



SAN FRANCISCO PLANNING DEPARTMENT

MEMO

DATE: September 14, 2016
TO: Architectural Review Committee of the Historic Preservation Commission
FROM: Eiliesh Tuffy, Historic Preservation Technical Specialist, (415) 575-9191
REVIEWED BY: Tim Frye, Historic Preservation Officer, (415) 575-6822
RE: **Review and Comment: Teatro ZinZanni**
Case No. 2015-016326COA

1650 Mission St.
Suite 400
San Francisco,
CA 94103-2479

Reception:
415.558.6378

Fax:
415.558.6409

Planning
Information:
415.558.6377

BACKGROUND

The Planning Department (Department) has requested review and comment before the Architectural Review Committee (ARC) regarding the proposal to construct a new 4-story hotel with associated theater and adjacent, publicly-accessible open space for the Teatro ZinZanni dinner theater company. The subject property consists of two Seawall Lots currently owned and managed by the Port of San Francisco. The project proposes new construction within the Northeast Waterfront Landmark District, which is listed in Appendix D of Article 10 of the San Francisco Planning Code (1983). The project site fronts onto the Embarcadero and is across the street from the Port of San Francisco Embarcadero National Register District (2006).

PROPERTY DESCRIPTION

The triangular-shaped project site includes two lots bounded by Davis Street to the west, Broadway to the south, and The Embarcadero to the east. Currently the land is used for surface parking. The project site is located within a C-2 (Community Business) Zoning District with a 40-X Height and Bulk Limit.

PROJECT DESCRIPTION

Hotel and Theater – Preferred Proposal (Plans: pages 6-17)

The project proposes to construct a 4-story-over-basement hotel with 180 rooms. The hotel's passenger loading area and main entrance is proposed to be located on Broadway. The north end of the building would terminate in a new theater structure, designed to enclose the theater company's historic, circular spiegel tent. The hotel's loading dock is proposed to be located along the Davis Street elevation.

Publicly-Accessible Open Space (Plans: pages 18-19)

The northernmost end of the project site is proposed to be developed as a privately owned, publicly accessible open space (POPOS) with connections through the property to the Embarcadero, creating a link to the waterfront.

In addition to the Historic Preservation Commission (HPC), the proposed project requires review by the Port's Waterfront Design Advisory Committee, the Port Commission, Planning Commission, and the Board of Supervisors. The HPC shall review the proposed project as part of a Certificate of Appropriateness (Planning Code Section 1006), as the project includes new

construction within the Northeast Waterfront Landmark District. The Planning Commission shall review the proposed project as part of the Conditional Use application for a hotel, and will review the proposal for consistency with the City's General Plan Objectives and Policies.

ENVIRONMENTAL REVIEW

The proposed project is currently undergoing environmental review.

APPENDIX D OF ARTICLE 10

The Northeast Waterfront Landmark District is locally designated in Article 10 of the San Francisco Planning Code (Appendix D). Contributing buildings within the boundaries of the roughly 9-block district are significant for their architecture, serving as outstanding examples of 19th-century maritime warehouses and early 20th-century industrial buildings from the district's identified period of significance: 1850s – 1940s.

The designation ordinance, under Section 1. Findings and Purposes, states the intention for new development to "maintain the scale and basic character of the Northeast Waterfront Historic District, through:

(c) Encouragement of the development of vacant and incompatible developed properties in accordance with the character of the area.

Character-defining features of the district include:

General:

- 19th century brick structures, of large bulk with minimal fenestration
- early 20th-century (post-1906) reinforced concrete structures, of plain design or with scaled-down Beaux Arts forms
- consistency of scale and proportion, materials, color and texture
- extant cobblestone paving (John Maher Street) and belt line railroad track (Green Street)

Overall Form:

- 4-6 story building height (lower buildings near the water)

Scale and Proportion:

- large in bulk
- regularity of form
- large ground floor openings, historically designed for easy vehicular access

Fenestration:

- deeply recessed, with strong shadow lines
- early structures: few windows; varied sizes; rhythmically spaced
- 1920s and later: industrial sashes; massive door openings for transferring goods

Materials/Color/Texture:

- brick (red is typical; 1 unpainted blonde brick building)
- reinforced concrete and stucco
- painted brick and concrete in muted earth tones
- rough-textured

STAFF ANALYSIS & RECOMMENDATIONS

The Department seeks the advice of the ARC regarding the compatibility of the new construction with the surrounding landmark district as defined by Secretary of the Interior's Standards for Rehabilitation (Secretary's Standards) and Article 10 of the San Francisco Planning Code.

Staff reviewed the compatibility of the Preferred Proposal (Plans: pages 6-19) based on:

- the Secretary of the Interior's Standards
- Appendix D to Article 10 of the Planning Code
- the character-defining features found on buildings within the district boundaries
- the character-defining features found on buildings constructed during the district's period of significance

Sponsor Packet – Visual Compatibility Analysis (Pages 20-35)

While contemporary buildings and alterations are referenced in the sponsor's submittal, staff did not consider these in its review.

Secretary of the Interior's Standards for Rehabilitation & Appendix D of Article 10

Department staff will undertake a complete analysis of the proposed project per the applicable Standards as part of the environmental review and the subsequent preservation entitlements (Certificate of Appropriateness). In addition, Department staff will undertake additional analysis of the proposed project per the standards outlined in Appendix D of Article 10, specifically to assess the project's conformance to the guidelines for new construction and compatibility within the surrounding landmark district.

Overall Form & Continuity, Scale & Proportion

General Site Plan and Open Space (Site Plan: page 6; Renderings: pages 36-42)

Due to the nature of the project site, the new construction will have three publicly-visible street elevations as well as a publicly-visible northern terminus, as viewed from the new landscaped open space. The intersection of Broadway and The Embarcadero lies at the southeast corner of the district boundaries and serves as a gateway into the Northeast Waterfront Landmark District.

Hotel Building

Buildings in the district exhibit consistency of scale and proportion, and a regularity of form. The historic warehouses and industrial buildings are blocky in their overall form and massing, with facades built out flush to the property line. Setbacks and arcades are identified in Sec. 7(b)(1) as unacceptable features for projects seeking a Certificate of Appropriateness in the Northeast Waterfront Landmark District. Overall, the Department believes that the 4-story scale of the hotel massing is compatible with lower-height buildings near the water, as outlined in the features of the district.

Recommendation:

The continuity of the façade and overall sense of large bulk is disrupted by the introduction of full-height glazed setbacks. As proposed, the glazed vertical breaks in the façade do not reference character-defining fenestration in the district such as industrial steel sash windows. Also, the only instance of a contemporary glass bridge connector is over Icehouse Alley, where one was approved in 1992 as a cross-alley link between two

separate buildings. Therefore full-height, glazed portions of the façade should be limited to applications such as a possible connector between the hotel and theater building, or in applications where it would aid in the creation of pedestrian-level visual connections to the Embarcadero.

Recommendation:

The design proposes a recessed, 3-bay arcade on the Broadway elevation and a recessed ground floor corner at Broadway & The Embarcadero, in conflict with Section 7(b)1 of the designating ordinance. The Department recommends removal of the nonconforming Broadway arcade in favor of a primary building entrance more in keeping with those found on contributing buildings in the district. Often primary building entrances are identified through more ornate treatment of the cladding material at the entrance surround. The building corner at Broadway and The Embarcadero has a high level of public visibility due to its proximity to a wide intersection. Historic buildings in the district have strong building corners that solidly meet the ground. The proposed recess at the base of the building corner should be removed, as it creates an arcade condition in conflict with Section 7(b)(1) of the ordinance and is out of character with the district.

Theater Building

The round theater building, which is designed to follow the form of the historic spiegelteint it encloses, does introduce a new building form not otherwise found in the district. When viewed on balance with the compatible brick cladding and the proportions of an industrial-style window for the fenestrated openings, it was determined to be one approach towards differentiating the new construction from the surrounding historic buildings.

Recommendation:

The sponsor's submittal includes an alternate design for a glass roof on the theater building (see the "Alternate Studies" section, pages 43-49). A glass roof would further differentiate the theater as new construction, however it could result in greater levels of rooftop illumination than that of the copper-shingle roof proposal.

Fenestration

Overall, the Department finds that the combination of a brick exterior with large rough openings and industrial-style window sash is a successful blending of character-defining features from both the early maritime and post-1920s phases of the district's development.

Recommendation:

While large openings are common on the ground floor of historic warehouses and industrial buildings, they typically have lintels that are either flat or slightly arched. Arches that spring from grade level, and recessed arcades are not characteristic of the district. The design of the ground floor fenestration should eliminate the full-height arches, incorporate a compatible lower bulkhead design and remove any exterior arcades from the floor plan to adhere to Section 7(b)1 of the designating ordinance. Window sashes, if drawing from the industrial-style fenestration in the district, should have muntin patterns and operating mechanisms compatible with historic industrial doors and windows found in the district.

Materials, Color and Texture

Brick is a building material commonly used in the construction of early maritime-era warehouses in the district. For brick buildings, red clay is typical. Texture is created visually through mottled brick coloration or variation in coursing. Texture is also created in plane through projecting trim molds at rough openings and structural bays, quoining, horizontal belt courses and outward-stepped cornices. Roofing material is rarely visible from street-level, as the historic buildings tend to have flat roofs obscured by either brick or stucco parapets.

Recommendation:

While brick was favorably received for its compatibility as a material, the proposed blonde brick has only one precedent in the district, on a smaller mid-block building at 55 Union Street. Because of its rarity in the district, blonde brick was not viewed as the most compatible choice for a large new construction project. In keeping with the character-defining features outlined in the designation ordinance, the Department determined that red brick is a more compatible material color for the exterior cladding. Additional texture should be incorporated into the design, drawing from character-defining features such as pilasters, quoins, belt coursing and masonry cornice details to bring the new construction into further compatibility with historic brick buildings in the district.

The Project Sponsor shall provide a physical sample of the proposed material palette prior to HPC.

Detail (Entrances)

A common feature of contributing buildings within the district is the incorporation of ornament and texture around primary entrances.

Recommendation:

Particular attention should be paid to the detailing of the façade where building entrances are located. The incorporation of ornament and texture may be used to help identify the entrance portals in the overall building design, and draw from character-defining features of the district.

Detail (Awnings/Canopies)

Entrance canopies, as defined by the Planning Department, are supported by columns and are prohibited in the Northeast Waterfront district under the provisions of Planning Code Section 608.15. For the purposes of this review, the proposed entrance coverings will be referred to as “awnings.” Awnings are not identified as character-defining features in the Northeast Waterfront Landmark District. Only one known comparable awning is installed on an historic building, at 1025 Battery Street.

Recommendation:

In order to provide some level of protection from sun and weather, an awning of compatible material and design located at the primary hotel entrance bay and not extending beyond the width of the rough opening could be considered. In addition to meeting the design standards for historic resources, awnings must adhere to the limitations set forth in the Northeast Waterfront Special Sign District (Planning Code Section 608.15), which regulates attachment to the building and the depth of projection.

Detail (Storefront Systems)

Ground floors fully fenestrated using commercial storefront systems are uncommon on historic buildings in the district, which typically retain high bulkheads next to the curb. More often, large ground floor openings that historically accommodated loading docks or other shipping access have been repurposed as entrance vestibules using compatible man-door or industrial-style storefront systems installed in-plane with the exterior building wall.

Recommendation:

The design of the ground floor exterior cladding and fenestration, except at areas where building entrances occur, should allow for a continuous lower bulkhead next to the curb. In select ground floor bays, operable ground floor windows may be considered if designed in a manner compatible with historic loading and shipping bays characteristic of the district.

Detail (Cornice)

While the designation ordinance states that historic cornices in the district are simple and abstract, both parapets and cornices were traditionally constructed of the same material as building envelope and integral with the façade design. As proposed, a 44”H solid metal safety railing serves the purposes of a roof parapet and cornice for the building.

Recommendation:

The highly visible metal safety railing was determined to be an incompatible design treatment and material for a rooftop parapet in the district. Brick and stucco, the two major building materials identified in the ordinance, were used for parapet walls and cornices, as a continuation of the exterior building cladding. The proposed rooftop parapet should be either brick or stucco, consistent with the final exterior wall cladding material, and should be finished in a form and profile that relates to historic cornices in the district such as an outward-projecting profile with corbel or dentil detailing.

Detail (Rooftop Structures)

Recommendation:

Rooftop appurtenances on historic buildings in the district are characterized by regularly spaced industrial skylights. Section 7(b)3 states, “In renovation or new construction, these particular design features should be retained or incorporated.” The project should not introduce new visible rooftop features into the district, but rather should minimize rooftop structures to what is required for safe roof deck egress. Setbacks from street-facing building walls, the relationship of the finished roof height to the parapet, and massing forms in keeping with industrial skylight shapes should also be studied to ensure minimal visibility and design compatibility of any required rooftop structures.

Detail (Signage)

The subject property is located within the Northeast Waterfront Special Sign District, therefore all tenant signage must conform to Planning Code Section 608.15, which limits a tenant’s primary signage to one sign per street frontage. No projecting sign shall exceed 10 sq. ft. in area. Flush-mounted sign lettering is limited to a maximum height of 18 inch lettering. The maximum allowable installation height on the building is limited to “no higher than three feet above the top of the ceiling level of the ground story.” Smaller, secondary tenant signage is allowed per the requirements of Section 608.15(b)8.

Detail (Signage) – cont'd.

Recommendation:

Staff recommends the development of a sign program that will be submitted and reviewed separately.

REQUESTED ACTION

The Department seeks comments on:

- Compatibility of the New Construction with the Northeast Waterfront Landmark District, as it relates to Appendix D of Article 10
- Recommendations for Overall Form & Continuity, Scale & Proportion;
- Recommendations on Fenestration;
- Recommendations for Materials, Color and Texture; and,
- Recommendations for Details.

ATTACHMENTS

Appendix D of Article 10

Sponsor submittal, by Hornberger + Worstell Architects (dated September 21, 2016)

APPENDIX D TO ARTICLE 10 - NORTHEAST WATERFRONT HISTORIC DISTRICT

SEC. 1. FINDINGS AND PURPOSES.

The Board of Supervisors hereby finds that the area known and described in this ordinance as the Northeast Waterfront has a special character and special historical, architectural and aesthetic interest and value and constitutes a distinct section of the City. The Board of Supervisors further finds that designation of this area as an Historic District will further and conform to the purposes and standards of Article 10 of the City Planning Code, and that preservation on an area basis, rather than on the basis of individual structures alone, is required in order to preserve the character of the Northeast Waterfront District.

This ordinance is intended to further the general purpose of historic preservation legislation as set forth in Section 101 of the City Planning Code through:

- (a) The protection, enhancement, perpetuation and use of structures, sites and areas that are reminders of past eras, events and persons important in local, State or national history, or which provide significant examples of architectural styles of the past or are landmarks in the history of architecture, or which are unique and irreplaceable assets to the City and its neighborhoods, or which provide for this and future generations examples of the physical surroundings in which past generations lived;
- (b) The development and maintenance of appropriate settings and environment for such structures;
- (c) The enhancement of property values, the stabilization of areas of the City, the increase of economic and financial benefits for the City and its inhabitants, and the promotion of tourist trade;
- (d) The preservation and encouragement of a city of varied architectural styles, reflecting the distinct phases of its architectural, cultural, economic, political and social history;
- (e) The enrichment of human life in its educational and cultural dimensions; to serve spiritual and material needs by fostering knowledge of the past.

