to sensitive receptors from air pollutants is not considered substantial and Article 38 is not applicable to the proposed project. Therefore, PEIR Mitigation Measure G-2 Air Quality for Sensitive Land Uses is not applicable to the proposed project, and impacts related to siting of new sensitive land uses would be less than significant.

Siting New Sources

The proposed project would not be expected to generate activity equivalent to 100 trucks per day or 40 refrigerated trucks per day. Therefore, Eastern Neighborhoods PEIR Mitigation Measure G-3 is not applicable. In addition, the proposed project would not include any sources that would emit DPM or other TACs. Therefore, Eastern Neighborhoods PEIR Mitigation Measure G-4 is not applicable and impacts related to siting new sources of pollutants would be less than significant.

Conclusion

For the above reasons, the proposed project would not result in significant air quality impacts that were not identified in the PEIR.

<i>Topics:</i>	<i>Significant Impact Peculiar to Project or Project Site</i>	<i>Significant Impact not Identified in PEIR</i>	<i>Significant Impact due to Substantial New Information</i>	<i>No Significant Impact not Previously Identified in PEIR</i>
7. GREENHOUSE GAS EMISSIONS—Would the project:				
a) Generate greenhouse gas emissions, either directly or indirectly, that may have a significant impact on the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Conflict with any applicable plan, policy, or regulation of an agency adopted for the purpose of reducing the emissions of greenhouse gases?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

The Eastern Neighborhoods PEIR assessed the GHG emissions that could result from rezoning of the Showplace Square/Potrero Hill Area Plan under the three rezoning options. The Eastern Neighborhoods Rezoning Options A, B, and C are anticipated to result in GHG emissions on the order of 4.2, 4.3, and 4.5 metric tons of CO₂E³⁰ per service population,³¹ respectively. The Eastern Neighborhoods PEIR concluded that the resulting GHG emissions from the three options analyzed in the Eastern Neighborhoods Area Plans would be less than significant. No mitigation measures were identified in the PEIR.

The proposed project was determined to be consistent with San Francisco's GHG Reduction Strategy,³² which is comprised of regulations that have proven effective in reducing San Francisco's overall GHG emissions; GHG emissions have measurably reduced when compared to 1990 emissions levels, demonstrating that the City has met and exceeded Executive Order S-3-05, AB 32, and the Bay Area 2010 Clean Air Plan GHG reduction goals for the year 2020.³³ Other existing regulations, such as those implemented through Assembly Bill (AB) 32, will continue to reduce a proposed project's contribution to climate change. Therefore, the proposed project's GHG emissions would not conflict with state, regional, and local GHG reduction plans and regulations, and thus the proposed project's contribution to GHG emissions would not be cumulatively considerable or generate GHG emissions, either directly or indirectly, that would have a significant impact on the environment.

As the proposed project is within the development projected under the Eastern Neighborhoods Rezoning and Area Plans, there would be no additional impacts on greenhouse gas emissions beyond those analyzed in the Eastern Neighborhoods PEIR.

³⁰ CO₂E, defined as equivalent Carbon Dioxide, is a quantity that describes other greenhouse gases in terms of the amount of Carbon Dioxide that would have an equal global warming potential.

³¹ Memorandum from Jessica Range to Environmental Planning staff, Greenhouse Gas Analyses for Community Plan Exemptions in Eastern Neighborhoods, April 20, 2010. This memorandum provides an overview of the GHG analysis conducted for the Eastern Neighborhoods PEIR and provides an analysis of the emissions using a service population (equivalent of total number of residents and employees) metric.

³² *Greenhouse Gas Analysis Compliance Checklist*, 88 Arkansas Street, March 18, 2015. This document is available for review at the San Francisco Planning Department, 1650 Mission Street, Suite 400 as part of Case File No. 2015-000453ENV.

³³ Executive Order S-3-05, Assembly Bill 32, and the Bay Area 2010 Clean Air Plan set a target of reducing GHG emissions to below 1990 levels by year 2020.

<i>Topics:</i>	<i>Significant Impact Peculiar to Project or Project Site</i>	<i>Significant Impact not Identified in PEIR</i>	<i>Significant Impact due to Substantial New Information</i>	<i>No Significant Impact not Previously Identified in PEIR</i>
8. WIND AND SHADOW—Would the project:				
a) Alter wind in a manner that substantially affects public areas?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Create new shadow in a manner that substantially affects outdoor recreation facilities or other public areas?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Wind

Based upon the experience of the Planning Department in reviewing wind analyses and expert opinion on other projects, it is generally (but not always) the case that projects under 80 feet in height do not have the potential to generate significant wind impacts. Although the proposed 48-foot-tall building would be taller than the immediately adjacent buildings, it would be similar in height to existing buildings in the surrounding area. As discussed in the Project Description section, the buildings on the subject block range from approximately 15 to 40 feet in height. For the above reasons, the proposed project is not anticipated to cause significant impacts related to wind that were not identified in the Eastern Neighborhoods PEIR.

Shadow

Planning Code Section 295 generally prohibits new structures above 40 feet in height that would cast additional shadows on open space that is under the jurisdiction of the San Francisco Recreation and Park Commission between one hour after sunrise and one hour before sunset, at any time of the year, unless that shadow would not result in a significant adverse effect on the use of the open space. Under the Eastern Neighborhoods Rezoning and Area Plans, sites surrounding parks could be redeveloped with taller buildings without triggering Section 295 of the Planning Code because certain parks are not subject to Section 295 of the Planning Code (i.e., under jurisdiction of departments other than the Recreation and Parks Commission or privately owned). The Eastern Neighborhoods PEIR could not conclude if the rezoning and community plans would result in less-than-significant shadow impacts because the feasibility of complete mitigation for potential new shadow impacts of unknown proposals could not be determined at that time. Therefore, the PEIR determined shadow impacts to be significant and unavoidable. No mitigation measures were identified in the PEIR.

The proposed project would construct a 48-foot-tall building (up to 65 feet with staircase and elevator penthouses); therefore, the Planning Department prepared a preliminary shadow fan analysis to determine whether the project would have the potential to cast new shadow on nearby parks.³⁴ The shadow fan analysis indicated that the proposed project could potentially shade the Jackson Playground and Recreation Center (Jackson Playground or “the park”), an approximately 4.4-acre park located across the street from the project site on the south side of 17th Street. As shown in **Figure 12**, Jackson Playground is a full-block park bounded by 17th Street to the north, Mariposa Street to the south, Arkansas Street to the east, and Carolina Street to the west.

³⁴ San Francisco Planning Department, *Preliminary Shadow Fan Analysis, 88 Arkansas Street*, February 10, 2015. This document is available for review at the San Francisco Planning Department, 1650 Mission Street, Suite 400 as part of Case File No. 2015-000453ENV.

Figure 13 – Jackson Playground and Recreation Center



- ① Public Entries
- ② Basketball Court
- ③ Tennis Court
- ④ Learning Garden
- ⑤ Children's Play Areas
- ⑥ Recreation Center
- ⑦ Picnic Areas (under trees)
- ⑧ Baseball fields

Jackson Playground is under the jurisdiction of the Recreation and Park Commission. Thus, the park is subject to Section 295 of the Planning Code. Based on the results of the preliminary shadow fan analysis, a more detailed shadow study was prepared for the proposed project.³⁵ The shadow study consisted of quantitative and qualitative analysis of the potential shadow impacts, including existing surrounding buildings and cumulative projects (i.e. other proposed development projects).

³⁵ PreVision Design, *Shadow Analysis Report for the Proposed 88 Arkansas Street Project Per SF Planning Section 295 Standards*, May 21, 2015. This document is available for review at the San Francisco Planning Department, 1650 Mission Street, Suite 400 as part of Case File No. 2015-000453ENV.

The Proposition K memorandum, dated February 3, 1989, was developed by the Recreation and Park Department and the Planning Department.³⁶ The memo established tolerance levels for new shading for specific parks and established criteria for parks not named in the memo but still subject to Section 295 review. The tolerance limits are based on the new shadow-foot-hours that would potentially be added to a park as a percentage of the theoretical total square-foot-hours (sfh)³⁷ of sunlight for that over a period of one year. Jackson Playground, which is shaded approximately 6.481 percent of the year, was not included specifically in the 1989 memo. However, the park would be subject to the generic criteria established for parks larger than two acres and shadowed less than 20 percent of the year. For parks with these characteristics, the Proposition K memo established a potentially permissible quantitative limit for additional shadows known as the Absolute Cumulative Limit. Projects may contribute new shadow to the park up to the Absolute Cumulative Limit of 1.0 percent if the new shadow also meets additional qualitative criteria. The qualitative criteria includes existing shadow profiles, important times of day and seasons in the year, the size and duration of new shadows, and the public good served by buildings casting new shadow.

The shadow analysis was conducted for representative times of the day for three representative days of the year. The representative days are the summer solstice (June 21), when the midday sun is at its highest and shadows are shortest; the winter solstice (December 21), when the midday sun is at its lowest and shadows are longest; and the spring/fall equinox (March 21/September 21), when shadows are midway through a period of lengthening.

New shadow would be cast by the proposed building between May 18 and July 25, for a total of 69 days. The summer solstice, June 21, was found to be the “worst case” day, when the estimated net new shadow on Jackson Playground would be at its largest and longest duration. On the day of maximum shading, new shadow would be present at 6:48 a.m. (sunrise + one hour) and would be gone by 7:15 AM. New shadow would reappear around 7:00 PM and last through 7:35 PM (sunset – one hour) (see **Figures 14-21**).³⁸

Thus, the proposed building would add new shadow to the park during hours regulated by Planning Code Section 295 (i.e., from one hour after sunrise to one hour after sunset). The longest duration of new shadow would be approximately 27 minutes and the average shadow would be cast for 15 minutes. New shadow on Jackson Playground would happen during normal hours of operation.

³⁶ San Francisco Planning Department, *Proposition K – The Sunlight Ordinance Memorandum, February 3, 1989*. This document is available for review at the San Francisco Planning Department, 1650 Mission Street, Suite 400 as part of Case File No. 2015-000453ENV.

³⁷ The amount of sun the park would receive throughout the year if there was no shade on the park at any time.

³⁸ Shadow figures for 9:00 a.m. - 7:00 p.m. not included as the proposed project would not shadow Jackson Playground at that time. Those figures are available in the *Shadow Analysis Report for the Proposed Project at 88 Arkansas Street*, which is available for review at the San Francisco Planning Department, 1650 Mission Street, Suite 400 as part of Case File No. 2015-000453ENV.

Figure 14 – June 21: 6:48 a.m. (Sunrise + 1 Hour)



88 ARKANSAS STREET
Shadow Profiles on Summer Solstice / Day of Max Shading



SUMMER SOLSTICE / DAY OF MAX SHADING
JUNE 21

6:48 AM

- Proposed 88 Arkansas Street
 - Current (existing) Shadow
 - Net New Shading from Project
 - Cumulative Projects (list on right) →
 - Profiles Cumulative Project Shading
- ① 540 De Haro Street
 - ② 580 De Haro Street
 - ③ 1301 16th Street
 - ④ 153 Arkansas Street
 - ⑤ 1601 Mariposa Street

- Jackson Playground
- Public Entries ①
 - Basketball Court ②
 - Tennis Court ③
 - Learning Garden ④
 - Children's Play Areas ⑤
 - Recreation Center ⑥
 - Picnic Areas ⑦
 - Grass/Ball fields ⑧

Figure 15 – June 21: 7:00 a.m.

PREVISION DESIGN











88 ARKANSAS STREET

Shadow Profiles on Summer Solstice / Day of Max Shading



SUMMER SOLSTICE / DAY OF MAX SHADING
JUNE 21

7:00 AM

- | | |
|---|--|
|  Proposed 88 Arkansas Street |  ① 540 De Haro Street |
|  Current (existing) Shadow |  ② 580 De Haro Street |
|  Net New Shading from Project |  ③ 1301 16th Street |
|  Cumulative Projects (list on right) → |  ④ 153 Arkansas Street |
|  Profiles Cumulative Project Shading |  ⑤ 1601 Mariposa Street |


- | | |
|---|-------------------------|
|  Jackson Playground | |
| Public Entries ① | Children's Play Areas ⑤ |
| Basketball Court ② | Recreation Center ⑥ |
| Tennis Court ③ | Picnic Areas ⑦ |
| Learning Garden ④ | Grass/Ball fields ⑧ |

Figure 16 – June 21: 7:15 a.m.

PREVISION DESIGN

88 ARKANSAS STREET

Shadow Profiles on Summer Solstice / Day of Max Shading



SUMMER SOLSTICE / DAY OF MAX SHADING
JUNE 21

7:15 AM

- Proposed 88 Arkansas Street
 - Current (existing) Shadow
 - Net New Shading from Project
 - Cumulative Projects (list on right) →
 - Profiles Cumulative Project Shading
- ① 540 De Haro Street
 - ② 580 De Haro Street
 - ③ 1301 16th Street
 - ④ 153 Arkansas Street
 - ⑤ 1601 Mariposa Street

- Jackson Playground
- Public Entries ①
- Basketball Court ②
- Tennis Court ③
- Learning Garden ④
- Children's Play Areas ⑤
- Recreation Center ⑥
- Picnic Areas ⑦
- Grass/Ball fields ⑧

Figure 17 – June 21: 8:00 a.m.

PREVISION DESIGN











88 ARKANSAS STREET

Shadow Profiles on Summer Solstice / Day of Max Shading



SUMMER SOLSTICE / DAY OF MAX SHADING
JUNE 21

8:00 AM

- | | |
|---|--|
|  Proposed 88 Arkansas Street |  ① 540 De Haro Street |
|  Current (existing) Shadow |  ② 580 De Haro Street |
|  Net New Shading from Project |  ③ 1301 16th Street |
|  Cumulative Projects (list on right) → |  ④ 153 Arkansas Street |
|  Profiles Cumulative Project Shading |  ⑤ 1601 Mariposa Street |

- | |
|---|
|  Jackson Playground |
| Public Entries ① |
| Basketball Court ② |
| Tennis Court ③ |
| Learning Garden ④ |
| Children's Play Areas ⑤ |
| Recreation Center ⑥ |
| Picnic Areas ⑦ |
| Grass/Ball fields ⑧ |

Figure 18 – June 21: 7:00 p.m.



88 ARKANSAS STREET

Shadow Profiles on Summer Solstice / Day of Max Shading



SUMMER SOLSTICE / DAY OF MAX SHADING
JUNE 21

7:00 PM

- | | |
|---------------------------------------|----------------------|
| Proposed 88 Arkansas Street | 540 De Haro Street |
| Current (existing) Shadow | 580 De Haro Street |
| Net New Shading from Project | 1301 16th Street |
| Cumulative Projects (list on right) → | 153 Arkansas Street |
| Profiles Cumulative Project Shading | 1601 Mariposa Street |

- | | |
|--------------------|-----------------------|
| Jackson Playground | |
| Public Entries | Children's Play Areas |
| Basketball Court | Recreation Center |
| Tennis Court | Picnic Areas |
| Learning Garden | Grass/Ball fields |

Figure 19 – June 21: 7:15 p.m.

PREVISION DESIGN

88 ARKANSAS STREET

Shadow Profiles on Summer Solstice / Day of Max Shading

SUMMER SOLSTICE / DAY OF MAX SHADING
JUNE 21**7:15 PM**

- Proposed 88 Arkansas Street
 - Current (existing) Shadow
 - Net New Shading from Project
 - Cumulative Projects (list on right) →
 - Profiles Cumulative Project Shading
- ① 540 De Haro Street
 - ② 580 De Haro Street
 - ③ 1301 16th Street
 - ④ 153 Arkansas Street
 - ⑤ 1601 Mariposa Street











- Jackson Playground
- Public Entries ①
- Basketball Court ②
- Tennis Court ③
- Learning Garden ④
- Children's Play Areas ⑤
- Recreation Center ⑥
- Picnic Areas ⑦
- Grass/Ball fields ⑧

Figure 20 – June 21: 7:25 p.m.

**88 ARKANSAS STREET**

Shadow Profiles on Summer Solstice / Day of Max Shading

**SUMMER SOLSTICE / DAY OF MAX SHADING
JUNE 21****7:35 PM**

- | | |
|---|--|
|  Proposed 88 Arkansas Street |  ① 540 De Haro Street |
|  Current (existing) Shadow |  ② 580 De Haro Street |
|  Net New Shading from Project |  ③ 1301 16th Street |
|  Cumulative Projects (list on right) → |  ④ 153 Arkansas Street |
|  Profiles Cumulative Project Shading |  ⑤ 1601 Mariposa Street |


- | | |
|---|-------------------------|
|  Jackson Playground | |
| Public Entries ① | Children's Play Areas ⑥ |
| Basketball Court ② | Recreation Center ⑦ |
| Tennis Court ③ | Picnic Areas ⑧ |
| Learning Garden ④ | Grass/Ball fields ⑤ |

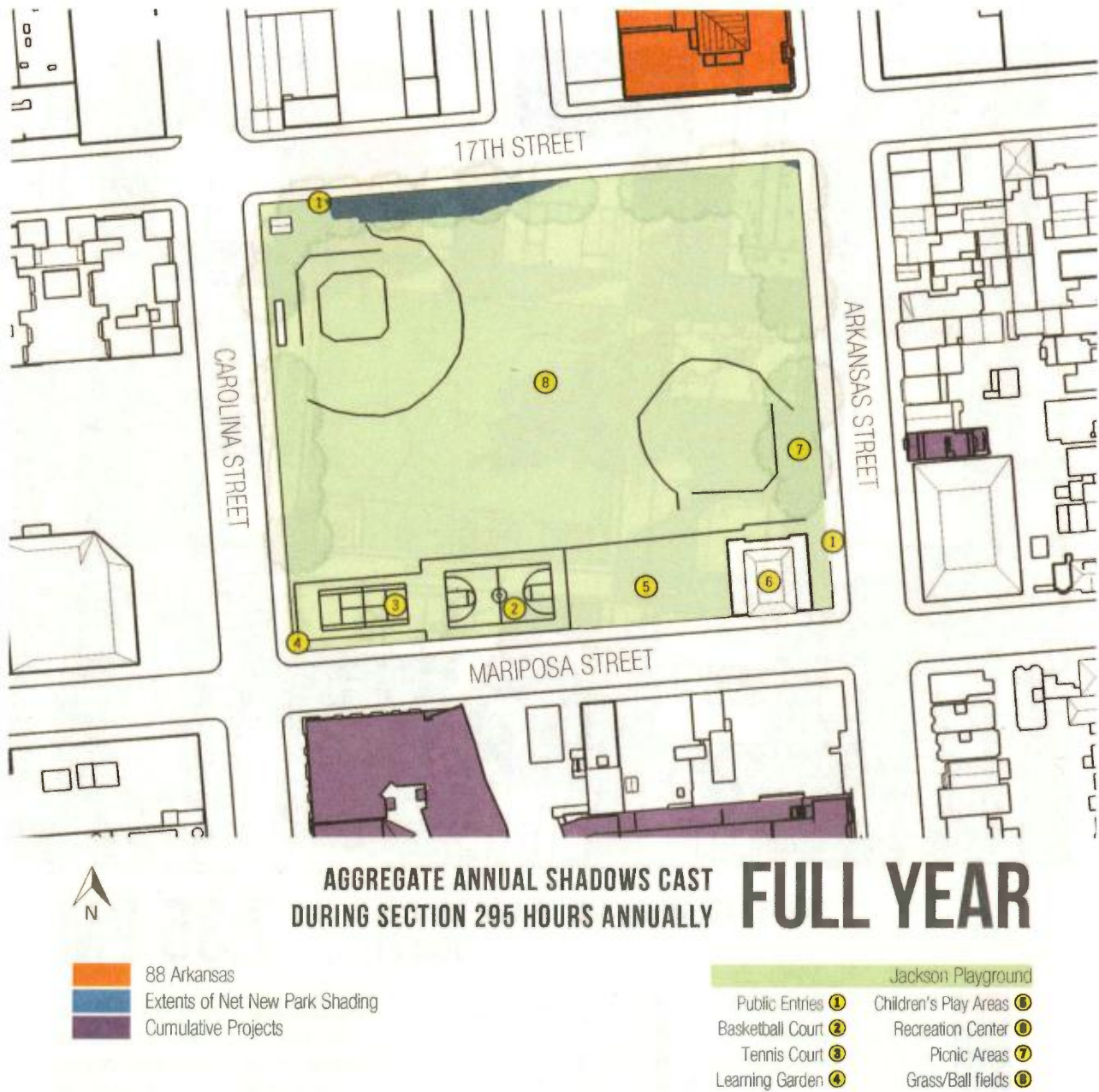
Figure 21 – June 21: 7:35 p.m. (Sunset – 1 Hour)

Figure 22 – Full Year Aggregate Shadow

PREVISION DESIGN

88 ARKANSAS STREET

Aggregate annual new project shadows



As shown in Figure 22, net new shadow would be cast on the northwestern edge and northeastern corner of the park. The proposed project would increase the total annual shadow coverage on Jackson

Playground by an estimated 0.003 percent, which would result in a new total annual shading of approximately 6.484 percent. The project-specific shadow increase is below the Absolute Cumulative Limit of 1.0 percent. When taking cumulative projects into consideration, the shadow study found that cumulative shadow from other proposed projects would increase shadow on the park by 0.406 percent. This would result in new total annual shading of approximately 0.467 percent. Thus, cumulative shading from the proposed project plus other known projects would be below the Absolute Cumulative Limit of 1.0 percent. In addition, new shadow from other foreseeable projects is not anticipated to intersect with shadow from the proposed project.

The qualitative analysis for the shadow study included six 30-minute field observations, which were conducted between April 21, 2015 and April 26, 2015 at various times of the day in order to assess park usage. Over the course of these site visits, approximately 40 to 144 users were observed at Jackson Playground. Peak activity at Jackson Playground takes place weekend mornings, with patrons primarily using the baseball fields or lying/sitting/crossing the grass. During the weekdays park usage was fairly consistent. Peak activity at the basketball courts and playground typically occurs during the morning and the basketball courts are used throughout the day. User activity was observed in the area where new morning shade would occur, which is near the northwestern baseball field. No user activity was observed in the area where new evening shade would occur. While the areas of new shade are located near the baseball field, the report found that new shadow would be located on areas of the park already shaded by existing trees. Even if the existing trees did not already provide shade in this area, the new shade would not be expected to substantially affect the use and enjoyment of the park as the shade would primarily occur in the early morning and would be of short duration. Therefore, the proposed project would result in less-than-significant shadow impacts on Jackson Playground.

The proposed project was not found to have the potential to affect other public open spaces in the site vicinity. The proposed project would shade portions of nearby streets and sidewalks and private property at times within the project vicinity. Shadows upon streets and sidewalks would not exceed levels commonly expected in urban areas and would be considered a less-than-significant effect under CEQA. Although occupants of nearby property may regard the increase in shadow as undesirable, the limited increase in shading of private properties as a result of the proposed project would not be considered a significant impact under CEQA.

For the above reasons, the proposed project would not result in significant impacts related to shadow that were not identified in the Eastern Neighborhoods PEIR.

