



SAN FRANCISCO PLANNING DEPARTMENT

Certificate of Appropriateness Case Report

HEARING DATE: NOVEMBER 18, 2015

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Filing Date: July 16, 2015
Case No.: **2012. 0262COA**
Project Address: **2301 SAN JOSE AVENUE/500 GENEVA AVENUE**
Historic Landmark: No. 180 – S.F. & San Mateo Railroad Co. Office Building
Zoning: P (Public Use District)
40-X Height and Bulk District
Block/Lot: 6972/036
Applicant: San Francisco Recreation and Parks Department
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PROPERTY DESCRIPTION

The **GENEVA OFFICE BUILDING** at **2301 SAN JOSE AVENUE (aka 500 GENEVA AVENUE)** is part of the Geneva Car Barn Complex which consists of two contiguous structures – the Office Building (12,916 square feet) and the Powerhouse (3,735 square feet), which are located on the southeastern side of the intersection of San Jose and Geneva Avenues. The Office Building, completed in 1901, is located to the north, and immediately south along San Jose Avenue is the Powerhouse, completed in 1903. Both structures are attributed to the architects James W. & Merritt J. Reid. These brothers completed numerous projects around the turn of the 19th century for the Spreckels family, who owned the San Francisco and San Mateo Railroad Company at the time of construction of both buildings. The Office Building consists of common (American) bond red brick bearing walls, rendered in Romanesque Revival style, with a wood corner turret and wood bays detailed in the Queen Anne style. Originally the Powerhouse was constructed of similar brick bearing walls; however, after the 1906 earthquake, the second level collapsed and in 1910 the second level façade was rebuilt in concrete in the Mission Revival style.

In 1989 the structures were damaged in the Loma Prieta earthquake and were initially slated for demolition in 1998 until a community coalition halted the plans. In the early 2000s, efforts were undertaken to stabilize the building until funds were in place for rehabilitation; this stabilization included new roofing, seismic retrofitting, and protecting windows in place with plywood. In 2004, ownership was transferred from SFMTA to the SF Recreation and Parks Department. Both buildings are currently vacant.

The Office Building is designated as San Francisco Landmark No. 180. The Geneva Car Barn Complex (Office Building and Powerhouse) is listed in the California and National Registers of Historic Places, and in the 1976 Architectural Survey with a rating of '3'. This building complex abuts the active San Francisco

MUNI Rail Yard (Cameron Beach Yard) with active rail lines accessing the Yard from both sides of the Complex.

PROJECT DESCRIPTION

The overall proposal is to rehabilitate the two structures in the Geneva Car Barn Complex and adaptively reuse the Office Building and Powerhouse as a community-serving facility for youth arts education and a community arts/events center, including a restaurant space. No building envelope expansion is proposed. Within the Office Building, a new floor is proposed to be inserted into the attic space, which will add approximately 2,400 square feet. The project is designed to move forward in two phases, due to anticipated funding allocations. The project has received Part 2 SHPO approval for Investment Tax Credits for the entire scope of work.

Under review for this Certificate of Appropriateness is the Phase 1 of work, with focus on the partial site work and minor work at the Office Building. The entire scope of work for the Powerhouse and Office Building is provided for review and input; however, a final Certificate of Appropriateness for Phase 2 will be sought at a later date. Specifically, the proposal for Phase 1 includes:

Phase 1.

This phase includes rehabilitation of the Powerhouse for use as a community arts and events facility, with construction anticipated to begin in Fall 2016 and occupancy anticipated in Summer 2017. In order for the facility to function, site improvements are proposed at the Office Building, for which a Certificate of Appropriateness is required to proceed.

Office Building

- Site alterations include the introduction of a new trash enclosure and transformer pad, with bollards, at the east elevation of the Office Building. This area is currently used for parking.
- At the Office Building, rear (east) elevation towards the north, a pair of wood-framed windows is located at the partial basement level adjacent to the proposed location of the transformer pad. The existing wood frames will be rehabilitated where possible, and mechanical louvers will be installed to replace the glazing. These are basement level windows that will serve a new mechanical room in the basement.

Powerhouse – Other Work.

- Exterior work including restoration of the historic brick wall at first level and the historic concrete wall at second level and parapet; restoration of the historic wood windows, including replacement of glazing with two layers of 1/8" thick glazing laminated with low-e coating; roof material replacement of existing slate roof, with slate or synthetic slate roofing; replacement of existing concrete roof assembly with a lightweight concrete composite assembly; installation of solar panels on roof; and at roof level, restoration of the steel muntin skylight; insertion of contemporary entry doors within the existing openings at both entry points of the Powerhouse

on the primary (west) façade, with original wood carriage doors rehabilitated and positioned to remain in place at open positions.

- Interior work will include a new concrete floor and structural glass enclosures encasing the location of two old turbines; introduction of temporary use rooms for storage and electrical underneath the existing iron mezzanine; and introduction of concrete shear walls for seismic upgrade.

Phase 2.

This phase includes rehabilitation of the Office Building and completion of work at the Powerhouse for use of entire complex as youth arts training facility, including art studios, black box theater, and restaurant, and continuation of use in phase 1. Signage, site and landscaping work, and work at the Office Building requires a Certificate of Appropriateness to proceed, and more details will be provided in the phase 2 Certificate of Appropriateness. Construction funding is being sought, with potential for start in 2018.

- A Signage program will be proposed for the Geneva Car Barn Complex, to include materials, sign dimensions, estimated placement, whether signage is to be illuminated or not, and proposed attachment details.
- Full site and landscape improvements to include completion of landscaping, pathways, and lighting, and removal of temporary ramps and restroom trailer in parking lot.

Office Building

- Exterior work including restoration of the historic brick; restoration of the historic wood windows, including replacement of glazing with two layers of 1/8" thick glazing laminated with low-e coating; roof replacement of existing non-historic corrugated metal roof with new corrugated metal roofing; at portion of north and east façade second level, replacement of 1906 unfinished tar paper siding with dark metal siding; retention of 1917 Carmen's Strike door on rear (east) elevation with glazing; restoration of historic wood bays and corner turret, with new metal roof; new openings to create doorway access between the new lobby at the Powerhouse; installation of solar panels and creation of new skylights at the east side of the roof; and restoration of historic wood entry doors.
- Interior work to include renovation of historic interior spaces; creation of new rooms in non-historic spaces; salvage/reuse of historic flooring, wall finishes and trim where feasible; insertion of a third floor in the existing attic space; insertion of a black box theater, with retractable seating platform and removable seats at second level; introduction of concrete shear walls for seismic upgrade; and addition of elevator, new stairs and new restrooms.

Powerhouse

- Interior work to include removal of temporary rooms under the existing mezzanine and insertion of catering kitchen under the existing mezzanine; and insertion of a new glass and steel theater lobby, stair and theater mezzanine in the Powerhouse to access the black box theater in the Office Building.
- Exterior work to include removal of temporary door and transition to window at rear (east) elevation.

OTHER ACTIONS REQUIRED

Approval by Civic Design Review and a General Plan Referral are required. Planning Commission approval is required, as well as a Variance is required to be obtained. A building permit(s) is required.

COMPLIANCE WITH THE PLANNING CODE PROVISIONS

The change of use to a Community Facility and a Restaurant requires Conditional Use Authorization by the Planning Commission. Lack of code-compliant parking and provision of bicycle parking requires a Variance to be obtained from the Zoning Administrator.

APPLICABLE PRESERVATION STANDARDS

ARTICLE 10

A Certificate of Appropriateness is required for any construction, alteration, removal, or demolition of a designated Landmark for which a City permit is required. In appraising a proposal for a Certificate of Appropriateness, the Historic Preservation Commission should consider the factors of architectural style, design, arrangement, texture, materials, color, and other pertinent factors. Section 1006.7 of the Planning Code provides in relevant part as follows:

The proposed work shall be appropriate for and consistent with the effectuation of the purposes of Article 10.

The proposed work shall be compatible with the historic structure in terms of design, materials, form, scale, and location. The proposed project will not detract from the site's architectural character as described in the designating ordinance. For all of the exterior and interior work proposed, reasonable efforts have been made to preserve, enhance or restore, and not to damage or destroy, the exterior architectural features of the subject property which contribute to its significance.

THE SECRETARY OF THE INTERIOR'S STANDARDS

Rehabilitation is the act or process of making possible a compatible use for a property through repair, alterations, and additions while preserving those portions or features that convey its historical, cultural, or architectural values. For phase one of the project at the Powerhouse, the Rehabilitation Standards provide, in relevant part(s):

- Standard 1. A property will be used as it was historically or be given a new use that requires minimal change to its distinctive materials, features, spaces, and spatial relationships.

The Office Building and Powerhouse will be adaptively reused from an industrial function to a community-serving facility. To achieve this reuse, the building envelope, exterior materials, roof configuration, fenestration pattern, and primary entry points will be retained. The proposed location of the trash enclosure would utilize space on the site that does not currently contribute to the historic character of the landmark and that would not require removal of historic materials.

- Standard 2. The historic character of a property shall be retained and preserved. The removal of historic materials or alteration of features and spaces that characterize a property shall be avoided.

No distinctive materials, spaces, features or spatial relationships that characterize the Office Building, or the surrounding site, will be altered. New enclosures for trash and a transformer pad will be introduced at the east elevation. The trash enclosure, approximately 7'-0" in height, will be a metal structure placed in front of but not attached to a solid portion of the Office Building. The transformer pad and bollards will be situated adjacent to (north of) the trash enclosure and in front of historic wood windows. Mechanical louvers will be installed to replace the glazing at the pair of existing wood-framed windows located in the Office Building, basement level, at proposed transformer pad.

- Standard 3. Each property will be recognized as a physical record of its time, place, and use. Changes that create a false sense of historical development, such as adding conjectural features or elements from other historic properties, will not be undertaken.

New architectural features will not be introduced at the Landmark. New site features will be of metal or concrete, and are contemporary materials that are compatible with the historic industrial nature of the site.

- Standard 4. Changes to a property that have acquired significance in their own right will be retained and preserved.

Several modifications to the Office Building have acquired significance since the building's construction in 1901. These changes are not proposed for review under this scope of work.

- Standard 5. Distinctive features, finishes, and construction techniques or examples of fine craftsmanship that characterize a property will be preserved.

The Office Building wall construction is of common (American) bond red brick bearing walls. Neither the trash enclosure nor the transformer pad will be attached to the existing historic material, therefore, will not damage the construction technique. The new work will not remove any distinctive materials or features.

- Standard 6. Deteriorated historic features will be repaired rather than replaced. Where the severity of deterioration requires replacement of a distinctive feature, the new feature will match the old in design, color, texture, and, where possible, materials. Replacement of missing features will be substantiated by documentary and physical evidence.

The transformer pad will be situated in front of two historic double-hung wood windows in the Office Building. The existing wood frames will be rehabilitated where possible, and mechanical louvers will be installed to replace the glazing. These are basement level windows that will serve a new mechanical room in the basement. If not rehabilitated, new windows matching the existing in profile, dimension and material will be specified for replacement.

- Standard 7. Chemical or physical treatments, if appropriate, will be undertaken using the gentlest means possible. Treatments that cause damage to historic materials will not be used.

Cleaning of brick, if required during installation of the trash enclosure or preparation for the transformer pad, will be by the gentlest means possible to remove organic growth, efflorescence, general soiling, paint haze and mortar smears.

- Standard 9. New additions, exterior alterations, or related new construction will not destroy historic materials, features, and spatial relationships that characterize the property. The new work will be differentiated from the old and will be compatible with the historic materials, features, size, scale and proportion, and massing to protect the integrity of the property and its environment.

The proposed site alteration will not destroy historic materials, features or spatial relationships. The trash enclosure will measure approximately 15'-0" by 12'-0" and be no more than 7'-0" in height and be composed of metal. Both will be differentiated from the existing historic Office Building, but compatible with the historic industrial nature of the site. The site is a working yard for MUNI rail cars and industrial functions have always occurred on this site.

- Standard 10. New additions and adjacent or related new construction shall be undertaken in such a manner that if removed in the future, the essential form and integrity of the historic property and its environment would be unimpaired.

The trash enclosure will be separate from and not attached to the main historic building; the enclosure could be easily removed in the future without harm to the Office Building. The transformer pad and bollards could be removed and the space re-incorporated into the MUNI yard site. The historic wood windows infilled with mechanical louvers could be replaced with glazing. If the proposed additions were removed in the future, the form and integrity of the Landmark would remain intact.

PUBLIC/NEIGHBORHOOD INPUT

The Department has received no public input on the project at the date of this report.

ISSUES & OTHER CONSIDERATIONS

Phase 1

- The scope of work for Phase 1 site alterations at the Office Building are under consideration for this Certificate of Appropriateness.
- Due to the amount and type of rehabilitation proposed at the exterior of the Powerhouse, staff recommends an historic architect/conservator be included in the Technical Specifications, as an Owner's Representative, to review appropriate Contractor's experience, test samples, cleaning methods/materials, patch materials, and replacement items/fixtures.

Phase 2

- Phase 2 includes these items as previously discussed in the Project Description section of this report, for review and comments prior to the future request for Certificate of Appropriateness:
 - Insertion of new floor is proposed at the rear (east) elevation, created from space within the existing attic space to be used for flexible studio space. The floor plate will be minimally visible at a level approximately beginning at and above the meeting rail of the existing window profile at rear (east) elevation. Finish material is painted wood.
 - Insertion of new black box theater at the second level of Office Building is proposed, visible from the front (west) elevation, at a portion of the second level. The theater platform is composed of seven retractable platforms at 3'-0" depth with removable seats (79 seats); when there are no performances the platform retracts into a storage unit approximately 4'-0" at the back of the theater. A black-out curtain system will be utilized for performances. It is unclear if the permanent storage unit extends above the existing window sill and/or will be a visible element through the window glazing from the public right of way, and a more detailed section drawing for review, including finish information of the unit, should be included in subsequent submittals.
 - Insertion of new stair from basement level to top level is proposed at the rear (east) elevation. The stairs will be visible through a column of existing glazing at the rear (east) elevation of the Office Building. Stair is proposed of steel stringers in a light color with railing assembly of same material and color, to be perforated.
 - Removal of a small portion of existing south wall at Office Building (shared with the Powerhouse) and installation of new access points between the Powerhouse and the Office Building are proposed at Phase 2.
 - A sign program will assist the applicant and future tenants with a comprehensive outline for incorporating this locational marketing item. The program should include proposed materials, estimated sign dimensions, estimated placement, whether signage is to be illuminated or not, and proposed attachment details and placement to types of materials.
 - Due to the amount and type of rehabilitation proposed at the exterior of the Office Building, staff recommends an historic architect/conservator be included in the Technical Specifications, as an owner's representative, to review appropriate Contractor's experience, test samples, cleaning methods/materials, patch materials, and replacement items/fixtures.
 - At the exterior of the Office Building, several features have gained significance over time. Tar paper covering a portion of the north and east elevation at second level is evidence of

the 1906 earthquake damage that was never repaired. A prominent feature of the east elevation is the doorway evident on the second floor of the central bay that was cut out of the existing window to allow rear access to the car barn during the Carmen strike of 1917. (The car barns, which were located immediately adjacent to the east, were demolished in the 1970s.). A mock-up of the aluminum panels for review of seams is requested for this introduction of a new material. Additionally, more information about glazing and trim, if applicable, for the Carmens' Strike door should be provided in the window schedule.

STAFF ANALYSIS

Based on the requirements of Article 10 and the Secretary of Interior's Standards, staff has determined that the proposed work of Phase 1 will not adversely affect the subject landmark site.

Staff finds that the project will introduce a trash enclosure and a transformer pad adjacent to the building and replace glazing with mechanical louvers in a pair of existing historic wood window frames in place of the glazing. The Landmark will be retained and preserved.

Site Improvements

Site work includes a new trash enclosure and transformer pad at the east elevation of the Office Building. The site has been altered since its original construction, although it has been continuously used as a rail yard. In addition to the Office Building and the Powerhouse, the site complex used to comprise a large car barn and maintenance shops. In the 1970s the car barn and ancillary buildings were demolished in the 1970s to accommodate modern needs for MUNI. At the east elevation, the new trash enclosure and transformer pad do not extend taller than approximately seven feet, or the partial basement level, at an elevation with two full levels, partial basement and pitched roof.

Recommendation:

- 1. Staff recommends a metal material be used for the trash enclosure and that a sample shall be provided for review and approval by staff.*
- 2. Staff recommends landscape planting materials and other site improvement materials that reference the historic industrial nature of the site. Staff will review and approve landscape and site plans prior to building permit issuance.*

ENVIRONMENTAL REVIEW STATUS

The effects of the project were fully reviewed under the Balboa Park Station Area Plan EIR certified by the San Francisco Planning Commission on December 4, 2008, by Motion No. 17774 (Planning Case No. 2004.1059E). On November 14, 2013, the Geneva Car Barn and Powerhouse project was determined to be consistent with the Balboa Park Area Plan EIR and exempt from environmental review per CEQA Guidelines Section 15183 (Planning Case No. 2012.0262E).

PLANNING DEPARTMENT RECOMMENDATION

Planning Department staff recommends APPROVAL WITH CONDITIONS of the proposed project as it appears to meet the Secretary of the Interior Standards for Rehabilitation.

ATTACHMENTS

Draft Motion
Sponsor submittal packet, including photographs
Plans



SAN FRANCISCO PLANNING DEPARTMENT

Historic Preservation Commission Draft Motion

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ADOPTING FINDINGS FOR A CERTIFICATE OF APPROPRIATENESS FOR PROPOSED WORK DETERMINED TO BE APPROPRIATE FOR AND CONSISTENT WITH THE PURPOSES OF ARTICLE 10, TO MEET THE STANDARDS OF ARTICLE 10 AND TO MEET THE SECRETARY OF INTERIOR’S STANDARDS FOR REHABILITATION, FOR THE PROPERTY LOCATED ON LOT 036 IN ASSESSOR’S BLOCK 6972, WITHIN AN P (PUBLIC USE) ZONING DISTRICT AND A 40-X HEIGHT AND BULK DISTRICT.

PREAMBLE

WHEREAS, on July 16, 2015, Nicole Avril (Project Sponsor) filed an application with the San Francisco Planning Department (hereinafter “Department”) for a Certificate of Appropriateness for alterations to the site of the Office Building, on the subject property located on Lot 036 in Assessor’s Block 6972.

Specifically, the project includes:

Phase 1.

This phase includes rehabilitation of the Powerhouse for use as a community arts and events facility, with construction anticipated to begin in Fall 2016 and occupancy anticipated in Summer 2017. In order for the facility to function, site improvements are proposed at the Office Building which requires a Certificate of Appropriateness to proceed.

Office Building

- Site alterations include the introduction of a new trash enclosure and transformer pad, with bollards, at the east elevation of the Office Building. This area is currently used for parking.
- At the Office Building, rear (east) elevation towards the north, a pair of wood-framed windows is located at the partial basement level adjacent to the proposed location of the transformer pad. The existing wood frames will be rehabilitated where possible, and mechanical louvers will be installed to replace the glazing. These are basement level windows that will serve a new mechanical room in the basement.

WHEREAS, the effects of the project were fully reviewed under the Balboa Park Station Area Plan EIR certified by the San Francisco Planning Commission on December 4, 2008, by Motion No. 17774 (Planning Case No. 2004.1059E). On November 14, 2013, the Geneva Car Barn and Powerhouse project was determined to be consistent with the Balboa Park Area Plan EIR and exempt from environmental review per CEQA Guidelines Section 15183 (Planning Case No. 2012.0262E). The Historic Preservation Commission (hereinafter "Commission") has reviewed and concurs with said determination.

WHEREAS, on November 18, 2015, the Commission conducted a duly noticed public hearing on the current project, Case No. 2012. 0262COA ("Project") for its appropriateness.

WHEREAS, in reviewing the Application, the Commission has had available for its review and consideration case reports, plans, and other materials pertaining to the Project contained in the Department's case files, has reviewed and heard testimony and received materials from interested parties during the public hearing on the Project.

MOVED, that the Commission hereby grants the Certificate of Appropriateness, in conformance with the architectural plans dated September 11, 2015 and labeled Exhibit A on file in the docket for Case No. 2012. 0262COA based on the following findings:

CONDITIONS OF APPROVAL

1. Staff recommends a metal material be used for the trash enclosure and that a sample shall be provided for review and approval by staff.
2. Staff recommends landscape planting materials and other site improvement materials that reference the historic industrial nature of the site. Staff will review and approve landscape and site plans prior to building permit issuance.

FINDINGS

Having reviewed all the materials identified in the recitals above and having heard oral testimony and arguments, this Commission finds, concludes, and determines as follows:

1. The above recitals are accurate and also constitute findings of the Commission.
2. Findings pursuant to Article 10:

The Historical Preservation Commission has determined that the proposed work is compatible with the character of the landmark as described in the designation report dated August 22, 1985.

- The Office Building and Powerhouse will be adaptively reused from an industrial function to a community-serving facility. To achieve this reuse, the building envelope, exterior materials, roof configuration, fenestration pattern, and primary entry points will be retained. The proposed location of the addition would utilize space on the site that does not currently contribute to the historic character of the landmark and that would not require removal of historic materials. The proposed scale of the site alteration would be compatible with the existing scale of the site and setting. The addition would rise to no higher than the level of the partial basement level. This spatial relationship would allow the addition to recede as a subordinate element of the site.
- The Office Building wall construction is of common (American) bond red brick bearing walls. Neither the trash enclosure nor the transformer pad will be attached to the existing historic material, therefore, will not damage the construction technique. The new work will not remove any distinctive materials or features.
- The transformer pad will be situated in front of two historic double hung wood windows. The existing wood frames will be rehabilitated where possible, and mechanical louvers will be infilled to replace the glazing. These are basement level windows which will serve a new mechanical room in the basement. If not rehabilitated, new windows matching the existing in profile, dimension and material will be specified for replacement.
- No distinctive materials, spaces, features or spatial relationships that characterize the Office Building, or the surrounding site, will be altered. New enclosures for trash and a transformer pad will be introduced at the east elevation. The trash enclosure, approximately 7'-0" in height, will be of a metal material placed in front of but not attached to a solid portion of the Office Building. The transformer pad and bollards will be situated adjacent to (north of) the trash enclosure and in front of historic wood windows. Mechanical louvers will be infilled to replace the glazing of a pair of existing wood-framed windows.
- New architectural features will not be introduced at the Landmark. New site features will be of painted metal or concrete, and are contemporary materials that are compatible with the historic industrial nature of the site, however will be a contemporary addition. The proposed project would not add any conjectural historical features or features that add a false sense of historical development.
- The proposed site alteration will not destroy historic materials, features or spatial relationships. The trash enclosure will measure approximately 15'-0" by 12'-0" and be no more than 7'-0" in height and be composed of metal. Both will be differentiated from the existing historic Office Building, but compatible with the historic industrial nature of the site. The site is a working yard for MUNI rail cars and industrial functions have always occurred on this site. If the proposed additions were removed in the future, the form and integrity of the Landmark would remain intact.

- The proposed project meets the following Secretary of the Interior's Standards for Rehabilitation:

Standard 1.

A property shall be used for its historic purpose or be placed in a new use that requires minimal change to the defining characteristics of the building and its site and environment.

Standard 2.

The historic character of a property shall be retained and preserved. The removal of historic materials or alteration of features and spaces that characterize a property shall be avoided.

Standard 3.

Each property will be recognized as a physical record of its time, place, and use. Changes that create a false sense of historical development, such as adding conjectural features or elements from other historic properties, will not be undertaken.

Standard 4.

Changes to a property that have acquired historic significance in their own right will be retained and preserved.

Standard 5.

Distinctive features, finishes, and construction techniques or examples of craftsmanship that characterize a property shall be preserved.

Standard 6.

Deteriorated historic features will be repaired rather than replaced. Where the severity of deterioration requires replacement of a distinctive feature, the new feature will match the old in design, color, texture, and, where possible, materials. Replacement of missing features will be substantiated by documentary and physical evidence.

Standard 7.

Chemical or physical treatments, if appropriate, will be undertaken using the gentlest means possible. Treatments that cause damage to historic materials will not be used.

Standard 9.

New additions, exterior alterations, or related new construction will not destroy historic materials, features, and spatial relationships that characterize the property. The new work will be differentiated from the old and will be compatible with the historic materials, features, size, scale and proportion, and massing to protect the integrity of the property and its environment.

Standard 10.

New additions and adjacent or related new construction shall be undertaken in such a manner that if removed in the future, the essential form and integrity of the historic property and its environment would be unimpaired.

3. **General Plan Compliance.** The proposed Certificate of Appropriateness is, on balance, consistent with the following Objectives and Policies of the General Plan:

I. URBAN DESIGN ELEMENT

THE URBAN DESIGN ELEMENT CONCERNS THE PHYSICAL CHARACTER AND ORDER OF THE CITY, AND THE RELATIONSHIP BETWEEN PEOPLE AND THEIR ENVIRONMENT.

GOALS

The Urban Design Element is concerned both with development and with preservation. It is a concerted effort to recognize the positive attributes of the city, to enhance and conserve those attributes, and to improve the living environment where it is less than satisfactory. The Plan is a definition of quality, a definition based upon human needs.

OBJECTIVE 1

EMPHASIS OF THE CHARACTERISTIC PATTERN WHICH GIVES TO THE CITY AND ITS NEIGHBORHOODS AN IMAGE, A SENSE OF PURPOSE, AND A MEANS OF ORIENTATION.

POLICY 1.3

Recognize that buildings, when seen together, produce a total effect that characterizes the city and its districts.

OBJECTIVE 2

CONSERVATION OF RESOURCES WHICH PROVIDE A SENSE OF NATURE, CONTINUITY WITH THE PAST, AND FREEDOM FROM OVERCROWDING.

POLICY 2.4

Preserve notable landmarks and areas of historic, architectural or aesthetic value, and promote the preservation of other buildings and features that provide continuity with past development.

POLICY 2.5

Use care in remodeling of older buildings, in order to enhance rather than weaken the original character of such buildings.

POLICY 2.7

Recognize and protect outstanding and unique areas that contribute in an extraordinary degree to San Francisco's visual form and character.

The goal of a Certificate of Appropriateness is to provide additional oversight for buildings and districts that are architecturally or culturally significant to the City in order to protect the qualities that are associated with that significance.

The proposed project qualifies for a Certificate of Appropriateness and therefore furthers these policies and objectives by maintaining and preserving the character-defining features of the Geneva Office Building (2301 San Jose Avenue) for the future enjoyment and education of San Francisco residents and visitors.

4. The proposed project is generally consistent with the eight General Plan priority policies set forth in Section 101.1 in that:

- A) The existing neighborhood-serving retail uses will be preserved and enhanced and future opportunities for resident employment in and ownership of such businesses will be enhanced:

The proposed project includes vacant buildings. The adaptive reuse may provide more opportunities for employment.

- B) The existing housing and neighborhood character will be conserved and protected in order to preserve the cultural and economic diversity of our neighborhoods:

The proposed project will strengthen neighborhood character by adaptively reusing vacant buildings into community-serving facilities.

- C) The City's supply of affordable housing will be preserved and enhanced:

The project does not have impact on housing.

- D) The commuter traffic will not impede MUNI transit service or overburden our streets or neighborhood parking:

The proposed project will not result in commuter traffic impeding MUNI transit service or overburdening the streets or neighborhood parking. The site is located adjacent to a MUNI rail line, and across the street from a regional transit BART line stop.

- E) A diverse economic base will be maintained by protecting our industrial and service sectors from displacement due to commercial office development. And future opportunities for resident employment and ownership in these sectors will be enhanced:

The proposed will not have any impact on industrial and service sector jobs.

- F) The City will achieve the greatest possible preparedness to protect against injury and loss of life in an earthquake.

Preparedness against injury and loss of life in an earthquake is improved by the proposed work. The work will eliminate unsafe conditions at the site and all construction will be executed in compliance with all applicable construction and safety measures.

- G) That landmark and historic buildings will be preserved:

The proposed project is in conformance with Article 10 of the Planning Code and the Secretary of the Interior's Standards.

H) Parks and open space and their access to sunlight and vistas will be protected from development:

The proposed project will not impact the access to sunlight or vistas for the parks and open space.

5. For these reasons, the proposal overall, is appropriate for and consistent with the purposes of Article 10, meets the standards of Article 10, and the Secretary of Interior's Standards for Rehabilitation, General Plan and Prop M findings of the Planning Code.

DECISION

That based upon the Record, the submissions by the Applicant, the staff of the Department and other interested parties, the oral testimony presented to this Commission at the public hearings, and all other written materials submitted by all parties, the Commission hereby **GRANTS a Certificate of Appropriateness** for the property located at Lot 036 in Assessor's Block 6972 for proposed work in conformance with the renderings and architectural sketches dated September 11, 2015 and labeled Exhibit A on file in the docket for Case No. 2012.0262COA.

APPEAL AND EFFECTIVE DATE OF MOTION: The Commission's decision on a Certificate of Appropriateness shall be final unless appealed within thirty (30) days. Any appeal shall be made to the Board of Appeals, unless the proposed project requires Board of Supervisors approval or is appealed to the Board of Supervisors as a conditional use, in which case any appeal shall be made to the Board of Supervisors (see Charter Section 4.135).

Duration of this Certificate of Appropriateness: This Certificate of Appropriateness is issued pursuant to Article 10 of the Planning Code and is valid for a period of three (3) years from the effective date of approval by the Historic Preservation Commission. The authorization and right vested by virtue of this action shall be deemed void and canceled if, within 3 years of the date of this Motion, a site permit or building permit for the Project has not been secured by Project Sponsor.

THIS IS NOT A PERMIT TO COMMENCE ANY WORK OR CHANGE OF OCCUPANCY UNLESS NO BUILDING PERMIT IS REQUIRED. PERMITS FROM THE DEPARTMENT OF BUILDING INSPECTION (and any other appropriate agencies) MUST BE SECURED BEFORE WORK IS STARTED OR OCCUPANCY IS CHANGED.

I hereby certify that the Historical Preservation Commission ADOPTED the foregoing Motion on November 18, 2015.

Jonas P. Ionin
Commission Secretary

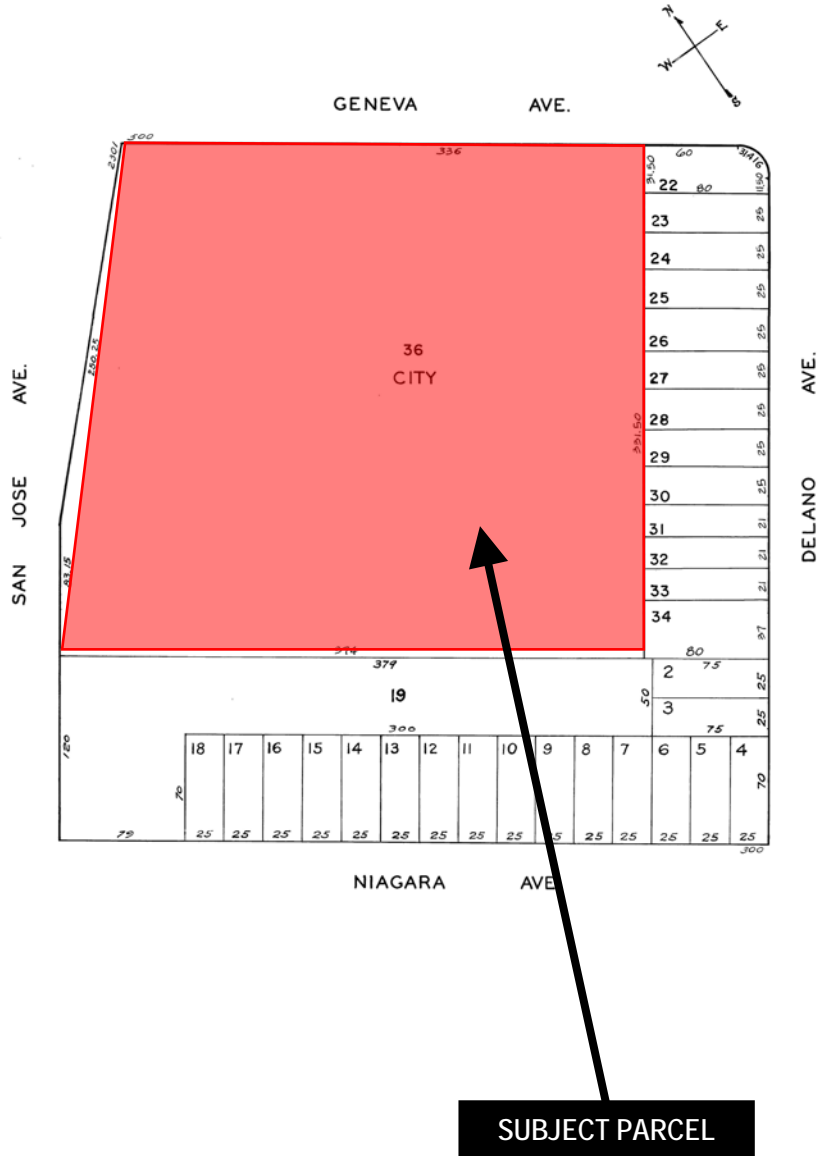
AYES: X

NAYS: X

ABSENT: X

ADOPTED:

Parcel Map

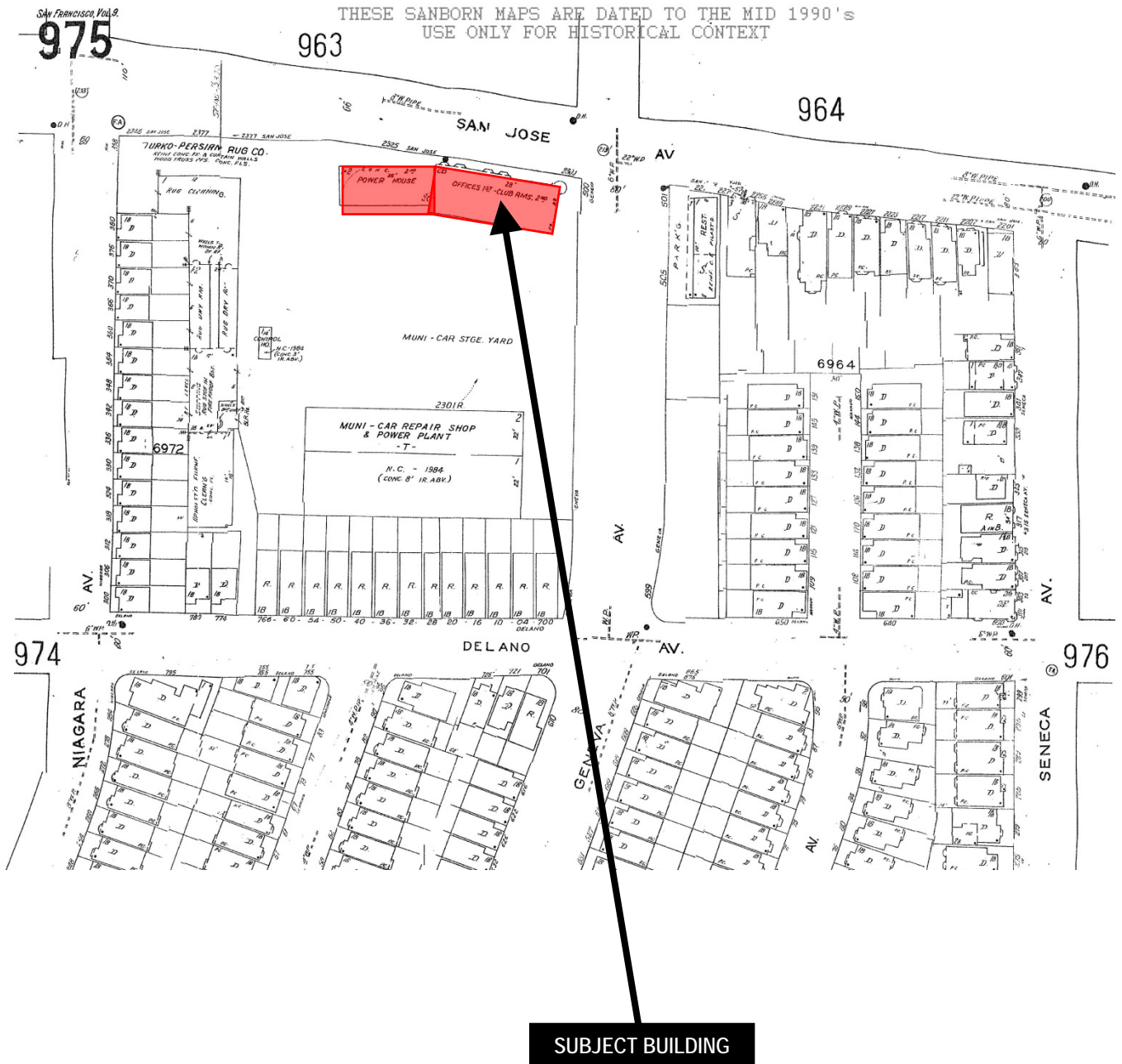


SUBJECT PARCEL



Certificate of Appropriateness Hearing
 Case Number 2012.0262COA
 Geneva Office Building/2301 San Jose Ave

Sanborn Map*



*The Sanborn Maps in San Francisco have not been updated since 1998, and this map may not accurately reflect existing conditions.



Certificate of Appropriateness Hearing
Case Number 2012.0262COA
Geneva Office Building/2301 San Jose Ave

Aerial Photo

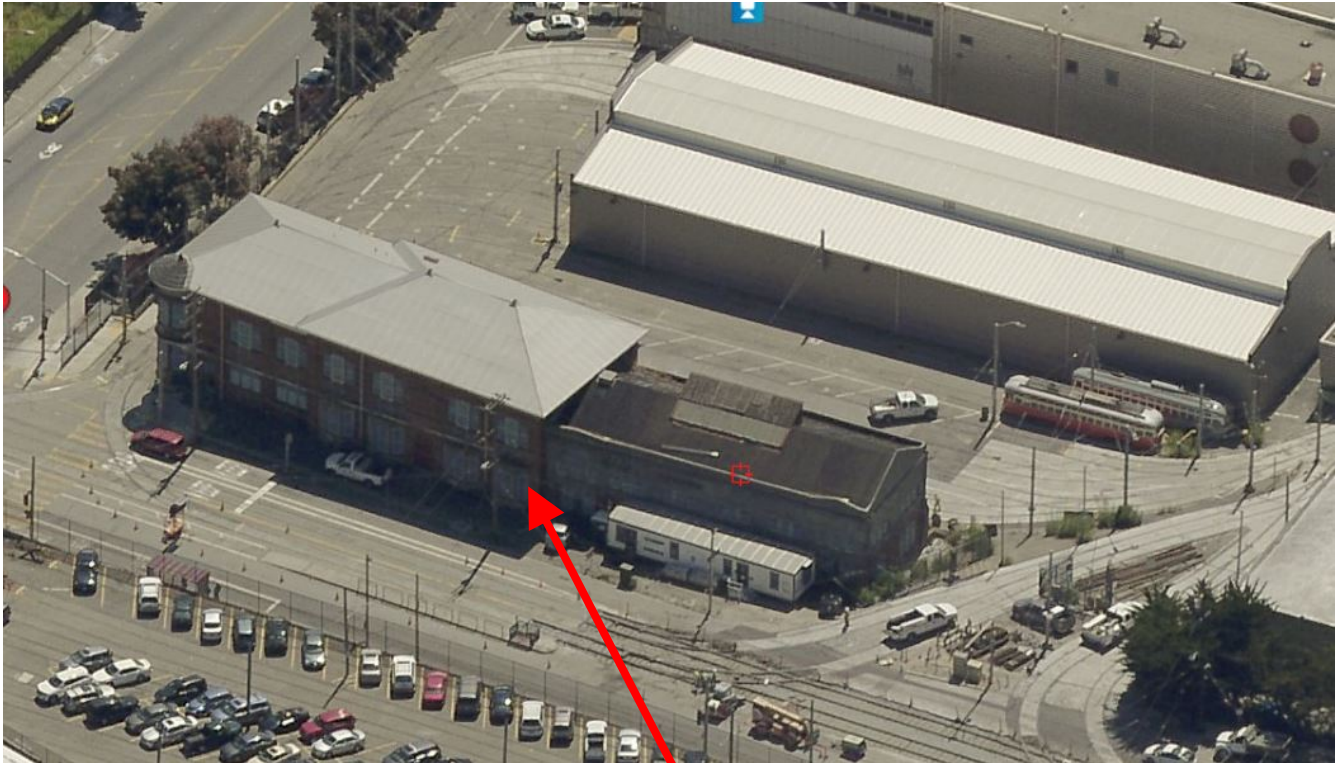


SUBJECT PROPERTY



Certificate of Appropriateness Hearing
Case Number 2012.0262COA
Geneva Office Building/2301 San Jose Ave

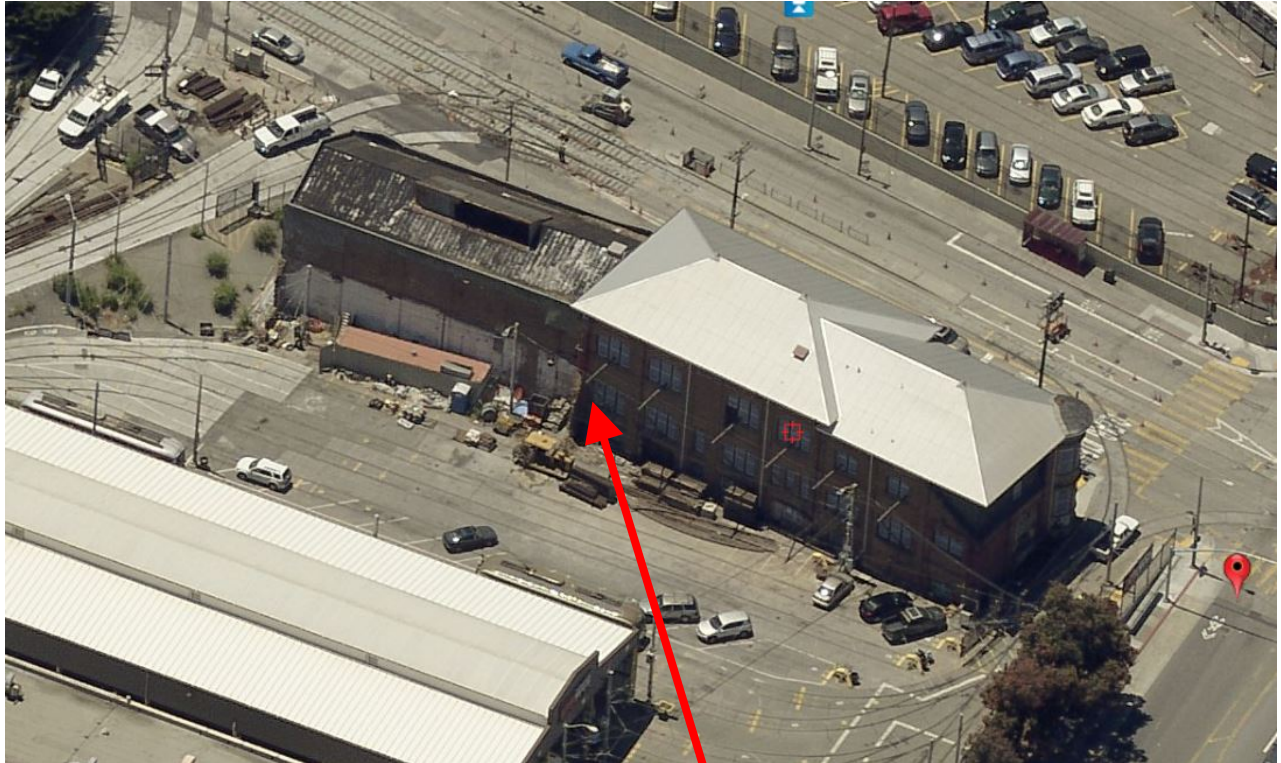
Photo



SUBJECT PROPERTY

Certificate of Appropriateness Hearing
Case Number 2012.0262COA
Geneva Office Building/2301 San Jose Ave

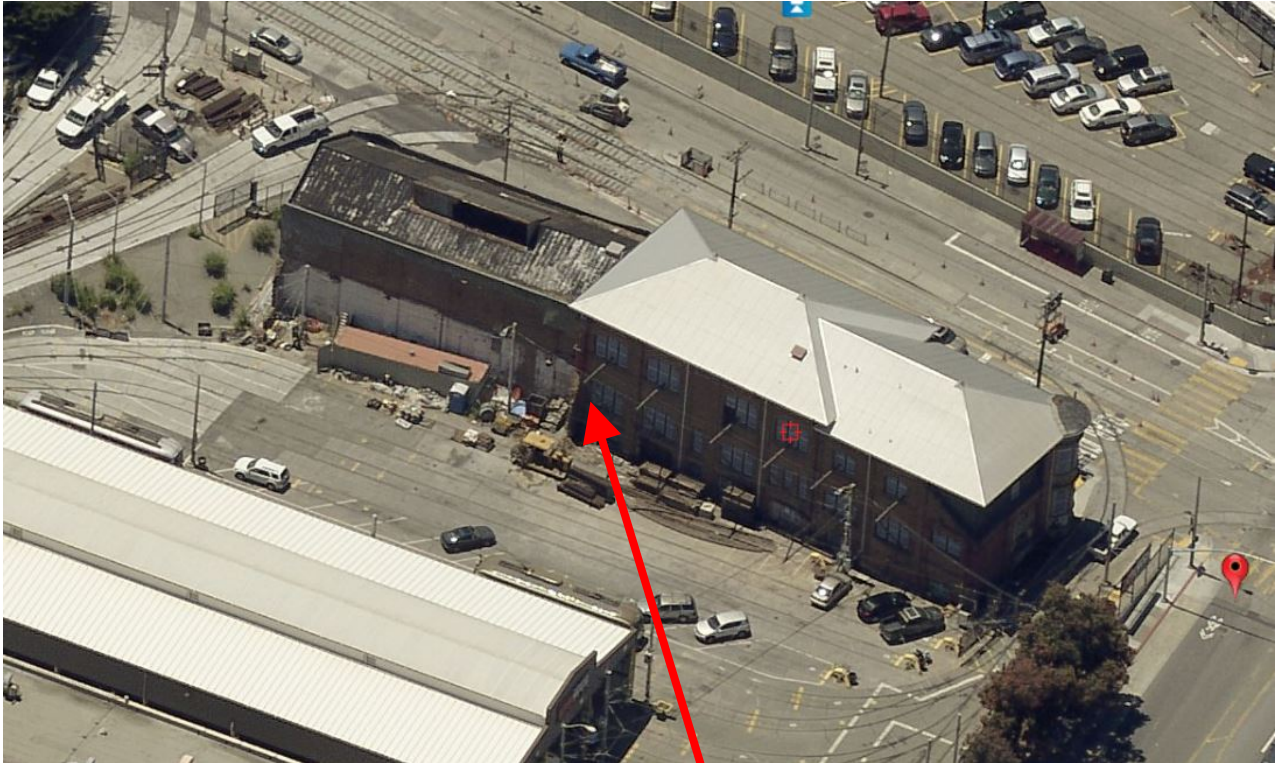
Photo



SUBJECT PROPERTY

Certificate of Appropriateness Hearing
Case Number 2012.0262COA
Geneva Office Building/2301 San Jose Ave

Photo



SUBJECT PROPERTY

Certificate of Appropriateness Hearing
Case Number 2012.0262COA
Geneva Office Building/2301 San Jose Ave



SHEET INDEX

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2	PROJECT NARRATIVE
3	SITE CONTEXT
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10	SITE PHOTOS - FIRST FLOOR OFFICE BUILDING
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13	SITE MATERIAL PALETTE
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15	PROPOSED VIEW
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GENEVA CAR BARN COMPLEX: GENEVA OFFICE BUILDING AND POWERHOUSE NARRATIVE

History

Designed by the Reid Brothers at the turn of the century, the Geneva Office Building and Power House were originally constructed as part of the larger Geneva Complex to serve the city's first electric railway system. Although the original adjoining car barn and ancillary maintenance shops have been demolished, the area remains active with various transportation-related uses. The property consists of two adjoining buildings – the Office Building, constructed in 1901, and the Power House, constructed in 1903 and heavily rebuilt in 1910 following the 1906 San Francisco Earthquake. The Office Building is a two-story brick utilitarian structure with a rectangular plan and a corrugated metal hipped roof. Its Queen Anne Style turret and the partial-hipped bay windows along the western front façade, both crafted and detailed in wood, provide contrast to the Romanesque Style masonry wall construction. The original program for the building combined a number of elements, including the administrative offices of the railroad, a dispatch office for the carmen, a staff lounge, and a large assembly hall. The Power House is a one-and-a-half-story, long rectangular structure positioned directly to the south of the Office Building, sharing a common wall with the adjacent building. The original brick walls and pilasters remain at the first floor of the Power House, while the concrete wall and parapet construction at the upper mezzanine level dates to the 1910 reconstruction. A large corrugated metal monitor straddles the gable roof, which is presently sheathed with a temporary membrane roof. The interior of the Power House consists of a large machine hall, with a small cast iron mezzanine along the west wall. Due to a lack of maintenance, both buildings remain in fair to poor condition; however, they both retain sufficient integrity to convey their architectural significance and historical associations, evoking an earlier era of transportation technology.

After being heavily damaged in the 1989 Loma Prieta earthquake, the Geneva Car Barn Complex was abandoned and left to deteriorate. In 1998, Muni announced plans to demolish the remaining structures. In response, the Friends of the Geneva Car Barn and Powerhouse was formed as a neighborhood citizens group and succeeded in halting the demolition plans. Between 1999 and 2000, a Stabilization Project for the building provided temporary seismic bracing and a new roof. In March 2004, ownership of the building was transferred to the San Francisco Department of Recreation and Parks who, in partnership with the Friends group, developed a specific program for adaptive reuse based on Chicago's award-winning Gallery 37.

Historic Status

The Geneva Office Building and Power House, also known as the Geneva Complex, possess both historical and architectural significance. In 1985, the Geneva Car Barn Complex was declared a San Francisco city landmark and in 2010, it was listed on the National Register of Historic Places. The buildings are historically significant for their local association with the development of San Francisco's electrical railway system, as well as for their local association with labor history in San Francisco. Originally constructed for The San Francisco and San Mateo Railroad Company between 1901 and 1903, the two adjoining buildings served as the administrative center and as a source of power for the city's first electric railroad company and are the last remaining physical reminders of it. The Geneva Complex also played an important role in labor history as the site of the Carmen's Strike of 1917, the impact of which is still evident in exterior modifications to the Office Building. The Office Building and Power House are also architecturally significant, as they embody the characteristics of both the Romanesque and Queen Anne Styles in an eclectic blend that also incorporates industrial elements appropriate for a working rail yard. The brick masonry construction is representative of the pre-1906 Earthquake period in San Francisco. In a city where brick is no longer a predominant building material, the Geneva Complex is a good example of pre-Earthquake use of brick to convey architectural detailing.

Project Summary

The rehabilitation of the Geneva Car Barn Complex will preserve and restore the exterior façade and historic finishes of the period-of-significance building while modifying the interior for code compliance, seismic strengthening and the building's new use as a youth arts education center and community cultural center. The building will house art education studios, a black box theater, a restaurant and support spaces within the Office Building. The Powerhouse will provide a 300 person event space for community and art functions, with a central lobby that provides access to the adjacent theater and support spaces within the Office Building.

Exterior

The exterior massing and envelope will remain and will be preserved. The entrances to the Office Building will be restored and access upgraded to current code and accessibility standards with new concrete stairs and integrated sloped walks that are deferential to the historic features. At the Powerhouse entrances, the large wood doors will be restored and placed in their open position with a new steel and glass window wall system inserted to provide accessible entrance and egress from the space. Solar panels will be added to the East-facing roof of the Office Building as well as both sides of the Powerhouse roof; the panels serve not only to provide power to the building, but also present a clear signal of the City's commitment to the environment while presenting a modern day nod to the Powerhouse's original use.

In addition to restoring the historic features of the Reid Brothers' design, the project will retain two elements that have historical significance as markers of significant events. In the 1906 earthquake, a piece of the north side of the Office Building collapsed and was subsequently infilled with a wood framed wall with tar paper; at this location, the wall will be rebuilt to meet seismic standards and will be sheathed with dark metal cladding to retain the visual appearance of the building paper in a durable material. In the Carmen Strike of 1917, an opening was cut in the east side to allow Carmen to access the trains without confronting those picketing the strike; the project will retain and glaze this opening.

Office Building

Within the Office Building, the Period of Significance configuration and finishes will be restored with code compliant stairs, elevator and bathrooms inserted in secondary spaces. The building will be fully seismically upgraded. On the second floor, the central rooms will be returned to a large open space of the black box theater. A third floor will be constructed within the building envelope utilizing the current attic space to provide additional flexible education studios and support spaces for the theater.

Powerhouse

Within the Powerhouse, a new steel and glass structure will be inserted to create an entrance lobby, stair and theater mezzanine for the space's public use. A new concrete floor will be installed with structural glass at two former turbine locations to provide vitrines. The existing iron mezzanine will be retained with infrastructure inserted below for catering support functions.

Where new elements are inserted, they will be clearly distinct and modern while being compatible in proportion and material to the existing in keeping with the Secretary of the Interior Standards. The project has received SHPO Part 2 approval.

Once complete, the adaptively renovated building will serve as a cultural hub for the community in a very underserved neighborhood.

Phasing

The Project is split into 2 Phases.

Phase 1: Renovation of Powerhouse for use as Community Arts and Events Facility
Construction in Fall of 2016 for use in Summer of 2017

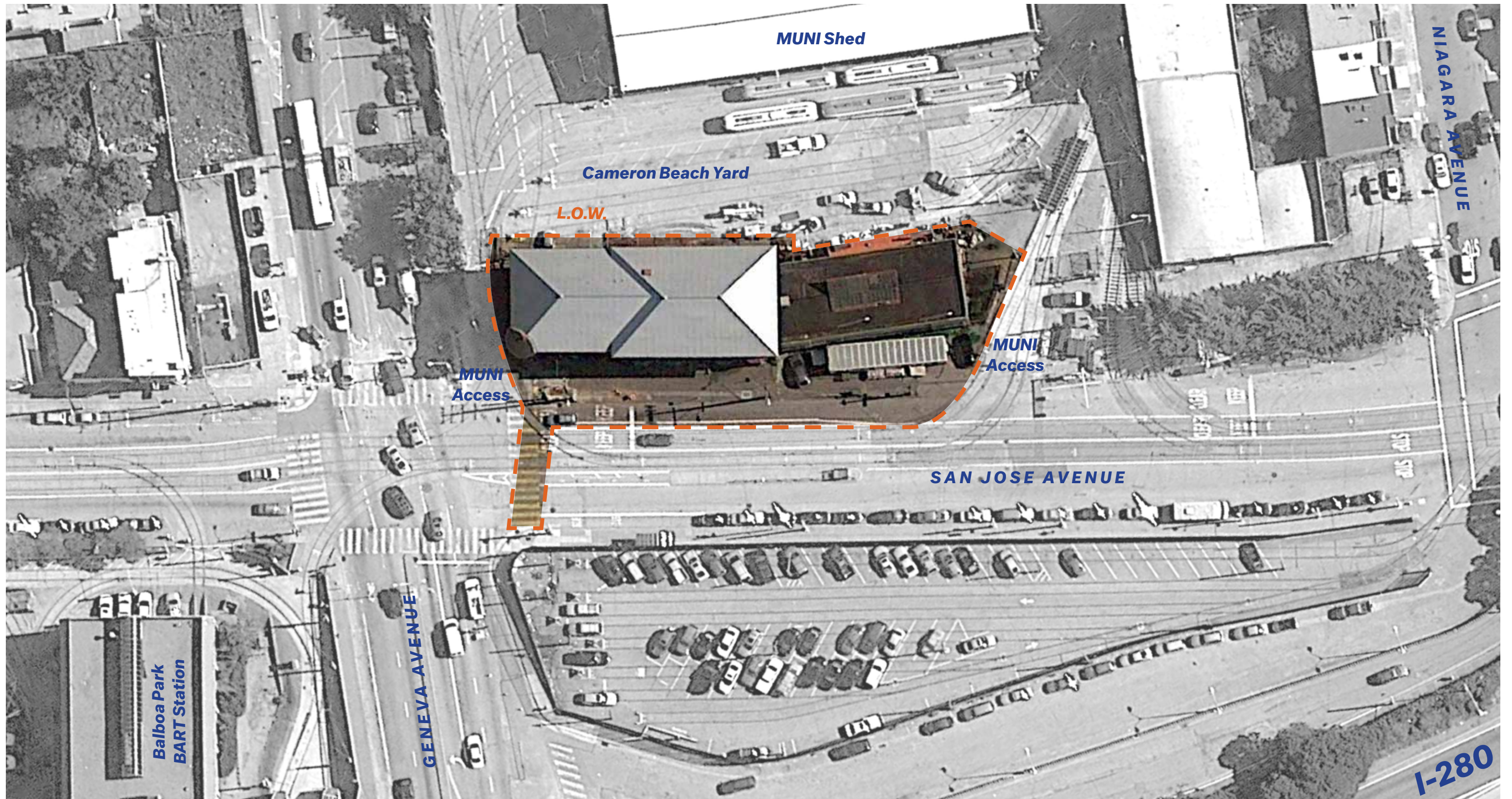
- Powerhouse façade restoration and installation of new entry doors
- Powerhouse seismic upgrade
- Powerhouse interior renovation including installation of temporary utility and storage enclosures under existing mezzanine.
- Site improvements to the north and west sides of the site, including a new sidewalk along San Jose and crosswalk improvements
- Installation of transformer and trash enclosure on East side of building
- Installation of temporary toilet trailer at east side of the site with temporary ramp from the Powerhouse through an opening of bricked-in original door

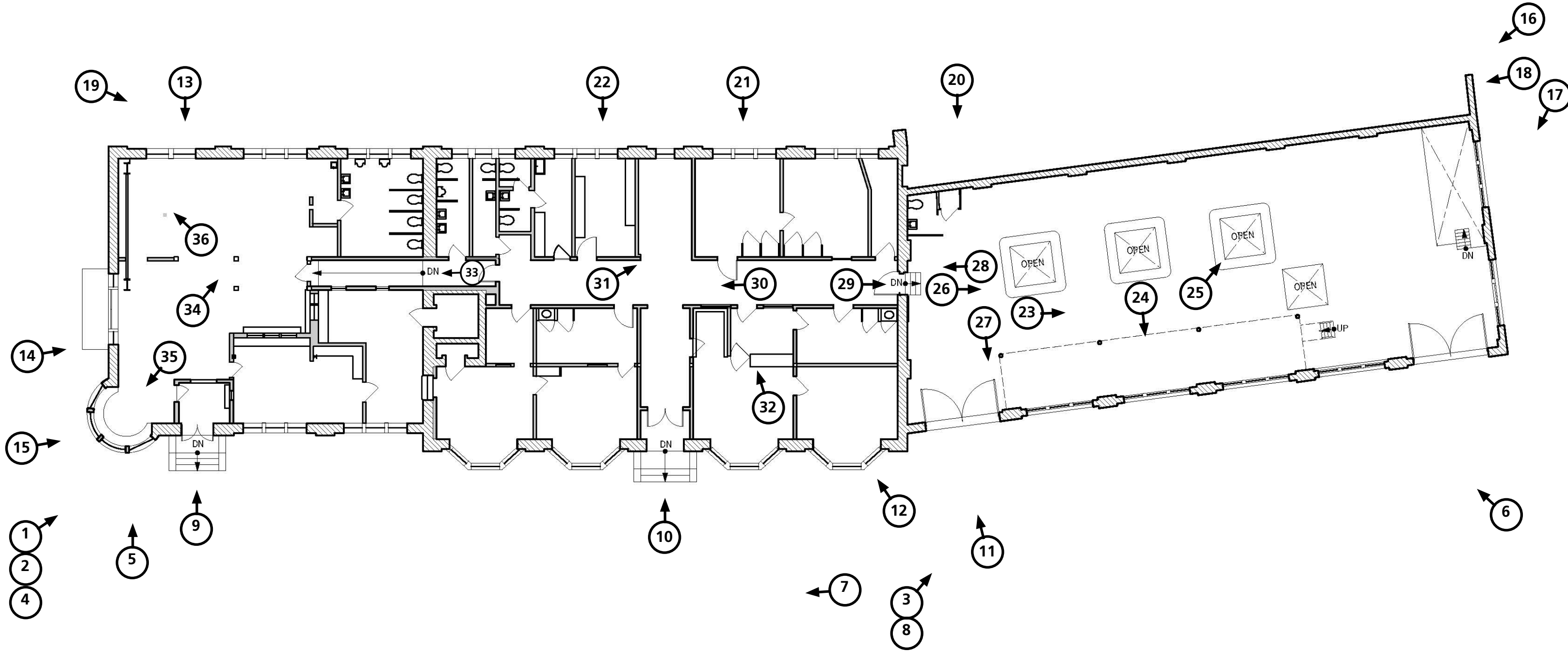
Phase 2: Renovation of Office Building for use as Youth Art Training Facility, including art studios, a black box theater and restaurant. Completion of work in Powerhouse to add lobby, stair and theater mezzanine. Construction awaiting Funding, potential for start in 2018 with completion in 2020

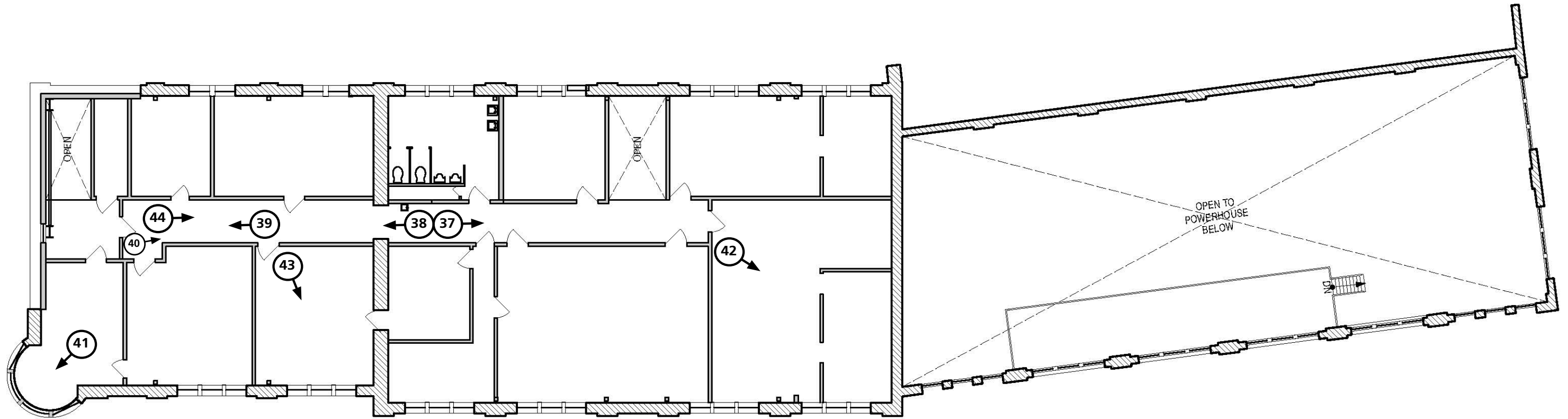
- Office Building façade restoration
- Office Building seismic upgrade
- Office Building interior renovation including insertion of third floor in attic space
- Powerhouse insertion of new entry lobby, stair and theater mezzanine
- Powerhouse removal of temporary insertions and addition of catering kitchen area under mezzanine
- Completion of site improvement work
- Signage program for both buildings



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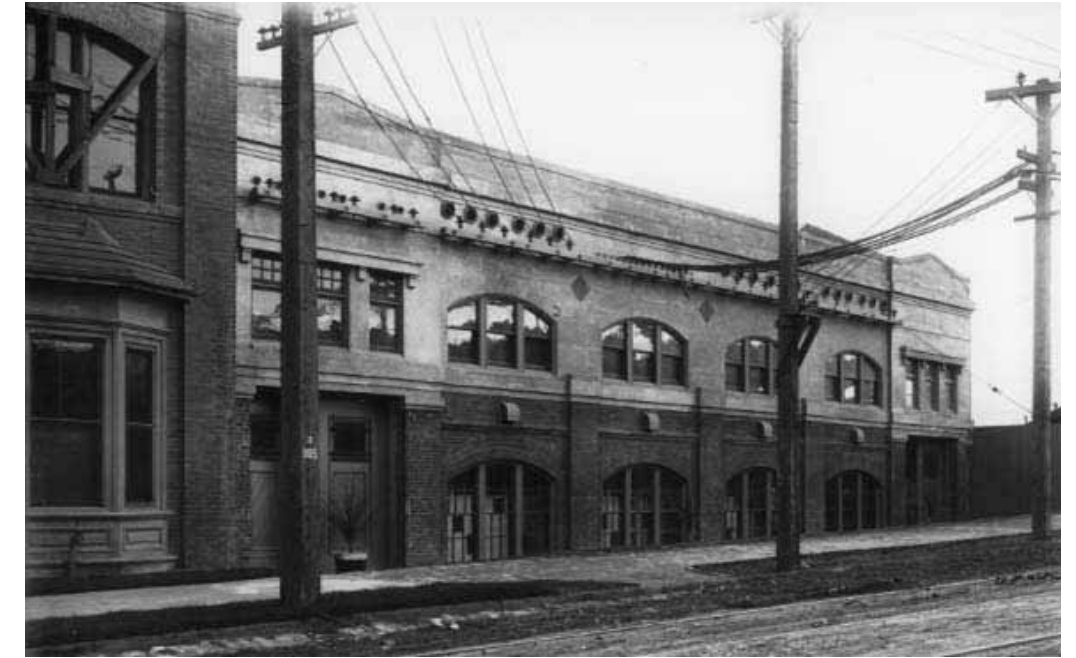




1. Historic Photo: Exterior View looking southeast



2. Historic Photo: Exterior view of Office Building looking east



3. Historic Photo: 1910 view of reconstructed Powerhouse from west



4. Exterior view of Office Building looking southeast



5. Office Building view of west elevation



6. Powerhouse view of west and south walls



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7. Office Building sidewalk and entrances



8. Powerhouse sidewalk and sloped entrances



9. Office Building north entrance



10. Office Building south entrance



11. Powerhouse north entrance



12. Office Building west elevation view of typical bay



13. Office Building east elevation view of 2nd floor wall reconstruction at 1906 earthquake damage



14. Office Building north elevation view of 2nd floor wall reconstruction at 1906 earthquake damage



15. Office Building view of corner turret





16. Powerhouse view of the south and east walls



17. Powerhouse view of sloped embankment at south elevation.



18. Powerhouse southeast corner wall fragment



19. View of working muni yard fragment



20. Powerhouse east elevation view of bricked in door opening



21. Office Building east elevation service doors to basement



22. Office Building east elevation view of window modified into door opening in Carmen Strike of 1917



23. Powerhouse interior view of skylight



24. Powerhouse interior view of upper west wall



25. Powerhouse interior view of former turbine location



26. Powerhouse interior view looking south



27. Powerhouse interior view of west mezzanine and entry doors



28. Powerhouse interior view of door opening to Office Building



29. Office Building south wing view of door to Powerhouse



30. Office Building south wing corridor looking north



31. Office Building south wing view of corridor at entry hall



32. Office Building south wing view of casework



33. Office Building hallway and ramp connecting south wing with north wing looking north



34. Office Building north wing view looking southeast



35. Office Building north wing view of turret



36. Office Building north wing view of northeast corner



37. Office Building Second Floor south wing hallway infill looking south



38. Office Building Second Floor south wing hallway infill looking north



39. Office Building Second Floor north wing hallway



40. Office Building Second Floor north wing hallway wainscot



41. Office Building Second Floor north wing view of turret



42. Office Building Second Floor south wing view of additions within auditorium



43. Office Building Second Floor north wing room

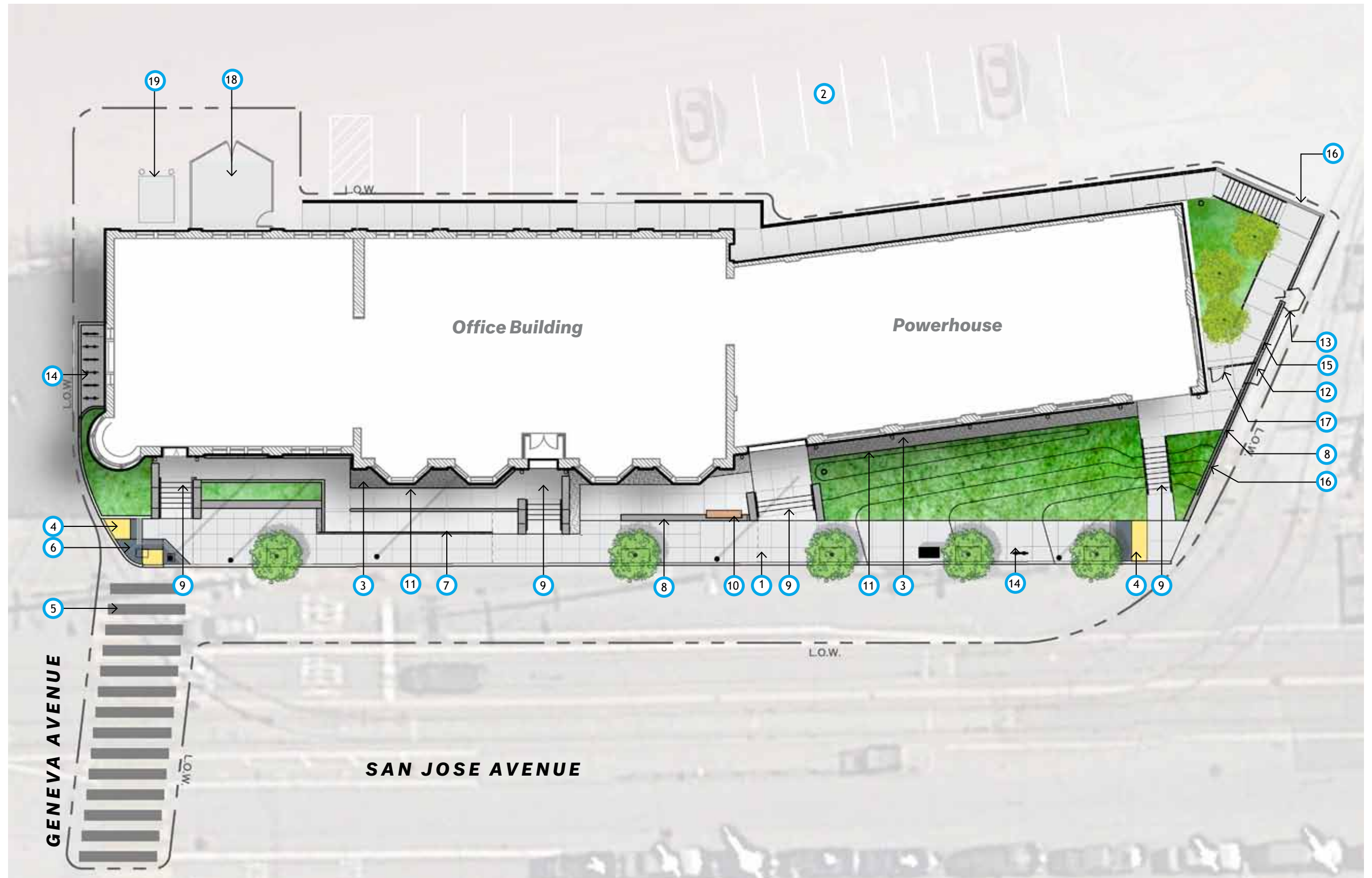


44. Office Building north wing view of attic



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- ① **Natural Concrete**
With Saw-Cut Joints
- ② **Muni Parking**
- ③ **Crushed Rock Mulch**
- ④ **Detectable Warning Paving**
- ⑤ **Painted Crosswalk**
- ⑥ **Concrete Curb Ramp**
- ⑦ **Weathered Steel Plate Sign**
- ⑧ **Concrete Seat Wall**
- ⑨ **Concrete Stair**
- ⑩ **Custom Wood Bench**
- ⑪ **Steel Landscape Header**
- ⑫ **(E) Wall to Remain**
- ⑬ **(E) Muni Yard Gate**
-To remain
- ⑭ **Bike Racks**
SF city standard bike racks
- ⑮ **Security Fence**
8'-0" w/ Welded Wire Mesh Infill
- ⑯ **Guardrail**
42" w/Wire mesh infill
- ⑰ **Emergency Egress Gate**
- ⑱ **Recycle/Trash Enclosure**
- ⑲ **Transformer**





Saw-Cut Natural Concrete



Security Fence and Gate- Amopanel by Ametco- color black



Crushed Rock Mulch- Black Basalt



Bike Rack Welle by Palmer Group



Weathered Steel Sign (by others)



Western Red Cedar Bench



Painted Steel Plate Curb (Custom Themec paint color)

Office Building - Corrugated Roof Color



- *Powerhouse - New Metal Entry Storefront and Metal Surround*
- *Office Building - Aluminum Panels at c1906 Wall*



Office Building Brick Color (For General Reference only, Not Accounting For Natural Color Variations)



Painted Sheet Metal Downspout, Bay Window Metal Roofing, Metal Railing Color









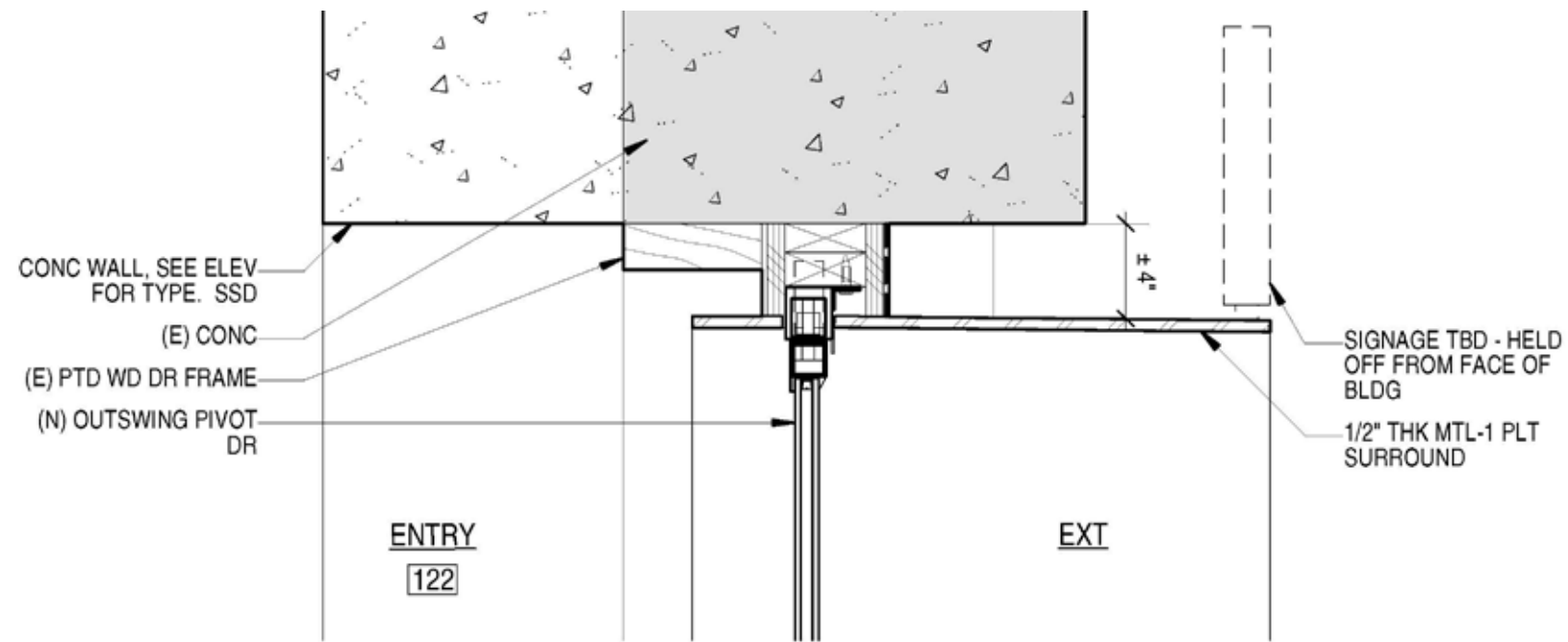




PROPOSED VIEW



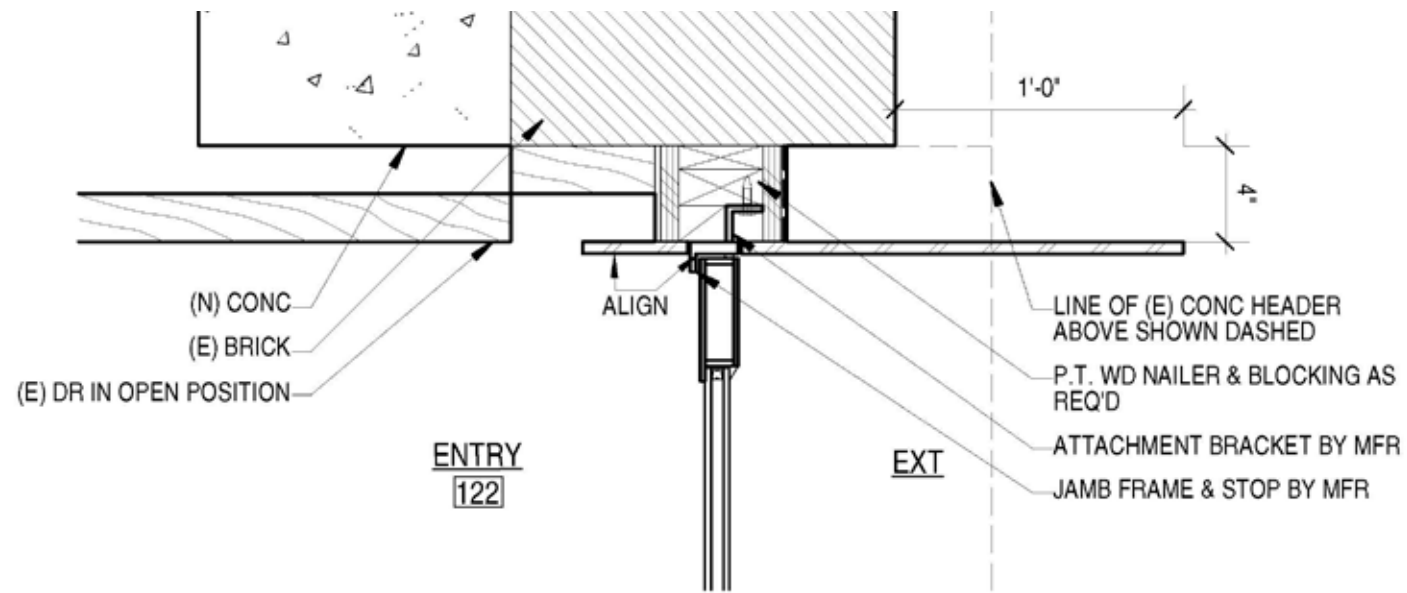
PROPOSED VIEW



SECTION DETAIL - SILL & HEAD AT DOOR 122A

0 8" 1' 1 1/2" = 1'-0"

11



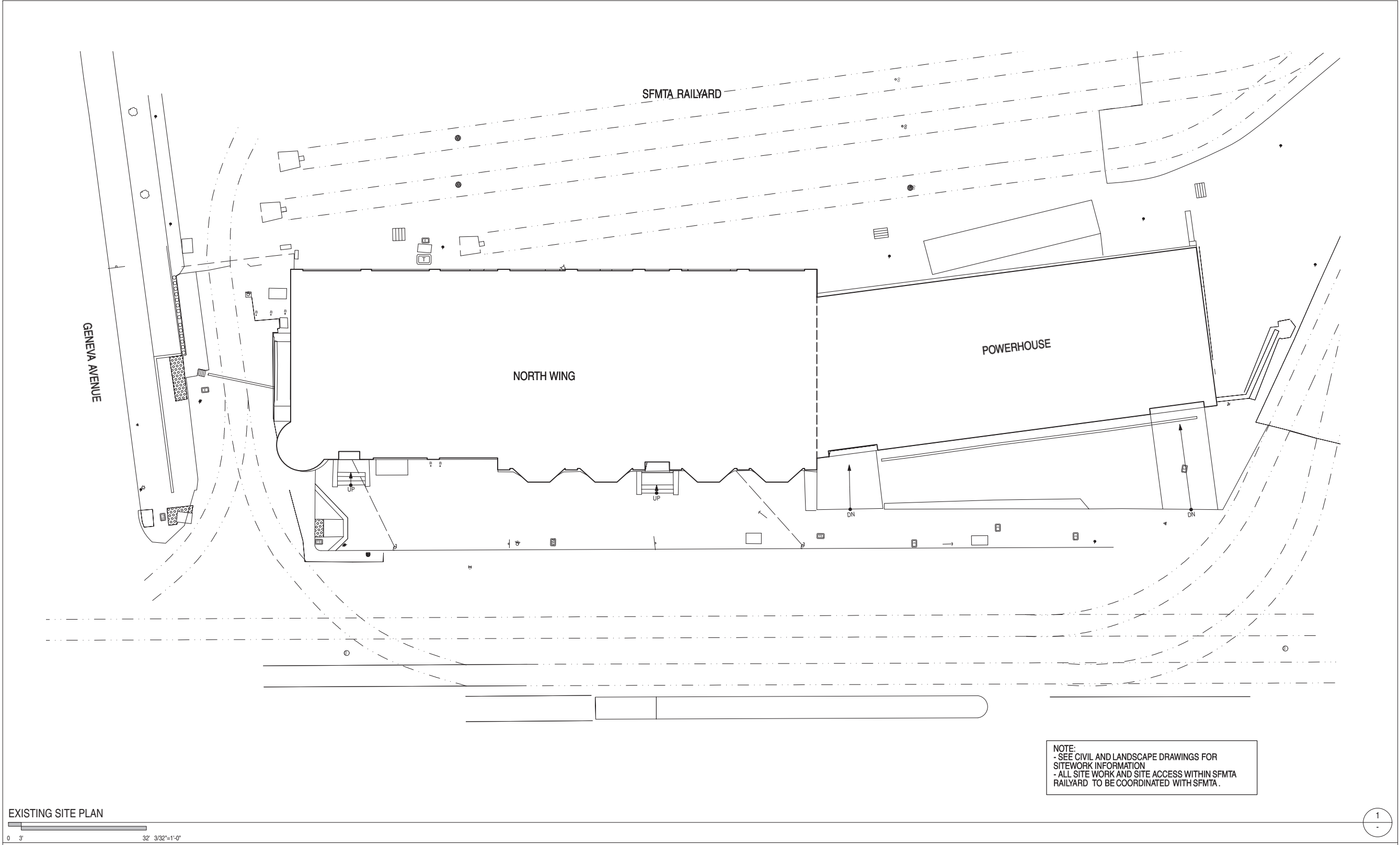
PLAN DETAIL - JAMB PROFILE AT DOOR 122A

0 8" 1' 1 1/2" = 1'-0"

18



EXISTING CONDITIONS AND PHASE 1 SCOPE OF WORK



NOTE:
 - SEE CIVIL AND LANDSCAPE DRAWINGS FOR SITEWORK INFORMATION
 - ALL SITE WORK AND SITE ACCESS WITHIN SFMTA RAILYARD TO BE COORDINATED WITH SFMTA.