In addition, this ordinance is intended to maintain the scale and basic character of the Northeast Waterfront Historic District, through:

- (a) Preservation of the basic characteristics and salient architectural details of structures within the Historic District;
- (b) Affording the widest possible scope for continuing vitality through private renewal and architectural creativity, within appropriate controls and standards;
- (c) Encouragement of the development of vacant and incompatibly developed properties in accordance with the character of the area.

(Added Ord. 171-83, App. 4/8/83)

SEC. 2. DESIGNATION.

Pursuant to Section 1004 of the City Planning Code, Chapter II, Part II of the San Francisco Municipal Code, the Northeast Waterfront is hereby designated as an Historic District, this designation having been duly approved by Resolution No. 9517 of the City Planning Commission.

(Added Ord. 171-83, App. 4/8/83)

SEC. 3. LOCATION AND BOUNDARIES.

The location and boundaries of the Northeast Waterfront Historic District shall be as designated on the Northeast Waterfront Historic District Map, the original of which is on file with the Clerk of the Board of Supervisors under File No. 90-82-15, which map is hereby incorporated as though fully set forth.

(Added Ord. 171-83, App. 4/8/83)

SEC. 4. RELATION TO CITY PLANNING CODE.

(a) Article 10 of the City Planning Code is the basic law governing historic preservation in the City and County of San Francisco. This ordinance, being a specific application of Article 10, is both subject to and in addition to the provisions thereof.

(b) Except as may be specifically provided to the contrary in the ordinance, nothing in this ordinance shall supersede, impair or modify any City Planning Code provisions applicable to property in the Northeast Waterfront Historic District, including but not limited to existing and future regulations controlling uses, height, bulk coverage, floor area ratio, required open space, off-street parking and signs.

(Added Ord. 171-83, App. 4/8/83)

SEC. 5. STATEMENT OF SIGNIFICANCE.

(a) **History of the Area.** The Northeast Waterfront District contains commercial warehouse buildings from nearly every decade of San Francisco's history. The area reflects the waterfront storage and maritime activities which, until recently, were an important aspect of San Francisco business history. These buildings range in age from the early clipper ship warehouses of Scotsman Daniel Gibb in the 1850's to the properties owned by the General Engineering and Drydock Co., a company crucial to the shipbuilding effort that made San Francisco Bay the major Pacific maritime support facility during World War II.

The original shoreline of Yerba Buena swept in a curve from Montgomery Street to roughly Jackson Street and on to the deeper waters slightly east of Battery. This point was known as Punta del Embarcadero. The cove itself fronting on the settlement was shallow and could be navigated only by small craft of shallow draft. The historic district boundary begins at what was the site of the first deep water landing in San Francisco at Clark's Point. The first vessel to moor at a wharf tied up at this site and discharged cargo in 1848. The first Pacific Mail steamer to

arrive in San Francisco disembarked passengers in this area in 1848 and the unique clipper ships built expressly for San Francisco's Gold Rush trade docked in this area in large numbers.

During the Gold Rush era, the boundaries of the area were extended eastward by landfill into the Bay in order to create new warehousing space. Large sections of Telegraph Hill were excavated for the fill. In the late 1880's a sea wall was built which settled the eastern boundaries of the area. The clipper ships of the Gold Rush era and paddlewheel steamers of the delta and river trade docked alongside its wharves discharging a multitude of cargo, much of which passed through local warehouses. In the 1880's and 1890's large numbers of iron-hulled square riggers were involved in hauling grain from California's Central Valley to European markets. The new transcontinental trains were loaded on barges from the other side of the Bay and were docked and discharged in this area. A number of ships are presumed to have been buried beneath the fill used to extend the district. A ship buried in 1851 as an extension of Frederick Griffling's wharf was recently unearthed during the excavation for Levi's Plaza on the northern boundary of the district.

Many distinguished San Francisco names and firms have been associated with the warehouses in the area: William T. Coleman head of the Vigilante Committee; George Howes, operator of clipper ships; Daniel Gibb, builder of the earliest warehouses in the area which were bought in 1861 by John Sanborn; Charles Minturn, steamboat tycoon; Isaac Friedman, the "Wheat King"; and Samuel Haslett, founder of a leading warehouse firm in the Bay Area. Pioneer firms such as American Bisquit Co., Cowell's Lime and Cement Co., W.P. Fuller Co., the Petri Italian-American Cigar Co., and the California Fruit Packing Co., have major extant structures in the area. After 1906 firms such as Bemis Bag Co., National Ice and Cold Storage, Cudahy Meat Packing, Armour and Co., Ciocca-Lombardi Wine Co., and the Italian-American Canning Co. located in the area. In the 1920's and 1930's the Sunset Press, Security Lithograph, and various typography and lithography operations were set up in the district, reflecting the increasing importance of this industry in San Francisco's economic development. In 1940 General Engineering and Drydock marshalled a number of buildings for ship repair and maintenance during World War II.

These warehouse facilities have been in continuous industrial use from the Gold Rush to the mid 1960's. Since that decade showrooms, office and retail uses have been integrated into renovated warehouse structures.

The area is architecturally significant as a representation of warehouse and industrial buildings from the brick structures of the Gold Rush era to the reinforced concrete buildings introduced after the turn of the century. Some of the present buildings date from well before the turn of the century, though they were rebuilt after the 1906 fire. Since warehouse architecture did not undergo profound stylistic changes until the introduction of reinforced concrete, the pre- and post-fire brick warehouses embody the original appearance and spirit of the early warehouse district. Of particular note is the block bounded by Front, Battery, Union, and Green streets, the most cohesive extant brick warehouse complex from this era in the City. In addition, cobblestone paving and the standard and narrow gauge belt line railroad track which served the district are visible at the foot of Commerce Street. The area clearly serves as a visual reminder of San Francisco's earlier maritime-warehousing commerce.

(b) **Basic Nature of the Area.** The Historic District includes historically and architecturally significant buildings dating from the 1850's to the present century. The buildings vary in height from approximately six stories closest to the base of Telegraph Hill to a maximum of four stories at The Embarcadero and the Bay. The area was pre-dominantly oriented toward shipping,

drayage and warehousing, and is now being converted to low rise office space. The addition of Levi's Plaza on the northern boundary offers a modern structure compatible with the district.

(c) **Architectural Character.** Common architectural features tie the area together and provide visual distinction. Two major building materials were used in the area; brick since the Gold Rush era and reinforced concrete after the 1906 fire.

Common architectural elements are repeated throughout the District. Of particular note are the large bulk and minimal fenestration of the earlier warehouse structures. Other common features are scale and proportion, materials, color, texture, and the minimal use of decorative elements. Visual distinction is due in some cases to the original construction plus the carefully designed addition of upper stories as evidenced in the original two-story Cudahy Meat Packing building by Henry Gielfus (1907) with a two-story addition by Ward and Blohme (1918).

(d) **Uniqueness and Location.** The unique quality of the Northeast Waterfront Historic District stems from the fact that so many buildings from approximately 14 decades of San Francisco history are clustered within this nine-block area. Its distinct character is contrasted by Gateway Commons on the south and Levi's Plaza on the north. The district is close to the downtown high-rise core and the Jackson Square Historic District.

(e) **Visual and Functional Unity.** The Historic District has an identifiable character and common architectural features. The primary unifying factors are scale and the texture of the buildings, as well as materials. In terms of function, there has been a continuous transition from the clipper ship services of the 1850's through the warehousing and shipping services of the early twentieth century, to the gradual transformation of warehouses into premium office space within walking distance of the downtown.

(f) **Dynamic Continuity.** The Northeast Waterfront District is an evolving commercial area. The area has proven its capacity for incremental adaption to new uses. Designation as an historic district is intended to complement the economic viability of the area.

(g) **Benefits to the City and its Residents.** Economically, the area in the past has housed specialized enterprises and is currently evolving into a satellite office district of the downtown core. Culturally, it provides a strong historical and educational resource and link with the past. Aesthetically, its architecture and visual appeal are immediate, while its value in terms of urban design within the City pattern as a whole is equally important.

(Added Ord. 171-83, App. 4/8/83)

SEC. 6. FEATURES.

(a) **Overall Form and Continuity.** Building height is generally within a six-story range, with the higher structures closer to the base of Telegraph Hill and lower buildings near the water. Many of the oldest structures are one or two stories in height.

(b) **Scale and Proportion.** The buildings are of typical warehouse design, large in bulk, often with large arches and openings originally designed for easy vehicular access. There is a regularity of overall form. The earlier brick structures blend easily with the scaled-down Beaux Arts forms of the turn of the century and the plain reinforced-concrete structures characteristic of twentieth century industrial architecture.

(c) **Fenestration.** Minimal glazing is deeply recessed, producing a strong shadow line. The earliest structures have few windows expressing their warehouse function. They are varied in size, rhythmically spaced, and relate in shape and proportion to those in nearby buildings. Larger industrial sash windows began to be incorporated in structures built from the 1920's and onward. Door openings are often massive to facilitate easy access of bulk materials.

(d) **Materials.** Standard brick masonry is predominant for the oldest buildings in the District, with reinforced concrete introduced after the 1906 fire. Some of the brick facades have been stuccoed over. One of the structures still has its metal shutters, which were once typical of the area.

(e) **Color.** Red brick is typical, with some yellow and painted brick. Muted earth tones predominate in shades of red, brown, green, gray, and blue.

(f) **Texture.** Typical facing materials give a rough-textured appearance. The overall texture of the facades is rough-grained.

(g) **Detail.** Arches are common at the ground floor, and are frequently repeated on upper floors. Flattened arches for window treatment are typical. Cornices are simple and generally tend to be abstract versions of the more elaborate cornices found on downtown commercial structures from the nineteenth century. Most of the surfaces of the later buildings are plain and simple, reflecting their function. Some of the earlier brickwork contains suggestions of pilasters, again highly abstracted. Where detail occurs, it is often found surrounding entryways.

(h) **Age.** All of the buildings in the area were either partially or totally destroyed by the earthquake and fire of 1906. As in the Jackson Square area, many were rebuilt along the lines of the previous design, often using materials salvaged from the construction site.

(Added Ord. 171-83, App. 4/8/83)

SEC. 7. ADDITIONAL PROVISIONS FOR CERTIFICATES OF APPROPRIATENESS.

The procedures, requirements, controls and standards in Sections 1006 through 1006.8 of Article 10 of the City Planning Code shall apply to all applications for Certificates of Appropriateness in the Northeast Waterfront Historic District. In addition the following provisions shall apply to all such applications; in the event of any conflict or inconsistency between the following provisions and Article 10, those procedures, requirements, controls and standards affording stricter protection to landmarks, landmark sites and the Historic District shall prevail.

(a) **Character of the Historic District.** The standards for review of all applications for Certificates of Appropriateness are set forth in Section 1006.7 of Article 10. For purposes of review pursuant to these standards, the character of the historic district shall mean the exterior architectural features of the Northeast Waterfront Historic District described in Section 6 of this ordinance.

(b) Additional Standards for Certain Features.

(1) **Facade Line Continuity.** Facade line continuity is historically appropriate. Therefore, setbacks and arcades, not generally being features of the Northeast Waterfront Historic District, are not acceptable.

(2) **Fenestration and Design Elements for New Construction.** In areas with a concentration of older brick buildings, new construction should reflect in design the predominant use of deeply recessed, limited fenestration. In addition, verticality and a high proportion of mass to void should be incorporated. In areas characterized by newer buildings in concrete or stucco with industrial-style fenestration, new construction should reflect those design elements.

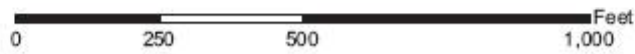
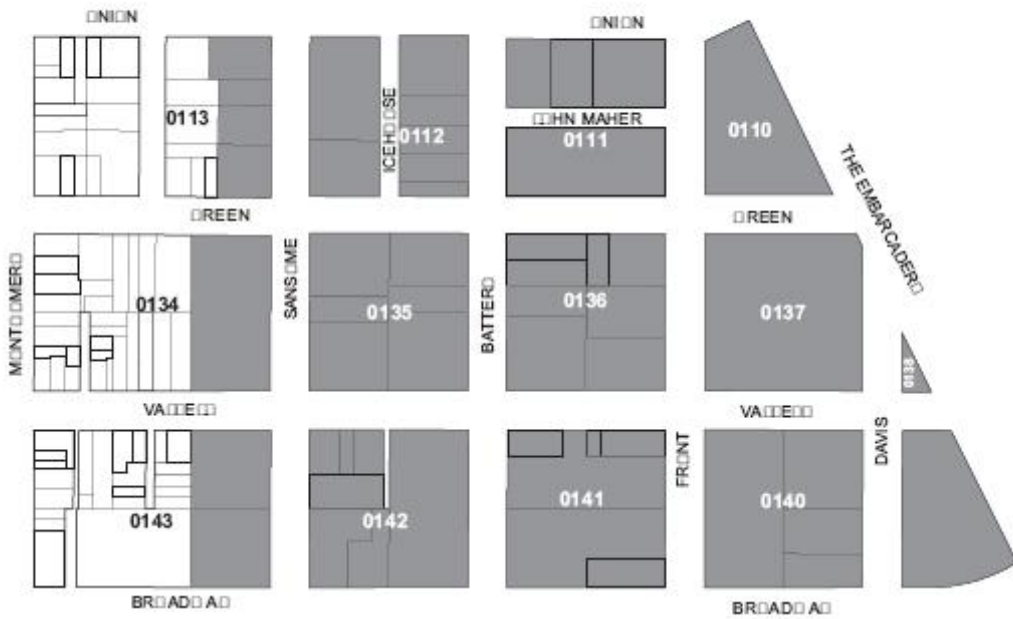
(3) **Roof Treatment.** Historically the view from Telegraph Hill over the Northeast Waterfront District has been one of roofs characterized by numerous regularly spaced industrial skylights. In renovation or new construction, these particular design features should be retained or incorporated.

(4) Signs shall be governed by the applicable provisions of Article 6, section 609.14, and this Appendix D. In the event of inconsistency among these provisions, the most restrictive provision shall prevail unless this Code specifically provides otherwise.

(c) **Exterior Changes Requiring Approval.** Exterior changes within the Northeast Waterfront Historic District shall require a Certificate of Appropriateness pursuant to the provisions of Article 10 when such work requires a city permit. In addition, a Certificate of Appropriateness shall be required for cleaning masonry surfaces with abrasives and/or treatment of such surfaces with waterproofing chemicals. Sandblasting and certain chemical treatments detrimental to older brick will not be approved.