<i>Topics:</i>	<i>Significant Impact Peculiar to Project or Project Site</i>	<i>Significant Impact not Identified in PEIR</i>	<i>Significant Impact due to Substantial New Information</i>	<i>No Significant Impact not Previously Identified in PEIR</i>
9. RECREATION—Would the project:				
a) Increase the use of existing neighborhood and regional parks or other recreational facilities such that substantial physical deterioration of the facilities would occur or be accelerated?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Include recreational facilities or require the construction or expansion of recreational facilities that might have an adverse physical effect on the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Physically degrade existing recreational resources?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

The Eastern Neighborhoods PEIR concluded that implementation of the Eastern Neighborhoods Rezoning and Area Plans would not result in substantial or accelerated deterioration of existing recreational resources or require the construction or expansion of recreational facilities that may have an adverse effect on the environment. No mitigation measures related to recreational resources were identified in the Eastern Neighborhoods PEIR.

As part of the Eastern Neighborhoods adoption, the City adopted impact fees for development in Eastern Neighborhoods that goes towards funding recreation and open space. Since certification of the PEIR, the voters of San Francisco passed the 2012 San Francisco Clean and Safe Neighborhood Parks Bond providing the Recreation and Parks Department an additional \$195 million to continue capital projects for the renovation and repair of parks, recreation, and open space assets. This funding is being utilized for improvements and expansion to Garfield Square, South Park, Potrero Hill Recreation Center, Warm Water Cove Park, and Pier 70 Parks Shoreline within the Eastern Neighborhoods Plan area. The impact fees and the 2012 San Francisco Clean and Safe Neighborhood Parks Bond are funding measures similar to that described in PEIR Improvement Measure H-1: Support for Upgrades to Existing Recreation Facilities.

An update of the Recreation and Open Space Element (ROSE) of the General Plan was adopted in April 2014. The amended ROSE provides a 20-year vision for open spaces in the City. It includes information and policies about accessing, acquiring, funding, and managing open spaces in San Francisco. The amended ROSE identifies areas within the Eastern Neighborhoods Plan area for acquisition and the locations where proposed new open spaces and open space connections should be built, consistent with PEIR Improvement Measure H-2: Support for New Open Space. Two of these open spaces, Daggett Park and at 17th and Folsom, are set to open in 2015 and 2016, respectively. In addition, the amended ROSE identifies the role of both the Better Streets Plan (refer to "Transportation" section for description) and the Green Connections Network in open space and recreation. Green Connections are streets and paths that connect people to parks, open spaces, and the waterfront, while enhancing the ecology of the street environment. Six routes identified within the Green Connections Network cross the Eastern Neighborhoods Plan area: Mission to Peaks (Route 6); Noe Valley to Central Waterfront (Route 8), a portion of which has been conceptually designed; Tenderloin to Potrero (Route 18); Downtown to Mission Bay (Route 19); Folsom, Mission Creek to McLaren (Route 20); and Shoreline (Route 24).

As discussed under the Shadow section, the project site is served by Jackson Playground and Recreation Center. With the addition of approximately 127 residential units, the proposed project would not substantially increase demand for, or use of, Jackson Playground or other neighborhood parks to the level where there would be a substantial physical deterioration of recreation facilities. The new residents of the proposed building are within the expected population increase of the Eastern Neighborhoods Plan area.

As the proposed project would not degrade recreational facilities and is within the development projected under the Eastern Neighborhoods Rezoning and Area Plans, there would be no additional impacts on recreation beyond those analyzed in the Eastern Neighborhoods PEIR.

<i>Topics:</i>	<i>Significant Impact Peculiar to Project or Project Site</i>	<i>Significant Impact not Identified in PEIR</i>	<i>Significant Impact due to Substantial New Information</i>	<i>No Significant Impact not Previously Identified in PEIR</i>
10. UTILITIES AND SERVICE SYSTEMS—Would the project:				
a) Exceed wastewater treatment requirements of the applicable Regional Water Quality Control Board?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Require or result in the construction of new water or wastewater treatment facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Require or result in the construction of new storm water drainage facilities or expansion of existing facilities, the construction of which could cause significant environmental effects?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Have sufficient water supply available to serve the project from existing entitlements and resources, or require new or expanded water supply resources or entitlements?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Result in a determination by the wastewater treatment provider that would serve the project that it has inadequate capacity to serve the project's projected demand in addition to the provider's existing commitments?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) Be served by a landfill with sufficient permitted capacity to accommodate the project's solid waste disposal needs?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
g) Comply with federal, state, and local statutes and regulations related to solid waste?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

The Eastern Neighborhoods PEIR determined that the anticipated increase in population would not result in a significant impact to the provision of water, wastewater collection and treatment, and solid waste collection and disposal. No mitigation measures were identified in the PEIR.

Since certification of the PEIR, the San Francisco Public Utilities Commission (SFPUC) adopted the 2010 Urban Water Management Plan (UWMP) in June 2011. The UWMP update includes City-wide demand projections to the year 2035, compares available water supplies to meet demand and presents water demand management measures to reduce long-term water demand. Additionally, the UWMP update includes a discussion of the conservation requirement set forth in Senate Bill 7 passed in November 2009 mandating a statewide 20% reduction in per capita water use by 2020. The UWMP includes a quantification of the SFPUC's water use reduction targets and a plan for meeting these objectives. The UWMP projects sufficient water supply in normal years and a supply shortfall during prolonged droughts. Plans are in place to institute varying degrees of water conservation and rationing as needed in response to severe droughts.

In addition, the SFPUC is in the process of implementing the Sewer System Improvement Program, which is a 20-year, multi-billion dollar citywide upgrade to the City's sewer and stormwater infrastructure to ensure a reliable and seismically safe system. The program includes planned improvements that will serve development in the Eastern Neighborhoods Plan area including at the Southeast Treatment Plant, the Central Bayside System, and green infrastructure projects, such as the Mission and Valencia Green Gateway.

As the proposed project is within the development projected under the Eastern Neighborhoods Rezoning and Area Plans, there would be no additional impacts on utilities and service systems beyond those analyzed in the Eastern Neighborhoods PEIR.

<i>Topics:</i>	<i>Significant Impact Peculiar to Project or Project Site</i>	<i>Significant Impact not Identified in PEIR</i>	<i>Significant Impact due to Substantial New Information</i>	<i>No Significant Impact not Previously Identified in PEIR</i>
11. PUBLIC SERVICES—Would the project:				
a) Result in substantial adverse physical impacts associated with the provision of, or the need for, new or physically altered governmental facilities, the construction of which could cause significant environmental impacts, in order to maintain acceptable service ratios, response times, or other performance objectives for any public services such as fire protection, police protection, schools, parks, or other services?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

The Eastern Neighborhoods PEIR determined that the anticipated increase in population would not result in a significant impact to public services, including fire protection, police protection, and public schools. No mitigation measures were identified in the PEIR.

The proposed project would include a new bulb-out extension on the northwest corner of the 17th Street and Arkansas Street intersection, extending the sidewalk by approximately 33 feet eastward at its widest point and approximately 18 feet southward at its widest point. The location of the proposed bulb-out contains a fire hydrant which may need to be relocated. As the proposed bulb-out would not extend into traffic lanes and would be subject to SFMTA and SFFD approval, the bulb out is not anticipated to impede emergency vehicle access to the site or existing fire suppression infrastructure. To the extent that the fire hydrant would be relocated nearby, any construction-related impacts associated with its relocation are covered by this environmental review. As the proposed project is within the development projected under the Eastern Neighborhoods Rezoning and Area Plans, there would be no additional impacts on public services beyond those analyzed in the Eastern Neighborhoods PEIR.

<i>Topics:</i>	<i>Significant Impact Peculiar to Project or Project Site</i>	<i>Significant Impact not Identified in PEIR</i>	<i>Significant Impact due to Substantial New Information</i>	<i>No Significant Impact not Previously Identified in PEIR</i>
12. BIOLOGICAL RESOURCES—Would the project:				
a) Have a substantial adverse effect, either directly or through habitat modifications, on any species identified as a candidate, sensitive, or special-status species in local or regional plans, policies, or regulations, or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

<i>Topics:</i>	<i>Significant Impact Peculiar to Project or Project Site</i>	<i>Significant Impact not Identified in PEIR</i>	<i>Significant Impact due to Substantial New Information</i>	<i>No Significant Impact not Previously Identified in PEIR</i>
b) Have a substantial adverse effect on any riparian habitat or other sensitive natural community identified in local or regional plans, policies, regulations or by the California Department of Fish and Game or U.S. Fish and Wildlife Service?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Have a substantial adverse effect on federally protected wetlands as defined by Section 404 of the Clean Water Act (including, but not limited to, marsh, vernal pool, coastal, etc.) through direct removal, filling, hydrological interruption, or other means?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Interfere substantially with the movement of any native resident or migratory fish or wildlife species or with established native resident or migratory wildlife corridors, or impede the use of native wildlife nursery sites?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Conflict with any local policies or ordinances protecting biological resources, such as a tree preservation policy or ordinance?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) Conflict with the provisions of an adopted Habitat Conservation Plan, Natural Community Conservation Plan, or other approved local, regional, or state habitat conservation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

As discussed in the Eastern Neighborhoods PEIR, the Eastern Neighborhoods Plan area is in a developed urban environment that does not provide native natural habitat for any rare or endangered plant or animal species. There are no riparian corridors, estuaries, marshes, or wetlands in the Plan Area that could be affected by the development anticipated under the Area Plan. In addition, development envisioned under the Eastern Neighborhoods Area Plan would not substantially interfere with the movement of any resident or migratory wildlife species. For these reasons, the PEIR concluded that implementation of the Area Plan would not result in significant impacts on biological resources, and no mitigation measures were identified.

The project site is located within Showplace Square/Potrero Hill Plan area of the Eastern Neighborhoods Area Plan and therefore, does not support habitat for any candidate, sensitive or special status species. As such, implementation of the proposed project would not result in significant impacts to biological resources not identified in the Eastern Neighborhoods PEIR.

<i>Topics:</i>	<i>Significant Impact Peculiar to Project or Project Site</i>	<i>Significant Impact not Identified in PEIR</i>	<i>Significant Impact due to Substantial New Information</i>	<i>No Significant Impact not Previously Identified in PEIR</i>
13. GEOLOGY AND SOILS—Would the project:				
a) Expose people or structures to potential substantial adverse effects, including the risk of loss, injury, or death involving:	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Topics:	Significant Impact Peculiar to Project or Project Site	Significant Impact not Identified in PEIR	Significant Impact due to Substantial New Information	No Significant Impact not Previously Identified in PEIR
i) Rupture of a known earthquake fault, as delineated on the most recent Alquist-Priolo Earthquake Fault Zoning Map issued by the State Geologist for the area or based on other substantial evidence of a known fault? (Refer to Division of Mines and Geology Special Publication 42.)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
ii) Strong seismic ground shaking?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
iii) Seismic-related ground failure, including liquefaction?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
iv) Landslides?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Result in substantial soil erosion or the loss of topsoil?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Be located on geologic unit or soil that is unstable, or that would become unstable as a result of the project, and potentially result in on- or off-site landslide, lateral spreading, subsidence, liquefaction, or collapse?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Be located on expansive soil, as defined in Table 18-1-B of the Uniform Building Code, creating substantial risks to life or property?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Have soils incapable of adequately supporting the use of septic tanks or alternative wastewater disposal systems where sewers are not available for the disposal of wastewater?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) Change substantially the topography or any unique geologic or physical features of the site?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

The Eastern Neighborhoods PEIR concluded that implementation of the Plan would indirectly increase the population that would be subject to an earthquake, including seismically induced ground-shaking, liquefaction, and landslides. The PEIR also noted that new development is generally safer than comparable older development due to improvements in building codes and construction techniques. Compliance with applicable codes and recommendations made in project-specific geotechnical analyses would not eliminate earthquake risks, but would reduce them to an acceptable level, given the seismically active characteristics of the Bay Area. Thus, the PEIR concluded that implementation of the Plan would not result in significant impacts with regard to geology, and no mitigation measures were identified in the Eastern Neighborhoods PEIR.

A geotechnical investigation was prepared for the proposed project.³⁹ The investigation included a subsurface investigation and reconnaissance of the project site and vicinity. The report determined that due to the presence of heterogeneous soil and weak marsh deposits on the project site the proposed building would need to be supported by a mat or pier/pile foundation. Installation of either foundation system would require use of temporary slopes and tieback anchors to support retaining walls and shoring. The project site is located within a liquefaction hazard zone, and the report concludes that the potential for

³⁹ H. Allen Gruen, *Geotechnical Investigation, Planned Development at 88 Arkansas and 1530 17th Streets, San Francisco, California*, February 21, 2015. This document is available for review at the San Francisco Planning Department, 1650 Mission Street, Suite 400 as part of Case File No. 2015-000453ENV.

damage from surface ruptures from faults, land sliding, liquefaction, or lateral spreading is relatively low and the proposed foundation systems would adequately address potential risks. The report concluded that the project site is appropriate for construction of the proposed project provided the project sponsor implements the recommendations provided in the report.

The project is required to conform to the San Francisco Building Code, which ensures the safety of all new construction in the City. DBI will review the project-specific geotechnical report during its review of the building permit for the project. In addition, DBI may require additional site specific soils report(s) through the building permit application process, as needed. The DBI requirement for a geotechnical report and review of the building permit application pursuant to DBI's implementation of the Building Code would ensure that the proposed project would have no significant impacts related to soils, seismic or other geological hazards.

In light of the above, the proposed project would not result in a significant effect related to seismic and geologic hazards. Therefore, the proposed project would not result in significant impacts related to geology and soils that were not identified in the Eastern Neighborhoods PEIR, and no mitigation measures are necessary.

<i>Topics:</i>	<i>Significant Impact Peculiar to Project or Project Site</i>	<i>Significant Impact not Identified in PEIR</i>	<i>Significant Impact due to Substantial New Information</i>	<i>No Significant Impact not Previously Identified in PEIR</i>
14. HYDROLOGY AND WATER QUALITY—Would the project:				
a) Violate any water quality standards or waste discharge requirements?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Substantially deplete groundwater supplies or interfere substantially with groundwater recharge such that there would be a net deficit in aquifer volume or a lowering of the local groundwater table level (e.g., the production rate of pre-existing nearby wells would drop to a level which would not support existing land uses or planned uses for which permits have been granted)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, in a manner that would result in substantial erosion or siltation on- or off-site?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Substantially alter the existing drainage pattern of the site or area, including through the alteration of the course of a stream or river, or substantially increase the rate or amount of surface runoff in a manner that would result in flooding on- or off-site?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e) Create or contribute runoff water which would exceed the capacity of existing or planned stormwater drainage systems or provide substantial additional sources of polluted runoff?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) Otherwise substantially degrade water quality?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
g) Place housing within a 100-year flood hazard area as mapped on a federal Flood Hazard Boundary or Flood Insurance Rate Map or other authoritative flood hazard delineation map?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

<i>Topics:</i>	<i>Significant Impact Peculiar to Project or Project Site</i>	<i>Significant Impact not Identified in PEIR</i>	<i>Significant Impact due to Substantial New Information</i>	<i>No Significant Impact not Previously Identified in PEIR</i>
h) Place within a 100-year flood hazard area structures that would impede or redirect flood flows?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
i) Expose people or structures to a significant risk of loss, injury or death involving flooding, including flooding as a result of the failure of a levee or dam?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
j) Expose people or structures to a significant risk of loss, injury or death involving inundation by seiche, tsunami, or mudflow?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

The Eastern Neighborhoods PEIR determined that the anticipated increase in population would not result in a significant impact on hydrology and water quality, including the combined sewer system and the potential for combined sewer outflows. No mitigation measures were identified in the PEIR.

The approximately 30,000-sf project site is fully developed with impervious surfaces consisting of two buildings, a surface parking lot, and minimal streetscaping. While the proposed development would be constructed over the entire footprint of the project site, project features, including two ground-floor, landscaped courtyards, flow-through planters, a landscaped bulb out, and approximately five additional street trees, would divert stormwater from the wastewater system. As a result, the proposed project would increase impervious surface cover and would not increase stormwater runoff.

Therefore, the proposed project would not result in any significant impacts related to hydrology and water quality that were not identified in the Eastern Neighborhoods PEIR.

<i>Topics:</i>	<i>Significant Impact Peculiar to Project or Project Site</i>	<i>Significant Impact not Identified in PEIR</i>	<i>Significant Impact due to Substantial New Information</i>	<i>No Significant Impact not Previously Identified in PEIR</i>
15. HAZARDS AND HAZARDOUS MATERIALS— Would the project:				
a) Create a significant hazard to the public or the environment through the routine transport, use, or disposal of hazardous materials?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Create a significant hazard to the public or the environment through reasonably foreseeable upset and accident conditions involving the release of hazardous materials into the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Emit hazardous emissions or handle hazardous or acutely hazardous materials, substances, or waste within one-quarter mile of an existing or proposed school?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Be located on a site which is included on a list of hazardous materials sites compiled pursuant to Government Code Section 65962.5 and, as a result, would it create a significant hazard to the public or the environment?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

<i>Topics:</i>	<i>Significant Impact Peculiar to Project or Project Site</i>	<i>Significant Impact not Identified in PEIR</i>	<i>Significant Impact due to Substantial New Information</i>	<i>No Significant Impact not Previously Identified in PEIR</i>
e) For a project located within an airport land use plan or, where such a plan has not been adopted, within two miles of a public airport or public use airport, would the project result in a safety hazard for people residing or working in the project area?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f) For a project within the vicinity of a private airstrip, would the project result in a safety hazard for people residing or working in the project area?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
g) Impair implementation of or physically interfere with an adopted emergency response plan or emergency evacuation plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
h) Expose people or structures to a significant risk of loss, injury, or death involving fires?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

The Eastern Neighborhoods PEIR noted that implementation of any of the proposed project's rezoning options would encourage construction of new development within the project area. The PEIR found that there is a high potential to encounter hazardous materials during construction activities in many parts of the project area because of the presence of 1906 earthquake fill, previous and current land uses associated with the use of hazardous materials, and known or suspected hazardous materials cleanup cases. However, the PEIR found that existing regulations for facility closure, Under Storage Tank (UST) closure, and investigation and cleanup of soil and groundwater would ensure implementation of measures to protect workers and the community from exposure to hazardous materials during construction.

Hazardous Building Materials

The Eastern Neighborhoods PEIR determined that future development in the Plan Area may involve demolition or renovation of existing structures containing hazardous building materials. Some building materials commonly used in older buildings could present a public health risk if disturbed during an accident or during demolition or renovation of an existing building. Hazardous building materials addressed in the PEIR include asbestos, electrical equipment such as transformers and fluorescent light ballasts that contain PCBs or di (2 ethylhexyl) phthalate (DEHP), fluorescent lights containing mercury vapors, and lead-based paints. Asbestos and lead based paint may also present a health risk to existing building occupants if they are in a deteriorated condition. If removed during demolition of a building, these materials would also require special disposal procedures. The Eastern Neighborhoods PEIR identified a significant impact associated with hazardous building materials including PCBs, DEHP, and mercury and determined that that Mitigation Measure L-1: Hazardous Building Materials would reduce effects to a less-than-significant level. Because the proposed development includes demolition of existing buildings constructed in 1906 (Lot 002) and 1923 (Lot 002A), hazardous building materials may be present in these structures. Therefore, Mitigation Measure L-1 would apply to the proposed project. The project sponsor has agreed to implement Mitigation Measure L-1 as Project Mitigation Measure 5 (see full text of Project Mitigation Measure 5 in the Mitigation Measures section below).

Soil and Groundwater Contamination

Since certification of the PEIR, Article 22A of the Health Code, also known as the Maher Ordinance, was expanded to include properties throughout the City where there is potential to encounter hazardous

materials, primarily industrial zoning districts, sites with industrial uses or underground storage tanks, sites with historic bay fill, and sites in close proximity to freeways or underground storage tanks. The over-arching goal of the Maher Ordinance is to protect public health and safety by requiring appropriate handling, treatment, disposal and when necessary, remediation of contaminated soils that are encountered in the building construction process. Projects that disturb 50 cubic yards or more of soil that are located on sites with potentially hazardous soil or groundwater within Eastern Neighborhoods Plan area are subject to this ordinance.

The project site was formerly occupied by a Hexol Disinfectant factory and is located within 100 feet of properties that have closed or active USTs. The proposed project would excavate approximately 18,000 cubic yards of soil, to a maximum depth of approximately 20 feet below ground, in order to construct a one-level subterranean parking garage. Therefore, the project is subject to the Maher Ordinance, which is administered and overseen by DPH. The Maher Ordinance requires the project sponsor to retain the services of a qualified professional to prepare a Phase I Environmental Site Assessment (ESA) that meets the requirements of Health Code Section 22.A.6. The Phase I ESA would determine the potential for site contamination and level of exposure risk associated with the project. Based on that information, the project sponsor may be required to conduct soil and/or groundwater sampling and analysis. Where such analysis reveals the presence of hazardous substances in excess of state or federal standards, the project sponsor is required to submit a site mitigation plan (SMP) to the DPH or other appropriate state or federal agency(ies), and to remediate any site contamination in accordance with an approved SMP prior to the issuance of any building permit.

In compliance with the Maher Ordinance, the project sponsor has submitted a Maher Application to DPH⁴⁰ and retained the services of a consultant to conduct a Phase I ESA for the property at 88 Arkansas Street/1500 17th Street (Lot 002).⁴¹ An addendum to the Phase I ESA was prepared to address the property at 1530 17th Street (Lot 002A).⁴² The site investigation found no evidence of mishandled hazardous substances or petroleum products on the subject properties or adjacent properties. Soil samples taken at the project site contained elevated levels of lead, which is likely due to the presence of earthquake fill used to raise the grade on the project site. The report therefore recommends that the off-site disposal of proposed excavated soil be conducted in accordance with state and federal regulations. No surface storage tanks or USTs were found on the project site. USTs and Leaking Underground Storage Tanks (LUSTs) were once located on properties within an eighth of a mile from the site. However, these cases have been closed in accordance with the San Francisco Local Oversight Program (LOP) and the State Water Resources Control Board (RWQCB) or are currently under regulatory investigation. In addition, due to their distance and downgrade location from the project site it is unlikely the UST and LUST sites could impact the project site. Thus, the report concludes that the UST and LUST cases do not constitute an environmental concern. Based on the results of the Phase I ESA investigation, the report concludes there is

⁴⁰ Julie Heinzler, Martin Building Company, Project Sponsor, *Maher Program Application*, submitted March 12, 2015. This document is available for review at the San Francisco Planning Department, 1650 Mission Street, Suite 400 as part of Case File No. 2015-000453ENV.

⁴¹ PII Environmental, *Phase I Environmental Site Assessment, 88 Arkansas Street, San Francisco, California*, August 7, 2014. This document is available for review at the San Francisco Planning Department, 1650 Mission Street, Suite 400 as part of Case File No. 2015-000453ENV.

⁴² PII Environmental, *Phase I Environmental Site Assessment Addendum Report, 1530 17th Street, San Francisco, California*, March 20, 2015. This document is available for review at the San Francisco Planning Department, 1650 Mission Street, Suite 400 as part of Case File No. 2015-000453ENV.

no indication of a Recognized Environmental Condition,⁴³ Historical Recognized Environmental Condition,⁴⁴ or Controlled Recognized Environmental Condition⁴⁵ in connection with the subject properties, nor is there information to indicate that additional investigation is warranted.