GENEVA CAR BARN AND POWERHOUSE

2301 SAN JOSE AVENUE
 SAN FRANCISCO, CA

DATE: 08.08.14

JOB: 0906

DRAWN: MH

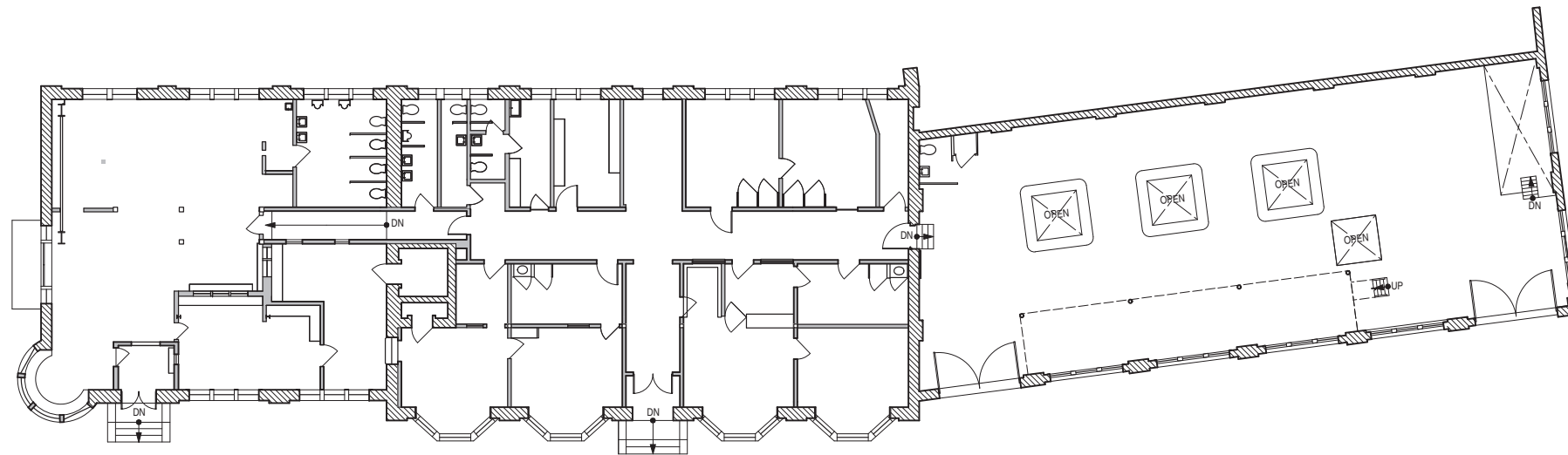
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100% DD SET	06.03.15
C OF A SET	09.11.15

EXISTING SITE PLAN



EXISTING SITE PLAN

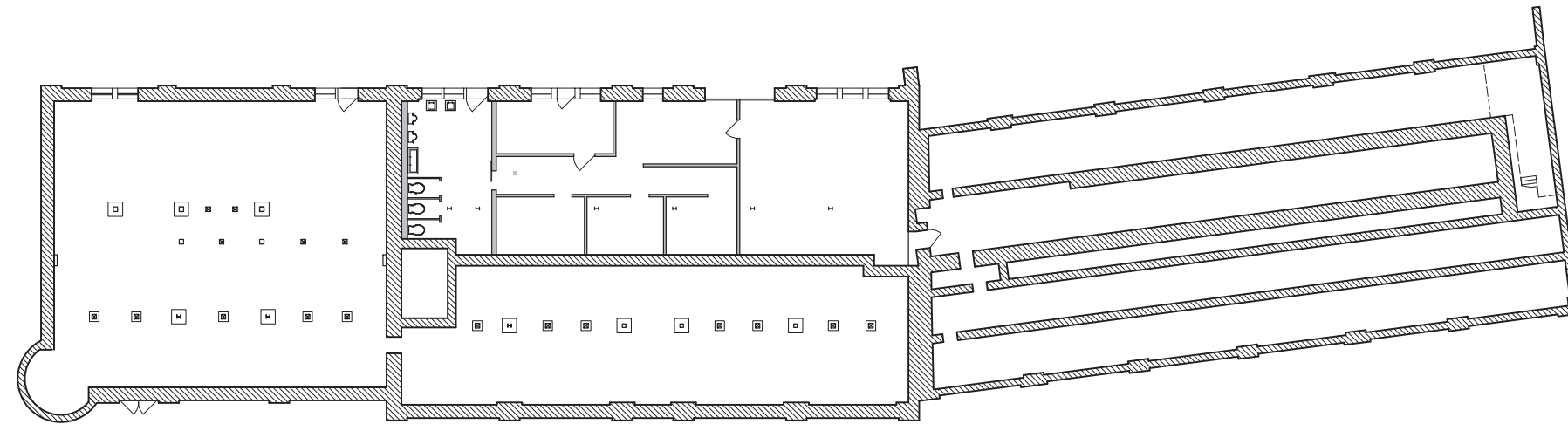


KEY

	(E) BRICK
	(E) WALL

LEVEL 1 EXISTING PLAN

0 3' 32' 3/32"=1'-0"



KEY

	(E) BRICK
	(E) WALL

BASEMENT LEVEL EXISTING PLAN

0 3' 32' 3/32"=1'-0"

2

GENEVA CAR BARN AND POWERHOUSE

2301 SAN JOSE AVENUE
SAN FRANCISCO, CA

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JOB: 0906

DRAWN: MH

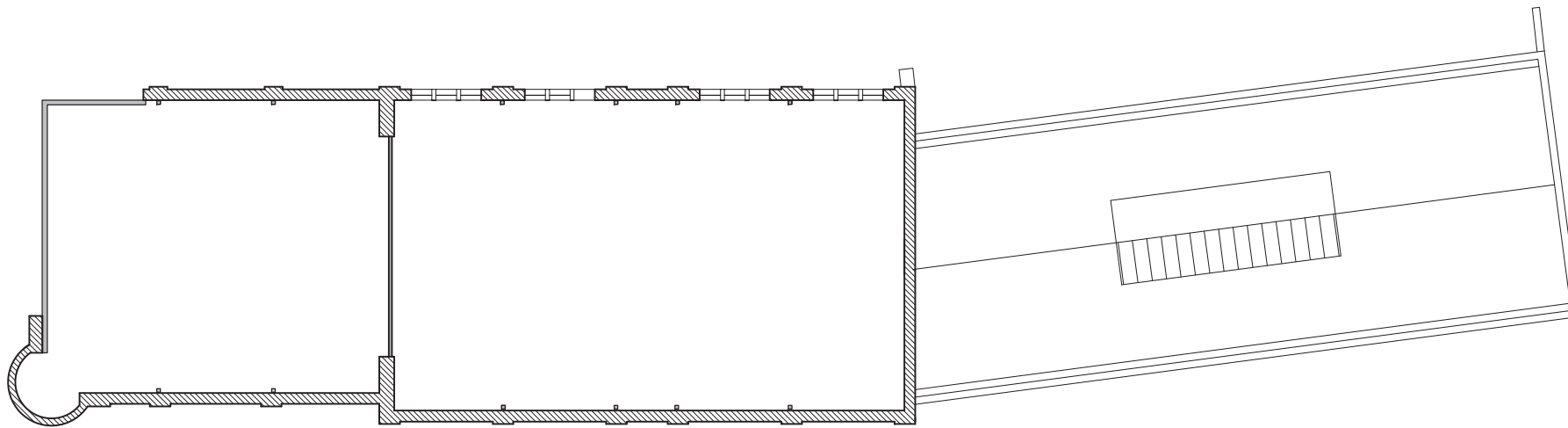
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ISSUE	DATE
50% DD SET	03.09.15
100% DD SET	06.03.15
C OF A SET	08.11.15



1

BASEMENT LEVEL &
LEVEL 1 EXISTING
PLANS

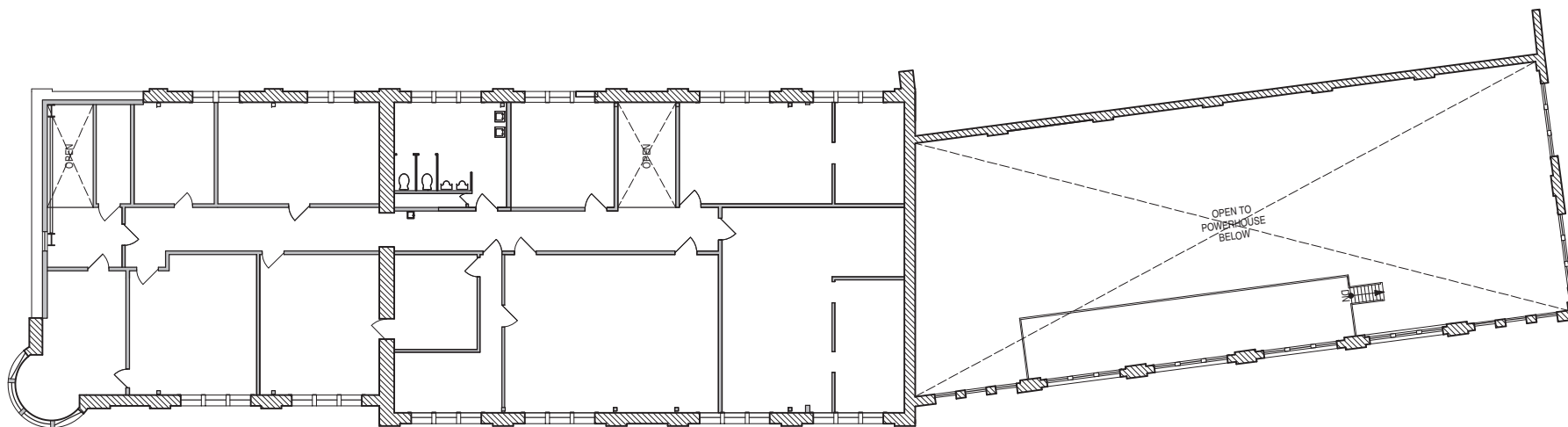


KEY	
	(E) BRICK
	(E) WALL

ATTIC LEVEL EXISTING PLAN

0 3' 32' 3/32"=1'-0"

2



KEY	
	(E) BRICK
	(E) WALL

LEVEL 2 EXISTING PLAN

0 3' 32' 3/32"=1'-0"

1

GENEVA CAR BARN AND POWERHOUSE

2301 SAN JOSE AVENUE
SAN FRANCISCO, CA

DATE: 08.08.14

JOB: 0906

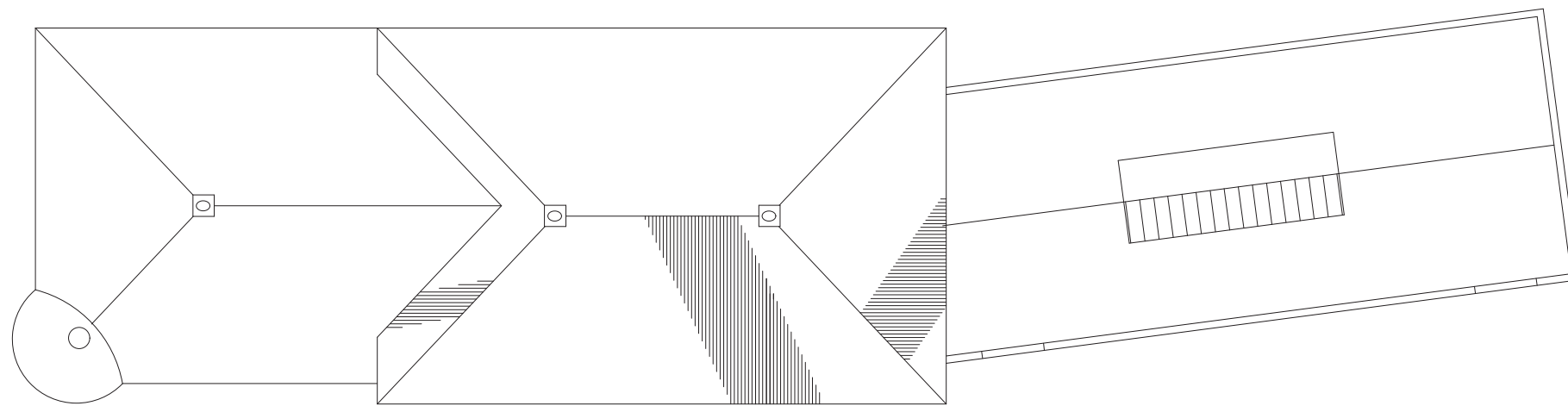
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SCALE: AS NOTED

ISSUE	DATE
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100% DD SET	06.03.15
C OF A SET	09.11.15



LEVEL 2 & LEVEL 3
EXISTING PLANS



KEY	
	(E) BRICK
	(E) WALL

ROOF DEMOLITION PLAN

0 3' 32' 3/32"=1'-0"

1
-



ROOF EXISTING PLAN

GENEVA CAR BARN AND POWERHOUSE

2301 SAN JOSE AVENUE
SAN FRANCISCO, CA

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DRAWN: MH

SCALE: AS NOTED

ISSUE	DATE
50% DD SET	03.09.15
100% DD SET	06.03.15
C OF A SET	09.11.15



aidlin darling design
 500 third street
 suite 410
 san francisco, ca 94107
 415 974 0849 fax
 415 974 5603 phone

**GENEVA CAR BARN AND POWERHOUSE
 PHASE ONE**

2301 SAN JOSE AVENUE
 SAN FRANCISCO, CA

DATE: 08.08.14

JOB: 0906

DRAWN: MH

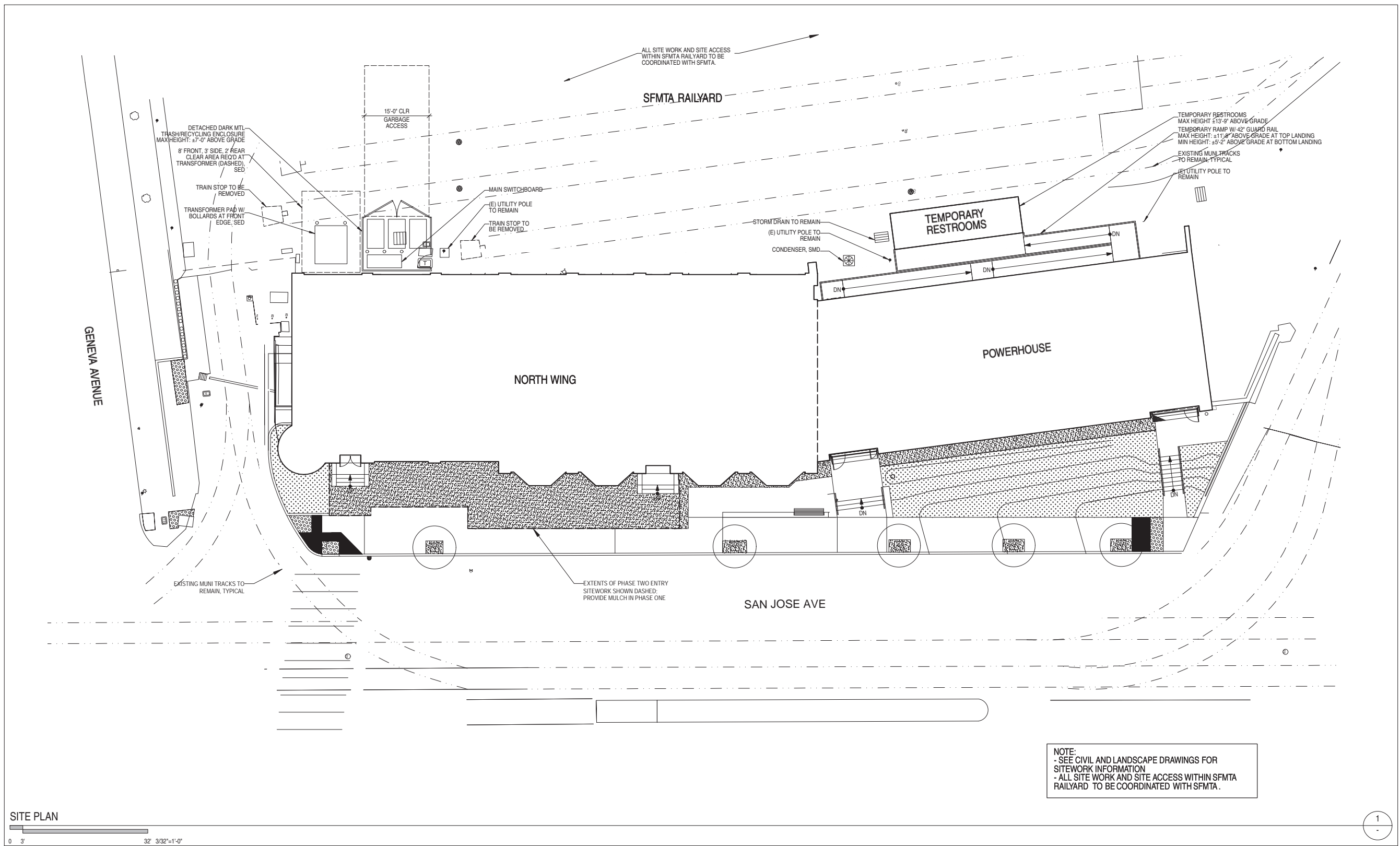
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ISSUE DATE

C OF A SET 08.11.15



**HPC
 A0.2
 PHASE ONE
 SITE PLAN**





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GENEVA CAR BARN
 AND POWERHOUSE
 PHASE ONE

2301 SAN JOSE AVENUE
 SAN FRANCISCO, CA

DATE: 08.08.14

JOB: 0906

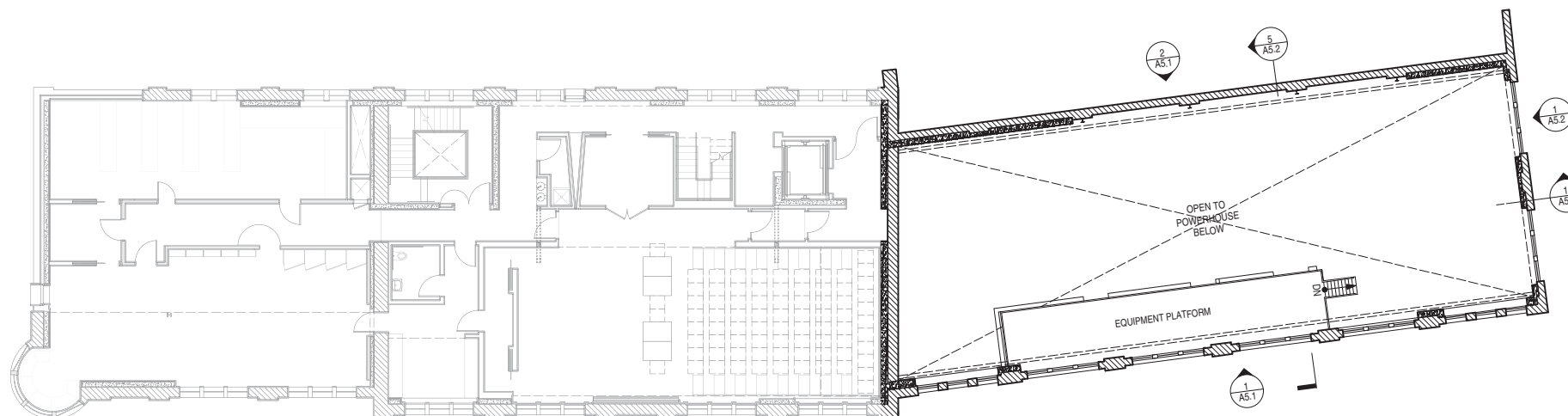
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SCALE: AS NOTED

ISSUE	DATE
C OF A SET	09.11.15



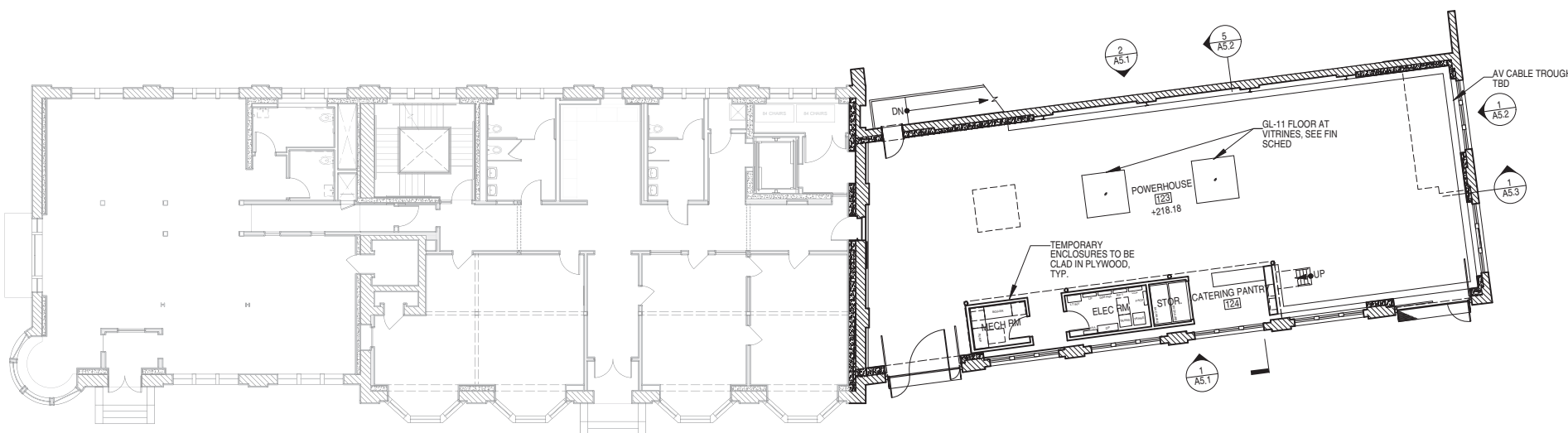
HPC
 A2.0
 PHASE ONE
 LEVEL 1 & LEVEL 2
 PLANS



PHASE ONE LEVEL 2 PLAN

0 3' 32' 3/32"=1'-0"

2



PHASE ONE LEVEL 1 PLAN

0 3' 32' 3/32"=1'-0"

1

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ENTIRE PROJECT/PHASE 2 SCOPE OF WORK



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GENEVA CAR BARN AND POWERHOUSE

2301 SAN JOSE AVENUE
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SCALE: AS NOTED

ISSUE DATE

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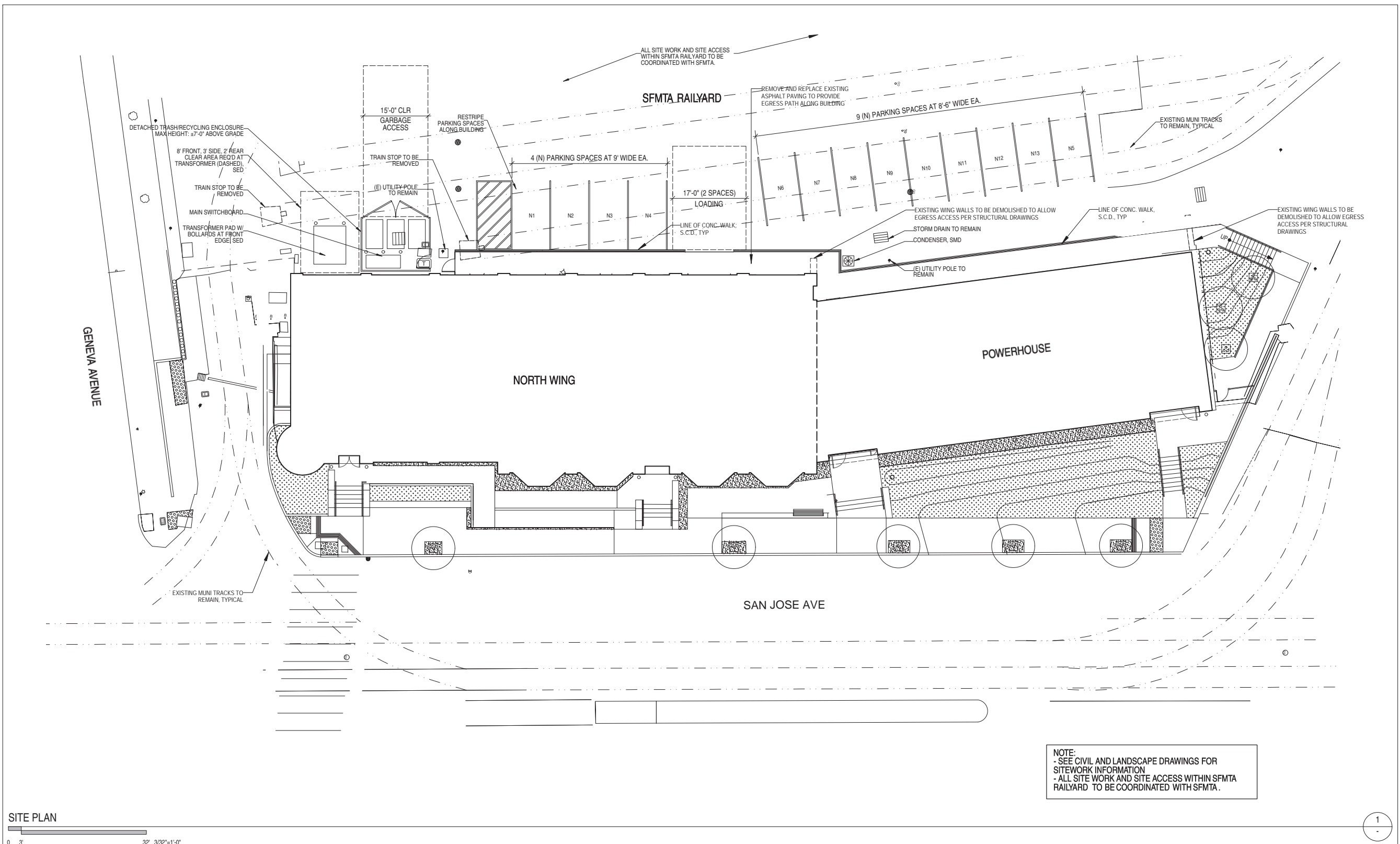
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A0.2

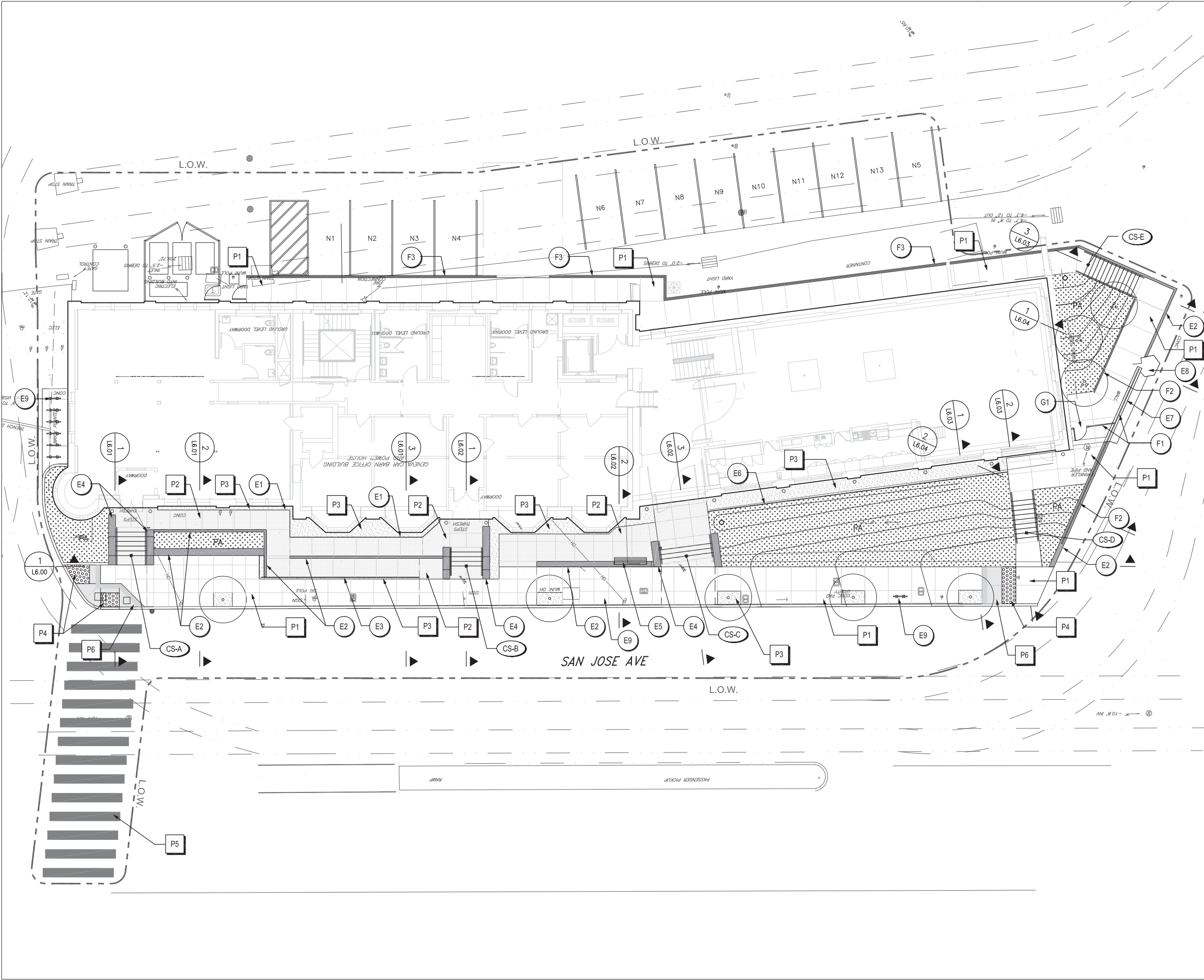
SITE PLAN



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SITE PLAN
 0 3' 32' 3/32"=1'-0"

1



PAVING SCHEDULE

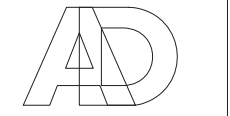
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P4	[Symbol]	DETECTABLE WARNING PAVER
P5	[Symbol]	PAINTED CROSSWALK
P6	[Symbol]	C.I.P. CONCRETE CURB RAMP

SITE ELEMENTS

TAG	SYMBOL	DESCRIPTION
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E2	[Symbol]	CIP CONCRETE RETAINING WALL
E3	[Symbol]	WEATHERED STEEL PLATE SIGN WALL
E4	[Symbol]	CIP CONCRETE CHEEK WALL
E5	[Symbol]	CUSTOM WOOD BENCH
E6	[Symbol]	STEEL LANDSCAPE HEADER
E7	[Symbol]	EXISTING WALL TO REMAIN
E8	[Symbol]	EXISTING MUNI YARD GATE POST TO REMAIN
E9	[Symbol]	BIKE RACK - CITY STANDARD

FENCE & GATE SCHEDULE

TAG	DESCRIPTION
F1	8' SECURITY FENCE W/ WELDED WIRE MESH INFILL
F2	42" GUARD RAIL W/ WELDED WIRE MESH INFILL
F3	42" GUIDE RAIL
G1	EMERGENCY EGRESS GATE



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 san francisco, ca 94107
 415 974 0849 fax
 415 974 5603 phone



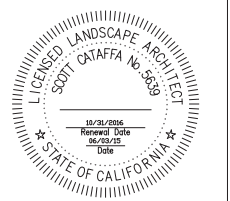
CONGER MOSS GUILLARD
 500 THIRD STREET SUITE 215
 SAN FRANCISCO, CA 94107
 TELEPHONE: 415/485-3070
 FACSIMILE: 415/485-3080

GENEVA CAR BARN
 AND POWERHOUSE

2301 SAN JOSE AVENUE
 SAN FRANCISCO, CA

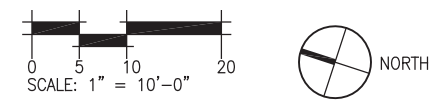
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100% DD SET	06.03.15
C OF A	09.11.15

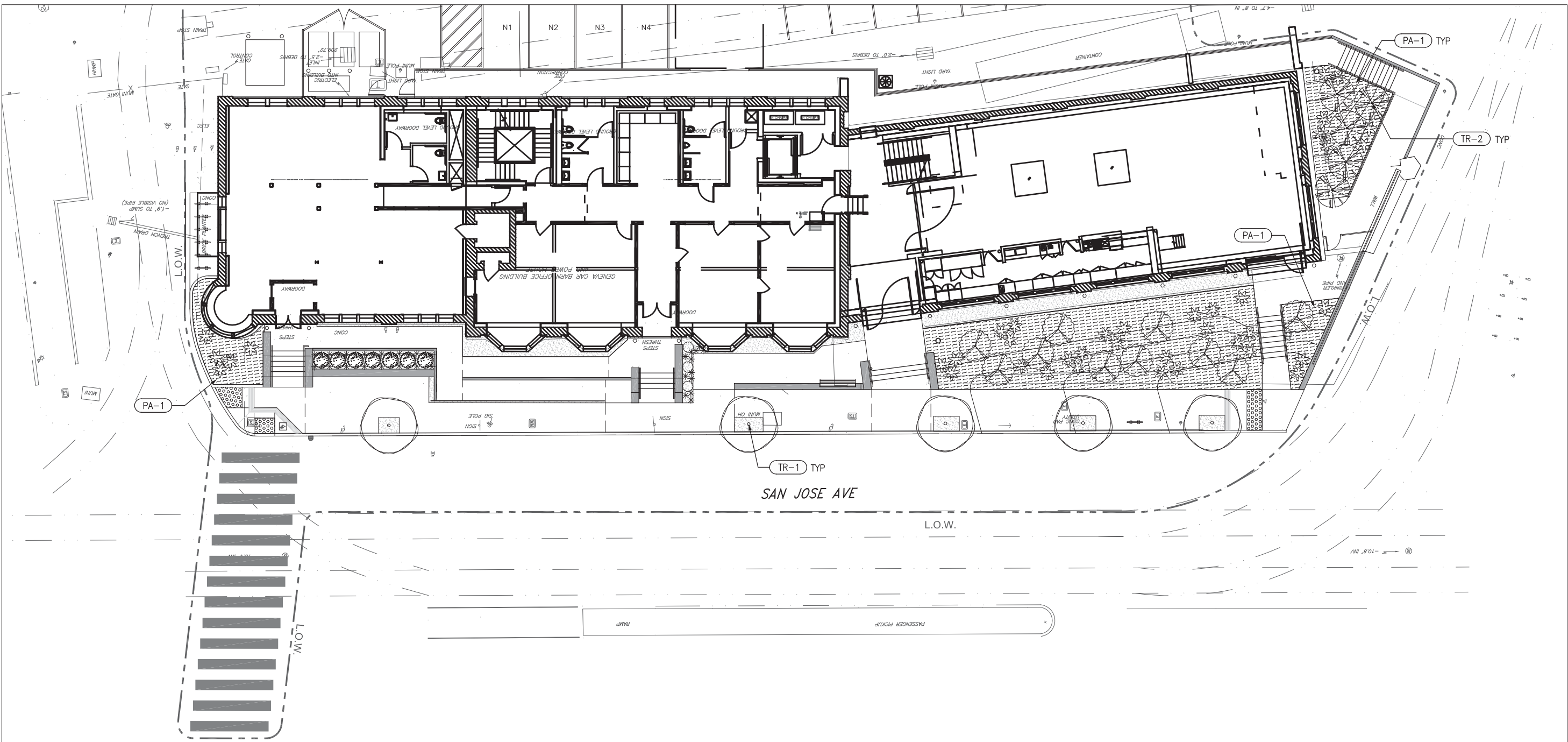
**NOT FOR
 CONSTRUCTION**



L1.01

MATERIALS
 PLAN





PLANTING SCHEDULE								
TYPE	SYMBOL	TAG	LATIN NAME	COMMON NAME	SIZE	SPACING	QUANT. / AREA	NOTES
TREES		TR-1	<i>Tristaniopsis laurina</i> 'Elegant'	ELEGANT BRISBANE BOX	24" BOX	PER PLAN	5	
		TR-2	<i>Hakea suaveolens</i>	SWEET HAKEA	24" BOX	PER PLAN	5	

PLANTING SCHEDULE								
TYPE	SYMBOL	TAG	LATIN NAME	COMMON NAME	SIZE	SPACING	QUANT. / AREA	NOTES
GROUND COVER		PA-1	<i>Ceanothus griseus</i> var. <i>horizontalis</i> 'Yankee Point'	CARMEL MOUNTAIN LILAC	1 GAL.	36"	1,700 SF	65% OF AREA
			<i>Fragaria chiloensis</i> 'Carole Anne'	COAST STRAWBERRY	1 GAL.	18"		25% OF AREA
			<i>Eriogonum latifolium</i>	COAST BUCKWHEAT	1 GAL.	18"		15% OF AREA
SHRUBS, GRASSES, & SUCCULENTS		LEU SAF	<i>Leucadendron</i> 'Safari Goldstrike'	YELLOW CONE BUSH	15 GAL.	PER PLAN	18	NO PHOSPHOROUS (P) ADDITIVES TO SOIL
		ARC DEN	<i>Arctostaphylos densiflora</i> 'Howard McMinn'	MANZANITA	15 GAL.	PER PLAN	11	
		AGA AME	<i>Agave americana</i>	CENTURY PLANT	5 GAL.	PER PLAN	39	
		AGA SHA	<i>Agave shawii</i> x <i>attenuata</i> 'Blue Flame'	BLUE FLAME AGAVE	5 GAL.	PER PLAN	3	
		ALO BLU	<i>Aloe</i> 'Blue Elf'	BLUE ELF ALOE	1 GAL.	PER PLAN	29	



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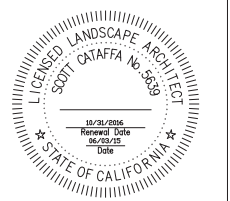
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 CONGER MOSS GUILLARD
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 TELEPHONE: 415/495-3070
 FACSIMILE: 415/495-3080

GENEVA CAR BARN AND POWERHOUSE

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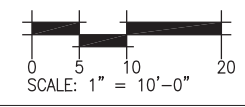
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PLANTING PLAN & SCHEDULE





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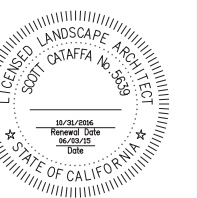
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DRAWN: MH

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L6.00

ELEVATIONS





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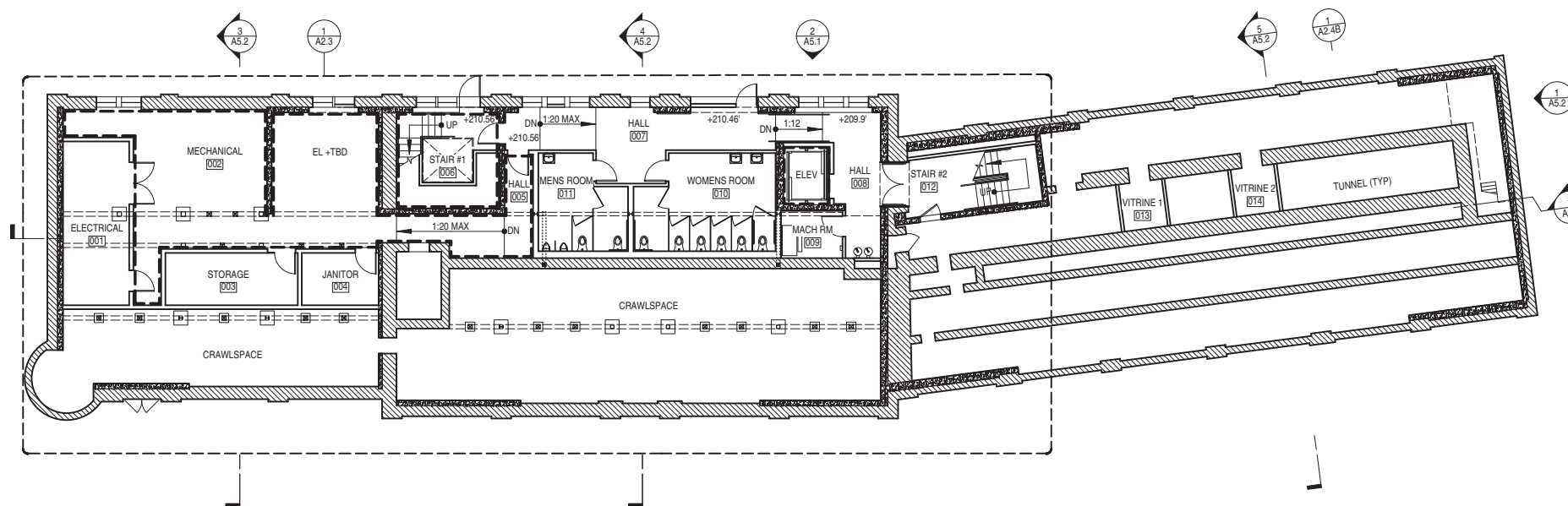
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BASEMENT LEVEL
 PLANS - DEMO &
 PROPOSED



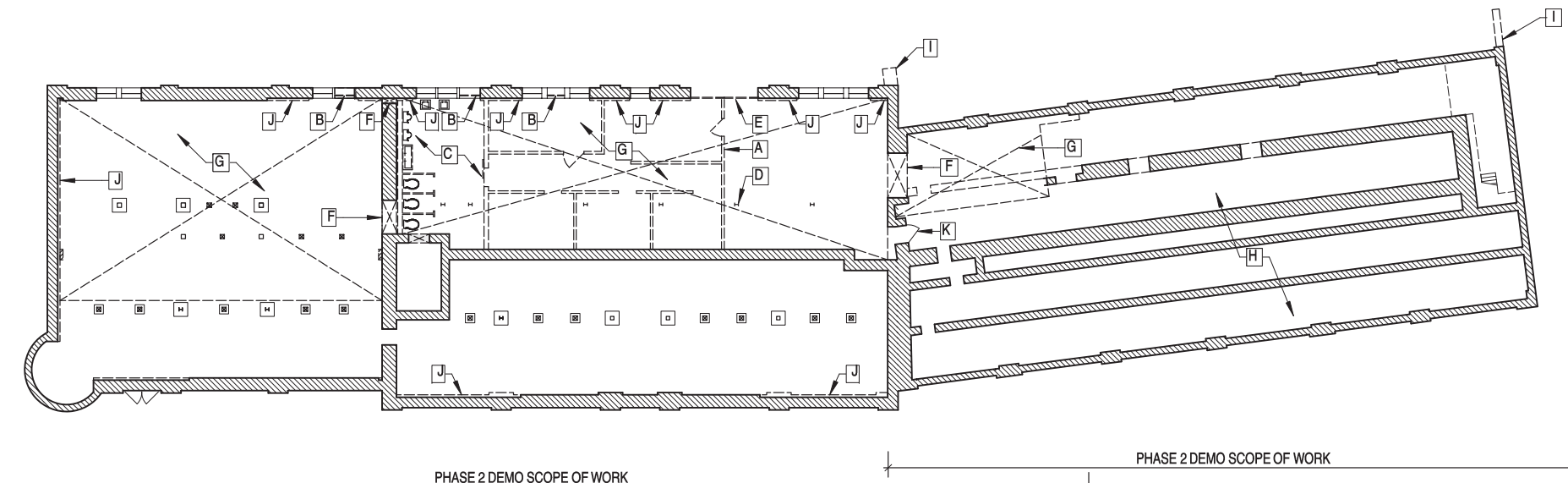
KEY

	(E) BRICK
	(E) WALL
	(N) WALL
	(N) CONCRETE, SSD
	1 HR RATED

BASEMENT LEVEL PLAN



2



KEY

	(E) BRICK
	(E) WALL
	DEMOLITION
	DEMOLITION - FLOOR
	DEMOLITION - PERIOD OF SIGNIFICANCE

BASEMENT LEVEL DEMOLITION PLAN



1

SHEET NOTES

- PROTECT ALL EXISTING HISTORIC MATERIAL TO REMAIN IN PLACE DURING CONSTRUCTION.
- PROTECT SALVAGED DEMOLITION MATERIALS AS REQUIRED FOR REINSTALLATION.
- CONFIRM ALL ITEMS FOR SALVAGE WITH HISTORIC ARCHITECT, INCLUDING BUT NOT LIMITED TO DOORS, WINDOWS, WOOD FLOORING, WOOD SIDING, WAINSCOTING, TRIM, PLUMBING FIXTURES, CASEWORK, AND HISTORIC ELEMENTS.
- REMOVE ALL PLYWOOD WINDOW AND DOOR COVERINGS.
- REMOVE WATER DAMAGED, BROKEN AND LOOSE PLASTER.
- COORDINATE REMOVAL OF HAZARDOUS MATERIALS FROM SITE. ASSUME EXISTING PAINT IS LEAD BASED AND WILL REQUIRE REMOVAL. AT POWERHOUSE, ASSUME FLAKING PAINT TO BE REMOVED WITH REMAINING PAINT TO BE RETAINED AND ENCAPSULATED.
- REMOVE ALL WINDOW SASHES FROM FRAMES FOR RESTORATION AND REPLACEMENT OF GLASS. SEE HISTORIC ARCHITECTURAL DRAWINGS.
- FOR ADDITIONAL INFORMATION, SEE INTERIOR ELEVATIONS

KEY NOTES - BASEMENT LEVEL DEMOLITION

- A** DEMO WALLS AND DOORS SHOWN DASHED, TYPICAL.
- B** DEMO EXTERIOR DOORS, THREE TOTAL.
- C** REMOVE EXISTING TOILET FIXTURES AND ACCESSORIES. REMOVE EXISTING FINISHES FOR NEW WORK.
- D** RETAIN EXISTING STEEL COLUMNS, TYPICAL.
- E** REMOVE AND RETAIN STEEL LOADING DOORS FOR DECORATIVE RE-HANGING.
- F** CUT NEW OPENINGS IN EXISTING BRICK WALL.
- G** DEMO EXISTING FLOOR SLAB / ADDITIONAL EXCAVATION.
- H** REMOVE SOIL FROM TUNNEL AREA, TYPICAL.
- I** SEE SITE PLAN FOR INFORMATION.
- J** REMOVE INTERIOR WYTHE OF EXISTING BRICK WALL FOR NEW STRUCTURE, TYPICAL. SEE STRUCTURAL DRAWINGS.
- K** EXISTING DOOR TO REMAIN.



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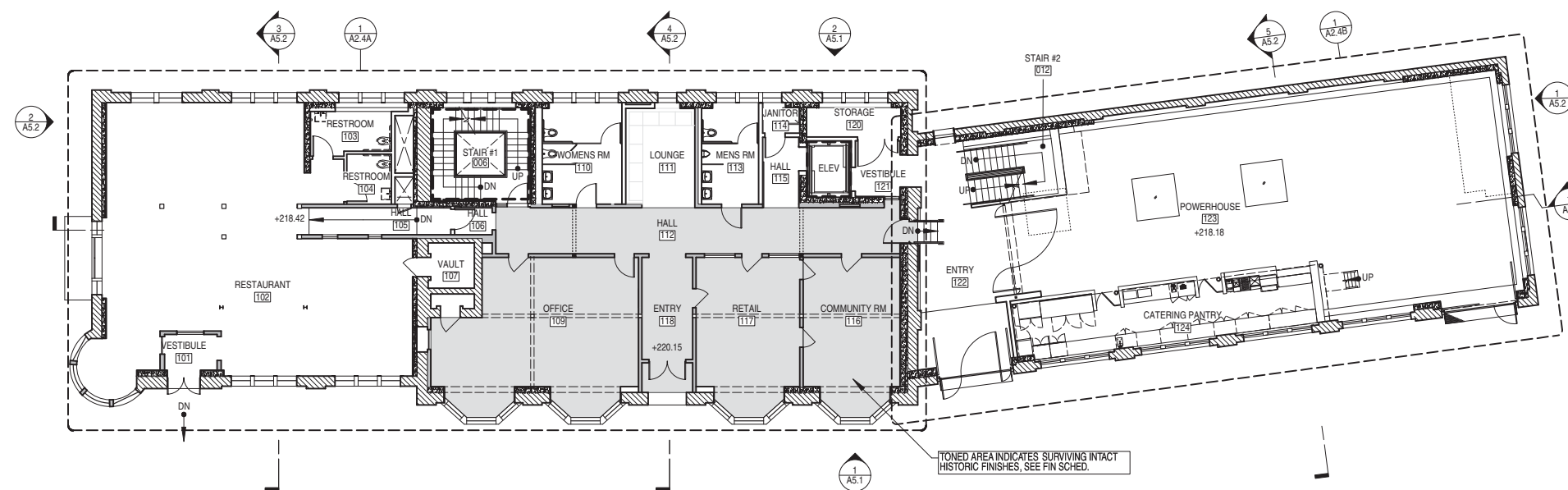
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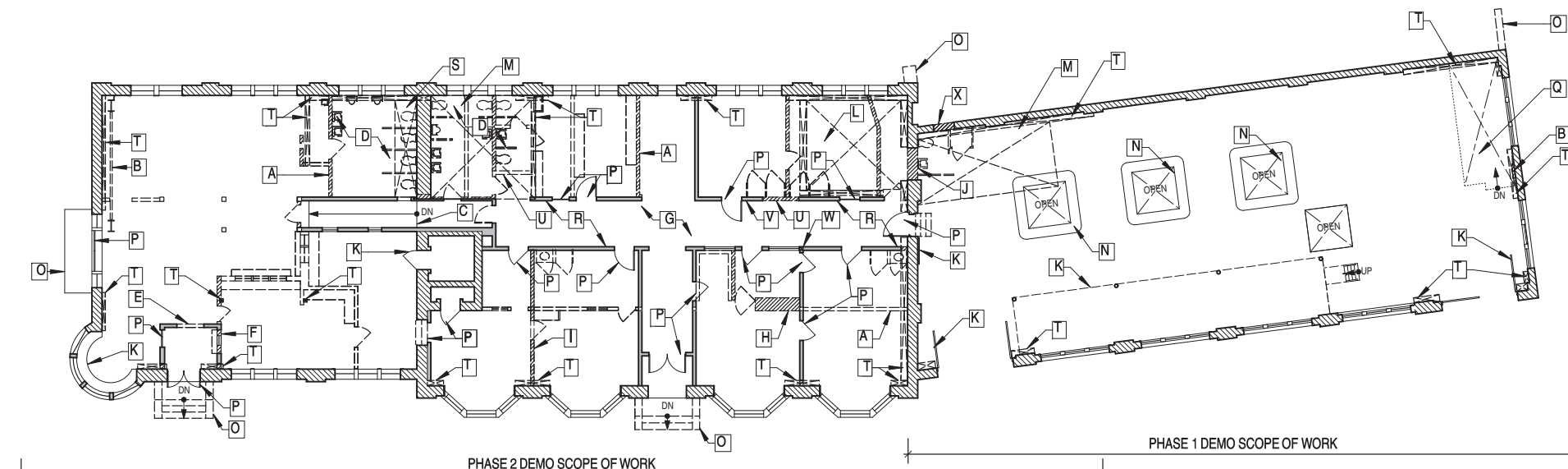
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LEVEL 1 PLAN

0 3' 32' 3/32"=1'-0"



LEVEL 1 DEMOLITION PLAN

0 3' 32' 3/32"=1'-0"

SEC. 1005 (ARTICLE 10) CALCS - LEVEL 1
 TOTAL REMOVED LEV 1 FLR PLATE: 855 SF
 TOTAL (E) LEV 1 FLR AREA: 8928 SF
 PORTION OF LEV 1 FLR PLATE REMOVED: 9.6%
 (9.6% < 75%)
 SEE ELEVATIONS FOR CALCULATION OF REMOVED SURFACE OF EXTERNAL WALLS.

SHEET NOTES

- PROTECT ALL EXISTING HISTORIC MATERIAL TO REMAIN IN PLACE DURING CONSTRUCTION.
- PROTECT SALVAGED DEMOLITION MATERIALS AS REQUIRED FOR REINSTALLATION.
- CONFIRM ALL ITEMS FOR SALVAGE WITH HISTORIC ARCHITECT, INCLUDING BUT NOT LIMITED TO DOORS, WINDOWS, WOOD FLOORING, WOOD SIDING, WAINSCOTING, TRIM, PLUMBING FIXTURES, CASEWORK, AND HISTORIC ELEMENTS
- REMOVE ALL PLYWOOD WINDOW AND DOOR COVERINGS.
- REMOVE WATER DAMAGED, BROKEN AND LOOSE PLASTER.
- COORDINATE REMOVAL OF HAZARDOUS MATERIALS FROM SITE. ASSUME EXISTING PAINT IS LEAD BASED AND WILL REQUIRE REMOVAL. AT POWERHOUSE, ASSUME FLAKING PAINT TO BE REMOVED WITH REMAINING PAINT TO BE RETAINED AND ENCAPSULATED.
- REMOVE ALL WINDOW SASHES FROM FRAMES FOR RESTORATION AND REPLACEMENT OF GLASS. SEE HISTORIC ARCHITECTURAL DRAWINGS.
- FOR ADDITIONAL INFORMATION, SEE INTERIOR ELEVATIONS

KEY NOTES - LEVEL 1 DEMOLITION

- A DEMO WALLS, DOORS AND CASEWORK SHOWN DASHED, TYPICAL.
- B DEMO EXISTING STRUCTURE SHOWN DASHED. SEE STRUCTURAL DRAWINGS.
- C DEMO EXISTING RAMP.
- D REMOVE EXISTING TOILET FIXTURES AND ACCESSORIES. REMOVE EXISTING FINISHES FOR NEW WORK.
- E DEMO OPENING FOR NEW WINDOW.
- F DEMO OPENING FOR NEW DOOR.
- G REMOVE AND RETAIN WOOD FLOORING THROUGHOUT FOR REUSE.
- H REMOVE AND RETAIN CASEWORK FOR RELOCATION.
- I REMOVE WALL BELOW COVED CEILING MOLDING. RETAIN SURFACE ABOVE.
- J CUT OPENING IN EXISTING BRICK WALL.
- K RETAIN AND PROTECT EXISTING HISTORIC ELEMENT, TYPICAL.
- L REMOVE STRUCTURE AS REQUIRED FOR ELEVATOR. SEE STRUCTURAL DRAWINGS.
- M REMOVE STRUCTURE AS REQUIRED FOR STAIR OPENING. SEE STRUCTURAL DRAWINGS.
- N REMOVE SOIL AND PLANT MATERIAL, TYPICAL.
- O SEE SITE PLAN FOR INFORMATION.
- P RETAIN AND PROTECT EXISTING DOOR.
- Q REMOVE EXISTING STEPS AND WOODEN RAILING AND PLATFORM.
- R RETAIN AND PROTECT EXISTING WOOD WAINSCOTING AND TRIM THROUGHOUT.
- S REMOVE STRUCTURE AS REQUIRED FOR MECHANICAL SHAFT.
- T DEMO EXISTING FLOOR AS REQUIRED FOR NEW STRUCTURE, TYPICAL. SEE STRUCTURAL DRAWINGS.
- U SALVAGE EXISTING WOOD WAINSCOTING AND TRIM FOR REUSE.
- V REMOVE TASKBOARD ABOVE WAINSCOTING, RETAIN WALL SURFACE.
- W REFRAME WALL AS REQUIRED FOR NEW COLUMN. SEE STRUCTURAL DRAWINGS.
- X CUT OPENING IN EXISTING BRICK WALL FOR NEW PHASE ONE DOOR, PHASE TWO WINDOW



A1.0b

LEVEL 1 PLANS -
 DEMO & PROPOSED



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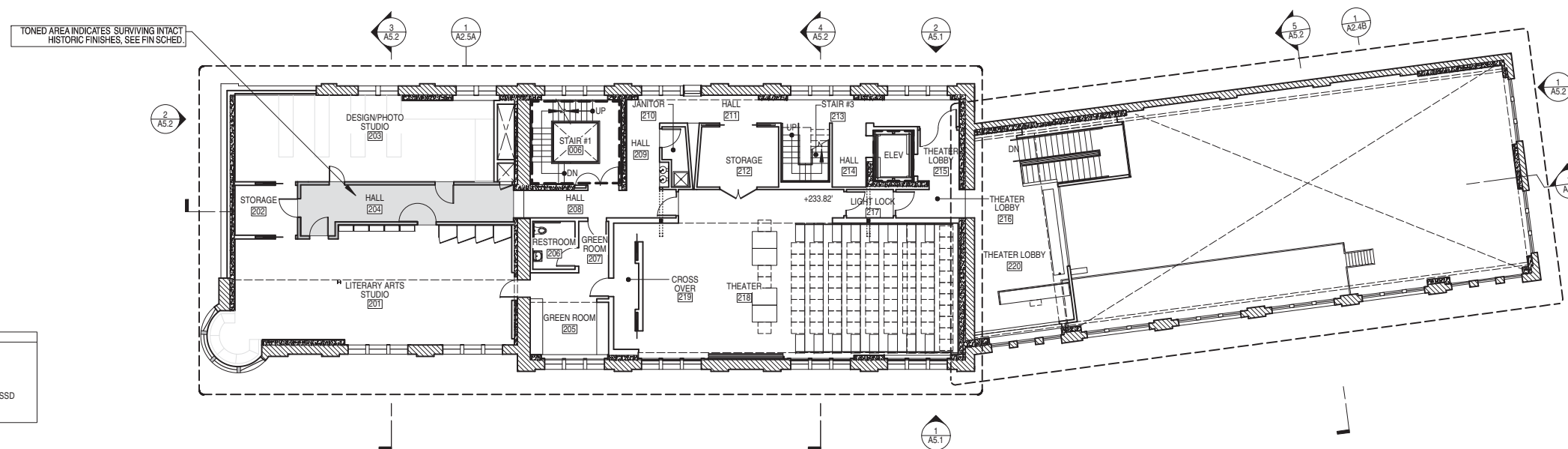
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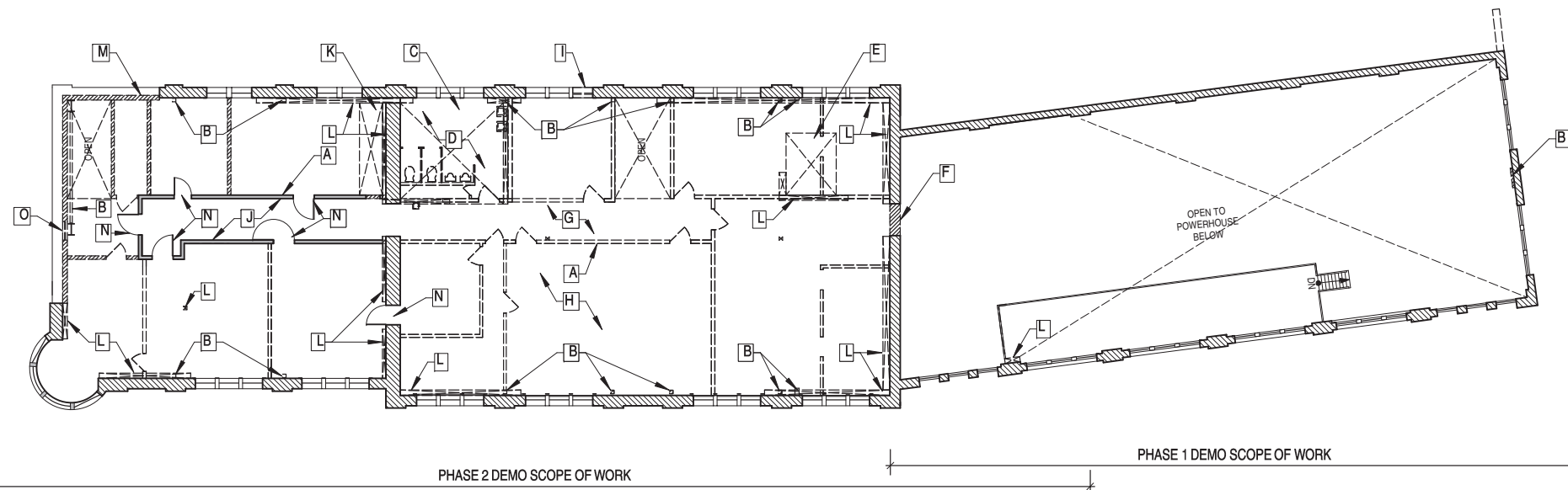
LEVEL 2 PLANS -
 DEMO & PROPOSED



KEY

(E) BRICK
(E) WALL
(N) WALL
(N) CONCRETE, SSD
1 HR RATED

LEVEL 2 PLAN



KEY

(E) BRICK
(E) WALL
DEMOLITION
DEMOLITION - FLOOR
DEMOLITION - PERIOD OF SIGNIFICANCE

LEVEL 2 DEMOLITION PLAN



SHEET NOTES

- PROTECT ALL EXISTING HISTORIC MATERIAL TO REMAIN IN PLACE DURING CONSTRUCTION.
- PROTECT SALVAGED DEMOLITION MATERIALS AS REQUIRED FOR REINSTALLATION.
- CONFIRM ALL ITEMS FOR SALVAGE WITH HISTORIC ARCHITECT, INCLUDING BUT NOT LIMITED TO DOORS, WINDOWS, WOOD FLOORING, WOOD SIDING, WAINSCOTING, TRIM, PLUMBING FIXTURES, CASEWORK, AND HISTORIC ELEMENTS
- REMOVE ALL PLYWOOD WINDOW AND DOOR COVERINGS.
- REMOVE WATER DAMAGED, BROKEN AND LOOSE PLASTER.
- COORDINATE REMOVAL OF HAZARDOUS MATERIALS FROM SITE. ASSUME EXISTING PAINT IS LEAD BASED AND WILL REQUIRE REMOVAL. AT POWERHOUSE, ASSUME FLAKING PAINT TO BE REMOVED WITH REMAINING PAINT TO BE RETAINED AND ENCAPSULATED.
- REMOVE ALL WINDOW SASHES FROM FRAMES FOR RESTORATION AND REPLACEMENT OF GLASS. SEE HISTORIC ARCHITECTURAL DRAWINGS.
- FOR ADDITIONAL INFORMATION, SEE INTERIOR ELEVATIONS

KEY NOTES - LEVEL 2 DEMOLITION

- A DEMO WALLS, DOORS AND CASEWORK SHOWN DASHED, TYPICAL.
- B DEMO EXISTING STRUCTURE SHOWN DASHED. SEE STRUCTURAL DRAWINGS.
- C REMOVE STRUCTURE AS REQUIRED FOR STAIR OPENING. SEE STRUCTURAL DRAWINGS.
- D REMOVE EXISTING TOILET FIXTURES AND ACCESSORIES. REMOVE EXISTING FINISHES FOR NEW WORK.
- E REMOVE STRUCTURE AS REQUIRED FOR ELEVATOR. SEE STRUCTURAL DRAWINGS.
- F CUT OPENING IN EXISTING BRICK WALL.
- G REMOVE AND RETAIN WOOD FLOORING THROUGHOUT FOR REUSE. DEMO SUBFLOOR TO EXISTING JOISTS.
- H REMOVE AND SALVAGE EXISTING WOOD WALL AND CEILING FINISH FOR REPLACEMENT WITHIN THEATER.
- I REMOVE WOOD PROTECTION AT CARMEN STRIKE DOOR. RETAIN AND PROTECT EXISTING SURROUNDING HISTORIC ELEMENTS, TYPICAL.
- J PROTECT AND RETAIN EXISTING WOOD WAINSCOTING AND TRIM.
- K REMOVE STRUCTURE AS REQUIRED FOR MECHANICAL SHAFT.
- L DEMO EXISTING FLOOR AS REQUIRED FOR NEW STRUCTURE, TYPICAL. SEE STRUCTURAL DRAWINGS.
- M DEMO EXISTING c.1906 TAR PAPER CLAD WALL.
- N EXISTING DOOR TO REMAIN.
- O DEMO EXISTING WINDOW IN EXISTING c.1906 TAR PAPER CLAD WALL.

SEC. 1005 (ARTICLE 10) CALCS - LEVEL 2

TOTAL REMOVED LEV 2 FLR PLATE:	521 SF
TOTAL (E) LEV 2 FLR AREA:	5893 SF
PORTION OF LEV 2 FLR PLATE REMOVED:	8.8% (8.8% < 75%)

SEE ELEVATIONS FOR CALCULATION OF REMOVED SURFACE OF EXTERNAL WALLS.

2

1



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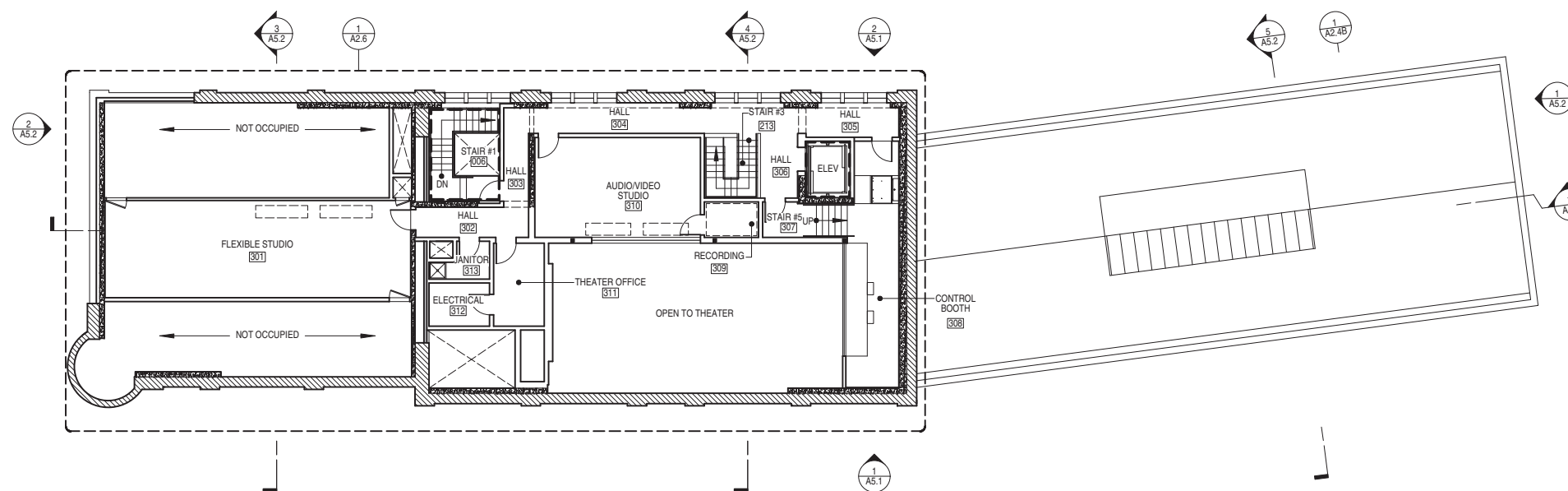
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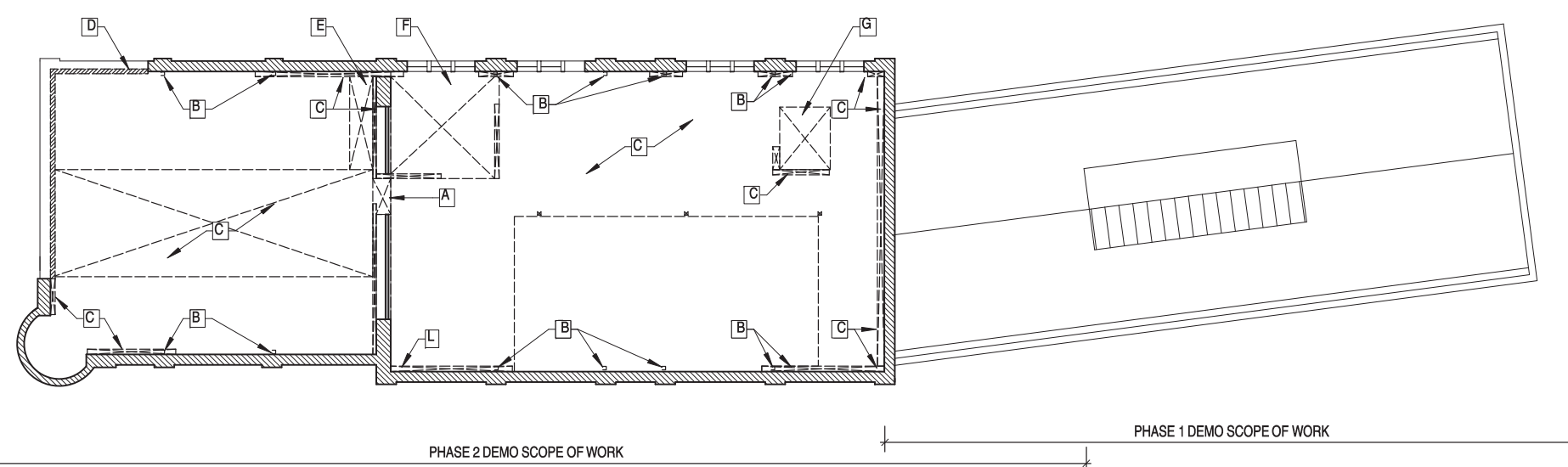
LEVEL 3 PLANS -
 DEMO & PROPOSED



KEY

	(E) BRICK
	(E) WALL
	(N) WALL
	(N) CONCRETE, SSD
	1 HR RATED

LEVEL 3 PLAN



KEY

	(E) BRICK
	(E) WALL
	DEMOLITION
	DEMOLITION - FLOOR
	DEMOLITION - PERIOD OF SIGNIFICANCE

LEVEL 3 DEMOLITION PLAN



SHEET NOTES

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- REMOVE ALL WINDOW SASHES FROM FRAMES FOR RESTORATION AND REPLACEMENT OF GLASS. SEE HISTORIC ARCHITECTURAL DRAWINGS.
- FOR ADDITIONAL INFORMATION, SEE INTERIOR ELEVATIONS

KEY NOTES - LEVEL 3 DEMOLITION

- A** DEMO WALLS, DOORS AND CASEWORK SHOWN DASHED, TYPICAL.
- B** DEMO EXISTING STRUCTURE SHOWN DASHED. SEE STRUCTURAL DRAWINGS.
- C** DEMO EXISTING FLOOR AS REQUIRED FOR NEW STRUCTURE, TYPICAL. SEE STRUCTURAL DRAWINGS.
- D** DEMO EXISTING c.1906 TAR PAPER CLAD WALL.
- E** REMOVE STRUCTURE AS REQUIRED FOR MECHANICAL SHAFT.
- F** REMOVE STRUCTURE AS REQUIRED FOR STAIR OPENING. SEE STRUCTURAL DRAWINGS.
- G** REMOVE STRUCTURE AS REQUIRED FOR ELEVATOR. SEE STRUCTURAL DRAWINGS.



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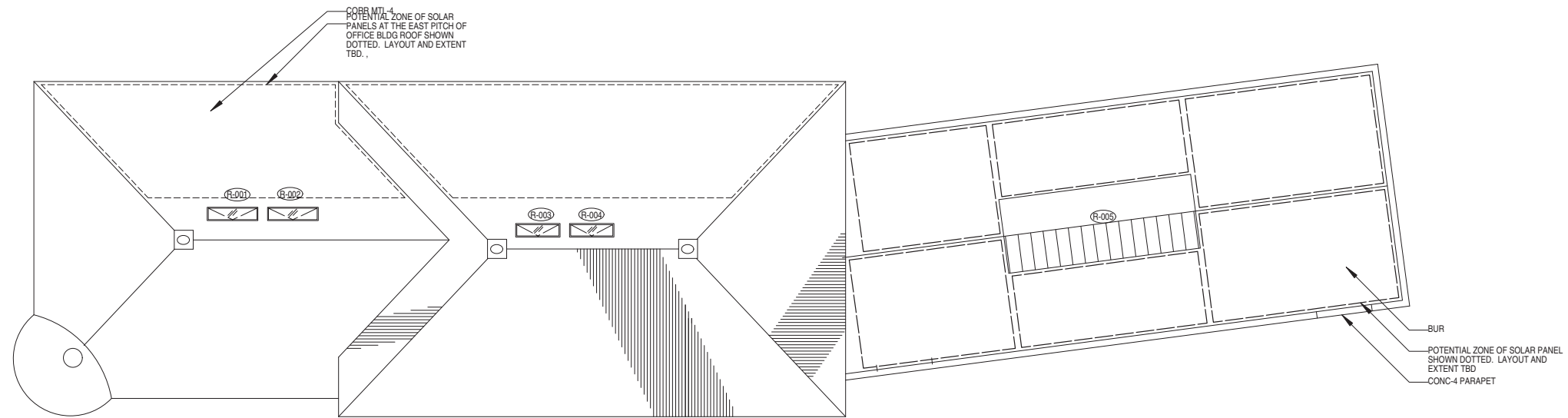
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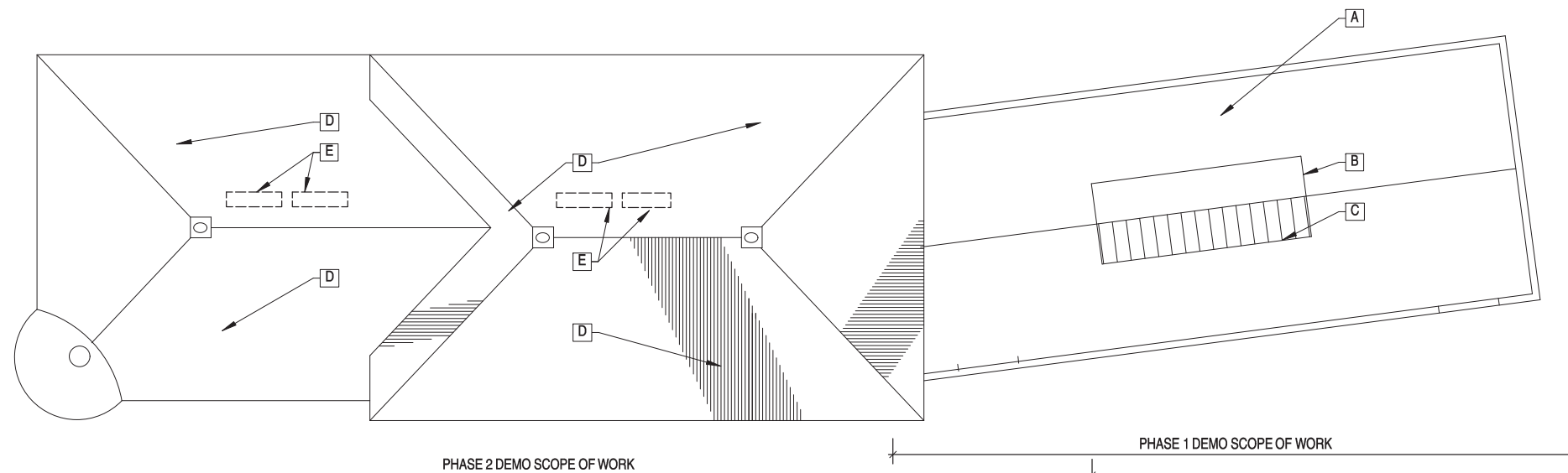
ROOF PLANS -
 DEMO & PROPOSED



ROOF PLAN

0 3' 32' 3/32"=1'-0"

2



ROOF DEMOLITION PLAN

0 3' 32' 3/32"=1'-0"

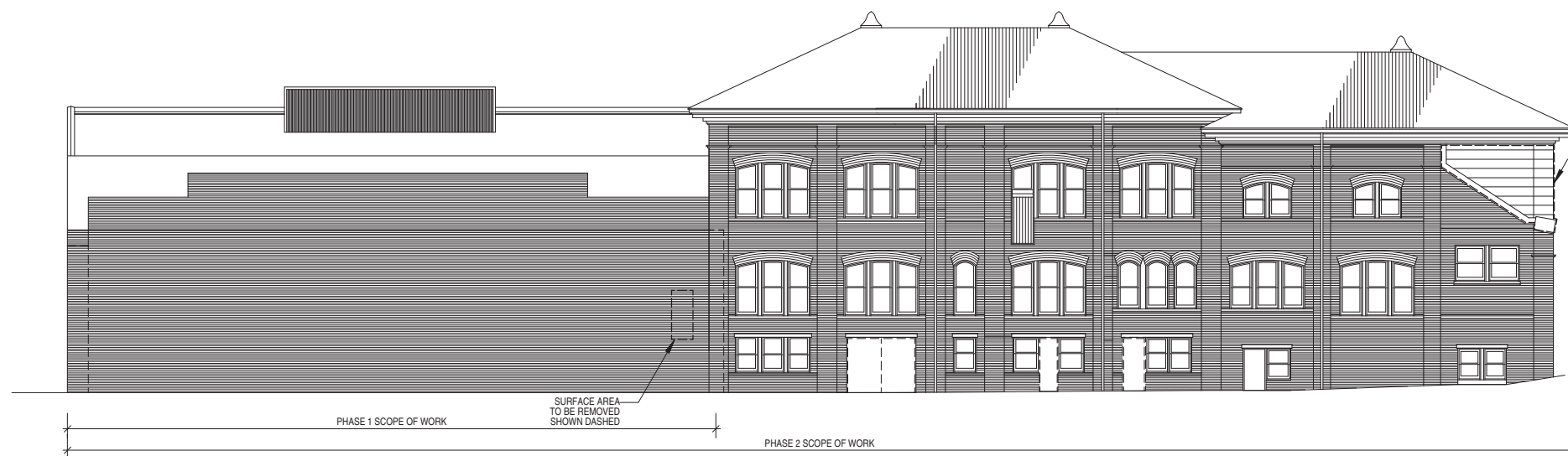
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SHEET NOTES

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- PROTECT SALVAGED DEMOLITION MATERIALS AS REQUIRED FOR REINSTALLATION.
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- REMOVE ALL PLYWOOD WINDOW AND DOOR COVERINGS.
- REMOVE WATER DAMAGED, BROKEN AND LOOSE PLASTER.
- COORDINATE REMOVAL OF HAZARDOUS MATERIALS FROM SITE. ASSUME EXISTING PAINT IS LEAD BASED AND WILL REQUIRE REMOVAL AT POWERHOUSE, ASSUME FLAKING PAINT TO BE REMOVED WITH REMAINING PAINT TO BE RETAINED AND ENCAPSULATED.
- REMOVE ALL WINDOW SASHES FROM FRAMES FOR RESTORATION AND REPLACEMENT OF GLASS. SEE HISTORIC ARCHITECTURAL DRAWINGS.
- FOR ADDITIONAL INFORMATION, SEE INTERIOR ELEVATIONS

KEY NOTES - ROOF DEMOLITION

- A DEMO EXISTING ROOF ASSEMBLY AT POWERHOUSE. SEE STRUCTURAL DRAWINGS.
- B REMOVE EXISTING CORRUGATED METAL CLADDING.
- C REMOVE ALL SKYLIGHT GLAZING. RETAIN MULLIONS AND MUNTINS FOR RESTORATION. SEE STRUCTURAL DRAWINGS.
- D REMOVE (E) CORRUGATED ROOFING. PROVIDE OPENINGS AS REQD FOR HVAC. SEE MECH AND PLUMB DRAWINGS.
- E DEMO FOR SKYLIGHTS.