(Added Ord. 171-83, App. 4/8/83; Ord. 59-08, File No. 031034, App. 4/10/2008)

NORTHEAST WATERFRONT HISTORIC DISTRICT





ZinZanni / Kenwood - The Theater & Hotel @ Broadway | San Francisco, California

A Teatro ZinZanni & Kenwood Investments Project

Architectural Design Review

September 21, 2016

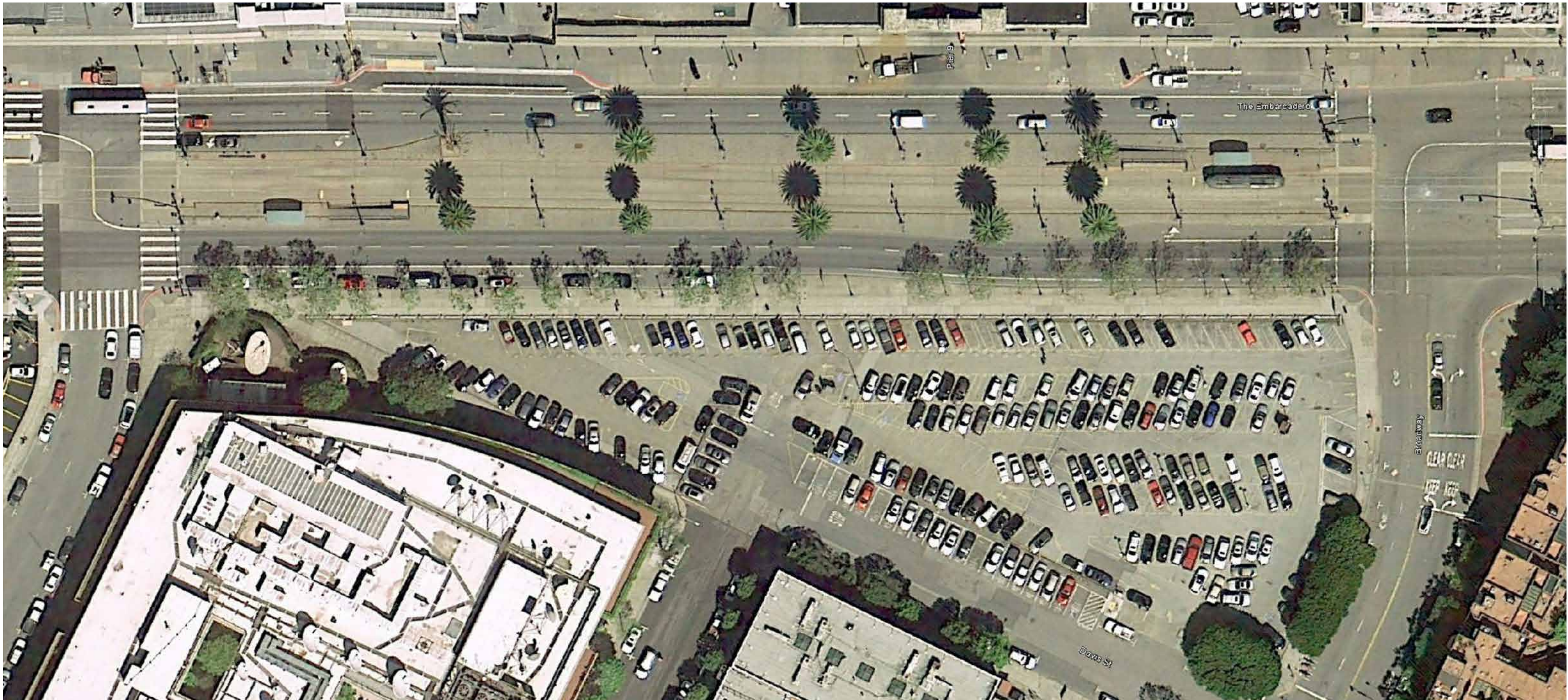


KENWOOD INVESTMENTS



Prepared for:
Teatro ZinZanni and Kenwood Investments

by:
Hornberger+Worstell Architects



Project Site / Existing Condition



Existing Site
Embarcadero Looking South



Existing Site
Davis Looking North



Existing Site
Broadway/Embarcadero Corner Looking North



Existing Site
Broadway/Davis Corner Looking East

Existing Site Images



KGO - from Davis & Vallejo



KGO - from Green St.



900 Front St. KGO - from Front & Vallejo



KGO - from The Embarcadero across project park site



Golden Gateway Commons - Broadway



Golden Gateway Commons - Broadway



Golden Gateway Commons - from The Embarcadero



Golden Gateway Commons - from The Embarcadero



The Waterfront Restaurant & Cafe - Pier 7 & The Embarcadero



Pier 9 & The Embarcadero

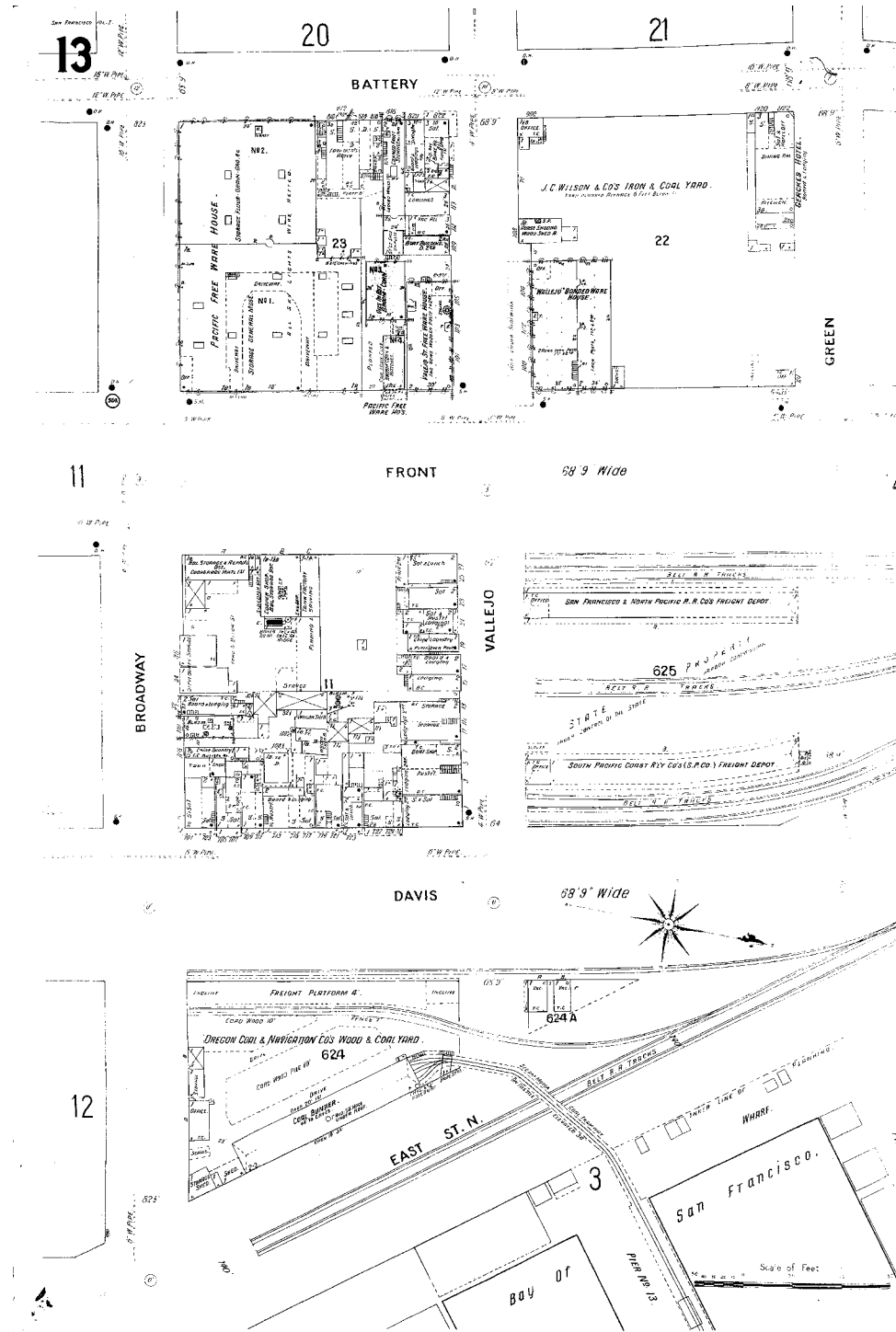


60 Broadway - from Davis St.



753 Davis - from across project site

Existing Context Images / Nearby Buildings



Sanborn Map



1500 Sansome

Historic Sanborn Map @ Project Site



ZinZanni / Kenwood - The Theater & Hotel @ Broadway | San Francisco, California
A Teatro ZinZanni & Kenwood Investments Project



KENWOOD INVESTMENTS



Prepared for:
Teatro ZinZanni and Kenwood Investments
by:
Hornberger + Worstell Architects



Broadway



The Embarcadero

Building Elevations | Previous Scheme



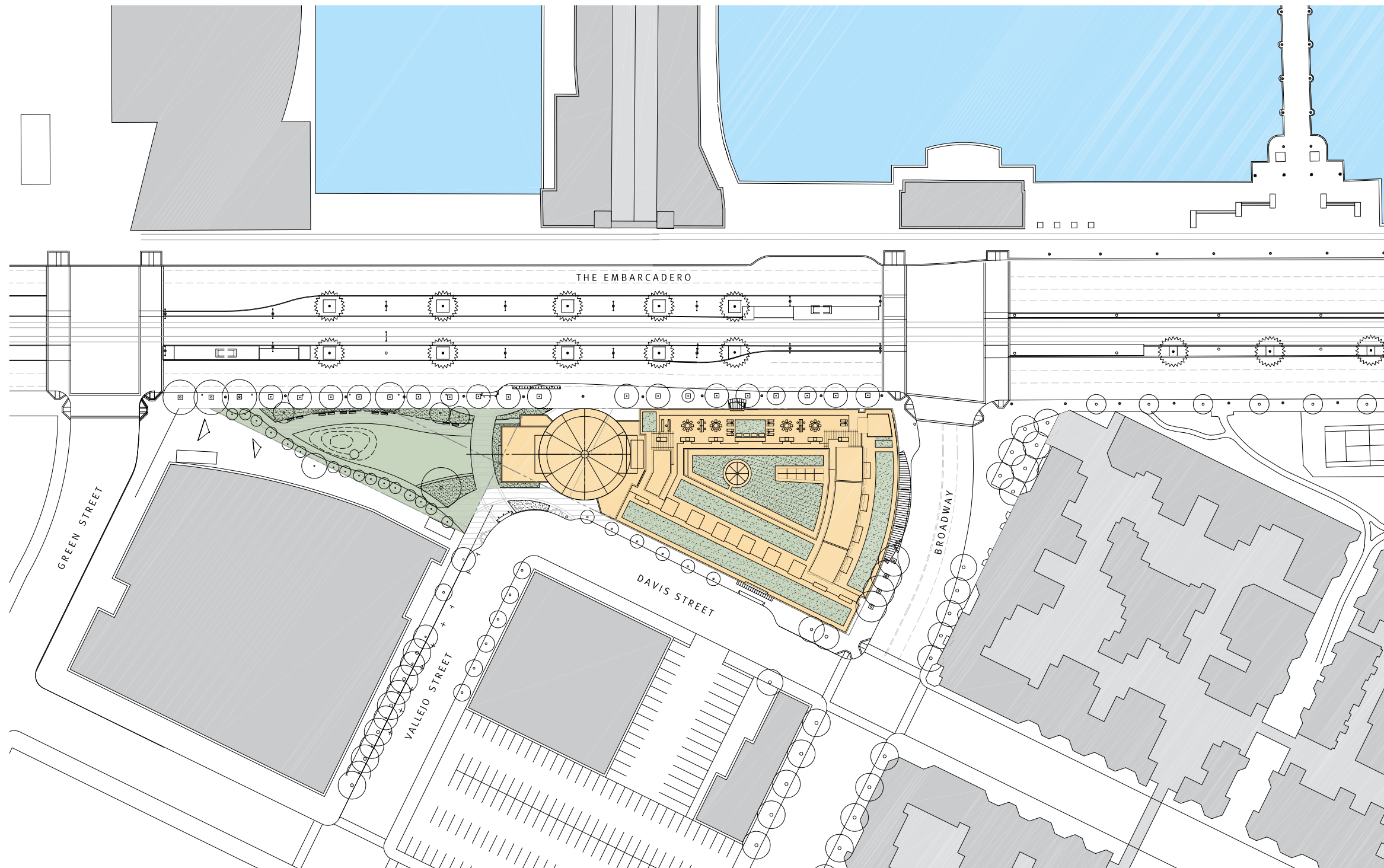
ZinZanni / Kenwood - The Theater & Hotel @ Broadway | San Francisco, California
A Teatro ZinZanni & Kenwood Investments Project



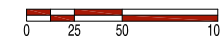
KENWOOD INVESTMENTS



Prepared for:
Teatro ZinZanni and Kenwood Investments
by:
Hornberger + Worstell Architects