Nevertheless, the proposed project would be required to remediate potential soil and groundwater contamination described above in accordance with Article 22A of the Health Code. DPH reviewed the Maher Program materials submitted for the project at 88 Arkansas Street, including the geotechnical report, Phase I ESA, and Limited Soils Characterization Investigation, and determined that the following items and procedures would be required:⁴⁶ (1) a Comprehensive Health and Safety Plan (HSP) to address all of the proposed tasks at the site; (2) a Dust Control Plan (DCP) and Air Monitoring Plan (AMP) that includes protocols for air dust surveillance; (3) a comprehensive site history that would include previous activities on the project site; (4) a Site Sampling Work Plan that would characterize subsurface soils for off-site transport and disposal; (5) a Site Mitigation Plan (SMP), which would provide soil management measures for soil excavation and grading and the off-site transport and disposal of impacted soils; and (6) a Final Disclosure Report that would summarize all activities on the site.

The project sponsor has prepared and submitted a SMP to DPH. A HSP, DCP, and an air monitoring program proposal were included as components of the SMP. The HSP includes procedures for the proper handling of soil, dust monitoring, fencing, tarping, the operation of excavation pits, and provisions for stopping work, if required, during construction activities. Should hazardous soil be located on the project site, it must be either directly loaded for off-site disposal or added to the hazardous waste stockpile and covered for future disposal. Excavated soil disposed of off-site must be characterized for disposal in accordance to the requirements established by the disposal facility. All off-site materials transport will be conducted in accordance with local, state, and federal regulations, and applicable licensing requirements. In addition to the HSP, the DCP would further reduce potential exposure during excavation through implementation of measures to protect construction workers and the public during construction activities that create dust or disturb/expose more than 10 cubic yards or 500 square feet of soil. These measures may include dust control procedures and work stoppage provisions. The air monitoring program would monitor airborne lead, asbestos, and particulate matter (PM₁₀) levels.

The project sponsor would be required to submit any outstanding materials and comply with all DPH requirements as prescribed by the Health Code Article 22A. Therefore, the proposed project would not result in any significant impacts related to hazardous materials that were not identified in the Eastern Neighborhoods PEIR.

⁴³ Recognized Environmental Condition: The presence or likely presence of any hazardous substances or petroleum products on a property under conditions that indicate an existing release, a past release, or a material release of any hazardous substances or petroleum products into structures on the property or into the ground, ground water, or surface water or the property.

⁴⁴ Historical Recognized Environmental Condition: The past release of any hazardous substances or petroleum products that has occurred in connection with the subject property that has been remediated and given regulatory closure with no restrictions on land use.

⁴⁵ Controlled Recognized Environmental Condition: The past release of any hazardous substances or petroleum products that has occurred in connection with the subject property which has been addressed to the satisfaction of the appropriate regulatory authority, but is subject to some form of control or restriction.

⁴⁶ San Francisco Department of Public Health. *88 Arkansas Street, San Francisco, Environmental Health Branch-Site Assessment and Mitigation Case Number 1233*, January 5, 2015. This document is available for review at the San Francisco Planning Department, 1650 Mission Street, Suite 400 as part of Case File No. 2015-000453ENV.

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16. MINERAL AND ENERGY RESOURCES— Would the project:				
a) Result in the loss of availability of a known mineral resource that would be of value to the region and the residents of the state?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Result in the loss of availability of a locally important mineral resource recovery site delineated on a local general plan, specific plan or other land use plan?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Encourage activities which result in the use of large amounts of fuel, water, or energy, or use these in a wasteful manner?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

The Eastern Neighborhoods PEIR determined that the Area Plan would facilitate the construction of both new residential units and commercial buildings. Development of these uses would not result in use of large amounts of fuel, water, or energy in a wasteful manner or in the context of energy use throughout the City and region. The energy demand for individual buildings would be typical for such projects and would meet, or exceed, current state and local codes and standards concerning energy consumption, including Title 24 of the California Code of Regulations enforced by DBI. The Plan Area does not include any natural resources routinely extracted and the rezoning does not result in any natural resource extraction programs. Therefore, the Eastern Neighborhoods PEIR concluded that implementation of the Area Plan would not result in a significant impact on mineral and energy resources. No mitigation measures were identified in the PEIR.

As the proposed project is within the development projected under the Eastern Neighborhoods Rezoning and Area Plans, there would be no additional impacts on mineral and energy resources beyond those analyzed in the Eastern Neighborhoods PEIR.

<i>Topics:</i>	<i>Significant Impact Peculiar to Project or Project Site</i>	<i>Significant Impact not Identified in PEIR</i>	<i>Significant Impact due to Substantial New Information</i>	<i>No Significant Impact not Previously Identified in PEIR</i>
17. AGRICULTURE AND FOREST RESOURCES:—Would the project:				
a) Convert Prime Farmland, Unique Farmland, or Farmland of Statewide Importance, as shown on the maps prepared pursuant to the Farmland Mapping and Monitoring Program of the California Resources Agency, to non-agricultural use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b) Conflict with existing zoning for agricultural use, or a Williamson Act contract?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c) Conflict with existing zoning for, or cause rezoning of, forest land (as defined in Public Resources Code Section 12220(g)) or timberland (as defined by Public Resources Code Section 4526)?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d) Result in the loss of forest land or conversion of forest land to non-forest use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

<i>Topics:</i>	<i>Significant Impact Peculiar to Project or Project Site</i>	<i>Significant Impact not Identified in PEIR</i>	<i>Significant Impact due to Substantial New Information</i>	<i>No Significant Impact not Previously Identified in PEIR</i>
e) Involve other changes in the existing environment which, due to their location or nature, could result in conversion of Farmland to non-agricultural use or forest land to non-forest use?	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

The Eastern Neighborhoods PEIR determined that no agricultural resources exist in the Area Plan; therefore the rezoning and community plans would have no effect on agricultural resources. No mitigation measures were identified in the PEIR. The Eastern Neighborhoods PEIR did not analyze the effects on forest resources.

As the proposed project is within the development projected under the Eastern Neighborhoods Rezoning and Area Plans, there would be no additional impacts on agriculture and forest resources beyond those analyzed in the Eastern Neighborhoods PEIR.

MITIGATION MEASURES

Project Mitigation Measure 1: Properties with No Previous Studies (Implementing Eastern Neighborhoods PEIR Mitigation Measure J-2)

The following mitigation measure is required to avoid any potential adverse effect from the proposed project on accidentally discovered buried or submerged historical resources as defined in *CEQA Guidelines* Section 15064.5(a) and (c). The project sponsor shall distribute the Planning Department archeological resource "ALERT" sheet to the project prime contractor; to any project subcontractor (including demolition, excavation, grading, foundation, pile driving, etc. firms); or utilities firm involved in soils disturbing activities within the project site. Prior to any soils disturbing activities being undertaken each contractor is responsible for ensuring that the "ALERT" sheet is circulated to all field personnel including, machine operators, field crew, pile drivers, supervisory personnel, etc. The project sponsor shall provide the Environmental Review Officer (ERO) with a signed affidavit from the responsible parties (prime contractor, subcontractor(s), and utilities firm) to the ERO confirming that all field personnel have received copies of the Alert Sheet.

Should any indication of an archeological resource be encountered during any soils disturbing activity of the project, the project Head Foreman and/or project sponsor shall immediately notify the ERO and shall immediately suspend any soils disturbing activities in the vicinity of the discovery until the ERO has determined what additional measures should be undertaken.

If the ERO determines that an archeological resource may be present within the project site, the project sponsor shall retain the services of an archaeological consultant from the pool of qualified archaeological consultants maintained by the Planning Department archaeologist. The archeological consultant shall advise the ERO as to whether the discovery is an archeological resource, retains sufficient integrity, and is of potential scientific/historical/cultural significance. If an archeological resource is present, the archeological consultant shall identify and evaluate the archeological resource. The archeological consultant shall make a recommendation as to what action, if any, is warranted. Based on this information, the ERO may require, if warranted, specific additional measures to be implemented by the project sponsor.

Measures might include: preservation in situ of the archeological resource; an archaeological monitoring program; or an archeological testing program. If an archeological monitoring program or archeological testing program is required, it shall be consistent with the Environmental Planning (EP) division guidelines for such programs. The ERO may also require that the project sponsor immediately implement a site security program if the archeological resource is at risk from vandalism, looting, or other damaging actions.

The project archeological consultant shall submit a Final Archeological Resources Report (FARR) to the ERO that evaluates the historical significance of any discovered archeological resource and describing the archeological and historical research methods employed in the archeological monitoring/data recovery program(s) undertaken. Information that may put at risk any archeological resource shall be provided in a separate removable insert within the final report.

Copies of the Draft FARR shall be sent to the ERO for review and approval. Once approved by the ERO, copies of the FARR shall be distributed as follows: California Archaeological Site Survey Northwest Information Center (NWIC) shall receive one (1) copy and the ERO shall receive a copy of the transmittal of the FARR to the NWIC. The Environmental Planning division of the Planning Department shall receive one bound copy, one unbound copy, and one unlocked, searchable PDF copy on CD; three copies of the FARR along with copies of any formal site recordation forms (CA DPR 523 series) and/or documentation for nomination to the National Register of Historic Places/California Register of Historical Resources. In instances of high public interest or interpretive value, the ERO may require a different final report content, format, and distribution than that presented above.

Project Mitigation Measure 2: Construction Noise (Implementing Eastern Neighborhoods PEIR Mitigation Measure F-2)

The project sponsor shall develop a set of site-specific noise attenuation measures under the supervision of a qualified acoustical consultant. Prior to commencing construction, a plan for such measures shall be submitted to the Department of Building Inspection to ensure that maximum feasible noise attenuation will be achieved. These attenuation measures shall include as many of the following control strategies as feasible:

- Erect temporary plywood noise barriers around the construction site, particularly where a site adjoins noise-sensitive uses;
- Utilize noise control blankets on the building structure as the building is erected to reduce noise emission from the site;
- Evaluate the feasibility of noise control at the receivers by temporarily improving the noise reduction capability of adjacent buildings housing sensitive uses;
- Monitor the effectiveness of noise attenuation measures by taking noise measurements; and
- Post signs on-site pertaining to permitted construction days and hours and complaint procedures and who to notify in the event of a problem, with telephone numbers listed.

Project Measure 3: Siting of Noise-Sensitive Uses (Implementing Eastern Neighborhoods PEIR Mitigation Measure F-4)

To reduce potential conflicts between existing noise-generating uses and new sensitive receptors, for new development including noise-sensitive uses, the project sponsor was required to provide an analysis that included, at a minimum, a site survey to identify potential noise-generating uses within 900 feet of, and that have a direct line-of-sight to, the project site, and including at least one 24-hour noise measurement (with maximum noise level readings taken at least every 15 minutes), prior to the first project approval action. The analysis which was prepared by persons qualified in acoustical analysis and/or engineering, demonstrated with reasonable certainty that Title 24 standards, where applicable, can be met, and that there are no particular circumstances about the proposed project site that appear to warrant heightened concern about noise levels in the vicinity. The report concluded that if its recommendations are incorporated into the design and construction of the proposed building, the project would achieve compliance with Title 24. Recommendations include the following: (1) on the ground floor, exterior windows and doors should have an STC rating of 32 along Arkansas Street, an STC rating of 35 along 17th Street, and an STC rating of 28 for all other locations; (2) on the second floor, exterior windows and doors should have an STC rating of 32 along Arkansas Street, an STC rating of 35 on the northeast corner of the project site, and an STC rating of 28 for all other locations; (3) on the third through fifth floors exterior windows and doors should have an STC rating of 32 along Arkansas Street, an STC rating of 35 along 17th

Street, an STC rating of 32 on the eastern half of the northern façade of the building, an STC rating of 30 on the a portion of the western half of the northern façade of the building, an STC rating of 38 on the southwest and southeast corners of the building, an STC rating of 35 on the northwest and northeast corners of the building, and an STC rating of 28 for all other locations. Overall, the proposed project would achieve compliance with the Title 24 standard of DNL 45 dBA for interior noise by installing exterior windows and doors with STC ratings between 28 and 38, depending on the location. The STC ratings shall be incorporated into the final construction drawings.

Project Measure 4: Open Space in Noisy Environments (Implementing Eastern Neighborhoods PEIR Mitigation Measure F-6)

The project sponsor shall ensure that open space required under the Planning Code for such uses be protected, to the maximum feasible extent, from existing ambient noise levels that could prove annoying or disruptive to users of the open space. Implementation of this measure could involve, among other things, site design that uses the building itself to shield on-site open space from the greatest noise sources, construction of noise barriers between noise sources and open space, and appropriate use of both common and private open space in multi-family dwellings, and implementation would also be undertaken consistent with other principles of urban design. This mitigation measure has been complied with as part of this environmental review process. No further actions are required to comply with Mitigation Measure F-6 of the Eastern Neighborhoods PEIR.

Project Mitigation Measure 5: Hazardous Building Materials (Implementation of Eastern Neighborhoods PEIR Mitigation Measure L-1)

The project sponsor shall ensure that any equipment containing PCBs or DEPH, such as fluorescent light ballasts, are removed and properly disposed of according to applicable federal, state, and local laws prior to the start of renovation, and that any fluorescent light tubes, which could contain mercury, are similarly removed and properly disposed of. Any other hazardous materials identified, either before or during work, shall be abated according to applicable federal, state, and local laws.

IMPROVEMENT MEASURES

Project Improvement Measure 1: Implementation of Transportation Demand Management Strategies

The project sponsor would implement a Transportation Demand Management (TDM) program to encourage the use of alternative forms of transportation for trips to and from the project vicinity, such as transit, bicycle, rideshare, and walking. Components of the improvement measure are as follows:

IM-1a TDM Coordinator: The project sponsor would identify a TDM Coordinator for the project site who would be responsible for the implementation and ongoing operation of all other TDM measures included in the proposed Project. The TDM Coordinator should be the single point of contact for all transportation-related questions from residents and City staff. The project sponsor may use an existing transportation management association (e.g., the Transportation Management Association of San Francisco) to supply the TDM Coordinator, or the TDM Coordinator may be an existing staff member (e.g., property manager). In either case, the TDM

Coordinator does not have to work full-time at the Project site. The TDM coordinator would be the single point of contact for all transportation-related questions from building occupants and City staff. The TDM Coordinator should provide TDM training to other building staff about the transportation amenities and options available in the project vicinity.

- IM-1b Transportation and Trip Planning Information Move-In Packet:** Provide a transportation insert for the move-in packet that includes information on transit service (local and regional, schedules and fares), information on where transit passes may be purchased, information on the 511 Regional Rideshare Program, and nearby bike and car share programs, and information on where to find additional web-based alternative transportation materials. The move-in packet should be continuously updated as transportation options change, and the packet should be provided to each new building occupant. Muni maps as well as San Francisco Bicycle and Pedestrian maps should be provided upon request.
- IM-1c City Access to Data Collection:** As part of an ongoing effort to quantify the efficacy of TDM measures, City staff may need to access the project site (including the garage) to perform trip counts, and/or intercept surveys and/or other types of data collection. All on-site activities shall be coordinated through the TDM Coordinator. The project sponsor assures future access to the site by City staff. Providing access to existing developments for data collection purposes is also encouraged.
- IM-1d Bicycle Parking:** Increase the number of on-site secured bicycle parking beyond Planning Code requirements and/or provide additional bicycle facilities in the public right-of-way adjacent to or within a quarter mile of the project site (e.g., sidewalks, on-street parking spaces).
- IM-1e Car-Share Parking:** Provide optional car share spaces as described in Planning Code Section 166(g).
- IM-1f Transit Pass:** Offer free or subsidized Muni passes to tenants. For example, the project sponsor may offer a 50 percent subsidy for one Muni monthly pass for new residents (one per household), and employees for up to one year. The recipient would be responsible for the remainder of the costs associated with the Muni monthly pass.

Project Improvement Measure 2: Pedestrian Safety Improvements

Install audible and visual warning devices to alert pedestrians of vehicles exiting the parking garage.

Project Improvement Measure 3: On-Street Loading Management

- IM-3a** Schedule and coordinate loading activities through building management to ensure that trucks can be accommodated in the curbside loading spaces. All regularly scheduled activities requiring use of the loading space (e.g., building supply deliveries) should be coordinated directly with building management.
- IM-3b** Trucks should be discouraged from parking illegally or obstructing vehicle, transit, bicycle, or pedestrian traffic flow along any of the streets adjacent to the project site.

Project Improvement Measure 4: Construction-Related Traffic Management

- IM-4a** Limit hours of construction-related traffic, including, but not limited to, truck movements, to avoid the weekday AM and PM peak hours (7:00 a.m. to 9:00 a.m. and 4:00 p.m. to 6:00 p.m.; or other times, if approved by SFMTA).
- IM4b** Construction contractor(s) should coordinate construction activities with other potential projects that may be constructed in the vicinity of the project site.

Project Improvement Measure 5: Off-Street Parking Traffic Enhancements

- IM-5a** Install a traffic signal on both the inside and outside of the garage opening to indicate the presence of approaching vehicles from the opposite direction. The traffic signal would operate on sensors/detectors, and alternate with two beacons, with one indicating a solid red signal or marked with "STOP" and another indicating a solid green signal or marked with "GO." The signals would be installed facing outward at the garage opening outside of the security gate, and at the interior of the garage facing inward into the garage ramp, and facing inward at the interior of the garage on the basement level, at the entry to the garage ramp.
- IM-5b** The owner/operator of any off-street parking facility, as determined by the Planning Director, with more than 20 parking spaces (excluding loading and car-share spaces) could ensure that recurring vehicle queues⁴⁷ do not occur on the public right of way. If a recurring queue occurs, the parking facility's owner/operator could implement abatement measures as needed to abate the queue.

Suggested abatement methods include but are not limited to the following: redesign of facility layout to improve vehicle circulation and/or on-site queue capacity; employment of parking attendants; installation of LOT FULL signs with active management by parking attendants; use of valet parking or other space-efficient parking techniques; use of off-site parking facilities or shared parking with nearby uses; use of parking occupancy sensors and signage directing drivers to available spaces; travel demand management strategies such those discussed in IM-1, customer shuttles or delivery services; and/or parking demand management strategies such as parking time limits, paid parking or validated parking.

If the Planning Director, or his or her designee, suspects that a recurring queue is present, the Department may notify the property owner in writing. Upon request, the owner/operator could hire a qualified transportation consultant to evaluate the conditions at the site for no less than seven days. The consultant could prepare a monitoring report to be submitted to the Planning Department for review. If the Planning Department determines that a recurring queue does exist, the facility owner/operator could abate the queue.

⁴⁷ Vehicle Queue: When one or more vehicles blocking any portion of a public street, alley or sidewalk for a consecutive period of three minutes or longer on a daily or weekly basis.



SAN FRANCISCO
PLANNING
DEPARTMENT

AFFIDAVIT FOR Compliance with the Inclusionary Affordable Housing Program

Date: January 11, 2013

Planning Department
1650 Mission Street
Suite 400
San Francisco, CA
94103-9425

T: 415.558.6378
F: 415.558.6409

To: Applicants subject to Planning Code Section 415: Inclusionary
Affordable Housing Program

From: San Francisco Planning Department

Re: Compliance with the Inclusionary Affordable Housing Program

All projects that involve ten or more new dwelling units must participate in the *Inclusionary Affordable Housing Program* contained in Section 415 of the Planning Code. Every project subject to Section 415 must pay an Affordable Housing Fee that is equivalent to the applicable percentage of the number of units in the principal project, which is 20% of the total number of units proposed (or the applicable percentage if subject to different area plan controls or requirements).

A project may be eligible for an Alternative to the Affordable Housing Fee if the developer chooses to commit to sell the new on- or off-residential units rather than offer them as rental units. Second, the project may be eligible for an Alternative to the Affordable Housing Fee if it has demonstrated to the Planning Department that the affordable units are not subject to the Costa Hawkins Rental Housing Act. All projects that can demonstrate that they are eligible for an alternative to the Affordable Housing Fee must provide the necessary documentation to the Planning Department and the Mayor's Office of Housing. Additional material may be required to determine if a project is eligible to fulfill the Program's requirements through an alternative.

Before the Planning Department and/or Planning Commission can act on the project, this *Affidavit for Compliance with the Inclusionary Affordable Housing Program* must be completed.

¹ California Civil Code Section 1954.50 et.al.

Affidavit for Compliance with the Inclusionary Affordable Housing Program: Planning Code Section 415

January 14, 2016

Date

I, Patrick McNerney, do hereby declare as follows:

- a. The subject property is located at (address and block/lot):

88 Arkansas Street

Address

3953/002 and 002A

Block / Lot

- b. The proposed project at the above address is subject to the Inclusionary Affordable Housing Program, Planning Code Section 415 et seq.

The Planning Case Number and/or Building Permit Number is:

2015-000453

Planning Case Number

201511021443

Building Permit Number

This project requires the following approval:

- ☐ Planning Commission approval (e.g. Conditional Use Authorization, Large Project Authorization)
- ☒ This project is principally permitted.

The Current Planner assigned to my project within the Planning Department is:

Kate Conner

Planner Name

Is this project within the Eastern Neighborhoods Plan Area?

- ☒ Yes (if yes, please indicate Tier) 1
- ☐ No

This project is exempt from the Inclusionary Affordable Housing Program because:

- ☐ This project is 100% affordable.

- c. This project will comply with the Inclusionary Affordable Housing Program by:

- ☐ Payment of the Affordable Housing Fee prior to the first site or building permit issuance (Planning Code Section 415.5).
- ☒ On-site or Off-site Affordable Housing Alternative (Planning Code Sections 415.6 and 416.7).

Affidavit for Compliance with the Inclusionary Affordable Housing Program

d. If the project will comply with the Inclusionary Affordable Housing Program through an **On-site** or **Off-site Affordable Housing Alternative**, please fill out the following regarding how the project is eligible for an alternative and the accompanying unit mix tables on page 4.

☐ **Ownership.** All affordable housing units will be sold as ownership units and will remain as ownership units for the life of the project.

☒ **Rental.** Exemption from Costa Hawkins Rental Housing Act.² The Project Sponsor has demonstrated to the Department that the affordable units are not subject to the Costa Hawkins Rental Housing Act, under the exception provided in Civil Code Sections 1954.50 through one of the following:

☐ Direct financial contribution from a public entity.

☐ Development or density bonus or other public form of assistance.

☒ Development Agreement with the City. The Project Sponsor has entered into or has applied to enter into a Development Agreement with the City and County of San Francisco pursuant to Chapter 56 of the San Francisco Administrative Code and, as part of that Agreement, is receiving a direct financial contribution, development or density bonus, or other form of public assistance.

e. The Project Sponsor acknowledges that failure to sell the affordable units as ownership units or to eliminate the on-site or off-site affordable ownership-only units at any time will require the Project Sponsor to:

(1) Inform the Planning Department and the Mayor's Office of Housing and, if applicable, fill out a new affidavit;

(2) Record a new Notice of Special Restrictions; and

(3) Pay the Affordable Housing Fee plus applicable interest (using the fee schedule in place at the time that the units are converted from ownership to rental units) and any applicable penalties by law.

f. The Project Sponsor must pay the Affordable Housing Fee in full sum to the Development Fee Collection Unit at the Department of Building Inspection for use by the Mayor's Office of Housing prior to the issuance of the first construction document, with an option for the Project Sponsor to defer a portion of the payment to prior to issuance of the first certificate of occupancy upon agreeing to pay a deferral surcharge that would be deposited into the Citywide Affordable Housing Fund in accordance with Section 107A.13.3 of the San Francisco Building Code.

g. I am a duly authorized officer or owner of the subject property.