SEC. 1005 (ARTICLE 10) CALCULATIONS - EAST ELEVATION

TOTAL REMOVED EAST ELEVATION SURFACE AREA: 181 SF
 TOTAL EXISTING EAST ELEVATION SURFACE AREA: 7730 SF
 PERCENTAGE OF EAST ELEVATION SURFACE AREA REMOVED: 2.1%

EXISTING EAST ELEVATION



SEC. 1005 (ARTICLE 10) CALCULATIONS - NORTH ELEVATION

TOTAL REMOVED NORTH ELEVATION SURFACE AREA: 400 SF
 TOTAL EXISTING NORTH ELEVATION SURFACE AREA: 1647 SF
 PERCENTAGE OF NORTH ELEVATION SURFACE AREA REMOVED: 24.3%

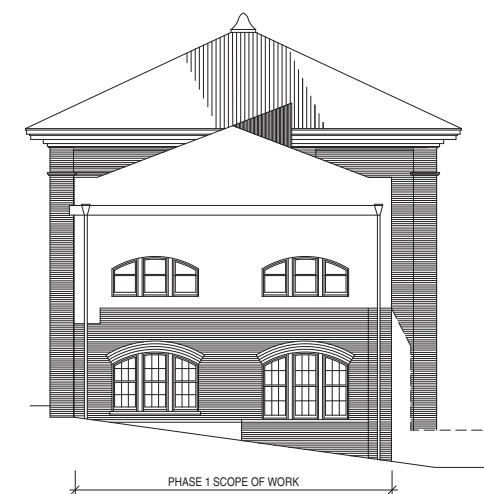
EXISTING NORTH ELEVATION



SEC. 1005 (ARTICLE 10) CALCULATIONS - WEST ELEVATION

TOTAL REMOVED WEST ELEVATION SURFACE AREA: 0 SF
 TOTAL EXISTING WEST ELEVATION SURFACE AREA: 6636 SF
 PERCENTAGE OF WEST ELEVATION SURFACE AREA REMOVED: 0%

EXISTING WEST ELEVATION



SEC. 1005 (ARTICLE 10) CALCULATIONS - SOUTH ELEVATION

TOTAL REMOVED SOUTH ELEVATION SURFACE AREA: 0 SF
 TOTAL EXISTING SOUTH ELEVATION SURFACE AREA: 1896 SF
 PERCENTAGE OF SOUTH ELEVATION SURFACE AREA REMOVED: 0%

EXISTING SOUTH ELEVATION



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A1.0f

EXISTING EXTERIOR ELEVATIONS



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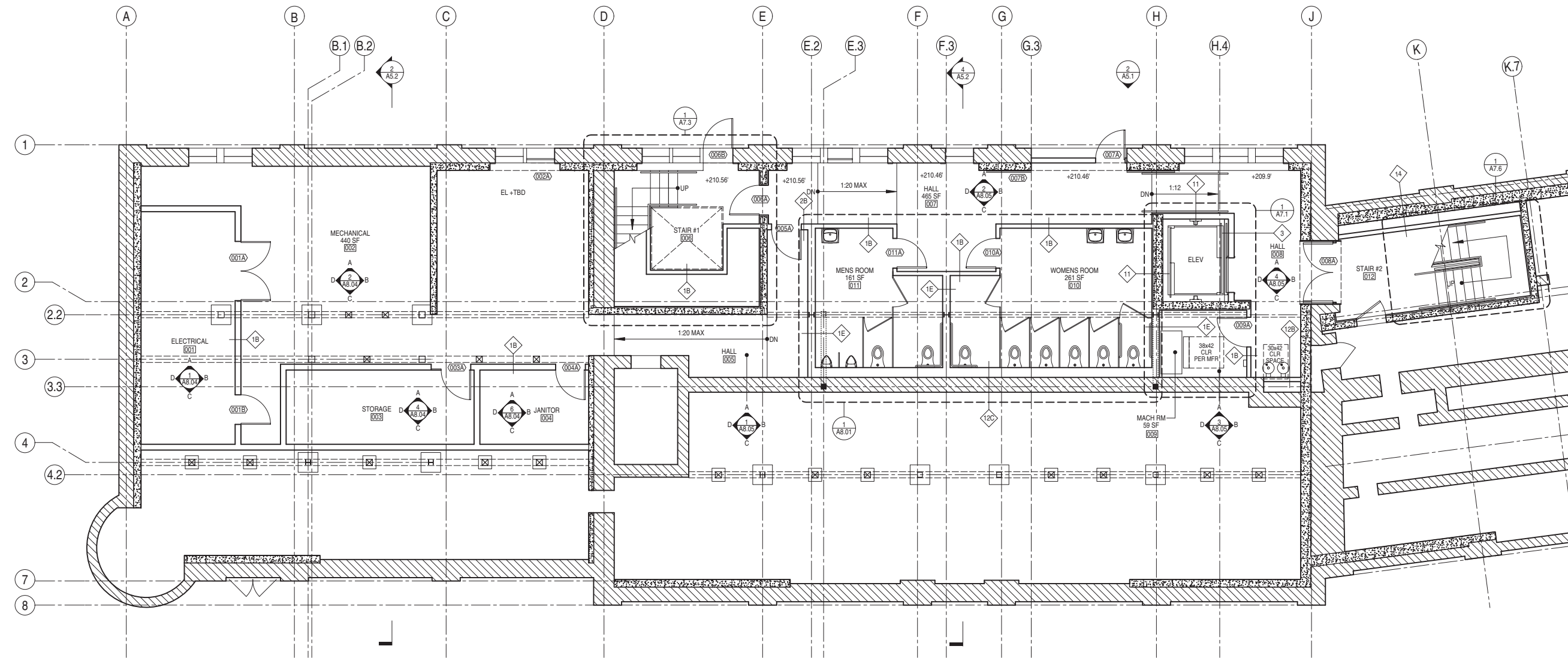
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A2.3

BASEMENT LEVEL
 PARTIAL PLAN



KEY

	(E) BRICK
	(E) WALL
	(N) WALL
	(N) CONCRETE, SSD

BASEMENT LEVEL ENLARGED PLAN

0 3' 16' 3/16"=1'-0"

1



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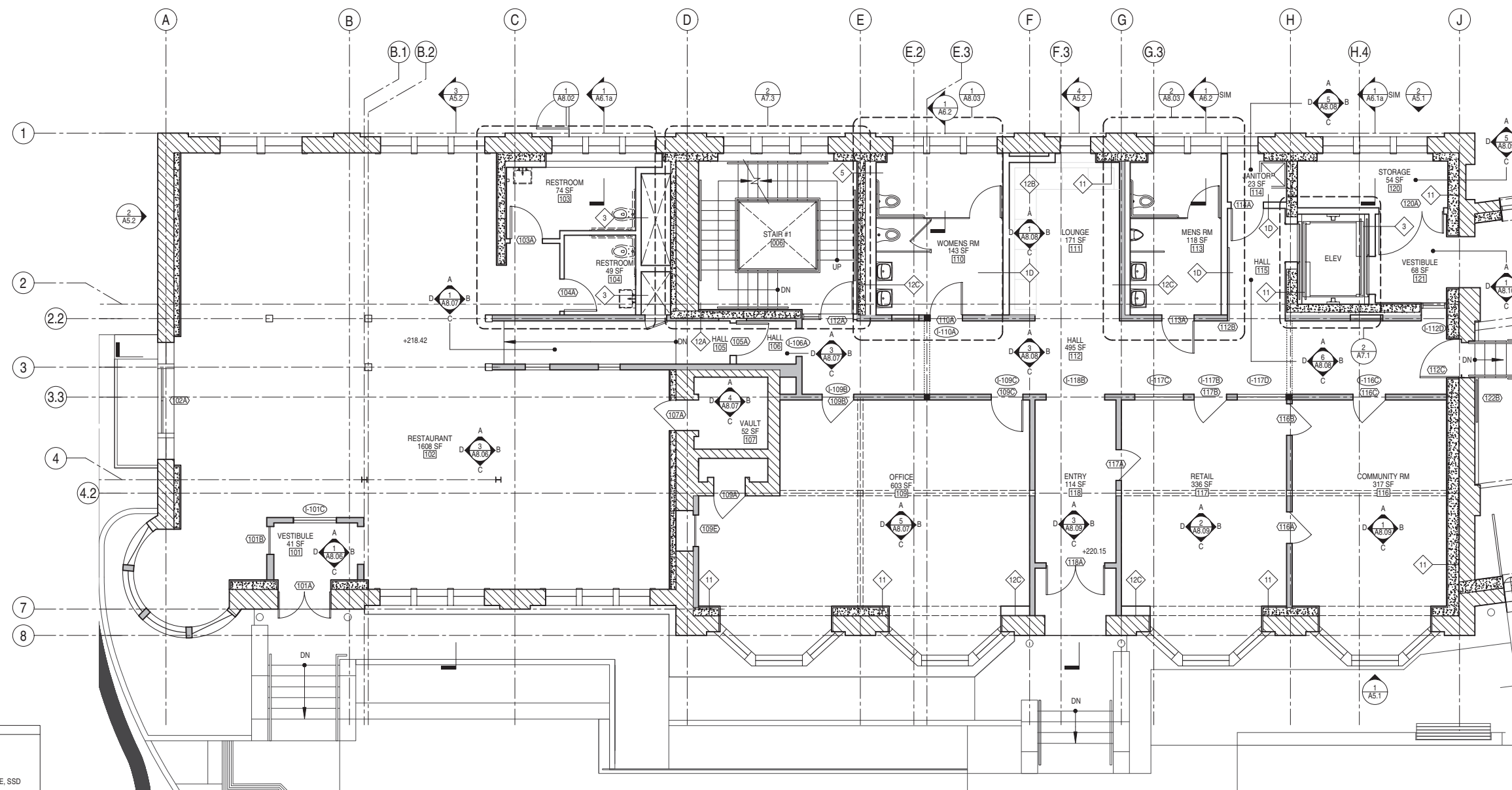
SCALE: AS NOTED

ISSUE	DATE
50% DD SET	03.09.15
100% DD SET	06.03.15
C OF A SET	09.11.15



A2.4A

LEVEL 1
 PARTIAL PLAN -
 NORTH



LEVEL 1 ENLARGED PLAN - NORTH

0 3' 16' 3/16"=1'-0"

1



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 415 974 5603 phone

GENEVA CAR BARN
 AND POWERHOUSE

2301 SAN JOSE AVENUE
 SAN FRANCISCO, CA

DATE: 08.08.14

JOB: 0906

DRAWN: MH

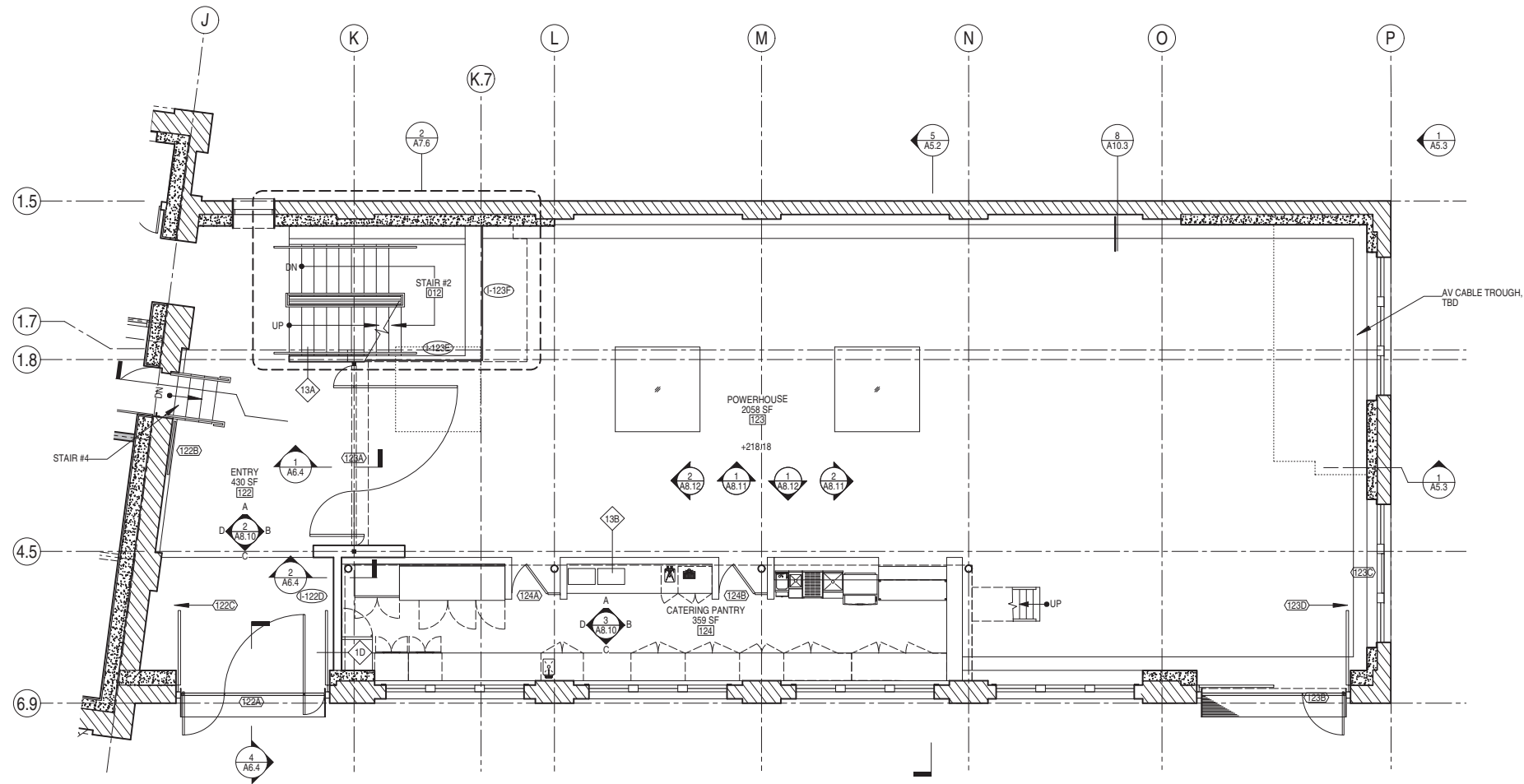
SCALE: AS NOTED

ISSUE	DATE
50% DD SET	03.09.15
100% DD SET	06.03.15
C OF A SET	09.11.15



A2.4B

LEVEL 1
 PARTIAL PLAN -
 SOUTH



KEY

	(E) BRICK
	(E) WALL
	(N) WALL
	(N) CONCRETE, SSD

LEVEL 1 ENLARGED PLAN - SOUTH

0 3' 16' 3/16"=1'-0"

1



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GENEVA CAR BARN
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DATE: 08.08.14

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DRAWN: MH

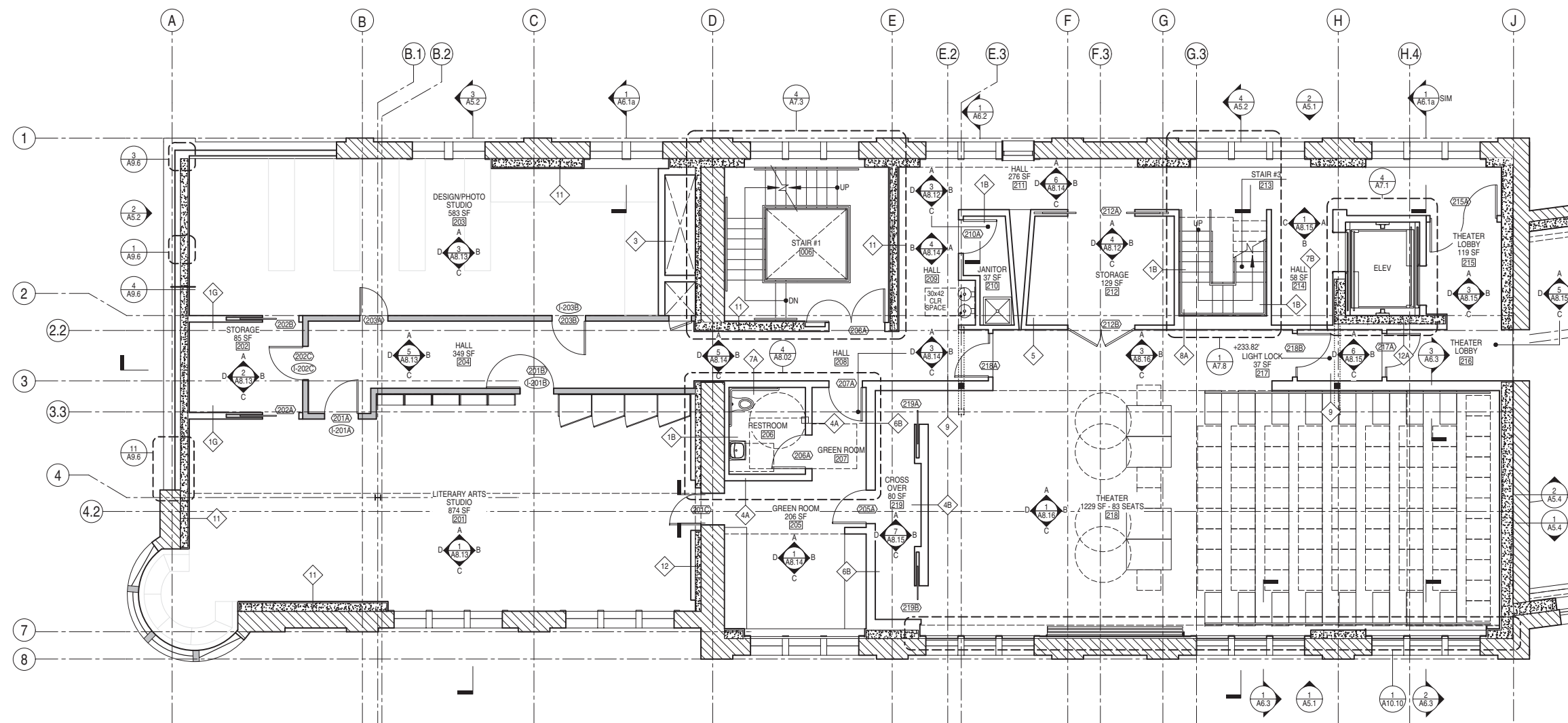
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ISSUE	DATE
50% DD SET	03.09.15
100% DD SET	06.03.15
C OF A SET	09.11.15



A2.5A

LEVEL 2
 PARTIAL PLAN -
 NORTH



KEY
 (E) BRICK
 (E) WALL
 (N) WALL
 (N) CONCRETE, SSD

LEVEL 2 ENLARGED PLAN - NORTH

0 3' 16' 3/16"=1'-0"

1



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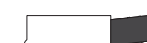
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JOB: 0906

DRAWN: MH

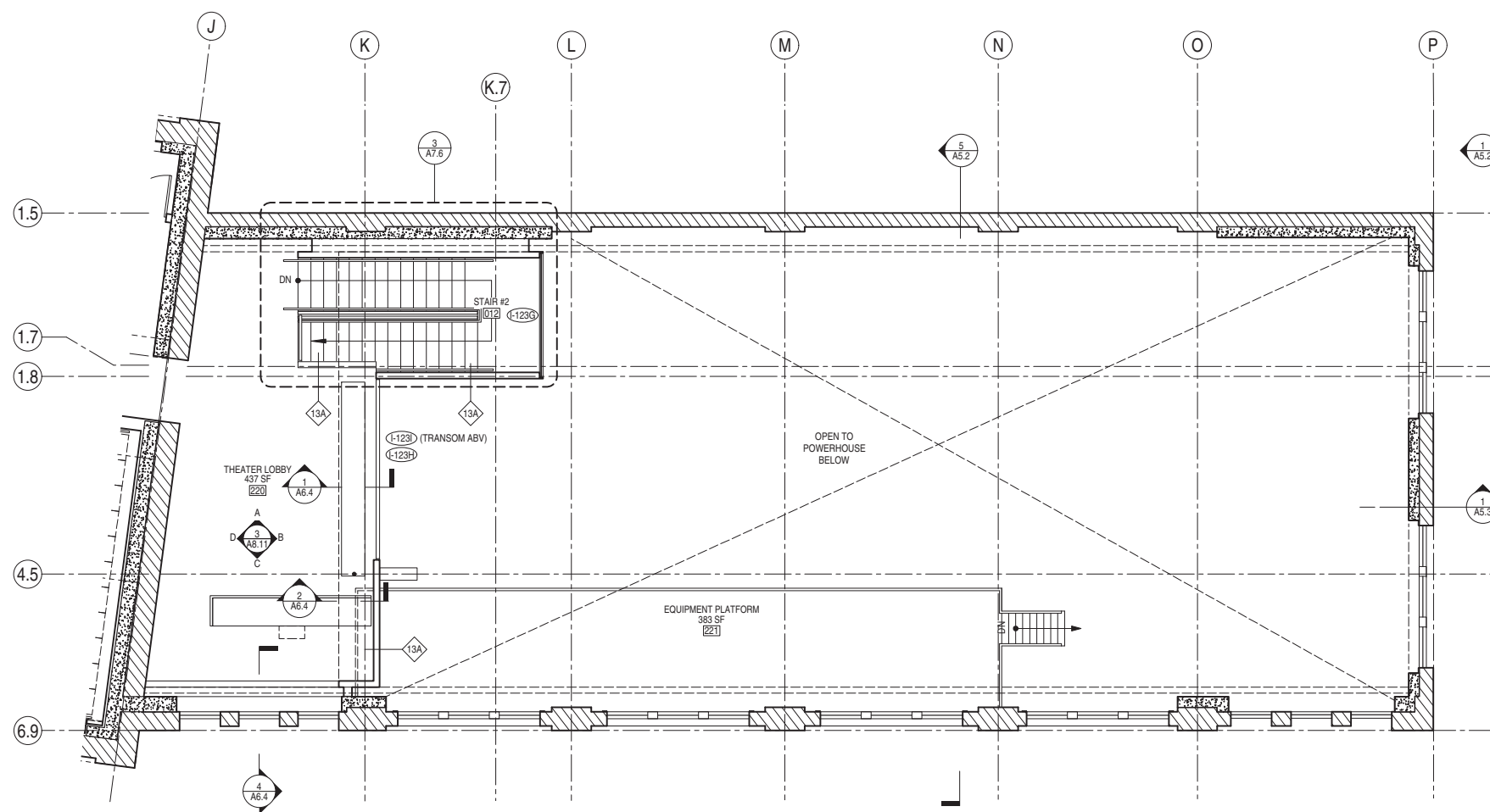
SCALE: AS NOTED

ISSUE	DATE
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100% DD SET	06.03.15
C OF A SET	09.11.15



A2.5B

LEVEL 2
 PARTIAL PLAN -
 SOUTH



KEY

	(E) BRICK
	(E) WALL
	(N) WALL
	(N) CONCRETE, SSD

LEVEL 2 ENLARGED PLAN - SOUTH

0 3' 16' 3/16"=1'-0"

1



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 AND POWERHOUSE

2301 SAN JOSE AVENUE
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JOB: 0906

DRAWN: MH

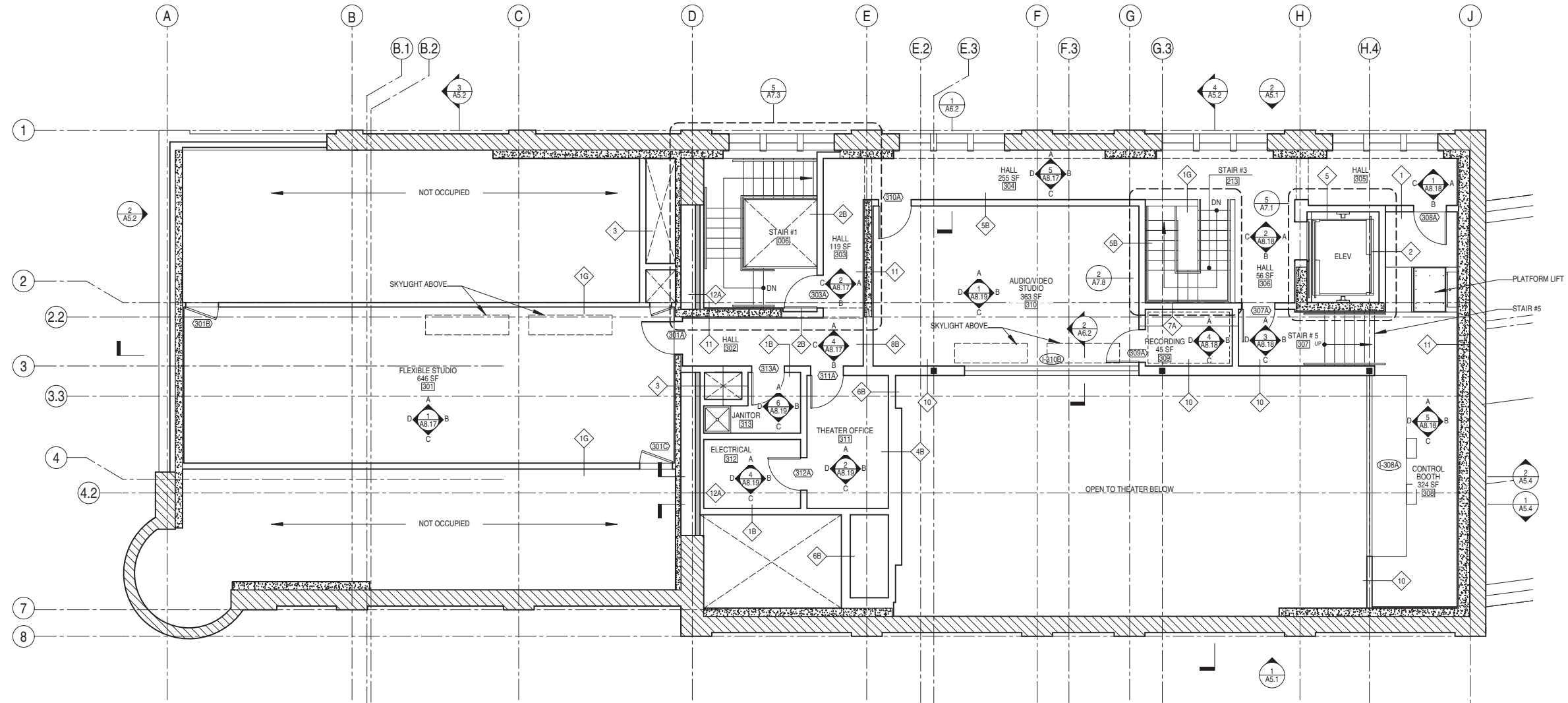
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100% DD SET	06.03.15
C OF A SET	09.11.15



A2.6

LEVEL 3
 PARTIAL PLAN -
 NORTH



KEY

	(E) BRICK
	(E) WALL
	(N) WALL
	(N) CONCRETE, SSD

LEVEL 3 ENLARGED PLAN - NORTH

0 3' 16' 3/16"=1'-0"

1



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**GENEVA CAR BARN
 AND POWERHOUSE**

2301 SAN JOSE AVENUE
 SAN FRANCISCO, CA

DATE: 08.08.14

JOB: 0906

DRAWN: MH

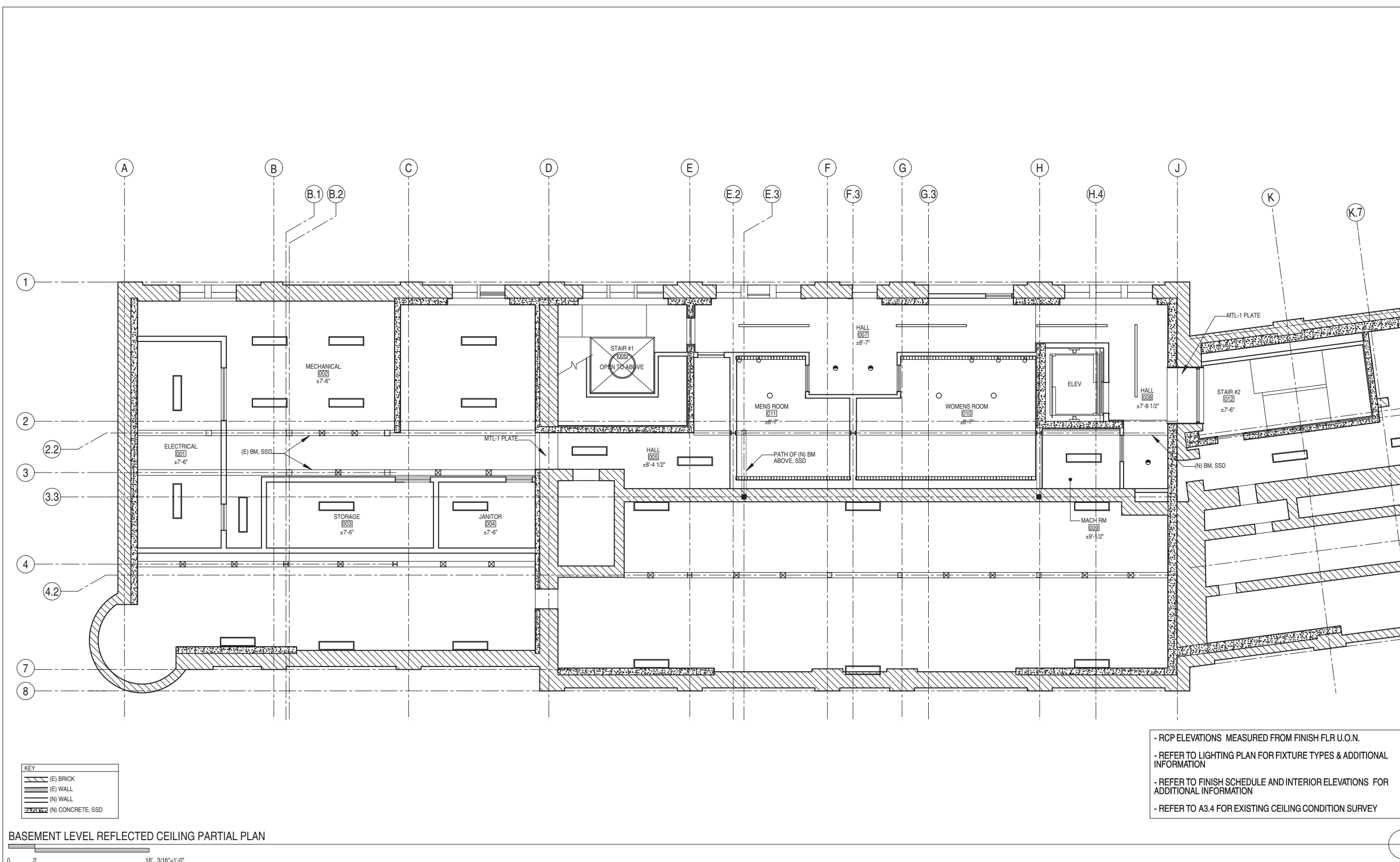
SCALE: AS NOTED

ISSUE	DATE
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100% DD SET	06.03.15
C OF A SET	09.11.15



A3.0

BASEMENT LEVEL
 REFLECTED
 CEILING PARTIAL
 PLAN



BASEMENT LEVEL REFLECTED CEILING PARTIAL PLAN

0 3' 16' 3/16"=1'-0"

KEY

	(E) BRICK
	(E) WALL
	(N) WALL
	(N) CONCRETE, SSD

- RCP ELEVATIONS MEASURED FROM FINISH FLR U.O.N.
 - REFER TO LIGHTING PLAN FOR FIXTURE TYPES & ADDITIONAL INFORMATION
 - REFER TO FINISH SCHEDULE AND INTERIOR ELEVATIONS FOR ADDITIONAL INFORMATION
 - REFER TO A3.4 FOR EXISTING CEILING CONDITION SURVEY

1



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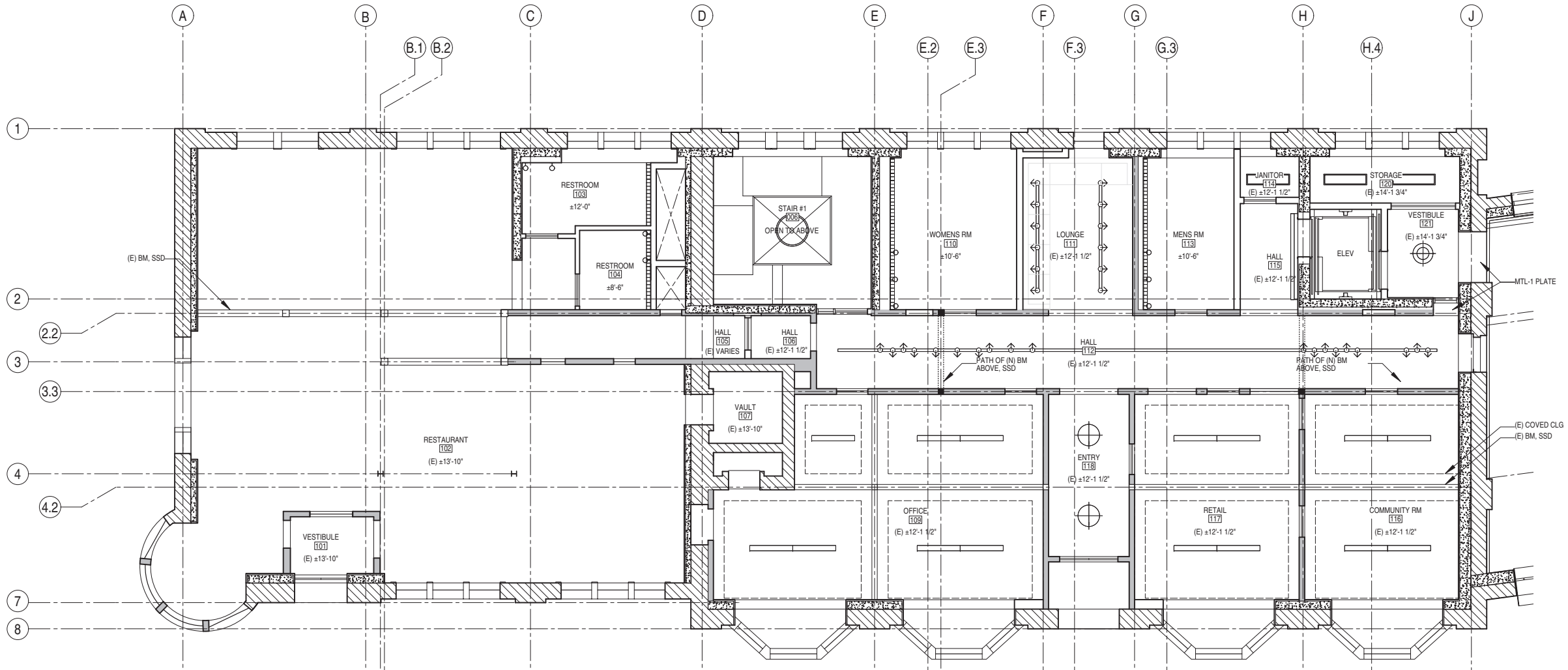
DATE: 08.08.14

JOB: 0906

DRAWN: MH

SCALE: AS NOTED

ISSUE	DATE
50% DD SET	03.09.15
100% DD SET	06.03.15
C OF A SET	09.11.15



KEY

	(E) BRICK
	(E) WALL
	(N) WALL
	(N) CONCRETE, SSD

LEVEL 1 REFLECTED CEILING PARTIAL PLAN - NORTH

0 3' 16' 3/16"=1'-0"

- RCP ELEVATIONS MEASURED FROM FINISH FLR U.O.N.
- REFER TO LIGHTING PLAN FOR FIXTURE TYPES & ADDITIONAL INFORMATION
- REFER TO FINISH SCHEDULE AND INTERIOR ELEVATIONS FOR ADDITIONAL INFORMATION
- REFER TO A3.4 FOR EXISTING CEILING CONDITION SURVEY

1



A3.1A

LEVEL 1
 REFLECTED
 CEILING PARTIAL
 PLAN - NORTH



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 AND POWERHOUSE

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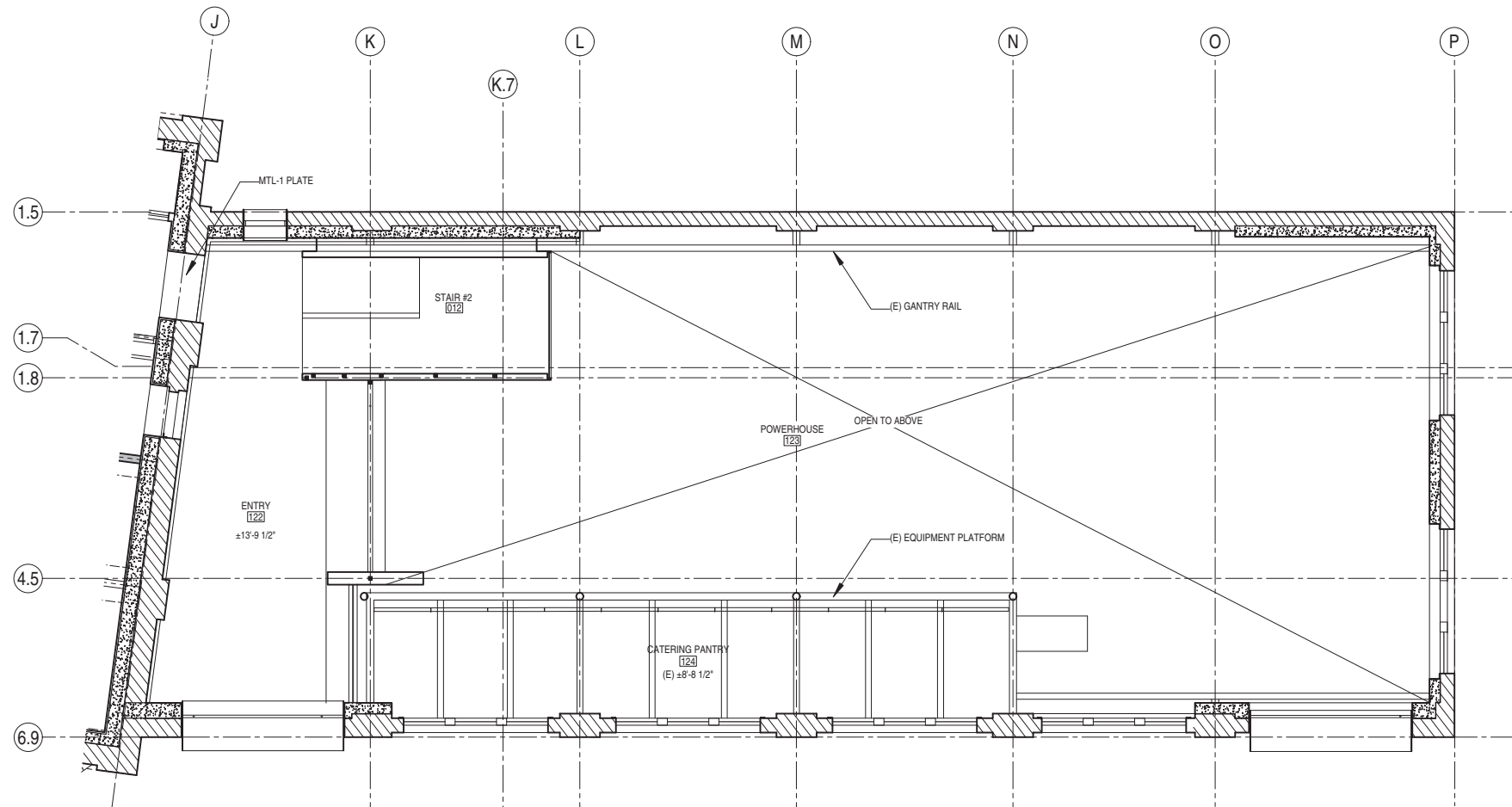
DATE: 08.08.14

JOB: 0906

DRAWN: MH

SCALE: AS NOTED

ISSUE	DATE
50% DD SET	03.09.15
100% DD SET	06.03.15
C OF A SET	09.11.15



KEY	
	(E) BRICK
	(E) WALL
	(N) WALL
	(N) CONCRETE, SSD

LEVEL 1 REFLECTED CEILING PARTIAL PLAN - SOUTH

0 3' 16' 3/16"=1'-0"

- RCP ELEVATIONS MEASURED FROM FINISH FLR U.O.N.
- REFER TO LIGHTING PLAN FOR FIXTURE TYPES & ADDITIONAL INFORMATION
- REFER TO FINISH SCHEDULE AND INTERIOR ELEVATIONS FOR ADDITIONAL INFORMATION
- REFER TO A3.4 FOR EXISTING CEILING CONDITION SURVEY

1



A3.1B

LEVEL 1
 REFLECTED
 CEILING PARTIAL
 PLAN - SOUTH



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DATE: 08.08.14

JOB: 0906

DRAWN: MH

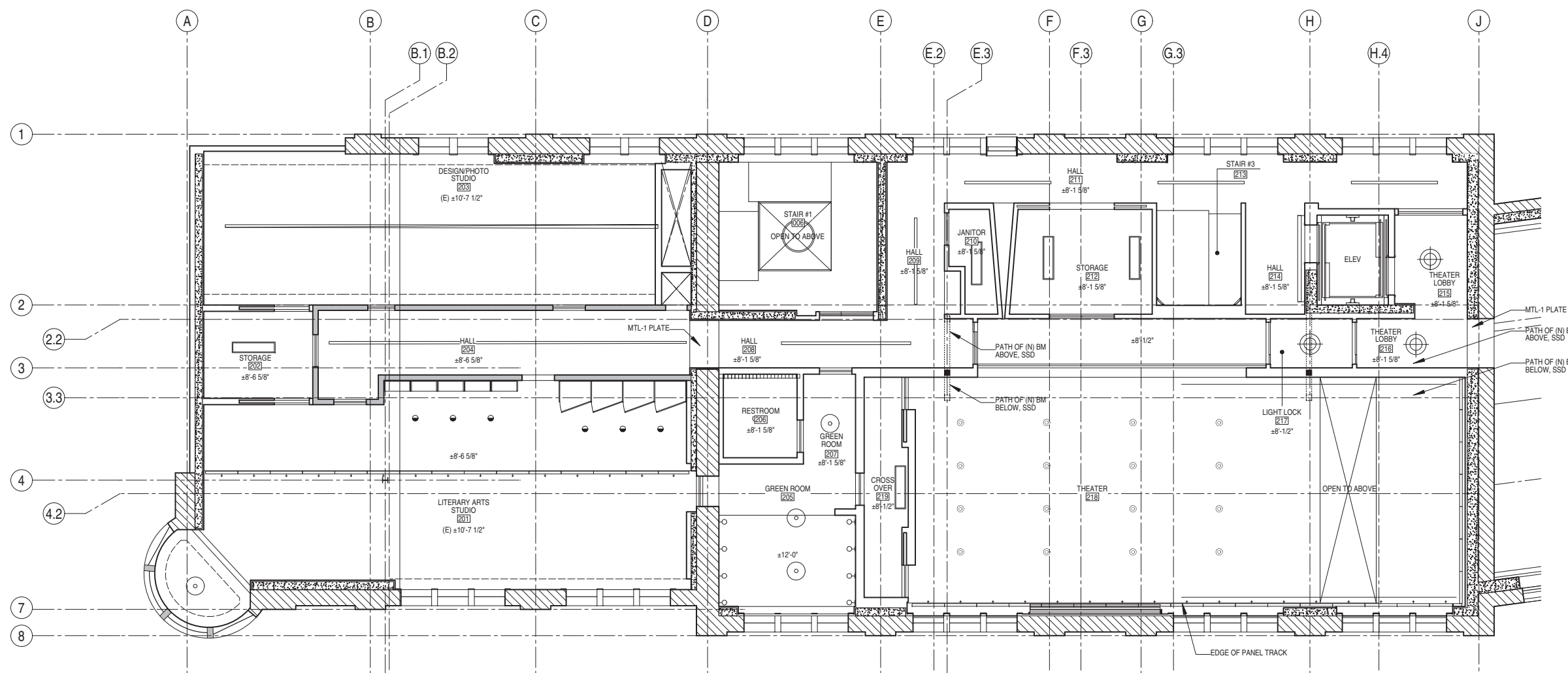
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ISSUE	DATE
50% DD SET	03.09.15
100% DD SET	06.03.15
C OF A SET	09.11.15



A3.2A

LEVEL 2
 REFLECTED
 CEILING PARTIAL
 PLAN - NORTH



KEY

	(E) BRICK
	(E) WALL
	(N) WALL
	(N) CONCRETE, SSD

LEVEL 2 REFLECTED CEILING PARTIAL PLAN - NORTH

0 3' 16' 3/16"=1'-0"

- RCP ELEVATIONS MEASURED FROM FINISH FLR U.O.N.
- REFER TO LIGHTING PLAN FOR FIXTURE TYPES & ADDITIONAL INFORMATION
- REFER TO FINISH SCHEDULE AND INTERIOR ELEVATIONS FOR ADDITIONAL INFORMATION
- REFER TO A3.4 FOR EXISTING CEILING CONDITION SURVEY

1



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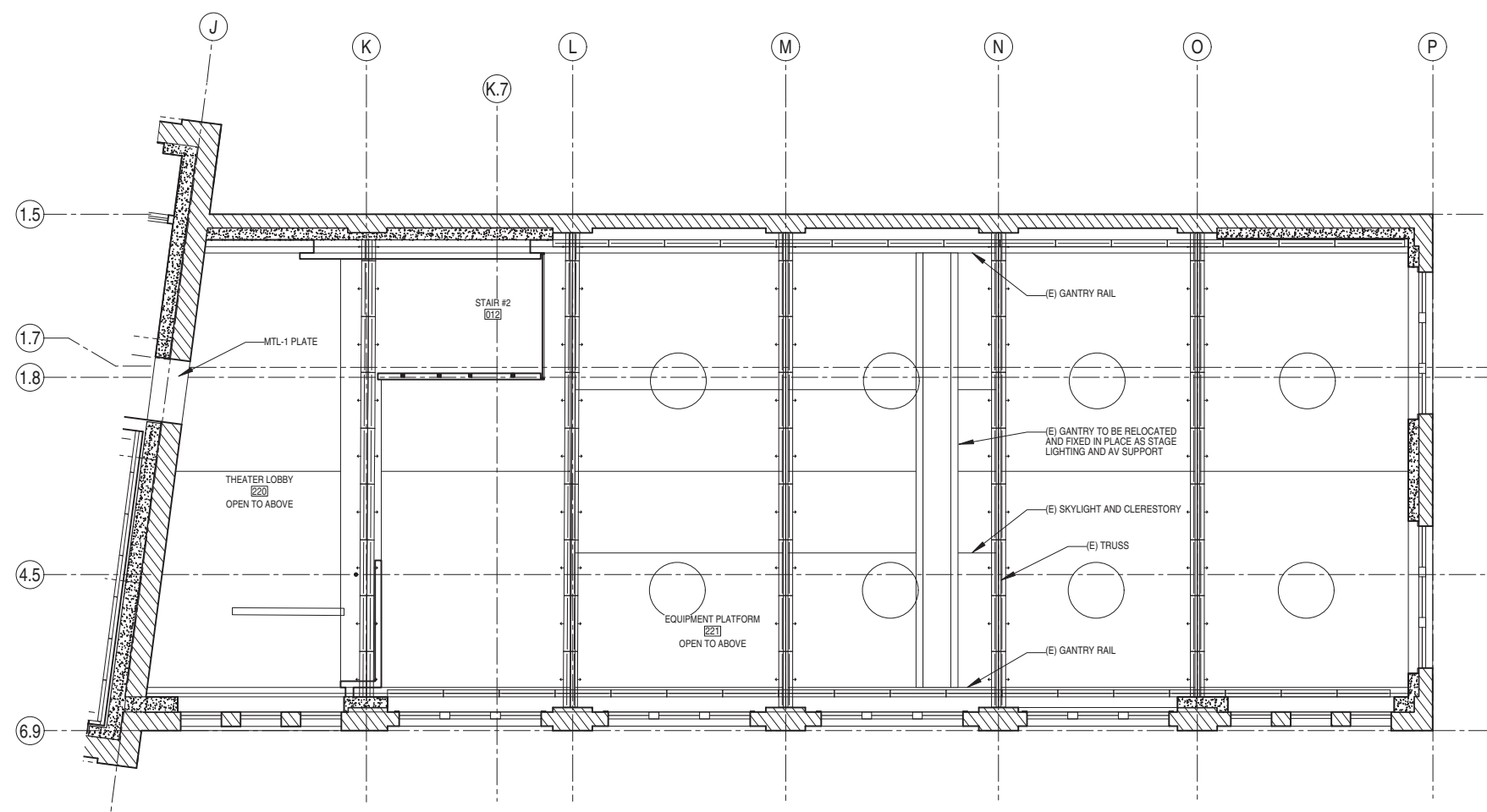
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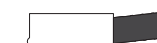
KEY

	(E) BRICK
	(E) WALL
	(N) WALL
	(N) CONCRETE, SSD

LEVEL 2 REFLECTED CEILING PARTIAL PLAN - SOUTH

0 3' 16' 3/16"=1'-0"

- RCP ELEVATIONS MEASURED FROM FINISH FLR U.O.N.
- REFER TO LIGHTING PLAN FOR FIXTURE TYPES & ADDITIONAL INFORMATION
- REFER TO FINISH SCHEDULE AND INTERIOR ELEVATIONS FOR ADDITIONAL INFORMATION
- REFER TO A3.4 FOR EXISTING CEILING CONDITION SURVEY



1
-

A3.2B

LEVEL 2
 REFLECTED
 CEILING PARTIAL
 PLAN - SOUTH



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 AND POWERHOUSE

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DRAWN: MH

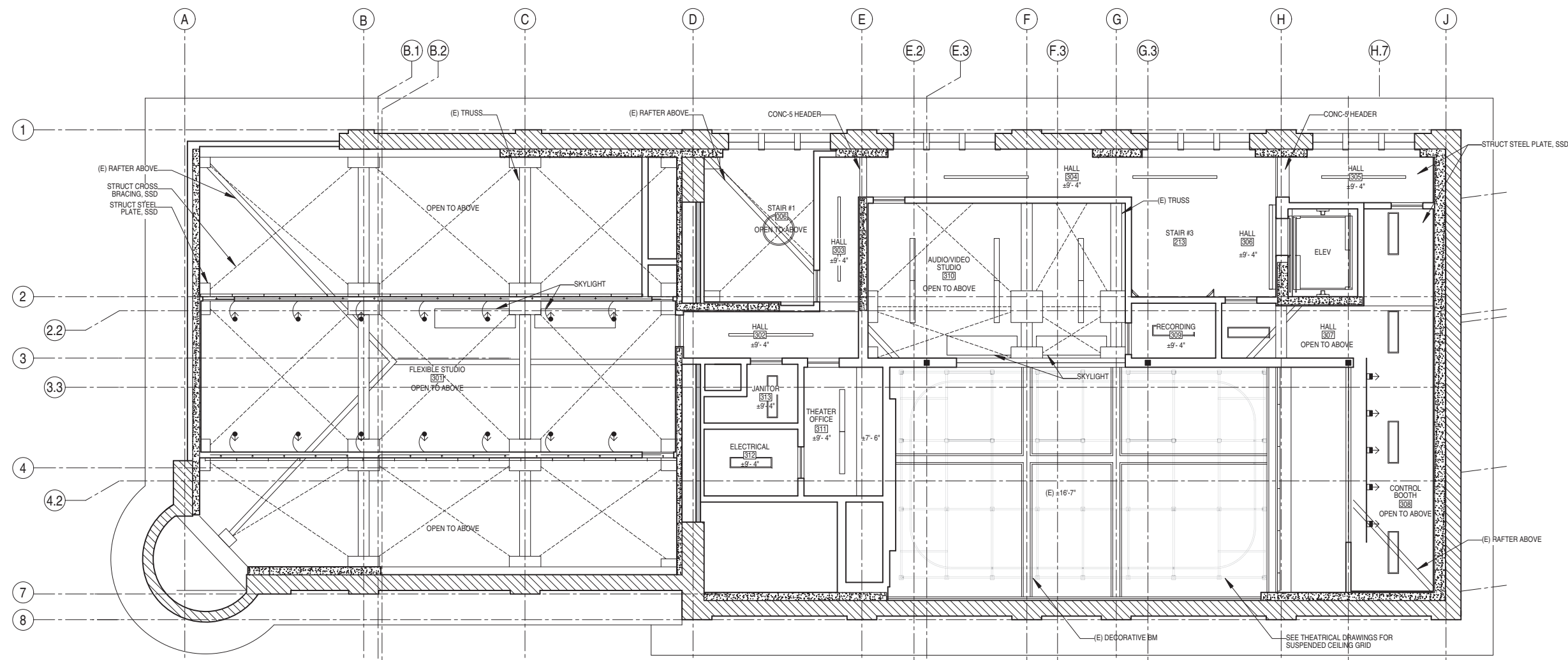
SCALE: AS NOTED

ISSUE	DATE
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100% DD SET	06.03.15
C OF A SET	09.11.15



A3.3

LEVEL 3
 REFLECTED
 CEILING PARTIAL
 PLAN - NORTH



KEY

	(E) BRICK
	(E) WALL
	(N) WALL
	(N) CONCRETE, SSD

LEVEL 3 REFLECTED CEILING PARTIAL PLAN - NORTH

0 3' 16' 3/16"=1'-0"

- RCP ELEVATIONS MEASURED FROM FINISH FLR U.O.N.
- REFER TO LIGHTING PLAN FOR FIXTURE TYPES & ADDITIONAL INFORMATION
- REFER TO FINISH SCHEDULE AND INTERIOR ELEVATIONS FOR ADDITIONAL INFORMATION
- REFER TO A3.4 FOR EXISTING CEILING CONDITION SURVEY

1



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DATE: 08.08.14

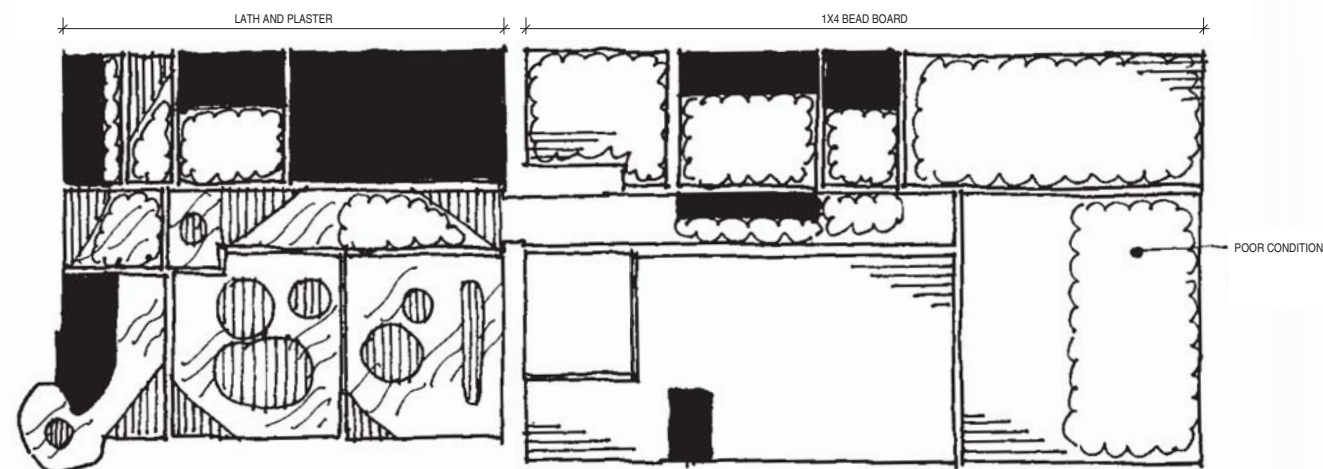
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SCALE: AS NOTED

ISSUE	DATE
100% DD SET	06.03.15
C OF A SET	09.11.15

2
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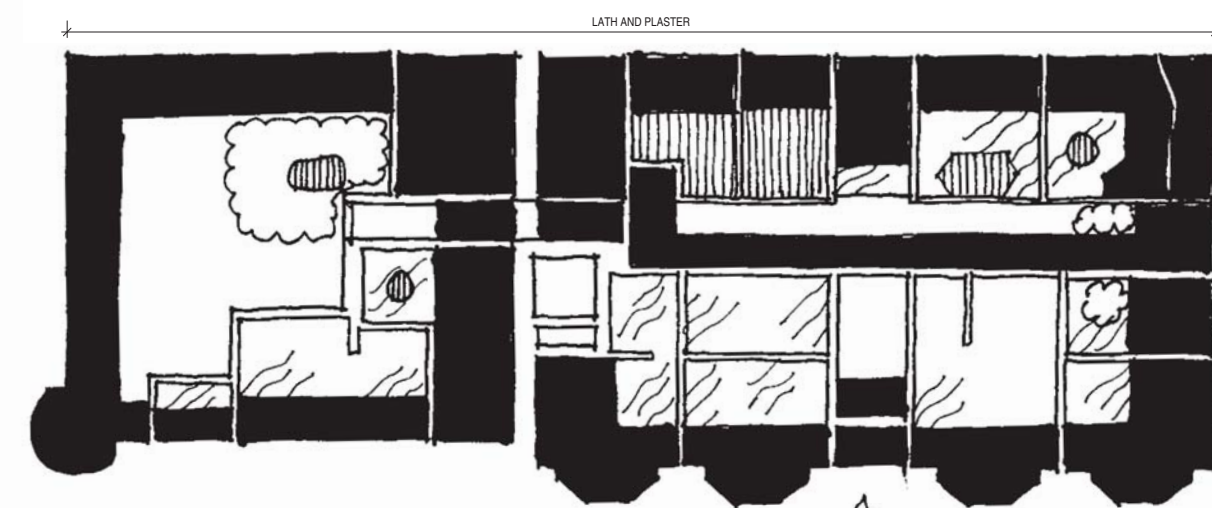


KEY

	BEAD BOARD
	MISSING LATH AND PLASTER/CLG
	MISSING PLASTER
	MOLD AND MILDEW
	CRACKS IN PLASTER

LEVEL 2 EXISTING CEILING CONDITION SURVEY - NORTH

N.T.S.



KEY

	BEAD BOARD
	MISSING LATH AND PLASTER/CLG
	MISSING PLASTER
	MOLD AND MILDEW
	CRACKS IN PLASTER

LEVEL 1 EXISTING CEILING CONDITION SURVEY - NORTH

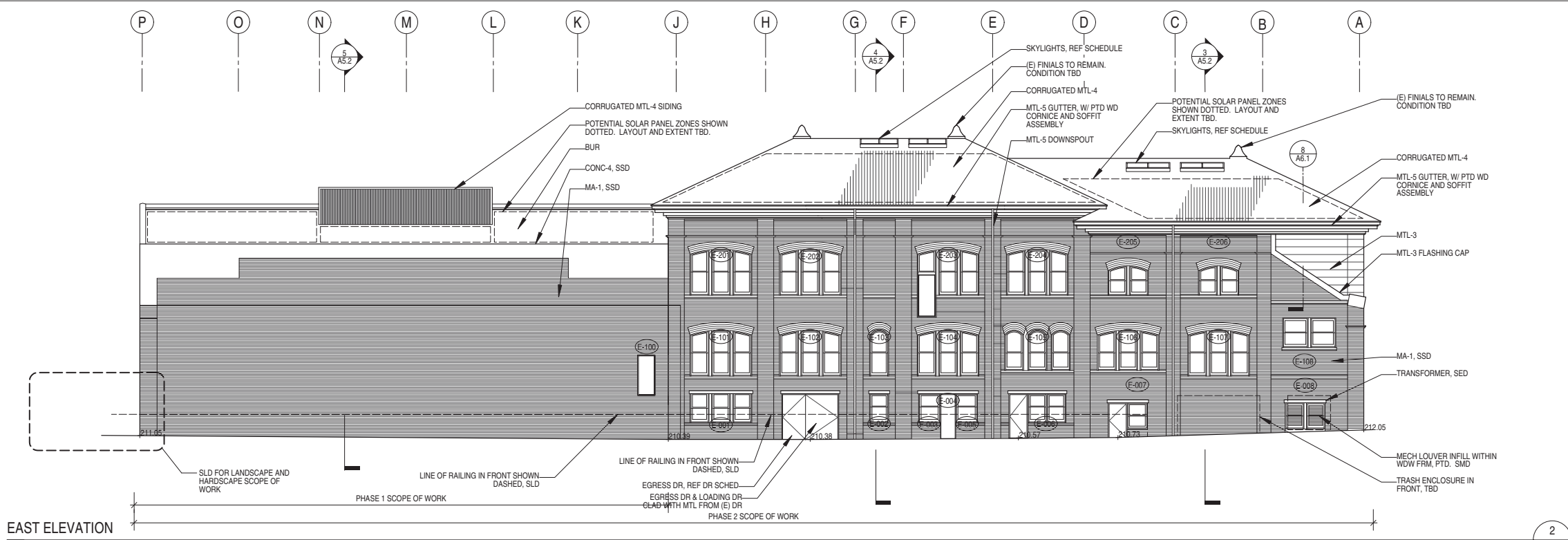
N.T.S.

1
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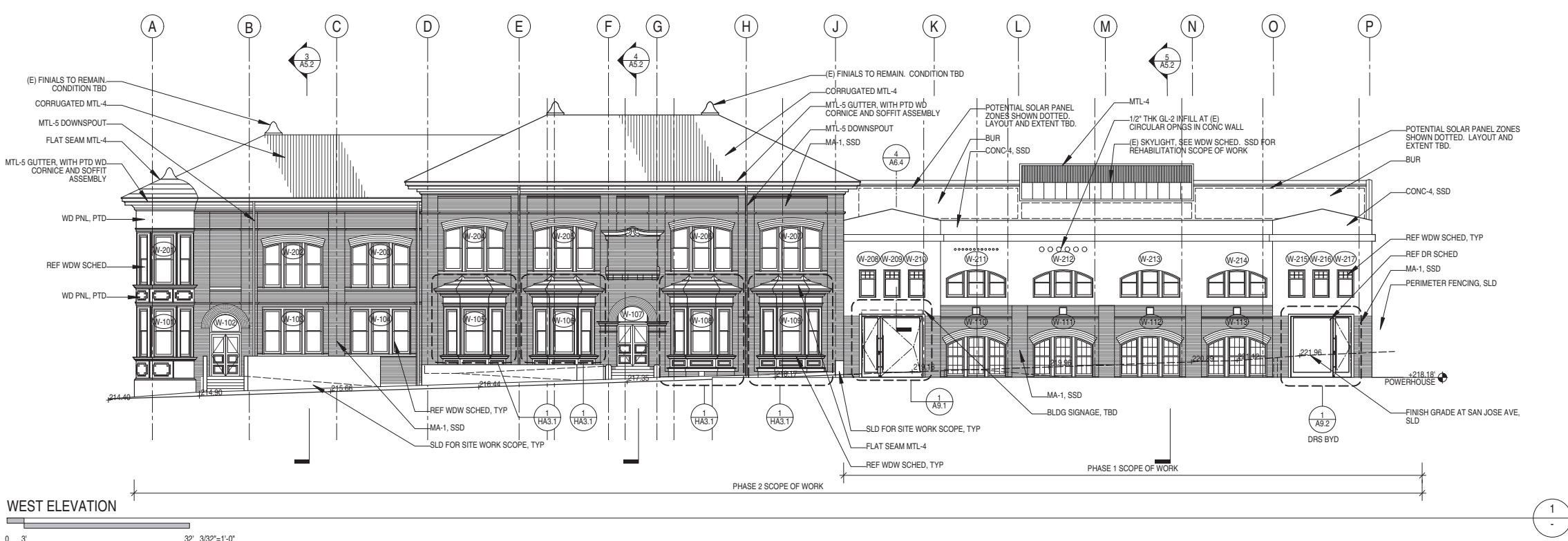


A3.4

EXISTING CEILING
 CONDITION SURVEY



EAST ELEVATION



WEST ELEVATION

- SHEET NOTES**
- PROTECT ALL EXISTING HISTORIC MATERIALS TO REMAIN IN PLACE DURING CONSTRUCTION.
 - SEE WINDOW / DOOR SCHEDULES AND HISTORIC ARCH DRAWINGS FOR (E) WINDOW AND (E) DOOR SCOPE OF WORK
 - SEE STRUCTURAL AND FACADE RESTORATION DRAWINGS FOR (E) WALL AND ROOF REHABILITATION AND STABILIZATION SCOPE OF WORK
 - ALL SITE WORK AND SITE ACCESS WITHIN SFMTA RAILYARD TO BE COORDINATED WITH SFMTA.

MATERIAL LEGEND

ABBREV.	DESCRIPTION
CONCRETE	
(INT) CONC-1	FINISHED TOPPING CONCRETE
(INT) CONC-2	EXPOSED STRUCTURAL SLAB
(INT) CONC-3	EXPOSED ARCH CONCRETE WALL
(INT) CONC-4	EXISTING CONCRETE WALL (REF STRUCT)
(INT) CONC-5	CONCRETE WALL (REF STRUCT)
(INT) CONC-6	PREFABST CONCRETE
MASONRY	
MA-1	EXISTING MASONRY WALL
METAL	
(INT) MTL-1	ORNAMENTAL STEEL
(INT) MTL-2	SHEET METAL GLAD WALL PANELS
MTL-3	EXTERIOR METAL SIDING
MTL-4	METAL ROOF
MTL-5	METAL GUTTER / DOWNSPOUT
WOOD	
(INT) WD-1	EXISTING WOOD FLOORING
(INT) WD-2	SALVAGED WOOD FLOORING
(INT) WD-3	EXISTING TRIM, PANELING AND FINISH WOODWORK
(INT) WD-4	NEW TRIM, PANELING AND FINISH WOODWORK
(INT) WD-5	WOOD VENEER CASEWORK
(INT) WD-6	PLYWOOD PANELS
(INT) WD-7	ACOUSTIC CHANNELED WOOD PANELS
(INT) WD-8	WOOD PLANK STAIR TREADS
(INT) WD-9	T&G SIDING TO MATCH WD 6
(INT) PLAM-1	PLAM CASEWORK
GLASS	
GL-1	LAMINATED CLEAR GLASS, WITH LOW-E INTERLAYER WITHIN (E) WINDOW FRAME ASSEMBLY
(INT) GL-2	LAMINATED CLEAR GLASS, WITH LOW-E INTERLAYER
(INT) GL-3	LAMINATED CLEAR GLASS
GL-4	NOT USED
GL-5	INSULATED GLASS, WITH LOW-E COATING
GL-6	INSULATED / LAMINATED SKYLIGHT GLASS



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GENEVA CAR BARN

2301 SAN JOSE AVENUE
 SAN FRANCISCO, CA

DATE: 08.08.14

JOB: 0906

DRAWN: MH

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A5.1

EXTERIOR ELEVATIONS



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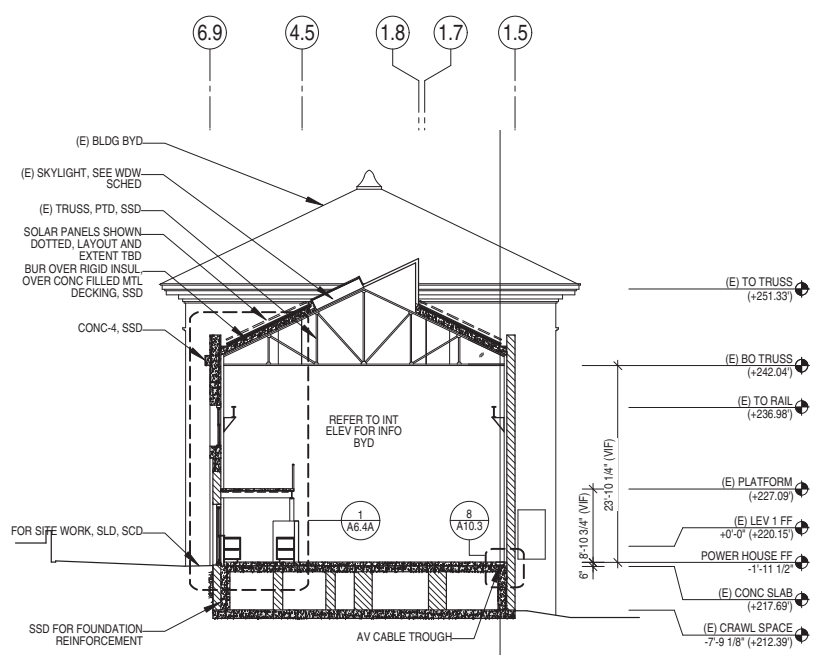
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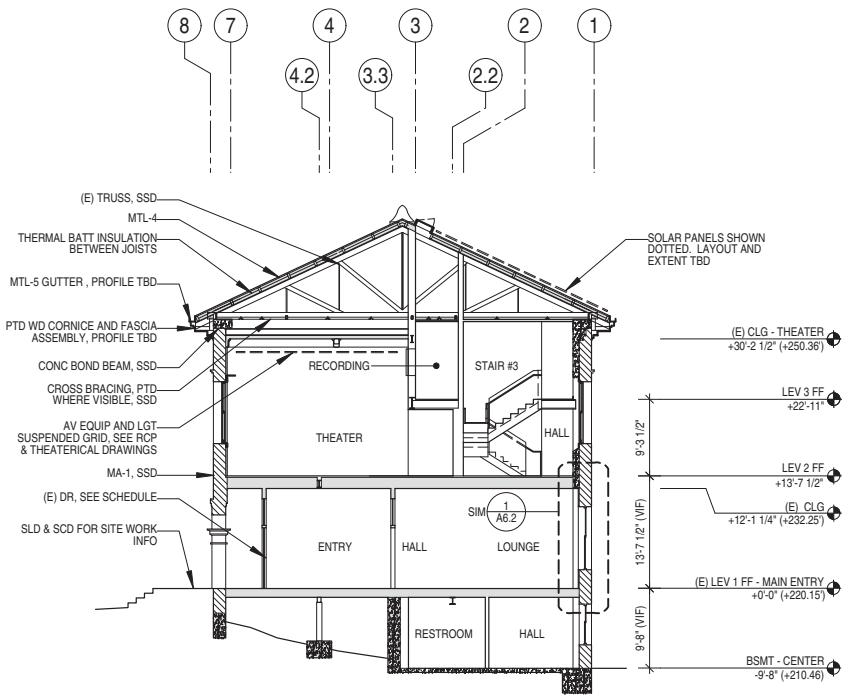
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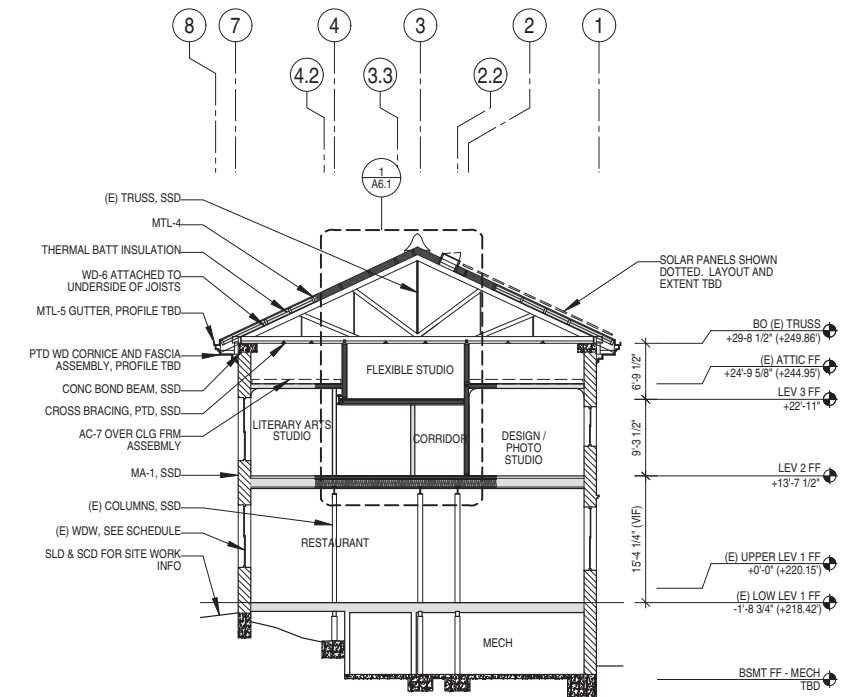
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C OF A SET	09.11.15



POWERHOUSE CROSS SECTION



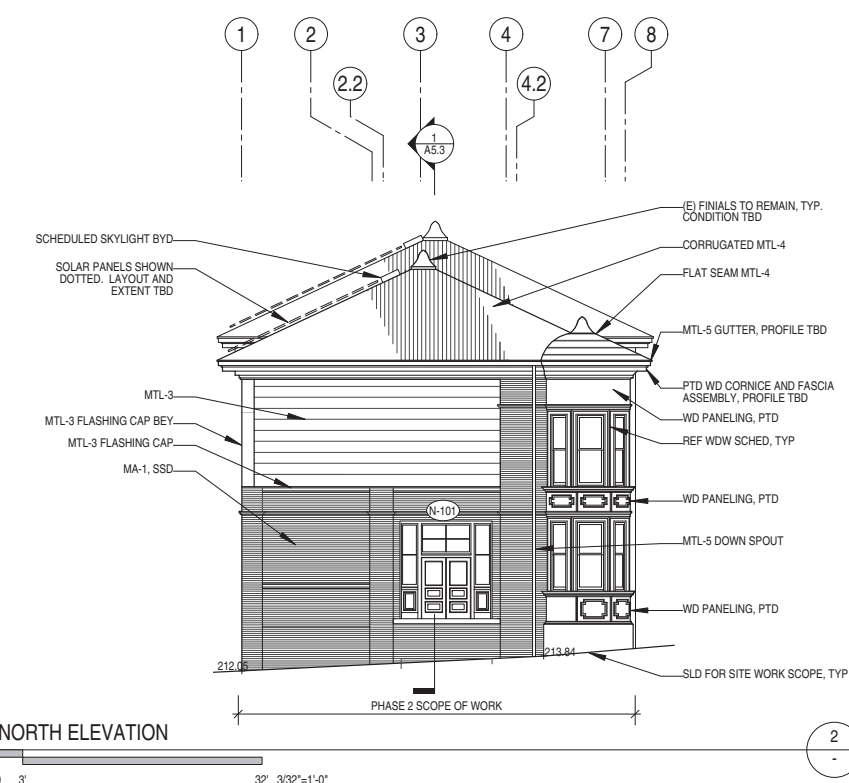
CROSS SECTION - THEATER



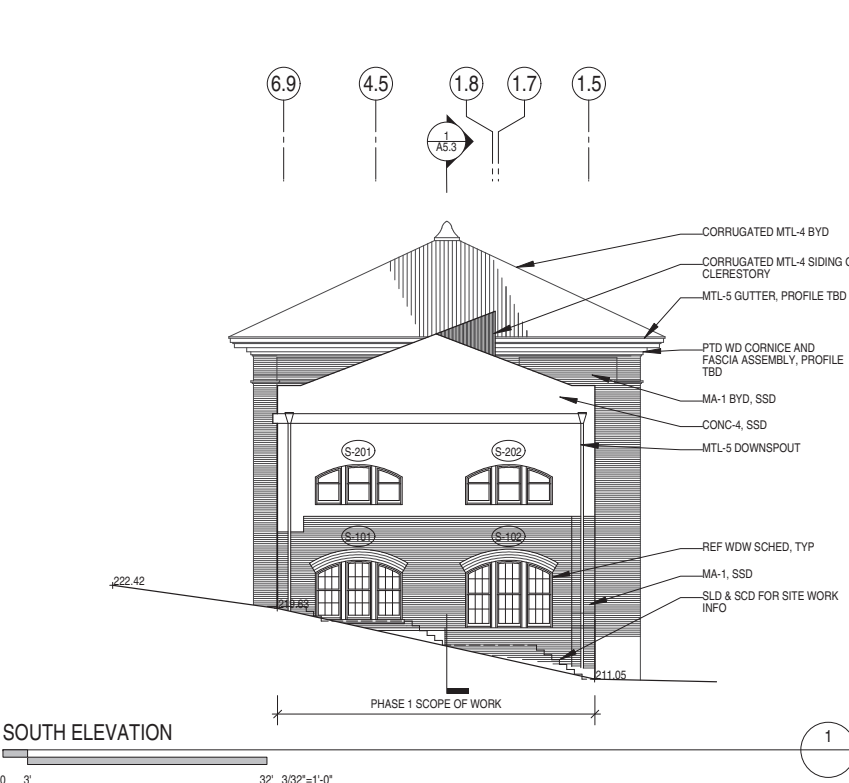
CROSS SECTION - DESIGN / LITERARY ARTS STUDIOS



MATERIAL LEGEND	
ABBREV.	DESCRIPTION
CONCRETE	
(INT) CONC-1	FINISHED TOPPING CONCRETE
(INT) CONC-2	EXPOSED STRUCTURAL SLAB
(INT) CONC-3	EXPOSED ARCH CONCRETE WALL
CONC-4	EXISTING CONCRETE WALL (REF STRUCT)
CONC-5	CONCRETE WALL (REF STRUCT)
(INT) CONC-6	PRECAST CONCRETE
MASONRY	
MA-1	EXISTING MASONRY WALL
METAL	
(INT) MTL-1	ORNAMENTAL STEEL
(INT) MTL-2	SHEET METAL GLAZ WALL PANELS
MTL-3	EXTERIOR METAL SIDING
MTL-4	METAL ROOF
MTL-5	METAL GUTTER / DOWNSPOUT
WOOD	
(INT) WD-1	EXISTING WOOD FLOORING
(INT) WD-2	SALVAGED WOOD FLOORING
(INT) WD-3	EXISTING TRIM, PANELING AND FINISH WOODWORK
(INT) WD-4	NEW TRIM, PANELING AND FINISH WOODWORK
(INT) WD-5	WOOD VENEER CASEWORK
(INT) WD-6	PLYWOOD PANELS
(INT) WD-7	ACOUSTIC CHANNLED WOOD PANELS
(INT) WD-8	WOOD PLANK STAIR TREADS
(INT) WD-9	T&G SIDING TO MATCH WD-8
(INT) FLAM-1	FLAM CASEWORK
GLASS	
GL-1	LAMINATED CLEAR GLASS, WITH LOW-E INTERLAYER WITHIN (E) WINDOW FRAME ASSEMBLY
(INT) GL-2	LAMINATED CLEAR GLASS, WITH LOW-E INTERLAYER
(INT) GL-3	LAMINATED CLEAR GLASS
GL-4	NOT USED
GL-5	INSULATED GLASS, WITH LOW-E COATING
GL-6	INSULATED / LAMINATED SKYLIGHT GLASS



NORTH ELEVATION



SOUTH ELEVATION



SHEET NOTES

- PROTECT ALL EXISTING HISTORIC MATERIALS TO REMAIN IN PLACE DURING CONSTRUCTION.
- SEE WINDOW / DOOR SCHEDULES AND HISTORIC ARCH DRAWINGS FOR (E) WINDOW AND (E) DOOR SCOPE OF WORK
- SEE STRUCTURAL AND FACADE RESTORATION DRAWINGS FOR (E) WALL AND ROOF REHABILITATION AND STABILIZATION SCOPE OF WORK
- ALL SITE WORK AND SITE ACCESS WITHIN SFMTA RAILYARD TO BE COORDINATED WITH SFMTA.