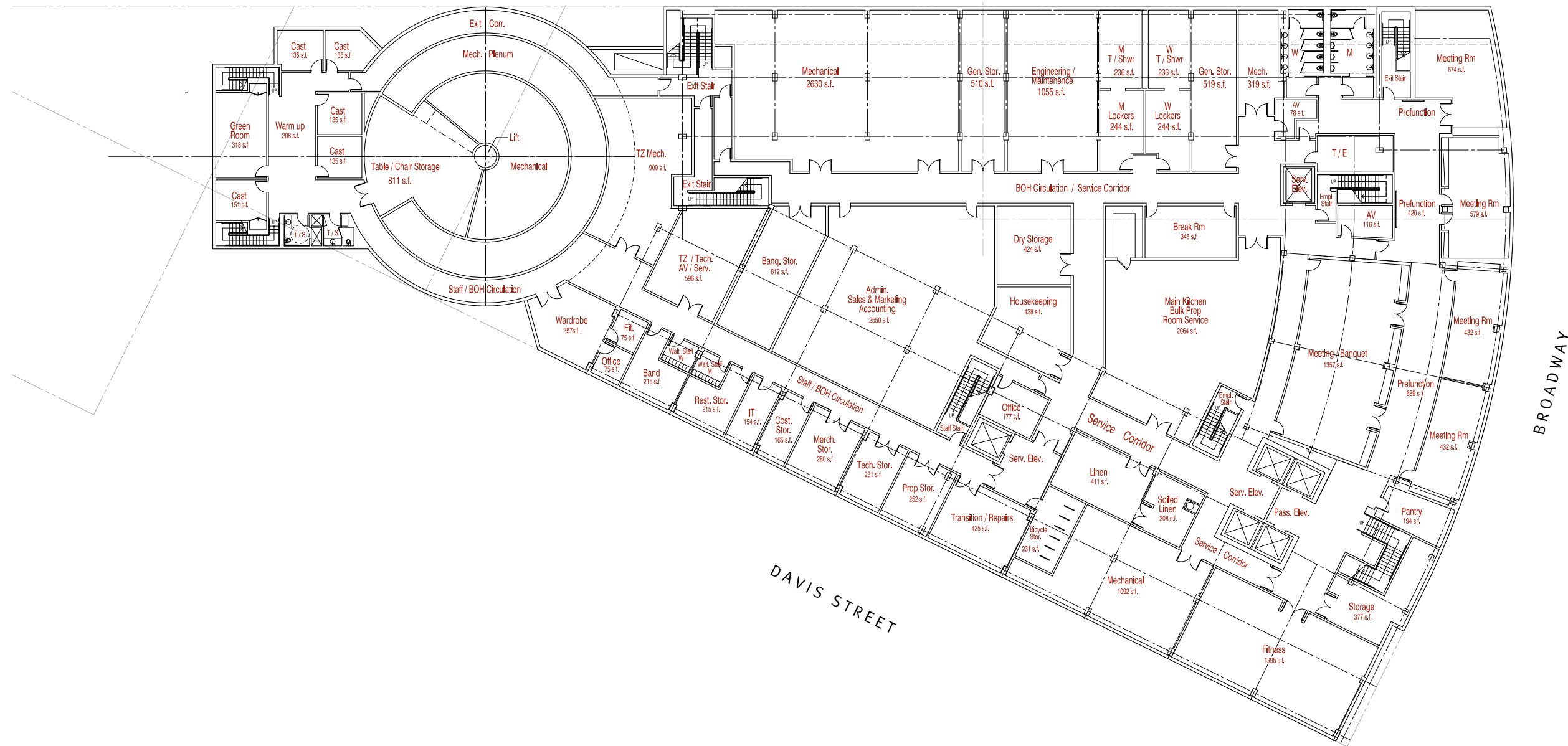


Project Area Plan

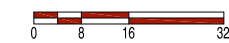


September 21, 2016 | 6

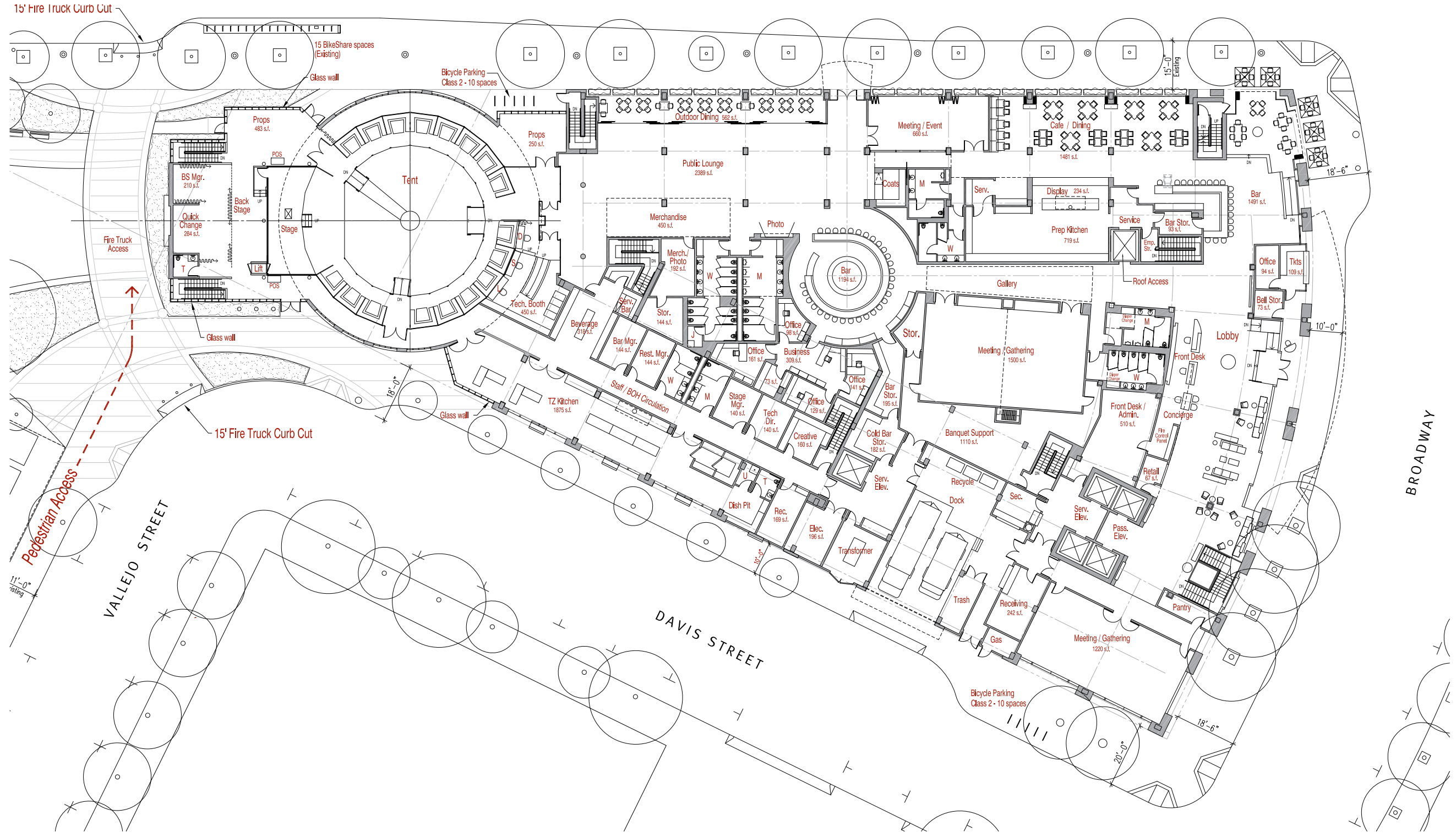
THE EMBARCADERO



Basement Floor Plan



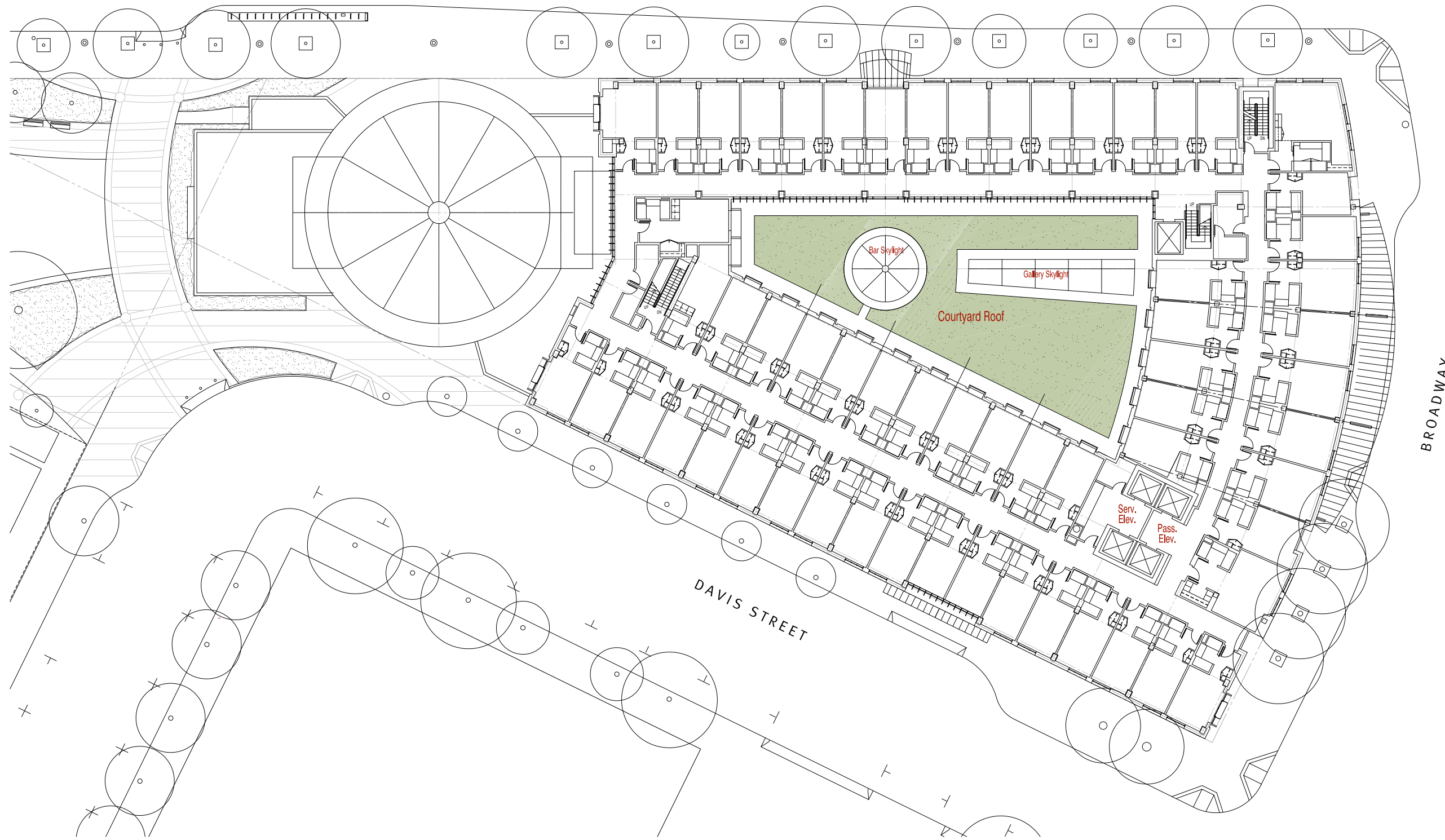
September 21, 2016 | 7



Ground Floor Plan



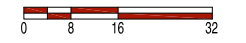
THE EMBARCADERO



BROADWAY

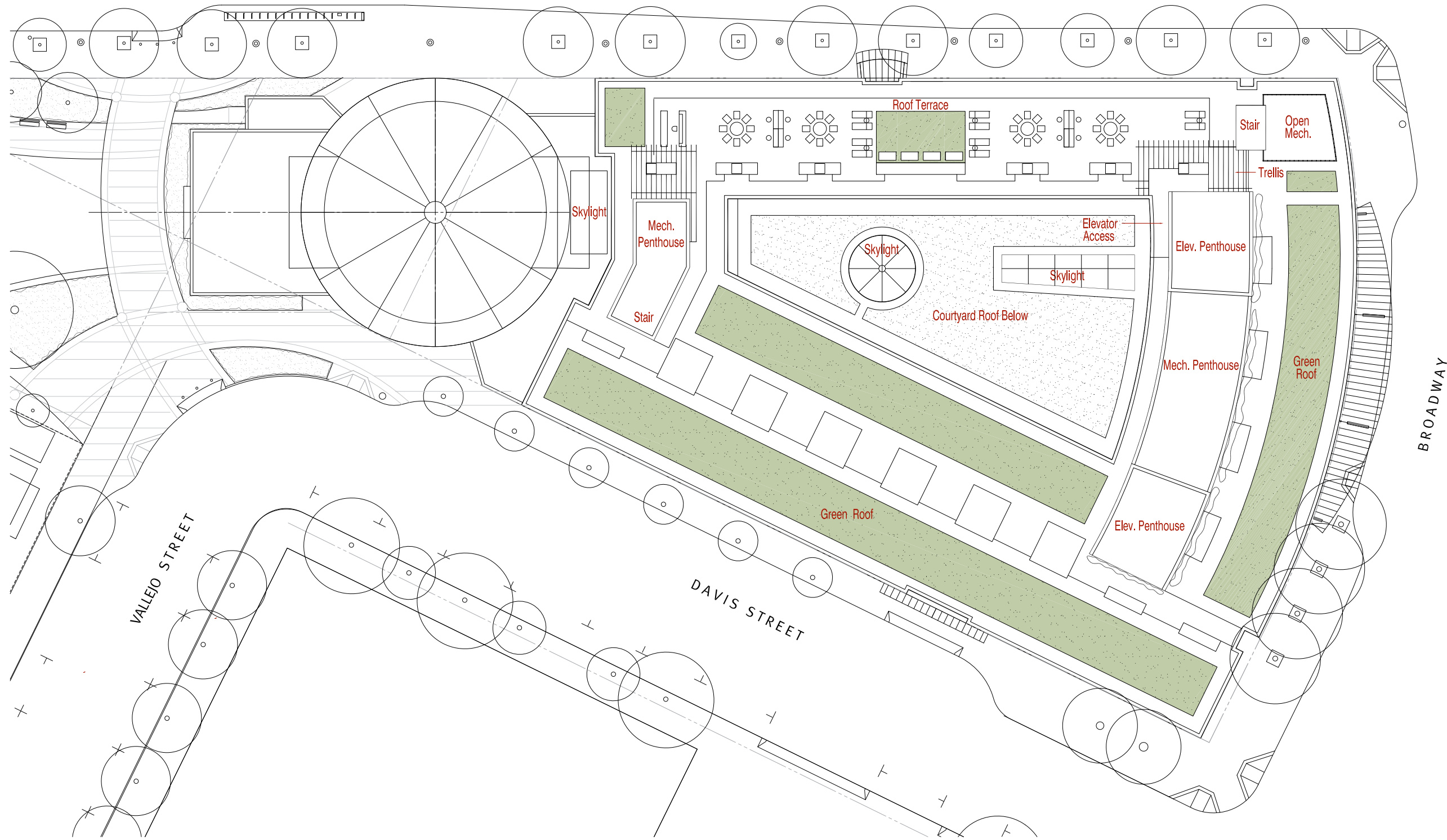
DAVIS STREET

Typical Guestroom Floor Plan



September 21, 2016 | 9

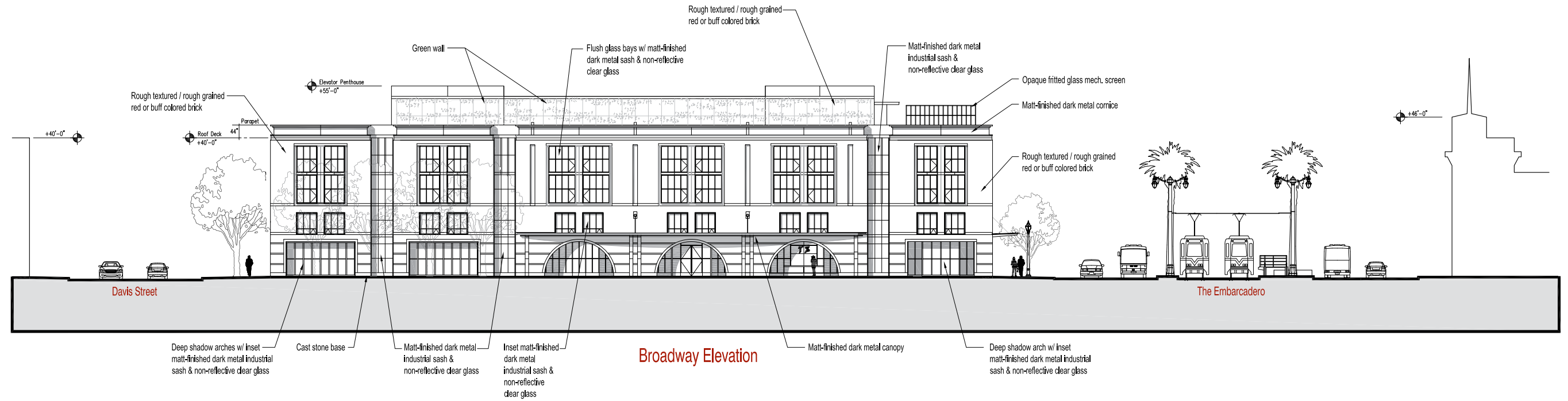
THE EMBARCADERO



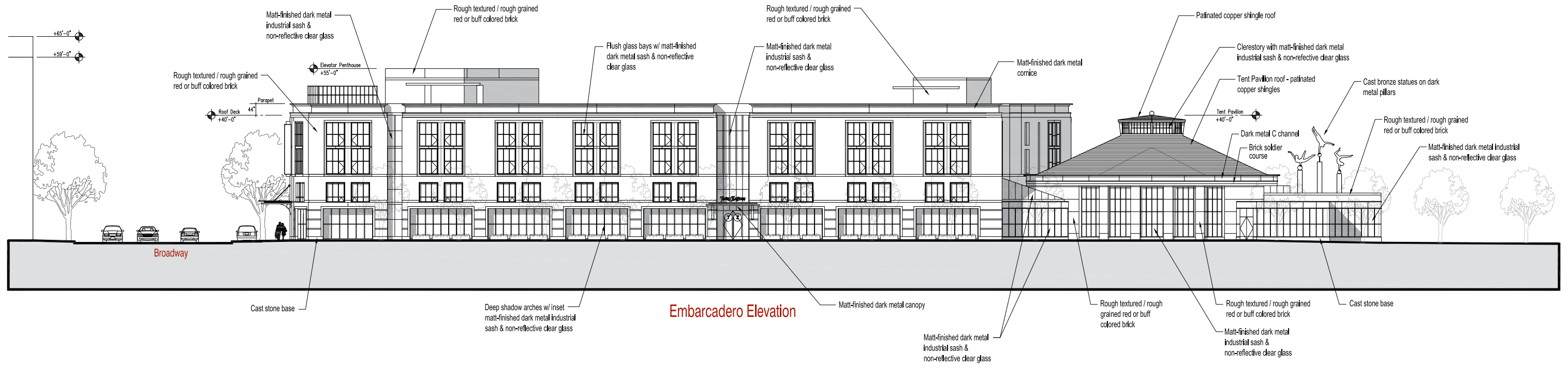
Roof Plan



September 21, 2016 | 10

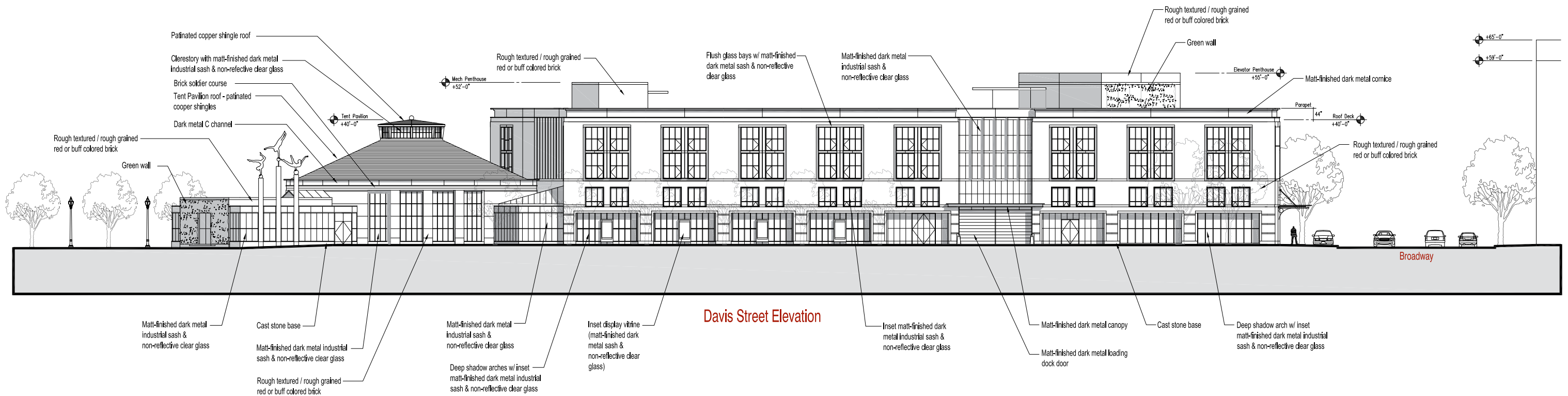
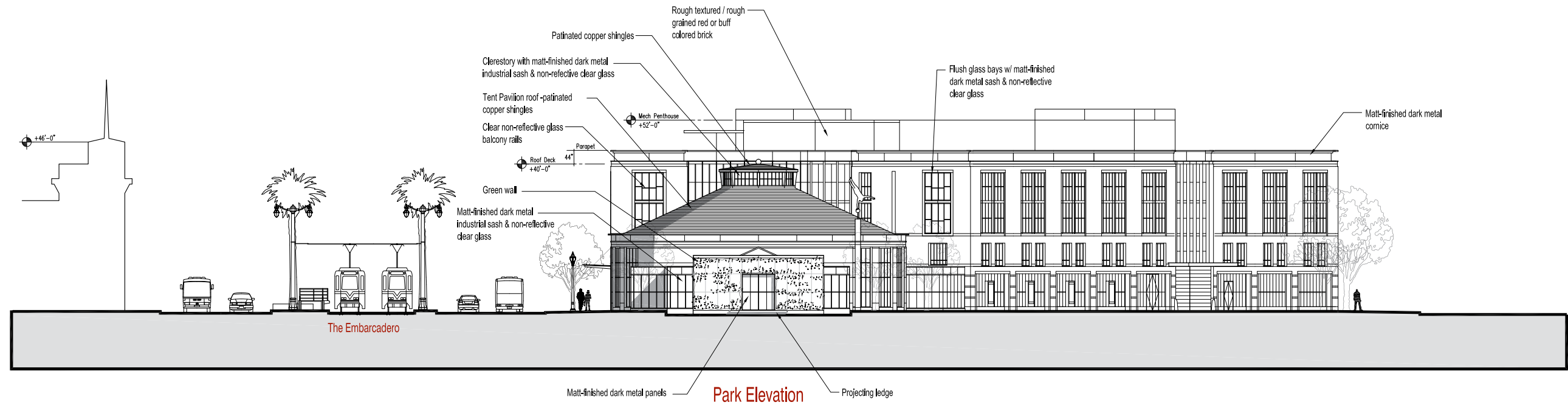


Broadway Elevation



Embarcadero Elevation

Building Elevations | Preferred Proposal



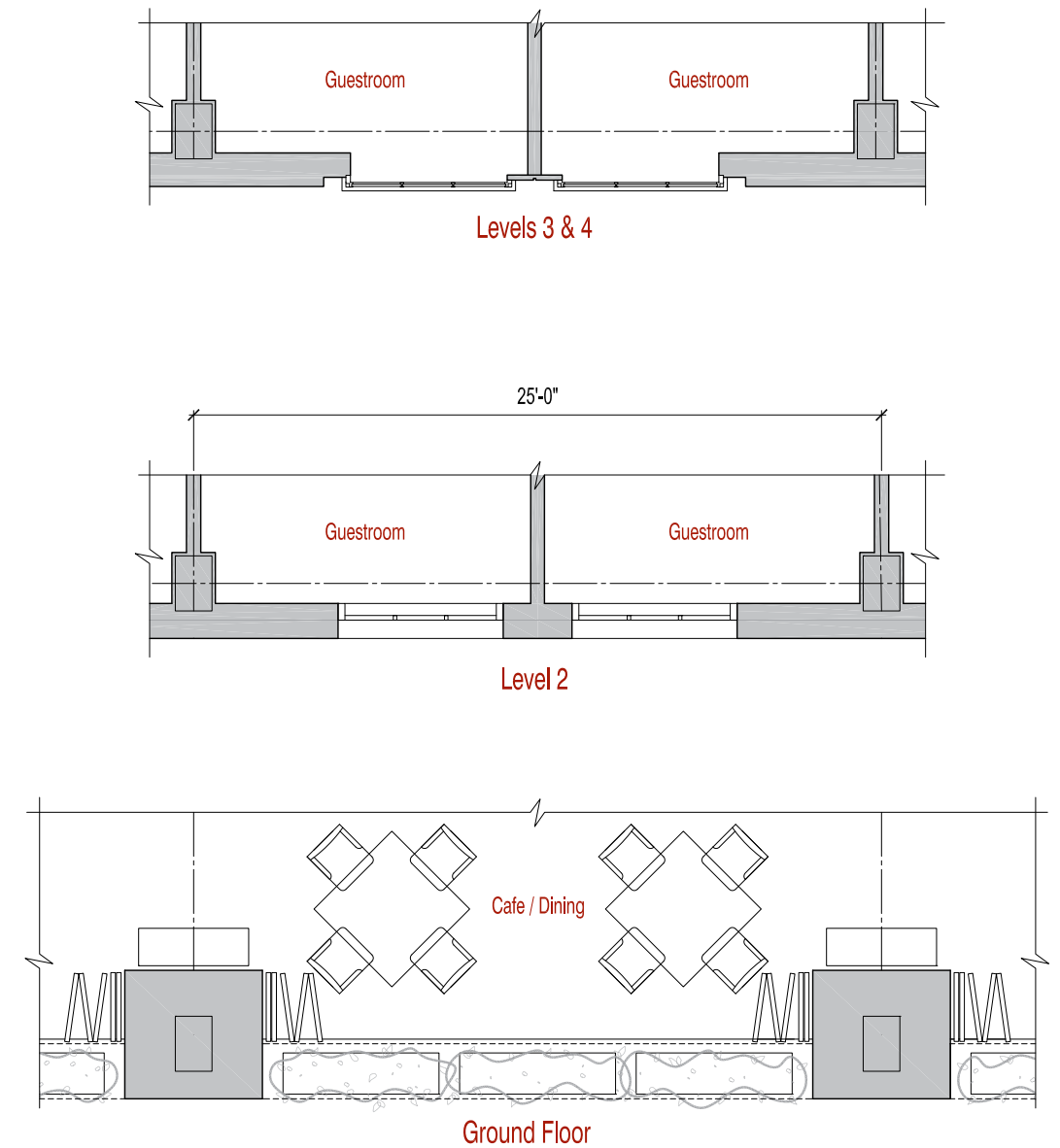
Building Elevations | Preferred Proposal



Wall Section

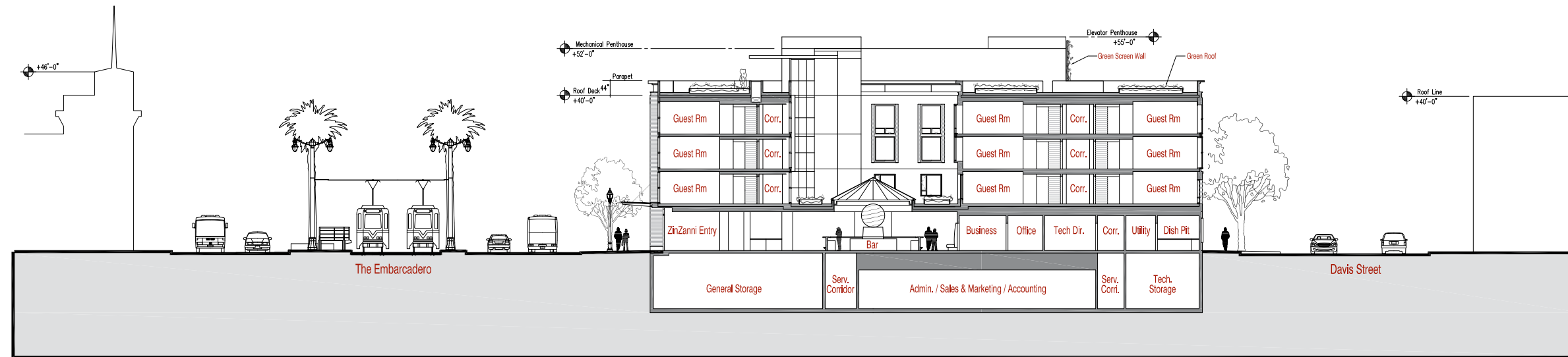


Elevation

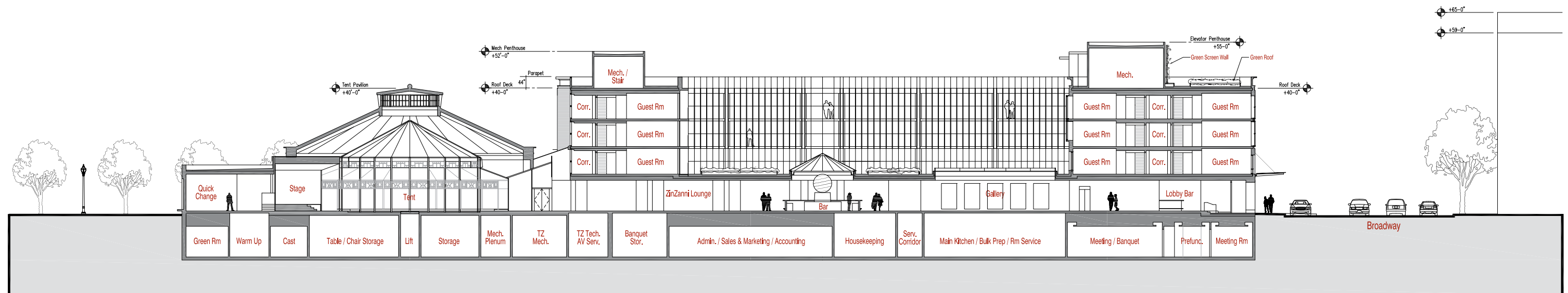


Plan Sections

Embarcadero | Rectangular Shoulder & Flush Bay



Cross Section looking South



Longitudinal Section looking East

Building Sections



Cross Section looking South



Longitudinal Section looking West

Building Sections



Broadway



The Embarcadero

Building Elevations - Preferred Proposal



Park



Davis Street

Building Elevations - Preferred Proposal



ZinZanni / Kenwood - The Theater & Hotel @ Broadway | San Francisco, California
A Teatro ZinZanni & Kenwood Investments Project



KENWOOD INVESTMENTS



Prepared for:
Teatro ZinZanni and Kenwood Investments

by:
Hornberger + Worstell Architects

THE EMBARCADERO



Site Plan | **New Theater and Hotel Park**



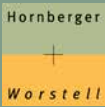
Landscape Images



ZinZanni / Kenwood - The Theater & Hotel @ Broadway | San Francisco, California
A Teatro ZinZanni & Kenwood Investments Project