I declare under penalty of perjury under the laws of the State of California that the foregoing is true and correct.
Executed on this day in:

SAN FRANCISCO
Location

2.10.16
Date

[Signature]
Signature

PATRICK McNEILLY
Name (Print), Title

415-348-4600
Contact Phone Number

cc: Mayor's Office of Housing
Planning Department Case Docket
Historic File, if applicable
Assessor's Office, if applicable

Unit Mix Tables

NUMBER OF ALL UNITS IN PRINCIPAL PROJECT:					
Total Number of Units	SRO	Studios	One-Bedroom Units	Two-Bedroom Units	Three-Bedroom Units
127	0	25	51	41	10

If you selected an On-site or Off-Site Alternative, please fill out the applicable section below:

- ☒ On-site Affordable Housing Alternative (Charter Section 16.110 (g) and Planning Code Section 415.6): calculated at 20% of the unit total.

NUMBER OF AFFORDABLE UNITS TO BE LOCATED ON-SITE					
Total Affordable Units	SRO	Studios	One-Bedroom Units	Two-Bedroom Units	Three-Bedroom Units
25	0	5	10	8	2

- ☐ Off-site Affordable Housing Alternative (Planning Code Section 415.7): calculated at 20% of the unit total.

NUMBER OF AFFORDABLE UNITS TO BE LOCATED OFF-SITE					
Total Affordable Units	SRO	Studios	One-Bedroom Units	Two-Bedroom Units	Three-Bedroom Units

Area of Dwellings in Principal Project (in sq. feet)	Off-Site Project Address	
Area of Dwellings in Off-Site Project (in sq. feet)		
Off-Site Block/Lot(s)	Motion No. (if applicable)	Number of Market-Rate Units in the Off-site Project

- ☐ Combination of payment of a fee, on-site affordable units, or off-site affordable units with the following distribution:

Indicate what percent of each option would be implemented (from 0% to 99%) and the number of on-site and/or off-site below market rate units for rent and/or for sale.

1. Fee _____ % of affordable housing requirement.

2. On-Site _____ % of affordable housing requirement.

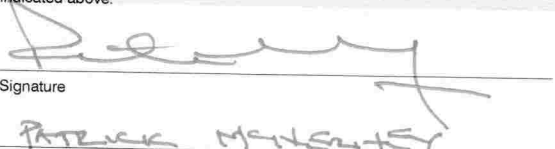
NUMBER OF AFFORDABLE UNITS TO BE LOCATED ON-SITE					
Total Affordable Units	SRO	Studios	One-Bedroom Units	Two-Bedroom Units	Three-Bedroom Units

3. Off-Site _____ % of affordable housing requirement.

NUMBER OF AFFORDABLE UNITS TO BE LOCATED OFF-SITE					
Total Affordable Units	SRO	Studios	One-Bedroom Units	Two-Bedroom Units	Three-Bedroom Units

Area of Dwellings in Principal Project (in sq. feet)	Off-Site Project Address	
Area of Dwellings in Off-Site Project (in sq. feet)		
Off-Site Block/Lot(s)	Motion No. (if applicable)	Number of Market-Rate Units in the Off-site Project

Affidavit for **Compliance with the Inclusionary Affordable Housing Program**

CONTACT INFORMATION AND DECLARATION OF SPONSOR OF PRINCIPAL PROJECT	CONTACT INFORMATION AND DECLARATION OF SPONSOR OF OFF-SITE PROJECT (IF DIFFERENT)
Company Name 88 Arkansas Affordable, LP	Company Name
Print Name of Contact Person Julie Heinzler	Print Name of Contact Person
Address 14 Mint Plz, 5th Floor	Address
City, State, Zip San Francisco	City, State, Zip
Phone, Fax 415.442.4800	Phone, Fax
Email julie@martinbuilding.com	Email
I hereby declare that the information herein is accurate to the best of my knowledge and that I intend to satisfy the requirements of Planning Code Section 415 as indicated above.	I hereby declare that the information herein is accurate to the best of my knowledge and that I intend to satisfy the requirements of Planning Code Section 415 as indicated above.
 Signature	Signature
PATRICK MANSUETI Name (Print), Title	Name (Print), Title

SUPPLEMENTAL INFORMATION FOR Anti-Discriminatory Housing Policy

1. Owner/Applicant Information

PROPERTY OWNER'S NAME: 88 Arkansas Affordable, LP	
PROPERTY OWNER'S ADDRESS: 14 Mint Plz 5th Floor San Francisco. CA 94103	TELEPHONE: (415) 348.4644 EMAIL: julie@martinbuilding.com
APPLICANT'S NAME: Martin Building Company Same as Above <input type="checkbox"/>	
APPLICANT'S ADDRESS: 14 Mint Plz 5th Floor San Francisco. CA 94103	TELEPHONE: (415) 348.4644 EMAIL: julie@martinbuilding.com
CONTACT FOR PROJECT INFORMATION: Julie Heinzler Same as Above <input checked="" type="checkbox"/>	
ADDRESS:	TELEPHONE: () EMAIL:
COMMUNITY LIAISON FOR PROJECT (PLEASE REPORT CHANGES TO THE ZONING ADMINISTRATOR): Same as Above <input type="checkbox"/>	
ADDRESS:	TELEPHONE: () EMAIL:

2. Location and Project Description

STREET ADDRESS OF PROJECT: 88 Arkansas Street		ZIP CODE: 94107
CROSS STREETS: 17th Street		
ASSESSORS BLOCK/LOT: 3953 / 002 & 002A	ZONING DISTRICT: UMU	HEIGHT/BULK DISTRICT: 48-X
PROJECT TYPE: (Please check all that apply) <input checked="" type="checkbox"/> New Construction <input checked="" type="checkbox"/> Demolition <input type="checkbox"/> Alteration <input type="checkbox"/> Other: _____	EXISTING DWELLING UNITS: 0	PROPOSED DWELLING UNITS: 127 NET INCREASE: 127

Compliance with the Anti-Discriminatory Housing Policy

1. Does the applicant or sponsor, including the applicant or sponsor's parent company, subsidiary, or any other business or entity with an ownership share of at least 30% of the applicant's company, engage in the business of developing real estate, owning properties, or leasing or selling individual dwelling units in States or jurisdictions outside of California? ☐ YES ☒ NO

1a. If yes, in which States? _____

- 1b. If yes, does the applicant or sponsor, as defined above, have policies in individual States that prohibit discrimination based on sexual orientation and gender identity in the sale, lease, or financing of any dwelling units enforced on every property in the State or States where the applicant or sponsor has an ownership or financial interest? ☐ YES ☐ NO

- 1c. If yes, does the applicant or sponsor, as defined above, have a national policy that prohibits discrimination based on sexual orientation and gender identity in the sale, lease, or financing of any dwelling units enforced on every property in the United States where the applicant or sponsor has an ownership or financial interest in property? ☐ YES ☐ NO

If the answer to 1b and/or 1c is yes, please provide a copy of that policy or policies as part of the supplemental information packet to the Planning Department.

Human Rights Commission contact information
Mullane Ahern at (415)252-2514 or mullane.ahern@sfgov.org

Applicant's Affidavit

Under penalty of perjury the following declarations are made:

- a: The undersigned is the owner or authorized agent of the owner of this property.
- b: The information presented is true and correct to the best of my knowledge.
- c: Other information or applications may be required.

Signature: _____



Date: _____

2/19/16

Print name, and indicate whether owner, or authorized agent:

MULLANE AHERN
Owner / Authorized Agent (circle one)

PLANNING DEPARTMENT USE ONLY

PLANNING DEPARTMENT VERIFICATION:

- ☐ Anti-Discriminatory Housing Policy Form is **Complete**
☐ Anti-Discriminatory Housing Policy Form is **Incomplete**

Notification of Incomplete Information made:

To: _____ Date: _____

BUILDING PERMIT NUMBER(S):

DATE FILED:

RECORD NUMBER:

DATE FILED:

VERIFIED BY PLANNER:

Signature: _____ Date: _____

Printed Name: _____ Phone: _____

ROUTED TO HRC:

DATE:

☐ Emailed to: _____



February 18, 2016

Mr. Rodney Fong
Planning Commission President
Planning Department, City and County of San Francisco
1650 Mission Street, Suite 400
San Francisco, CA 94103

**Re: 88 Arkansas Street: Design Summary and Community Outreach Hearing Date:
March 3, 2016**

Dear Commission President Fong,

We are pleased to present 88 Arkansas Street for your consideration at the March 3rd, 2016 Planning Commission hearing.

Design Summary

Located in the lower Potrero Hill district of San Francisco, 88 Arkansas is a mixed-used community that takes its cues from the surrounding neighborhood of low-rise commercial, light industrial and residential buildings. BAR architects recognizes that the project site is in a transitional neighborhood and the building design must fit within the current context while anticipating that the neighborhood will continue to evolve over time and new neighbors will arise. Along with our direction, BAR architects chose a contemporary expression of exposed concrete, steel, aluminum, and glass to harmonize with the predominantly industrial character of the neighborhood, while creating a distinctive structure. The building design embodies the established building pattern north of 17th street, consisting of a large footprint, horizontal in expression, industrial buildings. The best examples in the neighborhood, regardless of building material, contain regular patterns of large windows in a simple configuration. The proposed design for 88 Arkansas reconciles the "bigness" of these buildings with a residential scale consisting of regular twenty-five foot vertical building modules laced with a horizontal expression of projecting slab edges on the top three stories of the structure thus dividing the building into a distinct base and top.

The base of the building is a combination of one and two-story spaces that reflect the pedestrian scale. There is a ground floor commercial / restaurant space designed for the 17th Street frontage. This space will take advantage of southern exposure, the visual amenity and activity of the Jackson Playground to the south. This commercial space will augment the already rich interplay of restaurants along 17th Street. The Arkansas frontage is residential in nature and contains a series of residential stoops fronting an expanded landscape and sidewalk achieved by re-orienting the perpendicular parking to parallel. The residential lobby is a two-story volume between the commercial space at the corner and the residential stoops along Arkansas to the north. The parking garage is located below grade with the vehicular entrance mid-block on Arkansas.

The upper three stories of the building are designed to take advantage of the unique site feature of Jackson Playground and views of Potrero Hill to the south and views to the “city lights” to the northeast. The primary design feature of the upper stories consists of moveable sliding walls in the living areas to allow the inside and outside to blend into one space and encourage building occupants to activate the façade. The overall fenestration pattern is influenced by the rigor and simplicity of the surrounding industrial buildings.

Targeted to achieve LEED Platinum certification, this new 5-story, Type I mixed-use project will provide 127 dwelling units – 20% of which will be below market rate – and include neighborhood-serving retail. Residential amenities include a fitness center with two balconies, bicycle storage room with enough room for each unit to store a bike, two outdoor courtyards, a landscaped roof garden equipped with barbeques, dining and lounge seating, and pet area. The parking garage includes standard spaces as well as car stackers and a car share space.

The interior of the units are based on several other successful and coveted floor plans constructed previously by Martin Building Company, some even in the Eastern Neighborhoods Area. The main feature that most tenants are attracted to is the nested bedrooms that feature large oversized wood doors and large translucent glass panels. The nested bedrooms continue to be successful and comply completely with the California Building Code and City of San Francisco Department of Building Inspection Bulletins and Interpretations. They create the ability to maintain a larger, wider living room at the exterior of the building where most waking time is spent. The bedrooms also create for a more peaceful sleep experience in an urban environment by providing noise separation from the busy streets. Through-wall transfer fans allow air to be regulated from one space to another. It is our strong belief, proven through multiple years and hundreds of units of fully occupied buildings, that these floor plans are successful and in demand for both singles and families.

Community Outreach/Benefits Summary

On March 23rd 2015, the first formal neighborhood meeting was held at the project site. Since then our design and development team has presented to the Development Committee of the Potrero Boosters Neighborhood Association on June 6, 2016 as well as to the full membership on September 29th and November 24th. Their desire for additional three bedroom units has been incorporated into our project.

Given that our project is located immediately across 17th Street from Jackson Playground, we met twice with the Friends of Jackson Playground in August 2015, and continue to have an ongoing line of communication with their main representative. Upon direction from the Supervisor’s office and the community, we will request the Eastern Neighborhood Impact Fees be directly allocated to improvements to Jackson Playground. In addition to the Eastern Neighborhood Impact Fees, 88 Arkansas Street will also generate approximately \$150,000 in fees to the San Francisco Unified School District.

Our team has met with various members of the Potrero Hill community and led tours of other similar projects by Martin Building Company (mainly Potrero Launch at 2235 Third Street). Potrero Launch successfully demonstrates how well nested/inbound bedrooms can function both as great design and as an essential element for a livable, bright family-sized unit.

We have received formal support from San Francisco Housing Action Coalition, Livable City, 2Gen Equity, Friends of Mint Plaza, Del Popolo Pizza, and various members of the community.


Shadows on Jackson Playground

A comprehensive shadow study was conducted and found that 88 Arkansas Street would cast a new shadow on Jackson Playground. The new shading would only represent an increase of 0.003% annual square-foot-hours over current levels of shading. We presented to the San Francisco Recreation and Park Commission on January 21, 2016 and our project was unanimously approved.

Please see attached Exhibit A addressing the recently enacted [February 19th, 2016] *Interim Controls/Additional Standards for LPAs in Showplace Square, Potrero Hill and Central Waterfront Area Plans*.

Thank you for your consideration.

Sincerely,

A handwritten signature in black ink, appearing to read 'Patrick McNerney', with a large circular flourish at the end.

Patrick McNerney
President

Martin Building Company
14 Mint Plaza, 5th Floor
San Francisco, CA 94103

Exhibit A
88 Arkansas Street
Interim Controls
Additional Design Standards for LPAs within the
Showplace Square, Potrero Hill, and Central Waterfront Area Plans

- 1. An awareness of urban patterns, and harmonizes visual and physical relationships between existing buildings, streets, open spaces, natural features, and view corridors;**

The design for 88 Arkansas acknowledges the existing urban pattern of the predominantly large footprint, horizontal in orientation and industrial character of the immediate vicinity by utilizing a contemporary interpretation of industrial form, scale and massing while incorporating residential scale elements and features suggested by the Ground Floor Residential Design Guidelines. The design acknowledges the mixed-use character of 17th street by locating a neighborhood serving retail / restaurant space on that frontage. The re-orientation of the parking on Arkansas allows for an improved public realm in front of the residential entrances on that frontage. The project will implement a bulb-out at the intersection of 17th Street and Arkansas thus reducing pedestrian travel distance across that intersection and improving public safety.

- 2. An awareness of neighborhood scale and materials, and renders building facades with texture, detail, and depth; and**

The chosen materials for 88 Arkansas; exposed concrete, weathering steel façade, steel canopies, aluminum window systems and a variety of glass reference the industrial buildings in the immediate vicinity but are deployed in a way that introduces a human scale to the building. The expressed concrete frame, slab edges and caps on the window system all add depth to the otherwise simple form of the building.

- 3. A modulation of buildings vertically and horizontally, with rooftops and facades designed to be seen from multiple vantage points.**

There are two street frontages for 88 Arkansas. The 17th Street frontage is modulated vertically at the top three stories by a regular provision of wide balconies with moveable glass walls that take advantage of views to the south. These balconies correspond to recesses below that create entry opportunities for the tall retail / restaurant space facing 17th Street. The top three stories are linked horizontally by expressed slab edges that reinforce the predominant scale of buildings in the neighborhood. There is a vertical recessed break between the 17th Street building expression and the Arkansas Street building expression. This break creates a modulation in the cornice line and also serves to denote the residential entry. The Arkansas frontage includes a pattern of regular twenty-five foot vertical module consisting of a base and top. Similar to the 17th Street façade the top contains wide balconies with moveable glass walls. The base contains two stories of residential stoops and balconies facing the improved Arkansas Street public realm. Similar to the 17th Street frontage, the top three stories are linked horizontally by expressed slab edges that reinforce the predominant scale of buildings in the neighborhood. Thus, the regular palette and building form contains two distinct frontages for two distinct streets.

2Gen Equity

February 1, 2016

Re: 2Gen Equity Endorsement of 88 Arkansas Street

To Whom It May Concern:

I would like to take this opportunity to present my enthusiastic support for the proposed mixed-use housing development located at 88 Arkansas Street as the redevelopment of this property will create vital new affordable housing serving local families.

2Gen Equity is a non-profit organization currently working with low-income families in the Potrero Hill neighborhood. Our Mission is to end the cycle of poverty through a two-generation approach, motivating single mothers and their children to successfully overcome the systematic social and financial barriers that perpetuate poverty. We align our efforts with organizations, like Martin Building Company, expanding a network that creates access to critical resources for families, including safe, healthy and affordable housing. With our partner organizations, families and communities, we identify and create clear paths out of poverty to self-sufficiency.

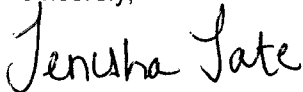
Accordingly, a significant need exists for affordable housing in the immediate Central Waterfront, Dogpatch, Potrero Hill, and Mission Bay neighborhoods. The redevelopment of the 88 Arkansas property will directly address this need. Martin Building Company elected to add extra affordability to the project by voluntarily increasing the on-site affordable units to 20% (25 BMR units) from the 14.4% (18 BMR units) required by the City. The project exceeds the requirements and shows a commitment to creating a mixed income community. 2Gen Equity strongly supports creative means to make housing more available to a wider range of incomes, and creating inclusionary BMR housing is an excellent way to do this.

In addition to the affordability component, the project is thoughtfully designed, and includes nested, inbound bedrooms that are ideal for families due to the increased noise separation from the street. The inbound bedrooms also allow for more generous living spaces -- where most time awake is spent. We believe this is an essential design element, creating an environment suitable for families -- a rare feature as San Francisco's apartments built today are almost exclusively designed for and marketed to people without children.

We congratulate MBC for producing a well-designed and well-situated project that will provide much needed affordable and family-geared housing for the residents of San Francisco.

Please feel free to contact me for any further assistance or information.

Sincerely,



Tenisha Tate
Executive Director
2Gen Equity

POTRERO BOOSTERS
NEIGHBORHOOD ASSOCIATION
SERVING THE HILL SINCE 1926

January 24, 2016

Rodney Fong, Commission President
Dennis Richards, Commission Vice President
Michael J. Antonini, Commissioner
Rich Hillis, Commissioner
Christine D. Johnson, Commissioner
Kathrin Moore, Commissioner
Cindy Wu, Commissioner
San Francisco Planning Commission
1650 Mission Street, Suite 400
San Francisco, CA 94103

Re: 88 Arkansas Street

Via Email

Dear Commissioners:

The Planning Commission is scheduled to consider the approval of a 127 dwelling unit development at 88 Arkansas on February 11, 2016. It is our understanding that the project sponsor (Martin Building) is seeking an exception to the unit mix requirements set forth in the Eastern Neighborhoods Plan for Urban Mixed Use Districts, specifically, an exception to the Planning Code Section 207.6(c)(2) requirement that at least 40% of the units have two or more bedrooms. Because there exists no extraordinary hardships or circumstances related to this property that would require such an exception to allow the development of the property, the Potrero Boosters Neighborhood Association requests that the Commission deny this exception.

Martin Building's Plans Violate the Unit Mix Requirements, Giving Rise to the Need for an Exception.

Martin Building is asking that units with rooms failing to meet the Planning definition of a bedroom be allowed to count towards the unit mix requirement. Section 102.29 of the Planning Code defines a bedroom as a "sleeping room" under the Building Code. As the Planning Department recognized in Zoning Administrator Bulletin No. 7 (the "Bulletin"), the Building Code does not contain a single definition of a "sleeping room;" what constitutes a sleeping room relates to a variety of technical factors specified in both the Building and Housing Codes. In the Bulletin, the Planning Department determined that there were three necessary elements for a bedroom: (1) at least 70 square feet of floor area, exclusive of closets, bathrooms, or similar spaces; (2) one window opening to either a street or rear yard space; and (3) clear designation as a bedroom on submitted plans.

The unit design for 88 Arkansas violates the second of these elements. Martin Building has designated 51 units as multi-bedroom units (or, a bare 40% of the total units). Of those 51 units,

10 are designated as three bedroom units and 41 are designated as two bedroom units. Of those units, 38 have at least designated one bedroom that does not have an exterior window. Even if we look at the data in the light most favorable to Martin Building (that is, if the 10 designated three bedroom units were legislatively two bedroom units), then 28 of the units designated by Martin Building as two bedroom units are actually one bedroom units; instead of the required 40%, only 18% of the units meet the dwelling mix requirements.

There Are No Grounds for the Commission to Grant an Exception.

The Commission should deny the requested exception because Martin Building cannot make the showings necessary for the Commission to grant an exemption. Planning Code Section 329(d)(6) permits the Commission to grant an exception to the Section 207.6 unit mix requirements if the Commission is able to make the findings normally required for a variance under Section 305 of the Planning Code. The Commission must make five separate findings to grant the exception. This is a high standard, as the Commission must make *all* five findings. Each necessary finding is analyzed below:

1. The Commission must find that there are exceptional or extraordinary circumstances applying to the Urban Mixed Use development of 88 Arkansas that do not apply generally to other property in the Eastern Neighborhoods Plan. As the Commission has seen in the approval of other Eastern Neighborhoods Plan projects, 88 Arkansas is not unique. The property is a regularly shaped corner lot. Its neighbors are of the regular type and uses for the district, and with the exception of Jackson Park across the street, all are zoned for Urban Mixed Use. There are no facts present that the 88 Arkansas site presents any hardships different from any other in the neighborhood that requires an exception to the unit mix requirement.
2. The Commission must find that, because of the extraordinary circumstances found on the 88 Arkansas site, literal enforcement of the unit mix requirement would result in practical difficulty or unnecessary hardships to Martin Building. Certainly, because there are no extraordinary circumstances, no such finding can be made. However, it is difficult to understand how a unique site could give rise this finding. Any residential unit will have access to light and air at some point in the unit. The Planning Code simply requires that the bedrooms counting to the unit-mix requirement be at those points. While one may argue that such a requirement is not optimal from a profitability or design standpoint, the requirement does not cause an unnecessary hardship.
3. The Commission must find that the exception is necessary for the preservation of a substantial property right possessed by other Urban Mixed Use parcels in the Eastern Neighborhoods. Over the last several months, the Commission has reviewed in detail the pipeline of projects in the Showplace Square/Potrero Hill and Central Waterfront. To our knowledge, Martin Building is the only developer under the Eastern Neighborhoods Plan to have requested an exception to the unit mix requirement. No other developer has the right to violate the requirements and none has sought to do so. Martin Building is seeking to gain, and not preserve, a right via their request for an exception to the unit mix requirements.
4. The Planning Commission must find that the granting of the exception will not be materially detrimental to the public welfare. The process that resulted in the Eastern Neighborhoods

Plan determined that the public welfare was served when the residential development of formerly industrial sites would include a variety of housing unit types, including housing for both families and individuals. The Building and Housing Codes have determined that the public welfare is best served when a room specifically intended for sleeping has direct access to light and air and a point of external egress. Granting the exception would be materially detrimental to both goals. The likelihood that an interior bedroom is used as such goes down; it is likely such a space will be used as a den, office or other accessory use. Granting an exception to the unit mix requirement would mean a reduction in the availability of family friendly units. In addition, when it is used for a bedroom, an interior bedroom lacks a second point of egress for its occupant in the event of an emergency. This violates the public safety driven policy leading to the requirement that bedrooms open to the street or rear-yard. Taken together, the violations of policies protecting the public welfare should be deemed material.