KEY	
	(E) BRICK
	(E) WALL / FLR / ROOF
	(E) CONCRETE
	(N) WALL / FLR / ROOF
	(N) CONCRETE

A5.2

EXTERIOR ELEVATIONS / BUILDING SECTIONS



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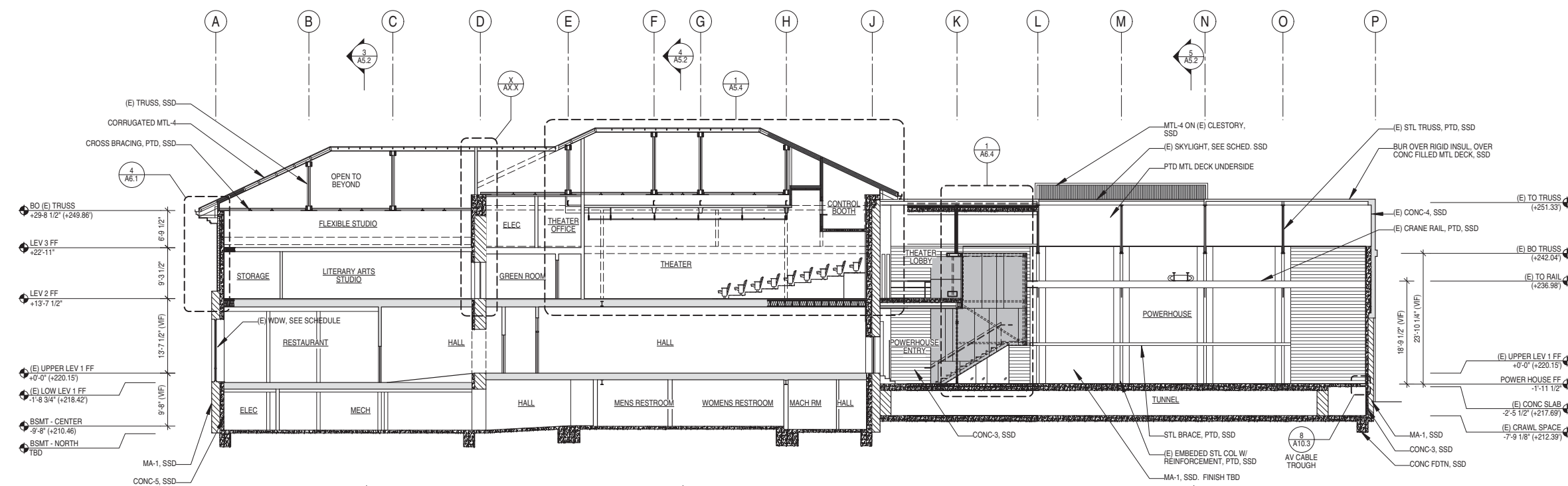
DATE: 08.08.14

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DRAWN: MH

SCALE: AS NOTED

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50% DD SET	03.09.15
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C OF A SET	09.11.15



LONGITUDINAL SECTION

0 3' 32' 3/32"=1'-0"

SHEET NOTES

- PROTECT ALL EXISTING HISTORIC MATERIALS TO REMAIN IN PLACE DURING CONSTRUCTION.
- SEE WINDOW / DOOR SCHEDULES AND HISTORIC ARCH DRAWINGS FOR (E) WINDOW AND (E) DOOR SCOPE OF WORK
- SEE STRUCTURAL AND FACADE RESTORATION DRAWINGS FOR (E) WALL AND ROOF REHABILITATION AND STABILIZATION SCOPE OF WORK
- ALL SITE WORK AND SITE ACCESS WITHIN SFMTA RAILYARD TO BE COORDINATED WITH SFMTA.

KEY

- (E) BRICK
- (E) WALL / FLR / ROOF
- (E) CONCRETE
- (N) WALL / FLR / ROOF
- (N) CONCRETE

A5.3

BUILDING SECTION



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DATE: 08.08.14

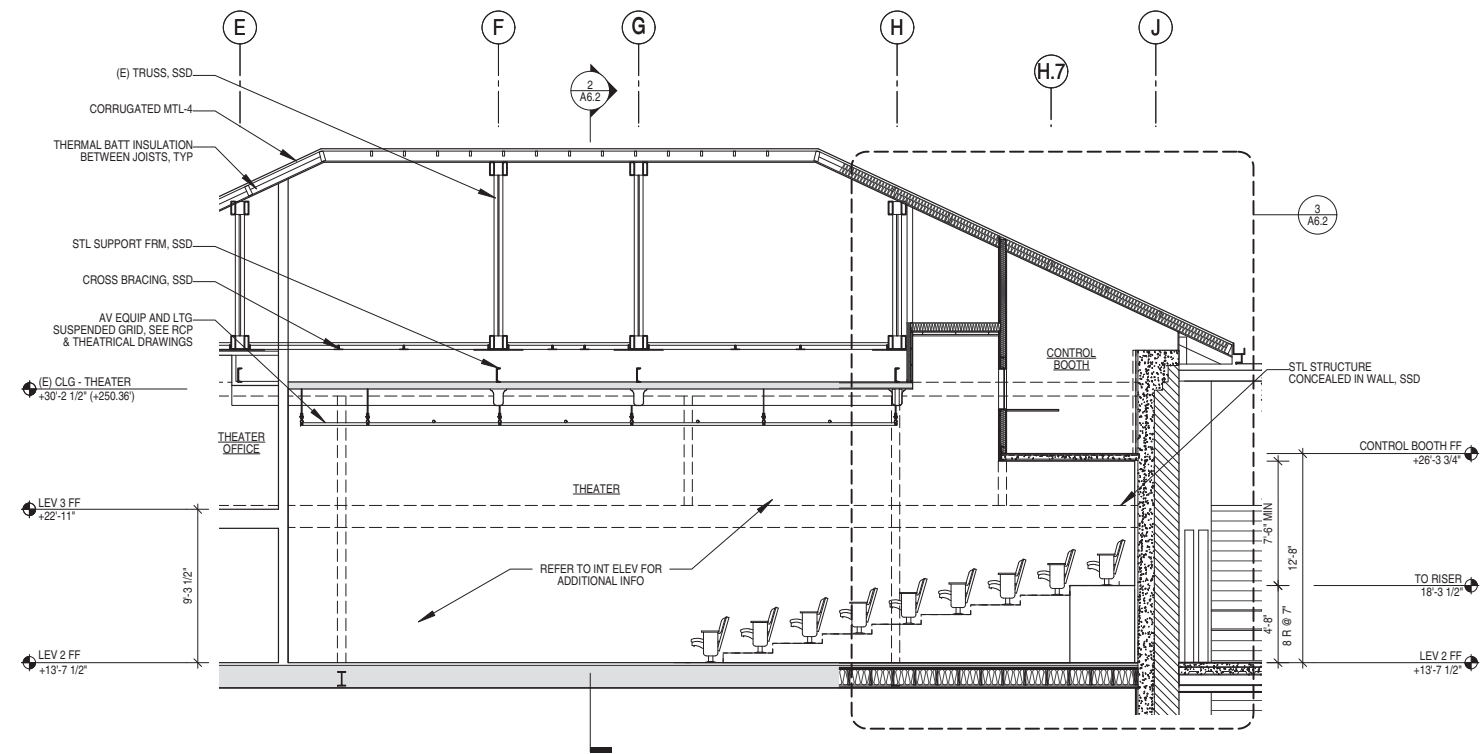
JOB: 0906

DRAWN: MH

SCALE: AS NOTED

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C OF A SET	09.11.15

NOT USED



ENLARGED BLDG SECTION - THEATER AND CONTROL BOOTH



SHEET NOTES

- PROTECT ALL EXISTING HISTORIC MATERIALS TO REMAIN IN PLACE DURING CONSTRUCTION.
- SEE WINDOW / DOOR SCHEDULES AND HISTORIC ARCH DRAWINGS FOR (E) WINDOW AND (E) DOOR SCOPE OF WORK
- SEE STRUCTURAL AND FACADE RESTORATION DRAWINGS FOR (E) WALL AND ROOF REHABILITATION AND STABILIZATION SCOPE OF WORK
- ALL SITE WORK AND SITE ACCESS WITHIN SFMTA RAILYARD TO BE COORDINATED WITH SFMTA.

2

1

A5.4

BUILDING SECTION



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GENEVA CAR BARN

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 SAN FRANCISCO, CA

DATE: 08.08.14

JOB: 0906

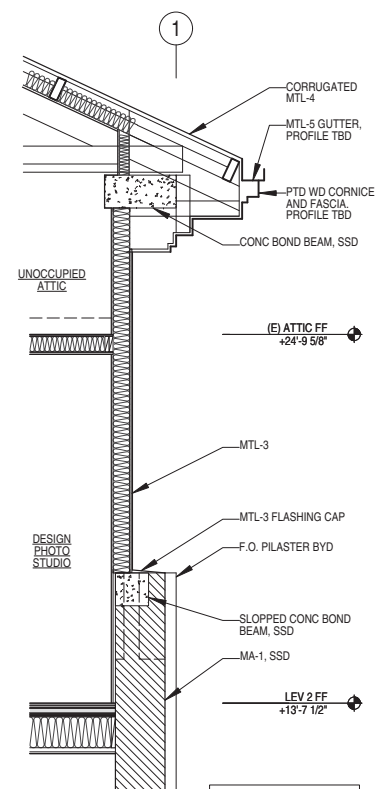
DRAWN: MH

SCALE: AS NOTED

ISSUE	DATE
50% DD SET	03.09.15
100% DD SET	06.03.15
C OF A SET	09.11.15

A6.1

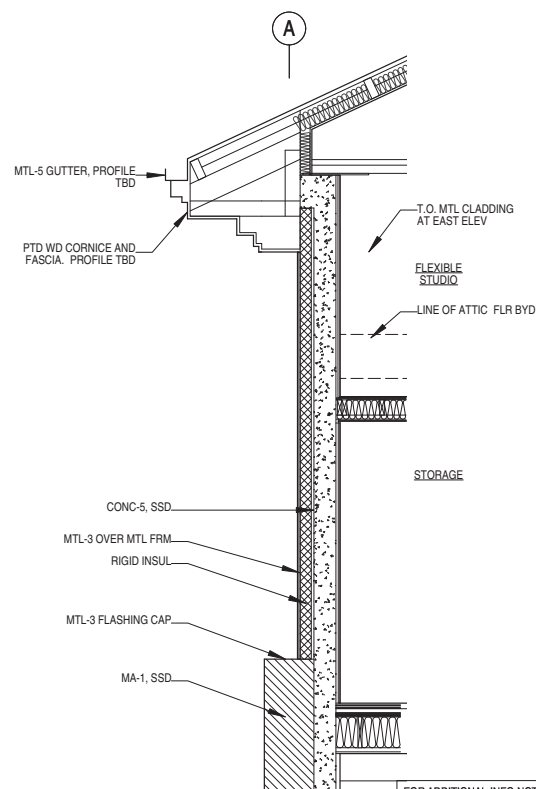
WALL SECTIONS - OFFICE NORTH



SECTION - CLASSROOM / STUDIO AT GRID 1



FOR ADDITIONAL INFO NOT INDICATED, REF DET 1 & 3



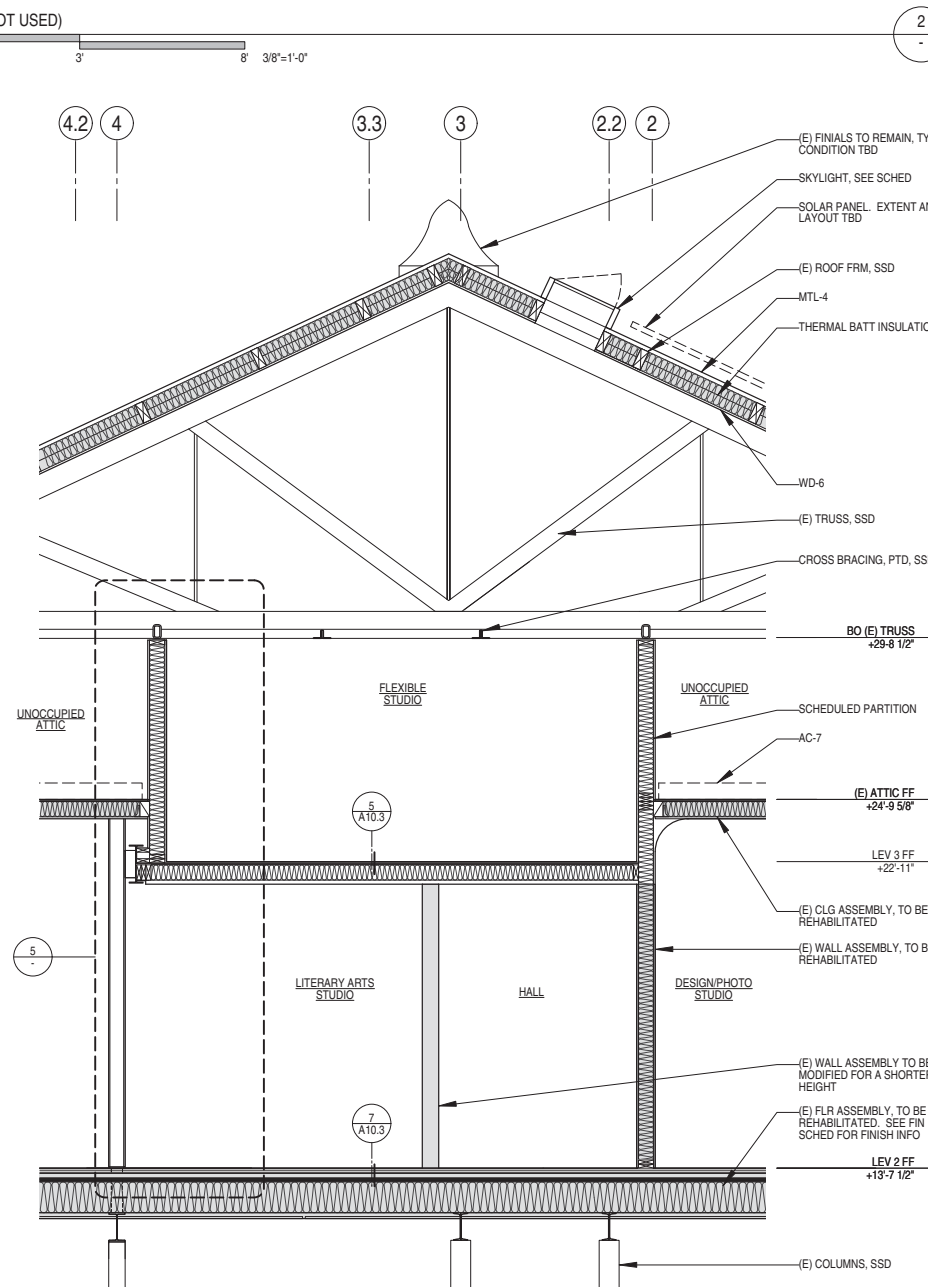
SECTION - CLASSROOM / STUDIO AT GRID A



FOR ADDITIONAL INFO NOT INDICATED, REF DET 3



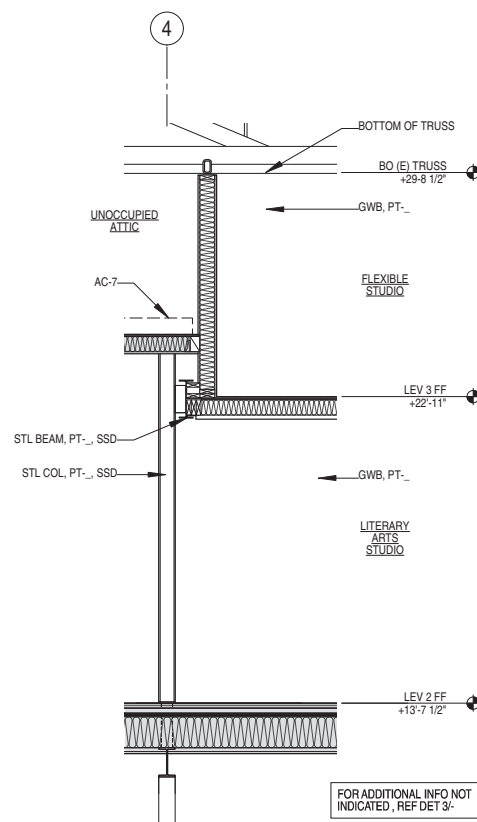
(NOT USED)



SECTION - STUDIO



SECTION - CLASSROOM / STUDIO AT GRID 4



SECTION - CLASSROOM / STUDIO AT GRID 4



FOR ADDITIONAL INFO NOT INDICATED, REF DET 3

SECTION - CLASSROOM / STUDIO AT GRID A



NOT ISSUED

NOT ISSUED

SECTION - CLASSROOM / STUDIO AT GRID 4



SECTION - CLASSROOM / STUDIO AT GRID 4



SECTION - CLASSROOM / STUDIO AT GRID A



SECTION - CLASSROOM / STUDIO AT GRID A



SECTION - STUDIO





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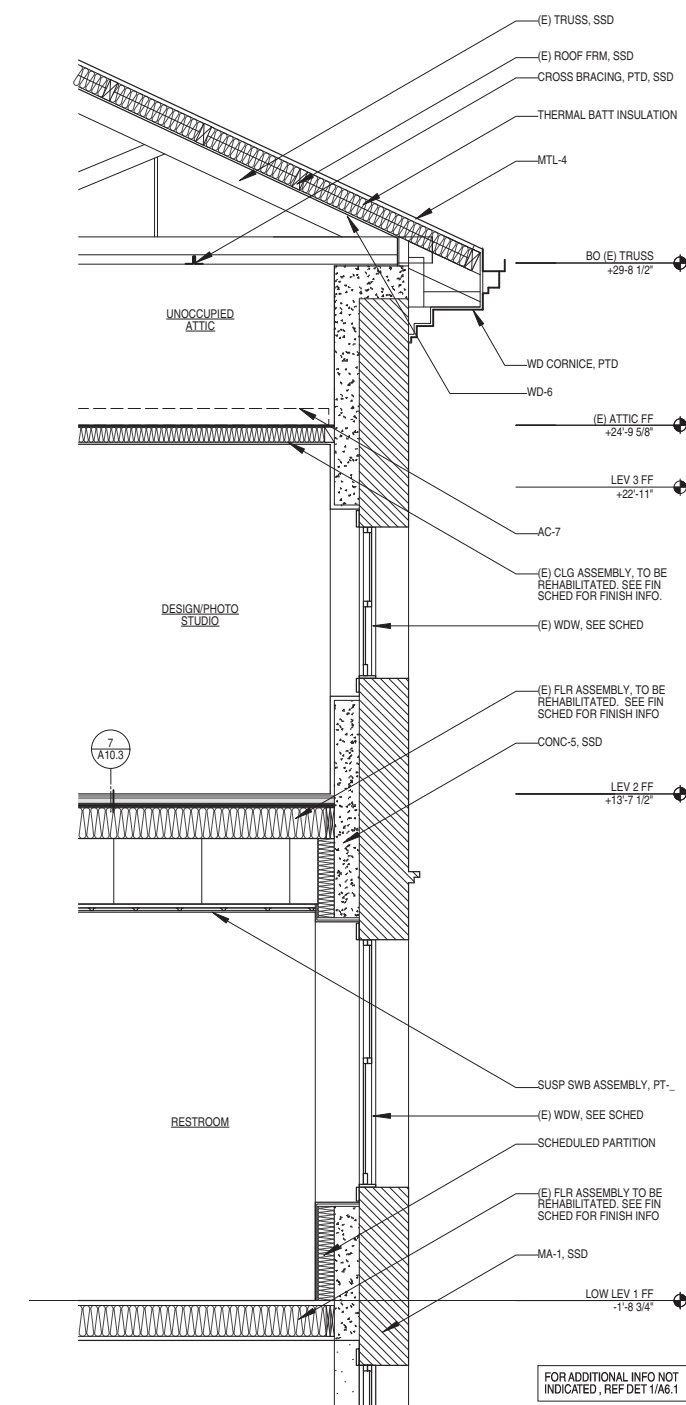
DATE: 08.08.14

JOB: 0906

DRAWN: MH

SCALE: AS NOTED

ISSUE	DATE
C OF A SET	09.11.15



WALL SECTION @ RESTROOM (LOOKING NORTH)



FOR ADDITIONAL INFO NOT INDICATED, REF DET 1/A6.1

1

A6.1a

WALL SECTIONS - OFFICE NORTH



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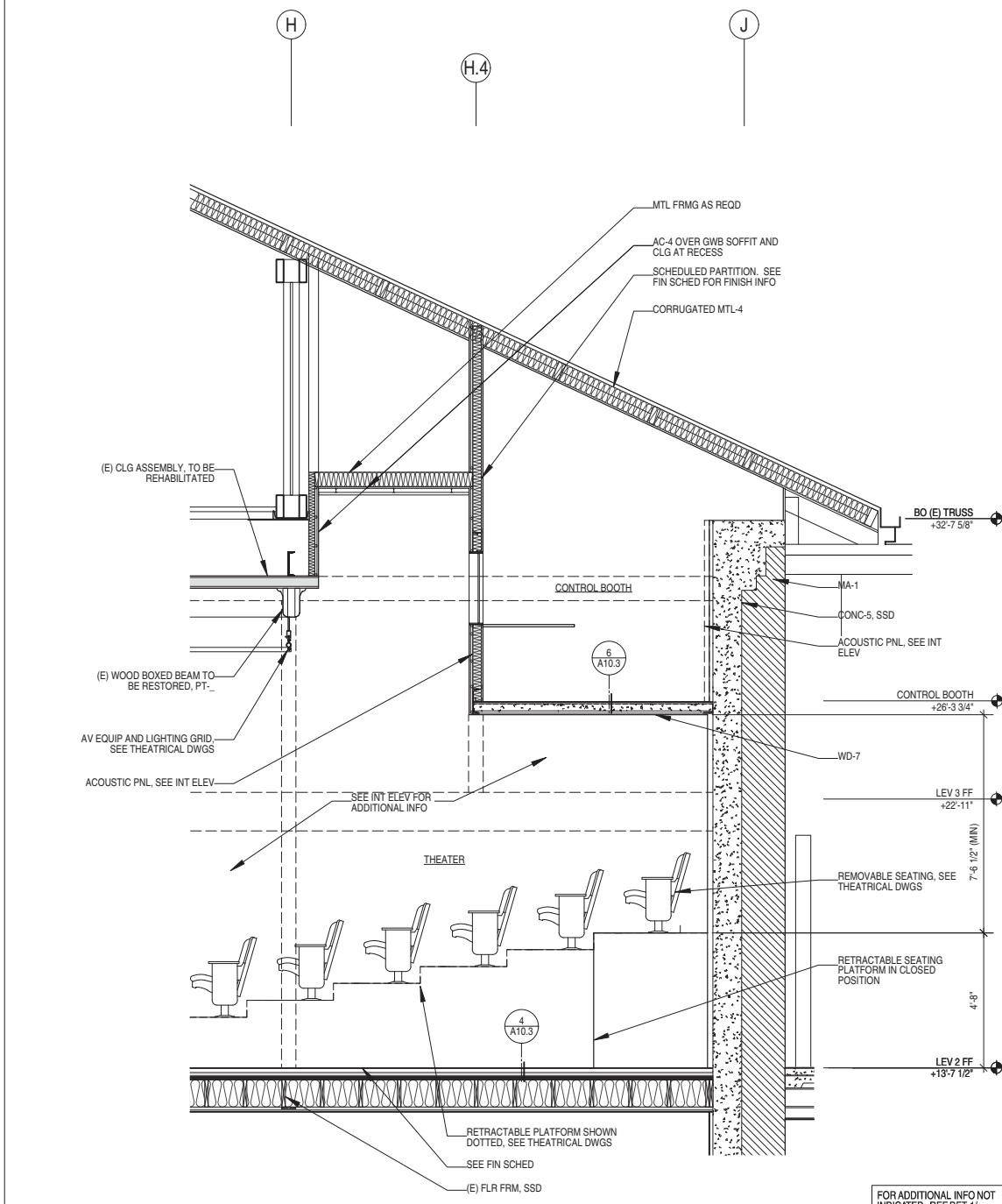
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SCALE: AS NOTED

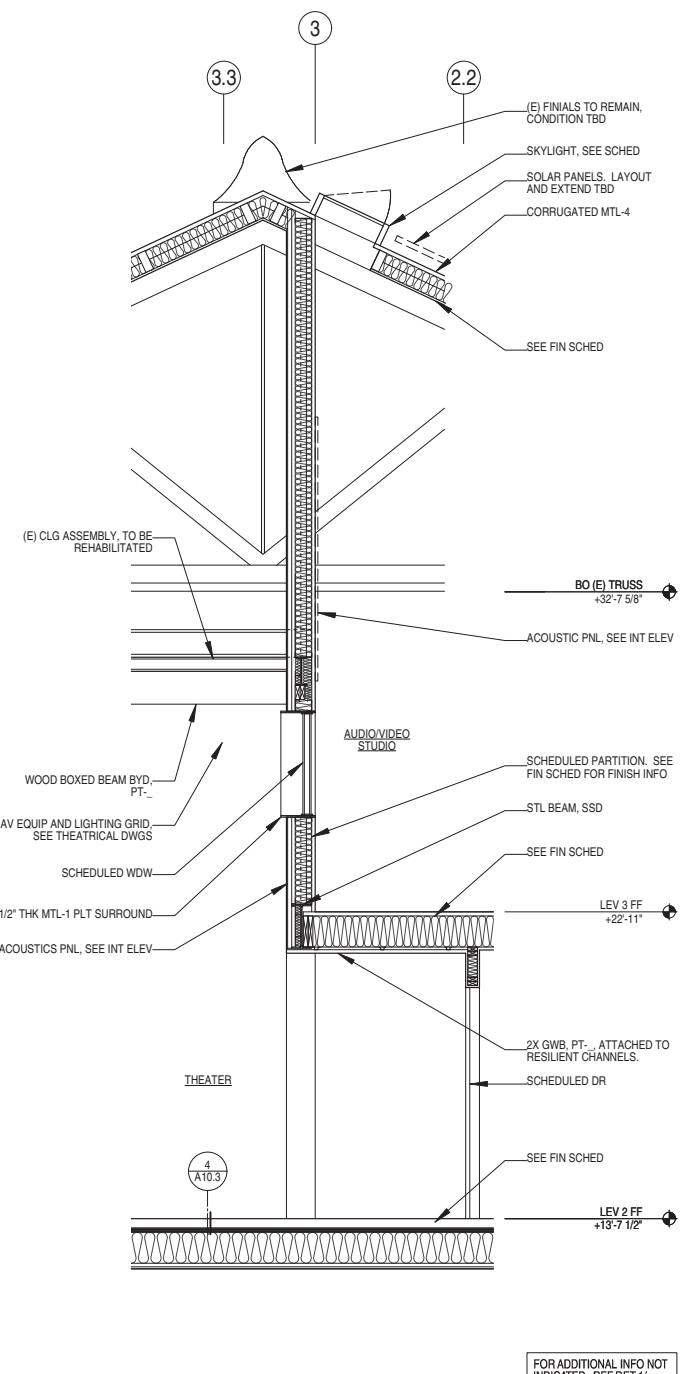
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50% DD SET	03.09.15
100% DD SET	06.03.15
C OF A SET	09.11.15

A6.2

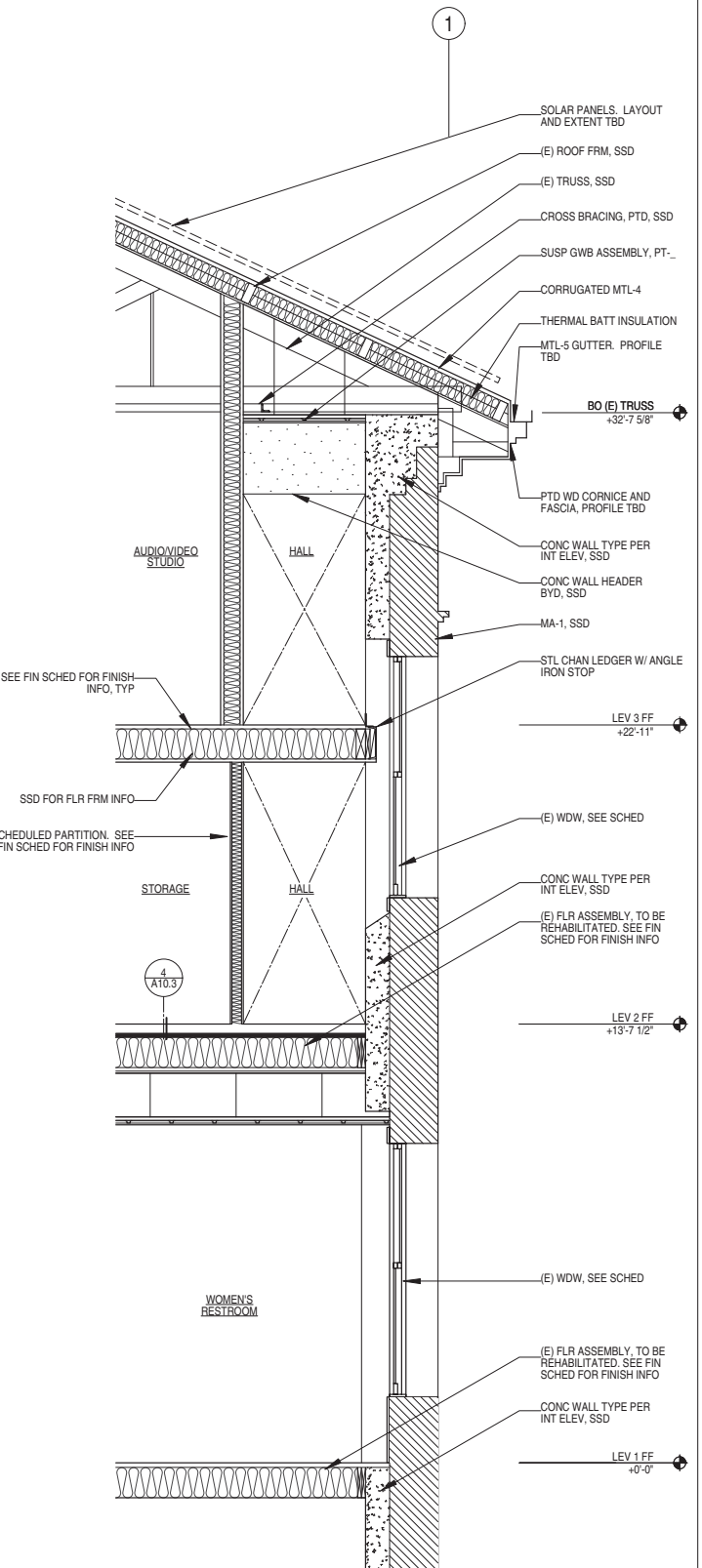
WALL SECTIONS -
 OFFICE CENTER



WALL SECTION @ THEATER - SOUTH WALL
 0 3' 8' 3/8"=1'-0"



WALL SECTION @ THEATER - EAST WALL
 0 3' 8' 3/8"=1'-0"



WALL SECTION @ HALL
 0 3' 8' 3/8"=1'-0"

FOR ADDITIONAL INFO NOT INDICATED, REF DET 1-

FOR ADDITIONAL INFO NOT INDICATED, REF DET 1-



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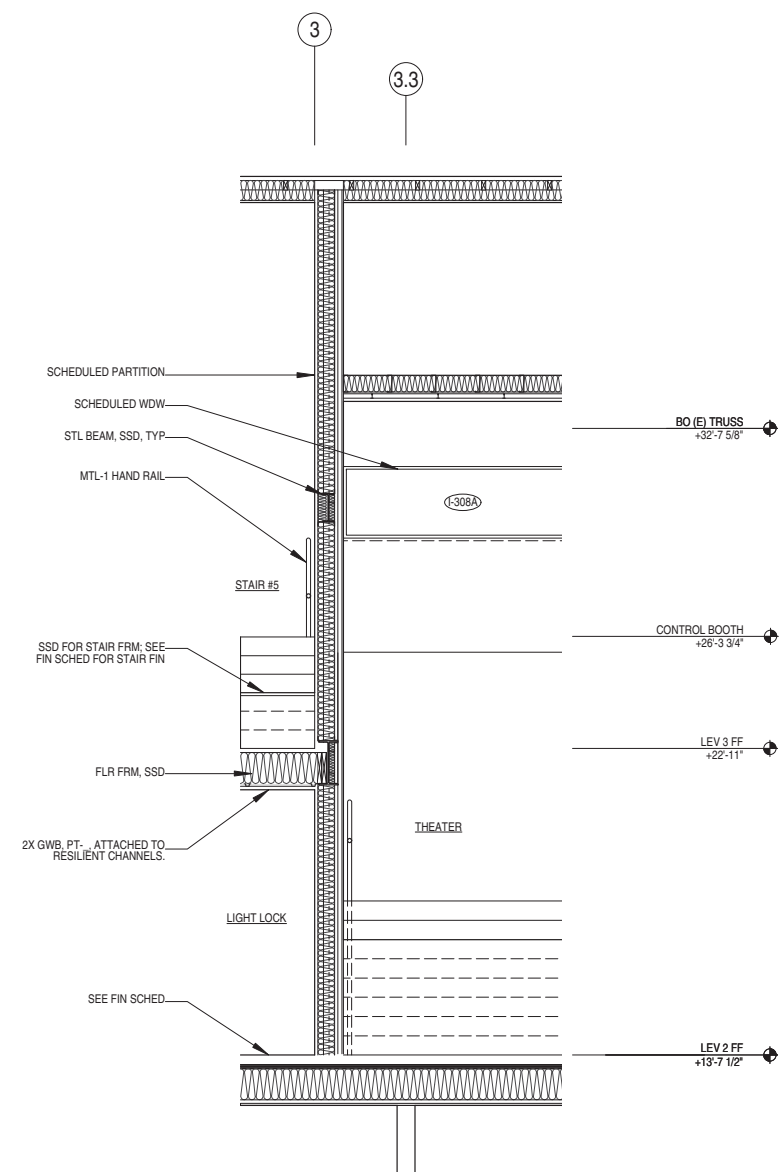
DATE: 08.08.14

JOB: 0906

DRAWN: MH

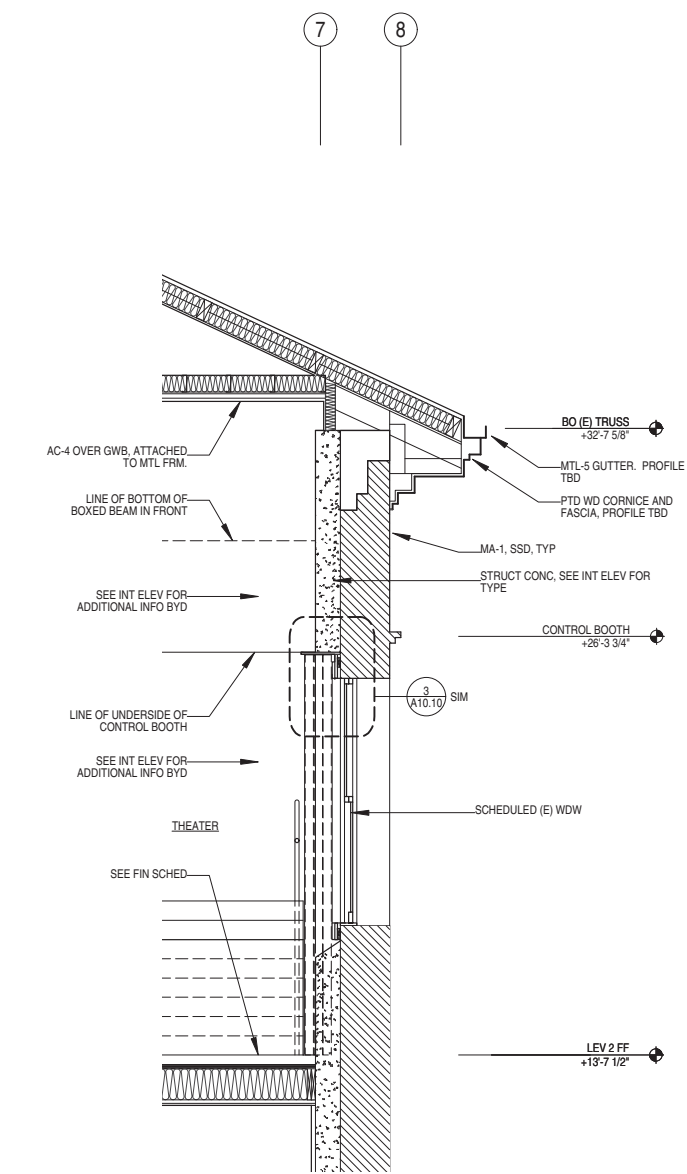
SCALE: AS NOTED

ISSUE	DATE
50% DD SET	03.09.15
100% DD SET	06.03.15
C OF A SET	09.11.15



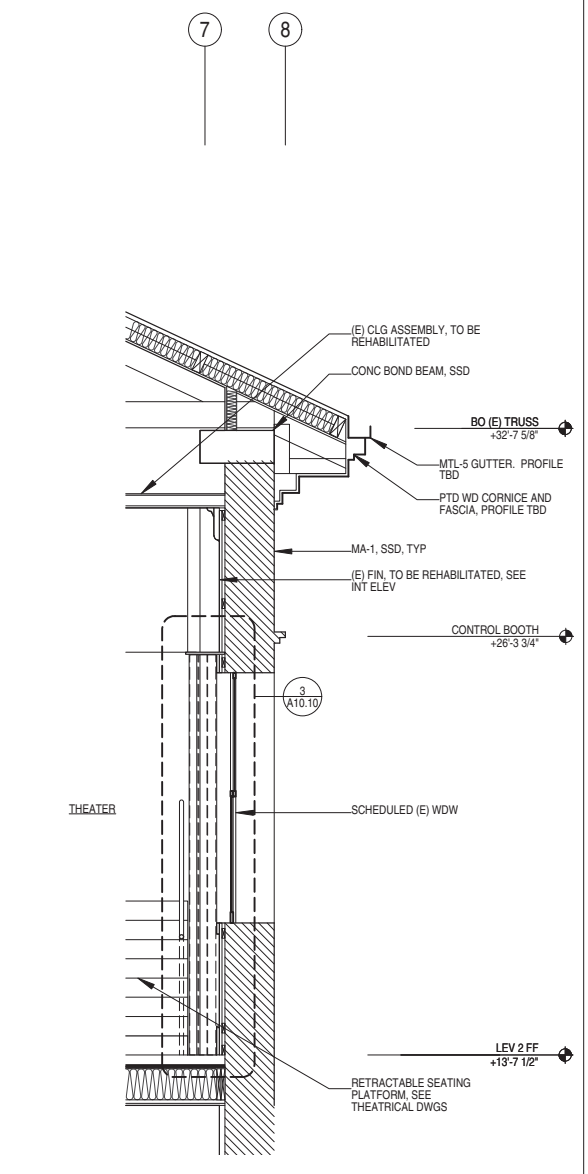
WALL SECTION @ HIGH THEATER SOFFIT - EAST WALL
 0 3' 8' 3/8"=1'-0"

FOR ADDITIONAL INFO NOT INDICATED, REF DET 2/



WALL SECTION @ HIGH THEATER SOFFIT - WEST WALL
 0 3' 8' 3/8"=1'-0"

FOR ADDITIONAL INFO NOT INDICATED, REF DET 1/



WALL SECTION @ THEATER - WEST WALL
 0 3' 8' 3/8"=1'-0"

FOR ADDITIONAL INFO NOT INDICATED, REF DET 1/A6.2



A6.3

WALL SECTIONS - OFFICE CENTER



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2301 SAN JOSE AVENUE
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DATE: 08.08.14

JOB: 0906

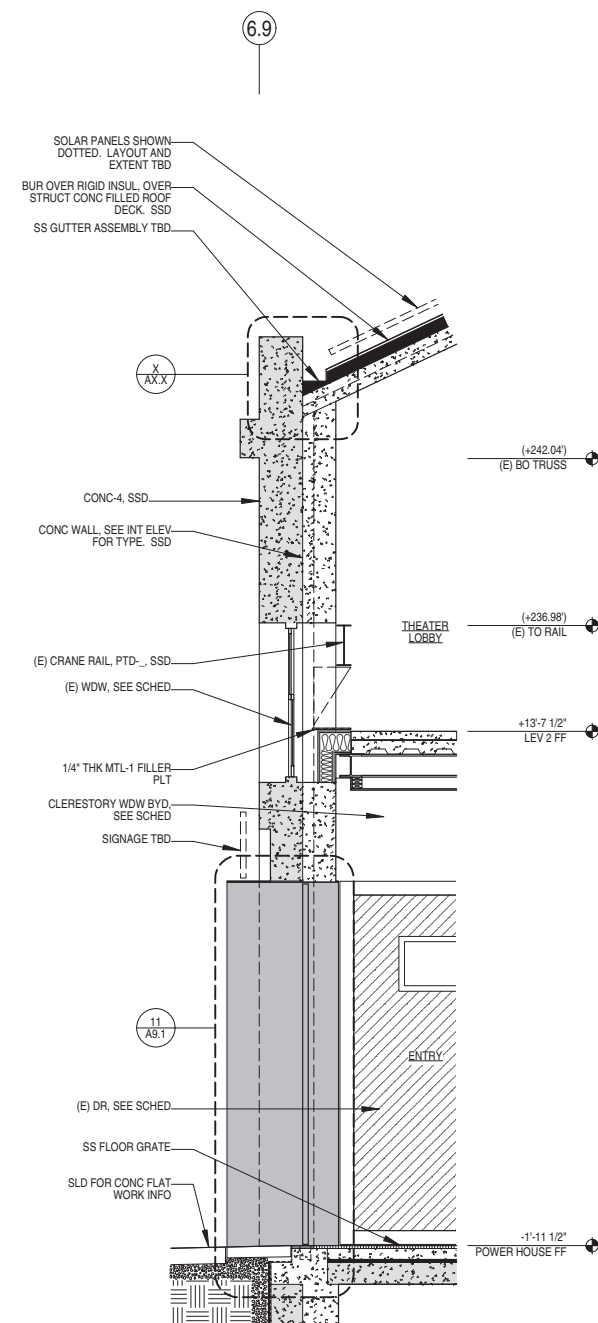
DRAWN: MH

SCALE: AS NOTED

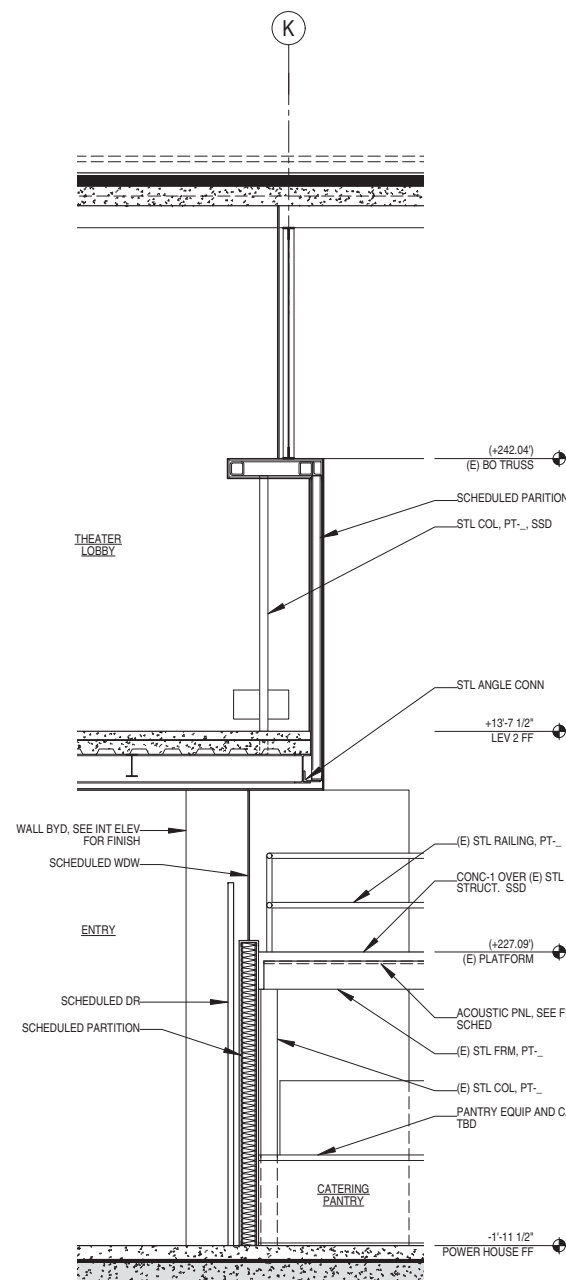
ISSUE	DATE
50% DD SET	03.09.15
100% DD SET	06.03.15
C OF A SET	09.11.15

A6.4

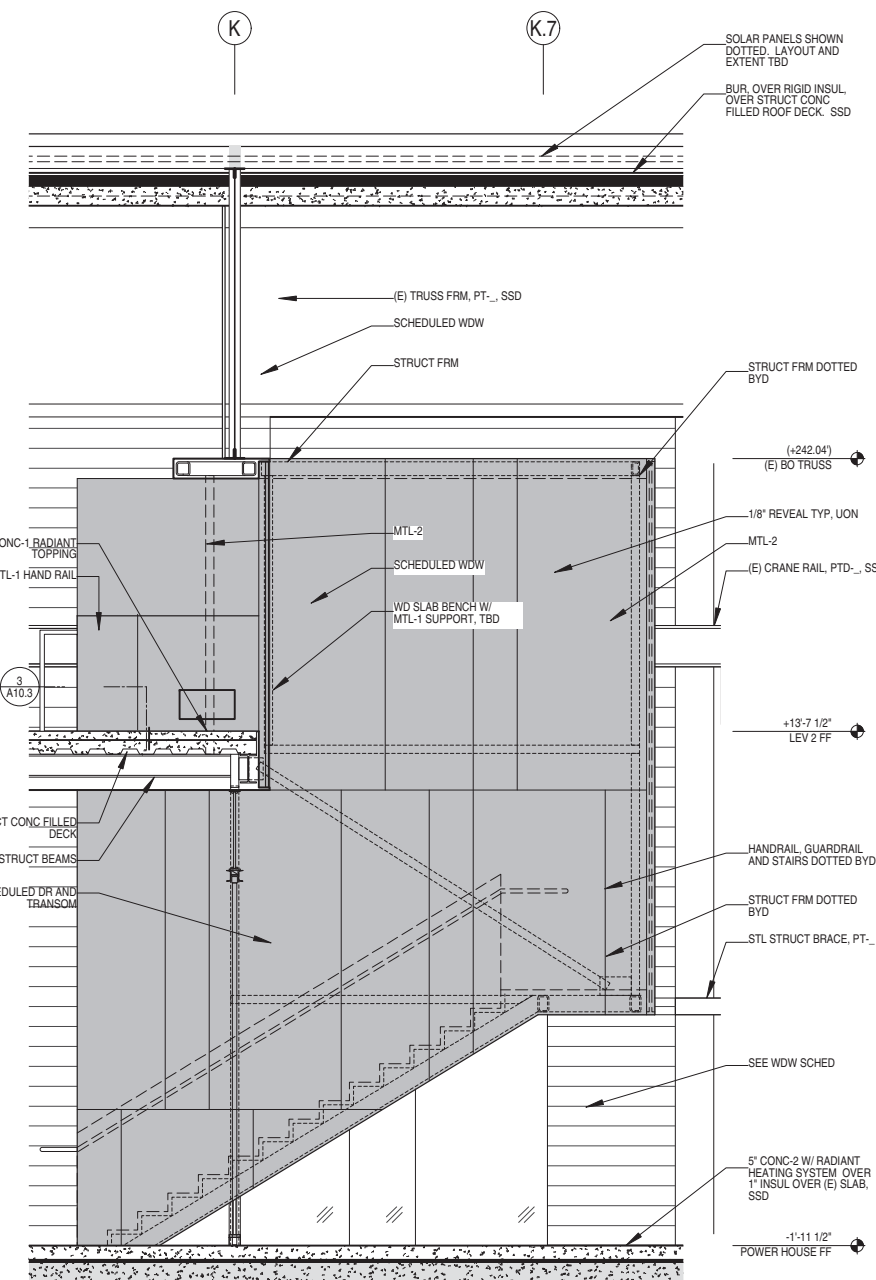
WALL SECTIONS -
 POWERHOUSE



NOT ISSUED



FOR ADDITIONAL INFO NOT INDICATED, REF DET 1/-



WALL SECTION @ POWERHOUSE LOBBY (LOOKING NORTH)

4

WALL SECTION @ POWERHOUSE LOBBY (LOOKING EAST)

3

WALL SECTION @ POWERHOUSE LOBBY (LOOKING EAST)

2

WALL SECTION @ POWERHOUSE LOBBY (LOOKING EAST)

1

0 3' 8' 3/8"=1'-0"

0 3' 8' 3/8"=1'-0"

0 3' 8' 3/8"=1'-0"

0 3' 8' 3/8"=1'-0"



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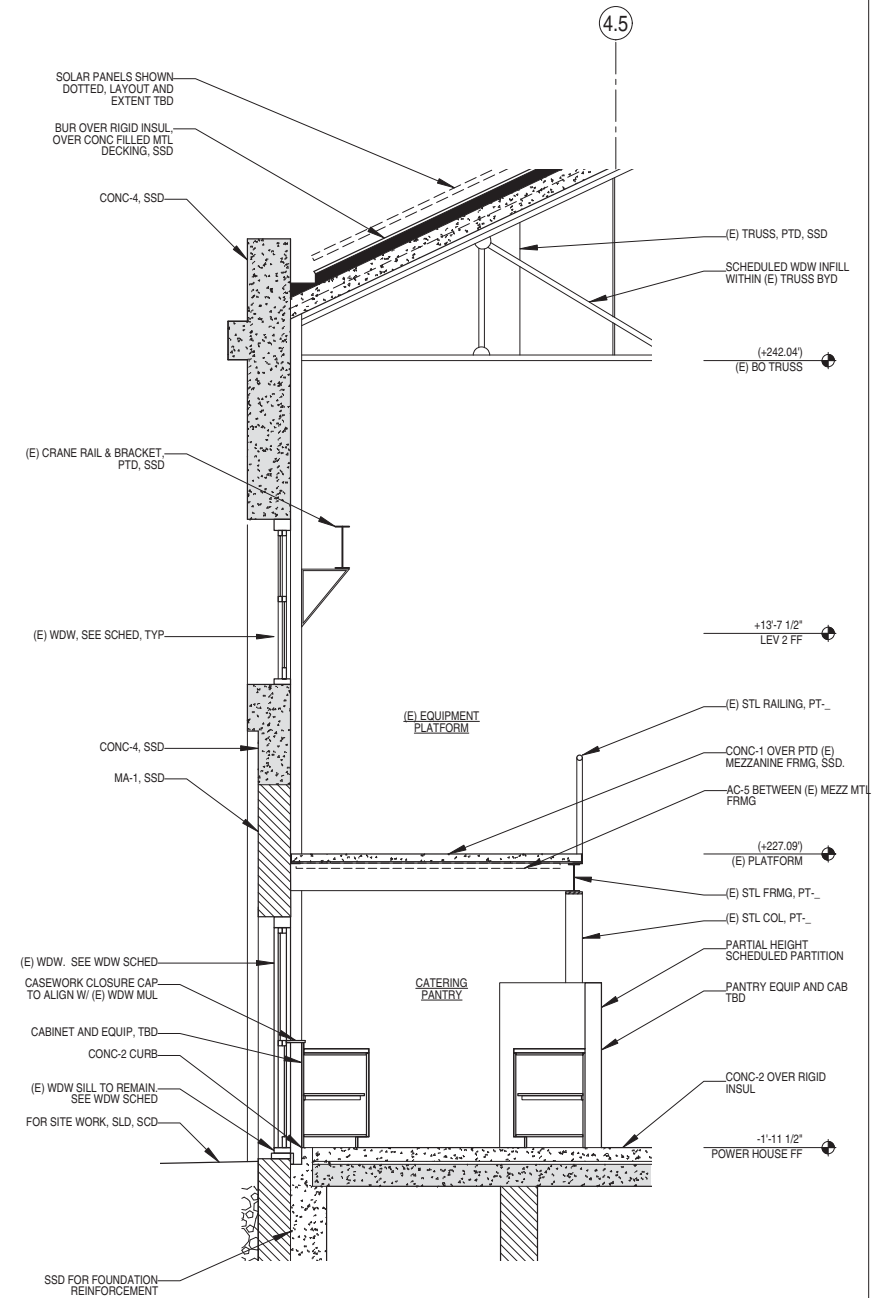
DATE: 08.08.14

JOB: 0906

DRAWN: MH

SCALE: AS NOTED

ISSUE	DATE
C OF A SET	09.11.15



WALL SECTION @ POWERHOUSE PANTRY (LOOKING NORTH)



A6.4A

WALL SECTIONS -
 POWERHOUSE



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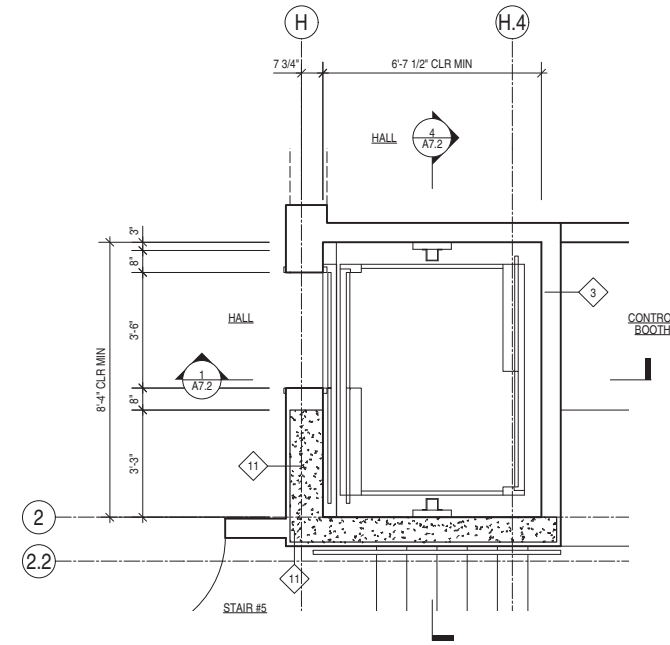
DATE: 08.08.14

JOB: 0906

DRAWN: MH

SCALE: AS NOTED

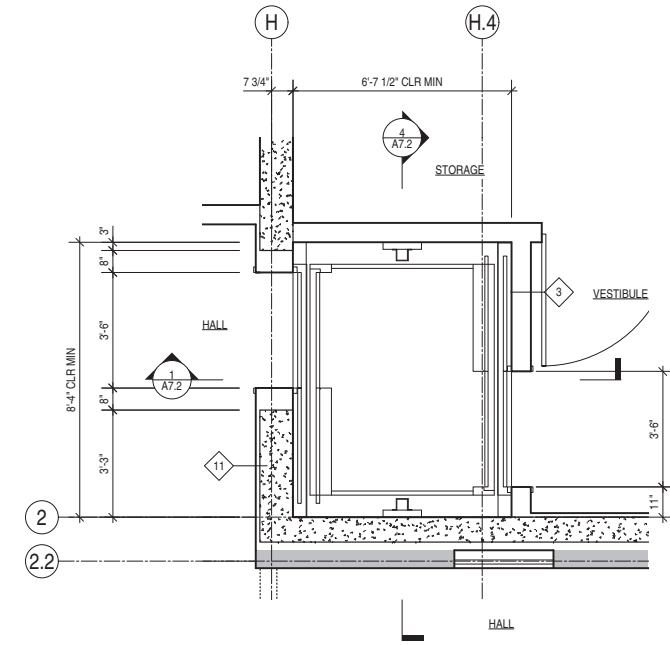
ISSUE	DATE
100% DD SET	06.03.15
C OF A SET	09.11.15



ELEVATOR - LEVEL 3 ENLARGED PLAN



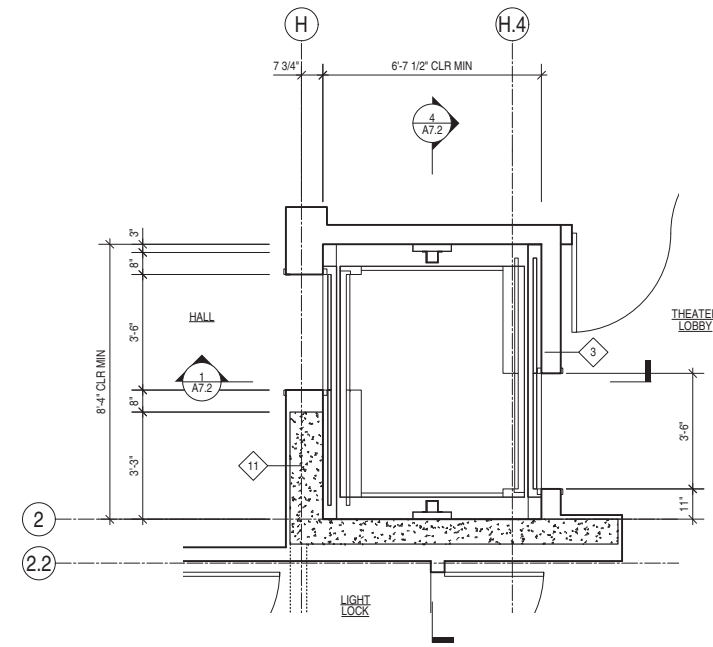
5



ELEVATOR - LEVEL 1 ENLARGED PLAN



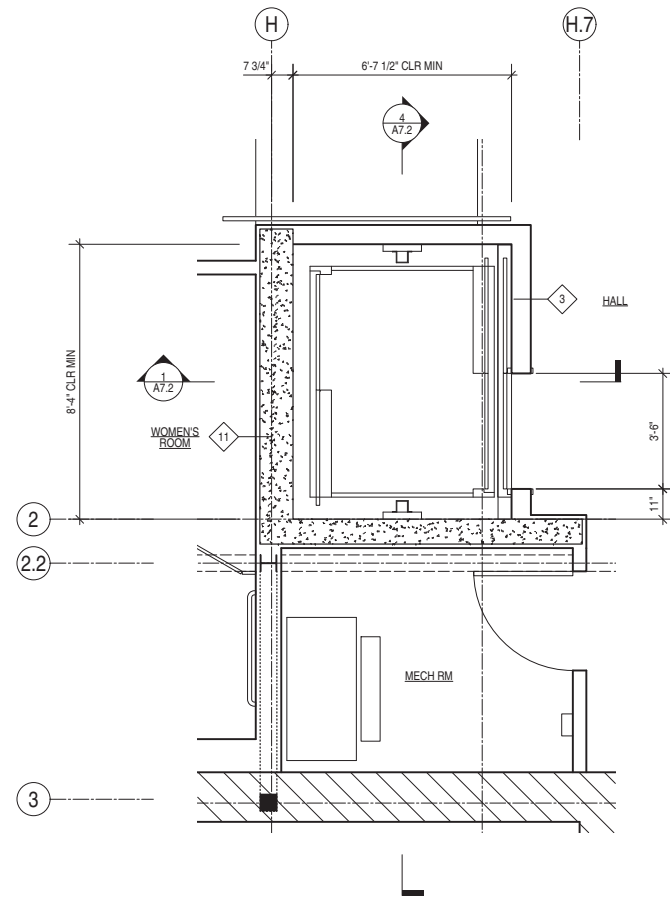
2



ELEVATOR - LEVEL 2 ENLARGED PLAN



4



ELEVATOR - BASEMENT ENLARGED PLAN



1

NOT ISSUED

ELEVATOR - CAB INTERIOR ELEVATIONS



7

A7.1
 ELEVATOR - ENLARGED
 PLANS



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**GENEVA CAR BARN
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DATE: 08.08.14

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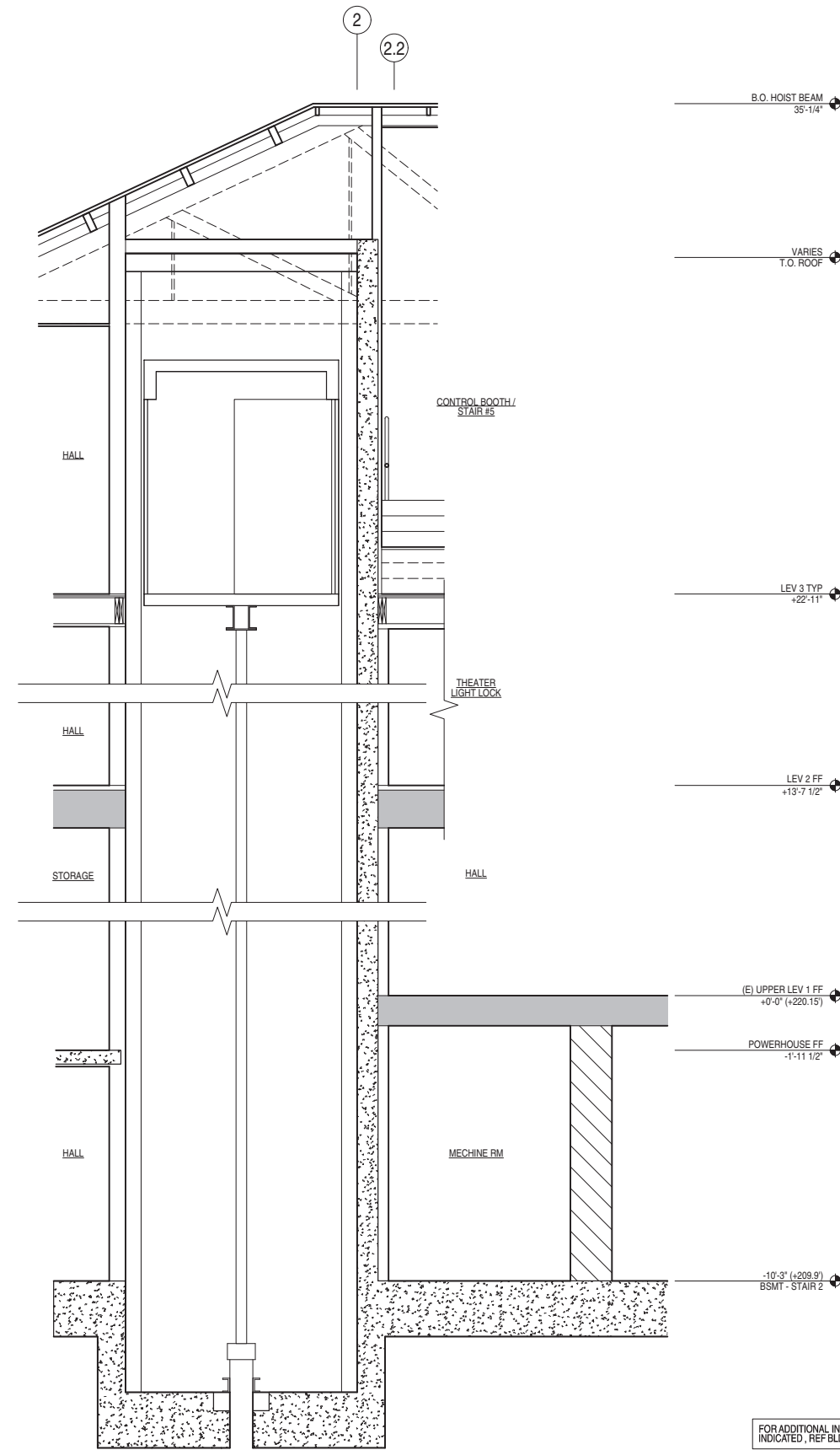
DRAWN: MH

SCALE: AS NOTED

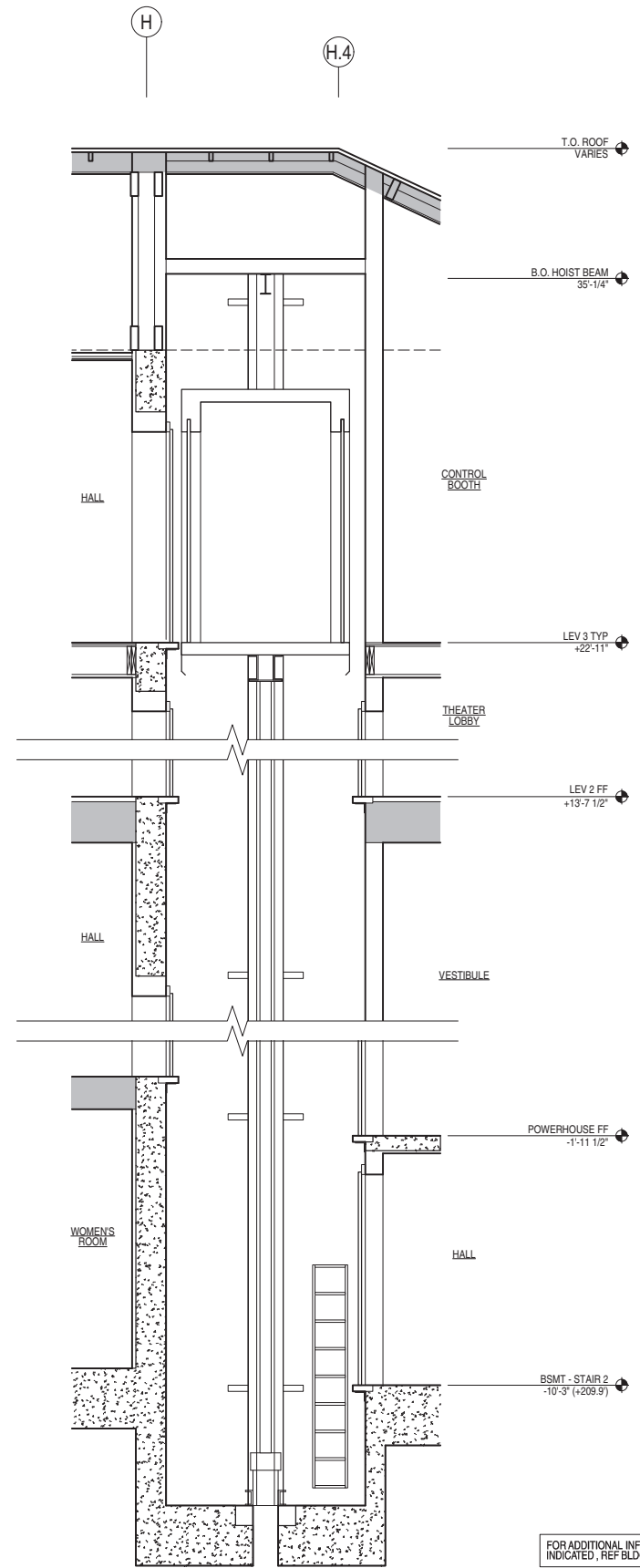
ISSUE	DATE
100% DD SET	06.03.15
C OF A SET	08.11.15

A7.2

ELEVATOR - ENLARGED
 SECTIONS



FOR ADDITIONAL INFO NOT INDICATED, REF BLDG SECTIONS



FOR ADDITIONAL INFO NOT INDICATED, REF BLDG SECTIONS

SECTION DETAIL - ELEVATOR DR SILL @ CONC WALL (11)



NOT ISSUED

NOT ISSUED

SECTION DETAIL - ELEVATOR DR SILL @ STUD WALL (10)

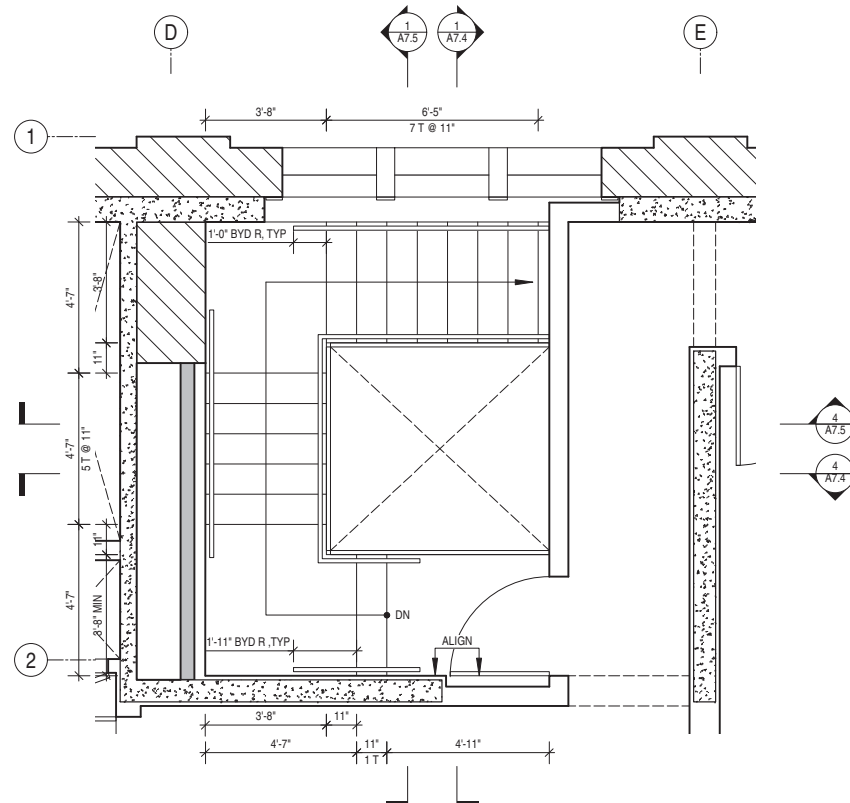


ELEVATOR - EAST/WEST SECTION (4)

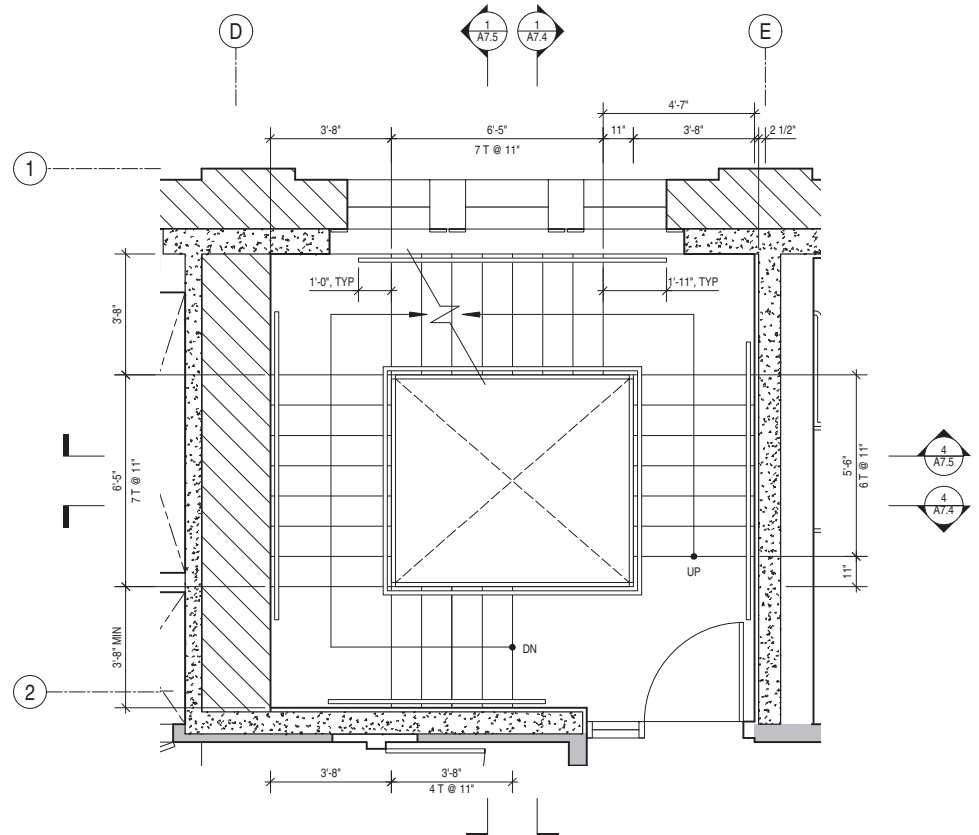


ELEVATOR - NORTH/SOUTH SECTION (1)

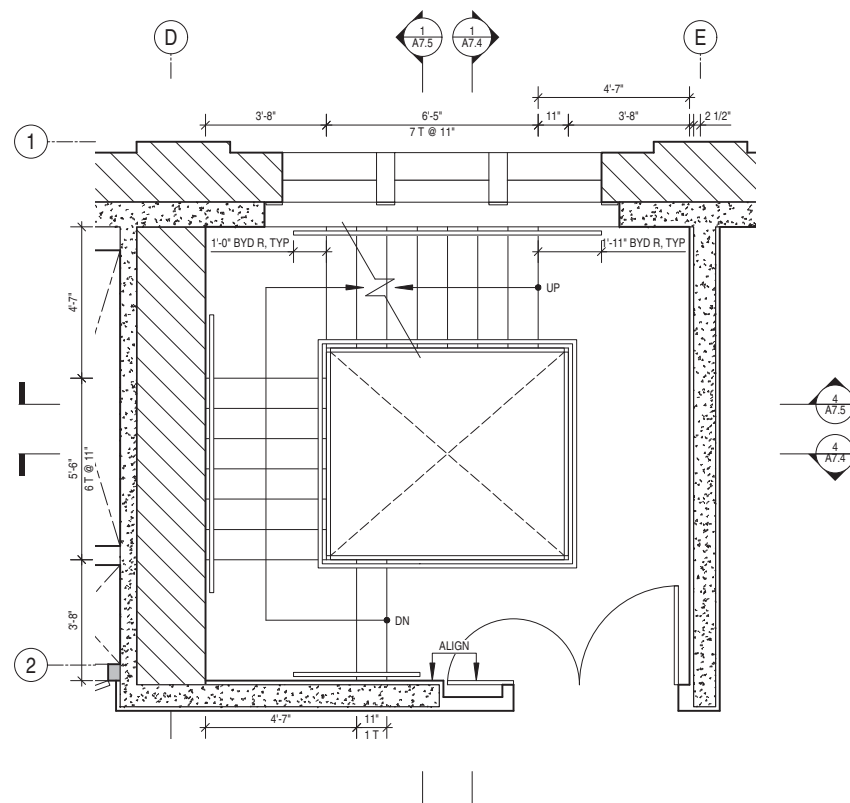




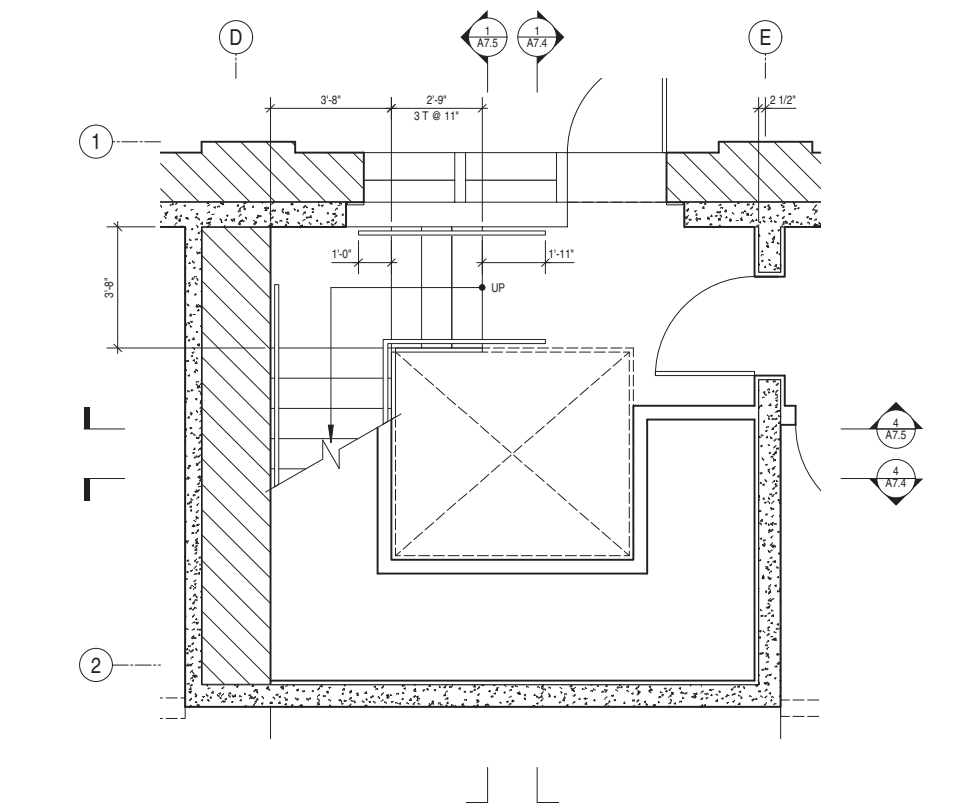
STAIR 1 - LEVEL 3 ENLARGED PLAN



STAIR 1 - LEVEL 1 ENLARGED PLAN



STAIR 1 - LEVEL 2 ENLARGED PLAN



STAIR 1 - BASEMENT LEVEL ENLARGED PLAN



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DATE:	08.08.14
JOB:	0906
DRAWN:	MH
SCALE:	AS NOTED
ISSUE	DATE
100% DD SET	06.03.15
C OF A SET	09.11.15

A7.3

STAIR 1 - ENLARGED
PLANS



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**GENEVA CAR BARN
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 SAN FRANCISCO, CA