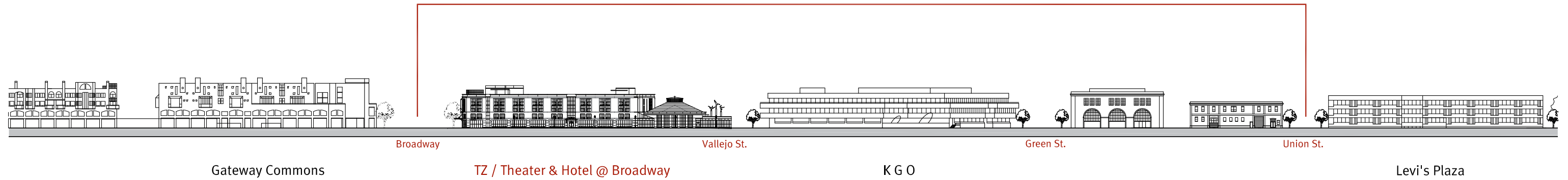


KENWOOD INVESTMENTS



Prepared for:
Teatro ZinZanni and Kenwood Investments
by:
Hornberger + Worstell Architects

Northeast Waterfront Historic District



Northeast Waterfront Historic District

Appendix D, Article 10 San Francisco Planning Code

Section 6. FEATURES

(b) Scale & Proportion:

- The buildings are of typical warehouse design, large in bulk, often with large arches and openings originally designed for easy vehicular access. The earlier brick structures blend easily with the scaled-down Beaux Arts forms of the turn of the century and the plain reinforced-concrete structures characteristic of twentieth century industrial architecture.

Embarcadero Elevation & Building Height Comparison

September 21, 2016 | 20

TEATRO ZINZANNI

KENWOOD INVESTMENTS

ZinZanni / Kenwood - The Theater & Hotel @ Broadway | San Francisco, California

A Teatro ZinZanni & Kenwood Investments Project

Hornberger + Worstell Architects

All drawings and written material appearing herein constitute the original and unpublished work of the architect and may not be duplicated, used, or disclosed without the prior written consent of the architect.
© 2015 Hornberger + Worstell, Inc.

Hornberger
+
Worstell



55 Union St.



Icehouse Alley: glass bridge approved by HRC 1992



John Maher St.



1050 Battery

1010 Battery

Historic District Images

Northeast Waterfront Historic District

Appendix D, Article 10 San Francisco Planning Code

Section 6. FEATURES

(b) Scale & Proportion:

- The buildings are of typical warehouse design, large in bulk, often with large arches and openings originally designed for easy vehicular access. The earlier brick structures blend easily with the scaled-down Beaux Arts forms of the turn of the century and the plain reinforced-concrete structures characteristic of twentieth century industrial architecture.