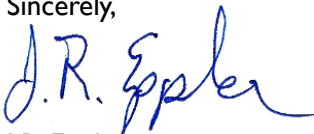
5. Last, the Planning Commission must find that the granting of the exception will be in harmony with the general purpose and intent of the Planning Code and will not adversely affect the General Plan. This finding is an overall balancing of the facts. The general purpose of the Code in this instance is to provide a variety of housing types in the Eastern Neighborhoods; as argued in the discussion of welfare, counting interior bedrooms (or, more accurately, den or office space) towards the unit mix violates of that purpose.

Even If Interior Bedrooms Were a Desirable Part of Minimum Unit Mix, the Legislature is the Appropriate Venue for Change.

The Eastern Neighborhoods Plan was legislated after a lengthy public process. As the Boosters have argued, it is not perfect. Even if Martin Building were to successfully argue that interior bedrooms are a desirable design feature and should be counted towards unit mix, granting an exception in this case, when the exception not tied to any overwhelming rationale and is simply in the interest of Martin Building, would be a significant change of policy taken via an end-run of the legislative process. Any change in the unit mix requirements deserves a full public hearing, including consideration of the concerns that led to the current Planning Code requirement. The appropriate venue for such a process is with the Board of Supervisors.

I am available to discuss this further with each of you at your convenience, and may be reached at president@potreroboosters.org or at 415-574-0775. Thank you for your consideration.

Sincerely,



J.R. Eppler
President

Cc: Supervisor Malia Cohen
John Rahaim, Director of Planning
Jonas P. Ionin, Commission Secretary
Kate Conner, Planner
Yoyo Chan, Legislative Aide to Supervisor Cohen

POTRERO BOOSTERS
NEIGHBORHOOD ASSOCIATION
SERVING THE HILL SINCE 1926

**Development Committee
Comments and Recommendations**

Date: June 1, 2015

Development Address: 88 Arkansas

We have reviewed the project and as a committee do not endorse or oppose it; that is up to the full membership. All comments are preliminary and offered in the expectation of a project that will benefit the neighborhood.

This 127-unit project is on the north side of Jackson Park, with 20% of the units designated as affordable. The voluntary increase from the normal 12% qualifies the project for fast-tracking through Planning and potential state-supported financing. This increase brings the total number of affordable units to 25 (instead of 18 required by law). The other benefit of the project is the inclusion of retail space, running almost the full length of the project along 17th street, intended to attract a neighborhood-serving restaurant.

The quality of the overall design and ground level commercial space attracted positive comments from members of the committee. The project includes a parklet, but no onsite public open space. The developer is encouraged to incorporate a setback on the 17th Street side of the building to expand the public realm, connecting it to Jackson Park and making it more inviting. This would also enhance the 17th street retail space where the restaurant is expected to be located. Developer contributions to publicly accessible open space, onsite or offsite, would help alleviate neighborhood concerns over cumulative impacts on parks and recreational facilities.

The unit mix includes 25 studios, 50 one-bedroom units, and 52 two-bedroom units, with the two-bedroom units averaging only 950 sq. feet. The most controversial issue amongst Committee members is that the units are relatively small overall and that 75% of the 2 bedroom apartments in the project don't have outside-facing windows for the second bedrooms, potentially reducing these units to one bedroom units with a den. Some members felt that this was not a family-friendly design, while others considered it is a viable option to maximize the living areas and common space, and felt that it would be appropriate for a younger child's room as well as being allowed by the Planning Code. With a goal of more family-friendly design, the Committee recommends that all projects include at least 10% three-bedroom units, as well as setting a goal of 60% units with two or more bedrooms. The Committee urges the developer for this project to reduce the overall number of units to incorporate larger units suitable for families.

The committee appreciates the developer's commitment to increase the number of affordable units in the project to 20%. As is the case with all neighborhood developments, we are interested in finding ways to increase that ratio to 30-33% of the units by making some apartments affordable to middle-income earners (making 80-120% of San Francisco median income). Current BMR requirements do not include any mandate for middle-income housing, and the City has a very large deficit for this demographic as a result. We are currently researching ways to enforce an increased affordable housing commitment, and will attempt to draft an agreement with the developers for this particular project.

The site is subject to the Maher Ordinance governing identification and removal of hazardous materials and contaminated soils. There are 12 UST's known to be within 1000 feet of the project and the committee has not seen a site mitigation plan as of yet. Given even the slightest potential for soil contamination with sensitive receptors in the immediate area, the Committee considers a Phase 2 study to be appropriate. The project will partially block public vistas from Jackson Park and the committee anticipates a shadow study to determine potential impacts on Jackson Park. They also expect to review a transportation study, which Planning expects will be completed in September or October 2015. Until these studies are complete it is not clear whether the project will, or should, qualify for a Community Plan Exemption.

The developer is expected to make a presentation to the full membership at a future Boosters meeting.

Del Popolo

January 24, 2016

RE: 88 Arkansas Street | Letter of Support

To Whom It May Concern:

I'm writing in full support of the proposed mixed-use residential project at 88 Arkansas Street in Lower Potrero Hill.

As someone who spends time in the area and lives nearby, I'm excited by the continued transformation of the neighborhood. Currently two vacant buildings sit on the project site, contributing little to the immediate area. The proposed project from Martin Building Company will not only bring 127 new housing units to the neighborhood, but it will also feature a community-serving restaurant/café space on the ground level that will connect with the surrounding sidewalks and adjacent Jackson Park to provide new pathways and experiences that are active, safe, comfortable, and engaging to further activate 17th Street and the park.

As the future commercial tenant of the proposed project, I look forward to contributing something new and beneficial to the neighborhood and its residents. I'm confident the 88 Arkansas project and its many pedestrian amenities will strengthen the vitality of our neighborhood and undoubtedly improve and activate this neglected part of 17th Street.

I strongly support this project as a model of neighborhood-sensitive development. It sets a high standard for future development in the area.

Best regards,

A handwritten signature in black ink, appearing to read "Jon Darsky", with a stylized flourish at the end.

Jon Darsky

Owner and Founder, Del Popolo

delpopolosf.com



February 16, 2016

Re: Friends of Mint Plaza | Support for 88 Arkansas

On behalf of Friends of Mint Plaza (FoMP), I wish to express our full support for development of the project at 88 Arkansas Street.

Friends of Mint Plaza is a non-profit organization that manages, maintains and programs Mint Plaza for the public's benefit. Martin Building Company was significantly involved in the development and construction of the plaza. Mint Plaza is the first-ever voluntary downtown street-to-plaza conversion completed by a developer in San Francisco, and the first ever in California to use Mello-Roos Community Facility District financing solely to create an urban pedestrian space. Mint Plaza is also adjacent to the Landmark status, Old U.S. Mint and several other historically significant buildings. Martin Building Company played an integral role in ensuring that Mint Plaza would be developed sensitively and involve all community stakeholders throughout the process. The end result was a unique urban space created for the benefit of the surrounding neighborhood and city at large.

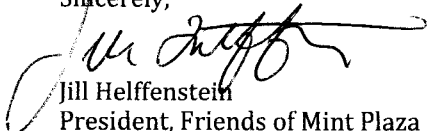
Mint Plaza combines a unique mix of neighborhood-serving uses and exceptional green design. Most notably, it has had a radically positive effect on the surrounding neighborhood. Jessie Street, once an unsafe, dirty and unsavory street, has transformed into a clean, pedestrian-friendly plaza. This project offers proof that with the right mix of pragmatism, invention and community investment, a derelict urban space can become a catalyst for future growth, and a model for smart development. Mint Plaza's flexible design ensures it will stay relevant to our community, even as the neighborhood continues to evolve.

FoMP endorses the 88 Arkansas project because it integrates so many of our organization's planning and design principles. The Arkansas development will redevelop a contaminated, blighted property into a viable into a vibrant mixed-use project with many green features, including usable open spaces and courtyards along the 17th Street and Arkansas Street corridors. This conversion will calm traffic and create a pedestrian-friendly public place in an area with limited greenery and public open space. In addition, Martin Building Company is actively supporting alternatives to car ownership by incorporating on-site car sharing vehicles as well as providing ample bicycle storage. These voluntary measures, combined with the project's proximity to existing transit and neighborhood services, means that future residents of 88 Arkansas will truly have the option of living car-free. We are delighted that Martin Building Company paid such close attention to integrating green building practices and sustainable transportation options, reducing environmental and traffic impacts from the project, and enhancing the surrounding public spaces.

Martin Building Company also seeks to work side-by-side with Friends of Jackson Park, the community, SF Recreation and Park, the Planning Department, and developers of neighborhood projects to help fund much needed future improvements to Jackson Park and Playground to ensure it becomes a treasured public space, accessible to all and preserved for future generations.

This project will improve the area immensely, and we believe this development can result in an even stronger and more robust contribution to the neighborhood fabric. We strongly endorse the redevelopment of 88 Arkansas.

Sincerely,



Jill Helffenstein
President, Friends of Mint Plaza



Livable
City

January 7, 2016

Mr. Patrick McNerney
President
Martin Building Company
14 Mint Plaza, 5th Floor
San Francisco, CA 94103

Re: 88 Arkansas Street

Dear Mr. McNerney:

On behalf of Livable City, I am pleased to offer our endorsement of Martin Building Company's project at 88 Arkansas Street.

The proposed project is a mixed-use building with 127 dwelling units, 3,100 square feet of ground-floor retail space facing onto 17th and Arkansas streets, and 127 bicycle parking spaces, one for each unit with room for more. It includes 98 parking spaces, the maximum permitted as of right in an UMU zoning district, and one City CarShare space.

The 88 Arkansas project as proposed will be a fine addition to the Potrero Hill neighborhood. It is fully in accord with the Showplace Square/Potrero Area Plan, and furthers the goals and policies of the neighborhood plan. 88 Arkansas and projects like it further our mission of creating robust and complete neighborhoods by providing well-designed housing and neighborhood-serving retail that promote walking, bicycling and transit. The neighborhood is well served by local and regional transit, with bicycle routes nearby. The ground floor retail and residential uses, along with the widened sidewalks and generous plantings, will green and enliven the neighborhood, and complement the park across the street.

We congratulate Martin Building on providing 20% affordable units on-site, exceeding the required 14.4%. The resulting 25 affordable dwelling units are needed in the Potrero Hill neighborhood.

Thank you for submitting this project to Livable City for our review. Please keep us informed on any changes or updates to the project as it moves forward. We are pleased to support this exciting project as it moves forward.

Sincerely,

Tom Radulovich
Executive Director
Livable City



San Francisco Housing Action Coalition (SFHAC) Project Report Card

Address: 88 Arkansas Street

Project Sponsor: Martin Building Company

Date of SFHAC Review: February 10, 2016

Grading Scale:

1 = Fails to meet project review guideline criteria

2 = Meets some project review guideline criteria

3 = Meets basic project review guideline criteria

4 = Exceeds basic project review guideline criteria

5 = Goes far beyond of what is required

Criteria for SFHAC Endorsement:

1. The project must have been presented to the SFHAC Project Review Committee;
2. The project must score a minimum of 3/5 on any given guideline.

<i>Guideline</i>	<i>Comments</i>	<i>Grade</i>
Land Use	The project would demolish the existing commercial buildings and surface parking lot and construct 127 homes with ground-floor retail and subterranean parking.	5
Density	The project takes advantage of the building envelope and actually increases its density by using a “nested-bedroom” unit plan. These are very livable units if designed well.	5
Affordability	The project exceeds the Inclusionary housing requirement by making 20 percent of the units permanently affordable with the help of tax credits.	5
Parking and Alternative Transportation	The project is located one block from the 16 th Street transit corridor. We urge the project sponsor to reduce car parking and increase the bike parking to one space per bedroom.	4
Preservation	There are no structures of significant cultural or historic merit on or near the site that would be impacted by the proposed project.	N/A
Urban Design	The project will significantly improve the streetscape, particularly along 17 th Street. The retail space offers generous ceiling heights. Open space is provided with a programmed roof deck.	5
Environmental Features	The project intends to achieve LEED Platinum. Features planned for the project include solar thermal, PV panels and measures to address water conservation.	5
Community Input	The project sponsor has presented to several groups, including the Potrero Boosters, and has offered to coordinate the outreach for future improvements to Jackson Playground.	5
Additional Comments	SFHAC has no objection to the “nested bedrooms.” They have been built successfully throughout the City and should succeed here.	N/A
Final Comments	SFHAC endorses the project at 88 Arkansas Street without reservation.	4.9/5

Please see attached letter for further explanation.



95 Brady Street
San Francisco, CA 94103
415 541 9001
info@sfhac.org
www.sfhac.org

Patrick McNerney, President
Martin Building Company
14 Mint Plaza, Fifth Floor
San Francisco, CA 94103

February 18, 2016

Re: 88 Arkansas Street – Mixed-use Development

Dear Mr. McNerney,

Thank you for presenting your plans for 88 Arkansas to the San Francisco Housing Action Coalition's (SFHAC) Project Review Committee on February 10, 2016. Upon thorough review and discussion, our members believe this project has merit and entirely aligns with our mission of increasing the supply of well-designed, well-located housing at *all* levels of affordability in San Francisco. Please review our letter, which explains how your project meets our guidelines. Also, see our report card, which grades the project according to each guideline. We have attached a copy of our Project Review Guidelines for your reference.

Project Description: You propose to demolish the existing commercial buildings and surface parking lot and construct a five-story, mixed-use building with 127 homes, ground-floor retail, and one level of subterranean parking for 97 cars.

Land Use: This project completely aligns with the goals of the Eastern Neighborhood Plan (ENP) by providing high-density housing in a growing neighborhood, one block from an emerging major transit corridor on 16th Street.

Density: The project takes advantage of the building envelope and provides a mix of unit types, ranging from studios, one, two and three-bedroom units. The "nested-bedroom" unit plan actually helps the project to achieve greater density.

We understand the Potrero Boosters opposed the nested bedroom unit plan. According to their interpretation of the ENP, the Plan does not allow for this design. Our members agree that the language in the ENP is vague on this issue. SFHAC has no objection to this design and has seen them successfully built in numerous projects across the City. If they're designed well, as these are, they are quite suitable for residents, whether single people or families.

Affordability: We applaud you and your team for voluntarily increasing your on-site inclusionary requirement from the mandated 14.4 percent to 20 percent. You are able to achieve this high percentage with the use of tax credits. By achieving higher density with the nested-bedroom design, you are able to increase your total unit count, thereby increasing the number of below-market-rate units, something SFHAC strongly supports.

Parking and Alternative Transportation: The project is one block from 16th Street, which is planned to be a major transit corridor and is planned to have its own bus rapid transit line. The building includes spaces for 97 cars, meeting the as-of-right ratio 0.75 spaces per unit. The plan to includes 127 bicycle parking spaces and one car-share space.

We would support reducing the car parking ratio and increasing the bike parking ratio to one space per bedroom as we commonly see in many projects brought before us. We've heard from several of our members that they over-estimated their need for car parking while under-estimating the need for bike parking in their projects. We'd also prefer the bicycle parking to be more accessible from the lobby. Under your current plan, tenants would have to go into the garage or use an elevator to access their bikes.

Preservation: There are no structures of significant cultural or historic merit on or near the site that would be impacted by the proposed project.

Urban Design: Your project significantly improves the streetscape, particularly along 17th Street. One-story flats with stoops will front this block and enjoy a generous 25-foot wide sidewalk with new landscaping. This will greatly improve the streetscape. The retail space has a very gracious ceiling height, about 20 feet. Our members agreed that this design of the retail space should ensure its success and occupancy.

Open space will be included on a programmed roof deck.

Environmental Features: You have committed to achieving LEED Platinum, something we applaud. Your project will include solar thermal, PV panels and water conservation features.

Community Input: You have made several presentations to the Potrero Boosters. One of our members who is active with the Boosters noted that you've done all the outreach that could reasonably be expected. However, the Boosters have decided to oppose the project because they don't believe the "nested-bedrooms" are legal under the ENP. The members of SFHAC respect the Boosters' position, but we have no objection to the design and believe delaying this project on that basis would set a poor precedent for other projects in the City. In response to feedback from the Boosters, you have increased your three-bedroom count to 10 units.

Finally, we strongly support your offer to manage coordination of Jackson Playground's improvements. We hope an arrangement can be worked out with the City so you help can contribute to the park's upgrading.

Patrick McNerney
February 18, 2016
Page Three

Thank you again for presenting your plan for 88 Arkansas Street to our Project Review Committee. We are pleased to endorse the project, without reservation. Please keep us abreast of any changes and let us know how we may be of assistance.

Sincerely,

A handwritten signature in blue ink, appearing to read "Tim Colen", with a long horizontal flourish extending to the right.

Tim Colen
Executive Director

SFHAC Project Review Guidelines

Land Use: Housing should be an appropriate use of the site given the context of the adjacent properties and the surrounding neighborhood and should enhance neighborhood livability.

Density: The project should take full advantage of the maximum unit density and/or building envelope, allowable under the zoning rules.

Affordability: The need for affordable housing, including middle income (120-150 of Area Median Income) housing, is a critical problem and SFHAC gives special support to projects that propose creative ways to expand or improve unit affordability beyond the legally mandated requirements.

Parking and Alternative Transportation: SFHAC expects the projects it endorses to include creative strategies to reduce the need for parking, such as ample bicycle storage, provision of space for car-share vehicles on-site or nearby, un-bundling parking cost from residential unit cost, and measures to incentivize transit use. Proximity to transit should result in less need for parking.

In districts with an as-of-right maximum and discretionary approval up to an absolute maximum, SFHAC will support parking exceeding the as-of-right maximum only to the extent the Code criteria for doing so are clearly met. In districts where the minimum parking requirement is one parking space per residential unit (1:1), the SFHAC will not, except in extraordinary circumstances, support a project with parking in excess of that amount.

Preservation: If there are structures of significant historic or cultural merit on the site, their retention and/or incorporation into the project consistent with historic preservation standards is encouraged. If such structures are to be demolished, there should be compelling reasons for doing so.

Urban Design: The project should promote principles of good urban design: Where appropriate, contextual design that is compatible with the adjacent streetscape and existing neighborhood character while at the same time utilizing allowable unit density; pleasant and functional private and/or common open space; pedestrian, bicycle and transit friendly site planning; and design treatments that protect and enhance the pedestrian realm, with curb cuts minimized and active ground floor uses provided.

Patrick McNerney
February 18, 2016
Page Five

Projects with a substantial number of multiple bedroom units should consider including features that will make the project friendly to families with children.

Environmental Features: SFHAC is particularly supportive of projects that employ substantial and/or innovative measures that will enhance their sustainability and reduce their carbon footprint.

Community Input: Projects for which the developer has made a good faith effort to communicate to the community and to address legitimate neighborhood concerns, without sacrificing SFHAC's objectives, will receive more SFHAC support.

February 14, 2016

RE: Endorsement of Proposed 88 Arkansas Street Project | Inbound Bedrooms

To Whom It May Concern:

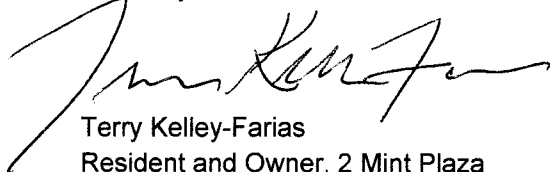
As a long-term resident and owner of one of Martin Building Company's developments at 2 Mint Plaza, I am writing in full support of the proposed project at 88 Arkansas Street, and in particular its thoughtfully designed nested, inbound bedrooms.

The inbound bedrooms are an indispensable component of urban living. First and foremost, the bedrooms provide noise separation from the busy streets and Mint Plaza right outside our window. This continually ensures a peaceful bedroom environment and uninterrupted sleep. The nested bedrooms are also ideal for families. My partner and I are expecting a baby in July, and we will be converting our bedroom into a nursery – this is only possible because of the inbound design.

Nested bedrooms also enable smarter unit layouts as they allow for larger living areas and kitchens, two places we spend most of our time. Additionally, the large glass window that separates the bedroom from the living room features a window covering that gives us flexibility to control how much light passes through without affecting the amount of natural light in the living space. This is going to be crucial for our life with a napping baby!

I strongly recommend Martin Building Company's 88 Arkansas Street project be approved by the Planning Commission.

Sincerely,

A handwritten signature in black ink, appearing to read "Terry Kelley-Farias", written over a horizontal line.

Terry Kelley-Farias
Resident and Owner, 2 Mint Plaza

88 ARKANSAS

LARGE PROJECT AUTHORIZATION

PROJECT DESCRIPTION

PROPOSED USE IS FOR ONE FIVE-STORY MIXED USE BUILDING INCLUDING GROUND FLOOR RETAIL AND MULTI-FAMILY RESIDENTIAL UNITS. PROJECT INCLUDES A ONE-LEVEL SUBTERRANEAN RESIDENTIAL PARKING GARAGE.

VICINITY MAP



PROJECT TEAM

OWNER: 88 ARKANSAS AFFORDABLE, LP
JULIE HEINZLER
14 MINT PLAZA, FIFTH FLOOR
SAN FRANCISCO, CA 94103
T: 415.442.4800

ARCHITECT: BAR ARCHITECTS
WILLIAM DUNCANSON
901 BATTERY STREET, SUITE 300
SAN FRANCISCO, CA 94111
T: 415.293.5700

LANDSCAPE: CLIFF LOWE ASSOCIATES
ARCHITECT: CLIFF LOWE
1175 FOLSOM STREET
SAN FRANCISCO, CA 94103
T: 415.431.0394

CIVIL: KCA ENGINEERS INC.
ENGINEER: PETER J. BEKEY
318 BRANNAN STREET
SAN FRANCISCO, CA 94107
T: 415.546.7111

SHEET INDEX

04/23/15 SUBMITTAL 10/28/15 SUBMITTAL 12/22/15 SUBMITTAL 01/08/16 SUBMITTAL	SHEET NUMBER	SHEET TITLE
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X X X X	2	PROJECT DATA & ZONING SUMMARY
X X X X	3	PLANNING CODE DIAGRAMS
	3a	OPEN SPACE DIAGRAMS
	3b	OPEN SPACE DIAGRAMS
	3c	SOLARIUM OPEN SPACE
X X X X	4	MODIFICATION REQUESTS
X X X X	5	SITE PLAN
X X X X	6	CONTEXT PHOTOS
X X X X	7	TOPOGRAPHIC SURVEY
	8	SIDEWALK IMPROVEMENT 1
	9	SIDEWALK IMPROVEMENT 2
	10	SIDEWALK IMPROVEMENT 3
X X X X	11	LANDSCAPE SITE PLAN
X X X X	12	LANDSCAPE ROOF PLAN
X X X X	13	PLANT MATRIX
X X X X	14	BASEMENT LEVEL
X X X X	15	GROUND FLOOR PLAN
X X X X	16	SECOND FLOOR PLAN
X X X X	17	THIRD & FOURTH FLOOR PLAN
X X X X	18	FIFTH FLOOR PLAN
X X X X	19	ROOF PLAN
X X X X	20	ELEVATIONS
X X X X	21	ELEVATIONS
X X X X	22	BUILDING SECTIONS
X X X X	23	PERSPECTIVE FROM 17th & ARKANSAS
X X X X	24	PERSPECTIVE FROM ARKANSAS
	25	STOOP PERSPECTIVES
	26	STOOP PLAN & ELEVATION
	27	STOOP SECTIONS
X X X X	28	UNIT PLANS

88 ARKANSAS

SAN FRANCISCO, CA

COVER SHEET

PROJECT WILL BE A PLANNED UNIT DEVELOPMENT PURSUANT TO SECTION 304 OF THE SAN FRANCISCO PLANNING CODE

PROPERTY SUMMARY

PARCEL# (BLOCK/LOT):	3953002 & 3953002A (3953/002 & 3953/002A)
PARCEL AREA:	
3953002	19,998 SF (PER ASSESSOR’S REPORT)
3953002A	10,000 SF (PER ASSESSOR’S REPORT)
PROJECT LOT AREA:	29,998 SF
NEIGHBORHOOD:	POTRERO HILL
ADDRESSES:	88 ARKANSAS STREET, SAN FRANCISCO, CA 94107
	1500 17TH STREET, SAN FRANCISCO, CA 94107
	1530 17TH STREET, SAN FRANCISCO, CA 94107

BUILDING STATISTICS

LEVEL	GSF*	STUDIO	1 BR	2BR	3BR	TOTAL
BASEMENT	2,853 SF	-	-	-	-	-
LEVEL 1	22,714 SF	3	6	7	3	19
LEVEL 2	20,275 SF	4	9	8	2	23
LEVEL 3	23,968 SF	6	12	10	1	29
LEVEL 4	23,968 SF	6	13	8	2	29
LEVEL 5	23,968 SF	6	11	8	2	27
ROOF	938 SF	-	-	-	-	-
TOTAL	118,684 SF	25	51	41	10	127
UNIT MIX %		20%	40%	32%	8%	100%

TOTAL RESIDENTIAL GSF*:	111,694 GSF	*DOES NOT INCLUDE BASEMENT, RETAIL OR ROOF AREA
TOTAL RETAIL/CAFE GSF**:	3,118 GSF	**AREA PER SECTION 102, FLOOR AREA GROSS (b)(1) & (6) & (8).