DATE: 08.08.14

JOB: 0906

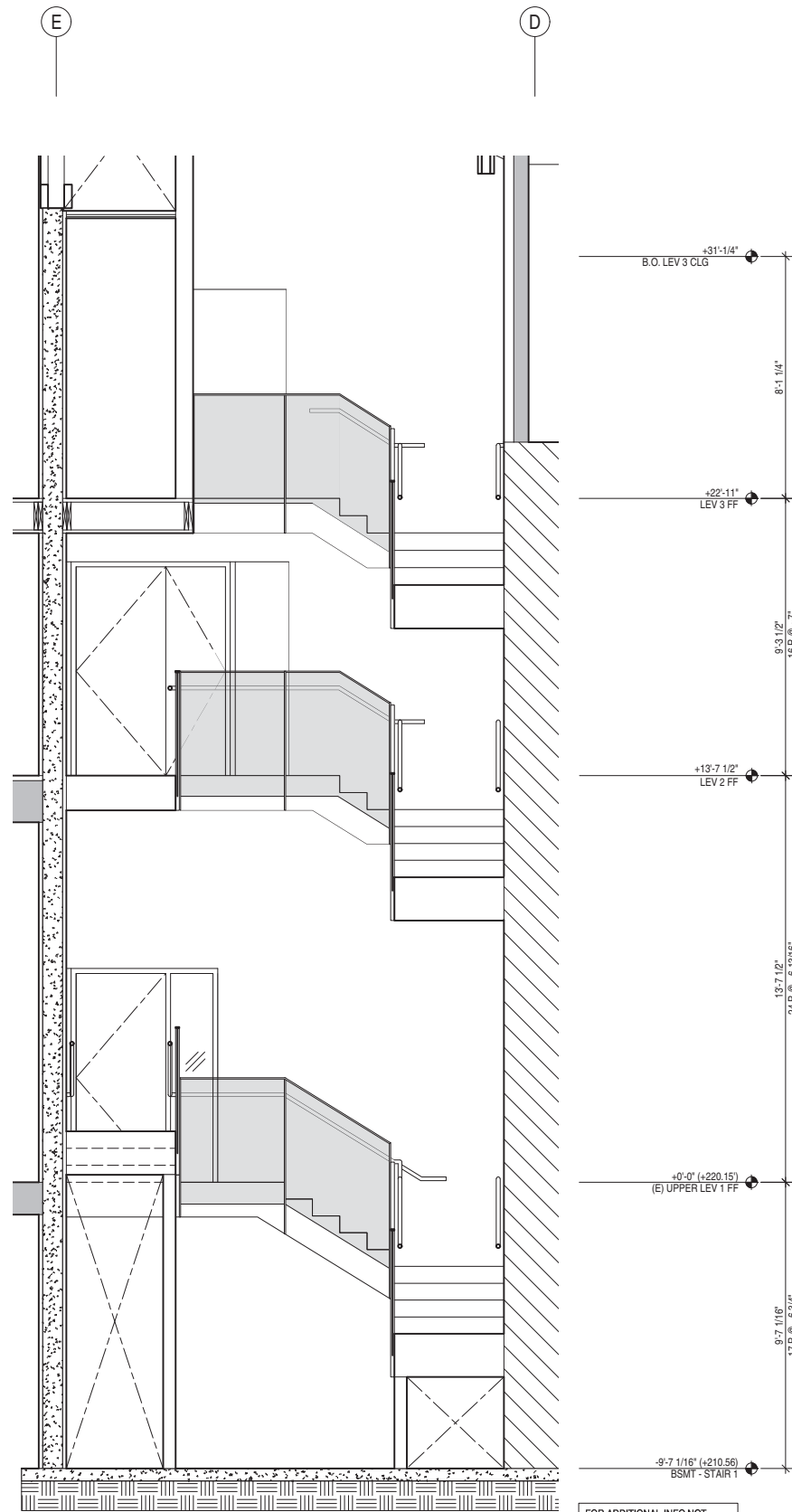
DRAWN: MH

SCALE: AS NOTED

ISSUE	DATE
100% DD SET	06.03.15
C OF A SET	09.11.15

A7.4

**STAIR 1 - ENLARGED
 SECTIONS**

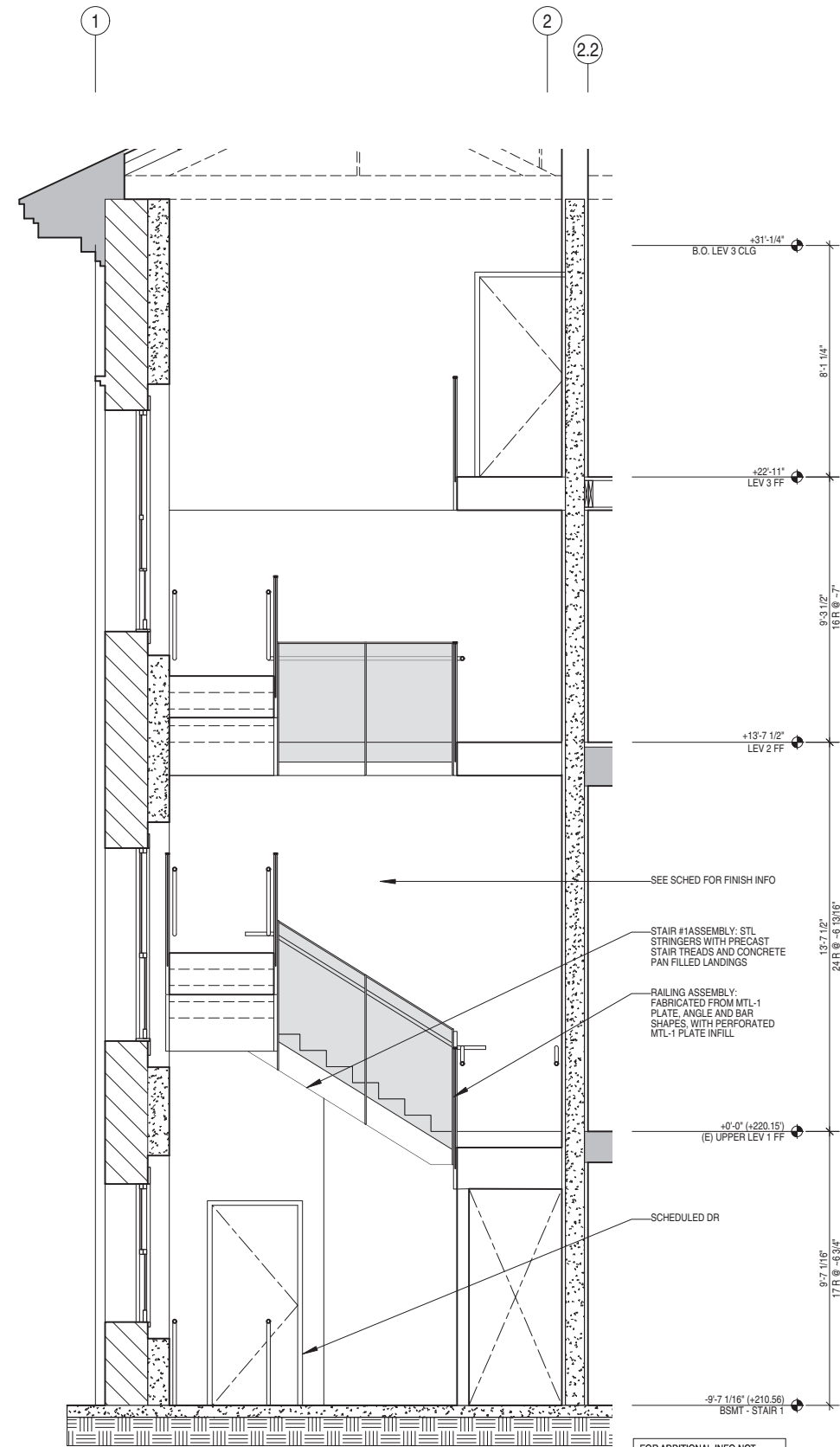


STAIR 1 - WEST SECTION/ELEVATION

0 3' 8' 3/8"=1'-0"

FOR ADDITIONAL INFO NOT
 INDICATED, REF BLDG SECTIONS

4



STAIR 1 - SOUTH SECTION/ELEVATION

0 3' 8' 3/8"=1'-0"

FOR ADDITIONAL INFO NOT
 INDICATED, REF BLDG SECTIONS

1



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**GENEVA CAR BARN
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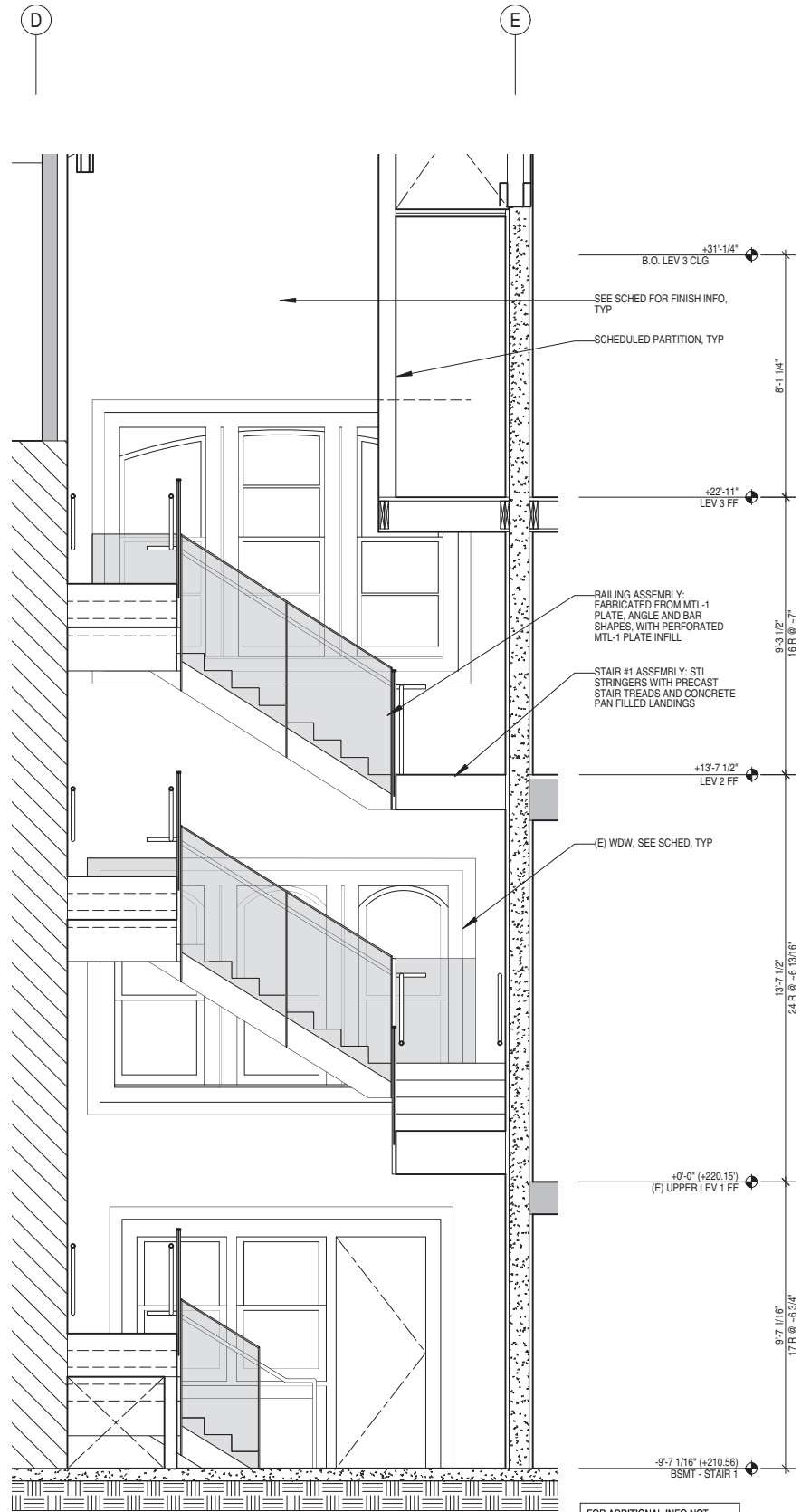
DATE: 08.08.14

JOB: 0906

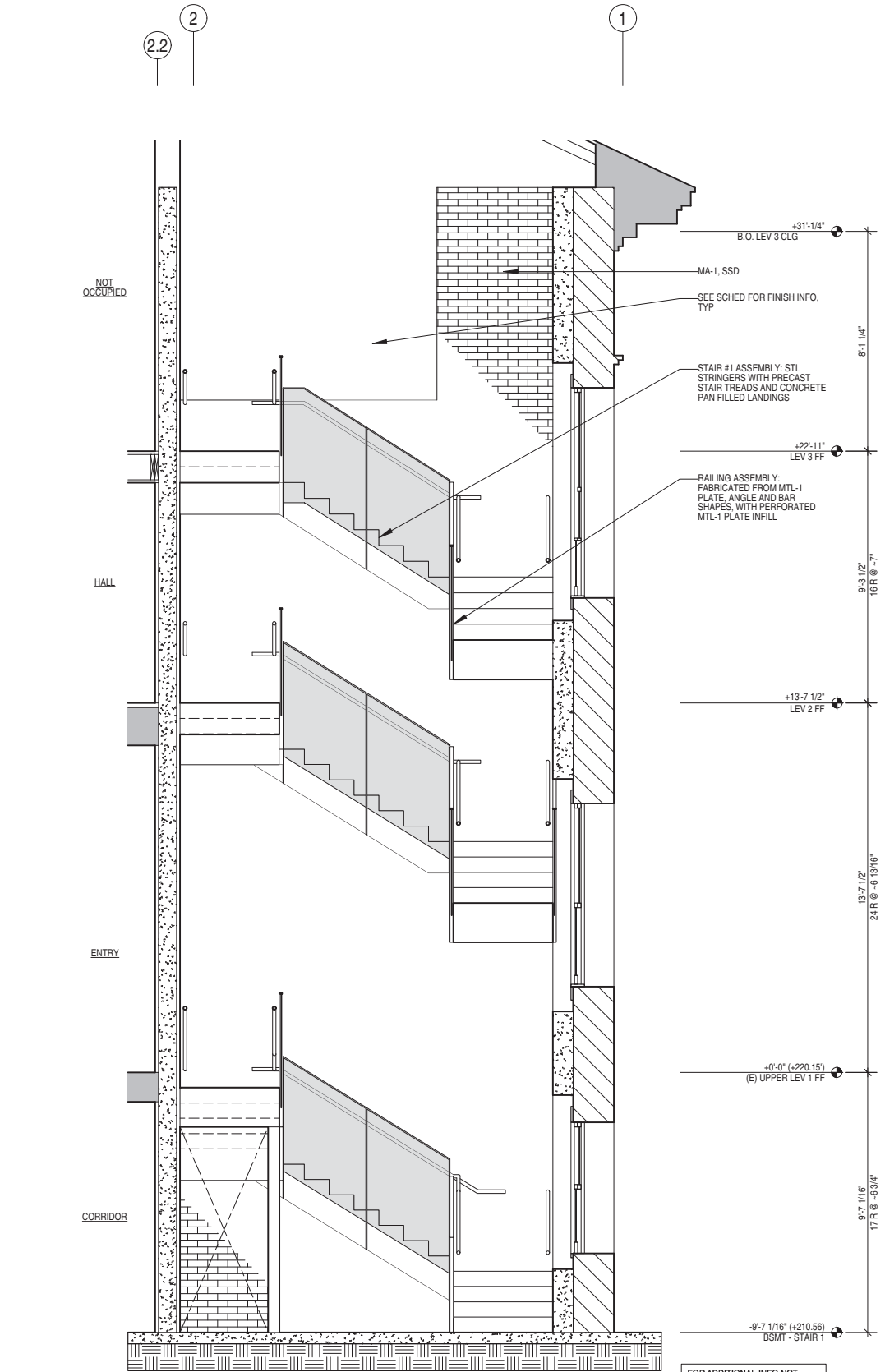
DRAWN: MH

SCALE: AS NOTED

ISSUE	DATE
100% DD SET	06.03.15
C OF A SET	09.11.15



STAIR 1 - EAST SECTION/ELEVATION
 0 3' 8' 3/8"=1'-0"



STAIR 1 - NORTH SECTION/ELEVATION
 0 3' 8' 3/8"=1'-0"

A7.5
 STAIR 1 - ENLARGED SECTIONS



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GENEVA CAR BARN AND POWERHOUSE

2301 SAN JOSE AVENUE
 SAN FRANCISCO, CA

DATE: 08.08.14

JOB: 0906

DRAWN: MH

SCALE: AS NOTED

ISSUE DATE

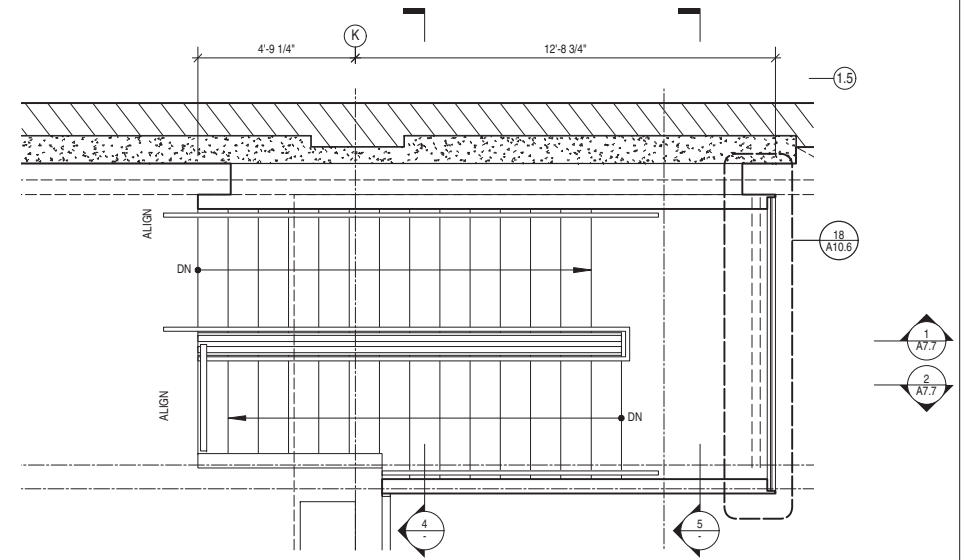
50% DD SET 03.09.15

100% DD SET 06.03.15

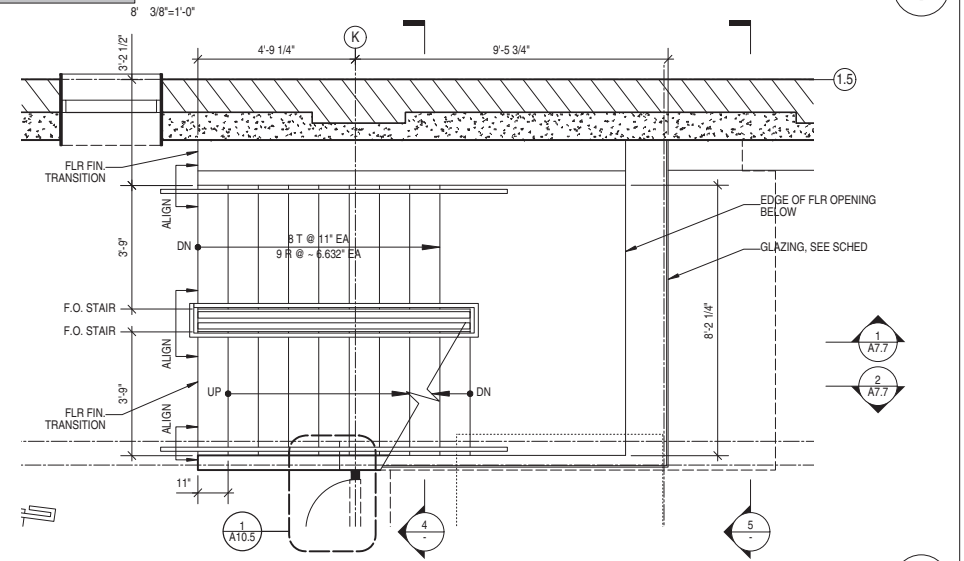
C OF A SET 09.11.15

A7.6

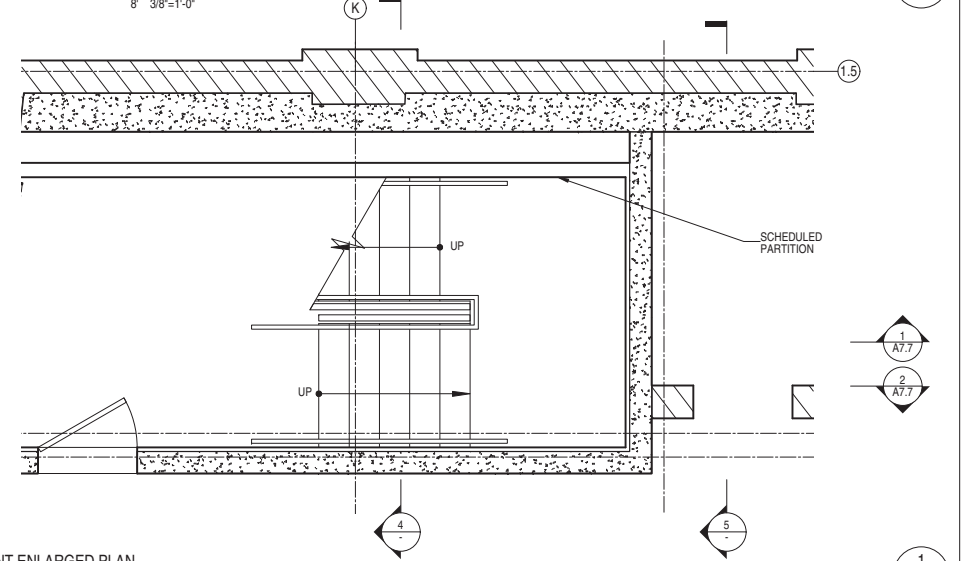
STAIR 2 - ENLARGED PLANS & SECTIONS



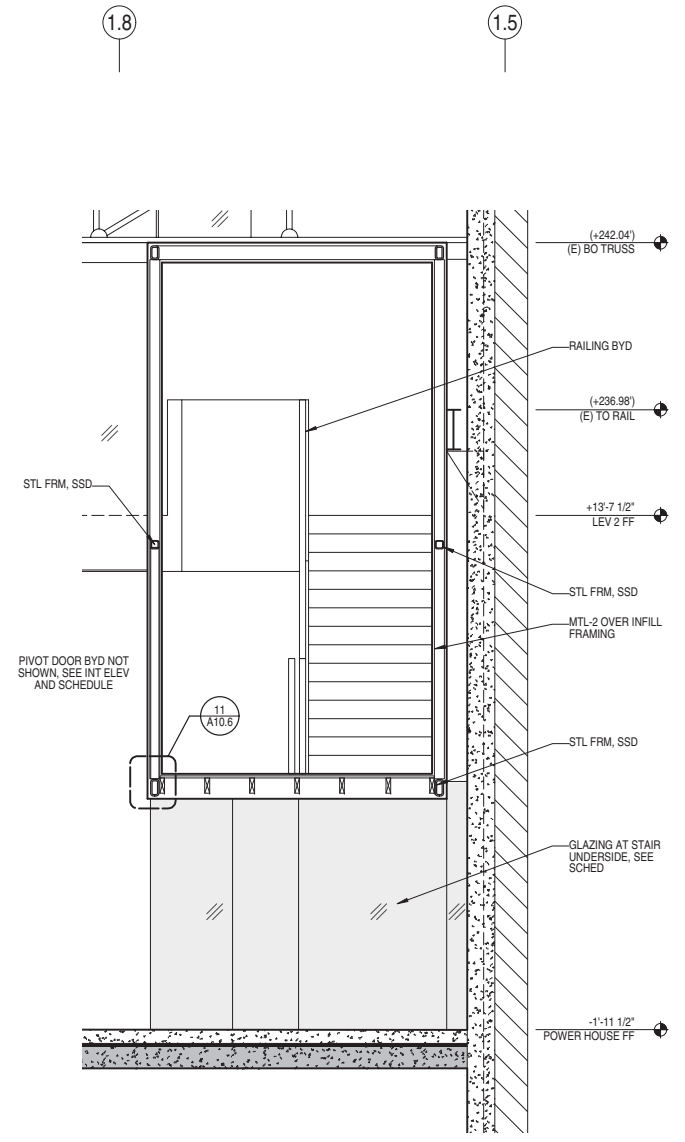
STAIR 2 - LEVEL 2 ENLARGED PLAN



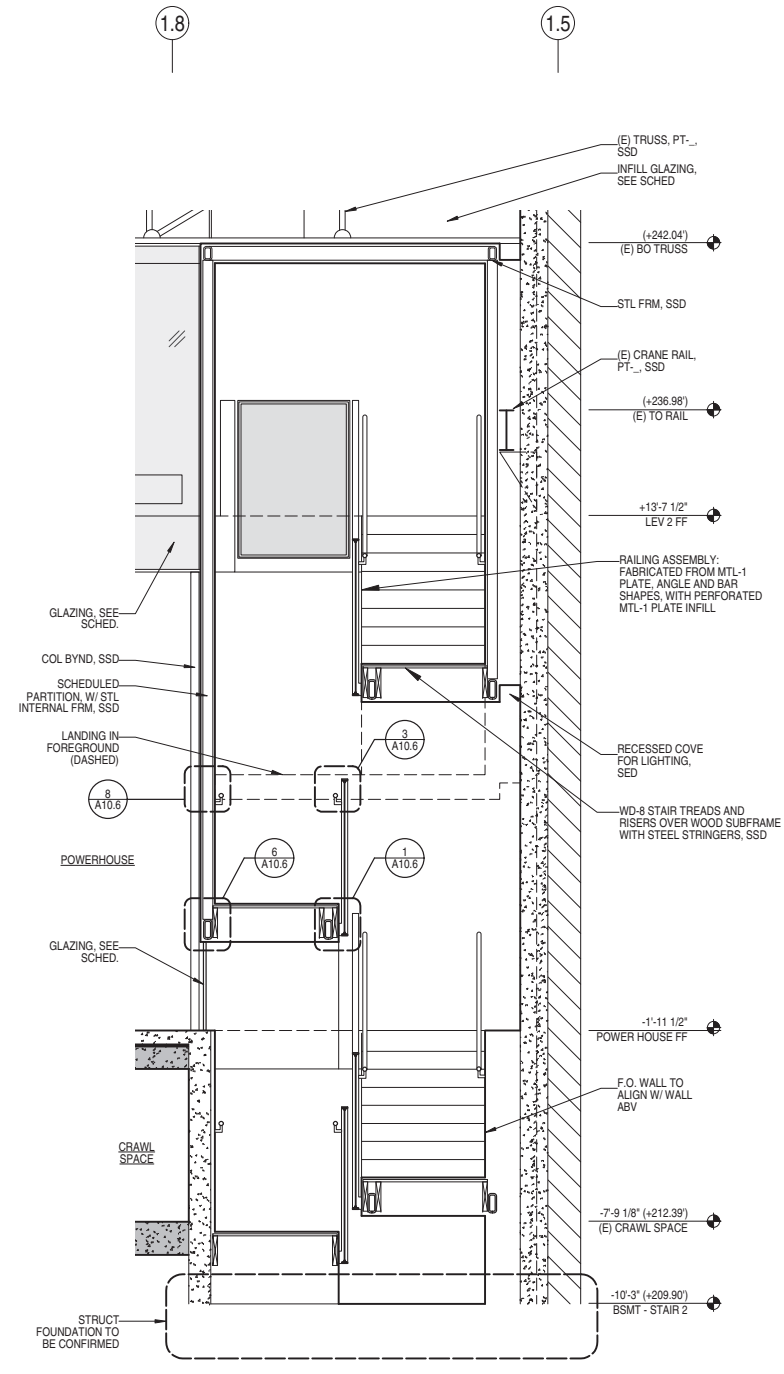
STAIR 2 - LEVEL 1 ENLARGED PLAN



STAIR 2 - BASEMENT ENLARGED PLAN



STAIR 2 - E/W SECTION THROUGH LANDING (LOOKING NORTH)



STAIR 2 - E/W SECTION THROUGH STAIR (LOOKING NORTH)

FOR ADDITIONAL INFO NOT INDICATED, REF DET 4-

5

4

1

2

3



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GENEVA CAR BARN AND POWERHOUSE

2301 SAN JOSE AVENUE
 SAN FRANCISCO, CA

DATE: 08.08.14

JOB: 0906

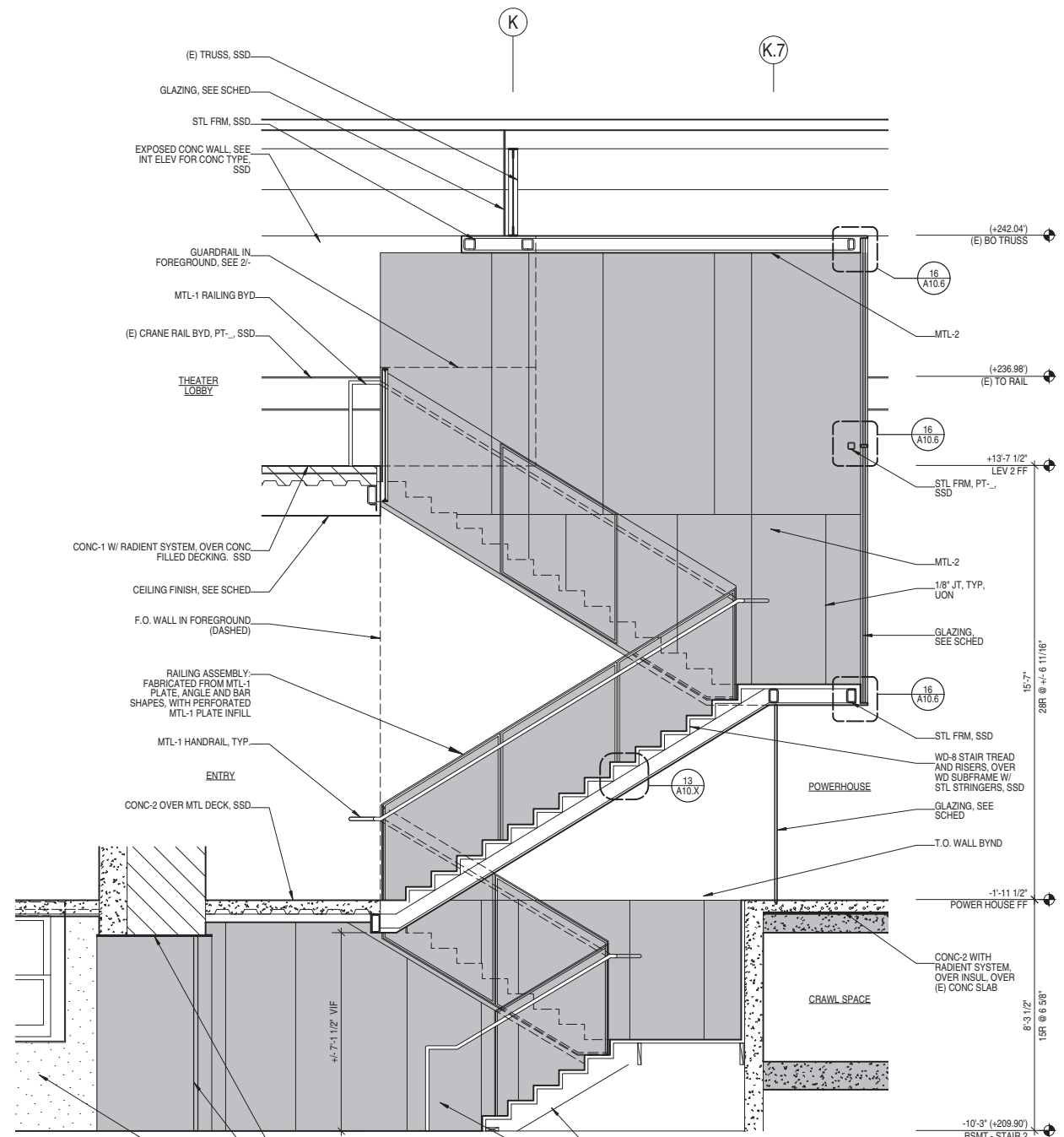
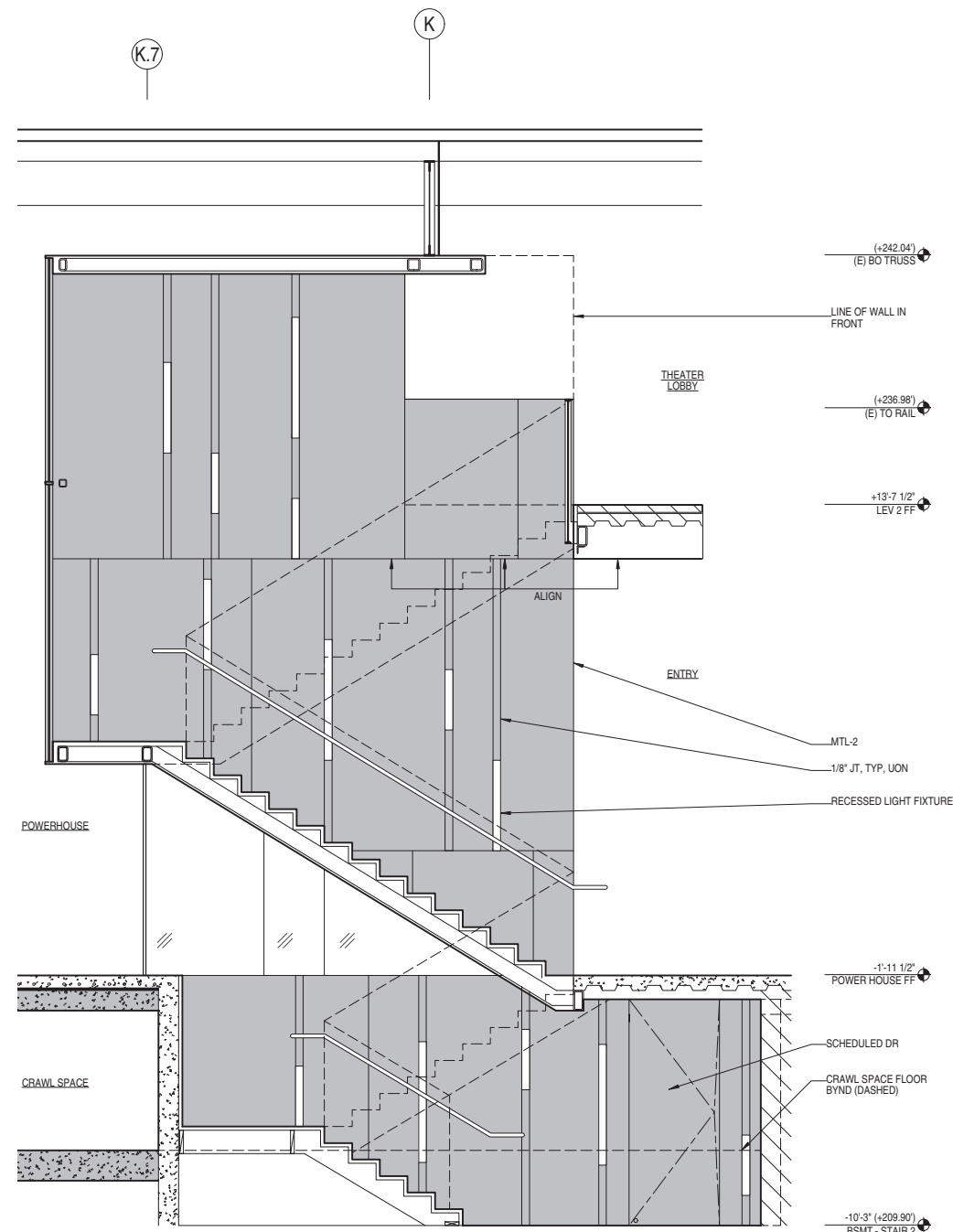
DRAWN: MH

SCALE: AS NOTED

ISSUE	DATE
50% DD SET	03.09.15
100% DD SET	06.03.15
C OF A SET	09.11.15

A7.7

STAIR 2 - ENLARGED SECTIONS



FOR ADDITIONAL INFO NOT INDICATED, REF DET 1/1-

STAIR 2 - N/S SECTION (LOOKING WEST)
 0 3' 8' 3/8"-1'-0"

2 STAIR 2 - N/S SECTION (LOOKING EAST)
 0 3' 8' 3/8"-1'-0"

1



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GENEVA CAR BARN
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DATE: 08.08.14

JOB: 0906

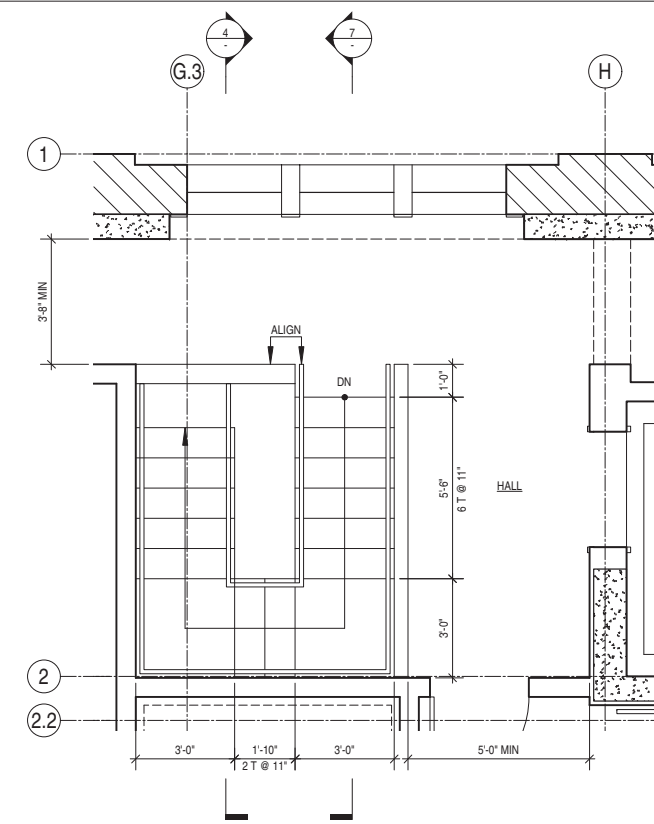
DRAWN: MH

SCALE: AS NOTED

ISSUE	DATE
100% DD SET	06.03.15
C OF A SET	09.11.15

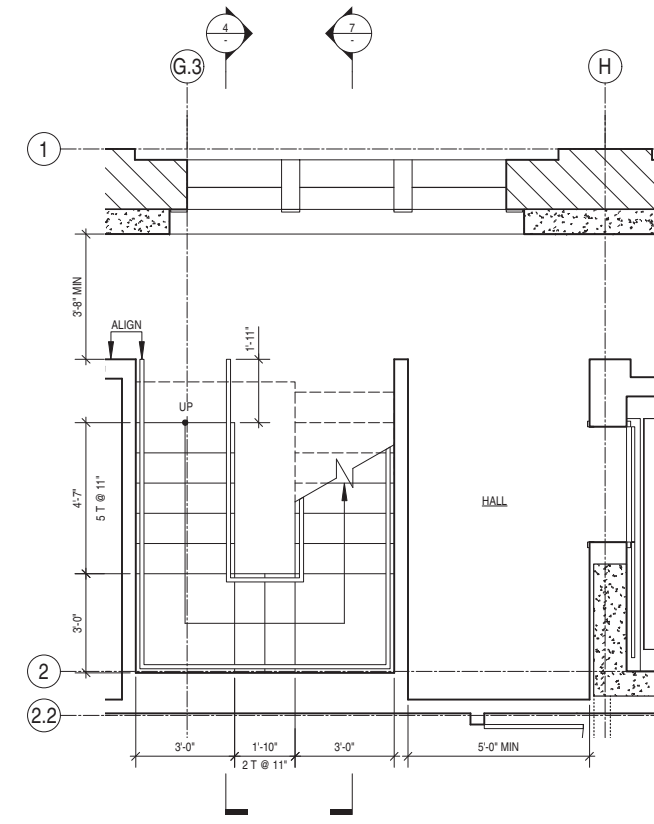
A7.8

STAIR 3 - ENLARGED
 PLANS & SECTIONS



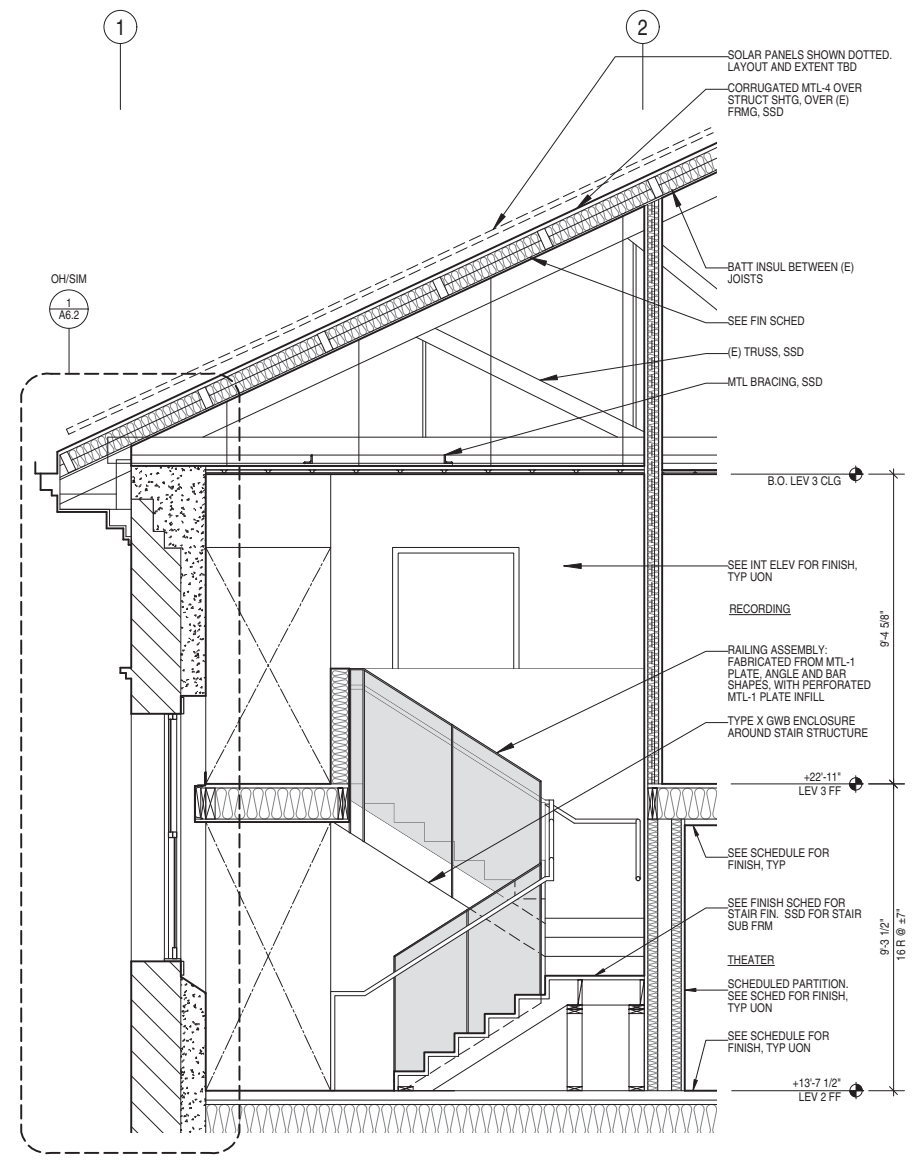
STAIR 3 - LEVEL 3 ENLARGED PLAN

0 3 8' 3/8"=1'-0"



STAIR 3 - LEVEL 2 ENLARGED PLAN

0 3 8' 3/8"=1'-0"



STAIR 3 - ENLARGED SECTION

0 3 8' 3/8"=1'-0"

FOR ADDITIONAL INFO NOT INDICATED, SEE BLDG SECTIONS

STAIR 3 - ENLARGED SECTION

0 3 8' 3/8"=1'-0"

NOT ISSUED



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**GENEVA CAR BARN
 AND POWERHOUSE**

2301 SAN JOSE AVENUE
 SAN FRANCISCO, CA

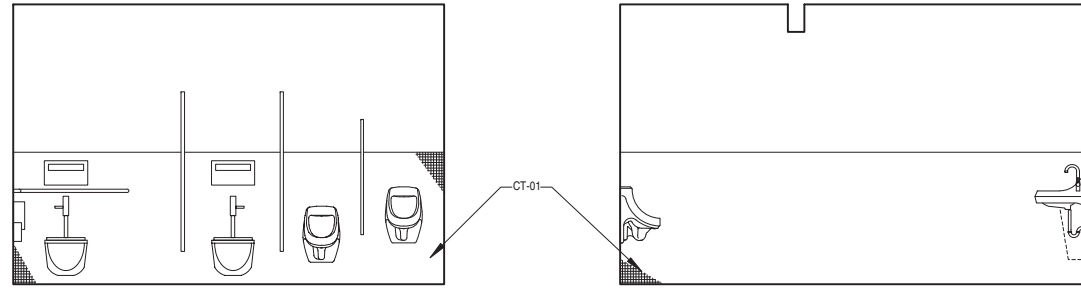
DATE: 02.03.15

JOB: 0906

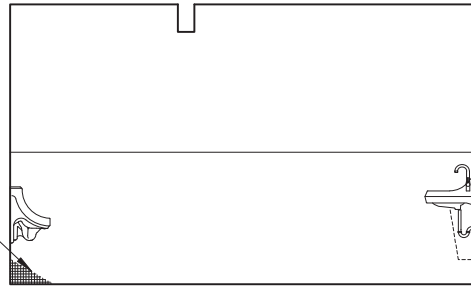
DRAWN: MH

SCALE: AS NOTED

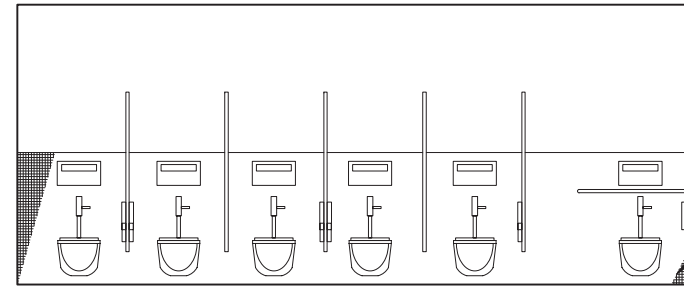
ISSUE	DATE
100% DD SET	06.03.15
C OF A SET	09.11.15



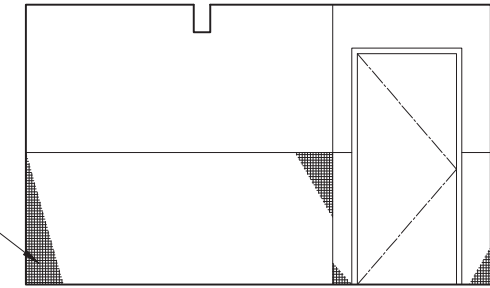
C-WEST



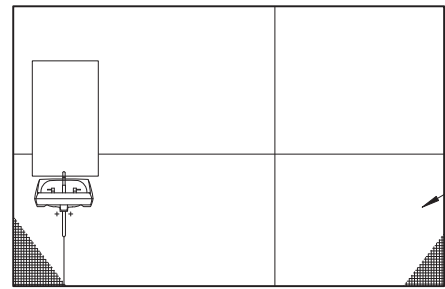
D-NORTH



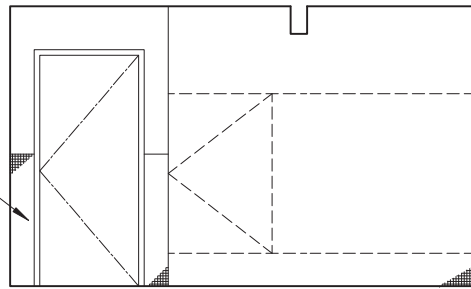
C-WEST



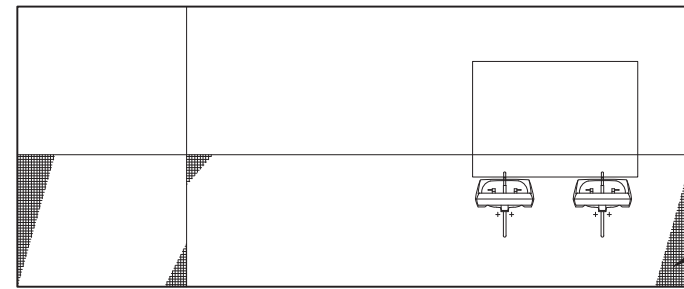
D-NORTH



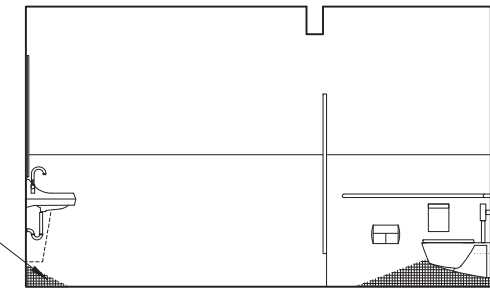
A-EAST



B-SOUTH



A-EAST



B-SOUTH

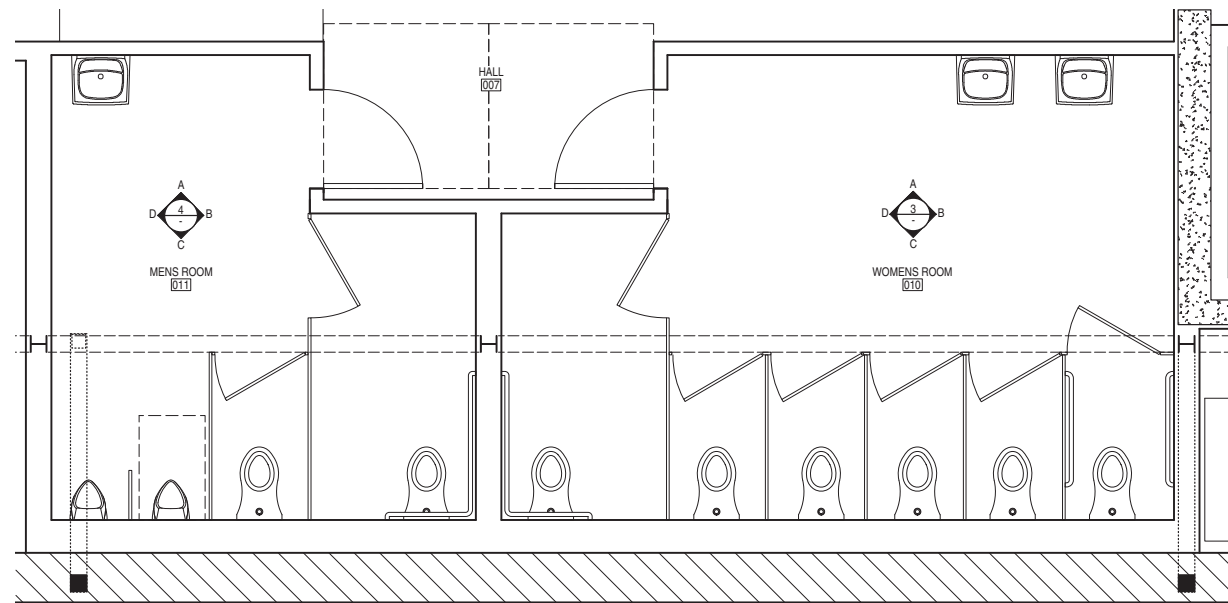
INTERIOR ELEVATIONS - MEN'S RESTROOM (011)



4 INTERIOR ELEVATIONS - WOMEN'S RESTROOM (010)



3



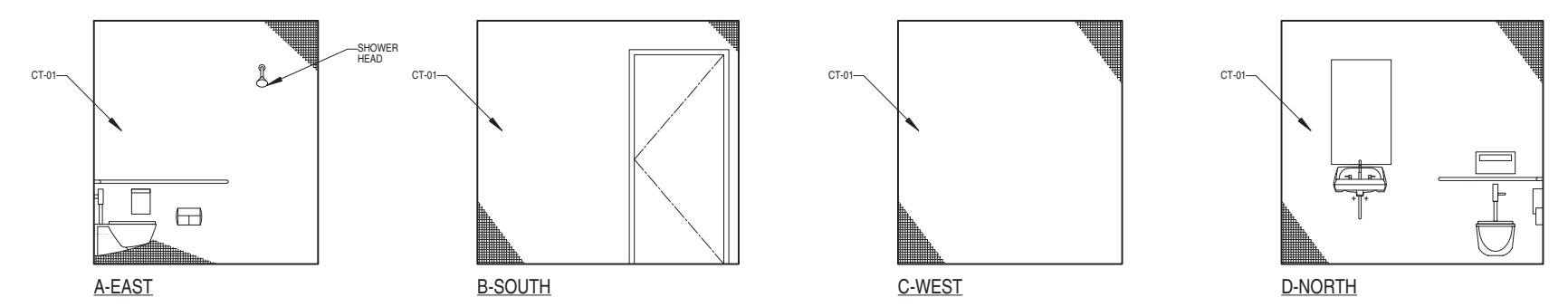
ENLARGED PLAN - BASEMENT WOMEN'S (010) & MEN'S (011) RESTROOMS



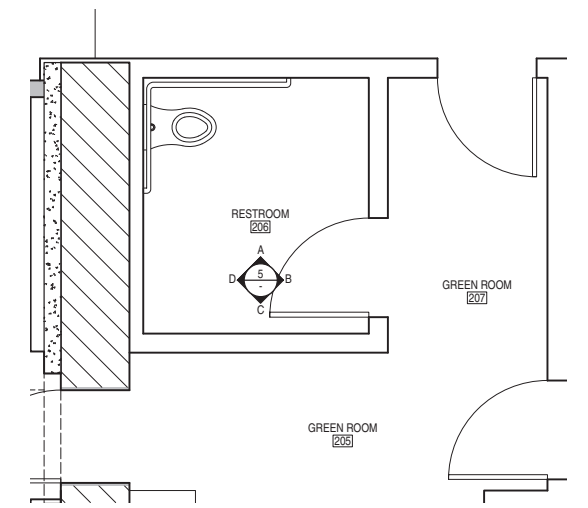
1

A8.01

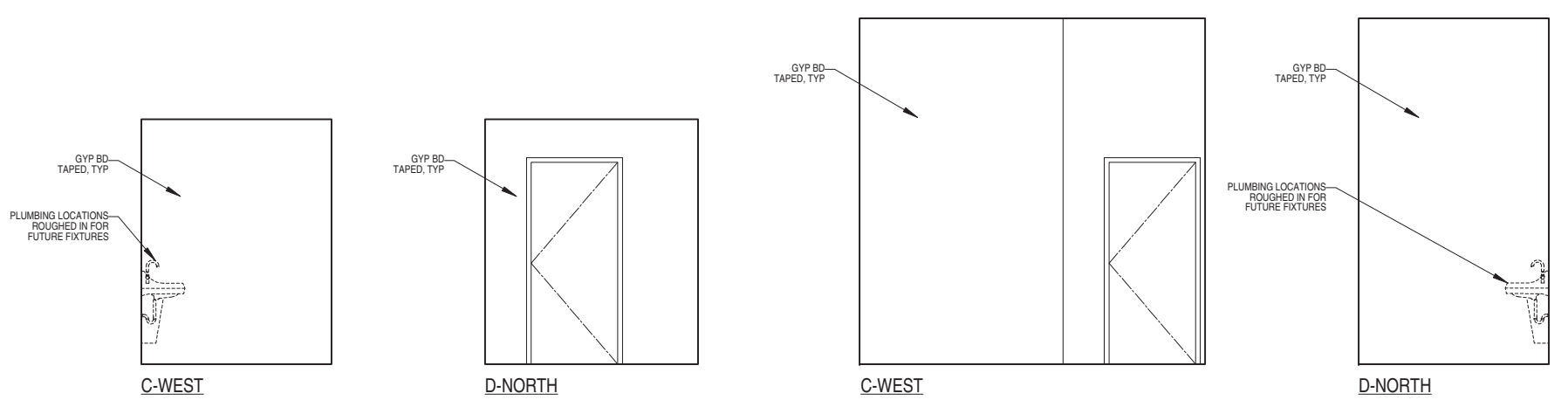
INTERIOR ELEVATIONS



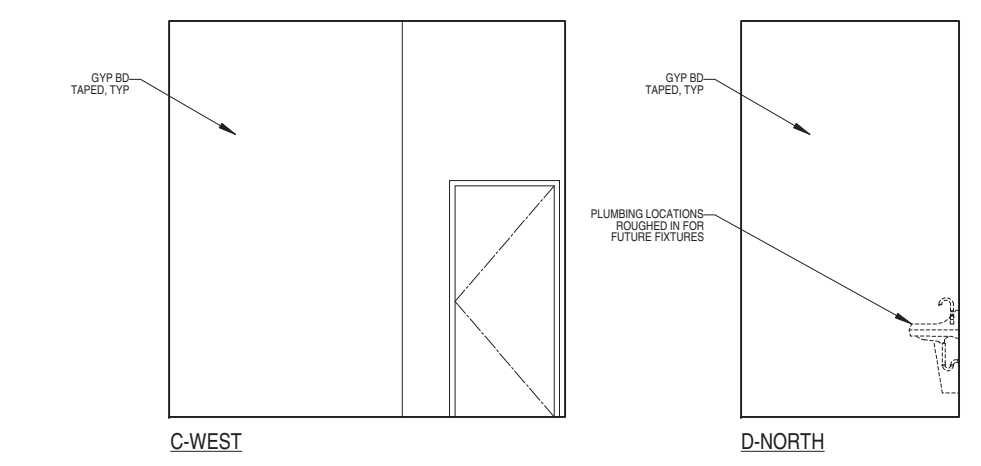
INTERIOR ELEVATIONS - RESTROOM (206)



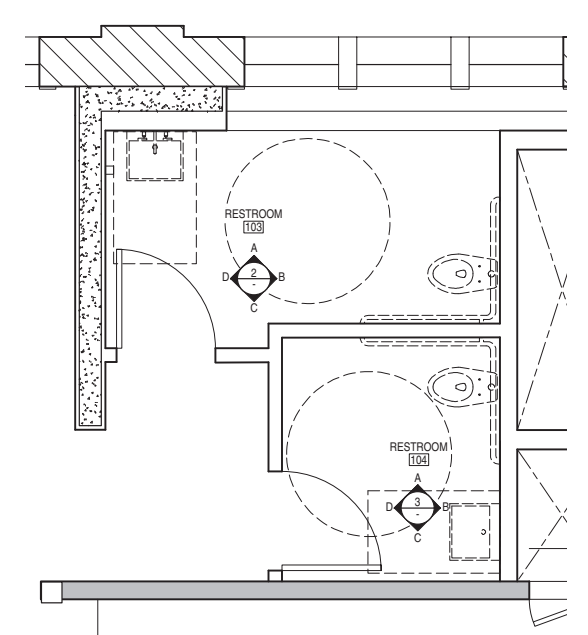
5 ENLARGED PLAN - RESTROOM (206)



INTERIOR ELEVATIONS - RESTROOM (104)



3 INTERIOR ELEVATIONS - RESTROOM (103)



2 ENLARGED PLAN - RESTROOMS (103 & 104)



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GENEVA CAR BARN AND POWERHOUSE

2301 SAN JOSE AVENUE
 SAN FRANCISCO, CA

DATE:	02.03.15
JOB:	0906
DRAWN:	MH
SCALE:	AS NOTED
ISSUE	DATE
100% DD SET	06.03.15
C OF A SET	09.11.15

A8.02
 INTERIOR ELEVATIONS



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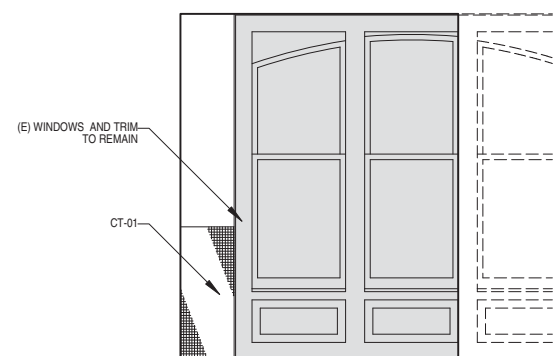
DATE: 02.03.15

JOB: 0906

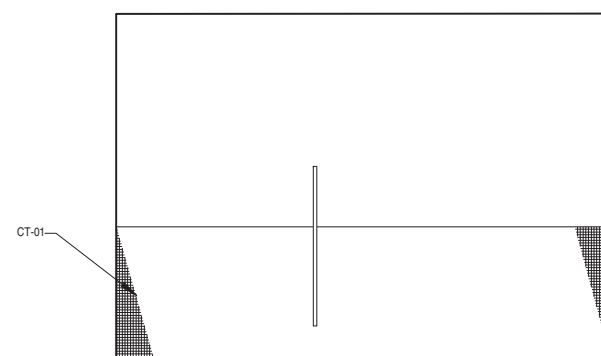
DRAWN: MH

SCALE: AS NOTED

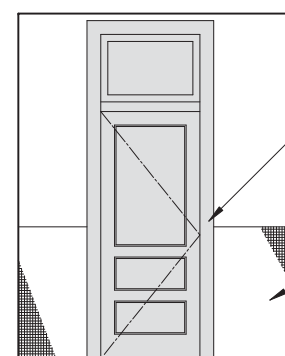
ISSUE	DATE
100% DD SET	06.03.15
C OF A SET	09.11.15



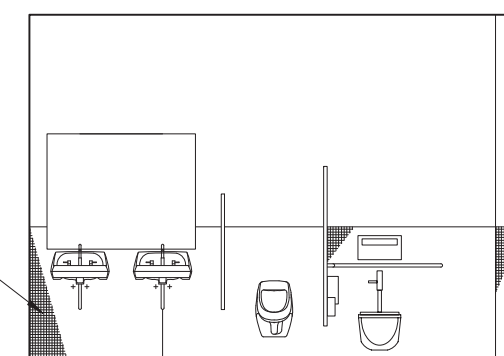
A-EAST



B-SOUTH



C-WEST

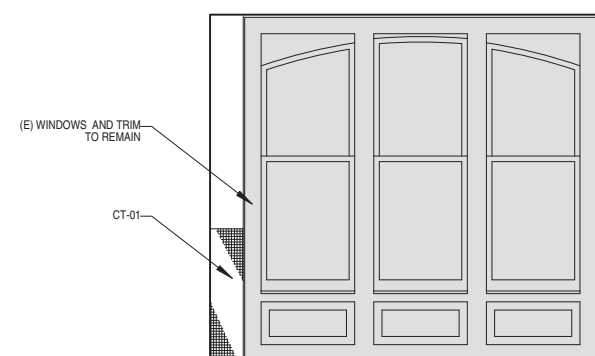


D-NORTH

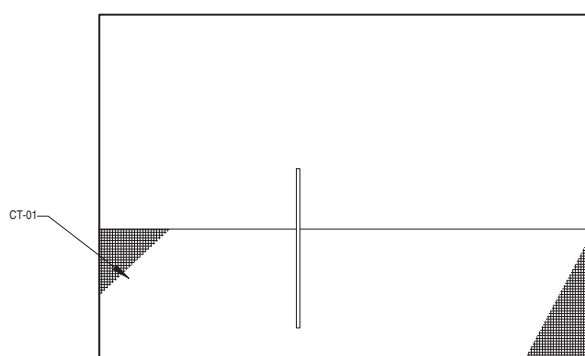
ENLARGED PLAN - MEN'S RESTROOM (113)



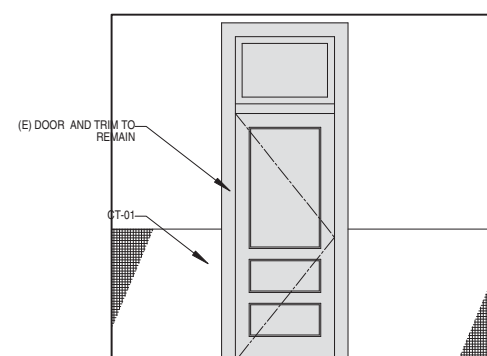
5



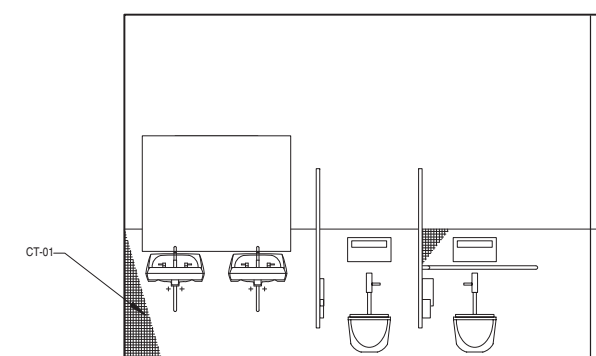
A-EAST



B-SOUTH



C-WEST

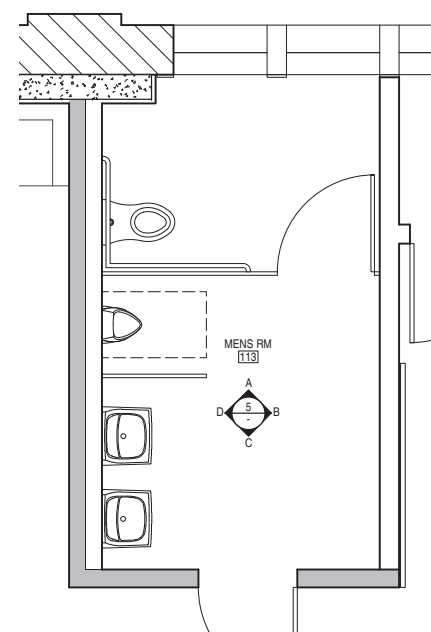


D-NORTH

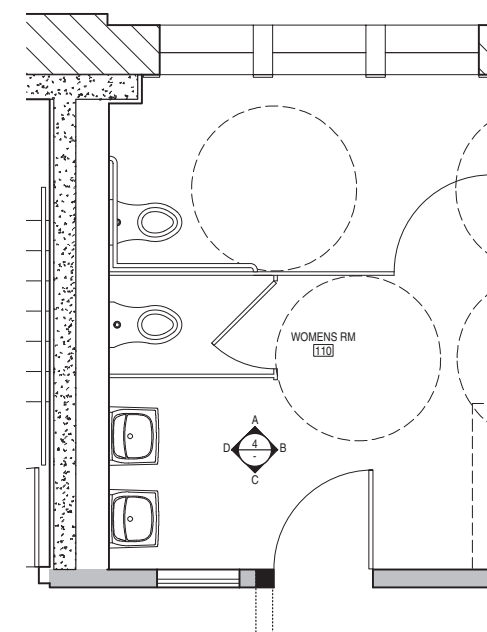
INTERIOR ELEVATIONS - WOMENS RESTROOM (110)



4



ENLARGED PLAN - MEN'S RESTROOM (113)



ENLARGED PLAN - WOMEN'S RESTROOM (110)



1

A8.03

INTERIOR ELEVATIONS



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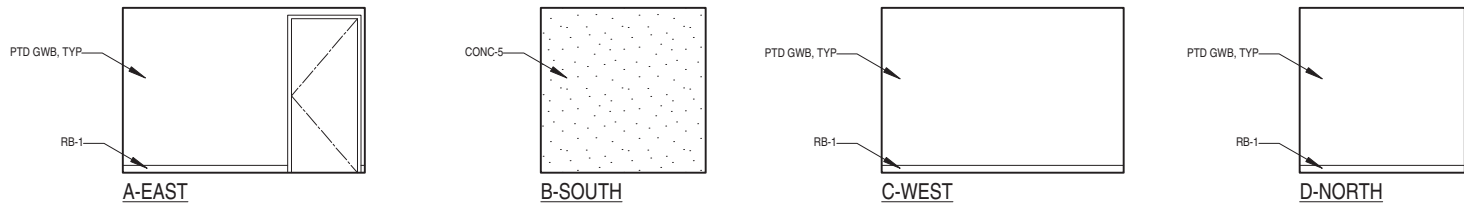
DATE: 02.03.15

JOB: 0906

DRAWN: MH

SCALE: AS NOTED

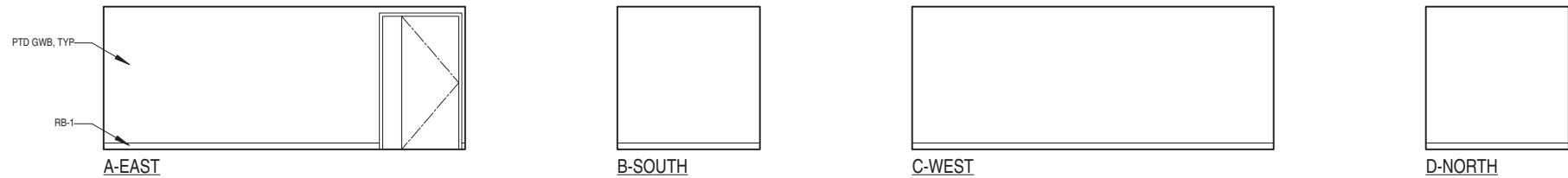
ISSUE	DATE
100% DD SET	06.03.15
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INTERIOR ELEVATIONS - JANITOR (004)



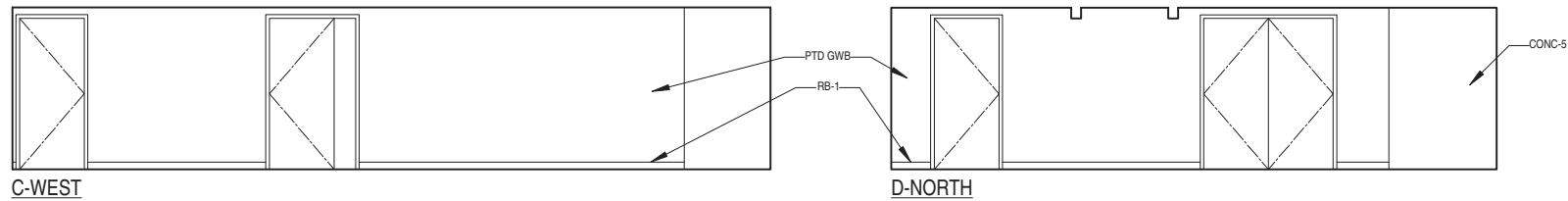
6
-



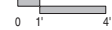
INTERIOR ELEVATIONS - STORAGE (003)



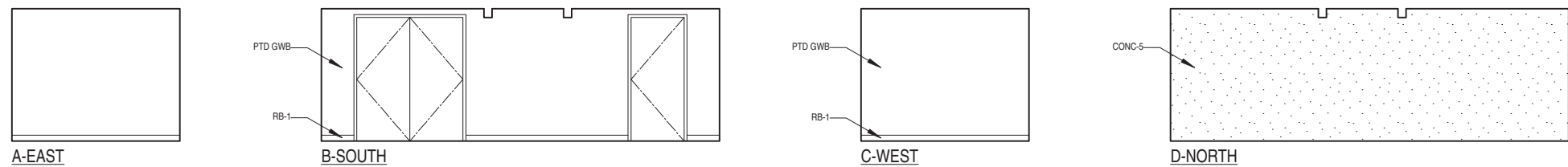
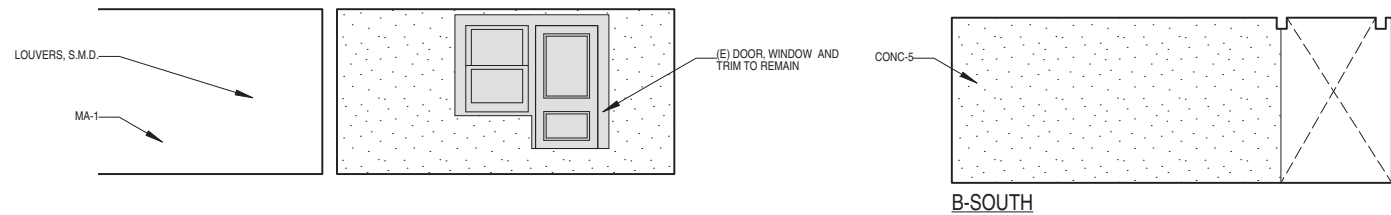
4
-



INTERIOR ELEVATIONS - MECHANICAL (002)



2
-



INTERIOR ELEVATIONS - ELECTRICAL (001)



1
-

A8.04
 INTERIOR ELEVATIONS



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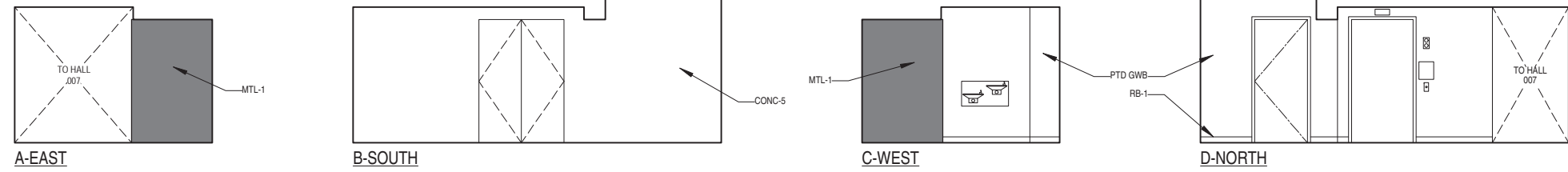
DATE: 02.03.15

JOB: 0906

DRAWN: MH

SCALE: AS NOTED

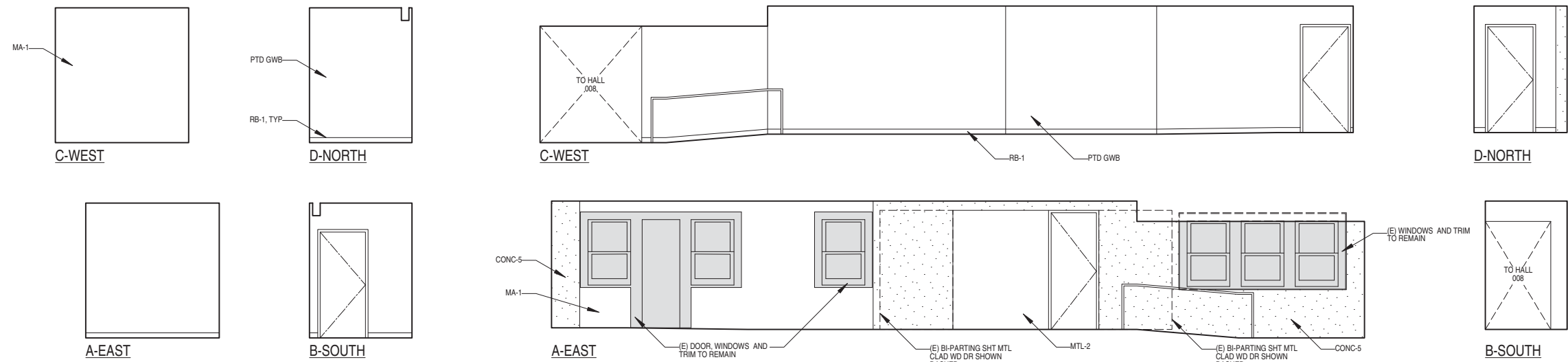
ISSUE	DATE
100% DD SET	06.03.15
C OF A SET	09.11.15



INTERIOR ELEVATIONS - HALL (008)



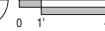
4



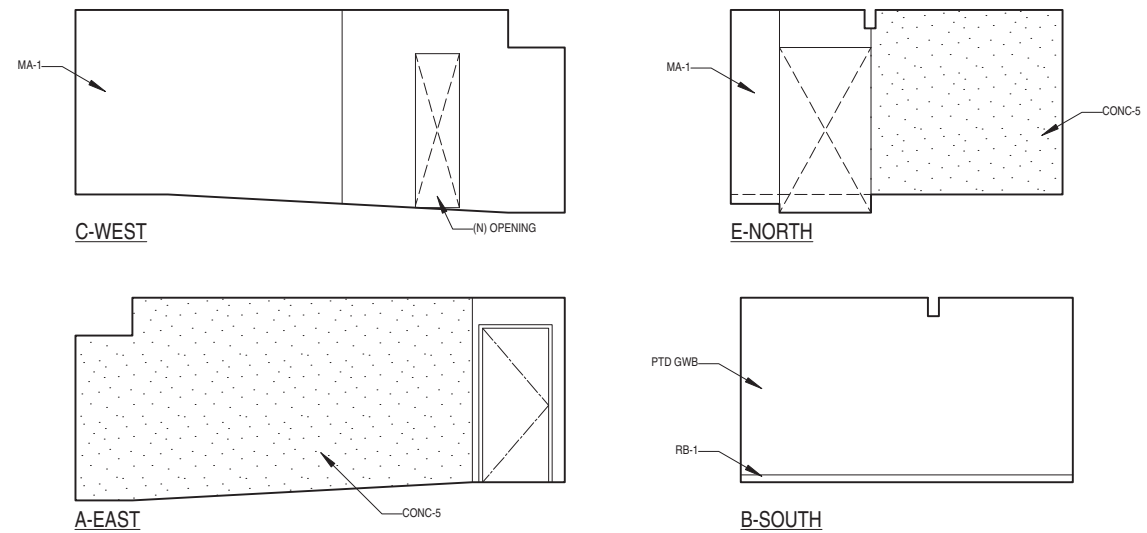
INTERIOR ELEVATIONS - MACHINE RM (009)



INTERIOR ELEVATIONS - HALL (007)



2



INTERIOR ELEVATIONS - HALL (005)



1

A8.05

INTERIOR ELEVATIONS



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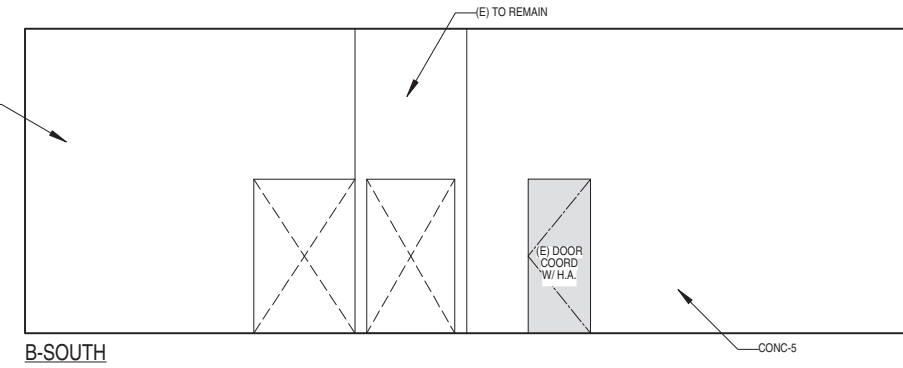
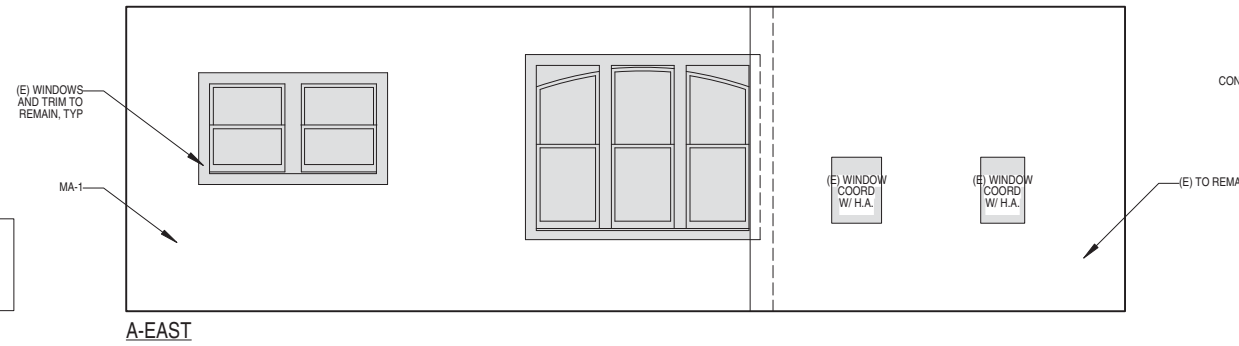
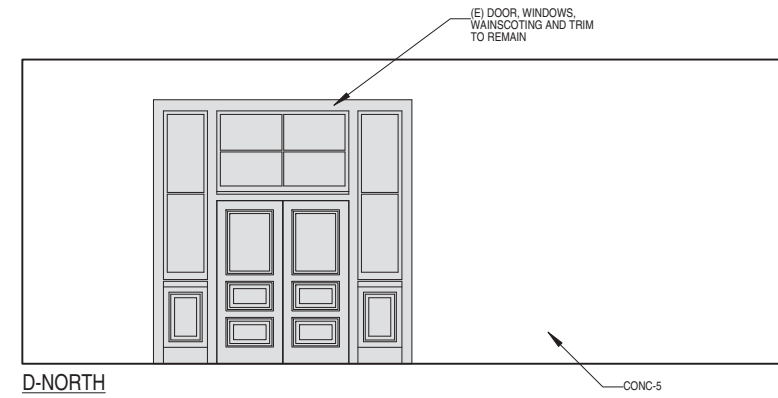
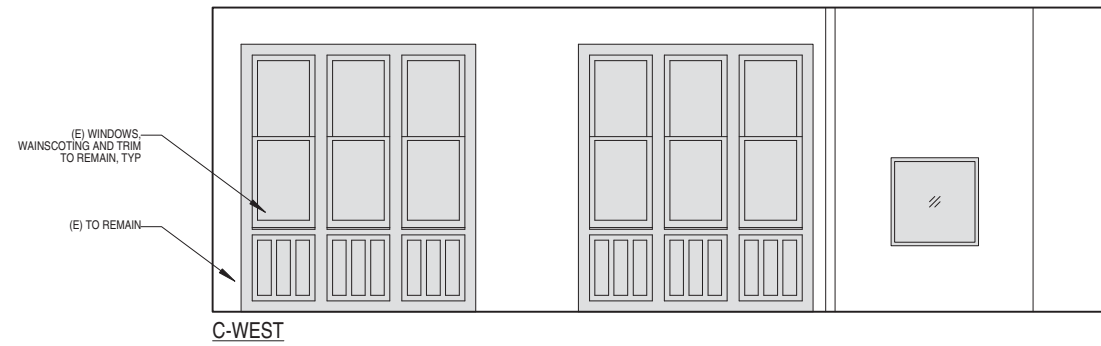
DATE: 02.03.15

JOB: 0906

DRAWN: MH

SCALE: AS NOTED

ISSUE	DATE
100% DD SET	06.03.15
C OF A SET	09.11.15

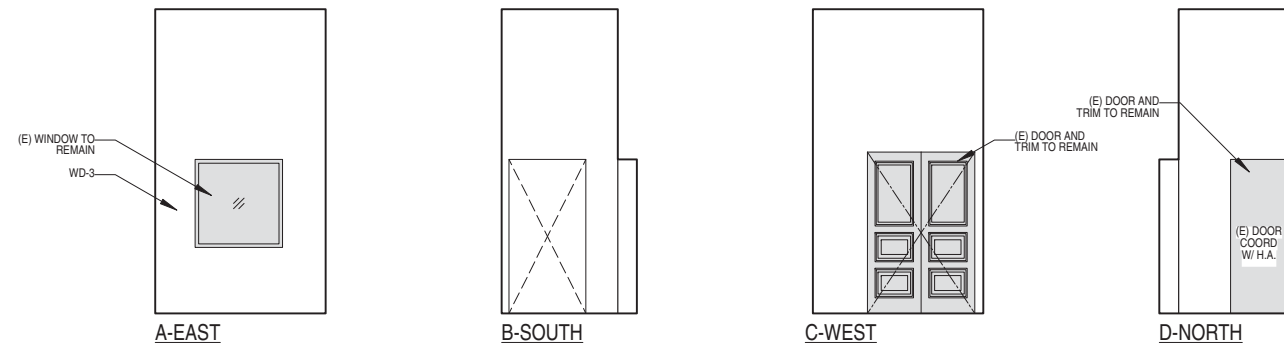


NOTE: WINDOWS & DOORS WIN RESTAURANT TO BE REFURBISHED. INTERIOR FINISHES TO REMAIN FOR FUTURE TENANT IMPROVEMENT

INTERIOR ELEVATIONS - RESTAURANT (102)