(d) Materials:

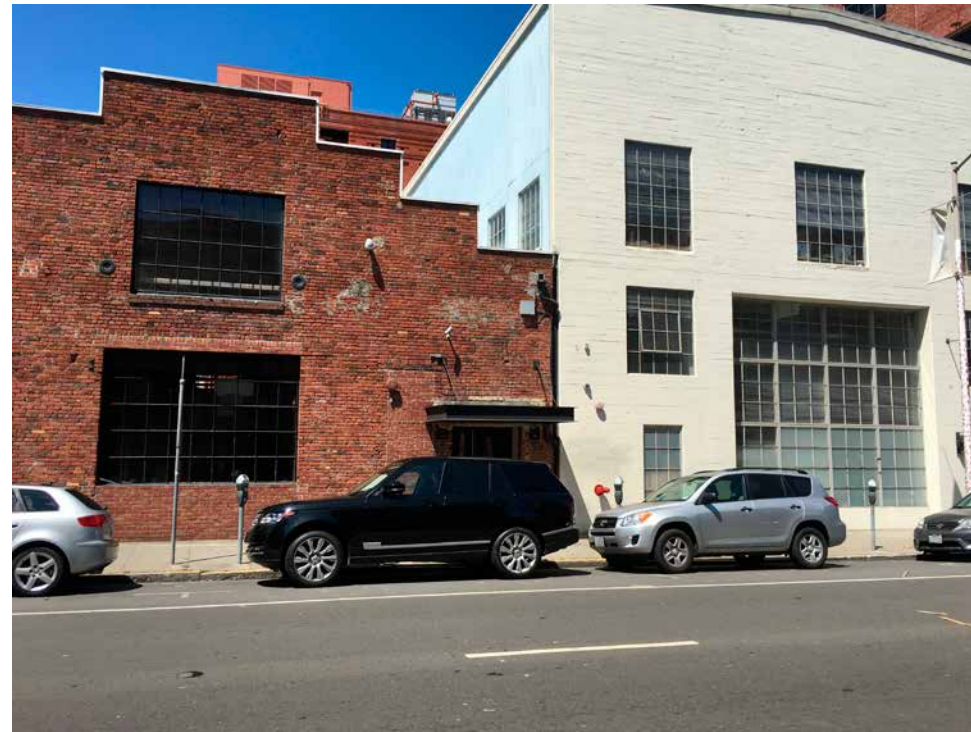
- Standard brick masonry is predominant for the oldest buildings in the District, with reinforced concrete introduced after the 1906 fire. Some of the brick facades have been stuccoed over. One of the structures still has its metal shutters, which were once typical of the area.

(e) Color:

- Red brick is typical, with some yellow and painted brick. Muted earth tones predominate in shades of red, brown, green, gray and blue.

(g) Detail:

- Arches are common on the ground floor, and are frequently repeated on upper floors. Flattened arches for window treatment are typical. Cornices are simple and generally tend to be abstract versions of the more elaborate cornices found on the downtown commercial structures from the nineteenth century. Most of the surfaces of the later buildings are plain and simple, reflecting their function. Some of the earlier brickwork contains suggestions of pilasters, again highly abstracted. Where detail occurs, it is often found surrounding entryways.



1025 Battery

1035 Battery

Bridge connector approved by HPC: 1992



Icehouse Alley

1035 Battery

Northeast Waterfront Historic District
Appendix D, Article 10 San Francisco Planning Code

Section 6. FEATURES

(c) Fenestration.

- They are varied in size, rhythmically spaced and relate in shape and proportion to those in nearby buildings. Larger industrial sash window began to be incorporated in structures built from the 1920's and onward. Door openings are often massive to facilitate easy access of bulk materials.



810 Battery



150 Broadway

120 Broadway



820 Battery

Canopies, Bays and Large Scale Openings in District



151 Union



1050 Battery

1010 Battery

Section 6. FEATURES

(a) Overall Form and Continuity.

- Building height is generally within a six-story range, with the higher structures closer to the base of Telegraph Hill and lower buildings near the water. Many of the oldest structures are one or two stories in height.



Embarcadero Elevation - Proposed Scheme

All quotes from Article 10: Preservation of Historical Architectural and Aesthetic Landmarks, Appendix D: Northeast Waterfront Historic District - San Francisco Planning Code

Northeast Waterfront Historic District
Appendix D, Article 10 San Francisco Planning Code

Historic District Images



1035 Battery



1050 Battery



1050 Battery

1010 Battery

Section 6. FEATURES

(b) Scale & Proportion:

- The buildings are of typical warehouse design, large in bulk, often with large arches and openings originally designed for easy vehicular access. The earlier brick structures blend easily with the scaled-down Beaux Arts forms of the turn of the century and the plain reinforced-concrete structures characteristic of twentieth century industrial architecture.



Embarcadero Elevation - Proposed Scheme

All quotes from Article 10: Preservation of Historical Architectural and Aesthetic Landmarks, Appendix D: Northeast Waterfront Historic District - San Francisco Planning Code

Northeast Waterfront Historic District
Appendix D, Article 10 San Francisco Planning Code

Historic District Images



151 Union Street



1088 Sansome Street



150 Green Street



1010 Battery



847 Front Street



Icehouse Alley: glass bridge approved by HRC 1992

(c) Fenestration.

- Minimal glazing is deeply recessed, producing a strong shadow line. The earliest structures have a few windows expressing their warehouse function. They are varied in size, rhythmically spaced and relate in shape and proportion to those in nearby buildings. Larger industrial sash window began to be incorporated in structures built from the 1920's and onward. Door openings are often massive to facilitate easy access of bulk materials.



Embarcadero Elevation - Proposed Scheme

All quotes from Article 10: Preservation of Historical Architectural and Aesthetic Landmarks, Appendix D: Northeast Waterfront Historic District - San Francisco Planning Code

Northeast Waterfront Historic District
Appendix D, Article 10 San Francisco Planning Code

Historic District Images



Embarcadero | Teatro ZinZanni Entrance

Northeast Waterfront Historic District
Appendix D, Article 10 San Francisco Planning Code

Section 6. FEATURES

(c) Fenestration.

- They are varied in size, rhythmically spaced and relate in shape and proportion to those in nearby buildings. Larger industrial sash window began to be incorporated in structures built from the 1920's and onward. Door openings are often massive to facilitate easy access of bulk materials.

(d) Materials:

- Standard brick masonry is predominant for the oldest buildings in the District, with reinforced concrete introduced after the 1906 fire. Some of the brick facades have been stuccoed over. One of the structures still has its metal shutters, which were once typical. ot

(e) Color.

- Red brick is typical, with some yellow and painted brick. Muted earth tones predominate in shades of red, brown, green, gray and blue.

(f) Texture.

- Typical facing materials give a rough-textured appearance. The overall texture of the facades is rough-grained.



60 Broadway



150 Green Street



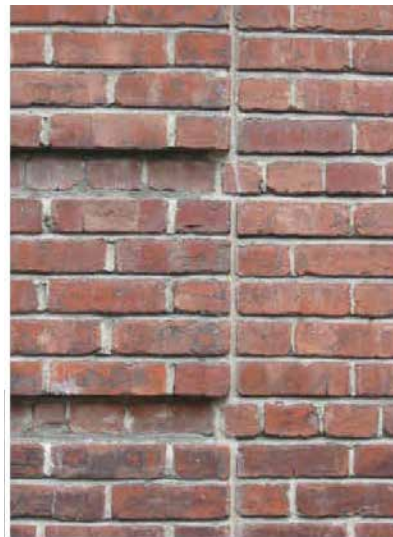
1035 Battery



Cobblestones - John Maher St.



55 Union St.

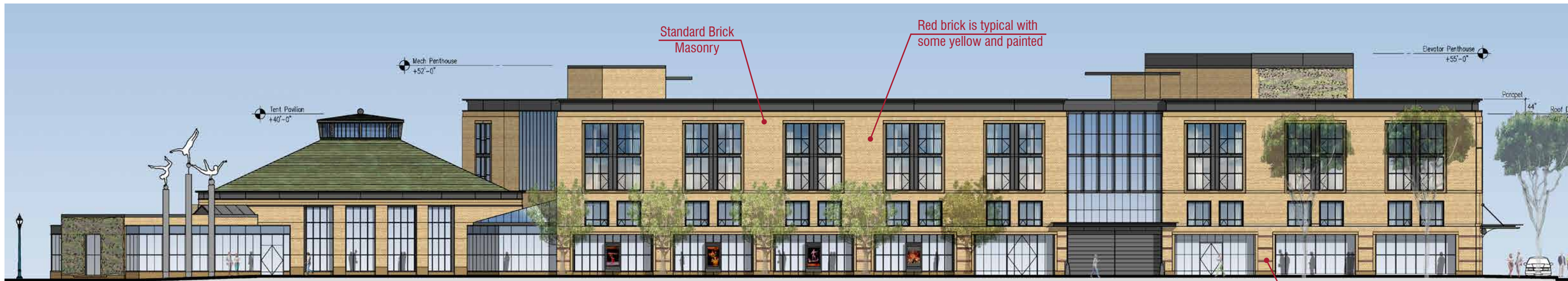


1010 Battery

Section 6. FEATURES

(d) Materials:

- Standard brick masonry is predominant for the oldest buildings in the District, with reinforced concrete introduced after the 1906 fire. Some of the brick facades have been stuccoed over. One of the structures still has its metal shutters, which were once typical of the area.



Standard Brick Masonry

Red brick is typical with some yellow and painted

Elevator Penthouse +55'-0"

Tent Pavilion +40'-0"

Mech Penthouse +52'-0"

Parapet 44' Roof D +47'-0"

Davis Elevation - Proposed Scheme

Rough-textured
Rough-grained
Brick

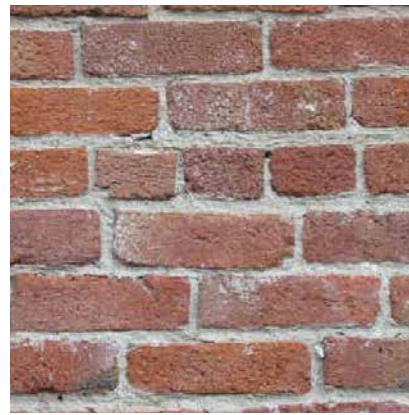
All quotes from Article 10: Preservation of Historical Architectural and Aesthetic Landmarks, Appendix D: Northeast Waterfront Historic District - San Francisco Planning Code

Northeast Waterfront Historic District
Appendix D, Article 10 San Francisco Planning Code

Historic District Images



150 Green St.



825 Battery



55 Union

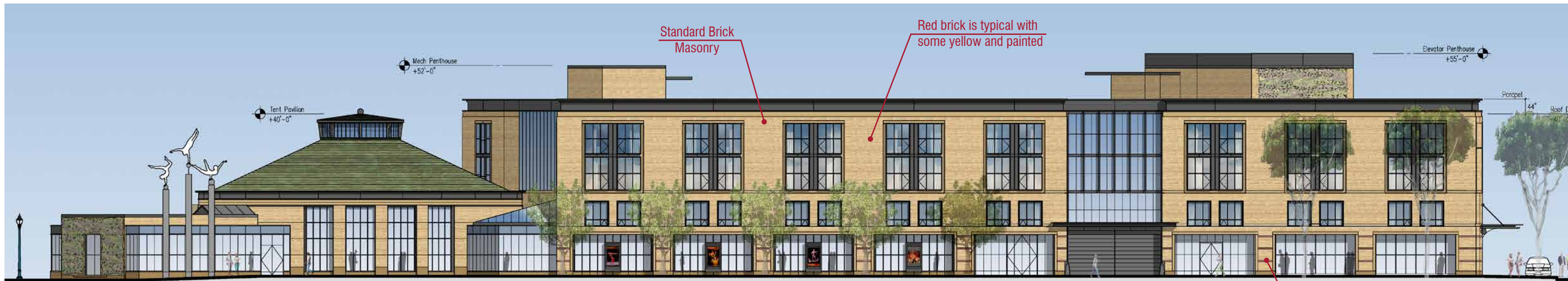


100 Green St.

Section 6. FEATURES

(e) Color.

- Red brick is typical, with some yellow and painted brick. Muted earth tones predominate in shades of red, brown, green, gray and blue.



Davis Elevation - Proposed Scheme

All quotes from Article 10: Preservation of Historical Architectural and Aesthetic Landmarks, Appendix D: Northeast Waterfront Historic District - San Francisco Planning Code

Northeast Waterfront Historic District
Appendix D, Article 10 San Francisco Planning Code

Historic District Images



1010 Battery



1088 Sansome



Icehouse Alley: glass bridge approved by HRC 1992

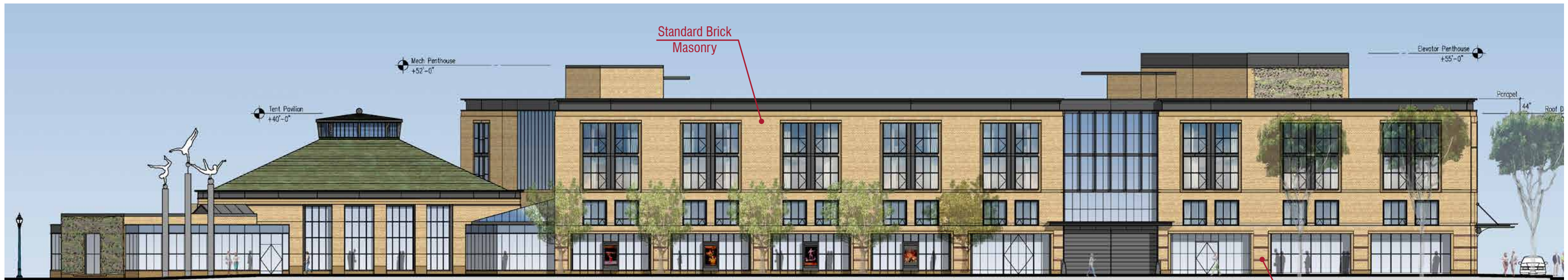


1025 Battery

Section 6. FEATURES

(f) Texture.

- Typical facing materials give a rough-textured appearance. The overall texture of the facades is rough-grained.



Davis Elevation - Proposed Scheme

Rough-textured
Rough-grained
Brick

All quotes from Article 10: Preservation of Historical Architectural and Aesthetic Landmarks, Appendix D: Northeast Waterfront Historic District - San Francisco Planning Code

Northeast Waterfront Historic District
Appendix D, Article 10 San Francisco Planning Code

Historic District Images



1001 Front St.



55 Union & 1050 Battery



1105 Battery



John Maher St.



101 Green St.

Section 6. FEATURES

(g) Detail.

- Arches are common on the ground floor, and are frequently repeated on upper floors. Flattened arches for window treatment are typical. Cornices are simple and generally tend to be abstract versions of the more elaborate cornices found on the downtown commercial structures from the nineteenth century. Most of the surfaces of the later buildings are plain and simple, reflecting their function. Some of the earlier brickwork contains suggestions of pilasters, again highly abstracted. Where detail occurs, it is often found surrounding entryways.



Broadway Elevation - Proposed Scheme

All quotes from Article 10: Preservation of Historical Architectural and Aesthetic Landmarks, Appendix D: Northeast Waterfront Historic District - San Francisco Planning Code

Northeast Waterfront Historic District
Appendix D, Article 10 San Francisco Planning Code

Historic District Images

1088 Sansome



151 Union St.



55 Union St.



1105 Battery



855 Battery



151 Union St.

Sec. 7. ADDITIONAL PROVISIONS FOR CERTIFICATES OF APPROPRIATENESS

(b) Additional Standards for Certain Features.