TOTAL PARKING

BASEMENT LEVEL

STANDARD & COMPACT SPACES:	13 SPACES
ACCESSIBLE SPACES:	2 SPACES
STACKER SPACES:	82 SPACES
CAR-SHARE SPACES:	1 CAR-SHARE SPACE
GRAND TOTAL:	98 SPACES (5 FOR 2-BED @ 1/DU + 122 DU @ 0.75/DU + 1 CAR-SHARE)

BICYCLE SPACES:

CLASS-1	127 RESIDENTAIL SPACES PROVIDED (REQUIRED: 1 SPACE FOR EVERY UNIT UP TO 100, THEN 1 FOR EVERY 4 UNITS = 107) 1 SPACE CAFE PROVIDED (REQUIRED = CAFE: 1 SPACE FOR EVERY 7,500 SQUARE FEET: 2,916 SF / 7,500 SF = 0.38 [ROUND UP TO 1])
CLASS-2	6 RESIDENTIAL SPACES PROVIDED (REQUIRED = 127 DU / 20 SPACES per DU = 6.35 [ROUND DOWN TO 6]) 6 CAFE SPACES PROVIDED (REQUIRED = 2,916 SF / 750 SF per BIKE = 3.89 [ROUND UP TO 4])

UNIT AVERAGE SIZES (NET)

STUDIO	435 NSF
1-BEDROOM	600 NSF
2-BEDROOM	866 NSF

OPEN SPACE STATISTICS (CONTRIBUTING AREA):

	COMMON	PRIVATE
SOUTH COURTYARD (AT PODIUM):	3,514 SF	-
NORTH COURTYARD (AT PODIUM):	-	240 SF
PATIO/BACLONIES ALONG ARKANSAS:	-	429 SF
SOLARIUM:	1,236SF	-
ARTIFICAIL TURF AREA:	1,520 SF	-
ROOF DECK:	3,260 SF	-
TOTAL	9,530 SF	669 SF = 10,199SF
80.3 SF OF OPEN SPACE PROVIDE PER UNIT (80 SF PER UNIT REQUIRED)		

REAR YARD:

SOUTH COURTYARD:	3,514 SF
NORTH COURTYARD:	1,905 SF
ALONG ARKANSAS:	867 SF
TOTAL:	6,286 SF
AT GRADE AREAS:	6,286 / 29,998 = 21.0% REAR YARD
INCLUDING ROOF DECKS:	11,066 / 29,998 = 36.9% REAR YARD

PLANNING DEPARTMENT ZONING SUMMARY

ZONING DISTRICT:	SEC.843	UMU - URBAN MIXED USE
HEIGHT & BULK DISTRICT:	SEC.843	48-X,
REAR YARD	SEC.134.1	25% LOT DEPTH, BUT IN NO CASE LESS THAN 15 FEET
STREET FRONTAGE:	SEC.145.1	REQUIRED
STREET FRONTAGE - ABOVE GRADE PARKING	SEC.145.1.c.1	MINIMUM 25 FEET SETBACK ON GROUND FLOOR, 15 FEET ON FLOORS ABOVE
STREET FRONTAGE - PARKING AND LOADING ACCESS RESTRICTIONS	SEC.145.1.c.2	PORTIONS ON ARKANSAS STREET
STREET FRONTAGE - ACTIVE USES REQUIRED	SEC.145.1.c.3	REQUIREMENTS APPLY
STREET FRONTAGE - GROUND FLOOR CEILING HEIGHT	SEC.145.1.c.4.A	MINIMUM FLOOR-TO-FLOOR HEIGHT OF 17 FEET AT NON-RESIDENTIAL
FLOOR AREA RATIO:	SEC.124.	3.0 TO 1 (RESIDENTIAL SF EXEMPT)
NON-RESIDENTIAL USE SIZE:	SEC.843.45-843.51	VARIABLES: RETAIL SALES PERMITTED UP TO 25,000 SF PER LOT; ABOVE 25,000 SF PERMITTED ONLY IF THE RATIO OF OTHER PERMITTED USES TO RETAIL IS AT LEAST 3:1
COMMERCIAL OFF-STREET PARKING:	SEC.843.11	NONE REQUIRED. LIMITS SET FORTH IN SEC.151.1
OFF-STREET FREIGHT LOADING:	SEC.152.	1 REQUIRED, RESIDENTIAL AREA IS GREATER THAN 100,000 SF
RESIDENTIAL DENSITY	SEC.843.24	NO DENSITY LIMIT
RESIDENTIAL UNIT MIX	SEC.207.6	40% OF ALL DWELLING UNITS MUST CONTAIN TWO OR MORE BEDROOMS
RESIDENTIAL OPEN SPACE:	SEC.843.11	80 SQ. FT. PER UNIT IF PRIVATE, 54 SQ. FT. IF PUBLICLY ACCESSIBLE
RESIDENTIAL PARKING:	SEC.843.08	NONE REQUIRED. LIMITS SET FORTH IN SEC.151.1 - FOR DWELLING UNITS AT LEAST 1,000 SQ. FT. IN SIZE WITH TWO OR MORE BEDROOMS, 1 CAR IS PERMITTED FOR EACH UNIT. FOR OTHER UNITS, UP TO 0.75 CARS FOR EACH UNIT IS PERMITTED.
CAR-SHARE PARKING:	SEC.166.d.1	1 CAR-SHARE SPACE REQUIRED
BICYCLE PARKING	SEC.155.2	ONE CLASS 1 SPACE FOR EACH UNIT UP TO 100 SPACES, AND ONE ADDITIONAL SPACE FOR EVERY FOUR UNITS OVER 100 UNITS. ONE CLASS 2 SPACE FOR EACH 20 UNITS.
HORIZONTAL MASS REDUCTION	SEC.270.1	IF FRONTAGE GREATER THAN 200 FEET

HISTORIC PRESERVATION EVALUATION SUMMARY

SHOWPLACE SQUARE/NORTHEAST MISSION HISTORIC RESOURCE SURVEY STATUS:

88 ARKANSAS STREET

PARCEL 3953002	
CEQA CATEGORY:	C - NOT A HISTORIC RESOURCE
EVALUATION DATE:	8/17/2011
SURVEY RATING:	6Z
RATING DESCRIPTION:	FOUND INELIGIBLE FOR NR, CR, OR LOCAL DESIGNATION THROUGH SURVEY EVALUATION

1530 17TH STREET

PARCEL 3953002A	
CEQA CATEGORY:	C - NOT A HISTORIC RESOURCE
EVALUATION DATE:	8/17/2011
SURVEY RATING:	6Z
RATING DESCRIPTION:	FOUND INELIGIBLE FOR NR, CR, OR LOCAL DESIGNATION THROUGH SURVEY EVALUATION

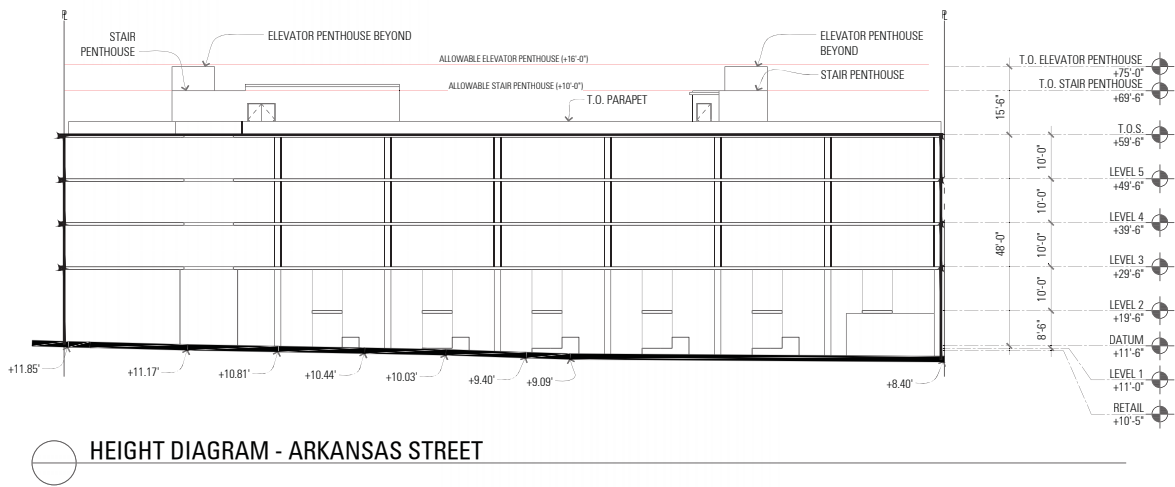
SUSTAINABILITY

THIS PROJECT WILL PURSUE LEED FOR HOMES PLATINUM.

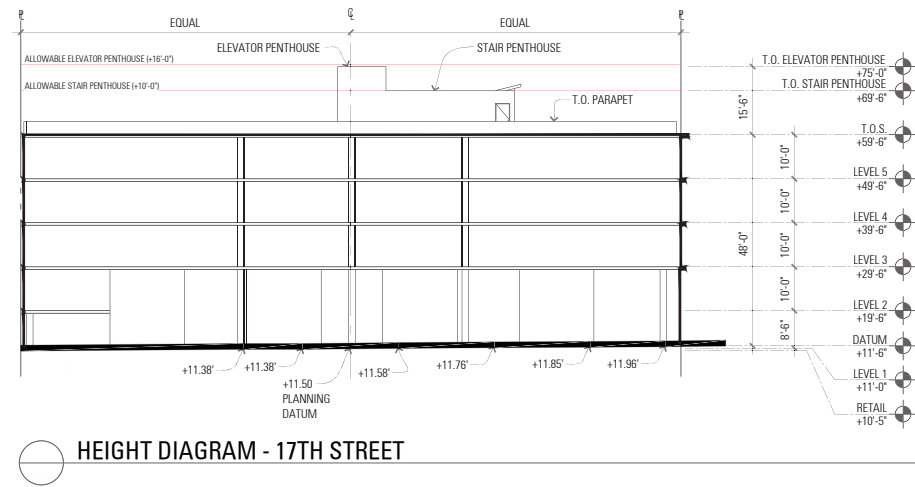
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PROJECT DATA & ZONING SUMMARY



HEIGHT DIAGRAM - ARKANSAS STREET



HEIGHT DIAGRAM - 17TH STREET

BUILDING HEIGHT DIAGRAMS

- Height & Bulk District: 48-X
- Building height complies with section 260 requirements.
- Per 260(b)(1)(B), Elevator, stair and mechanical penthouses shall be limited to the top 10 feet of such features where the height limit is 65 feet or less. However, for elevator penthouses, the exemption shall be limited to the top 16 feet and limited to the footprint of the elevator shaft, regardless of the height limit of the building.
- Height measured from curb on 17th Street (datum 11'-6") per section 102.12(a)(b)(d).

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1 OPEN SPACE PLAN - LEVEL 1



2 OPEN SPACE PLAN - LEVEL 2

- OPEN SPACE LEGEND
- OS-PRIVATE-CONTRIBUTE
 - OS-PUBLIC-CONTRIBUTE
 - OS-PRIVATE-NON-CONTRIBUTING
 - OS-PUBLIC-NON-CONTRIBUTING



3 OPEN SPACE PLAN - LEVEL 3

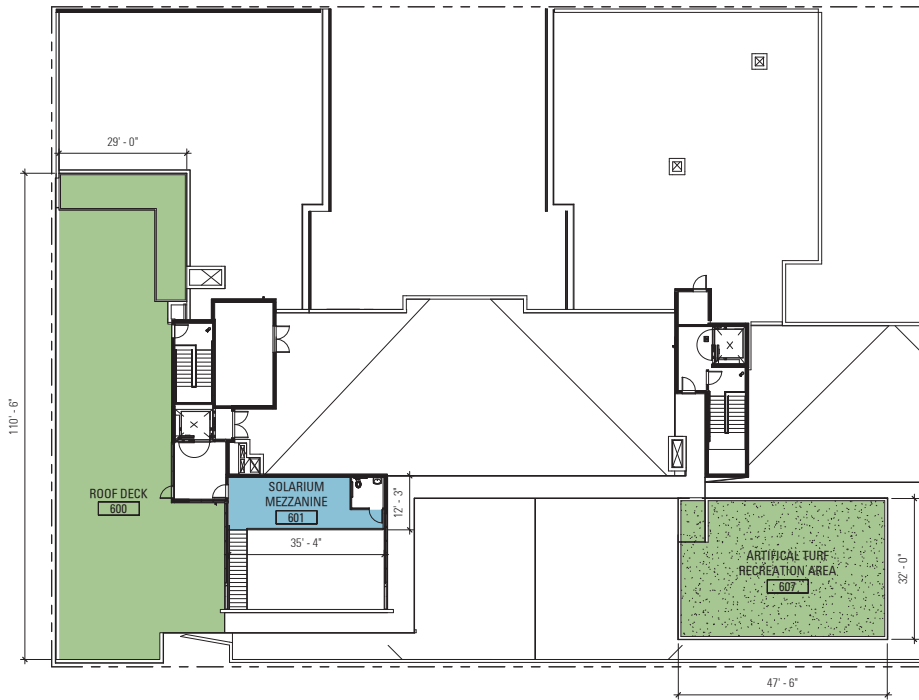


4 OPEN SPACE PLAN - LEVEL 4

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1
3b
OPEN SPACE PLAN - LEVEL 5



2
3b
OPEN SPACE PLAN - ROOF DECK

OPEN SPACE - CONTRIBUTING

PRIVATE USABLE			COMMON USABLE		
UNIT NUMBER	OS TYPE	AREA	UNIT NUMBER	OS TYPE	AREA
LOWER LEVEL 1			LOWER LEVEL 1		
102	PRIVATE STOOP	39 SF		SOUTH COURTYARD	3,514 SF
104	PRIVATE STOOP	39 SF			
107	PRIVATE STOOP	39 SF	LEVEL 5		
108	PRIVATE STOOP	39 SF	528	SOLARIUM	1,236 SF
110	PRIVATE STOOP	39 SF			
111	PRIVATE PATIO	80 SF	ROOF DECK		
112	PRIVATE PATIO	80 SF	600	ROOF DECK	3,260 SF
115	PRIVATE PATIO	80 SF	607	ARTIFICIAL TURF	1,520 SF
					9,530 SF
LEVEL 2					
204	PRIVATE DECK	39 SF			
206	PRIVATE DECK	39 SF			
209	PRIVATE DECK	39 SF			
210	PRIVATE DECK	39 SF			
211	PRIVATE DECK	39 SF			
213	PRIVATE DECK	39 SF			
		669 SF			

OPEN SPACE - NON-CONTRIBUTING

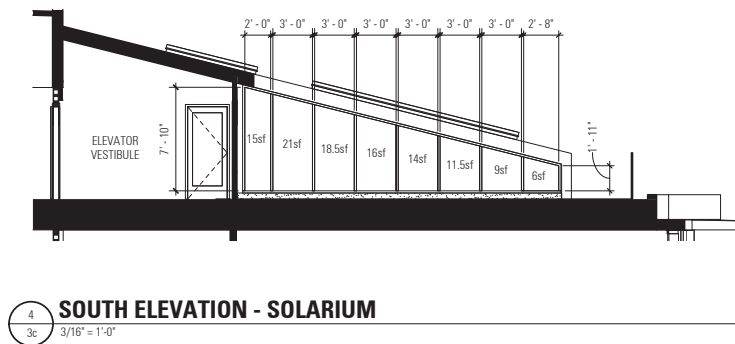
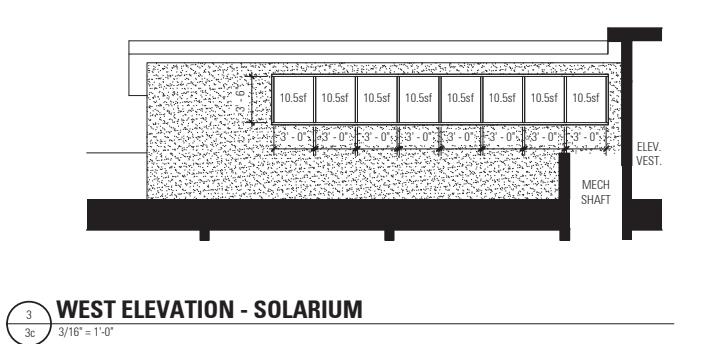
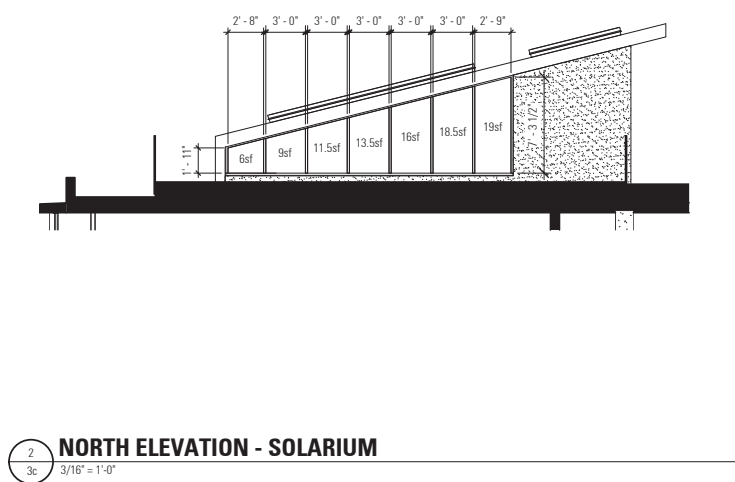
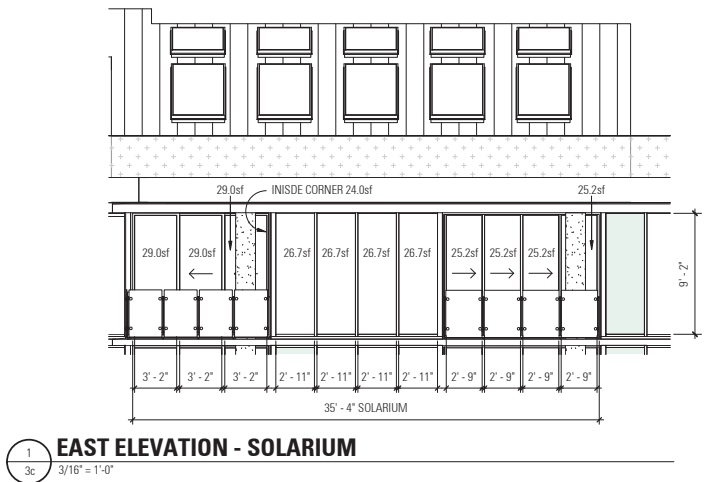
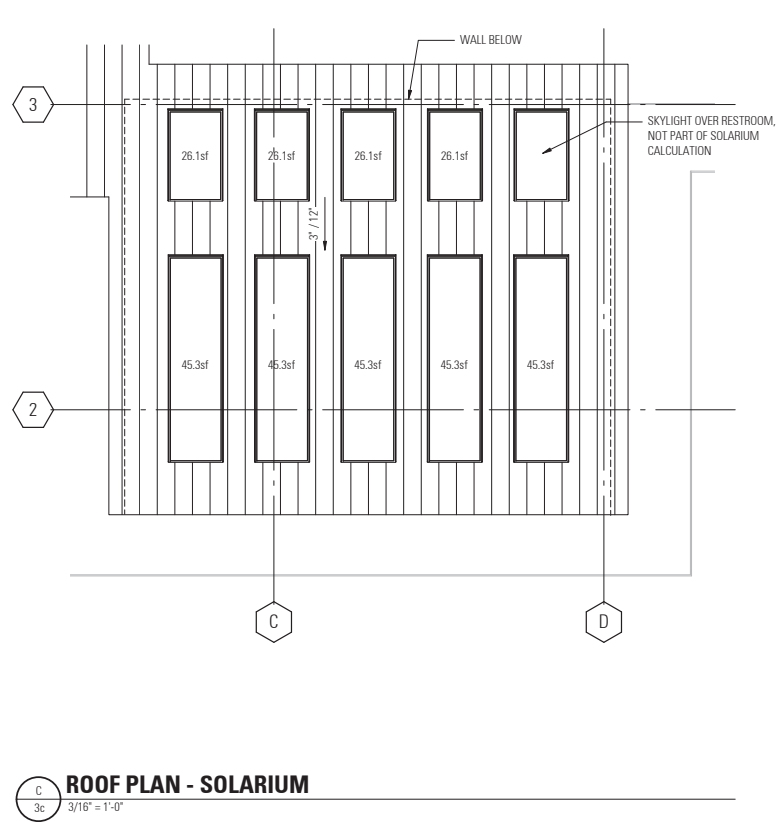
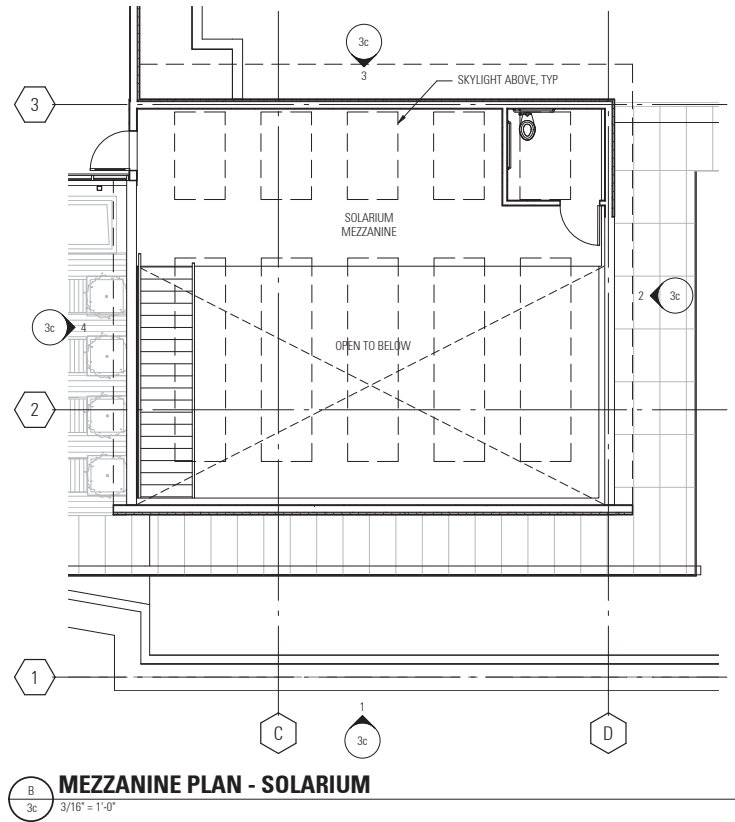
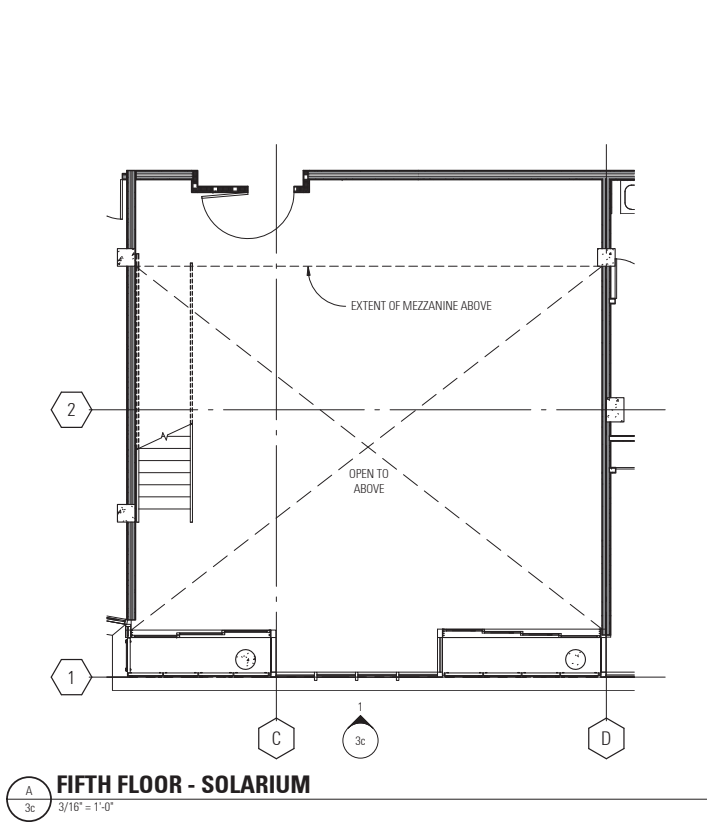
PRIVATE USABLE			COMMON USABLE		
UNIT NUMBER	OS TYPE	AREA	UNIT NUMBER	OS TYPE	AREA
LOWER LEVEL 1			LOWER LEVEL 1		
102	PRIVATE STOOP	11 SF			
104	PRIVATE STOOP	11 SF	402	PRIVATE DECK	42 SF
107	PRIVATE STOOP	11 SF	403	PRIVATE DECK	42 SF
108	PRIVATE STOOP	11 SF	405	PRIVATE DECK	33 SF
110	PRIVATE STOOP	11 SF	407	PRIVATE DECK	36 SF
111	PRIVATE PATIO	20 SF	409	PRIVATE DECK	36 SF
112	PRIVATE PATIO	16 SF	412	PRIVATE DECK	36 SF
115	PRIVATE PATIO	105 SF	414	PRIVATE DECK	36 SF
LEVEL 2			416	PRIVATE DECK	36 SF
222	PRIVATE DECK	42 SF	418	PRIVATE DECK	37 SF
LEVEL 3			424	PRIVATE DECK	42 SF
302	PRIVATE DECK	42 SF	426	PRIVATE DECK	42 SF
303	PRIVATE DECK	42 SF	428	PRIVATE DECK	42 SF
305	PRIVATE DECK	33 SF	LEVEL 5		
307	PRIVATE DECK	36 SF	502	PRIVATE DECK	42 SF
309	PRIVATE DECK	36 SF	503	PRIVATE DECK	42 SF
312	PRIVATE DECK	36 SF	507	PRIVATE DECK	36 SF
314	PRIVATE DECK	36 SF	510	PRIVATE DECK	36 SF
316	PRIVATE DECK	36 SF	512	PRIVATE DECK	36 SF
318	PRIVATE DECK	37 SF	513	PRIVATE DECK	36 SF
324	PRIVATE DECK	42 SF	514	PRIVATE DECK	37 SF
326	PRIVATE DECK	42 SF	522	PRIVATE DECK	42 SF
328	PRIVATE DECK	42 SF	524	PRIVATE DECK	42 SF
			526	PRIVATE DECK	42 SF
			528	PRIVATE DECK	33 SF
			528	PRIVATE DECK	36 SF
					1,617 SF