3



INTERIOR ELEVATIONS - VESTIBULE (101)



1

A8.06

INTERIOR ELEVATIONS

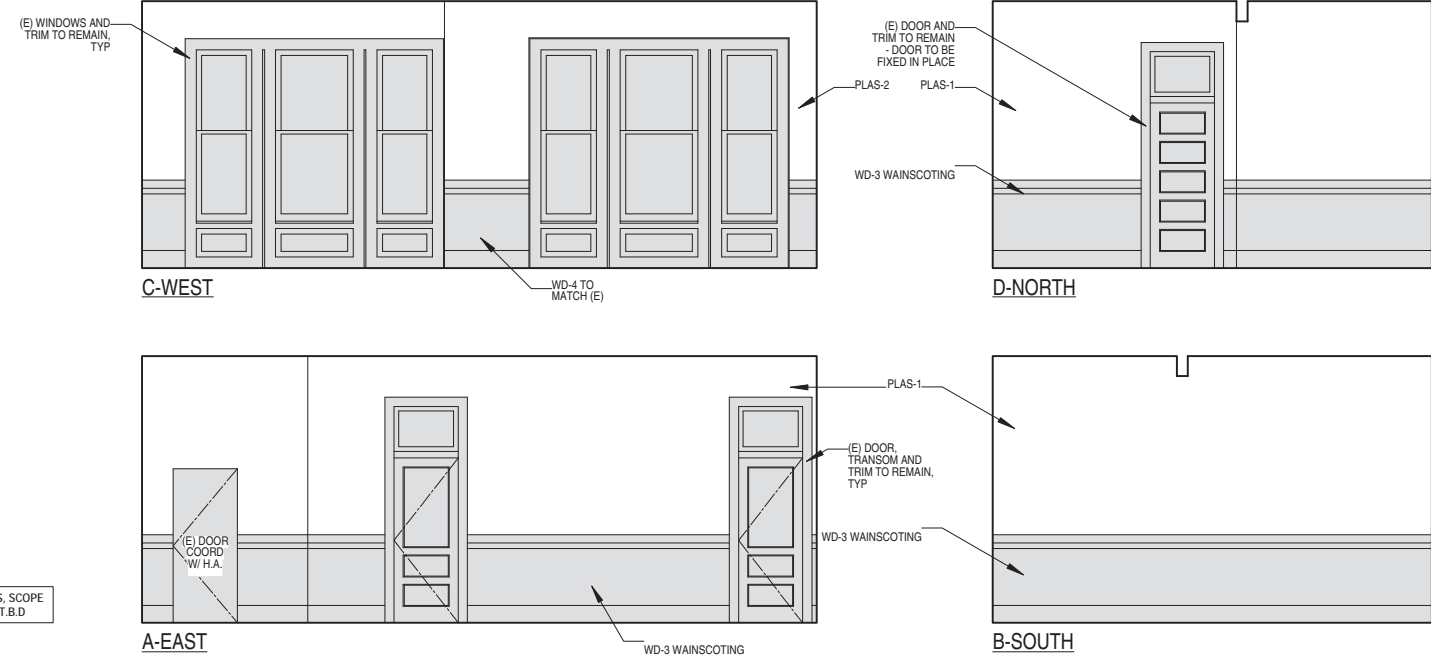


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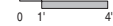
2301 SAN JOSE AVENUE
 SAN FRANCISCO, CA

DATE:	02.03.15
JOB:	0906
DRAWN:	MH
SCALE:	AS NOTED
ISSUE	DATE
100% DD SET	06.03.15
C OF A SET	09.11.15

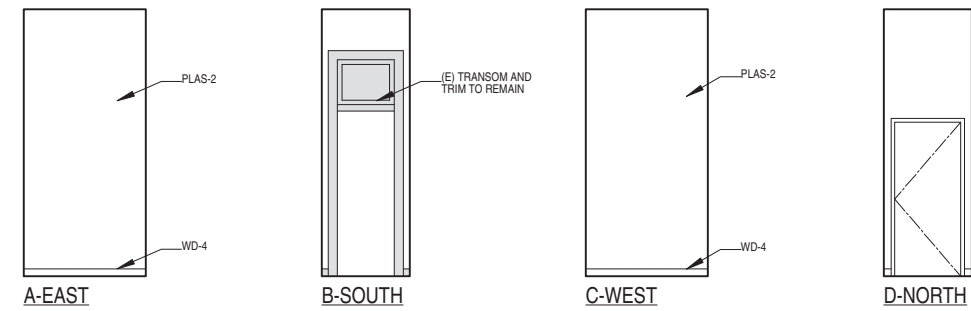


NOTE: HISTORIC FINISHES, SCOPE OF RESTORATION WORK T.B.D

INTERIOR ELEVATIONS - OFFICE (109)



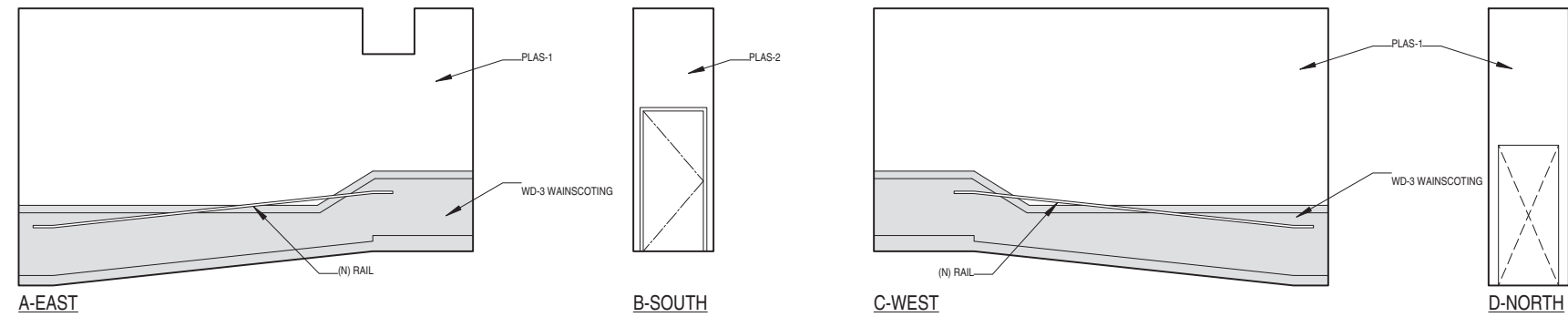
5



INTERIOR ELEVATIONS - HALL (106)



3



NOTE: HISTORIC FINISHES, SCOPE OF RESTORATION WORK T.B.D

INTERIOR ELEVATIONS - HALL (105)



1

A8.07
 INTERIOR ELEVATIONS



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DATE: 02.03.15

JOB: 0906

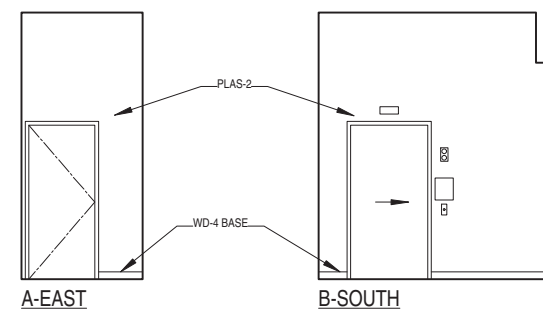
DRAWN: MH

SCALE: AS NOTED

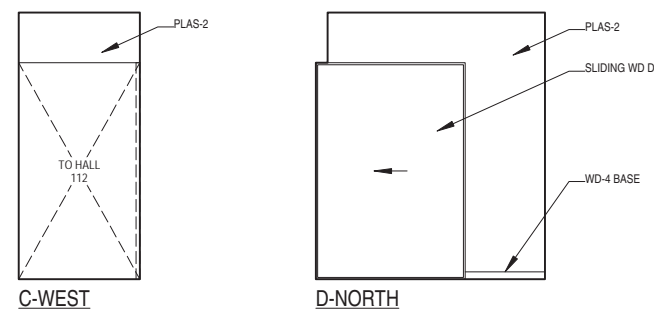
ISSUE	DATE
100% DD SET	06.03.15
C OF A SET	09.11.15

A8.08

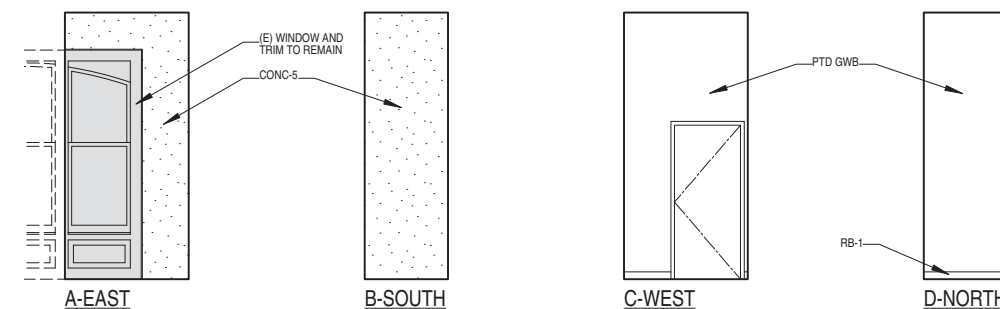
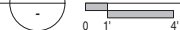
INTERIOR ELEVATIONS



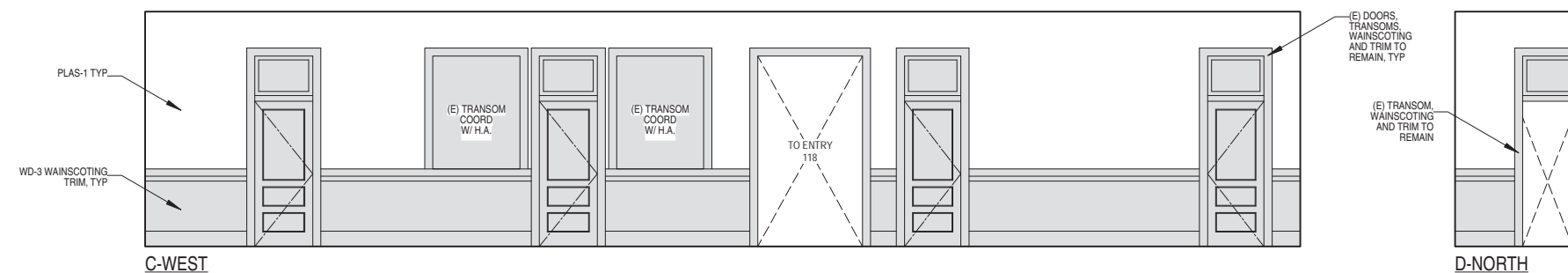
INTERIOR ELEVATIONS - HALL (115)



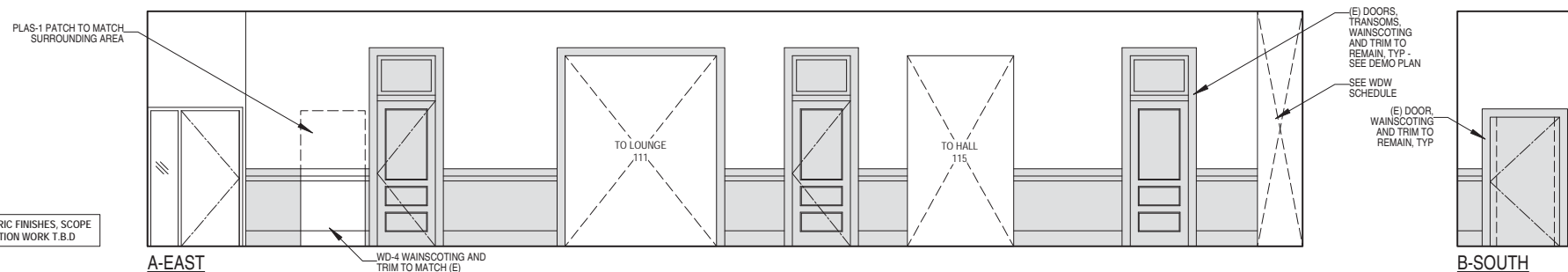
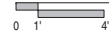
6 INTERIOR ELEVATIONS - STORAGE (114)



5



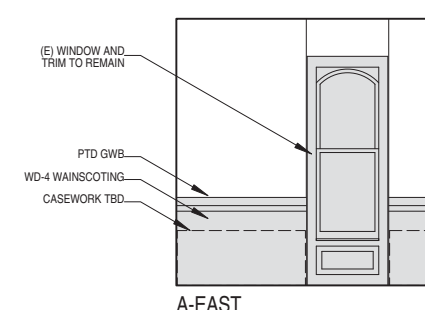
INTERIOR ELEVATIONS - HALL (112)



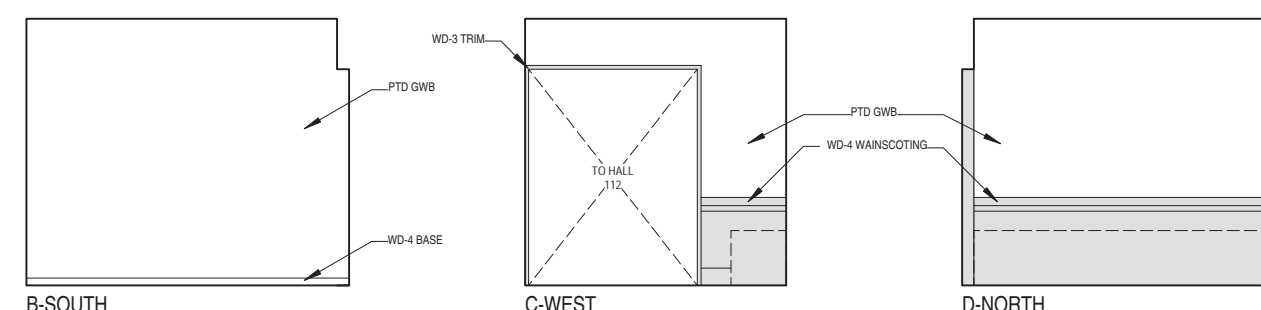
3



NOTE: HISTORIC FINISHES, SCOPE OF RESTORATION WORK T.B.D



INTERIOR ELEVATIONS - LOUNGE (111)



1





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DATE: 02.03.15

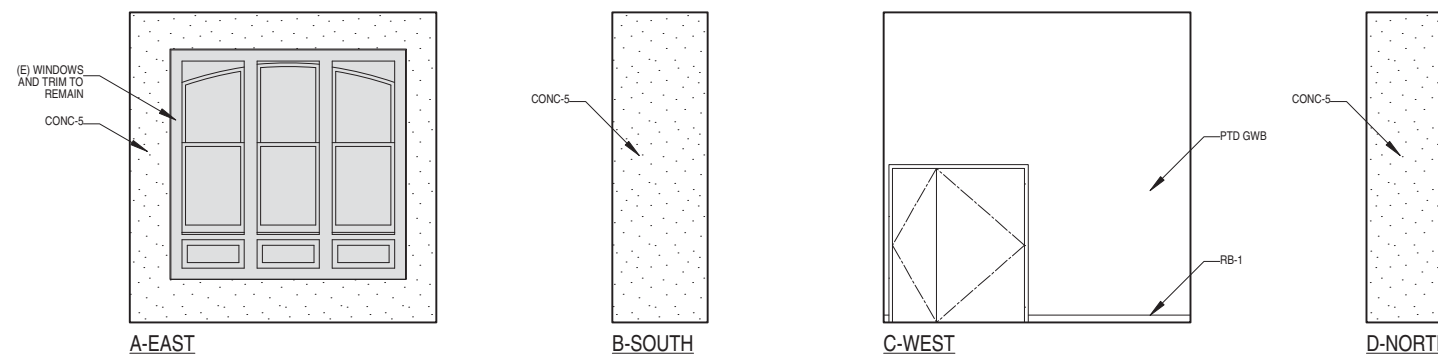
JOB: 0906

DRAWN: MH

SCALE: AS NOTED

ISSUE	DATE
100% DD SET	06.03.15
C OF A SET	09.11.15

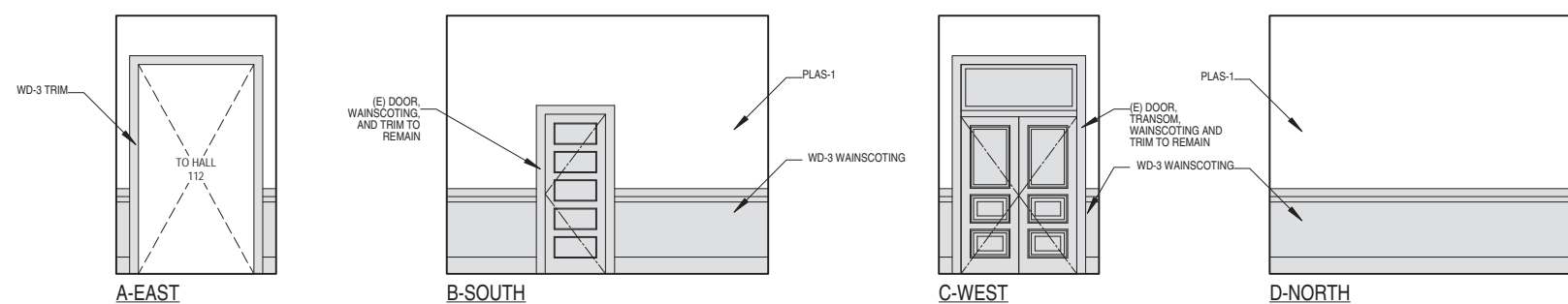
A8.09
 INTERIOR ELEVATIONS



INTERIOR ELEVATIONS - STORAGE (120)



5

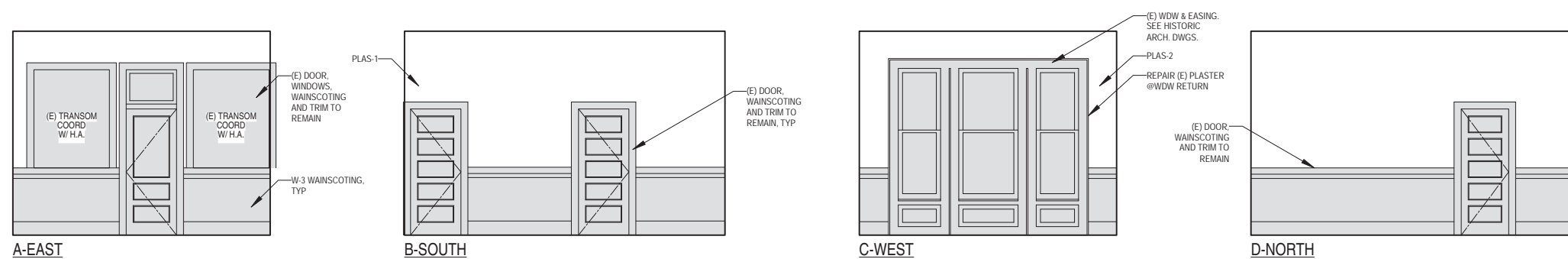


NOTE: HISTORIC FINISHES, SCOPE
 OF RESTORATION WORK T.B.D

INTERIOR ELEVATIONS - ENTRY (118)



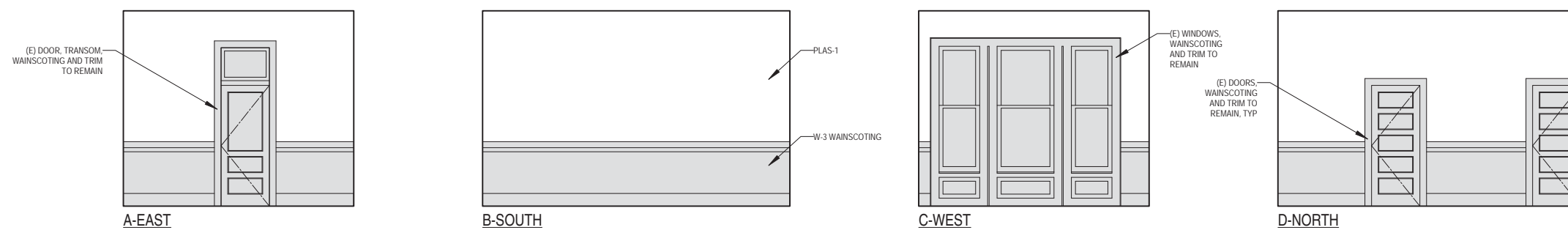
3



INTERIOR ELEVATIONS - RETAIL (117)



2



INTERIOR ELEVATIONS - COMMUNITY RM (116)



1



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DATE: 02.03.15

JOB: 0906

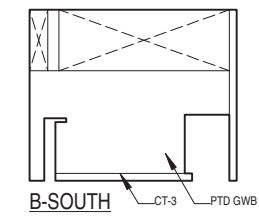
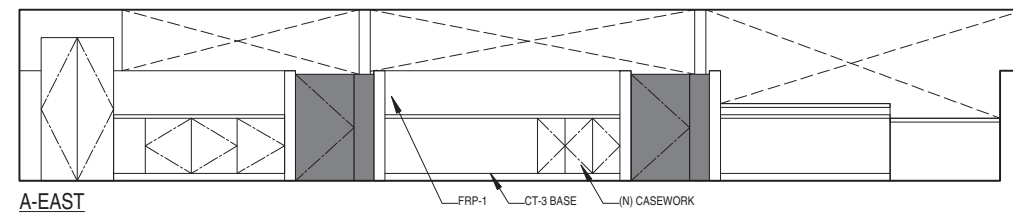
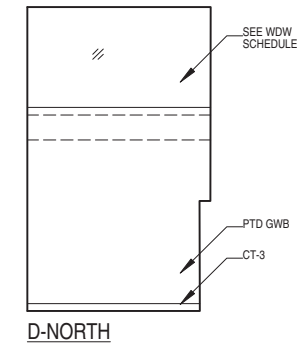
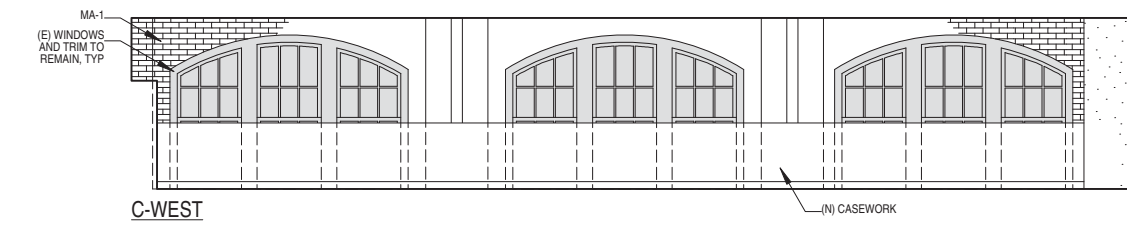
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SCALE: AS NOTED

ISSUE DATE

100% DD SET 06.03.15

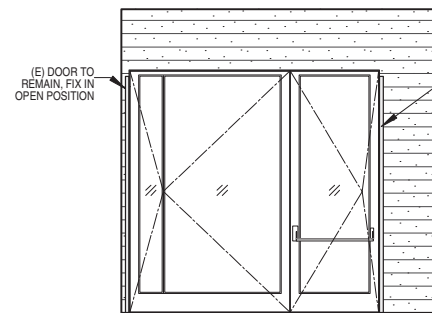
C OF A SET 09.11.15



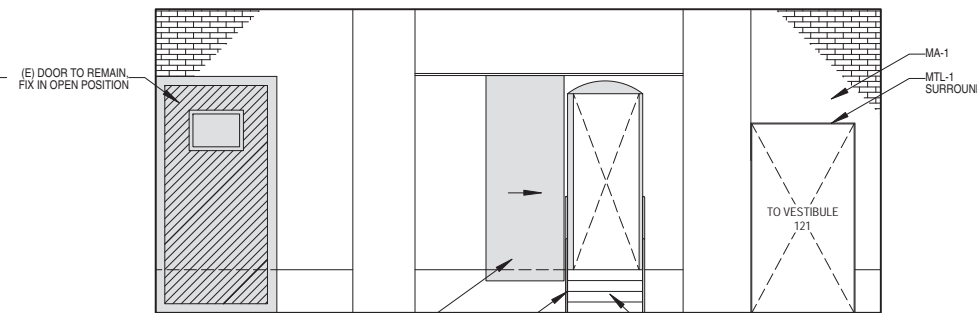
INTERIOR ELEVATIONS - CATERING PANTRY (124)

0 1' 4'

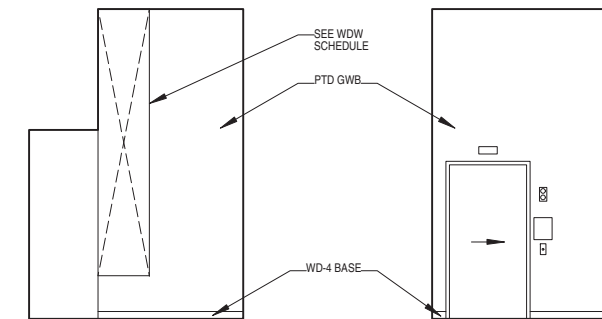
3



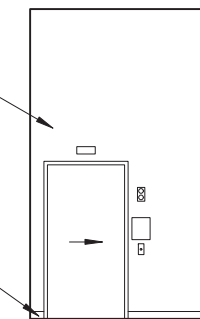
C-WEST



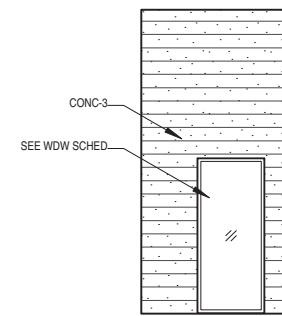
D-NORTH



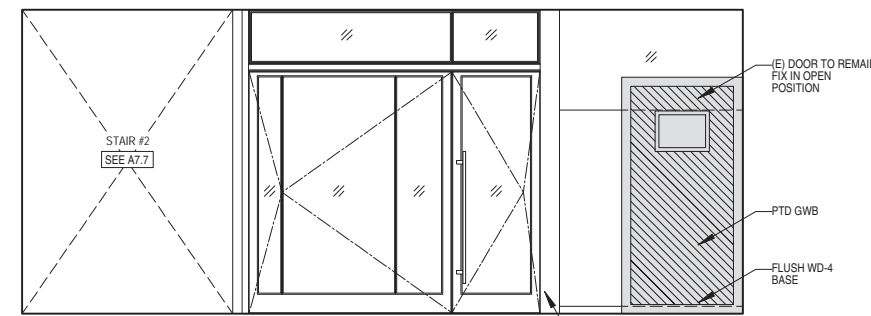
C-WEST



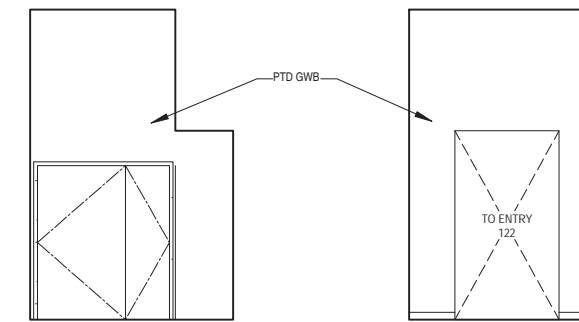
D-NORTH



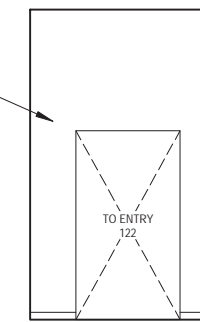
A-EAST



B-SOUTH



A-EAST



B-SOUTH

INTERIOR ELEVATIONS - ENTRY (122)

0 1' 4'

2

INTERIOR ELEVATIONS - VESTIBULE (121)

0 1' 4'

1

A8.10

INTERIOR ELEVATIONS



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DATE: 02.03.15

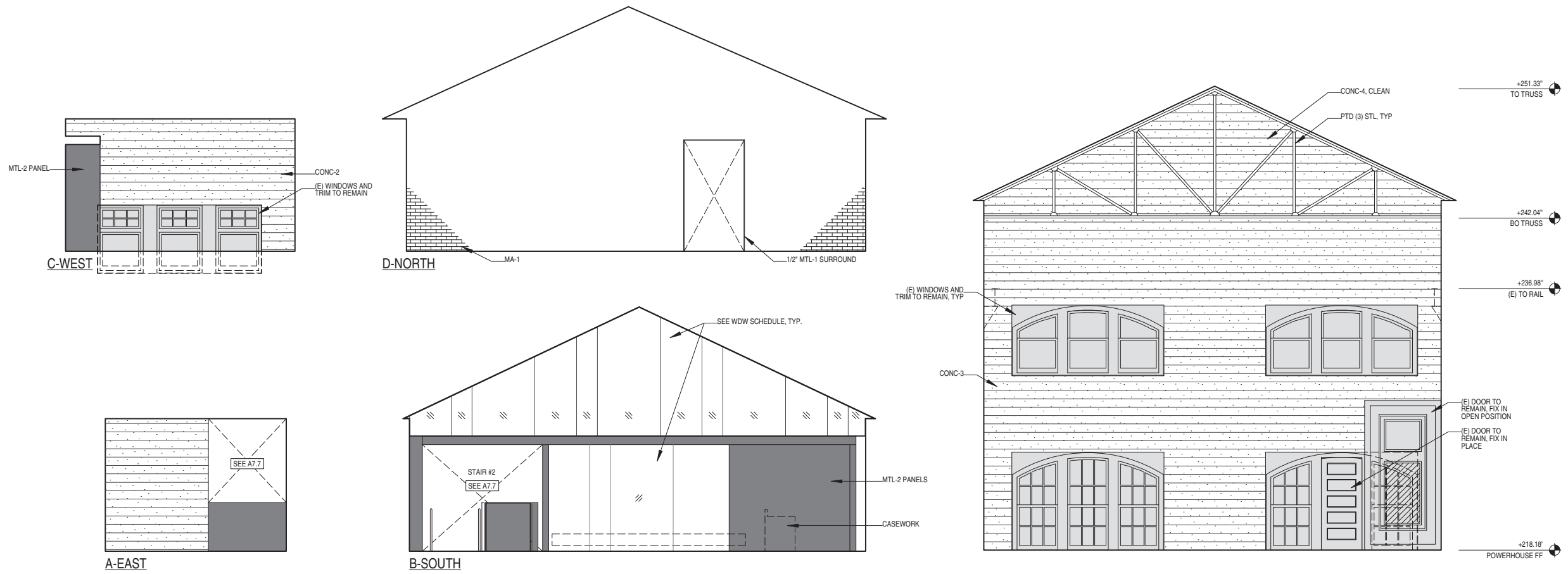
JOB: 0906

DRAWN: MH

SCALE: AS NOTED

ISSUE	DATE
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C OF A SET	DATE
	09.11.15

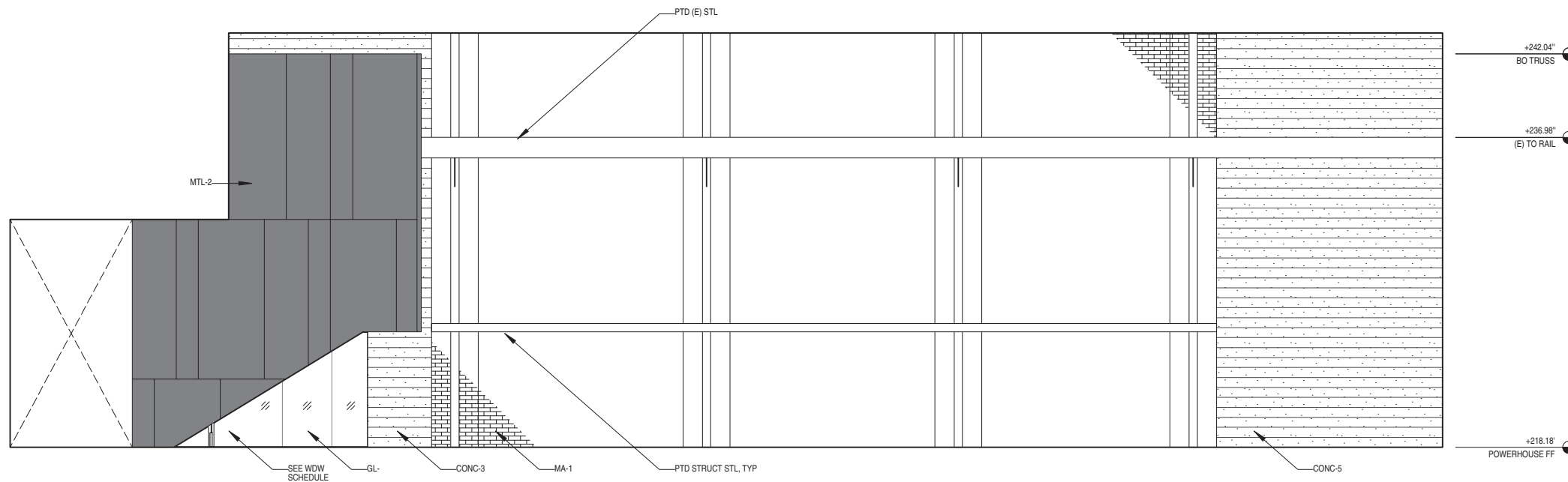


INTERIOR ELEVATIONS - THEATER LOBBY (220)

0 1' 4'

3 INT ELEV - POWERHOUSE (123) - SOUTH

0 1' 4'



INT ELEV - POWERHOUSE (123) - EAST

0 1' 4'

2

0

1

0

A8.11

INTERIOR ELEVATIONS

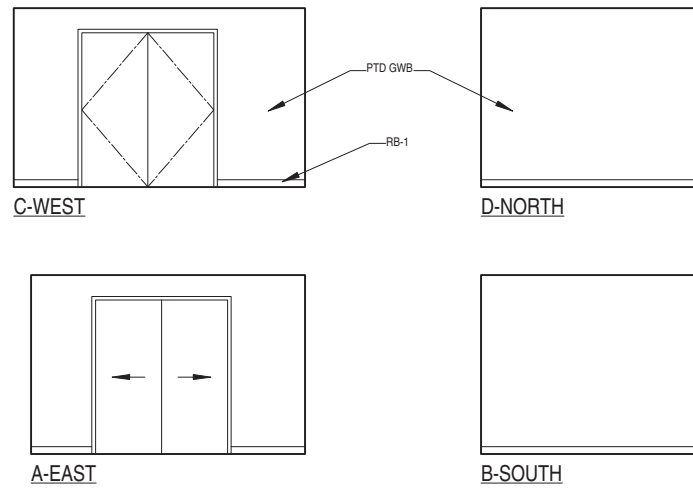


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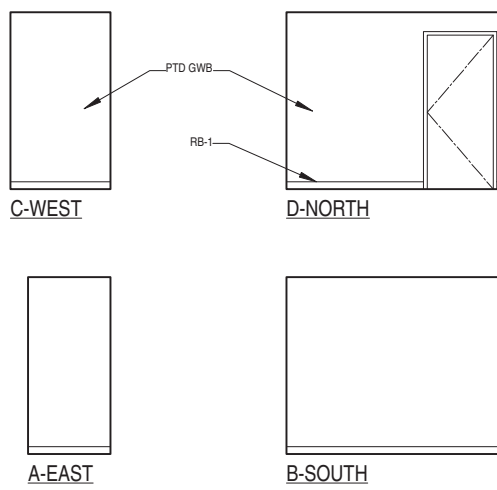
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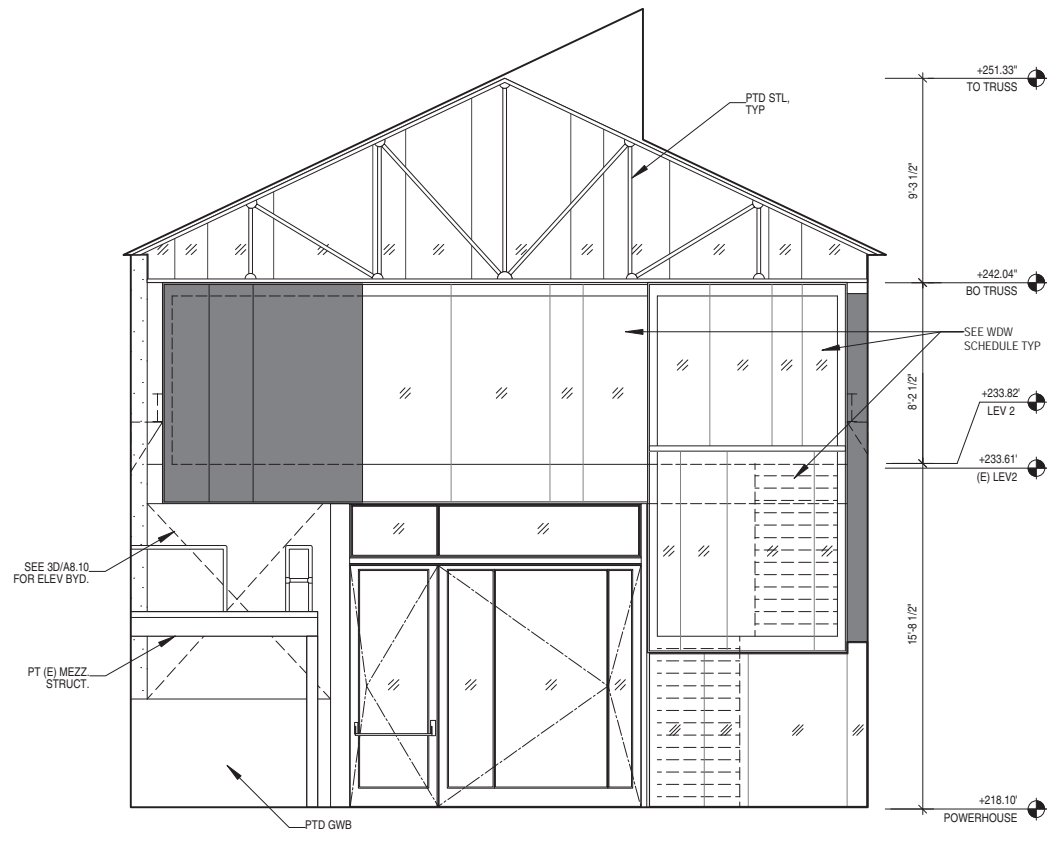
DATE:	02.03.15
JOB:	0906
DRAWN:	MH
SCALE:	AS NOTED
ISSUE	DATE
100% DD SET	06.03.15
C OF A SET	09.11.15



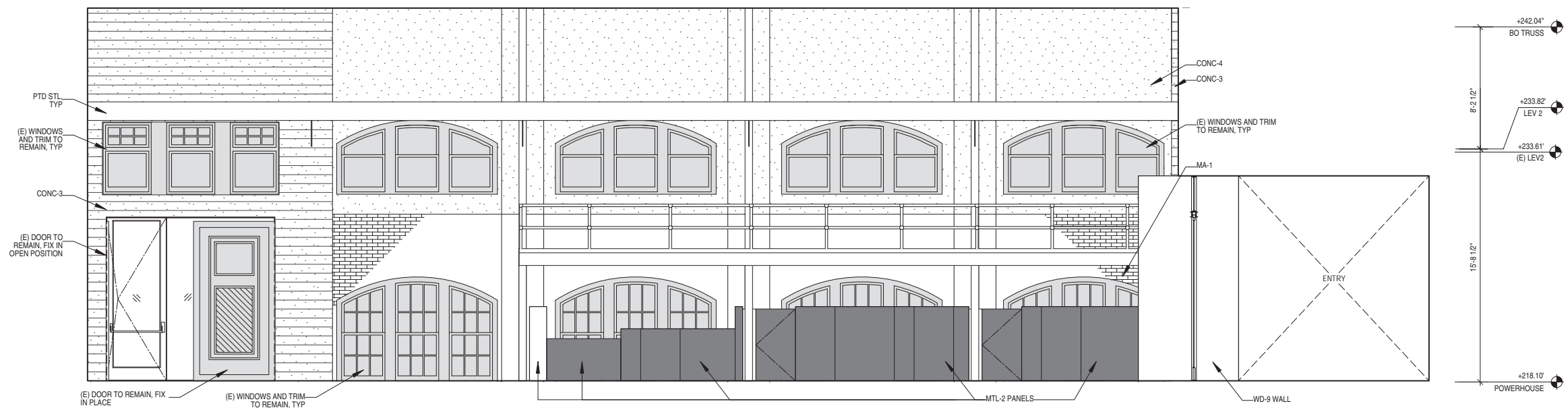
INTERIOR ELEVATIONS - STORAGE (212)



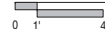
4 INTERIOR ELEVATIONS - JANITOR (210)



3 INT ELEV - POWERHOUSE (123) - NORTH



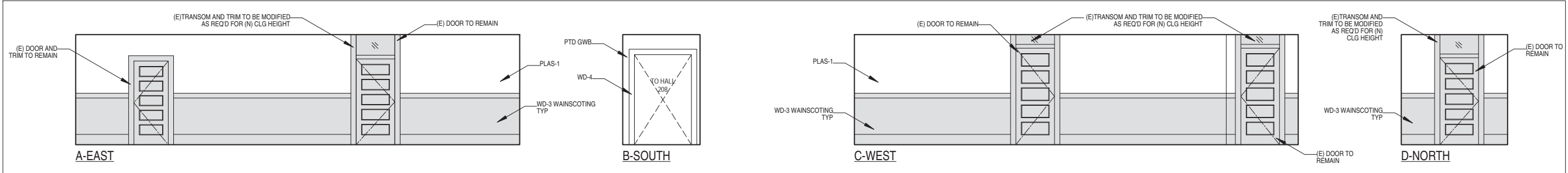
1 INT ELEV - POWERHOUSE (123) - WEST



A8.12

INTERIOR ELEVATIONS

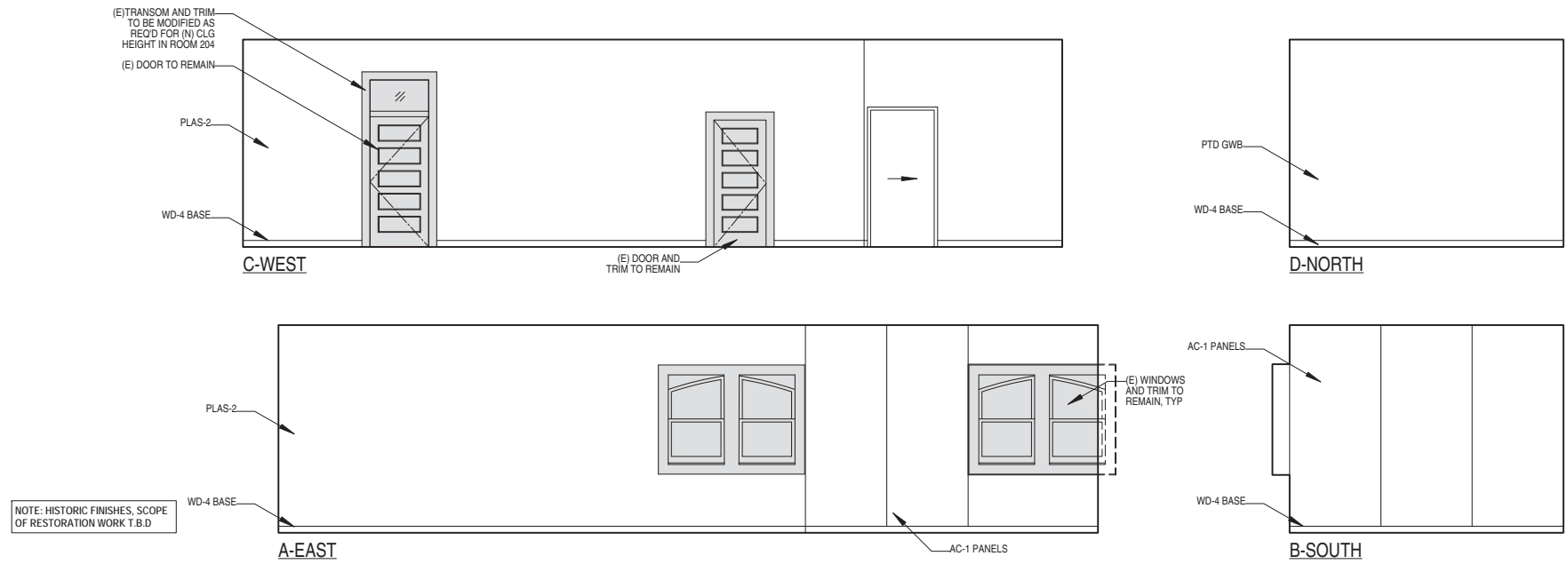
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INTERIOR ELEVATIONS - HALL (204)



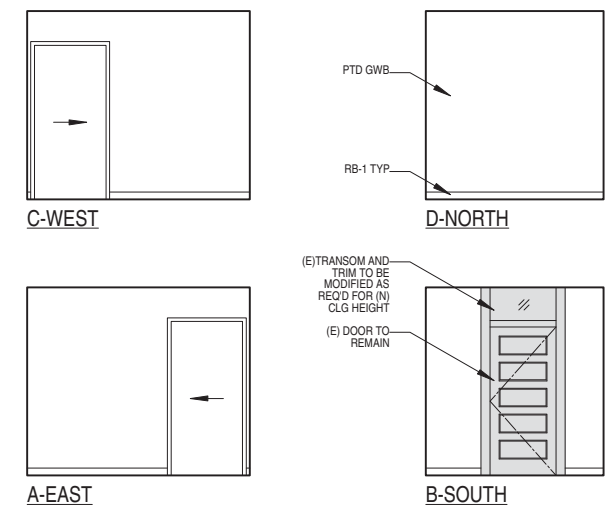
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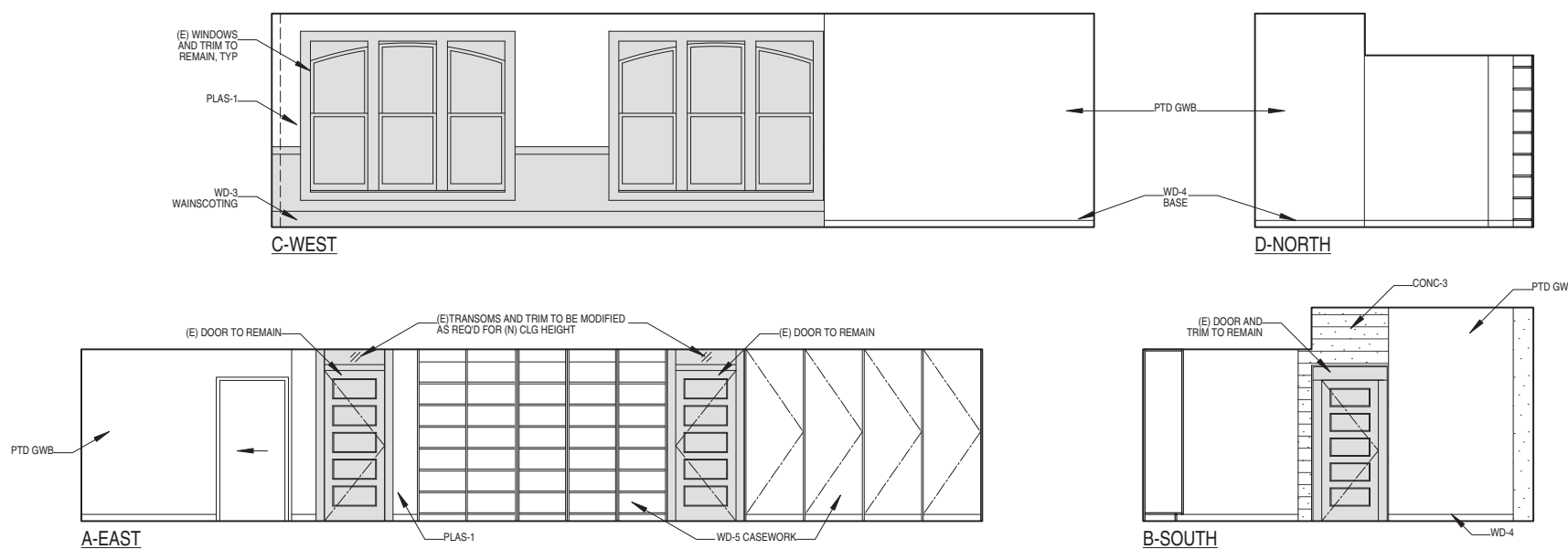
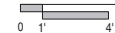
INTERIOR ELEVATIONS - DESIGN/PHOTO STUDIO (203)



3



INTERIOR ELEVATIONS - STORAGE (202)



INTERIOR ELEVATIONS - LITERARY ARTS STUDIO (201)



1



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GENEVA CAR BARN AND POWERHOUSE

2301 SAN JOSE AVENUE
 SAN FRANCISCO, CA

DATE:	02.03.15
JOB:	0906
DRAWN:	MH
SCALE:	AS NOTED
ISSUE	DATE
100% DD SET	06.03.15
C OF A SET	09.11.15

A8.13
 INTERIOR ELEVATIONS



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GENEVA CAR BARN
 AND POWERHOUSE

2301 SAN JOSE AVENUE
 SAN FRANCISCO, CA

DATE: 02.03.15

JOB: 0906

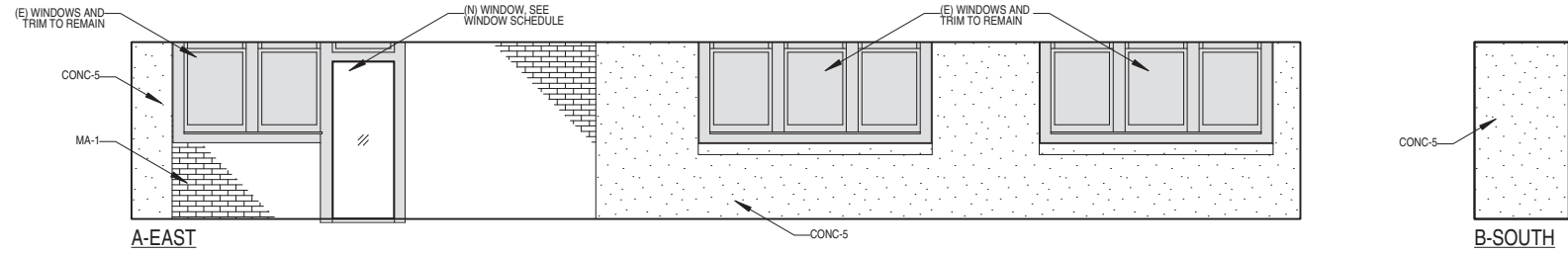
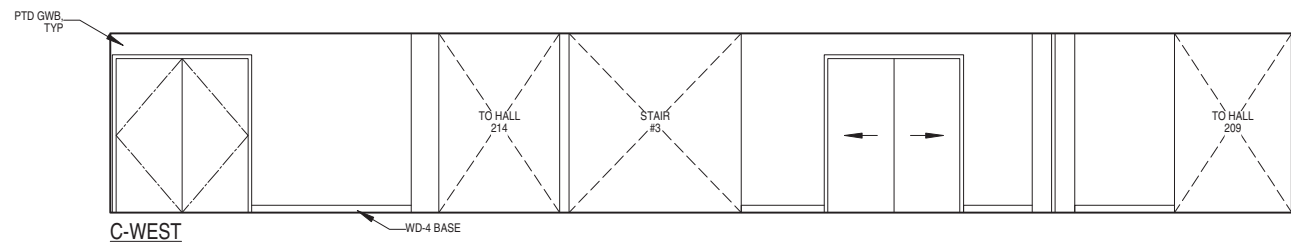
DRAWN: MH

SCALE: AS NOTED

ISSUE	DATE
100% DD SET	06.03.15
C OF A SET	09.11.15

A8.14

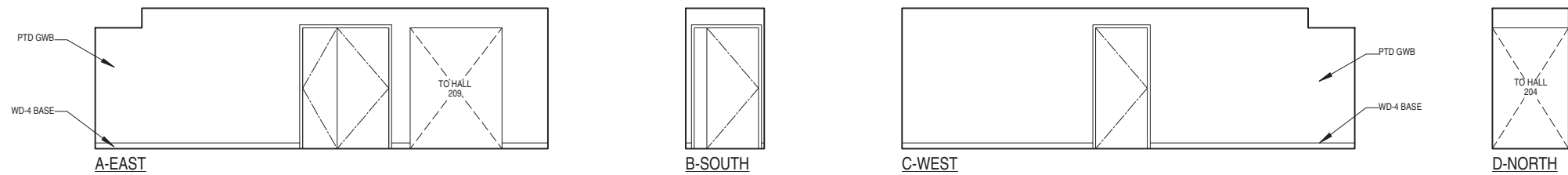
INTERIOR ELEVATIONS



INTERIOR ELEVATIONS - HALL (211)

0 1' 4'

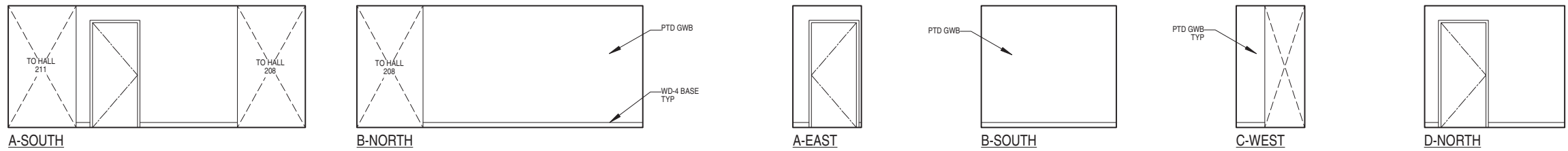
6



INTERIOR ELEVATIONS - HALL (208)

0 1' 4'

5



INTERIOR ELEVATIONS - HALL (209)

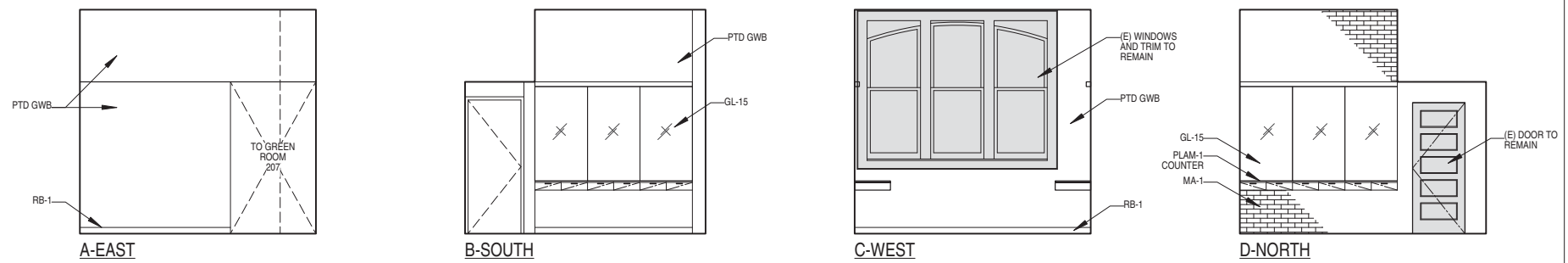
0 1' 4'

4

INTERIOR ELEVATIONS - GREEN ROOM (207)

0 1' 4'

3



INTERIOR ELEVATIONS - GREEN ROOM (205)

0 1' 4'

1



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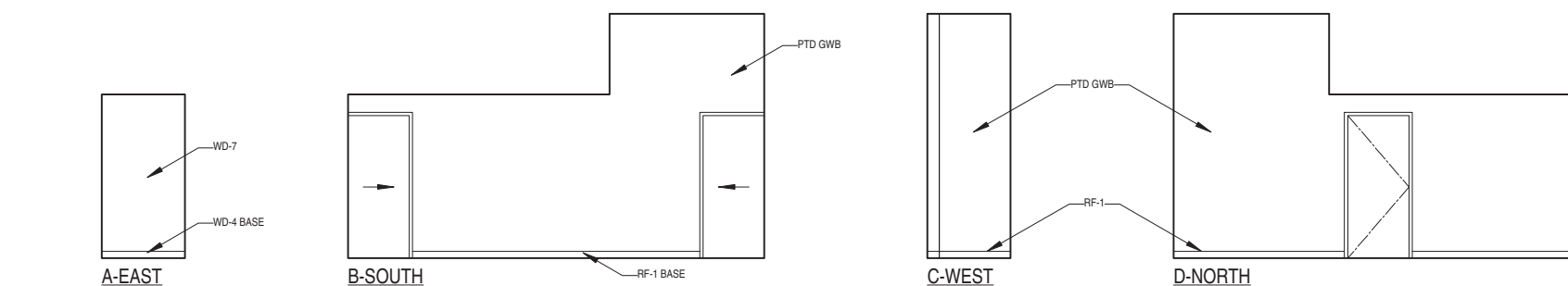
DATE: 02.03.15

JOB: 0906

DRAWN: MH

SCALE: AS NOTED

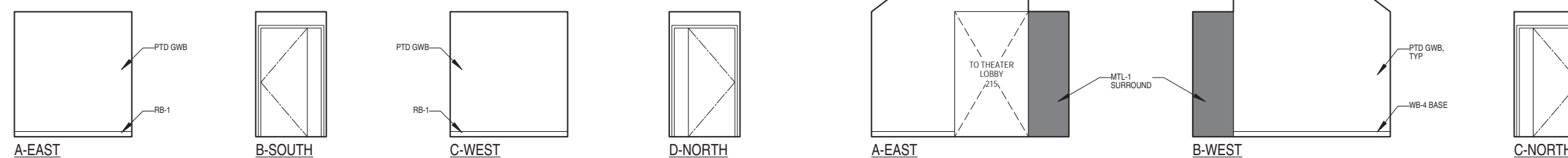
ISSUE	DATE
100% DD SET	06.03.15
C OF A SET	09.11.15



INTERIOR ELEVATIONS - CROSSOVER (219)



7



INTERIOR ELEVATIONS - LIGHT LOCK (217)

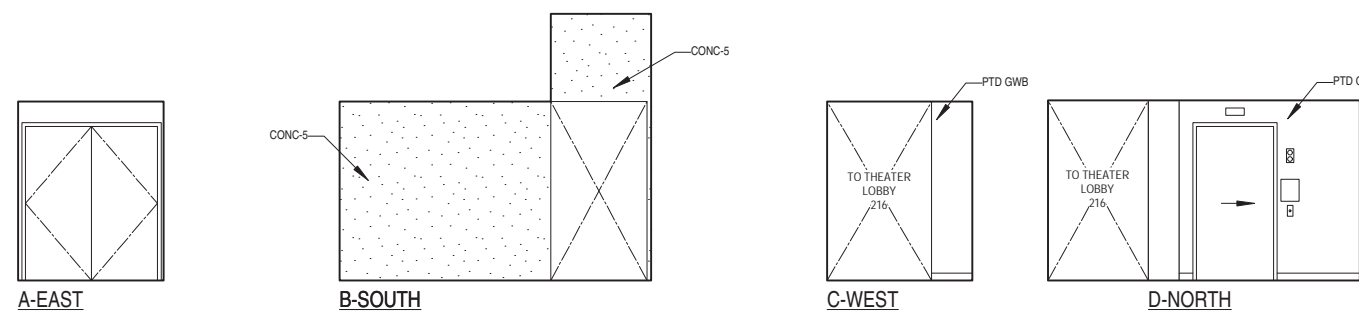


6

INTERIOR ELEVATIONS - THEATER LOBBY (216)



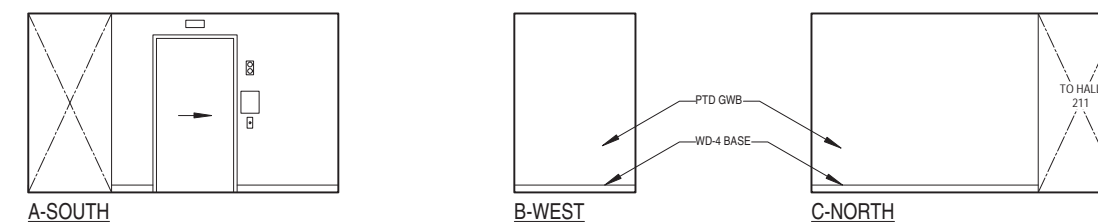
5



INTERIOR ELEVATIONS - THEATER LOBBY (215)



3



INTERIOR ELEVATIONS - ELEVATOR LOBBY (214)



1

A8.15
 INTERIOR ELEVATIONS



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DATE: 02.03.15

JOB: 0906

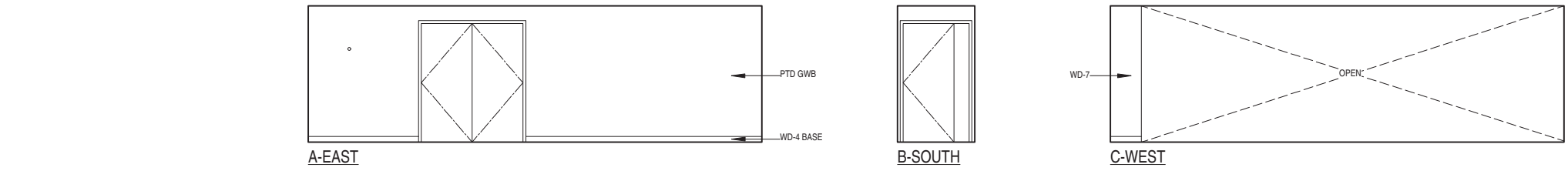
DRAWN: MH

SCALE: AS NOTED

ISSUE	DATE
100% DD SET	06.03.15
C OF A SET	09.11.15

A8.16

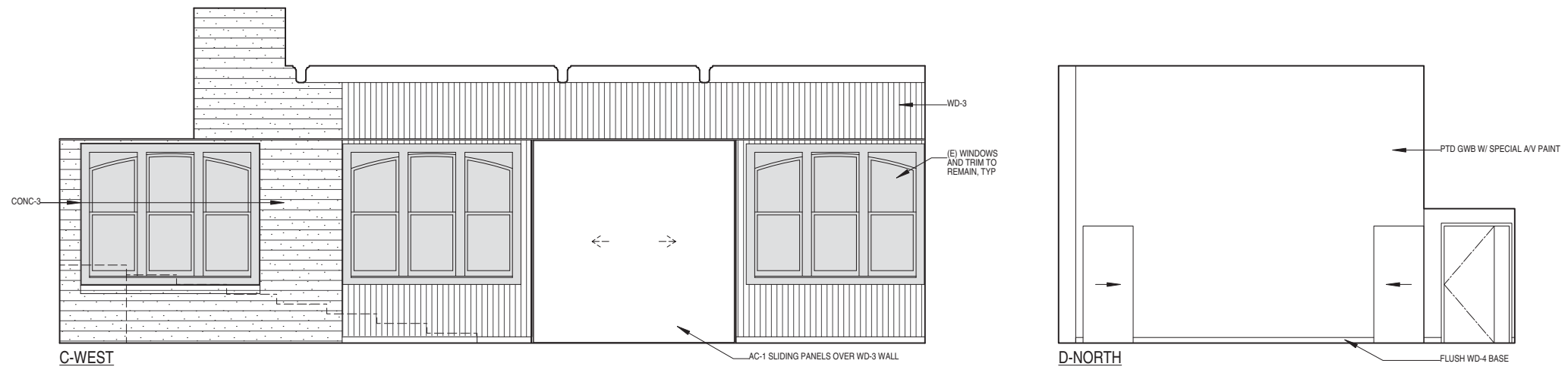
INTERIOR ELEVATIONS



INTERIOR ELEVATIONS - THEATER HALL (218)

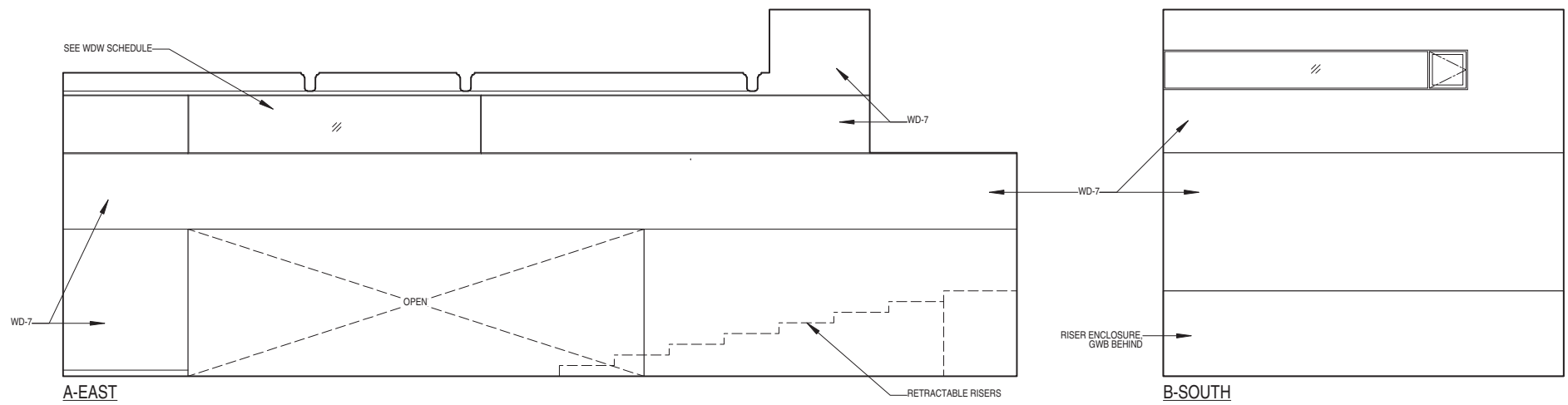


3



C-WEST

D-NORTH



A-EAST

B-SOUTH

INTERIOR ELEVATIONS - THEATER (218)



1



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DATE: 02.03.15

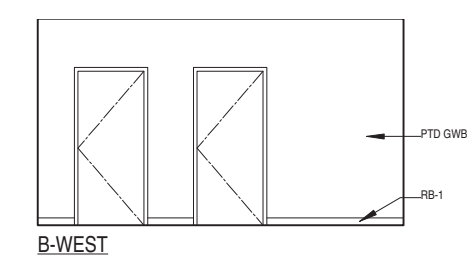
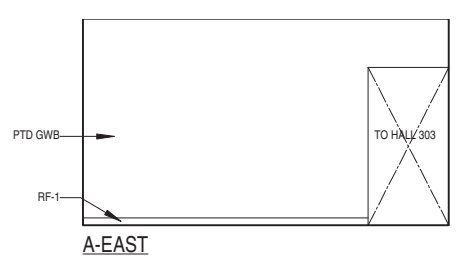
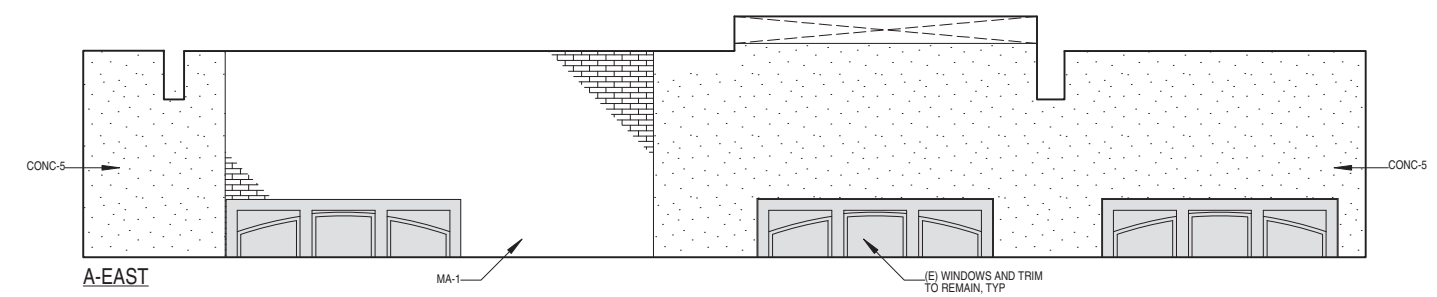
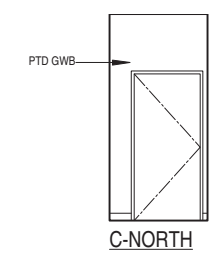
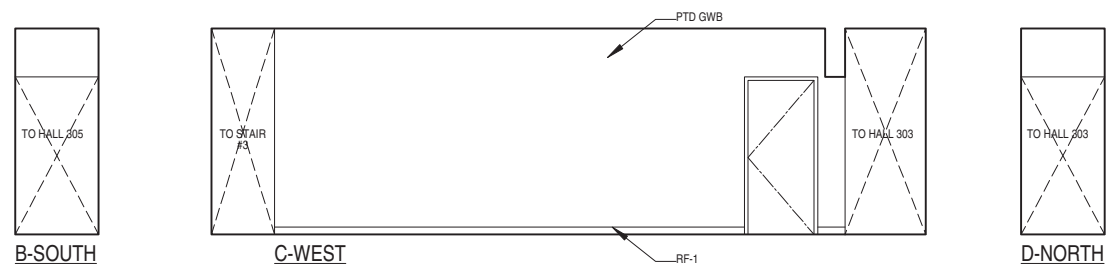
JOB: 0906

DRAWN: MH

SCALE: AS NOTED

ISSUE	DATE
100% DD SET	06.03.15
C OF A SET	09.11.15

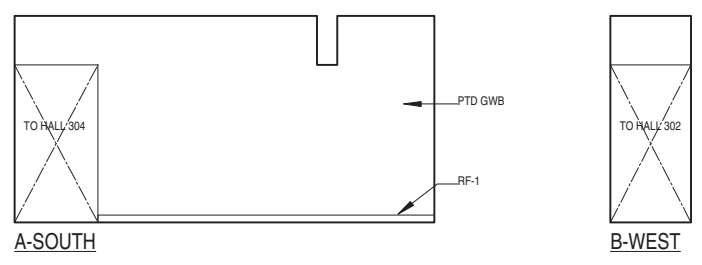
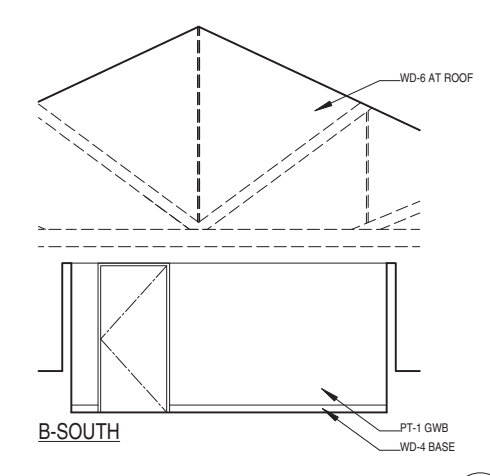
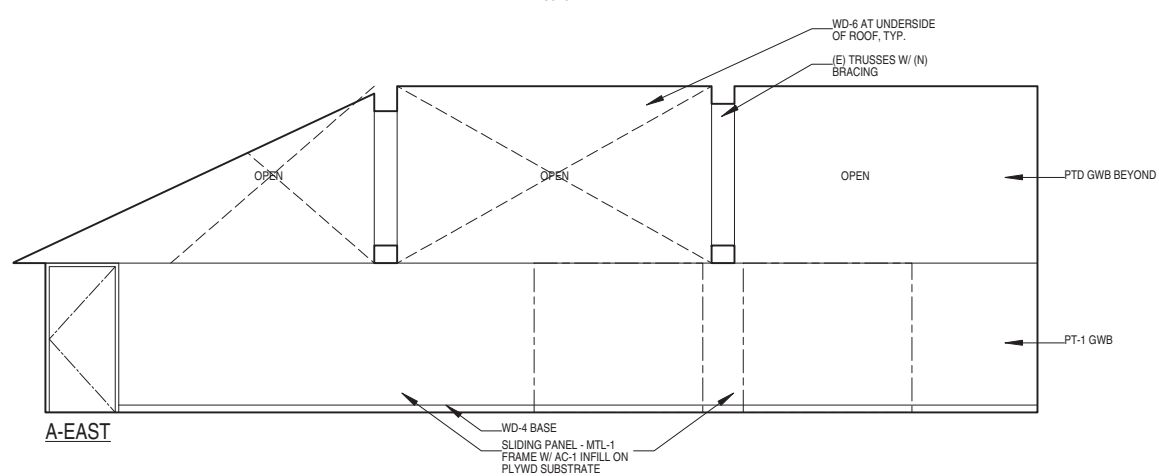
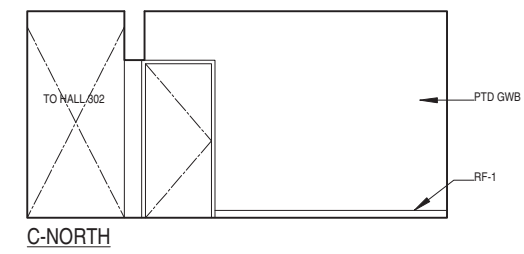
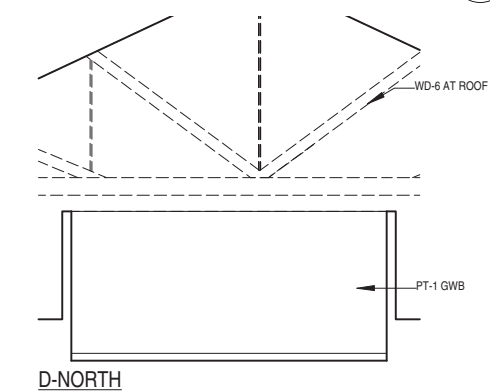
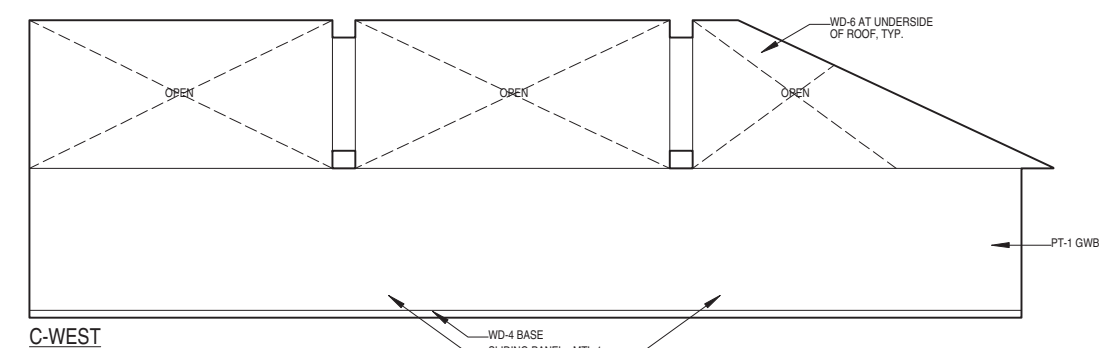
A8.17
 INTERIOR ELEVATIONS



INTERIOR ELEVATIONS - HALL (304)
 0 1' 4'

5 INTERIOR ELEVATIONS - HALL (302)
 0 1' 4'

4



INTERIOR ELEVATIONS - HALL (303)
 0 1' 4'

2 INTERIOR ELEVATIONS - FLEXIBLE STUDIO (301)
 0 1' 4'

1



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GENEVA CAR BARN
 AND POWERHOUSE

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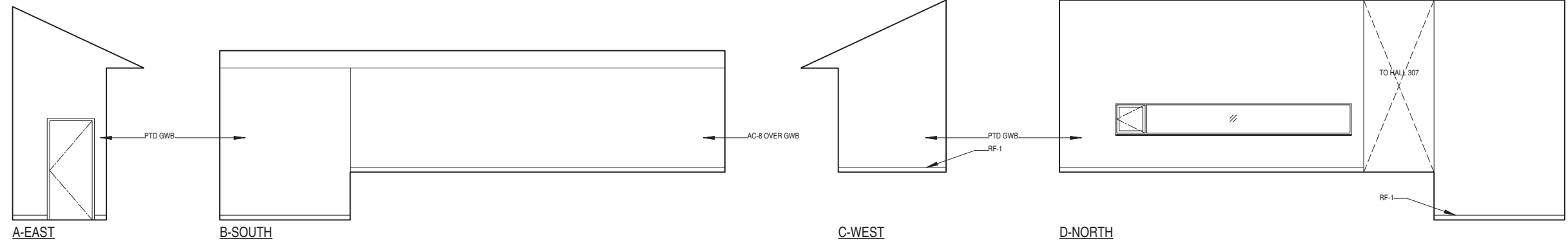
DATE: 02.03.15

JOB: 0906

DRAWN: MH

SCALE: AS NOTED

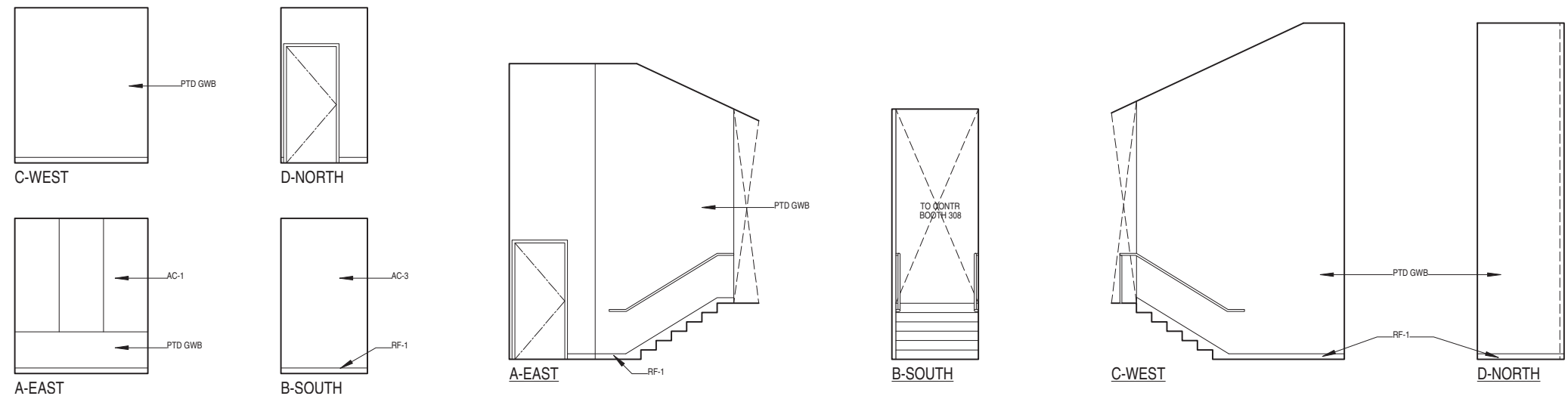
ISSUE	DATE
100% DD SET	06.03.15
C OF A SET	09.11.15



INTERIOR ELEVATIONS - CONTROL BOOTH (308)



5



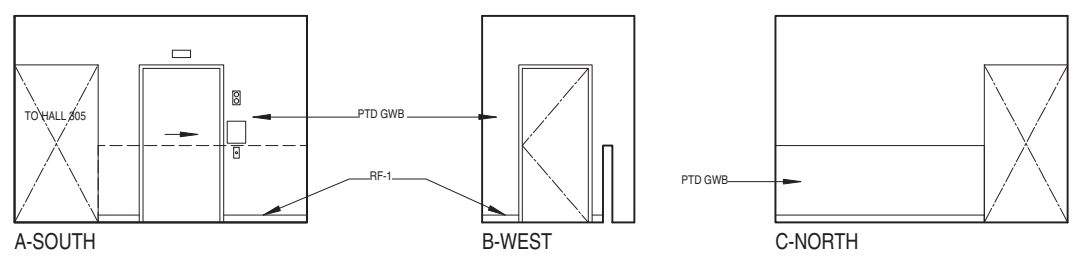
INTERIOR ELEVATIONS - RECORDING (309)



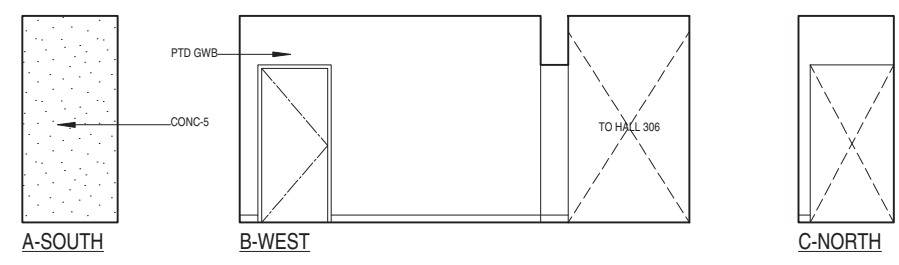
INTERIOR ELEVATIONS - STAIR #5 (307)