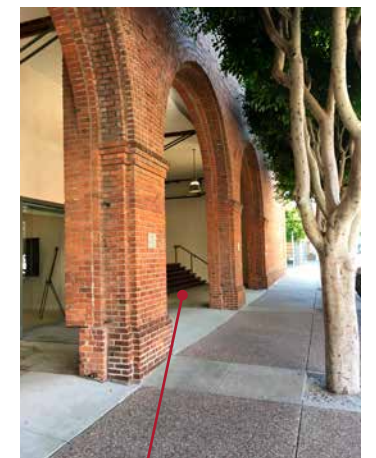
(1) Facade Line Continuity.

- Facade line continuity is historically appropriate. Therefore, setbacks and arcades, not generally being features of the Northeast Waterfront Historic District, are not acceptable.



Broadway Elevation - Proposed Scheme

Arches are common on the ground floor



1010 Battery

Arcade in Historic District

All quotes from Article 10: Preservation of Historical Architectural and Aesthetic Landmarks, Appendix D: Northeast Waterfront Historic District - San Francisco Planning Code

Northeast Waterfront Historic District
Appendix D, Article 10 San Francisco Planning Code

Historic District Images



Existing Rooftop View



Proposed Rooftop View

Sec. 7. ADDITIONAL PROVISIONS FOR CERTIFICATES OF APPROPRIATENESS

(3) Roof Treatment.

- Historically the view from Telegraph Hill over the Northeast Waterfront District has been one of roofs characterized by numerous regularly spaced industrial skylights. In renovation or new construction, these particular design features should be retained or incorporated.



Davis Elevation - Proposed Scheme

All quotes from Article 10: Preservation of Historical Architectural and Aesthetic Landmarks, Appendix D: Northeast Waterfront Historic District - San Francisco Planning Code

Northeast Waterfront Historic District
Appendix D, Article 10 San Francisco Planning Code

Historic District Images



1010 Battery



1010 Battery

Northeast Waterfront Historic District
Appendix D, Article 10 San Francisco Planning Code

Section 6. FEATURES

(g) Detail.

- Arches are common on the ground floor, and are frequently repeated on upper floors. Flattened arches for window treatment are typical. Cornices are simple and generally tend to be abstract versions of the more elaborate cornices found on the downtown commercial structures from the nineteenth century. Most of the surfaces of the later buildings are plain and simple, reflecting their function. Some of the earlier brickwork contains suggestions of pilasters, again highly abstracted. Where detail occurs, it is often found surrounding entryways.

Northeast Waterfront Historic District
Appendix D, Article 10 San Francisco Planning Code

Section 6. FEATURES

(c) Fenestration.

- They are varied in size, rhythmically spaced and relate in shape and proportion to those in nearby buildings. Larger industrial sash window began to be incorporated in structures built from the 1920's and onward. Door openings are often massive to facilitate easy access of bulk materials.



101 Green St.



69 Green St.

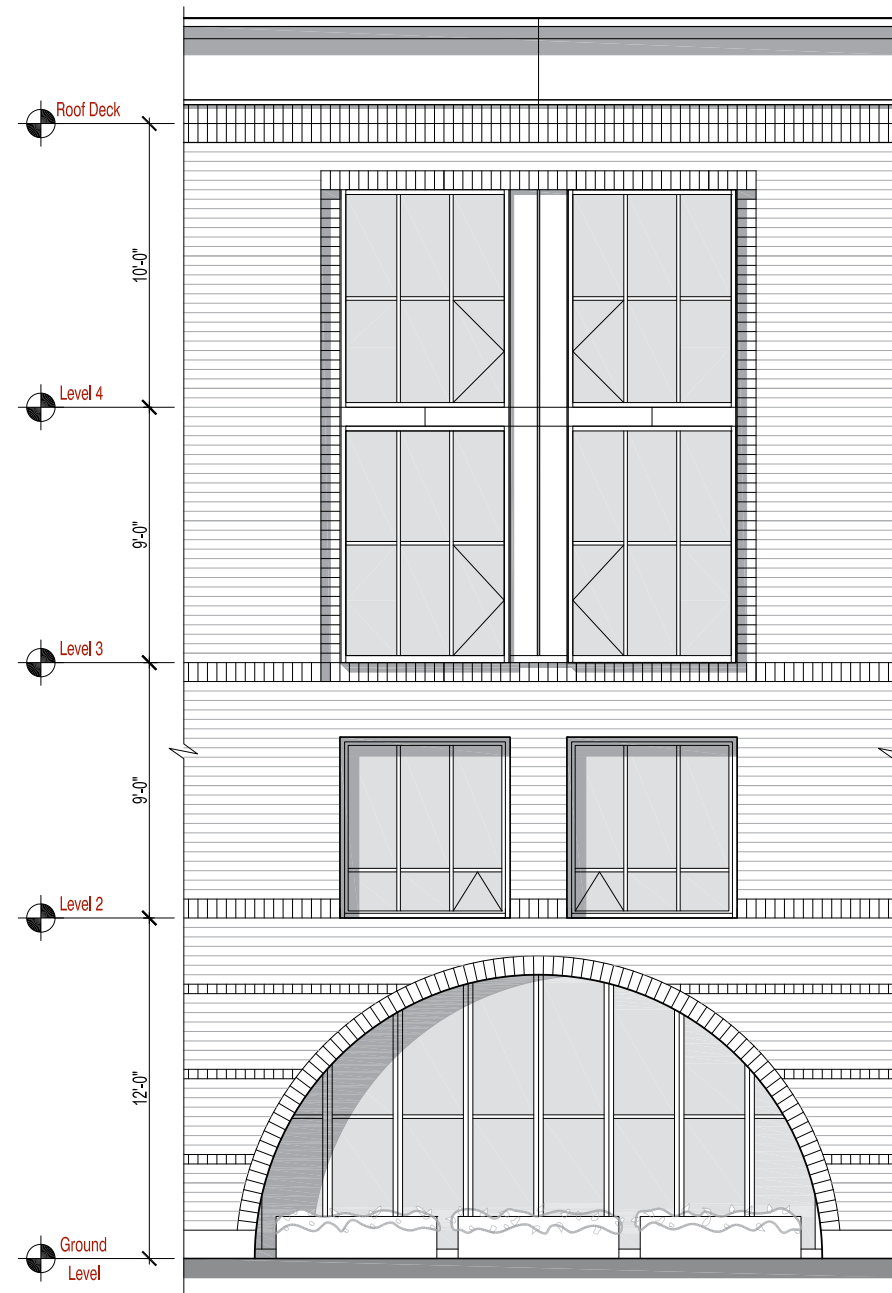
Sec. 7. ADDITIONAL PROVISIONS FOR CERTIFICATES OF APPROPRIATENESS

(b) Additional Standards for Certain Features.

(1) Facade Line Continuity.

- Facade line continuity is historically appropriate. Therefore, setbacks and arcades, not generally being features of the Northeast Waterfront Historic District, are not acceptable.

Arched Openings & Loggia in District



Partial Elevation - Arched Bay



1010 Battery St.

Arched Opening in Historic District



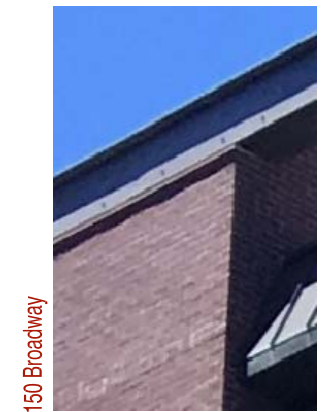
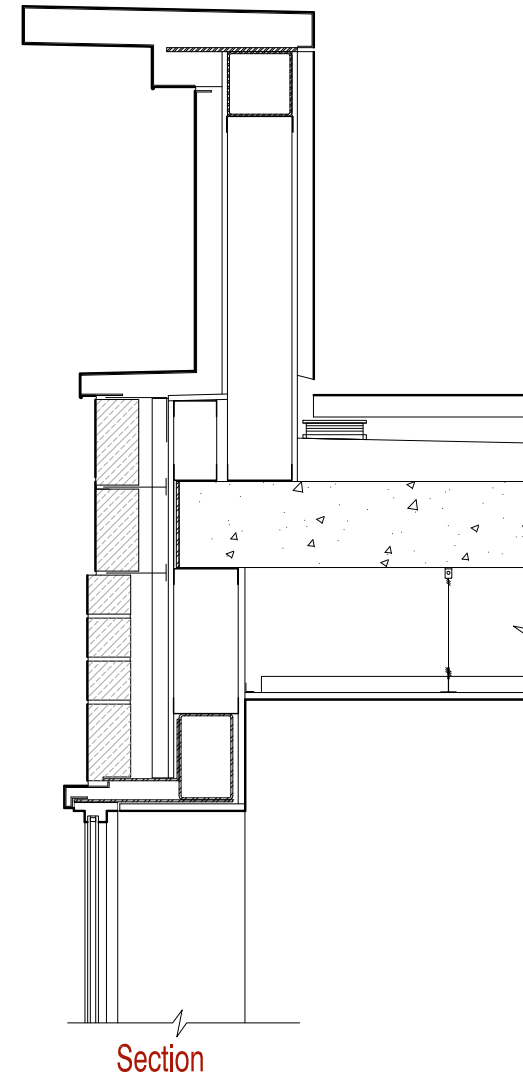
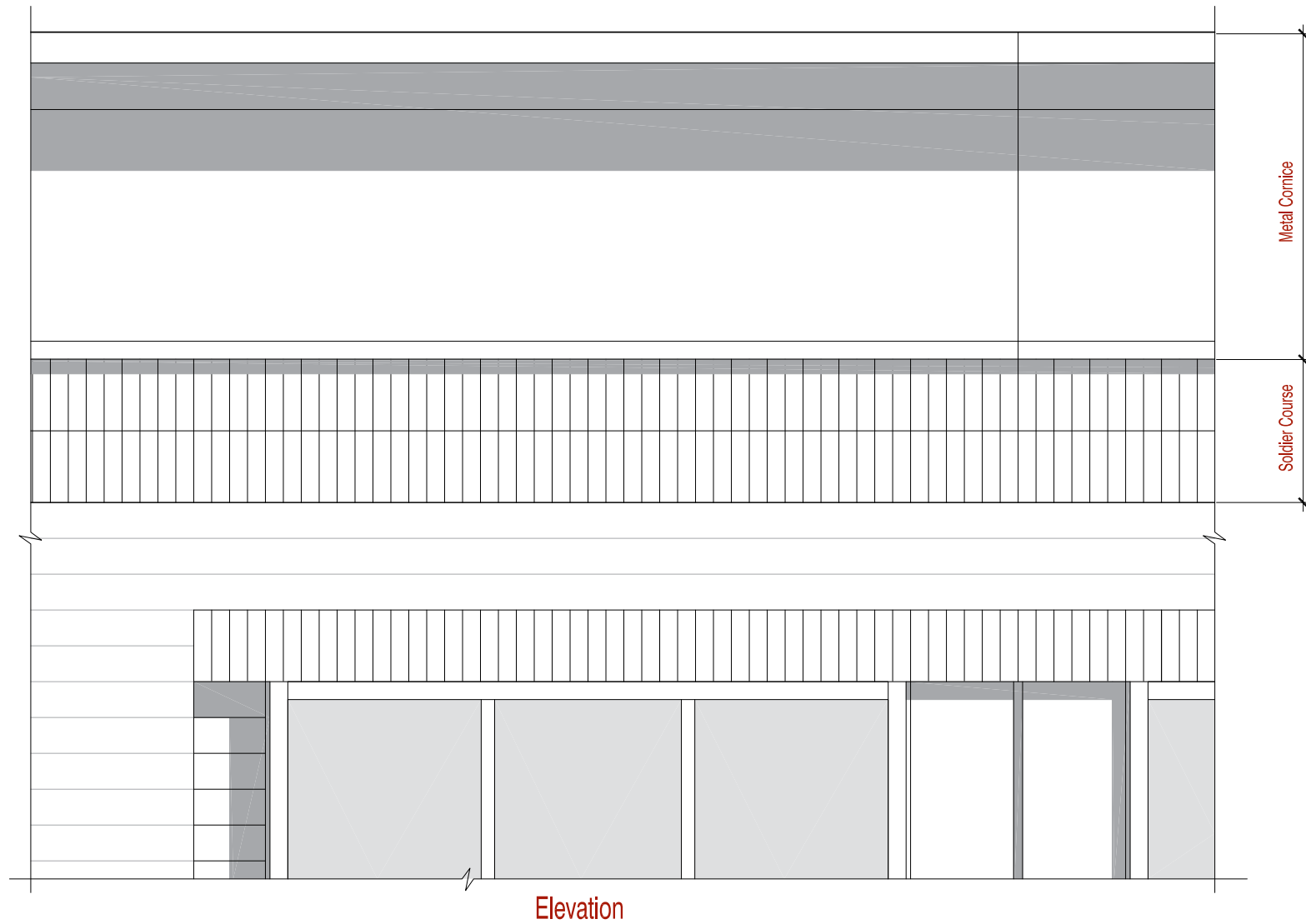
1010 Battery

Northeast Waterfront Historic District
 Appendix D, Article 10 San Francisco Planning Code
 Section 6. FEATURES

(g) Detail.

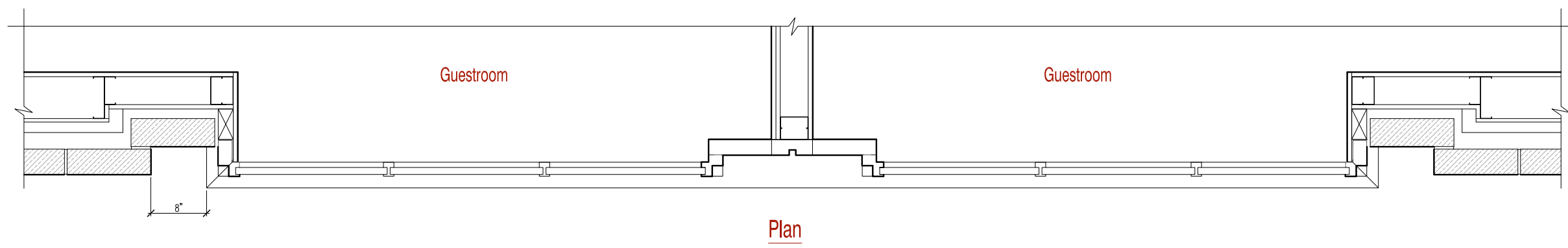
- Arches are common on the ground floor, and are frequently repeated on upper floors. Flattened arches for window treatment are typical. Cornices are simple and generally tend to be abstract versions of the more elaborate cornices found on the downtown commercial structures from the nineteenth century. Most of the surfaces of the later buildings are plain and simple, reflecting their function. Some of the earlier brickwork contains suggestions of pilasters, again highly abstracted. Where detail occurs, it is often found surrounding entryways.

Broadway | Arch & Flush Bay



HPC Approved 2003
District Images

Sec. 6. FEATURES.
(g) Detail.
Cornices are simple and generally tend to be abstract versions of more elaborate cornices found on downtown commercial structures from the nineteenth century.