OPEN SPACE (Section 135 & 843.11)

Code Required:
80 SF/DU if private (Min. 10'x10' at grade/podium, 6'x6' as balconies)

Minimum Open Space Required: 127 DU x 80 SF/DU = 10,160

	Contributing	Non-Contributing
Private Usable Open Space Provided:	669 SF	1,617 SF
Common Usable Open Space Provided:	9,530 SF	1,876 SF
	10,199 SF	3,493 SF
Grand Total:	13,692 SF	



SOLARIUM CALCULATIONS

PERIMETER AREA

EAST ELEVATION GLAZING:	342.6 SF
NORTH ELEVATION GLAZING:	93.5 SF
WEST ELEVATION GLAZING:	84SF
SOUTH ELEVATION GLAZING:	111 SF

TOTAL GLAZING AREA:	631.1 SF
TOTAL INTERIOR WALL AREA:	2,064 SF

TOTAL GLAZING REQUIRED:	30%
TOTAL GLAZING PROVIDED:	631.1 SF / 2,064 SF = 30.6%

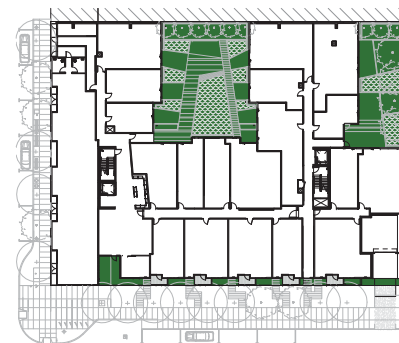
ROOF AREA

TOTAL GLAZING AREA:	330.9 SF
TOTAL ROOF AREA:	1,092 SF

TOTAL GLAZING REQUIRED:	30%
TOTAL GLAZING PROVIDED:	330.9 SF / 1,092 SF = 30.3%

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REAR YARD - LEVEL 1

MODIFICATION REQUEST: REAR YARD (Section 134)

Required

Per Section 134(a)(1) UMU Districts are required to provide a rear yard depth of 25% total lot depth, but no less than 15 feet. 25% of lot depth = 50'-0"

Provided

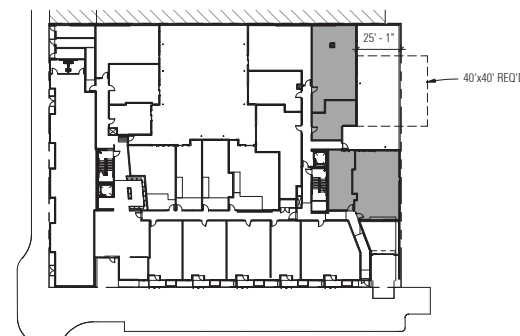
A Rear Yard area of 6,286SF is provided, which is 21.0% of the site area.

Justification

The rear yard requirement in Eastern Neighborhoods Mixed Use Districts may be modified or waived by the Planning Commission pursuant to Section 329. Per Section 134(f)(1). The project proposes providing a comparable amount of open space elsewhere in the project while providing an excess of 3,493 SF of Open Space beyond the required amount.

MODIFICATION REQUEST: Section 207.6 Exception to ZA Interpretation of Bedrooms with Shared Light

- There are a total of 38 two-bedroom and 10-three bedroom units in which at least one bedroom does not have an exterior window. The ZA's interpretation will be recinded in early 2016. See sheets 15-18 for affected unit locations, marked with an asterisk *.



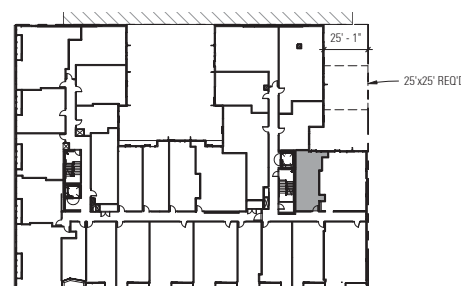
EXPOSURE PLAN - LEVEL 1



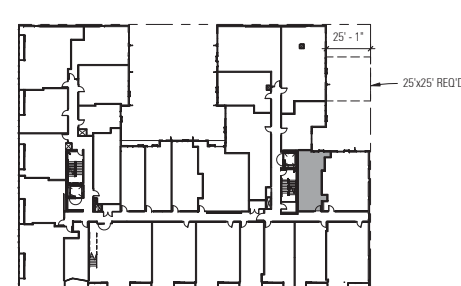
EXPOSURE PLAN - LEVEL 2



EXPOSURE PLAN - LEVEL 3



EXPOSURE PLAN - LEVEL 4



EXPOSURE PLAN - LEVEL 5

MODIFICATION REQUEST: UNIT EXPOSURE (Section 140)

Required

Section 140(a) all units to face an open space. At least one room that meets the 120sf min. floor area shall face directly on an open area per the requirements of 140(a)(1) or (2). The open area shall be 25 feet in every horizontal direction at the level at which the dwelling unit is located and the floor immediately above it, with an increase of five feet in every horizontal dimension at each subsequent floor.

Required Exposure Depth

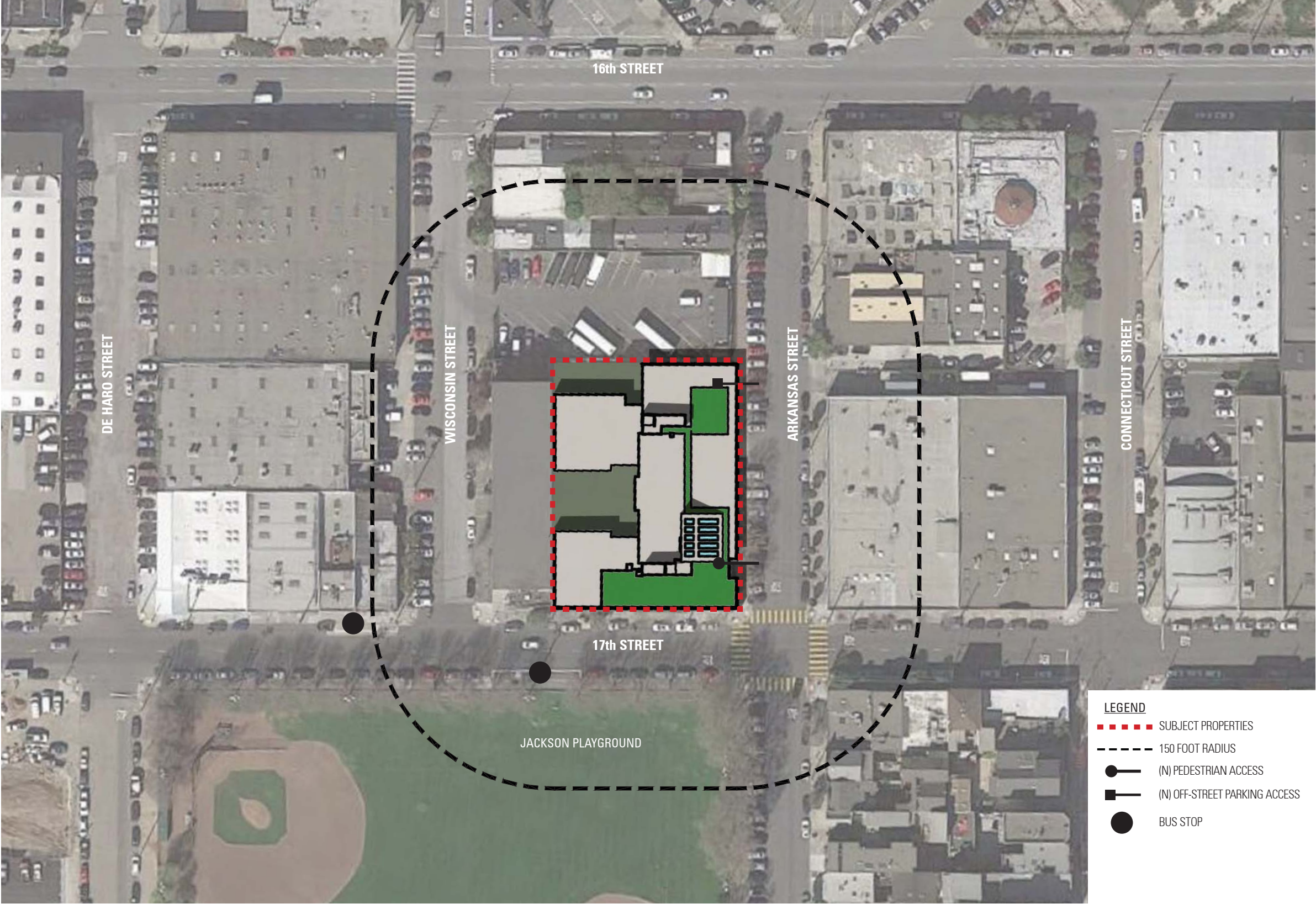
Level 1:	40-ft x 40-ft
Level 2:	35-ft x 35-ft
Level 3:	30-ft x 30-ft
Level 4:	25-ft x 25-ft
Level 5:	25-ft x 25-ft

Provided

All units, except those shown graphically shaded on this sheet, meet the open space requirements by providing one room of at least 120sf floor area that faces a public way, rear yard, or an unobstructed open area greater than 25-ft in all horizontal directions at the floor at which the dwelling unit is located and the floor immediately above. Fourteen (14) units in total are requesting a modification.

Justification

The corner lot configuration creates a building geometry where inside corner units require a modification. However the intent of Section 140 is met since the units have view out to the open area from a room greater than 120sf.



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SITE PLAN



1. 17th and Arkansas Streets, looking north-west



2. 17th and Arkansas Streets, looking south-east



3. 17th and Arkansas Streets, looking south-west



4. 17th and Wisconsin Streets, looking north-west



5. 17th and Wisconsin Streets, looking north-east



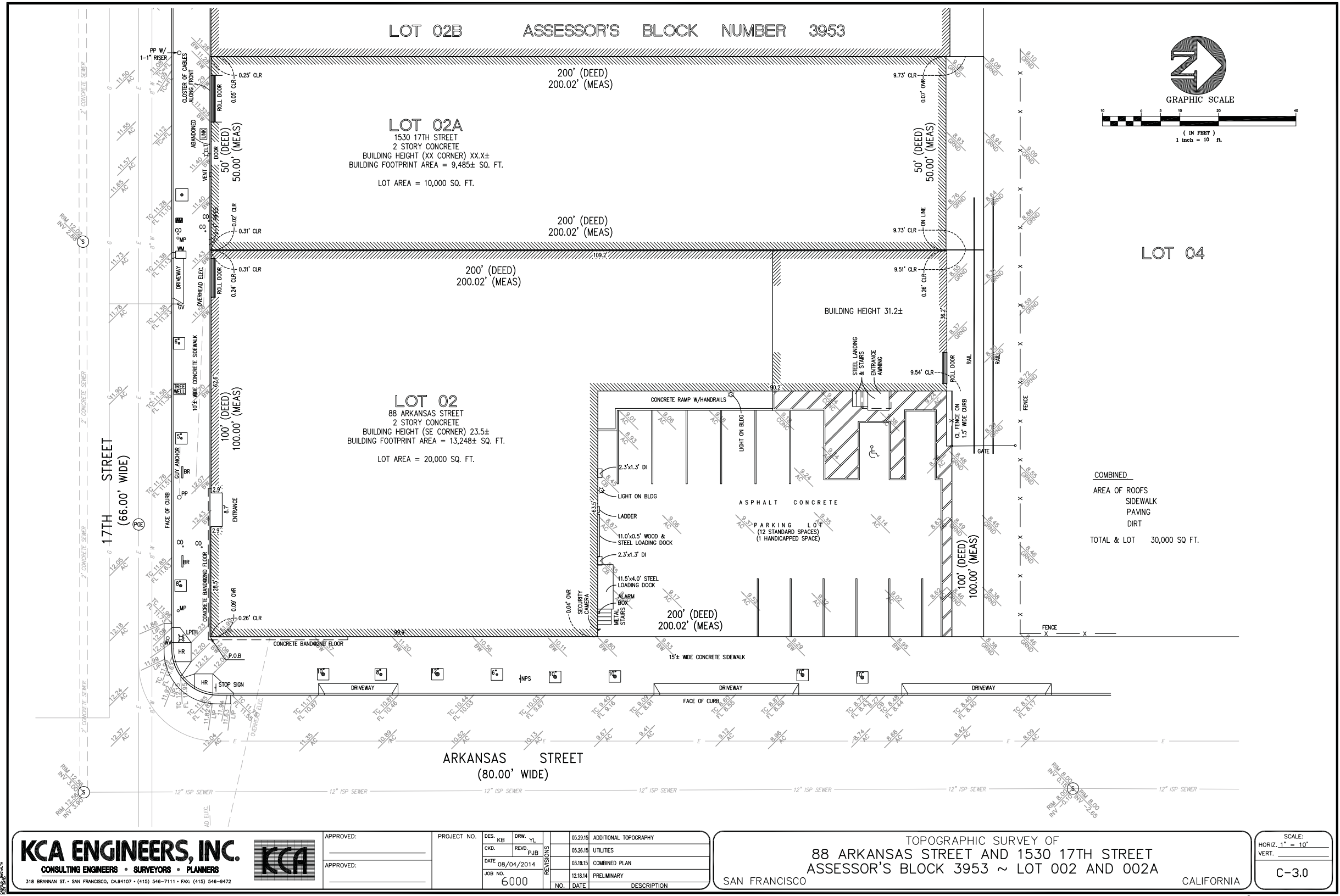
Site Aerial & Context

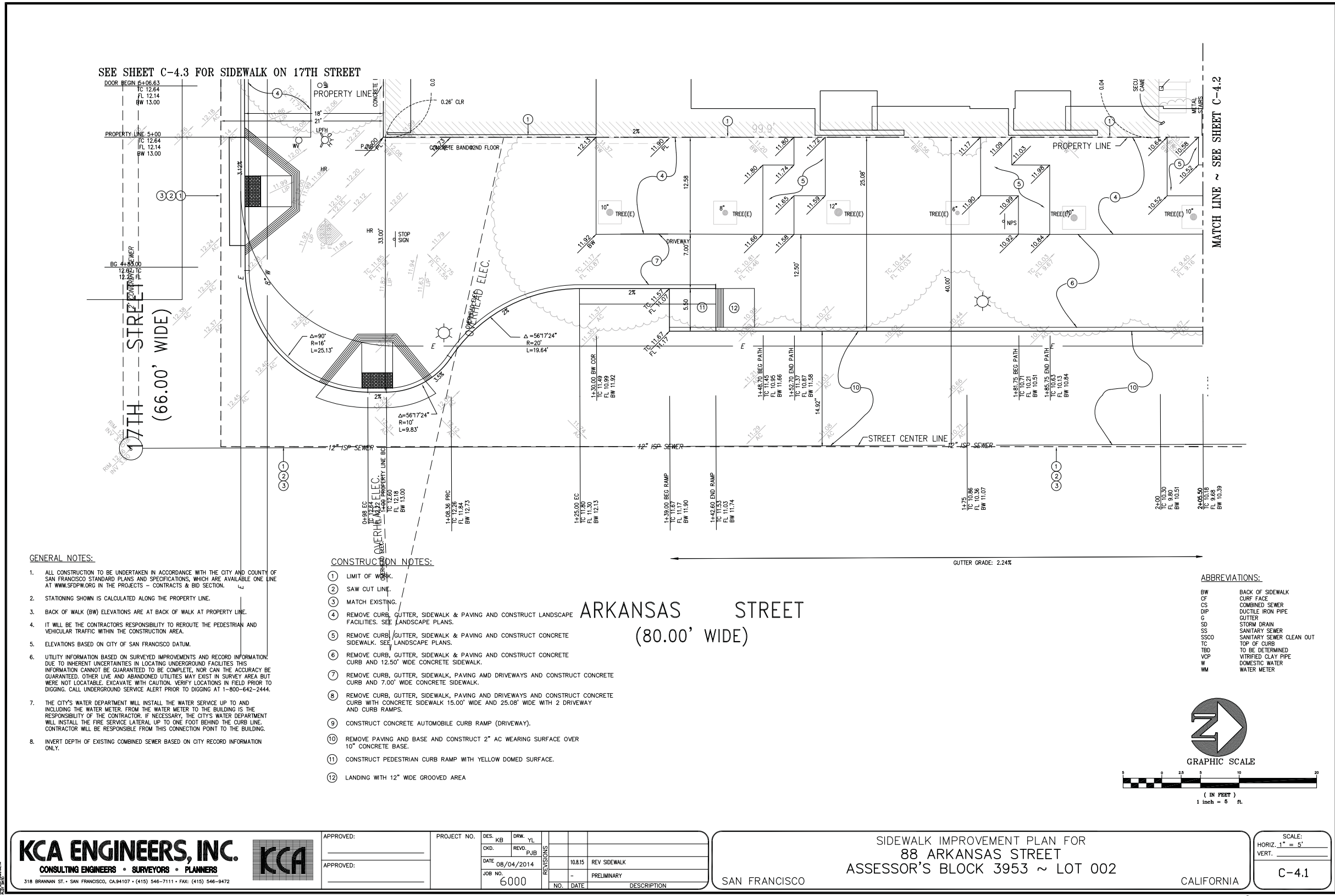


6. 17th between Arkansas and Wisconsin Streets, looking south



7. Arkansas Street, looking south





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SIDEWALK IMPROVEMENTS 1

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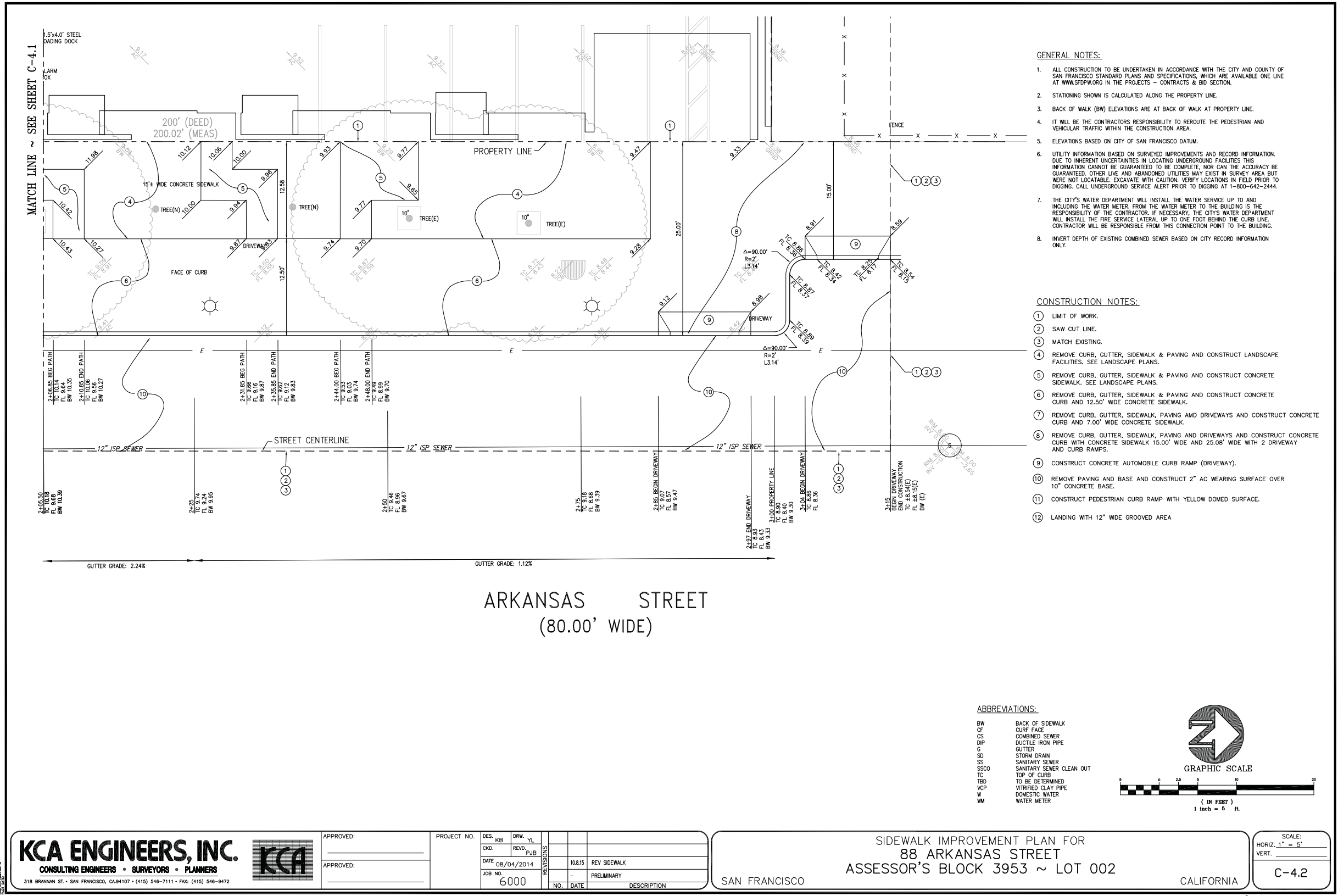
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SIDEWALK IMPROVEMENTS 2