3



INTERIOR ELEVATIONS - HALL (306)



INTERIOR ELEVATIONS - HALL (305)



1

A8.18

INTERIOR ELEVATIONS



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 SAN FRANCISCO, CA

DATE: 02.03.15

JOB: 0906

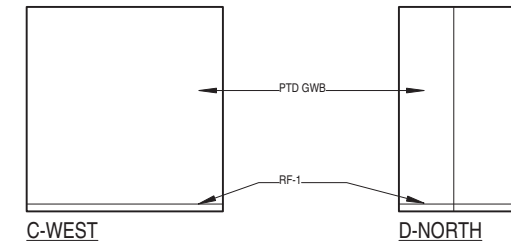
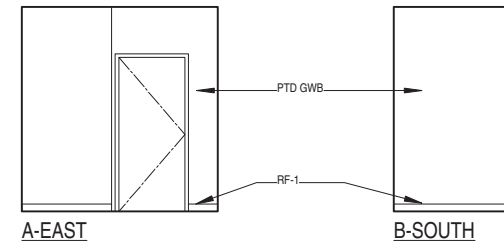
DRAWN: MH

SCALE: AS NOTED

ISSUE	DATE
100% DD SET	06.03.15
C OF A SET	09.11.15

A8.19

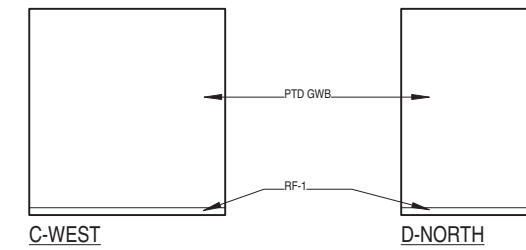
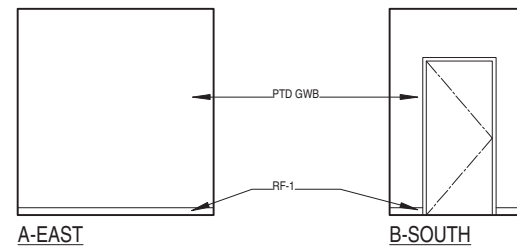
INTERIOR ELEVATIONS



INTERIOR ELEVATIONS - JANITOR (313)



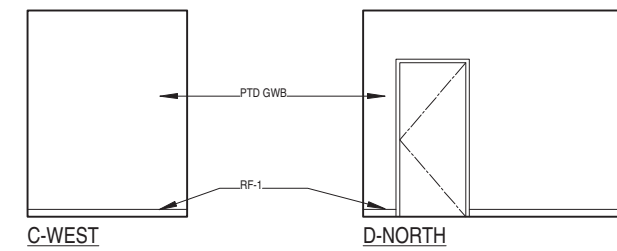
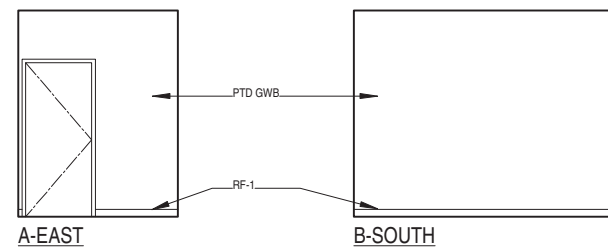
6
-



INTERIOR ELEVATIONS - ELECTRICAL (312)



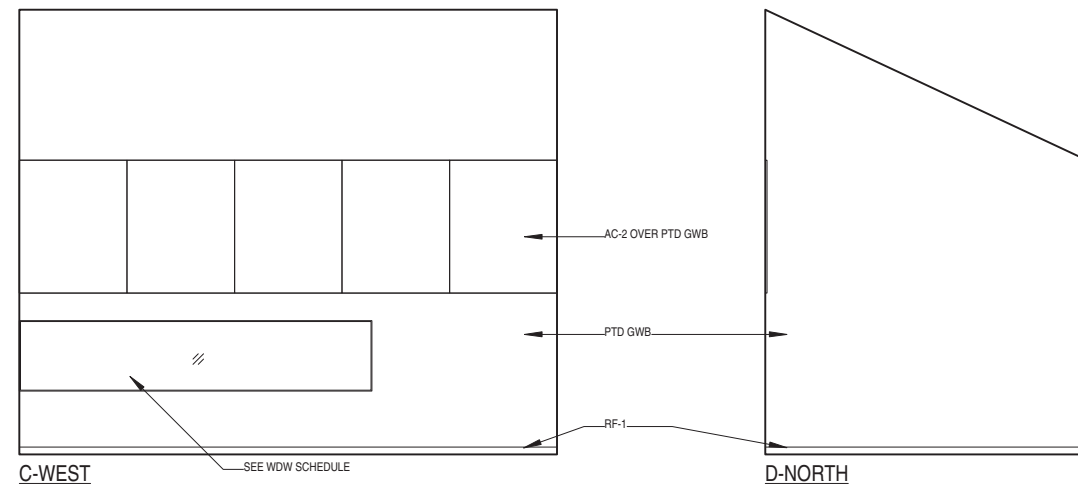
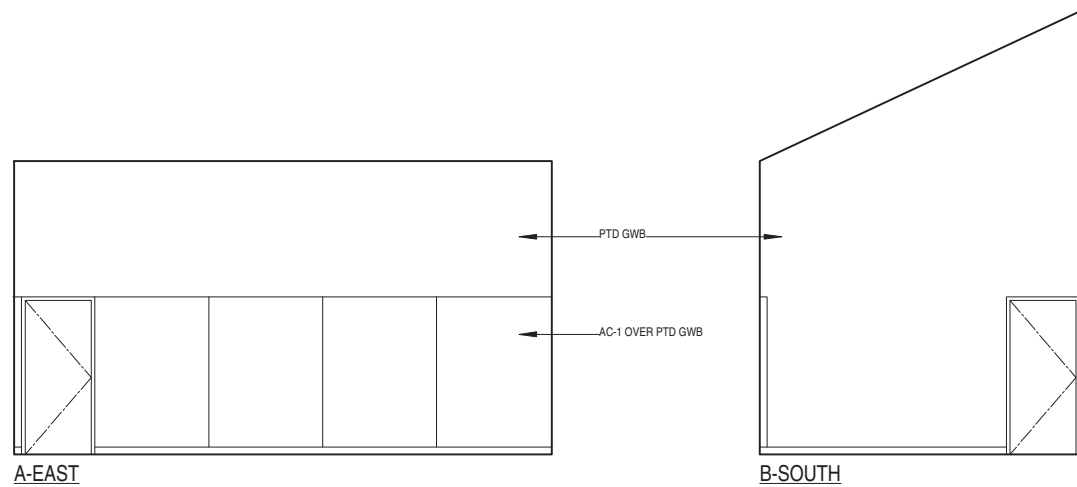
4
-



INTERIOR ELEVATIONS - THEATER OFFICE (311)



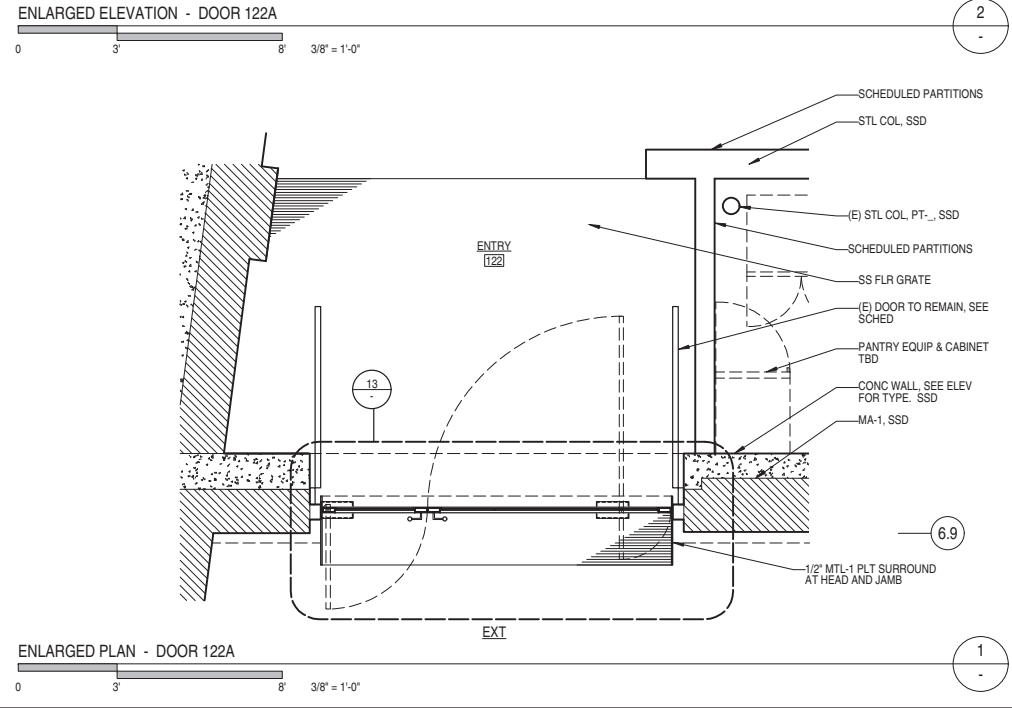
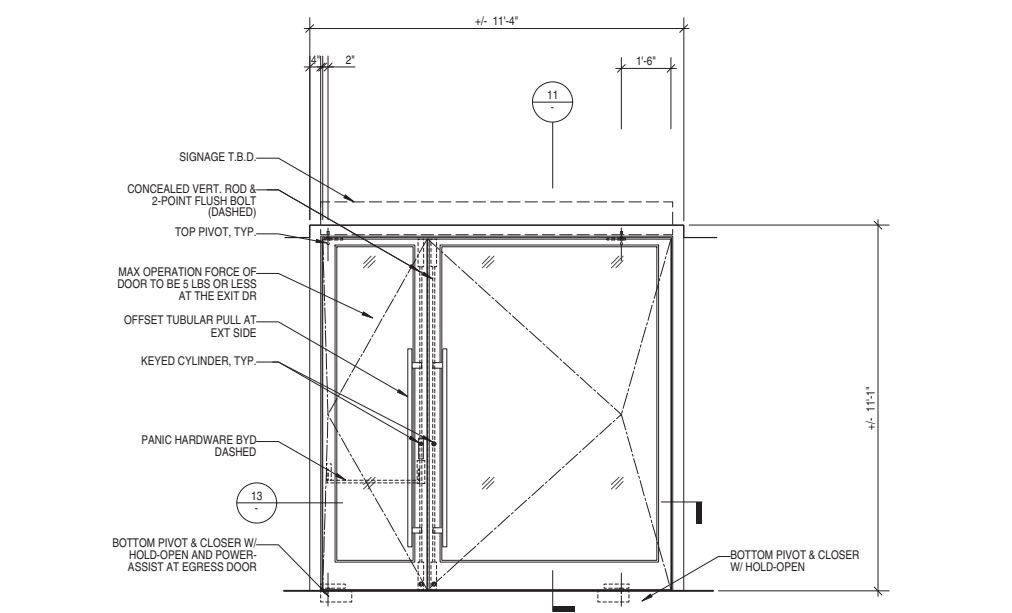
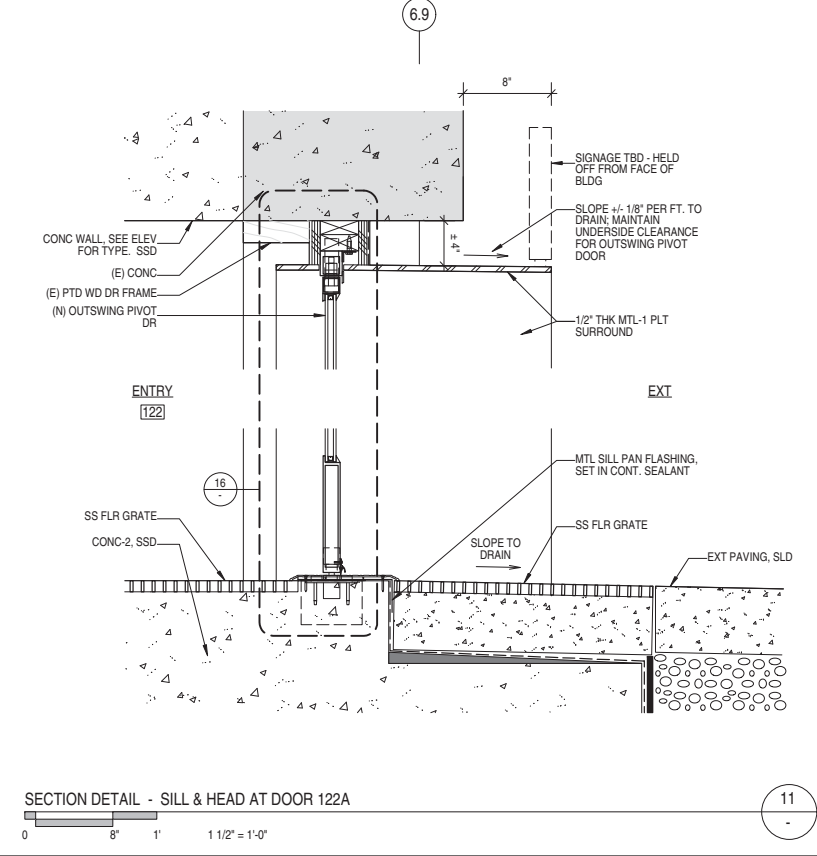
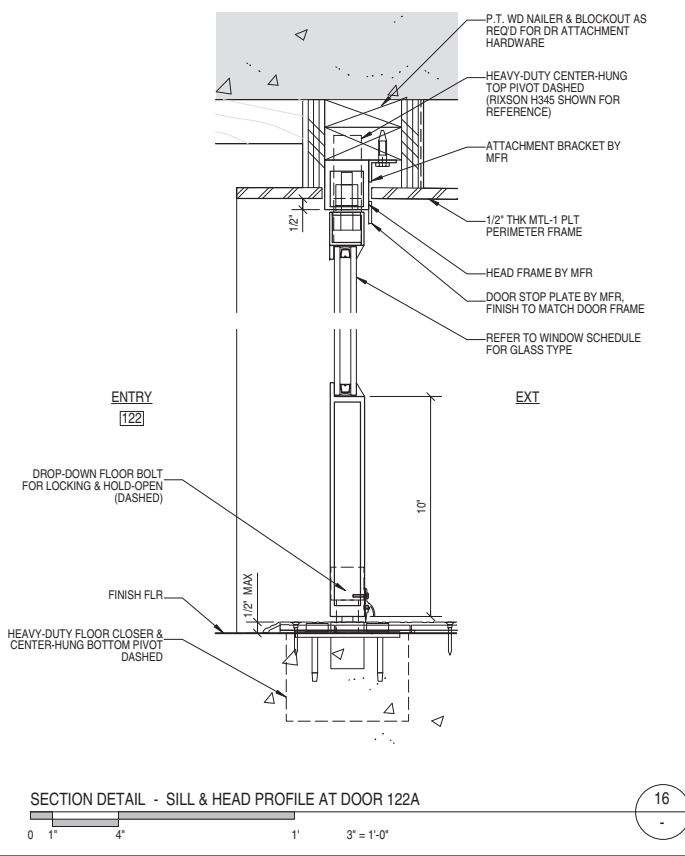
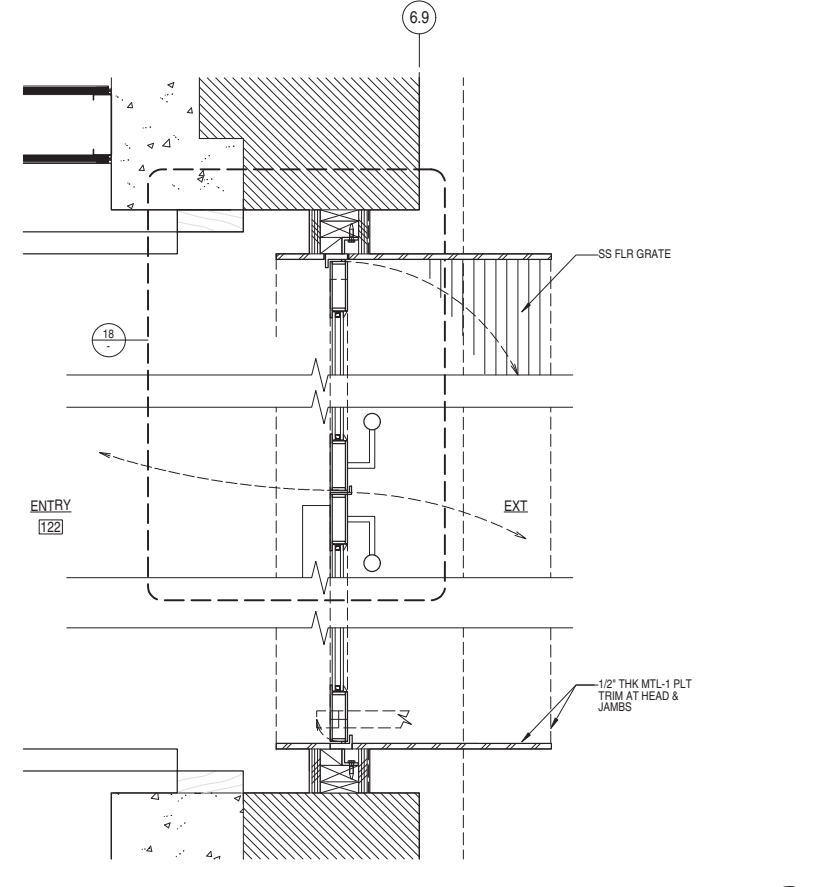
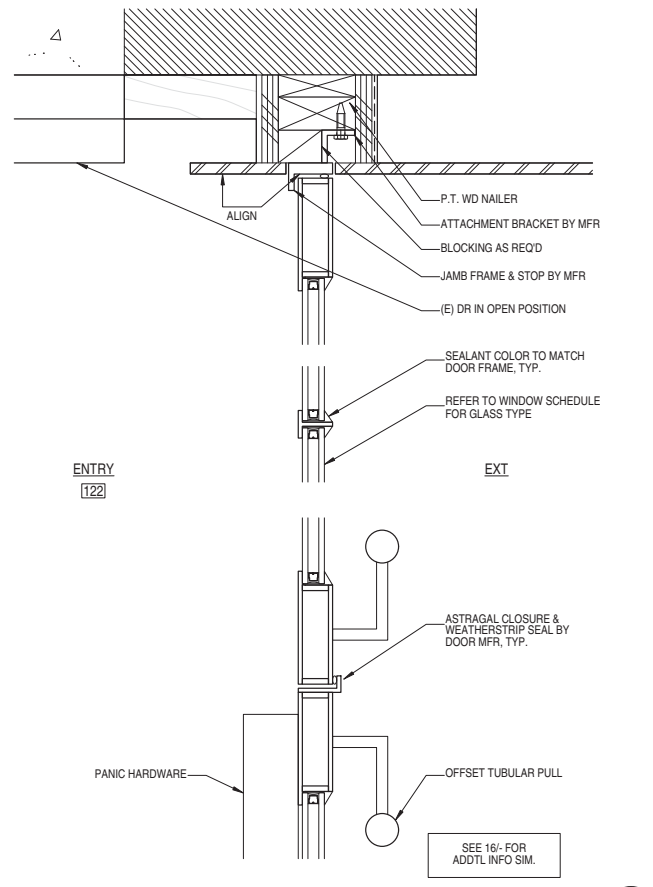
2
-



INTERIOR ELEVATIONS - AUDIO/VIDEO STUDIO (310)



1
-



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GENEVA CAR BARN AND POWERHOUSE

2301 SAN JOSE AVENUE
SAN FRANCISCO, CA

DATE:	02.05.15
JOB:	0906
DRAWN:	--
SCALE:	AS NOTED
ISSUE	DATE
100% DD SET	06.03.15
C OF A SET	09.11.15

A9.1

EXTERIOR DETAILS - POWERHOUSE



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**GENEVA CAR BARN
 AND POWERHOUSE**

2301 SAN JOSE AVENUE
 SAN FRANCISCO, CA

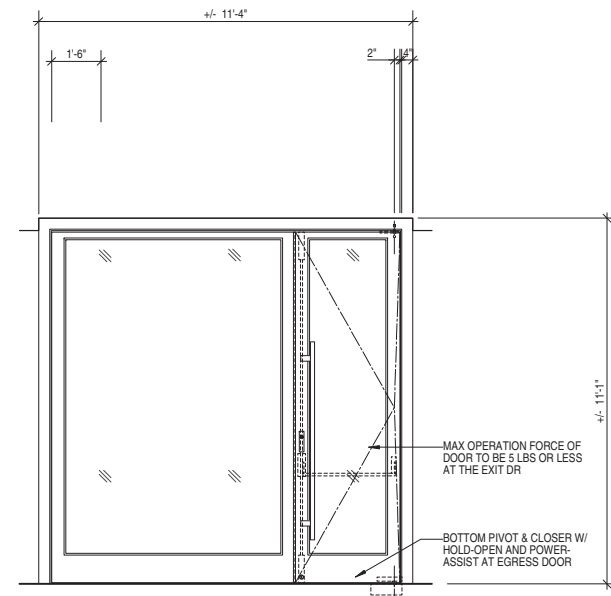
DATE: 02.05.15

JOB: 0906

DRAWN: --

SCALE: AS NOTED

ISSUE	DATE
100% DD SET	06.03.15
C OF A SET	09.11.15

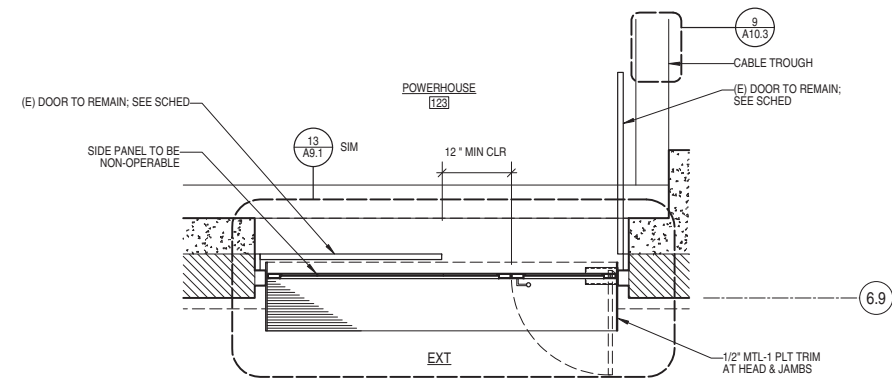


FOR ADDITIONAL INFO NOT INDICATED, REF DET 2/A9.1

ENLARGED ELEVATION - DOOR 123B

0 3' 8' 3/8" = 1'-0"

2



ENLARGED PLAN - DOOR 123B

0 3' 8' 3/8" = 1'-0"

1

**A9.2
 EXTERIOR DETAILS -
 POWERHOUSE**



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2301 SAN JOSE AVENUE
 SAN FRANCISCO, CA

DATE: 02.05.15

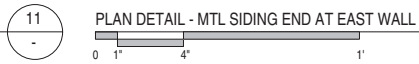
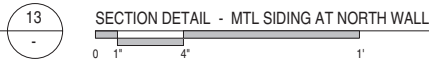
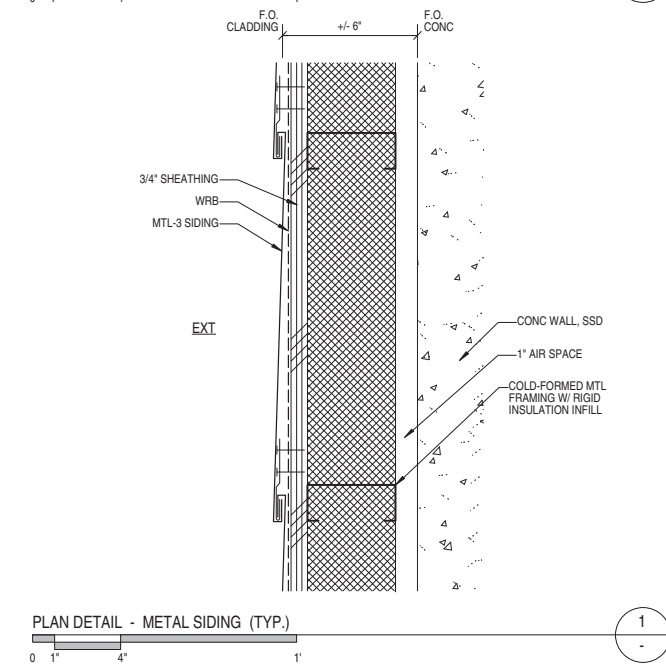
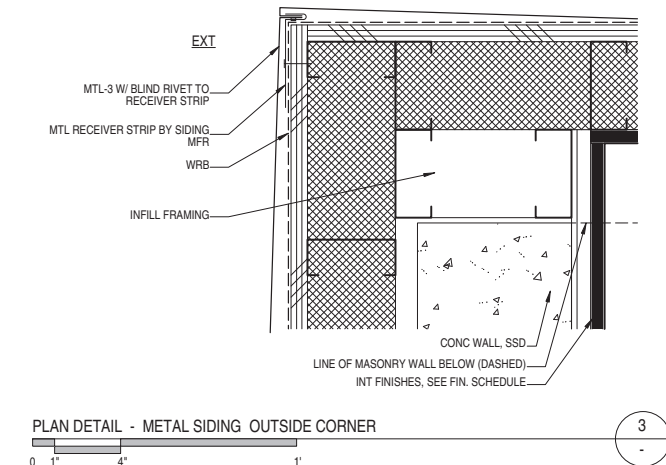
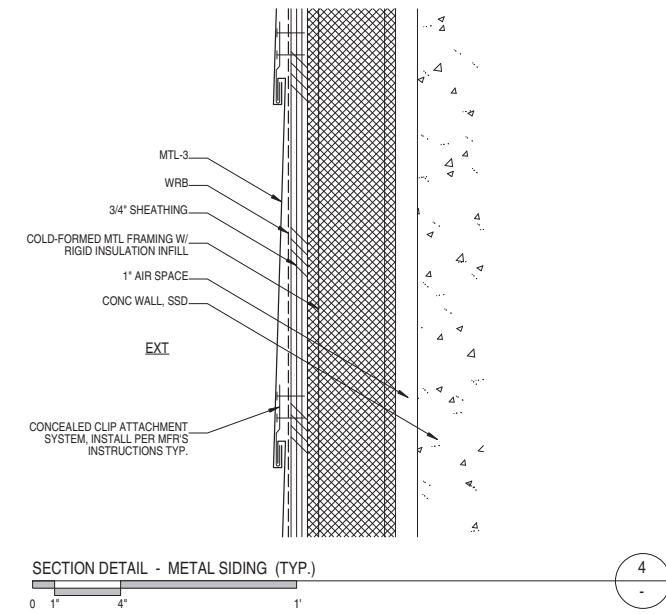
JOB: 0906

DRAWN: --

SCALE: AS NOTED

ISSUE	DATE
100% DD SET	06.03.15
C OF A SET	09.11.15

A9.6
 EXTERIOR DETAILS -
 NORTH WING





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SAN FRANCISCO, CA

DATE: 08.08.14

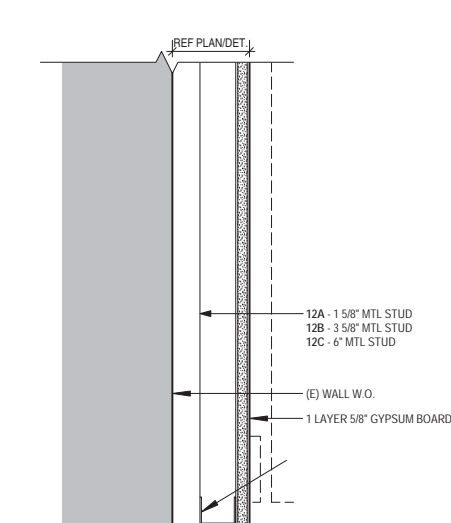
JOB: 0906

DRAWN: MH

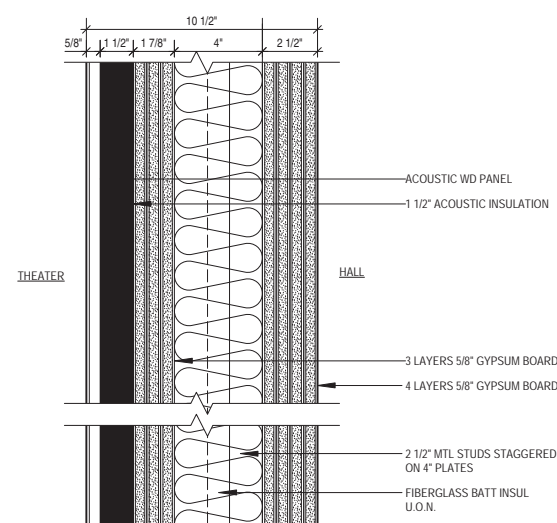
SCALE: AS NOTED

ISSUE	DATE
100% DD SET	06.03.15
C OF A SET	09.11.15

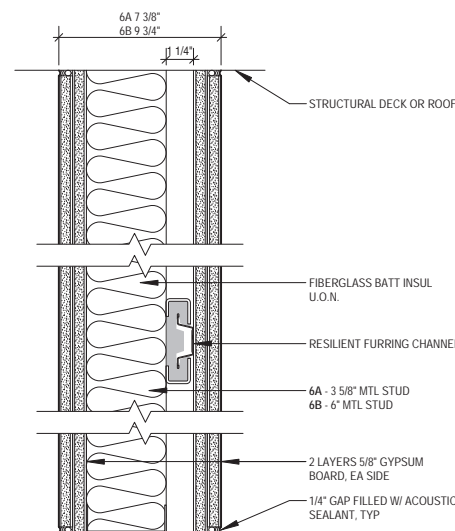
A10.1
PARTITION
SCHEDULE



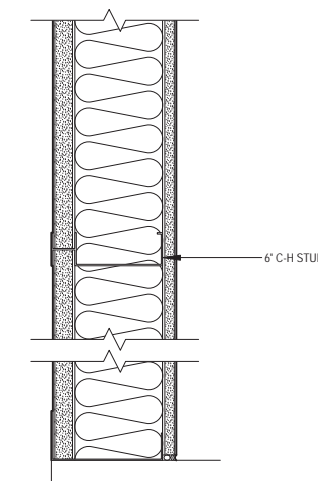
INT ACOUSTIC PARTITION



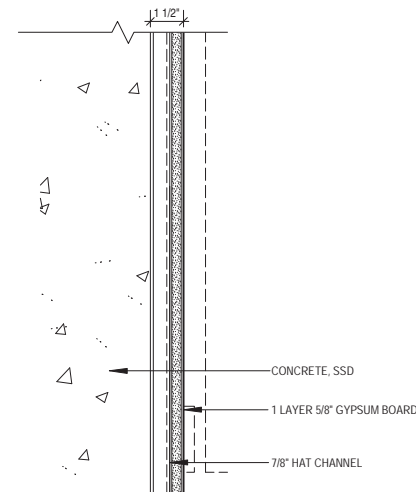
ACOUSTIC PARTITION BETWEEN THEATER/HALL



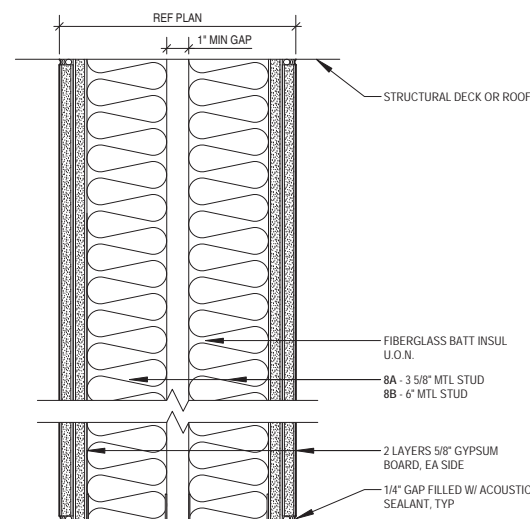
INT ACOUSTIC PARTITION



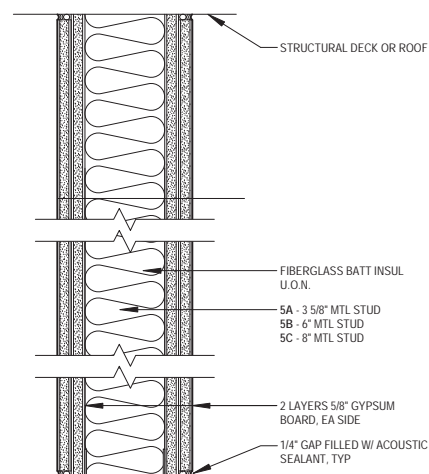
INT SHAFT WALL - 1 HR RATED



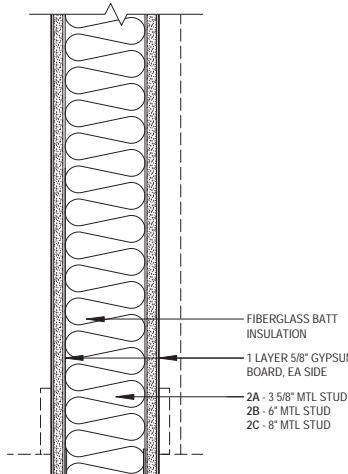
INT ACOUSTIC PARTITION



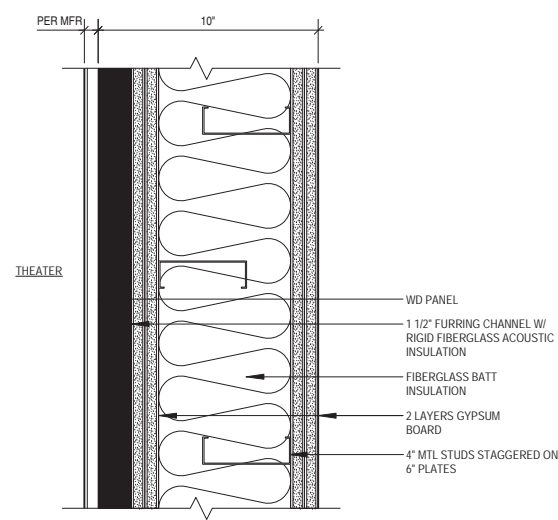
INT ACOUSTIC PARTITION



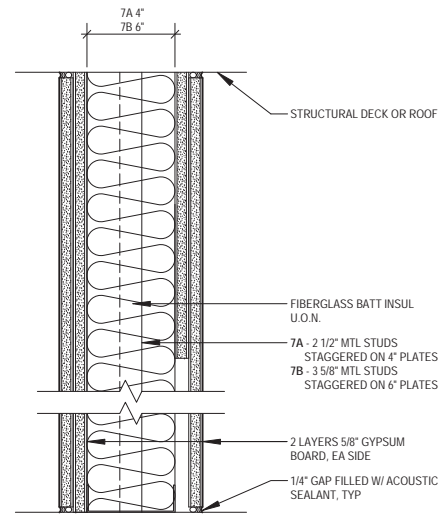
INT ACOUSTIC PARTITION



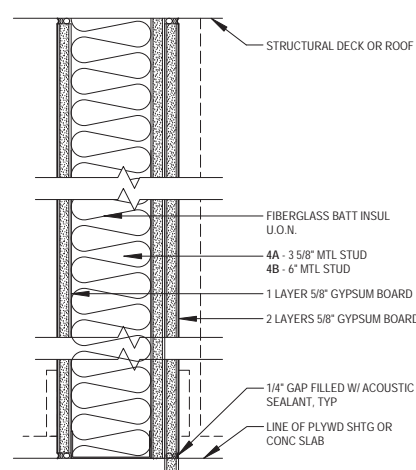
INT PARTITION - 1 HR RATED



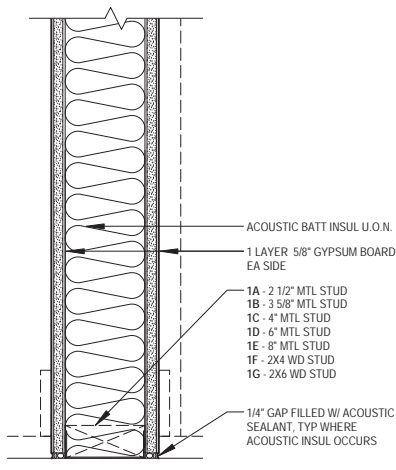
INT ACOUSTIC PARTITION



INT ACOUSTIC PARTITION



INT ACOUSTIC PARTITION



INT PARTITION - NR





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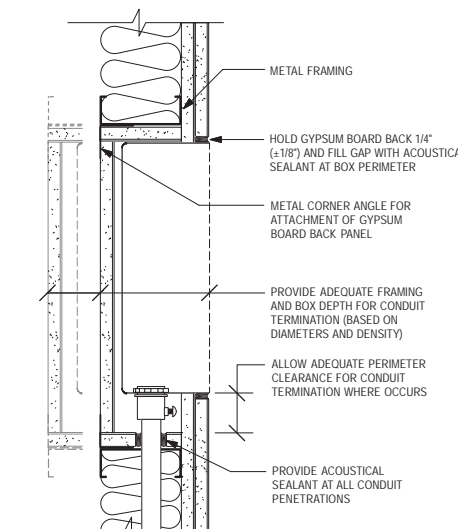
DATE: 08.08.14

JOB: 0906

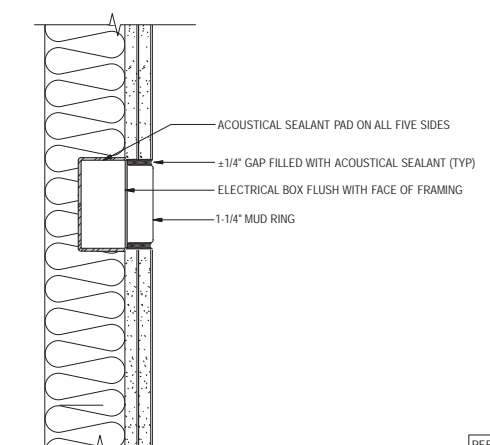
DRAWN: MH

SCALE: AS NOTED

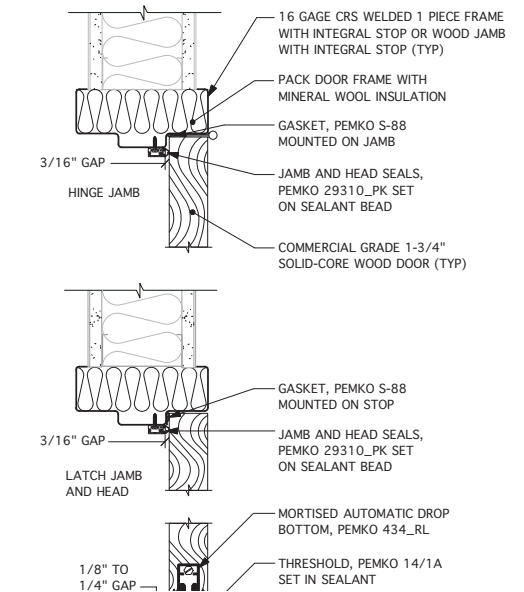
ISSUE	DATE
100% DD SET	06.03.15
C OF A SET	09.11.15



REF: CHARLES M. SALTER & ASSOCIATES DET. E3



REF: CHARLES M. SALTER & ASSOCIATES DET. E1



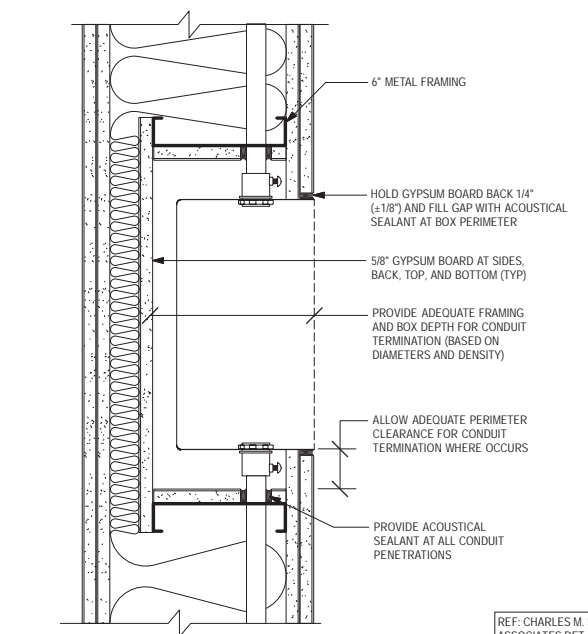
REF: CHARLES M. SALTER & ASSOCIATES DET. D2

RECESSED ELECTRICAL BOXES AT BOXES LARGER THAN 4' X 8' (ALTERNATE B) 11

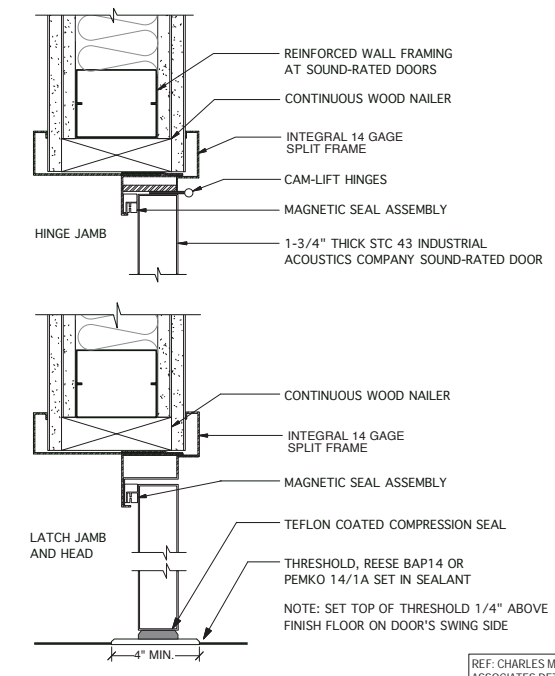
RECESSED ELECTRICAL BOX SEALANT PAD AT BOXES 4' X 8' AND SMALLER 8

TYPE "SG" SOUND-GASKETED WD DR 5

NOT USED 15

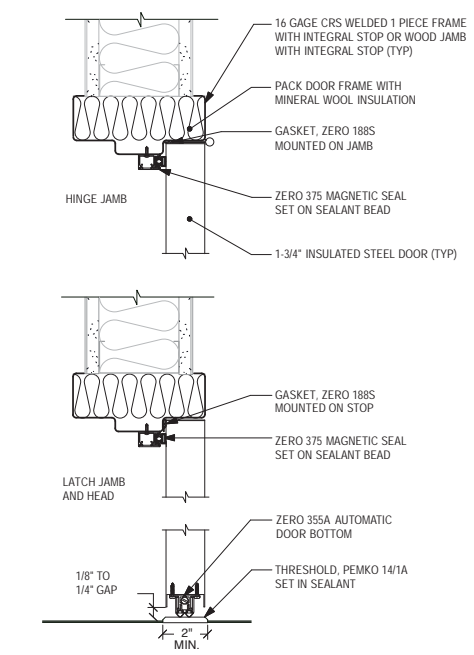


REF: CHARLES M. SALTER & ASSOCIATES DET. E2



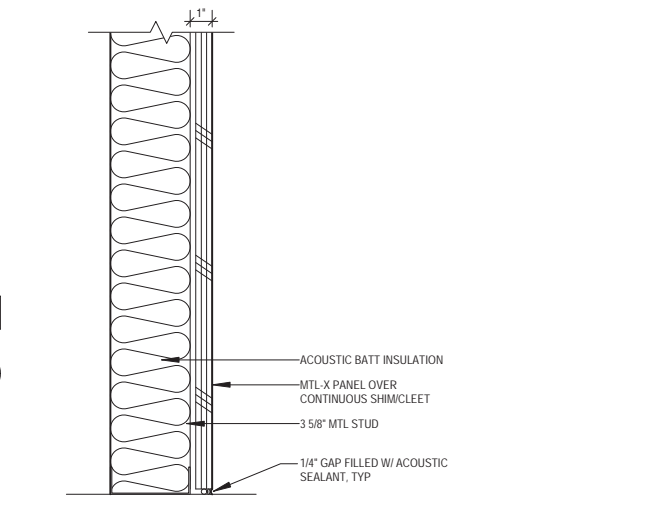
REF: CHARLES M. SALTER & ASSOCIATES DET. D3

STC 43 OR 47 SOUND-RATED DR 7

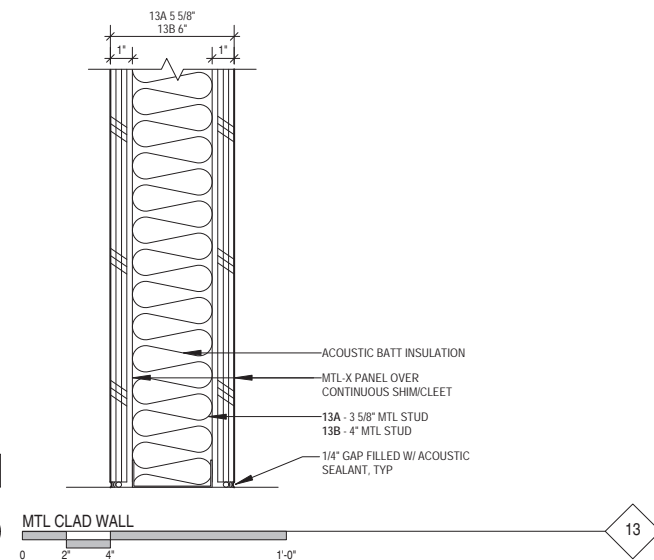


REF: CHARLES M. SALTER & ASSOCIATES DET. D1

TYPE "SG" SOUND-GASKETED MTL DR 4



MTL CLAD WALL 14



MTL CLAD WALL 13

A10.2
 PARTITION
 SCHEDULE /
 DETAILS



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DATE: 08.08.14

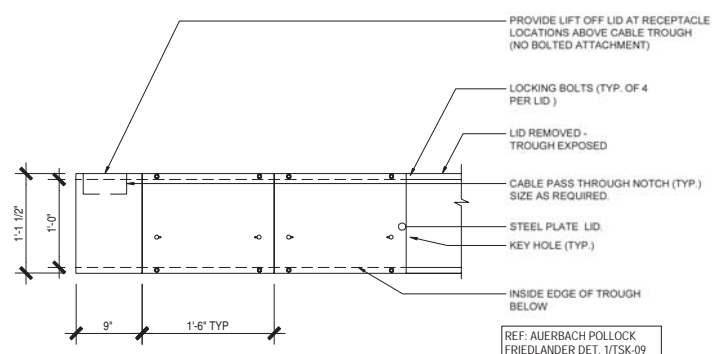
JOB: 0906

DRAWN: MH

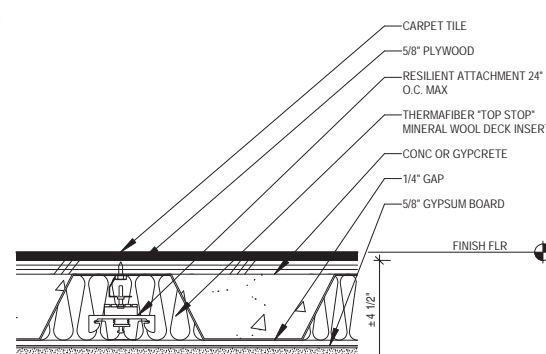
SCALE: AS NOTED

ISSUE	DATE
100% DD SET	06.03.15
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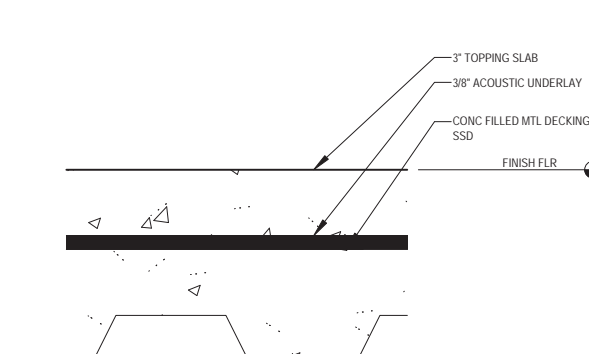
A10.3
 FLOOR FINISH
 ASSEMBLIES



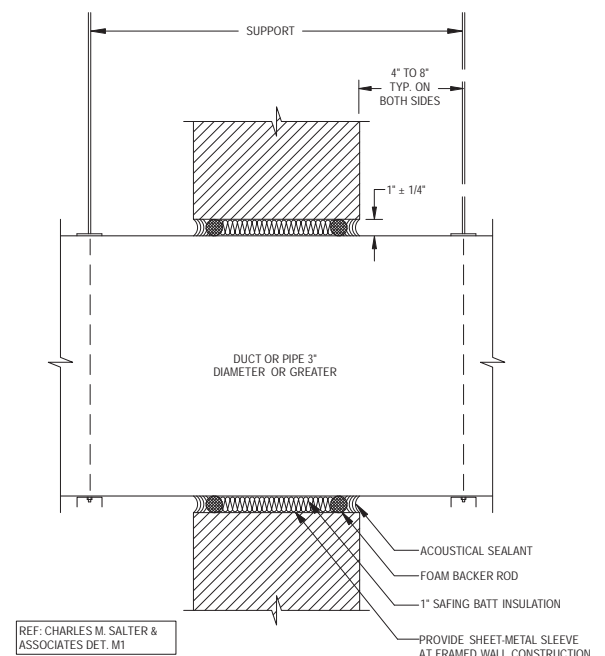
PLAN DET - POWERHOUSE CABLE TROUGH 9



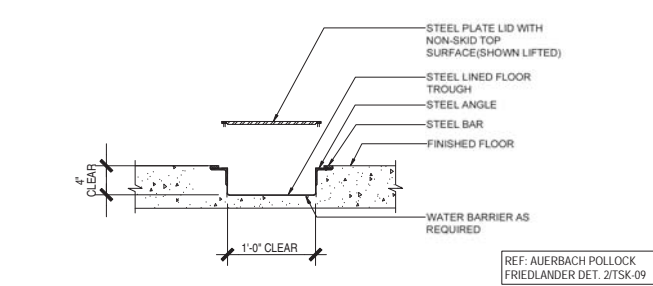
CONTROL BOOTH FLR 6



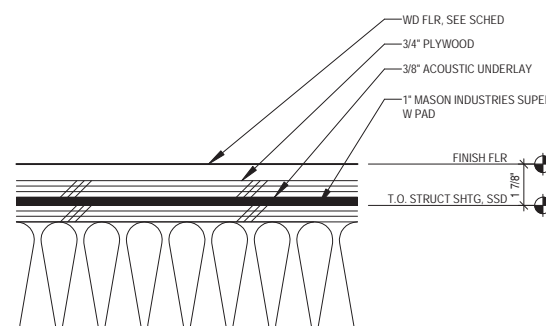
THEATER LOBBY FLR 3



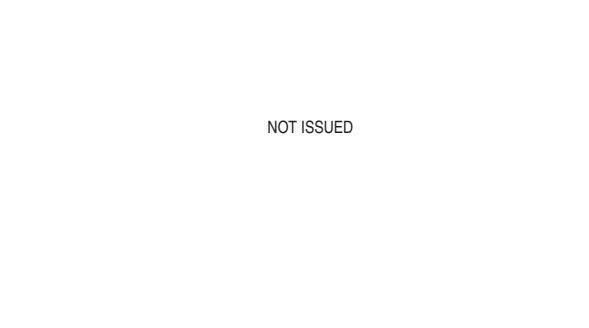
TYPICAL PENETRATIONS FOR DUCTS, PIPES, & CONDUIT GREATER THAN 3\"/>



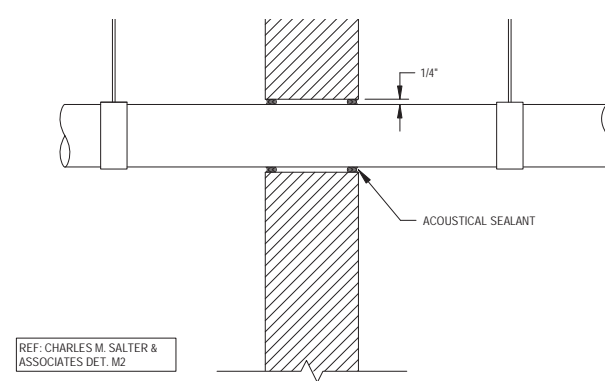
SECT DET - POWERHOUSE CABLE TROUGH 8



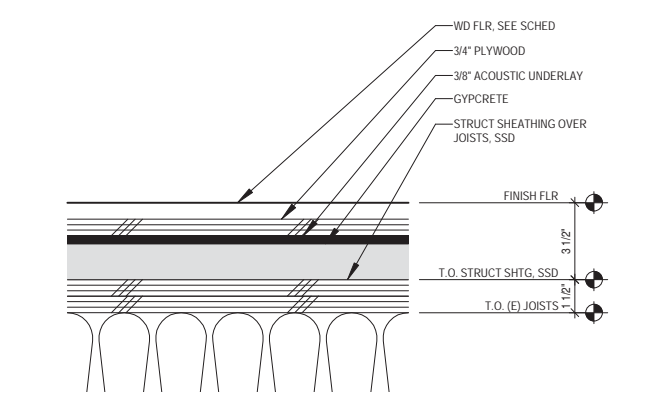
FLEXIBLE STUDIO FLR 5



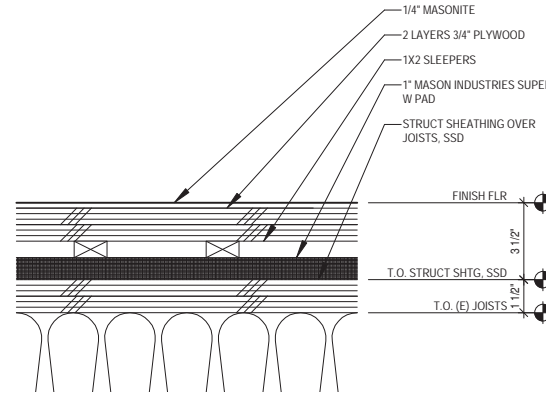
3RD FLR - OFFICE BLDG CENTER 2



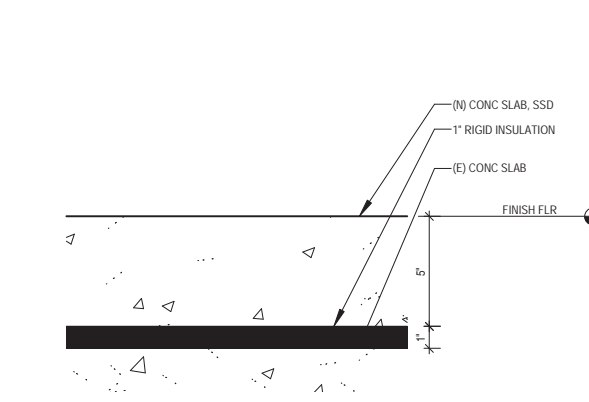
TYPICAL PENETRATIONS FOR DUCTS, PIPES, & CONDUIT 3\"/>



STUDIO CLASSROOMS & HALLWAY FLR 7



THEATER FLR 4



POWERHOUSE FLOOR 1

NOT ISSUED

REF: CHARLES M. SALTER & ASSOCIATES DET. M1

REF: AUERBACH POLLOCK FRIEDLANDER DET. 2/TSK-09

REF: CHARLES M. SALTER & ASSOCIATES DET. M2

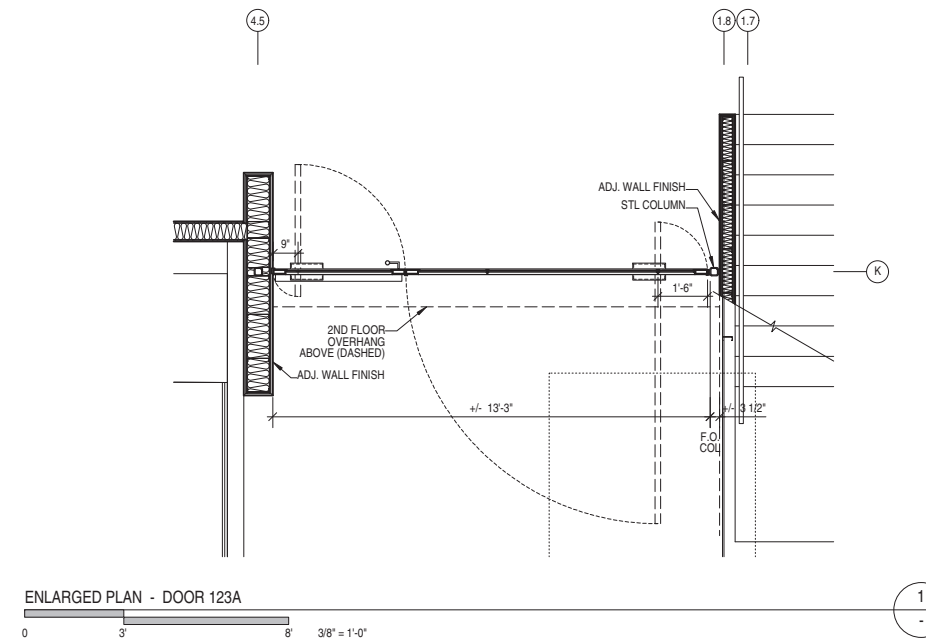
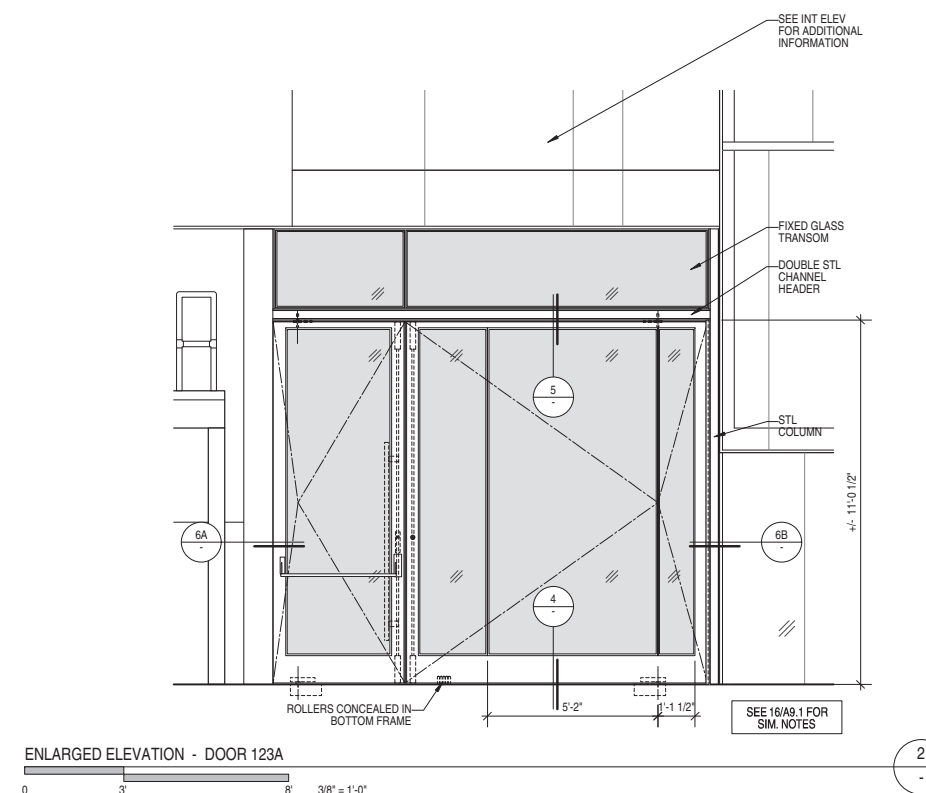
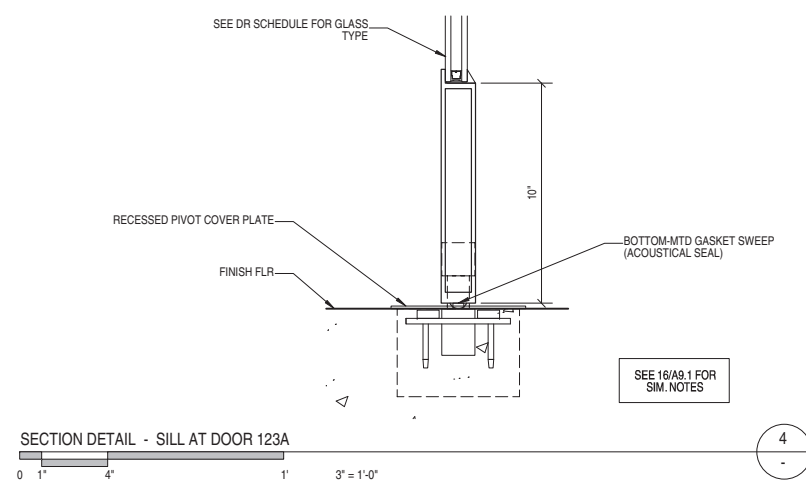
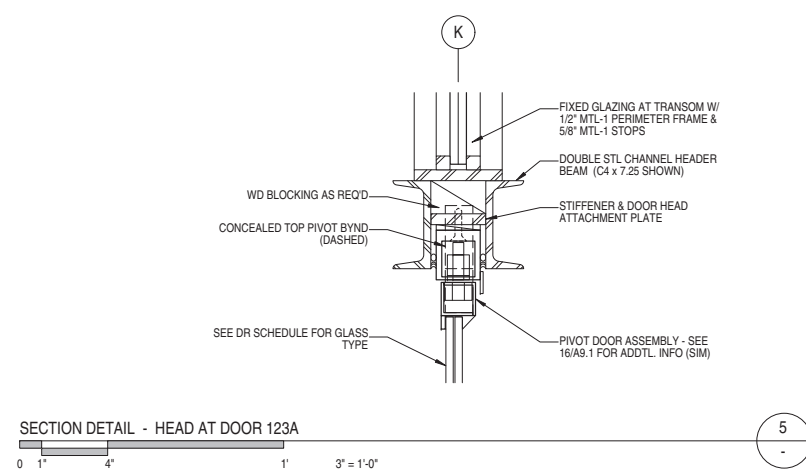
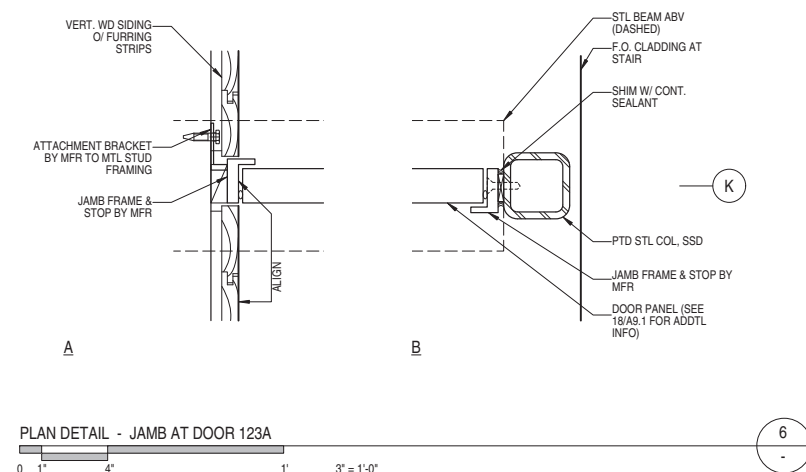


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GENEVA CAR BARN
AND POWERHOUSE

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SAN FRANCISCO, CA

DATE:	02.05.15
JOB:	0906
DRAWN:	--
SCALE:	AS NOTED
ISSUE	DATE
100% DD SET	06.03.15
C OF A SET	09.11.15



A10.5
INTERIOR DETAILS -
POWERHOUSE



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2301 SAN JOSE AVENUE
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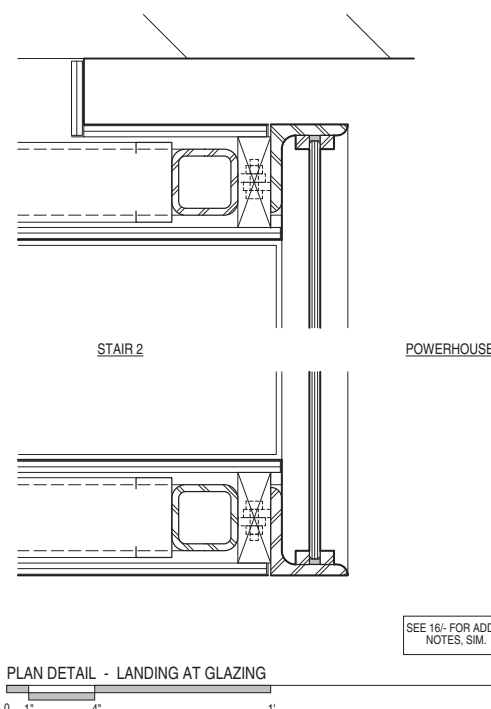
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SCALE: AS NOTED

ISSUE	DATE
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C OF A SET	09.11.15

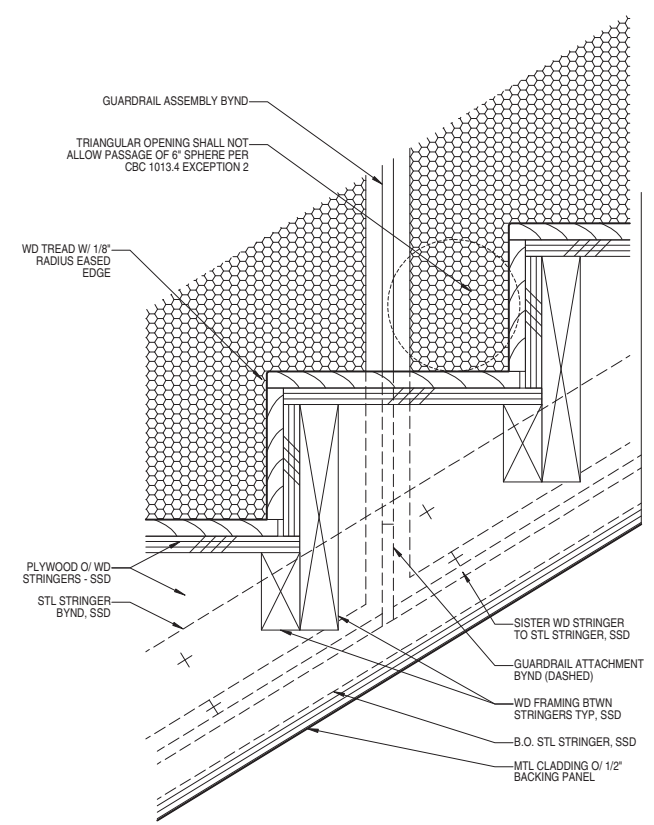
A10.6

INTERIOR DETAILS -
 POWERHOUSE
 STAIR 2



SEE 16- FOR ADDTL
 NOTES, SIM.

18



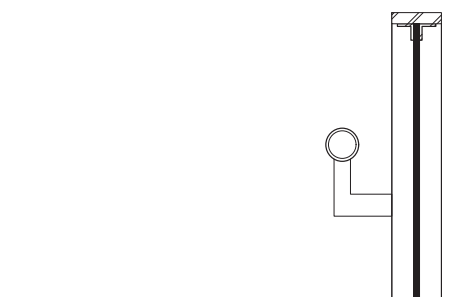
SECTION DETAIL - TYP TREAD & RISER

13



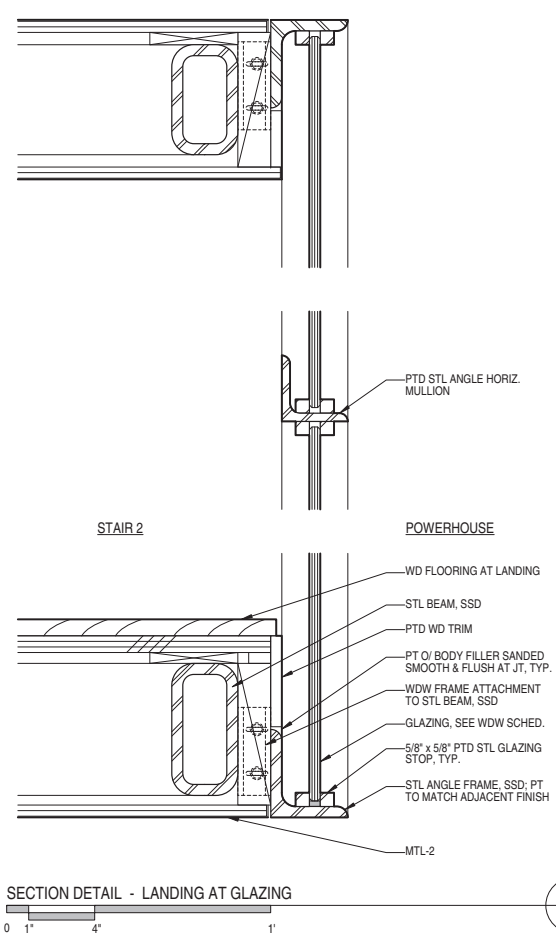
SECTION DETAIL - STAIR EDGE AT WALL

8



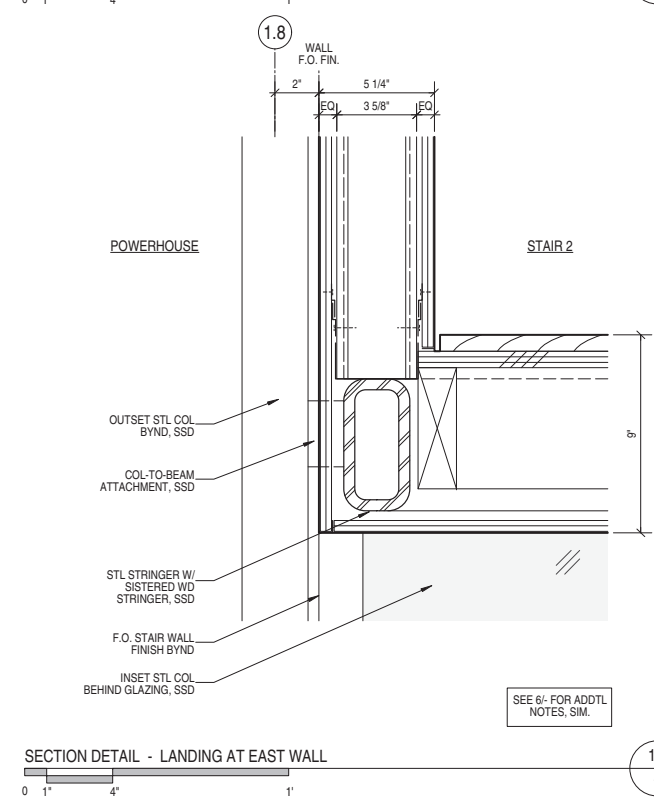
SECTION DETAIL - STAIR EDGE AT GUARDRAIL

3



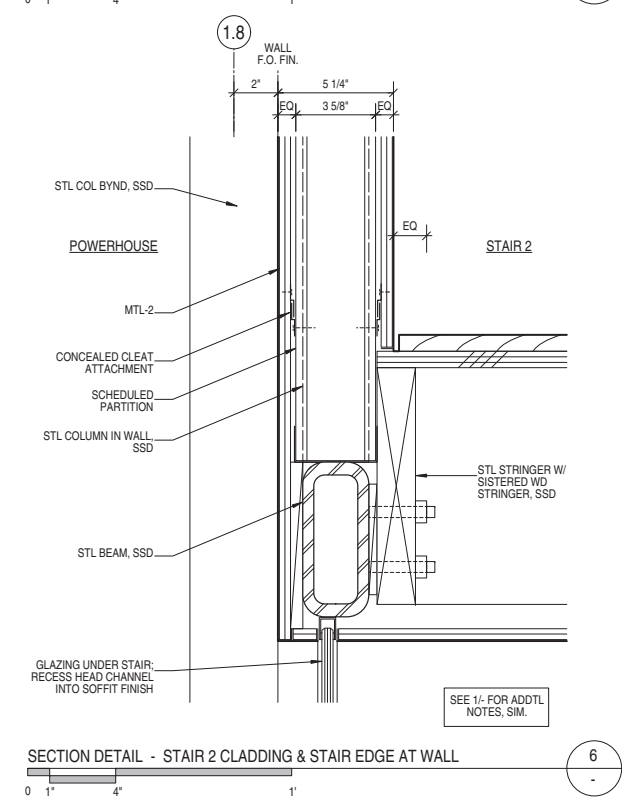
SECTION DETAIL - LANDING AT GLAZING

16



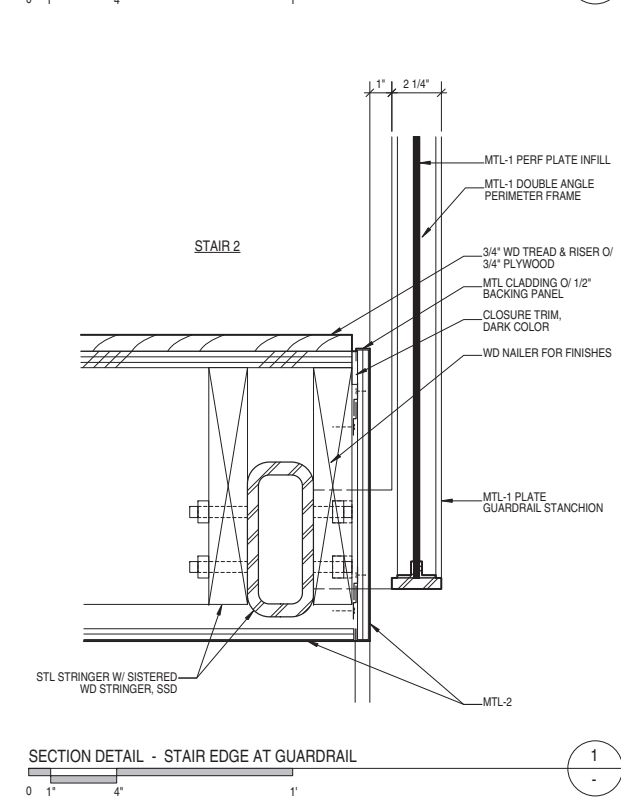
SECTION DETAIL - LANDING AT EAST WALL

11



SECTION DETAIL - STAIR 2 CLADDING & STAIR EDGE AT WALL

6

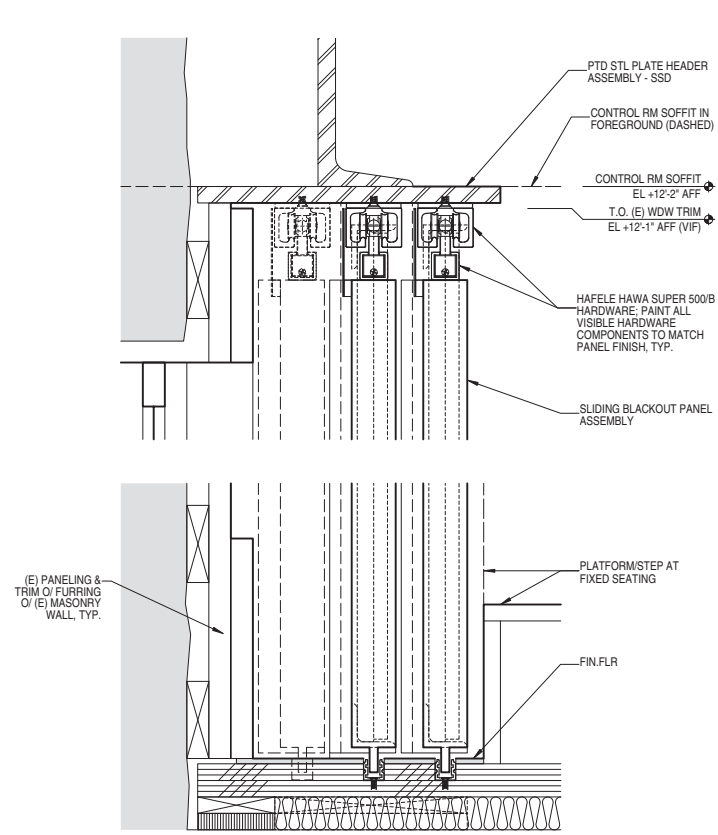
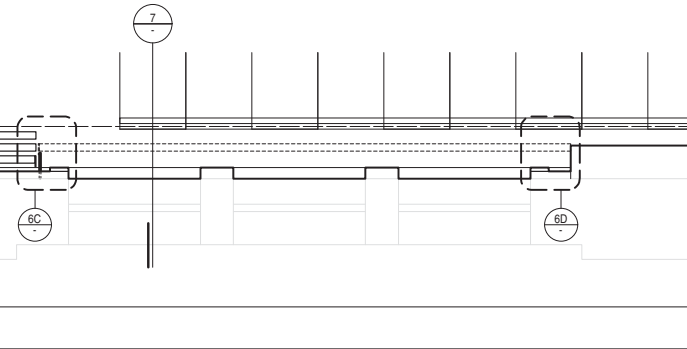
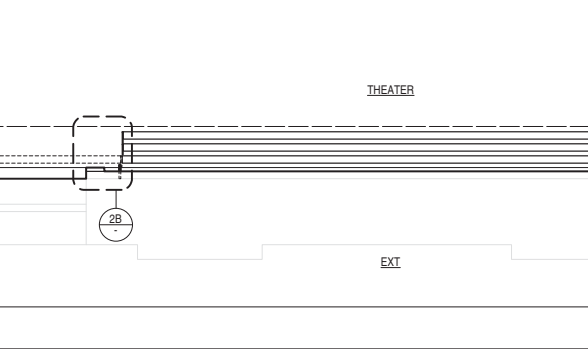
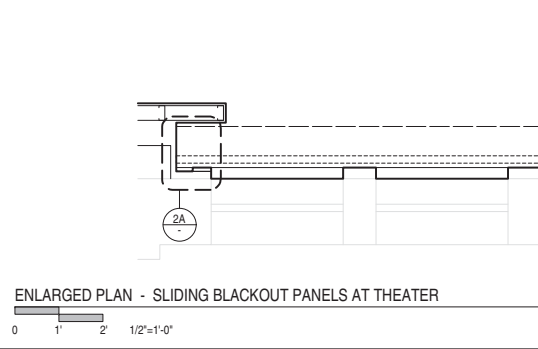
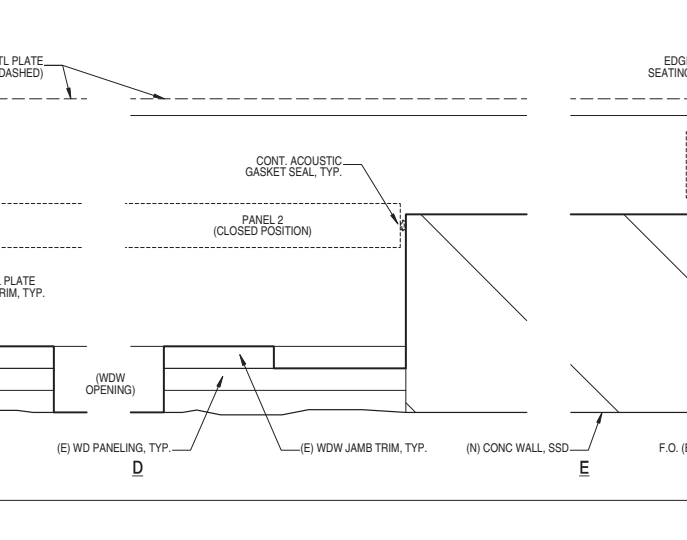
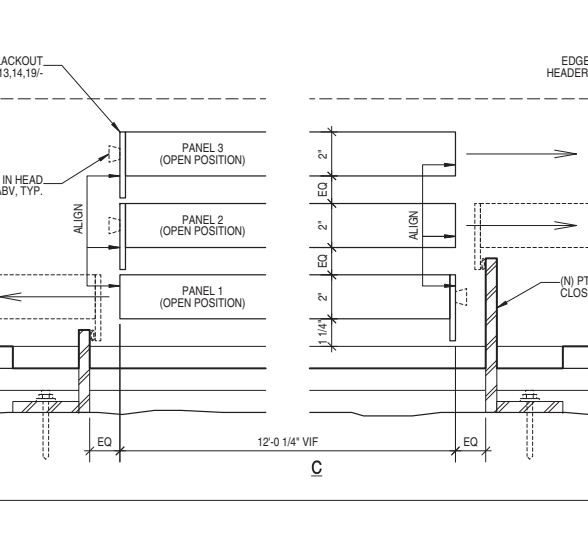
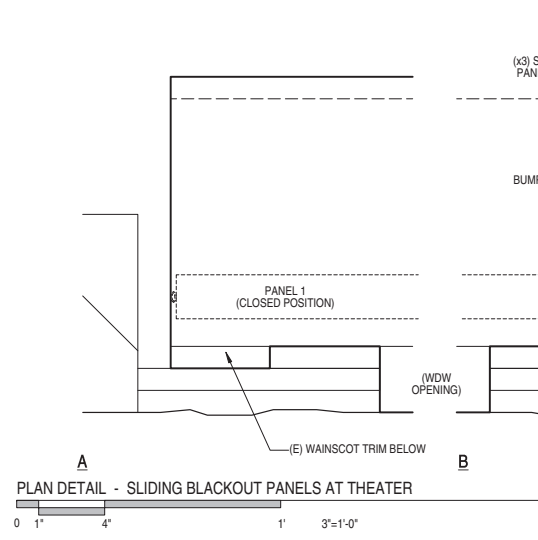
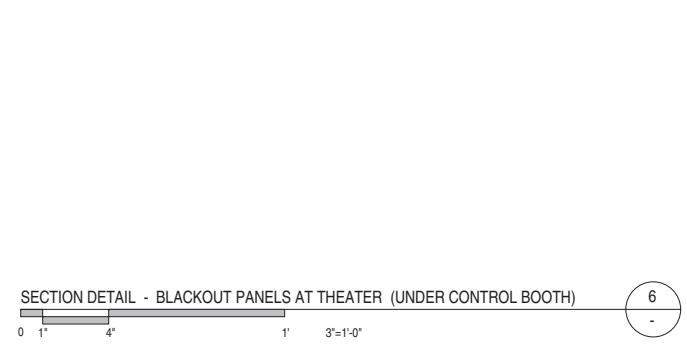
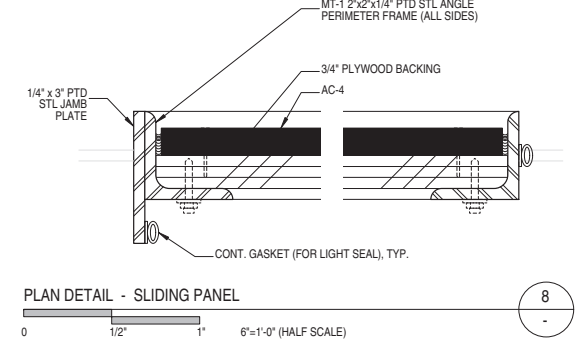
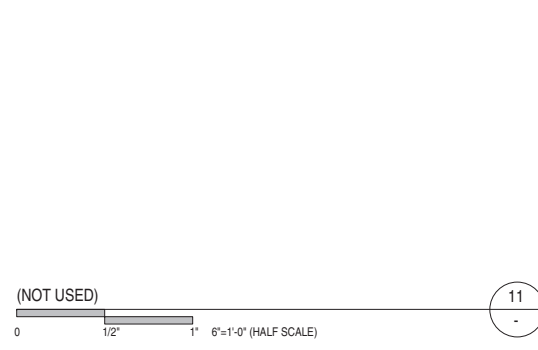
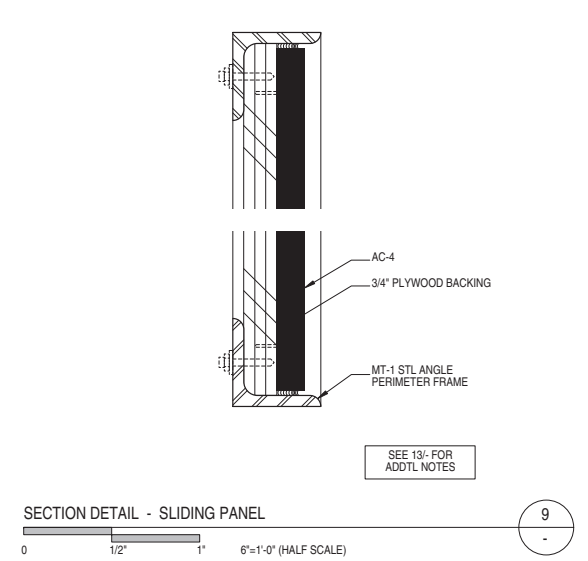
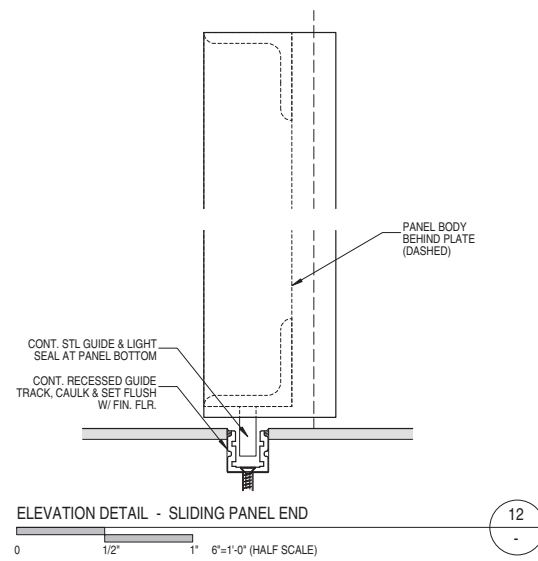


SECTION DETAIL - STAIR EDGE AT GUARDRAIL

1

SEE 6- FOR ADDTL
 NOTES, SIM.