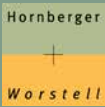
Metal Cornice @ Flush Bay



ZinZanni / Kenwood - The Theater & Hotel @ Broadway | San Francisco, California
A Teatro ZinZanni & Kenwood Investments Project



KENWOOD INVESTMENTS



Prepared for:
Teatro ZinZanni and Kenwood Investments
by:
Hornberger + Worstell Architects

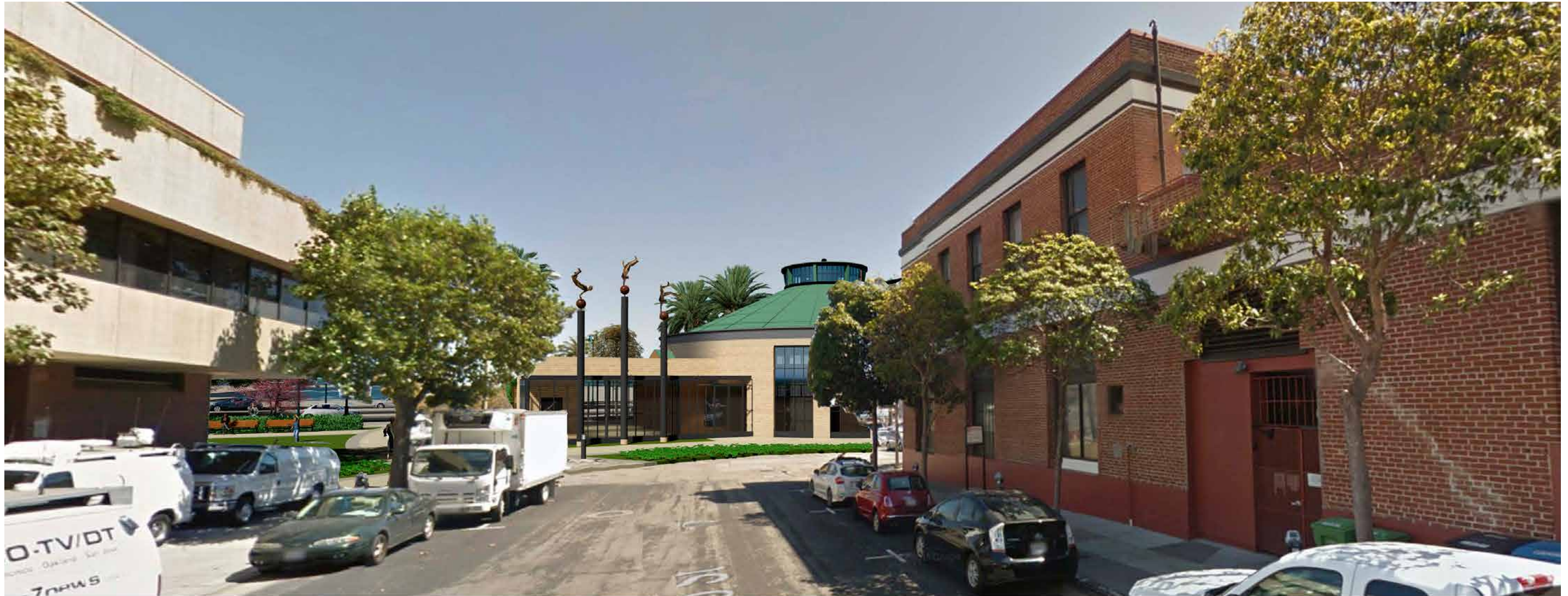


Existing



Proposed

Visual Analysis | View from Sansome & Vallejo



Visual Analysis | View from Vallejo



Visual Analysis | Park View from Davis Street



Existing



Proposed

Visual Analysis | **View 3 - Looking North Along Davis Street**



Existing



Proposed

Visual Analysis | View Looking North Along The Embarcadero



Visual Analysis | **View From The Embarcadero @ Broadway**



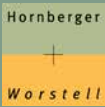
Visual Analysis | Park View looking South



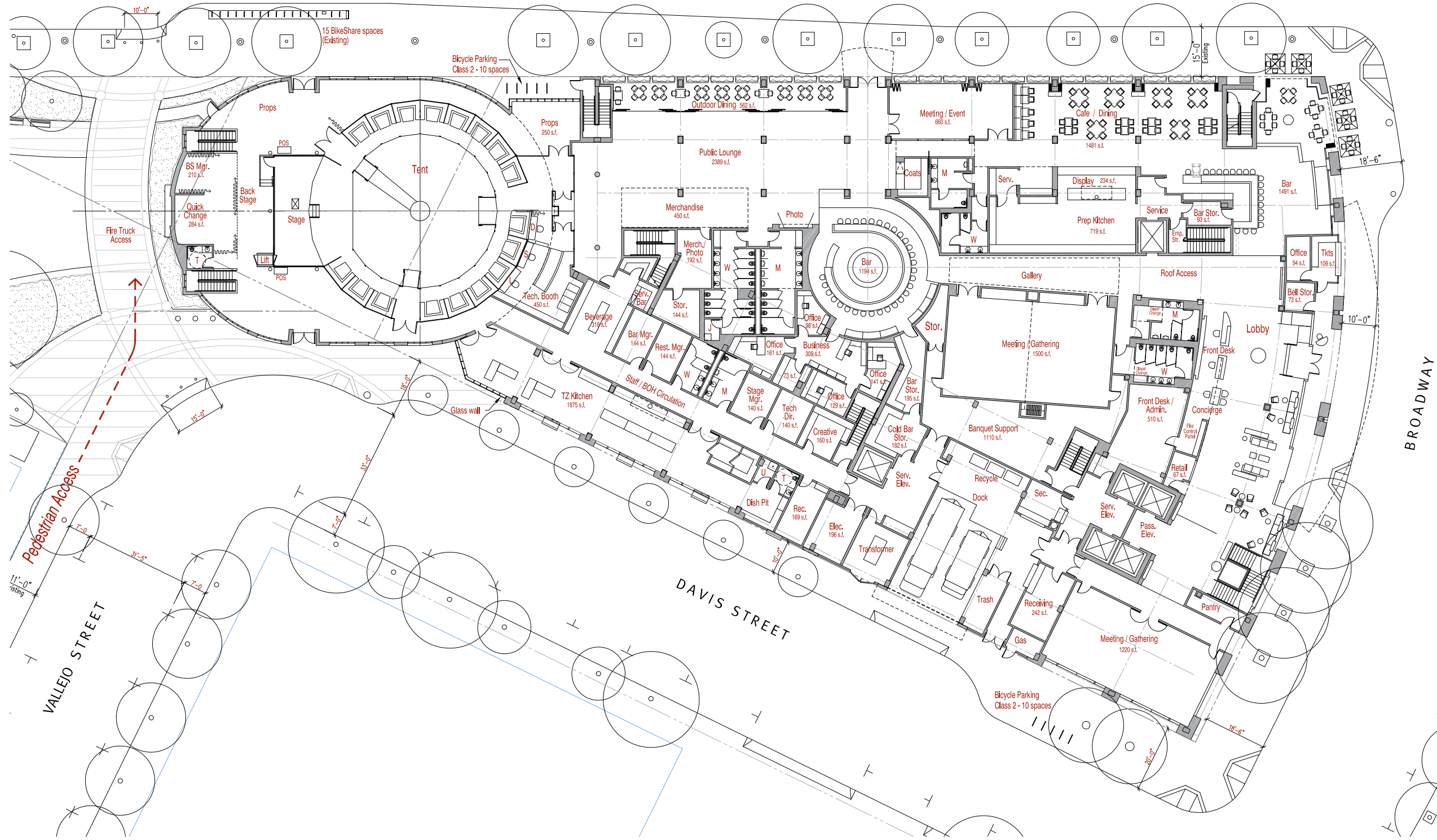
ZinZanni / Kenwood - The Theater & Hotel @ Broadway | San Francisco, California
A Teatro ZinZanni & Kenwood Investments Project



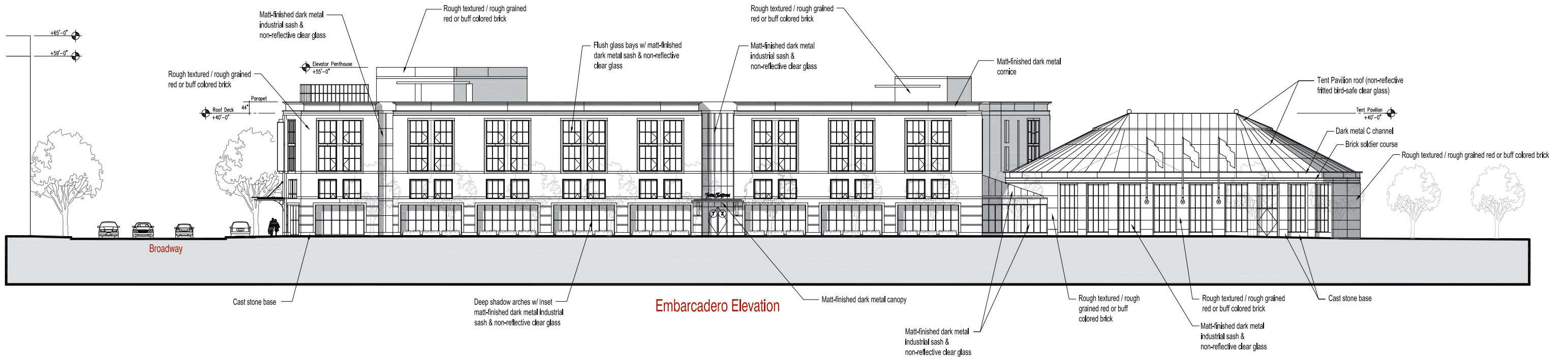
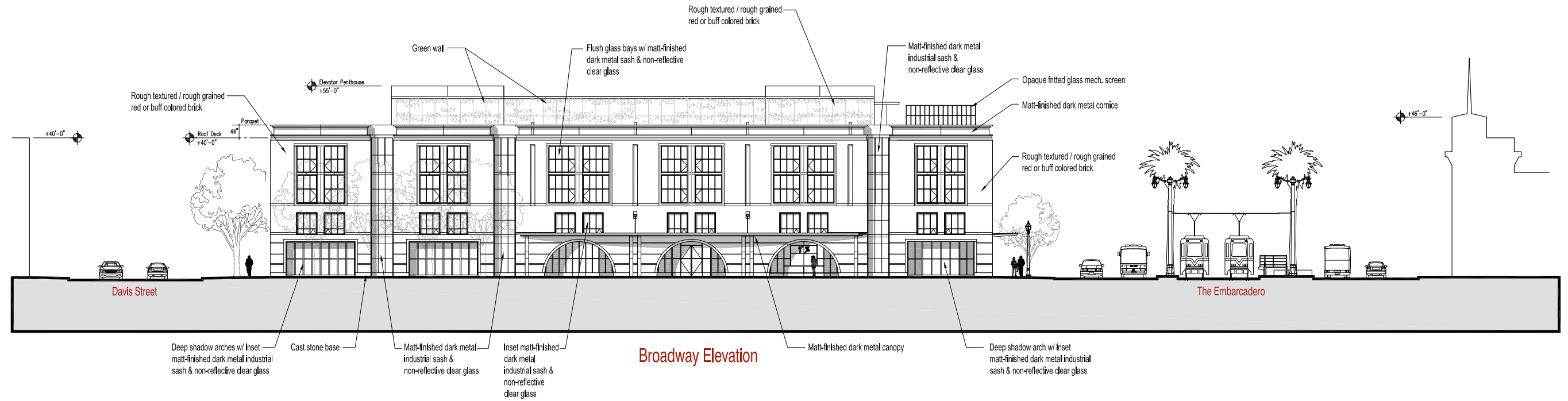
KENWOOD INVESTMENTS



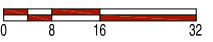
Prepared for:
Teatro ZinZanni and Kenwood Investments
by:
Hornberger + Worstell Architects



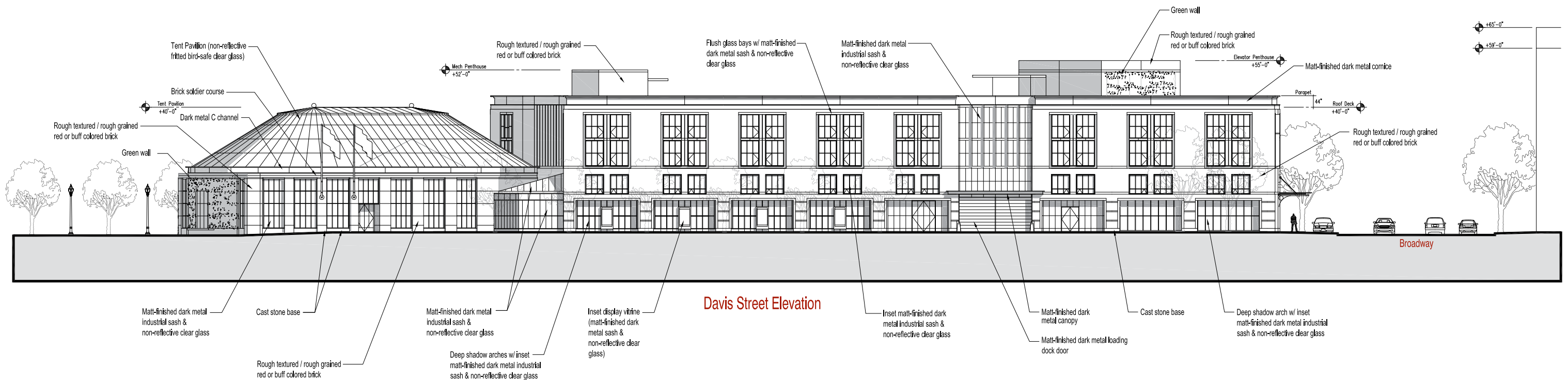
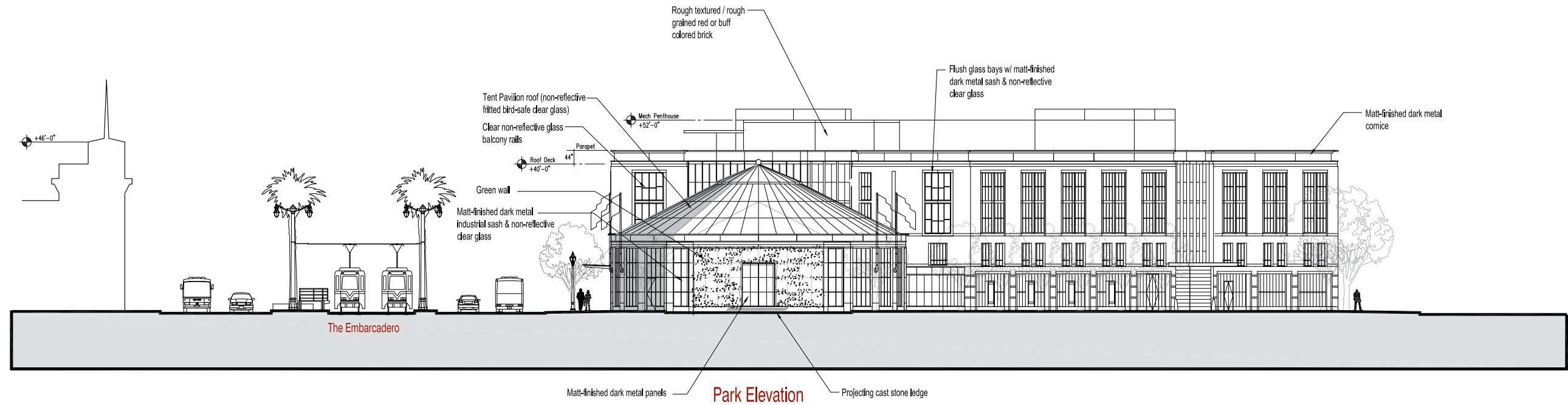
Ground Floor Plan - Planning Department Alternate



Building Elevations - Planning Department Alternate



September 21, 2016 | 44



Building Elevations - Planning Department Alternate

0 8 16 32

September 21, 2016 | 45



Broadway



The Embarcadero

Building Elevations - Planning Department Alternate



Park



Davis Street

Building Elevations - Planning Department Alternate



Rendering | View Corridor Planning Department Alternate



Rendering | Park View - Planning Department Alternate