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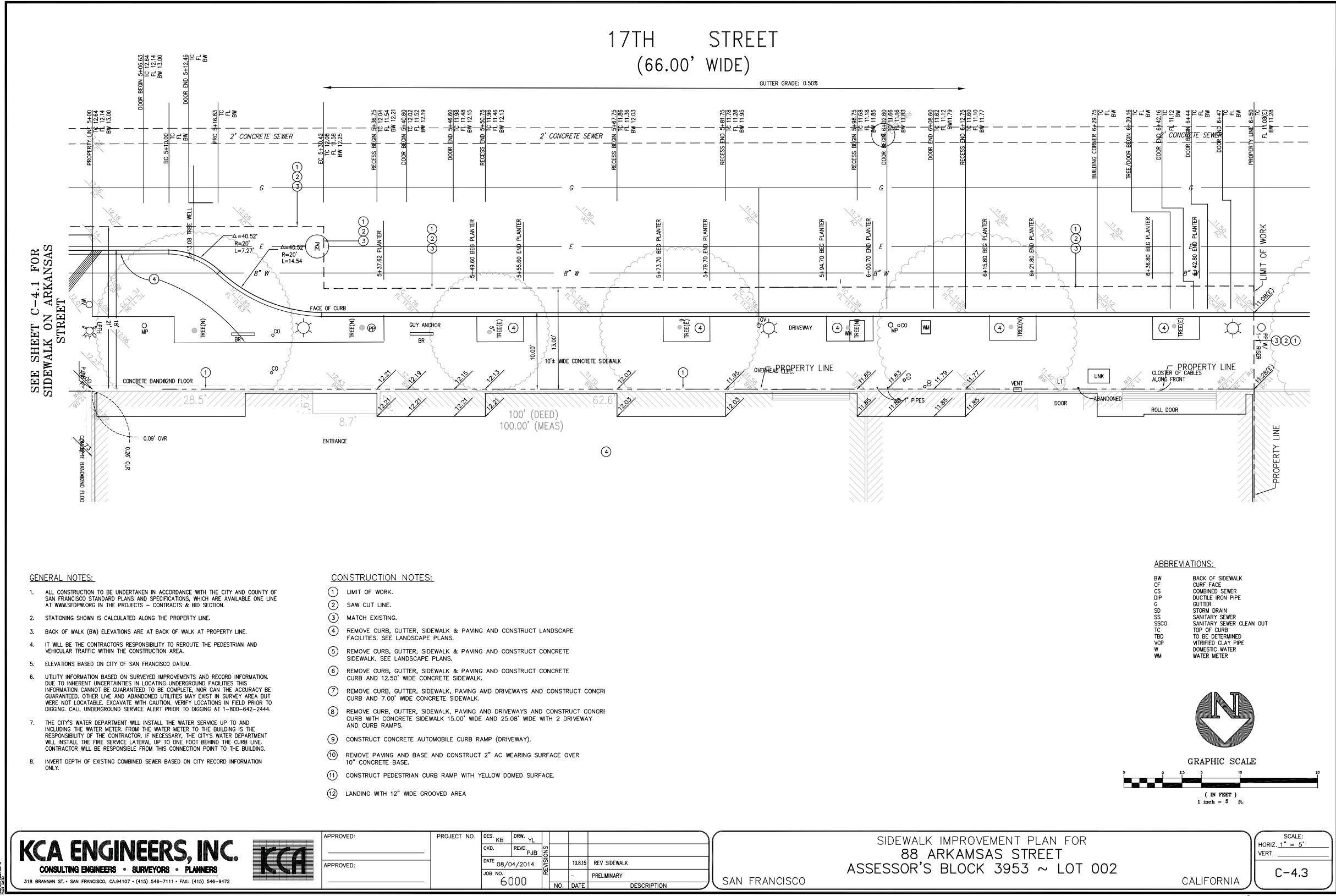
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SIDWALK IMPROVEMENTS 3

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LANDSCAPE SITE PLAN



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LANDSCAPE ROOF PLAN

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STREET TREE (to match existing)
London Plane Tree - *Platanus x acerifolia*



NEW ZEALAND FLAX
Phormium sp.



NATIVE GRASS - Siskiyow Blue Fescue
Festuca idahoensis 'Siskiyow Blue'



NATIVE PERENNIAL
London Plane Tree - *Platanus racemosa* -



NATIVE PERENNIAL- Foothill Penstemon
Penstemon 'Margarita BOP'



NATIVE ANNUAL - California Poppy
Escholzia californica 'Apricot Chiffon'

STREETSCAPE PLANT PALETTE



TREE - FLOW-THROUGH PLANTERS
River birch - *Betula nigra*



NATIVE RUSH - FLOW-THROUGH PLANTERS
California Rush - *Juncus patens* 'Elk Blue'



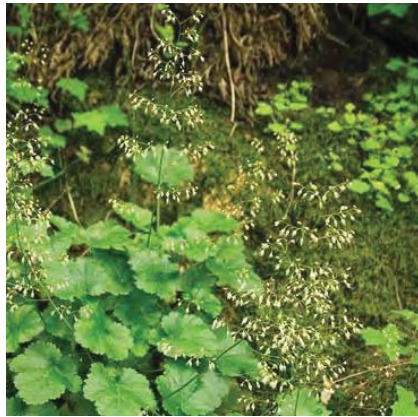
NATIVE ACCENT TREE - Western Redbud
Cercis occidentalis



NATIVE EVERGREEN SHRUB - Bush Mallow
Carpenteria californica 'Elizabeth'



EVERGREEN GRASSLIKE - Dwarf Mat Rush
Lomandra longifolia 'Breeze'



NATIVE PERENNIAL
Alum Root - *Heuchera micrantha*

COURTYARD PLANT PALETTE



FIESTA NEW ZEALAND FLAX
Phormium 'Jester'



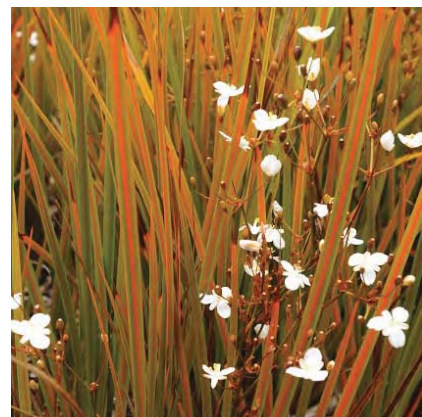
Blonde Ambition Gramma Grass
Bouteloua gracilis 'Blonde Ambition'



Asparagus Fern
Asparagus densiflorus 'Myers'



Aeonium
Aeonium 'Carol'



Orange Libertia
Libertia peregrinans



Golden Sedum
Sedum rupestre 'Angelina'

ROOFTOP PLANT PALETTE

17TH STREET



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BASEMENT LEVEL





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SECOND FLOOR PLAN



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THIRD & FOURTH FLOOR PLAN

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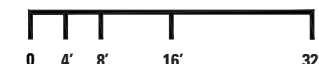
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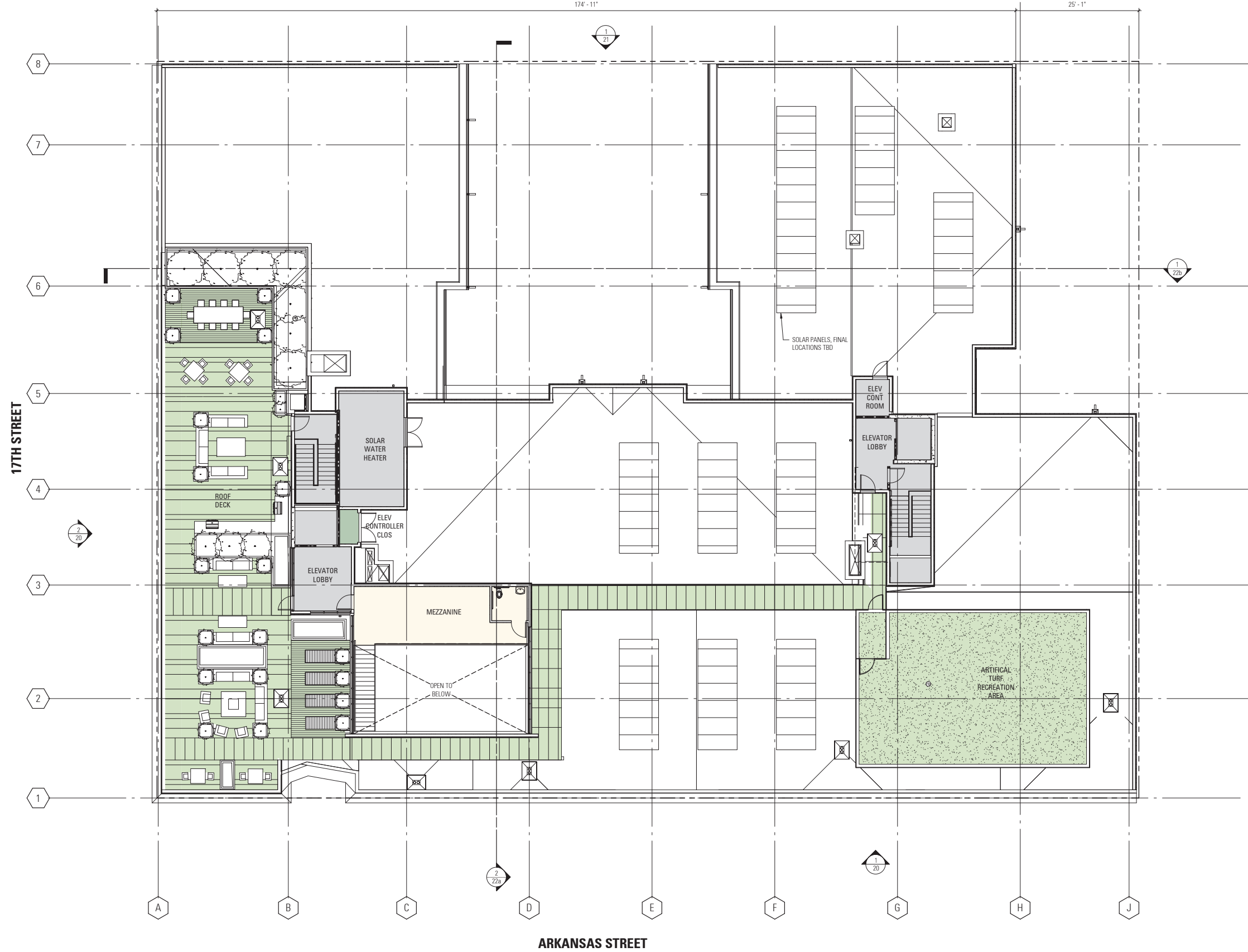




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FIFTH FLOOR PLAN



ARKANSAS STREET

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ROOF PLAN



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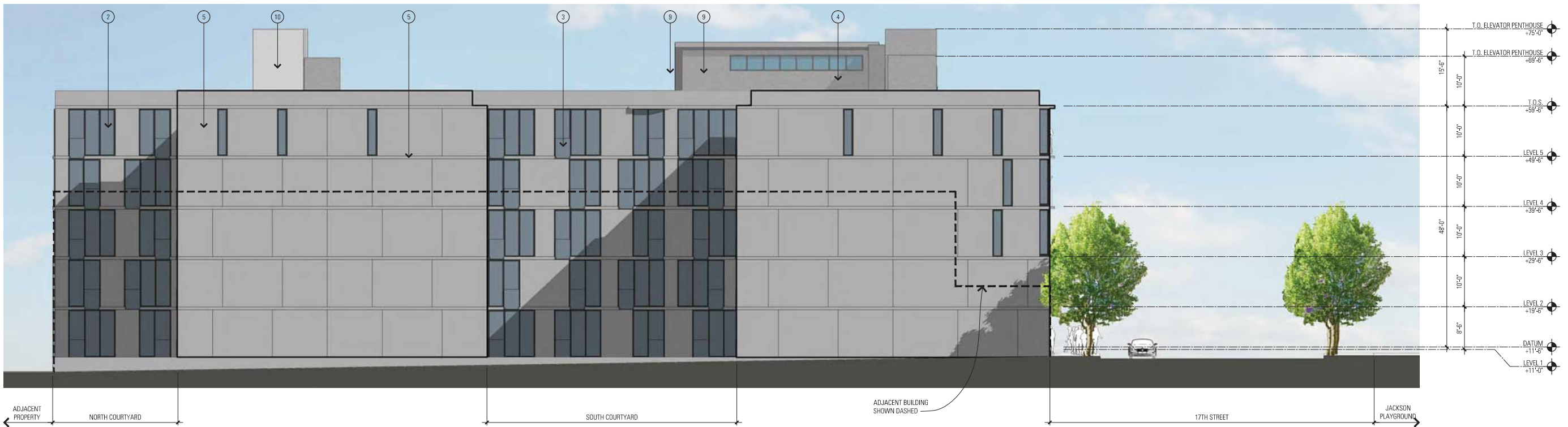


20



- MATERIALS KEY**
- 1. CONCRETE COLUMN
 - 2. ALUMINUM WINDOW
 - 3. TEMPERED GLASS GUARDRAIL
 - 4. STOREFRONT WINDOW SYSTEM
 - 5. CEMENT PLASTER/CEMENT BOARD
 - 6. CHANNEL GLASS
 - 7. SPANDREL GLASS
 - 8. METAL CANOPY
 - 9. SLOPED ROOF
 - 10. EXPOSED CONCRETE SLAB
 - 11. ARCHITECTURAL CONCRETE
 - 12. GARAGE DOOR
 - 13. SKYLIGHT

2
21 NORTH ELEVATION



1
21 WEST ELEVATION

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ELEVATIONS

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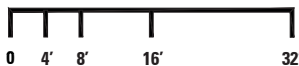
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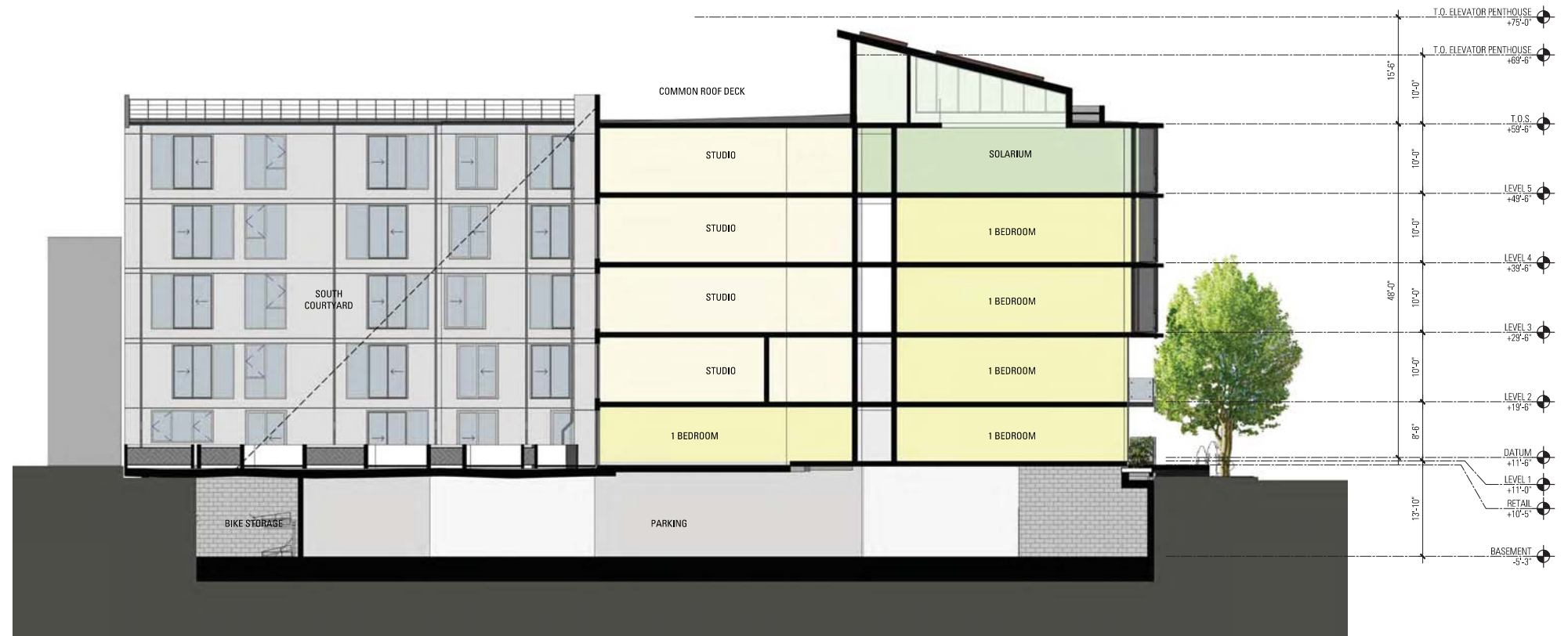
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B
22 NORTH/SOUTH SECTION THROUGH COURTYARDS



A
22 EAST/WEST SECTION THROUGH SOLARIUM

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BUILDING SECTIONS

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PERSPECTIVE AT 17th & ARKANSAS

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STOOP PERSPECTIVE



BIRDS EYE PERSPECTIVE

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STOOP PLAN



STOOP ELEVATION

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STOOP PLAN & ELEVATION

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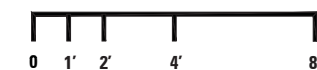
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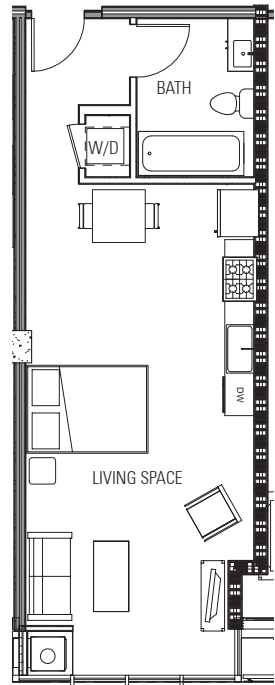
STOOP SECTION @ WALKWAY



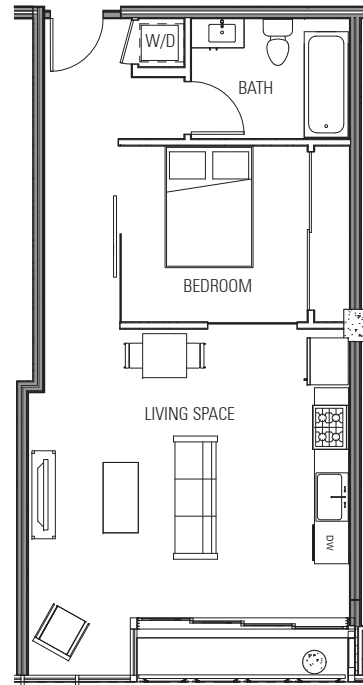
STOOP SECTION @ PLANTER

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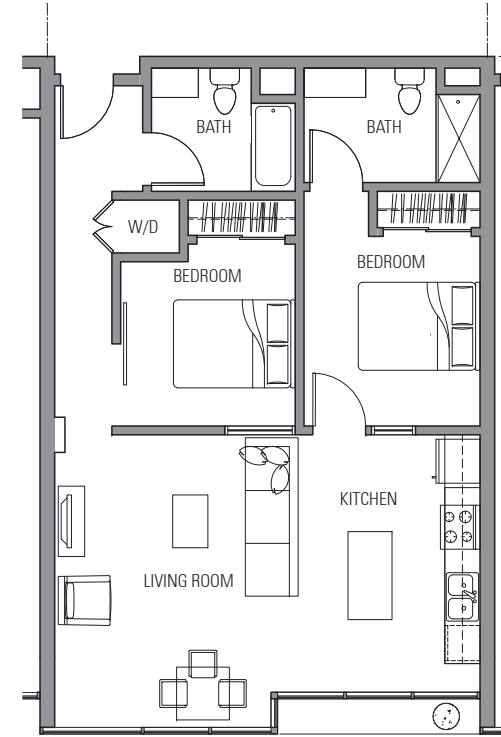
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TYPICAL STUDIO



TYPICAL 1 BEDROOM



TYPICAL 2 BEDROOM

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