SEE 1- FOR ADDTL
 NOTES, SIM.



NOT ISSUED



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GENEVA CAR BARN AND POWERHOUSE

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SAN FRANCISCO, CA

DATE:	02.05.15
JOB:	0906
DRAWN:	--
SCALE:	AS NOTED
ISSUE	DATE
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A10.10
INTERIOR DETAILS -
THEATER SLIDING
PANELS



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 SAN FRANCISCO, CA

DATE: 08.08.14

JOB: 0906

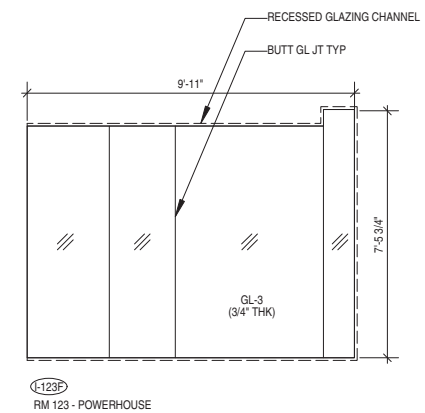
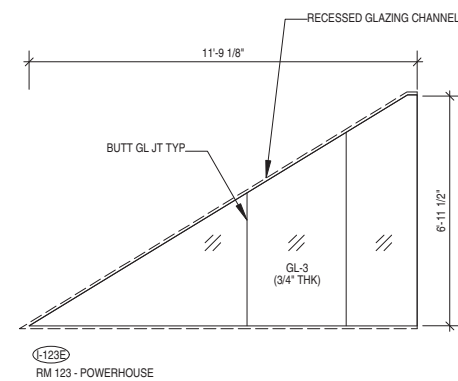
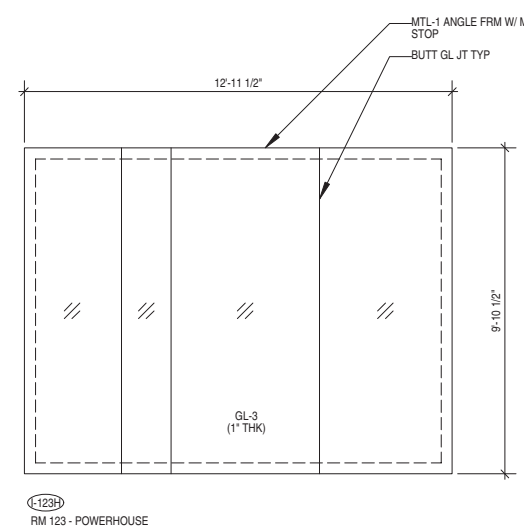
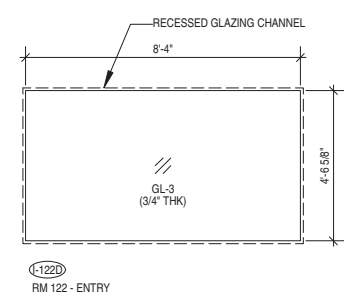
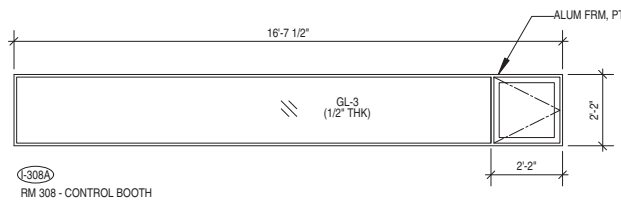
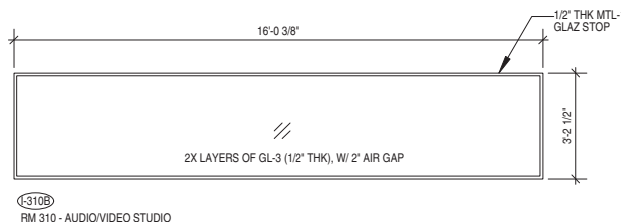
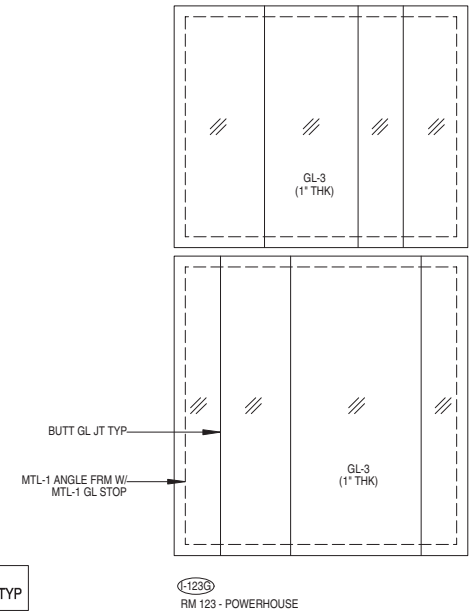
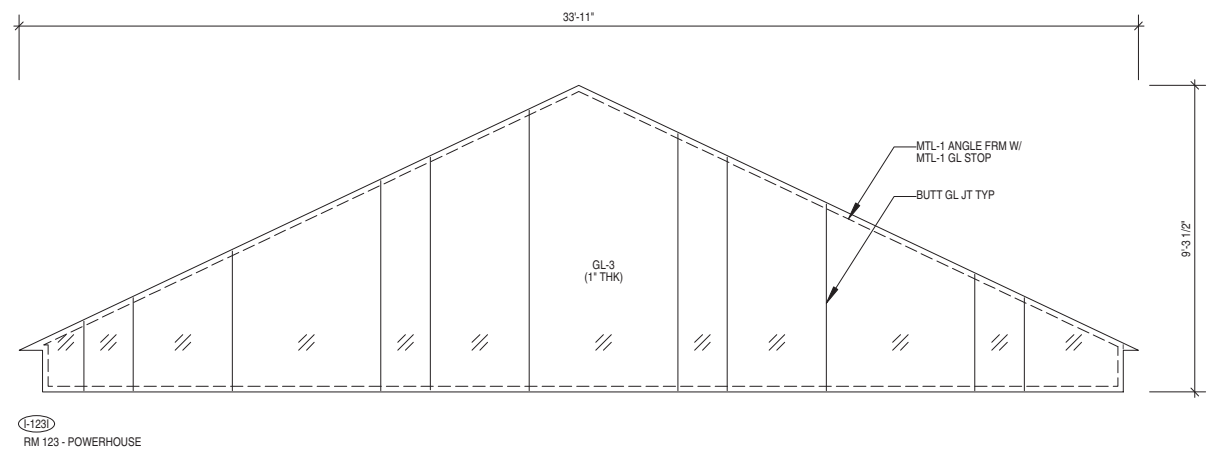
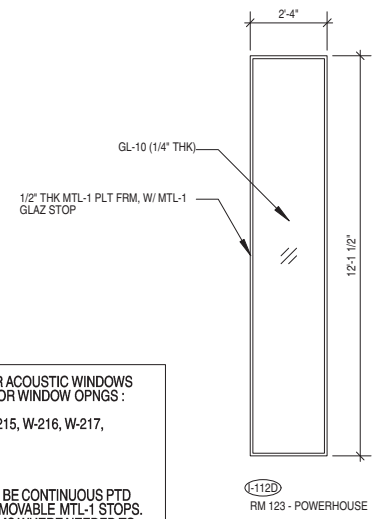
DRAWN: MH

SCALE: AS NOTED

ISSUE	DATE
100% DD	06.03.15
C OF A SET	09.11.15

PROVIDE ADDITIONAL INTERIOR ACOUSTIC WINDOWS AT THE FOLLOWING (E) EXTERIOR WINDOW OPNGS:
 W-110, W-111, W-112, W-113,
 W-211, W-212, W-213, W-214, W-215, W-216, W-217,
 W-205, W-206, W-207,
 S-101, S-102,
 S-201, S-202

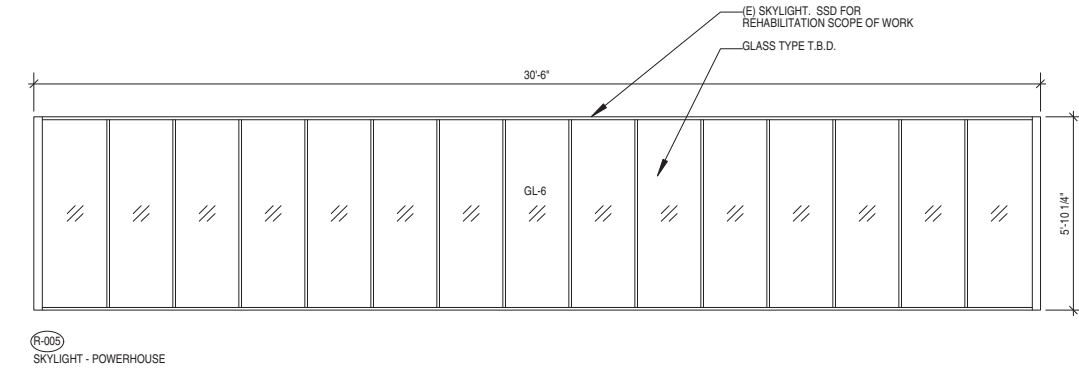
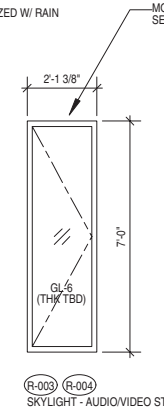
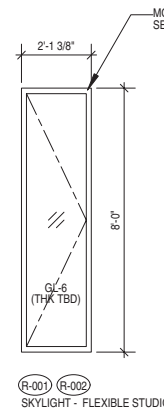
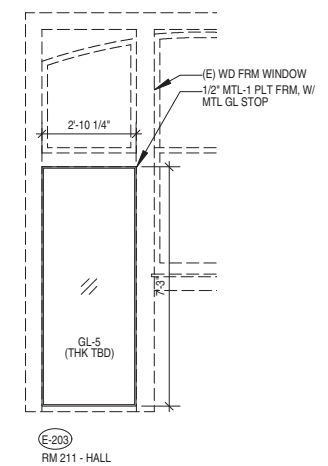
INTERIOR WINDOW FRAMES TO BE CONTINUOUS PTD.
 MTL-1 ANGLE FRAMES WITH REMOVABLE MTL-1 STOPS.
 GLAZING TO BE 1/2" GL-10. SEAMS WHERE NEEDED TO
 BE LOCATED AT (E) VERTICAL WINDOW MULLIONS.



NOTE: ALL DIMENSIONS ARE APPROXIMATE, TO BE CONFIRMED, TYP

NOTE:
 - ALL DIMENSIONS ARE APPROXIMATE, TO BE CONFIRMED, TYP
 - REFER TO EXTERIOR BUILDING ELEVATION AND HISTORIC ARCHITECTURAL DWGS FOR INFORMATION REGARDING EXISTING EXTERIOR WINDOWS

INTERIOR WINDOW SCHEDULE



EXTERIOR WINDOW SCHEDULE



A11.3
 WINDOW SCHEDULES

WINDOW TYPES

SHEET NOTES

- A. SEE SHEET HA2.1 FOR WINDOW TYPES.
- B. SEE FLOOR PLANS FOR ADDITIONAL INFORMATION.
- C. SEE EXTERIOR ELEVATIONS FOR ADDITIONAL INFORMATION AND WINDOW LOCATIONS.
- D. SEE SPECIFICATION SECTION 08 55 00 - WOOD WINDOW RESTORATION, FOR ADDITIONAL INFORMATION.
- E. THE EXTERIOR SIDE OF WINDOW SASH WAS COVERED WITH PLYWOOD AND WERE NOT SURVEYED, TYPICAL.



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CAREY & CO. INC.
 ARCHITECTURE
 Old Engine Co. No. 2
 460 Bush Street
 San Francisco, CA 94108
 415.773.0773 f. 415.773.1773

WINDOW REPAIR NOTES

WINDOW REPAIR NOTES APPLY TO ALL EXTERIOR WINDOWS AND DESCRIBE SCOPE OF WORK.

1. THE GENERAL CONTRACTOR IS RESPONSIBLE FOR FIELD VERIFYING ALL INFORMATION SHOWN ON WINDOW REPAIR SCHEDULE. THE GENERAL CONTRACTOR SHALL NOTIFY ARCHITECT OF ANY DISCREPANCIES, PRIOR TO COMMENCING WORK.
2. REMOVE ALL BOARDS COVERING WINDOW OPENINGS AND ALL FASTENERS. FILL ALL FASTENER HOLES IN WOOD WINDOW FRAME AND / OR SASH, TYPICAL.
3. REMOVE ABANDONED ELECTRICAL CONDUIT, BOXES, DEVICES AND FASTENERS FROM WINDOW FRAMES, WHERE OCCURS. FILL ALL RESULTING FASTENER HOLES IN WOOD, TYPICAL.

REMOVE ABANDONED PLUMBING PIPING AND FASTENERS FROM WINDOW FRAMES, WHERE OCCURS. FILL ALL RESULTING FASTENER HOLES IN WOOD, TYPICAL.

4. LABEL AND CATALOG ALL WINDOW SASH, REMOVE FROM WINDOW FRAME. DISCARD SASH CORDS, STORE SASH AND SHOP REPAIR, TYPICAL.
5. REMOVE ALL ORIGINAL GLASS IN EXTERIOR WINDOW SASH AND GLAZING PUTTY, TYPICAL. APPROXIMATELY 50% OF ORIGINAL GLASS IS EXTANT.
6. REMOVE ALL EXISTING WINDOW HARDWARE AND SALVAGE FOR OWNER. LABEL AND CATALOG WINDOW HARDWARE WITH WINDOW NUMBER.
7. REPAIR WINDOW SASH AND MUNTINS, WHERE OCCURS, AS SCHEDULED AND PREPARE FOR PAINTING OR STAINING. REPAIRED WINDOW SASH MAY BE SHOP PRIMED, EXCEPT WHERE INTERIOR STAIN FINISH IS SCHEDULED. DO NOT DIP STRIP STAINED WOOD WINDOW SASH OR SHOP PRIME WOOD WHERE SCHEDULED TO BE STAINED.
8. REPAIR WINDOW FRAME, INTERIOR & EXTERIOR TRIM, STOOL, SILL, STOPS AND PARTING BEADS. CHEMICAL STRIP EXISTING PAINTED FINISH IN THE FIELD, AS SPECIFIED.
9. PREPARE WINDOW FRAME, INTERIOR & EXTERIOR TRIM, STOOL, SILL & STOPS FOR PAINTING OR STAINING.
10. REINSTALL REPAIRED WINDOW SASH TO ORIGINAL OPENING.
11. PROVIDE AND INSTALL NEW WINDOW HARDWARE AT OPERABLE WINDOWS, AS SCHEDULED & SPECIFIED.
12. RESTORE ORIGINAL WINDOW HARDWARE AND REINSTALL, AS SCHEDULED.

GENEVA CAR BARN AND POWERHOUSE

2301 SAN JOSE AVENUE
 SAN FRANCISCO, CA

DATE: 06.03.15

JOB: 14018

DRAWN: SS

SCALE: AS NOTED

ISSUE	DATE
50% DD SET	03.09.15
100% DD SET	06.03.15

HA2.1

WINDOW TYPES



TYPE A
DOUBLE HUNG - RADIUS SASH & GLASS

TYPE B
FIXED TRANSOM

TYPE C
DOUBLE HUNG

TYPE D
DOUBLE HUNG - BAY WINDOW

TYPE E
FIXED TRANSOM

TYPE F
DOUBLE HUNG

TYPE G
DOUBLE HUNG

TYPE H
DOUBLE HUNG

TYPE I
DOUBLE HUNG

TYPE J
FIXED

TYPE K
DOUBLE HUNG

TYPE L
DOUBLE HUNG

TYPE M
DOUBLE HUNG

TYPE N
DOUBLE HUNG

TYPE O
DOUBLE HUNG

TYPE P
DOUBLE HUNG

TYPE Q
FIXED TRANSOM & SIDELIGHTS

TYPE R
DOUBLE HUNG

TYPE S
DOUBLE HUNG

TYPE T
DOUBLE HUNG

TYPE U
DOUBLE HUNG

TYPE V
DOUBLE HUNG

TYPE W
DOUBLE HUNG

TYPE X
DOUBLE HUNG

WINDOW REPAIR SCHEDULE

BASEMENT LEVEL						WINDOW GLAZING			WINDOW SASH (INTERIOR)			INTERIOR FRAME				EXTERIOR FRAME				WINDOW	NOTES:
WINDOW NUMBER	WINDOW TYPE	MATERIAL	STYLE	WINDOW SIZE W X H	INTERIOR FINISH	GLASS TYPE	TEMPERED GLASS	GLAZING PUTTY	UPPER SASH	LOWER SASH	MUNTINS	LEFT JAMB	RIGHT JAMB	WINDOW HEAD	WINDOW STOOL	LEFT JAMB	RIGHT JAMB	WINDOW HEAD	WINDOW SILL	HARDWARE GROUP	
E001a	S	WOOD	DBL HUNG	2'-9" X 4'-4"	PT	N3	-	N4	-	G1	-	D5, F, G1, L2	D5, F, G1, L2	F, G1, L2	NS	F, G1	F, G1	G1, R1	NS	-	WINDOW EXTERIOR COVERED BY PLYWOOD.
E001b	S	WOOD	DBL HUNG	2'-9" X 4'-4"	PT	N3	-	N4	-	G1	-	D5, F, G1, L2	D5, F, G1, L2	F, G1, L2	NS	F, G1	F, G1	G1, R1	NS	-	WINDOW EXTERIOR COVERED BY PLYWOOD.
E001c	S	WOOD	DBL HUNG	2'-9" X 4'-4"	PT	N3	-	N4	-	-	-	D5, F, G1, L2	D5, F, G1, L2	F, G1, L2	NS	F, G1	F, G1	G1, R1	NS	-	WINDOW EXTERIOR COVERED BY PLYWOOD.
E002	T	WOOD	DBL HUNG	2'-11" X 4'-4"	PT	N3	-	N4	NS	F, G1	NS	F, G1	F, G1	F	-	G1	G1	R1	NS	-	WINDOW EXTERIOR COVERED BY PLYWOOD.
E003	U	WOOD	DBL HUNG	2'-10" X 4'-4"	PT	N3	-	N4	-	-	-	NS	E, F, G1	-	-	F	F	E	NS	-	WINDOW EXTERIOR COVERED BY PLYWOOD.
E005	U	WOOD	DBL HUNG	2'-10" X 4'-4"	PT	N3	-	N4	NS	-	-	E, F	E, J1	E, F	-	F	F	E, F	NS	-	WINDOW EXTERIOR COVERED BY PLYWOOD.
E006a	V	WOOD	DBL HUNG	2'-10" X 4'-4"	PT	N3	YES	N4	NS	NS	NS	NS	NS	NS	NS	D2, NO	D2, NO	D5, M1	NS	-	WINDOW EXTERIOR COVERED BY PLYWOOD.
E006b	V	WOOD	DBL HUNG	2'-10" X 4'-4"	PT	N3	-	N4	NS	NS	NS	NS	NS	NS	NS	D2, NO	D2, NO	D2, NO	NS	-	WINDOW EXTERIOR COVERED BY PLYWOOD.
E007	W	WOOD	DBL HUNG	2'-11" X 4'-0"	PT	N3	YES	N4	NS	NS	NS	D5, F, G1	D5, F, G1	D5, G1	D5	D5, F	F	D5, E, F, R1	NS	-	WINDOW EXTERIOR COVERED BY PLYWOOD.
E008a	X	WOOD	DBL HUNG	2'-11" X 4'-0"	PT	N3	-	N4	E	NS	NS	D5, F, G1	D5, F, G1	D5, F, G1	D5, F, G1	E, F	F	E, F, R1	F	-	WINDOW EXTERIOR COVERED BY PLYWOOD.
E008b	X	WOOD	DBL HUNG	2'-11" X 4'-0"	PT	N3	-	N4	NS	NS	NS	D5, F, G1	D5, F, G1	D5, F, G1	D5, F, G1	E, F	F	F	-	-	WINDOW EXTERIOR COVERED BY PLYWOOD.

INTERIOR WINDOW SCHEDULE						WINDOW GLAZING			WINDOW SASH			FRAME - INTERIOR SIDE				FRAME - EXTERIOR SIDE				WINDOW	NOTES:
WINDOW NUMBER	WINDOW TYPE	MATERIAL	STYLE	WINDOW SIZE W X H	INTERIOR FINISH	GLASS TYPE	TEMPERED GLASS	GLAZING PUTTY	SASH		MUNTINS	LEFT JAMB	RIGHT JAMB	WINDOW HEAD	WINDOW STOOL	LEFT JAMB	RIGHT JAMB	WINDOW HEAD	WINDOW STOOL	HARDWARE GROUP	
I-117C																					-
I-117D																					-
I-118B																					-
																					-
																					-

SHEET NOTES

- A. SEE SHEET HA2.1 FOR WINDOW TYPES.
- B. SEE FLOOR PLANS FOR ADDITIONAL INFORMATION.
- C. SEE EXTERIOR ELEVATIONS FOR ADDITIONAL INFORMATION AND WINDOW LOCATIONS.
- D. SEE SPECIFICATION SECTION 08 55 00 - WOOD WINDOW RESTORATION, FOR ADDITIONAL INFORMATION.
- E. THE EXTERIOR SIDE OF WINDOW SASH WAS COVERED WITH PLYWOOD AND WAS NOT SURVEYED, TYPICAL.

REPAIR KEY NOTES

- B CLEAN BIOLOGICAL GROWTH OFF WOOD.
- C1 CLEAN MINOR INSECT INFESTATION.
- C2 CLEAN BIRD DROPPINGS / DEBRIS.
- D1 REPLACE DAMAGED WOOD TRIM, BEYOND REPAIR.
- D2 REPLACE DAMAGED WOOD STOP, BEYOND REPAIR.
- D3 DRY-ROT DAMAGE, REPLACE WOOD TRIM.
- D4 DRY-ROT DAMAGE, REPLACE WOOD STOP.
- D5 REPAIR DAMAGED WOOD WINDOW FRAME.
- E REMOVE ELECTRICAL DEVICE AND PATCH HOLE(S), TYP.
- F REMOVE METAL FASTENER(S) IN WOOD ELEMENT.
- G1 DUTCHMAN PATCH GOUGE IN WOOD TRIM.
- G2 DUTCHMAN PATCH GOUGE IN WOOD STOP.
- H1 PATCH SMALL HOLE IN WOOD SASH OR TRIM.
- H2 PATCH SMALL HOLE IN WOOD STOP.
- J1 REPAIR OPEN JOINT IN WOOD SASH OR TRIM.
- J2 REPAIR OPEN JOINT IN WOOD STOP.
- L1 REATTACH LOOSE WOOD TRIM.
- L2 REATTACH LOOSE WOOD STOP.
- M1 REPLACE MISSING WOOD TRIM.
- M2 REPLACE MISSING WOOD STOP.
- N1 PROVIDE AND INSTALL NEW WINDOW SASH.
- N2 PROVIDE AND INSTALL NEW WINDOW HARDWARE, AS SPECIFIED. N2 - (HARDWARE GROUP NUMBER).
- N3 PROVIDE & INSTALL NEW WINDOW GLAZING, AS SPECIFIED.
- N4 PROVIDE & INSTALL NEW GLAZING PUTTY, AS SPECIFIED.
- N5 PROVIDE & INSTALL NEW WOOD WINDOW STOOL.
- NO REMOVE NON-ORIGINAL ELEMENT.
- NS NOT SURVEYED, COVERED OR INACCESSIBLE.
- P REMOVE PLUMBING PIPING AND FASTENERS, TYPICAL.
- PB REPLACE DAMAGED OR MISSING WOOD PARTING BEAD.
- PT PAINTED WOOD FINISH, AS SPECIFIED.
- R1 CLEAN RUST OFF WINDOW LINTEL AT EXTERIOR.
- R2 REMOVE CORRODED METAL WINDOW HARDWARE.
- R3 REMOVE DRAPERY HARDWARE AND DISCARD.
- S1 REMOVE NON-ORIGINAL WINDOW SASH AND DISCARD.
- S2 REPAIR DAMAGED WOOD WINDOW SASH.
- ST STAINED WOOD FINISH, AS SPECIFIED.



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 460 Bush Street
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GENEVA CAR BARN AND POWERHOUSE

2301 SAN JOSE AVENUE
 SAN FRANCISCO, CA

DATE: 06.03.15

JOB: 14018

DRAWN: SS

SCALE: AS NOTED

ISSUE	DATE
50 % DD SET	03.09.15
100 % DD SET	06.03.15

HA2.4

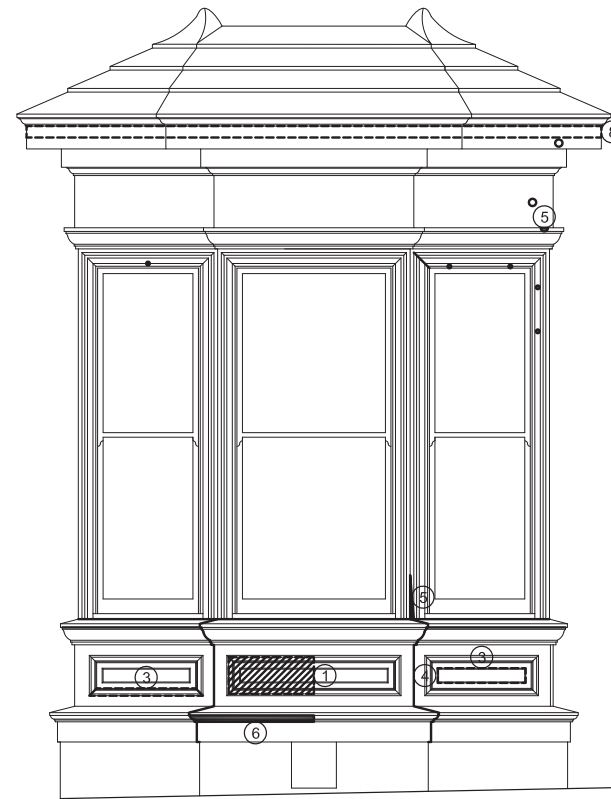
WINDOW REPAIR SCHEDULE



W 105



W 106



W 108



W 109

1 BAY WINDOW ELEVATIONS
SCALE: 1/2" = 1'-0"



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LEGEND

- FASTENER ATTACHED TO WOOD TRIM
- HOLE IN WOOD TRIM
- SEPARATION IN WOOD JOINT OR CRACK
- MISSING WOOD TRIM
- ◐ MISSING AREA OF DAMAGED WOOD TRIM
- - - LOOSE OR DAMAGED WOOD TRIM
- ▨ OBJECT ATTACHED TO WOOD TRIM

KEY NOTES

1. REMOVE SIGN AND WOOD BLOCKING ATTACHED TO WOOD TRIM, REPAIR SURFACE AND PATCH HOLES.
2. REMOVE FASTENER OR ELECTRICAL INSULATOR IN WOOD TRIM AND PATCH HOLE(S).
3. REATTACH LOOSE WOOD TRIM, TYPICAL.
4. FILL GAPS BETWEEN WOOD TRIM WHERE DIFFERENTIAL MOVEMENT HAS OPENED WOOD JOINTS, TYPICAL.
5. DUTCHMAN PATCH HOLES / GOUGES IN WOOD TRIM.
6. PROVIDE AND INSTALL NEW WOOD TRIM WHERE MISSING. MATCH EXISTING WOOD SPECIES AND TRIM PROFILE, TYPICAL.
7. REMOVE SHEET METAL CLOSURE AND PATCH HOLES. REPAIR / DUTCHMAN / REPLACE WOOD TRIM COVERED BY SHEET METAL.
8. REPAIR EAVE WOOD FASCIA. REMOVE METAL FLASHING, NAILS AND PATCH HOLES, TYPICAL.

SHEET NOTES

- A. SEE SHEET HA2.1 FOR WINDOW TYPES.
- B. SEE FLOOR PLANS FOR ADDITIONAL INFORMATION.
- C. SEE EXTERIOR ELEVATIONS FOR ADDITIONAL INFORMATION AND WINDOW LOCATIONS.
- D. NOT ALL REPAIRS ARE INDICATED WITH KEY NOTES. PLEASE SEE LEGEND FOR EXPLANATION OF REQUIRED REPAIRS SHOWN ON DRAWING.
- E. BAY WINDOW ROOF REPAIR IS NOTED ON EXTERIOR ELEVATIONS.

HA3.1

**ENLARGED
EXTERIOR
ELEVATIONS**



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 Princeton | San Francisco | Seattle | Washington, D.C.

F5.1

WEST ELEVATION

Façade Repair Legend					
General Format: Condition Symbol / Repair Symbol / Quantity Symbol					
Condition Symbol		Repair Symbol		Quantity Symbols	
Δ	Displaced Masonry	RM	Replace and/or rebuild masonry.	#	Sq. Ft
		P	Install mortar patch.	#	Sq. Ft
S	Spall at Masonry Areas	DP	Install dutchman patch.	#	Sq. Ft.
		RM	Install replacement unit.	EA	Entire Block
					See elevations and visit site for specific locations and configurations
FP	Failed or Non-matching Patch	P	Install mortar patch.	#	Sq. Ft
		RM	Replace and/or rebuild masonry.	#	Sq. Ft
C	Crack	RP	Rout & Patch w/ Mortar.	#	Length (ft)
		LPI	Low pressure epoxy injection	#	Length (ft)
		EPA	Epoxy pin anchor.	#	Length (ft)
		HPI	High pressure epoxy injection	#	Length (ft)
MM	Missing or Non-matching Masonry	RM	Install replacement unit.	EA	Entire Block
		RM*	Install ornamental replacement unit.		See elevations and visit site for specific locations and configurations
I	Abandoned Metal Insert	P	Remove insert. Patch substrate w/ mortar	EA	Each Location
MS	Mortar Smears on Masonry Surface	CL	Clean affected surface.		
EB	Efflorescence, Biological Growth or Paint Haze	CL	Clean affected surface.		

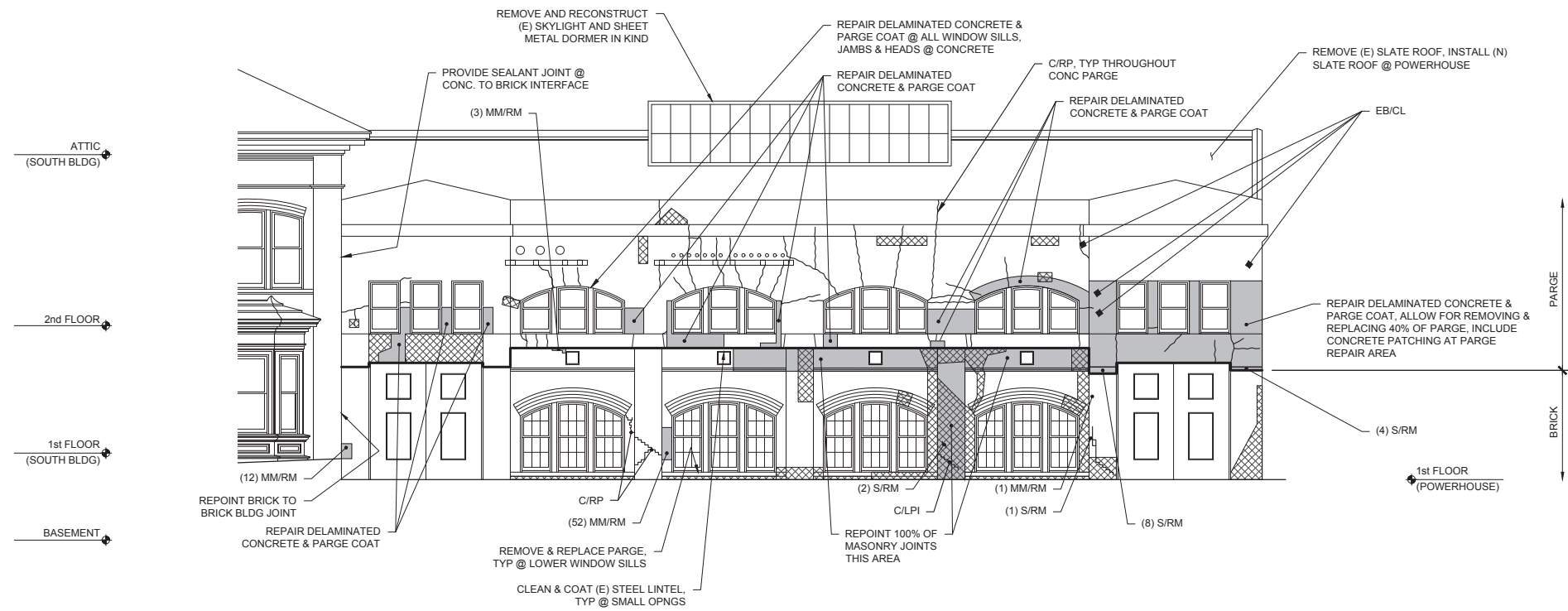
Notes:
 1. Multiple repair and quantity symbols can be used for a single noted condition.
 2. "*" indicates less than 1 unit of quantity.

GENERAL NOTES:

- PROTECT ALL EXISTING HISTORIC MATERIAL TO REMAIN IN PLACE DURING CONSTRUCTION.

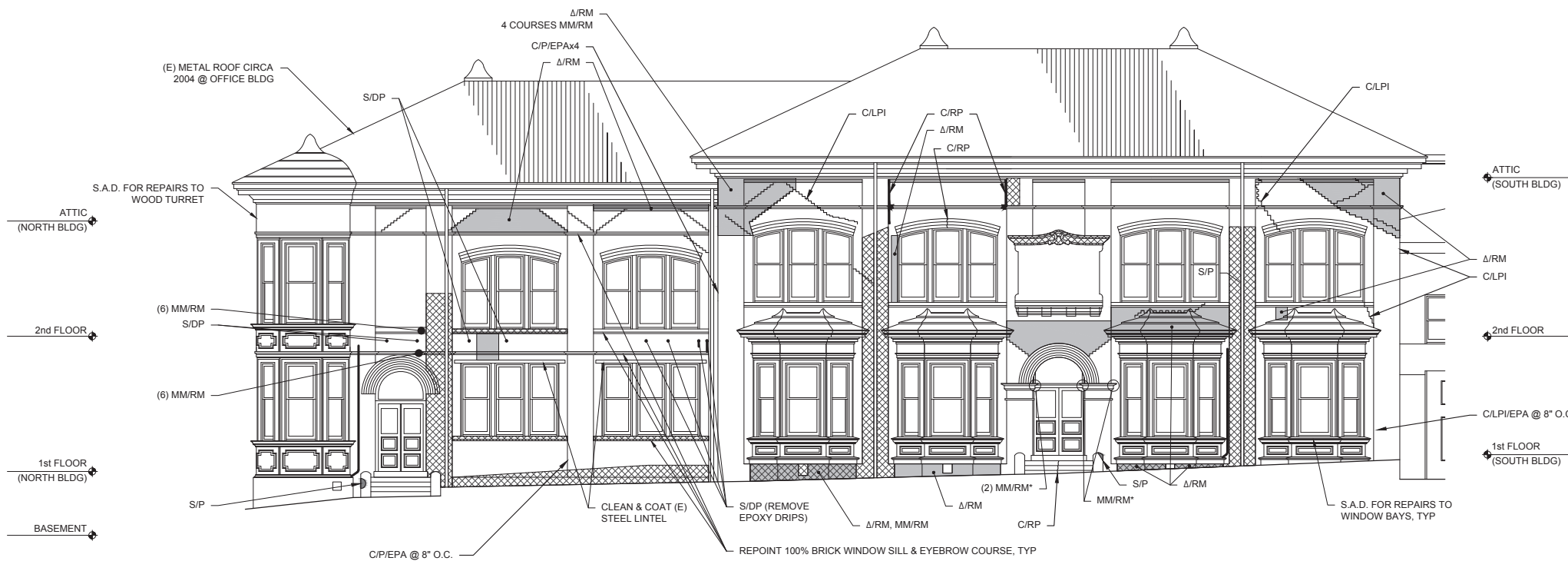
SHEET NOTES:

- ANCHOR EXTERIOR BRICK TO BACK-UP W/ 10mm x 15" HELICAL ANCHOR @ 16" O.C. EACH WAY, WEST ELEVATION ONLY.
- ALLOW FOR REPOINTING 60% OF MORTAR JOINTS IN ADDITION TO THOSE SPECIFICALLY NOTED.
- S.A.D. FOR ADDITIONAL FAÇADE REPAIRS.



1 PARTIAL WEST ELEVATION - POWERHOUSE

SCALE: 1/8" = 1'-0"



2 PARTIAL WEST ELEVATION - CAR BARN

SCALE: 1/8" = 1'-0"



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F5.2

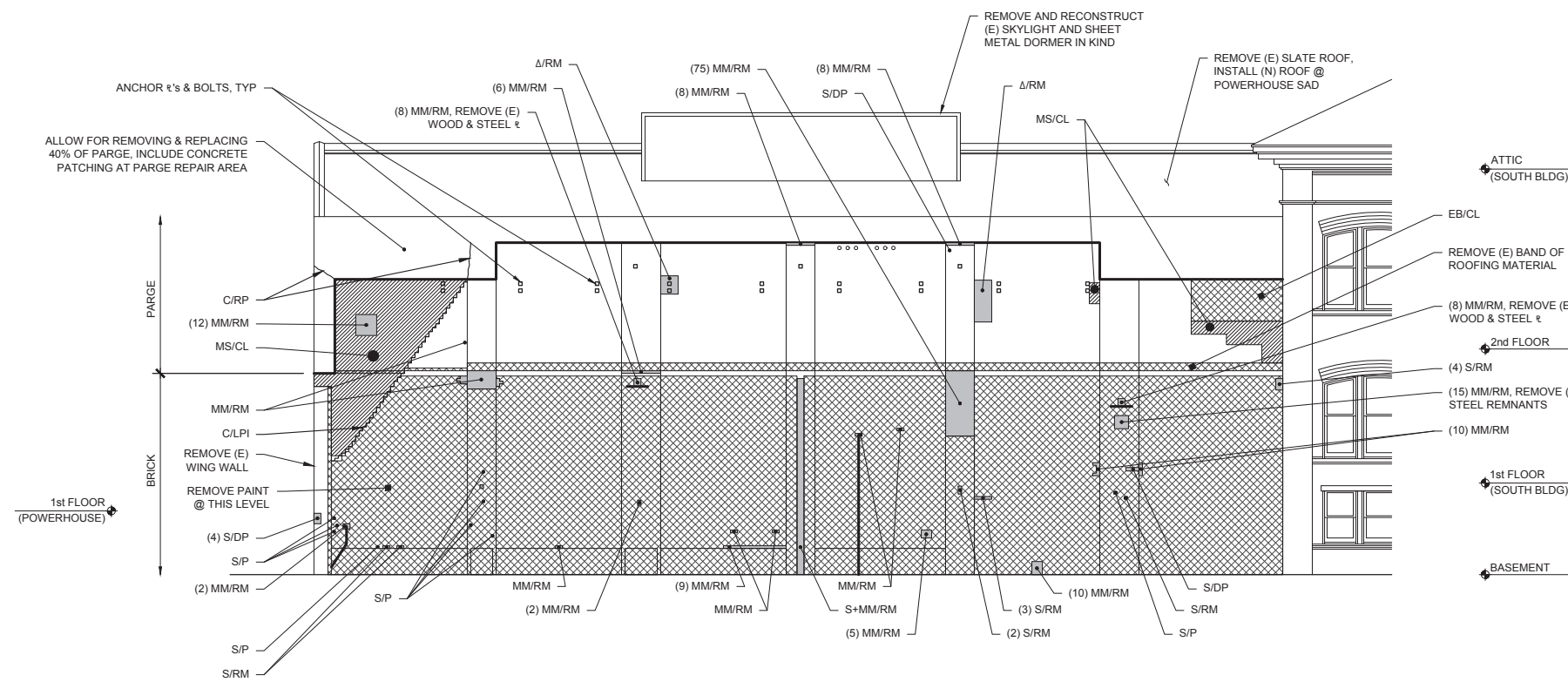
EAST ELEVATION

GENERAL NOTES:

1. PROTECT ALL EXISTING HISTORIC MATERIAL TO REMAIN IN PLACE DURING CONSTRUCTION.

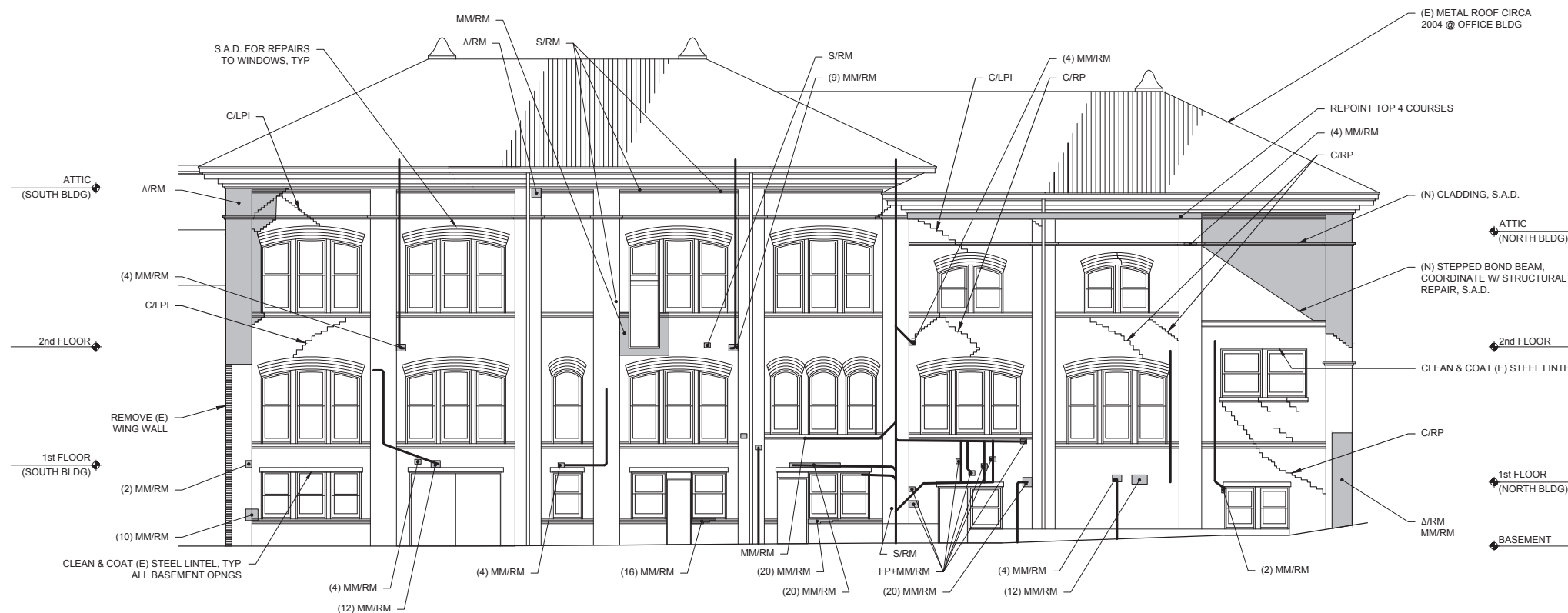
SHEET NOTES:

1. ANCHOR EXTERIOR BRICK TO BACK-UP W/ 10mm x 15" HELICAL ANCHOR @ 16" O.C. EACH WAY, WEST ELEVATION ONLY.
2. ALLOW FOR REPOINTING 60% OF MORTAR JOINTS IN ADDITION TO THOSE SPECIFICALLY NOTED.
3. S.A.D. FOR ADDITIONAL FACADE REPAIRS.



1 PARTIAL EAST ELEVATION - POWERHOUSE

SCALE: 1/8" = 1'-0"

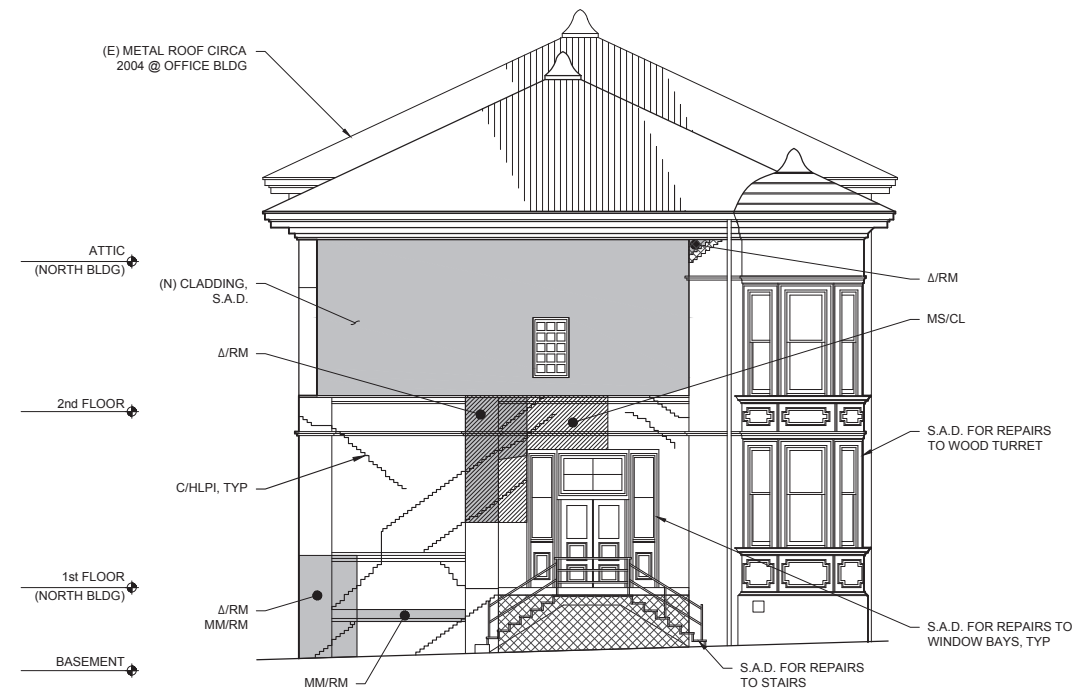


2 PARTIAL EAST ELEVATION - CAR BARN

SCALE: 1/8" = 1'-0"

Façade Repair Legend					
General Format: Condition Symbol / Repair Symbol / Quantity Symbol					
Condition Symbol	Repair Symbol	Quantity Symbols			
A Displaced Masonry	RM Replace and/or rebuild masonry.	#	Sq. Ft	Area indicated	
S Spall at Masonry Areas	P Install mortar patch.	#	Sq. Ft		
	DP Install dutchman patch.	#	Sq. Ft.		
	RM Install replacement unit.	EA	Entire Block	See elevations and visit site for specific locations and configurations	
FP Failed or Non-matching Patch	P Install mortar patch.	#	Sq. Ft	Sq. ft. of projected surface area	
	RM Replace and/or rebuild masonry.	#	Sq. Ft		
C Crack	RP Rout & Patch w/ Mortar.	#	Length (ft)		
	LPI Low pressure epoxy injection	#	Length (ft)		
	EPA Epoxy pin anchor.	#	Length (ft)		
	HPI High pressure epoxy injection	#	Length (ft)		
MM Missing or Non-matching Masonry	RM Install replacement unit.	EA	Entire Block	See elevations and visit site for specific locations and configurations	
	RM* Install ornamental replacement unit.				
I Abandoned Metal Insert	P Remove insert. Patch substrate w/ mortar	EA	Each Location		
MS Mortar Smears on Masonry Surface	CL Clean affected surface.				
EB Efflorescence, Biological Growth or Paint Haze	CL Clean affected surface.				

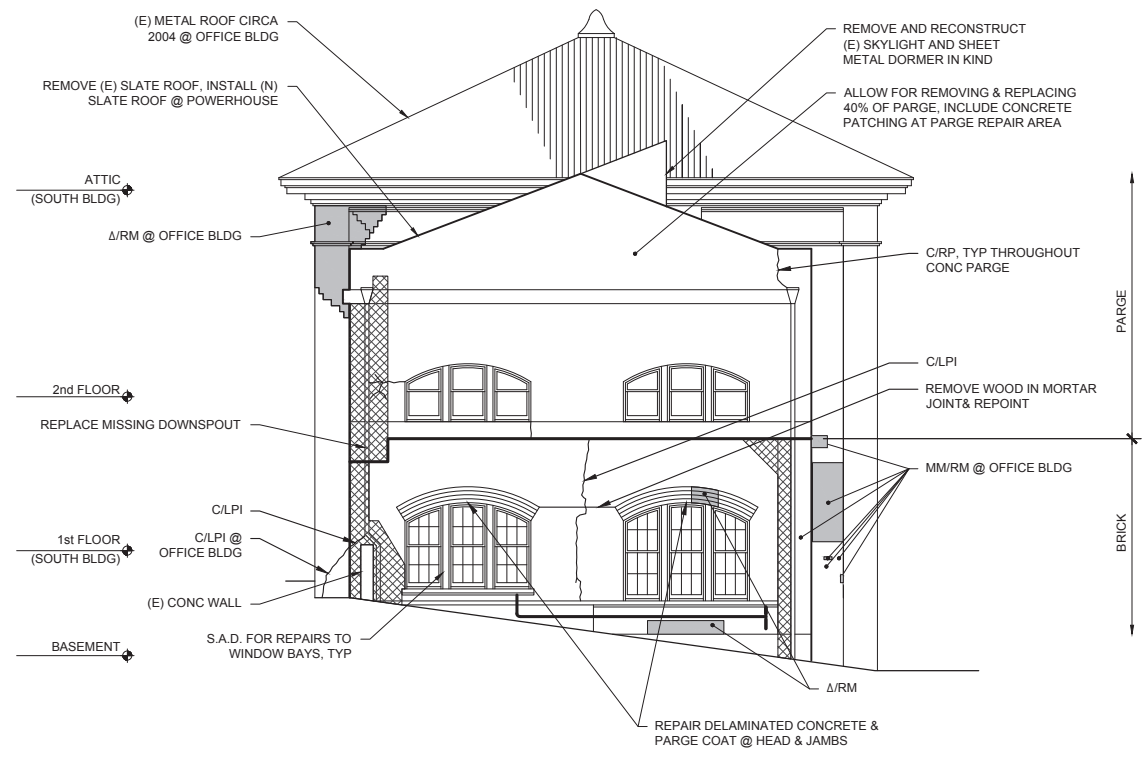
- Notes:
1. Multiple repair and quantity symbols can be used for a single noted condition.
 2. "*" indicates less than 1 unit of quantity.



NOTES

- ELEVATION SHOWS EXISTING CONDITIONS & CONCEPTUAL REPAIRS ASSOCIATED W/ RETAINING & REPAIRING (E) BRICK IN-PLACE

1 NORTH ELEVATION
SCALE: 1/8" = 1'-0"



2 SOUTH ELEVATION
SCALE: 1/8" = 1'-0"

GENERAL NOTES:

- PROTECT ALL EXISTING HISTORIC MATERIAL TO REMAIN IN PLACE DURING CONSTRUCTION.

SHEET NOTES:

- ANCHOR EXTERIOR BRICK TO BACK-UP W/ 10mm x 15" HELICAL ANCHOR @ 16" O.C. EACH WAY, WEST ELEVATION ONLY.
- ALLOW FOR REPOINTING 60% OF MORTAR JOINTS IN ADDITION TO THOSE SPECIFICALLY NOTED.
- S.A.D. FOR ADDITIONAL FAÇADE REPAIRS.

Façade Repair Legend					
General Format: Condition Symbol / Repair Symbol / Quantity Symbol					
Condition Symbol	Repair Symbol	Quantity Symbols			
Δ	Displaced Masonry	RM	Replace and/or rebuild masonry.	# Sq. Ft	Area indicated
S	Spall at Masonry Areas	P	Install mortar patch.	# Sq. Ft	
		DP	Install dutchman patch.	# Sq. Ft.	
		RM	Install replacement unit.	EA	Entire Block
FP	Failed or Non-matching Patch	P	Install mortar patch.	# Sq. Ft	Sq. ft. of projected surface area
		RM	Replace and/or rebuild masonry.	# Sq. Ft	
C	Crack	RP	Rout & Patch w/ Mortar.	# Length (ft)	
		LPI	Low pressure epoxy injection	# Length (ft)	
		EPA	Epoxy pin anchor.	# Length (ft)	
		HPI	High pressure epoxy injection	# Length (ft)	
MM	Missing or Non-matching Masonry	RM	Install replacement unit.	EA	Entire Block
		RM*	Install ornamental replacement unit.		See elevations and visit site for specific locations and configurations
I	Abandoned Metal Insert	P	Remove insert. Patch substrate w/ mortar	EA	Each Location
MS	Mortar Smears on Masonry Surface	CL	Clean affected surface.		
EB	Efflorescence, Biological Growth or Paint Haze	CL	Clean affected surface.		

Notes:

- Multiple repair and quantity symbols can be used for a single noted condition.
- *"1" indicates less than 1 unit of quantity.



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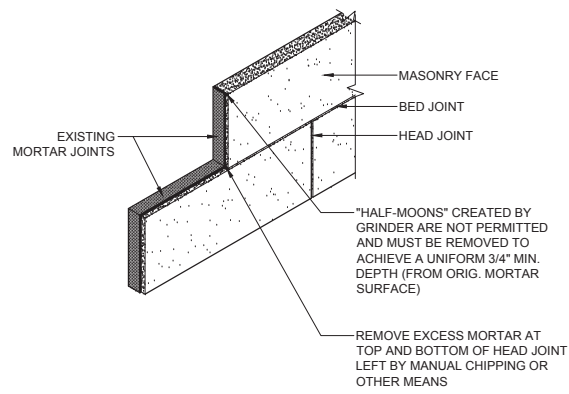


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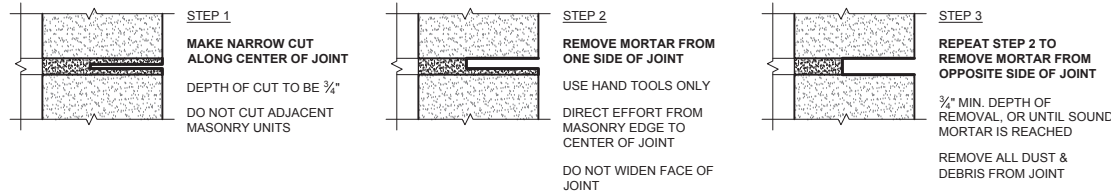
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F5.3

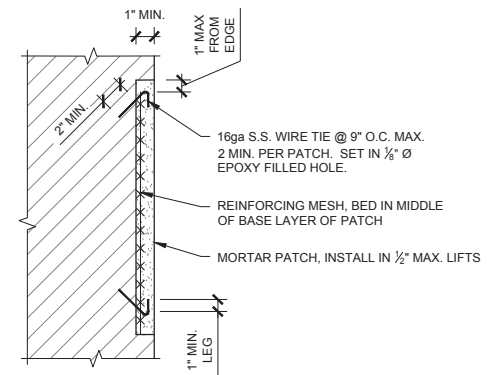
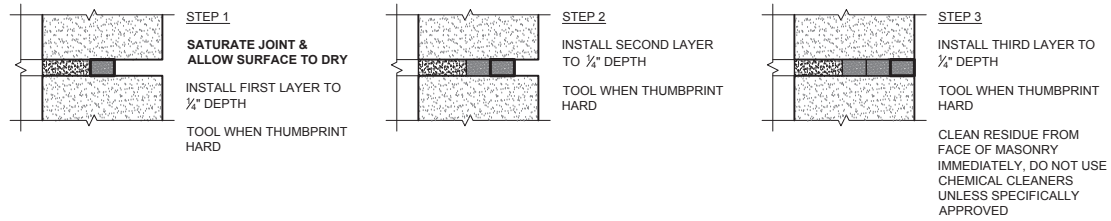
NORTH & SOUTH ELEVATIONS



MORTAR REMOVAL PROCEDURES:



MORTAR INSTALLATION PROCEDURES, NORTH, NORTHEAST & EAST ELEVATIONS:



OUTLINE REPAIR PROCEDURE:

1. IDENTIFY DETERIORATED MASONRY BY SOUNDING. MARK PERIMETER OF DETERIORATION FOR A/E REVIEW.
2. SAW CUT PERIMETER OF PATCH AREA TO SQUARE OFF THE SIDES. DO NOT OVERRUN CUTS.
3. REMOVE SOUND AND UNSOUND MATERIAL TO A MINIMUM DEPTH OF 1".
4. AT EXPOSED ANCHORS DO NOT CUT ANCHOR. CLEAN CORROSION AND COAT W/ RUST INHIBITIVE COATING. IF ANCHOR HAS LESS THAN 1" COVER CUT OFF AT BASE OF PATCH AND INSTALL NEW HELICAL ANCHOR.
5. INSTALL PATCH REINFORCEMENT & ANCHORS.
6. INSTALL COLOR-MATCHED PATCHING MORTAR IN A 1/2" MAX LIFTS. FINISH COLOR, TEXTURE AND PROFILE OF PATCH TO MATCH ORIGINAL MASONRY.
7. DO NOT EXTEND PATCHES ACROSS MORTAR JOINTS. WHERE PATCHES CROSS JOINTS, BLOCK OUT JOINT AND POINT AFTER PATCH HAS CURED.
8. CURE PATCH FOR 7 DAYS WITH PLASTIC COVERING.

1 JOINT REPOINTING GUIDELINES

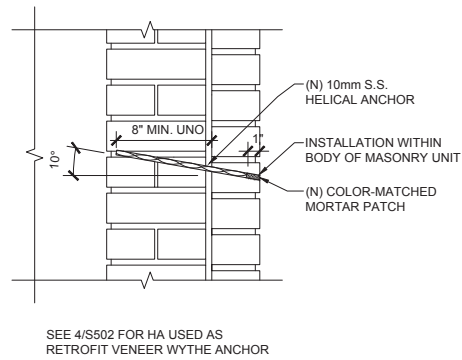
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2 JOINT REPOINTING REPAIR PROCEDURE

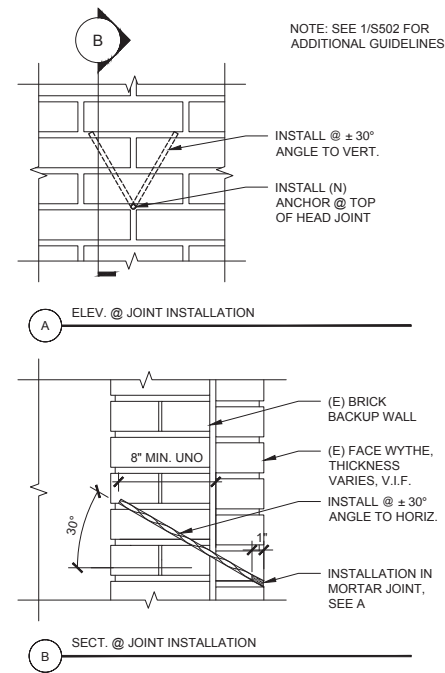
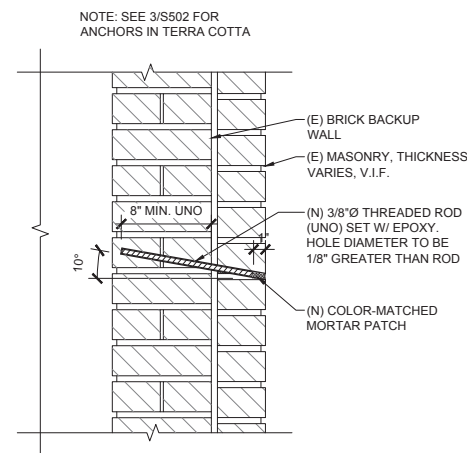
SCALE: 6" = 1'-0"

3 TYP. MORTAR PATCHING GUIDELINES

SCALE: 1 1/2" = 1'-0"



SEE 4/S502 FOR HA USED AS RETROFIT VENEER WYTHE ANCHOR



NOTE: SEE 1/S502 FOR ADDITIONAL GUIDELINES

4 TYP. HELICAL ANCHOR

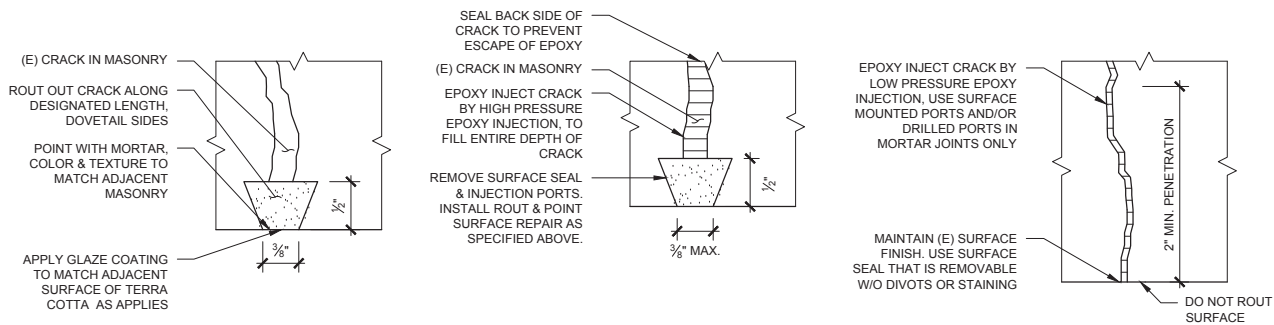
SCALE: 1 1/2" = 1'-0"

5 TYP. EPOXY PIN ANCHOR

SCALE: 1 1/2" = 1'-0"

6 RETROFIT VENEER WYTHE ANCHOR

SCALE: 1 1/2" = 1'-0"



7 ROUT & POINT CRACK REPAIR

SCALE: 1" = 1'-0"

8 IFP CRACK REPAIR

SCALE: 1" = 1'-0"

9 IPP CRACK REPAIR

SCALE: 1" = 1'-0"



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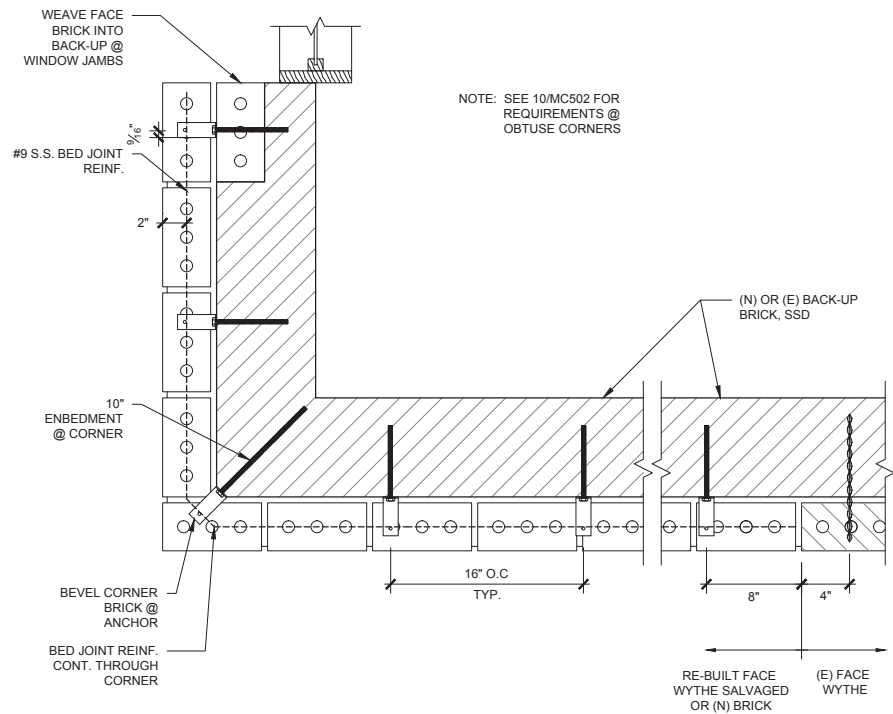


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Emeryville, California 94608
510.428.2907 tel | 510.428.0456 fax
www.wja.com

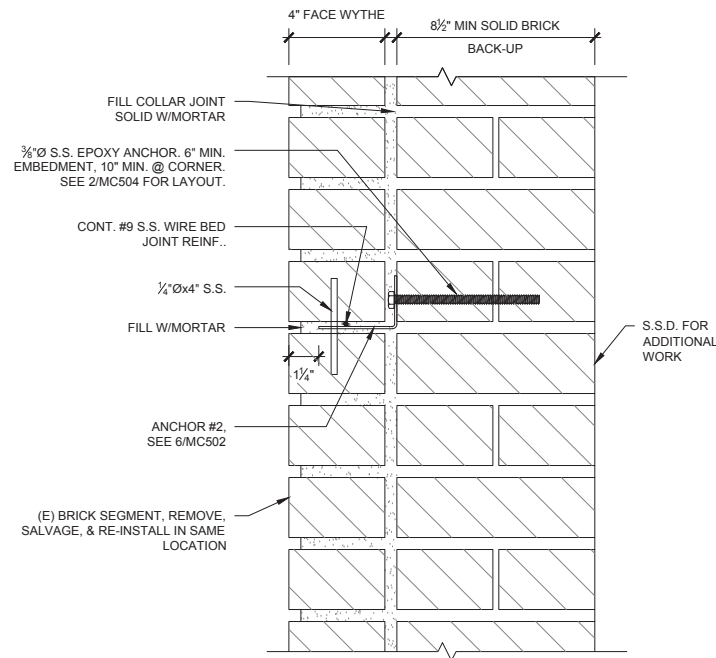
Headquarters & Laboratories: Northbrook, Illinois
Atlanta | Austin | Boston | Chicago | Cleveland | Dallas | Denver | Detroit
Houston | Kansas City | Los Angeles | Minneapolis | New Haven | New York
Phoenix | San Francisco | Seattle | Washington, D.C.

F9.1

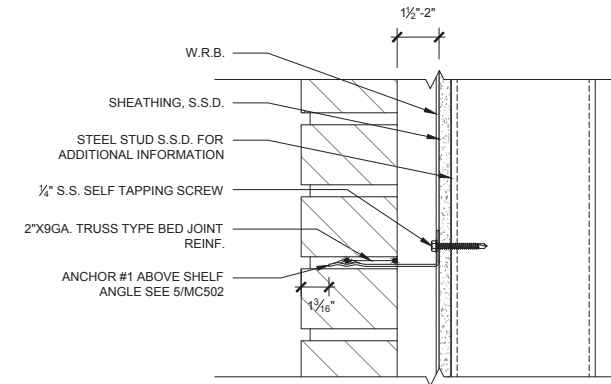
TYPICAL DETAILS



1 FACE WYTHER RECONSTRUCTION GUIDELINES - SALVAGED OR (N) BRICK
SCALE: 1 1/2" = 1'-0"



2 SALVAGED BRICK ASSEMBLAGE INSTALLATION
SCALE: 3" = 1'-0"



3 TYP. TIE-BACK ANCHOR
SCALE: 3" = 1'-0"



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500 third street
suite 410
san francisco, ca 94107
415 974 0849 fax
415 974 5603 phone

GENEVA CAR BARN AND POWERHOUSE

2301 SAN JOSE AVENUE
SAN FRANCISCO, CA

DATE: 06.03.15

JOB: 2007.3889.2

DRAWN: SJF

SCALE: AS NOTED

ISSUE	DATE
50% DD	03/09/2015
100% DD	06/03/2015



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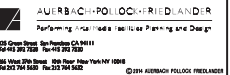
Headquarters & Laboratories: Northbrook, Illinois
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Houston | Los Angeles | Minneapolis | New Haven | New York
Portland | San Francisco | Seattle | Washington, D.C.

F9.2

TYPICAL DETAILS



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GENEVA CAR BARN
 AND POWERHOUSE

2301 SAN JOSE AVENUE
 SAN FRANCISCO, CA

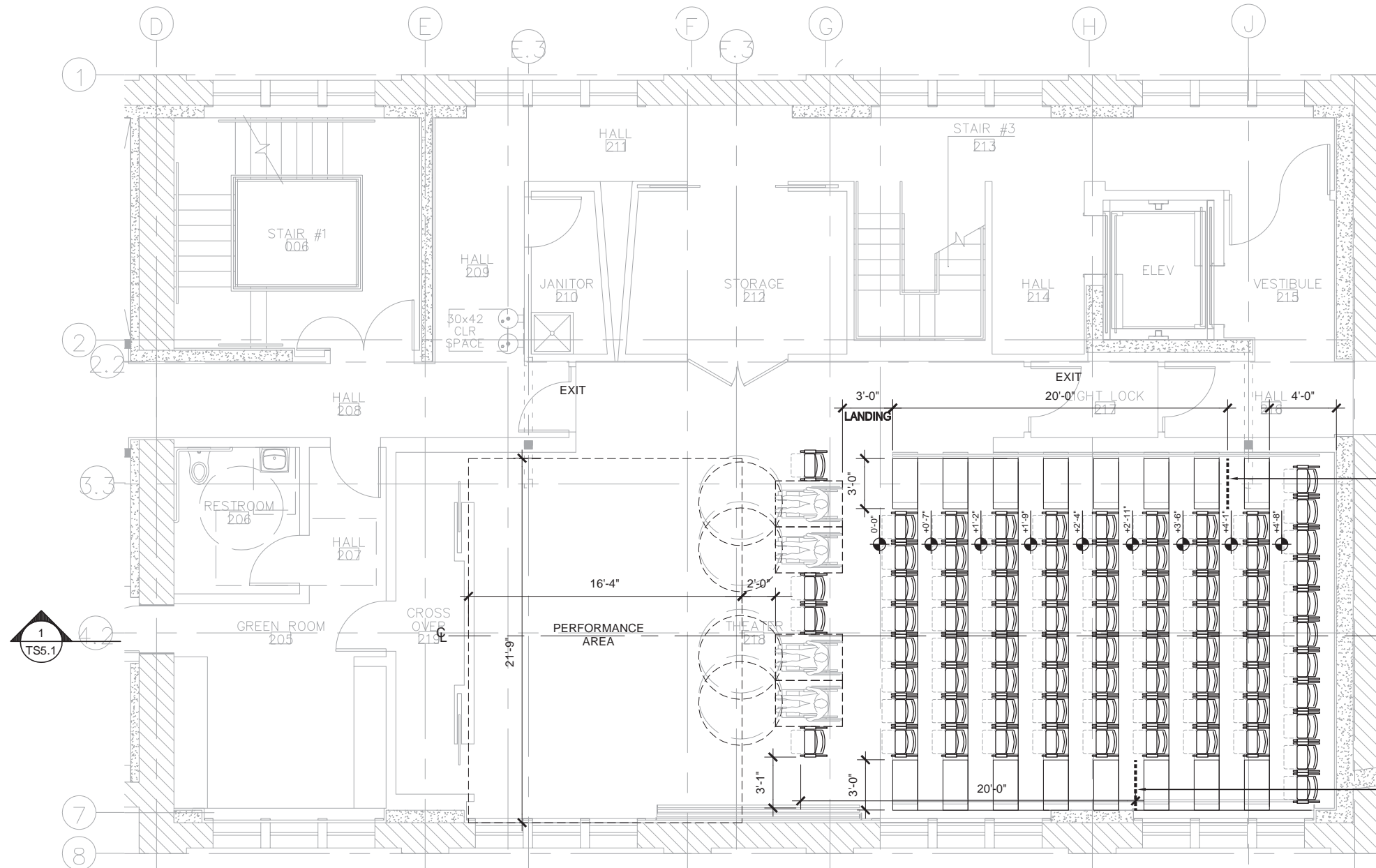
DATE: 06.03.15

JOB: 0906

DRAWN: APF

SCALE: AS NOTED

ISSUE	DATE
50% DD SET	03.09.15
PROGRESS	05.13.15

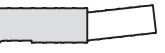
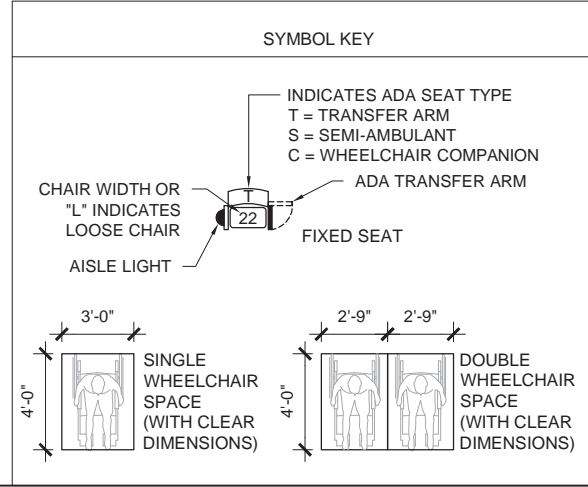


20 FOOT DISTANCE MARK OF DEAD END AISLE. SEATS BEYOND THIS MARK MEET THE REQUIREMENTS OF CBC 2013 SECTION 1028.9.5 EXCEPTION #2 SO THIS AISLE IS ALLOWED TO EXCEED THE 20'-0" MAXIMUM DISTANCE.

20 FOOT DISTANCE MARK OF DEAD END AISLE. SEATS BEYOND THIS MARK MEET THE REQUIREMENTS OF CBC 2013 SECTION 1028.9.5 EXCEPTION #2 SO THIS AISLE IS ALLOWED TO EXCEED THE 20'-0" MAXIMUM DISTANCE.

1 THEATRICAL SEATING PLAN
 1/4" = 1'-0"

- GENERAL NOTES**
- SEATING LAYOUT INDICATES DESIGN INTENT ONLY. FINAL LAYOUT ON SHOP DRAWINGS INCLUDING CHAIR DIMENSIONS AND QUANTITIES ARE THE SOLE RESPONSIBILITY OF THE THEATRICAL SEATING CONTRACTOR.
 - CLEAR AISLE DIMENSIONS INDICATED ON THE DRAWINGS ARE TO BE FOLLOWED WHEN PREPARING SHOP DRAWINGS. THESE DIMENSIONS ARE BASED ON PROJECT CODE REQUIREMENTS.
 - VERIFY ALL DIMENSIONS AND CONDITIONS IN FIELD PRIOR TO PREPARATION OF SHOP DRAWINGS.
 - WHEN LAYING OUT SEATS IN ROWS WHICH CONTAIN VARYING WIDTH SEATS, ADHERE TO THE FOLLOWING GUIDELINES WHERE POSSIBLE:
 - A NARROWER SEAT IS TO BE MOUNTED ADJACENT TO AN AISLE.
 - A WIDER SEAT IS TO BE MOUNTED ADJACENT TO SIDE WALLS OR RAILINGS.
 - THE REMAINING VARYING WIDTH SEATS ARE TO BE DISTRIBUTED THROUGHOUT THE ROW SO THAT NARROW SEATS ARE NOT MOUNTED ADJACENT TO ONE ANOTHER.
 - WHERE POSSIBLE, SEATS SHOULD BE STAGGERED FROM ROW TO ROW.
 - ALL EQUIPMENT BY SEATING CONTRACTOR UNLESS OTHERWISE NOTED.
 - AISLE LIGHT FIXTURES PREWIRED WITH 18" FLEX STEEL CONDUIT SUPPLIED BY SEATING CONTRACTOR AND TERMINATED BY DIVISION 26. THEATER SEATING FLOOR BOX, WIRE, AND CONDUIT PROVIDED BY DIVISION 26
 - AISLE LIGHT TRANSFORMER(S) SUPPLIED BY SEATING CONTRACTOR AND INSTALLED BY DIVISION 26.



TS2.5A

THEATRICAL SEATING PLAN
 LEVEL 2



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AUGUST 10, 2015
 PERFORMING ARCHITECTURAL PLANNING AND DESIGN
 1000 CALIFORNIA STREET, SUITE 410, SAN FRANCISCO, CA 94109
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GENEVA CAR BARN AND POWERHOUSE

2301 SAN JOSE AVENUE
 SAN FRANCISCO, CA

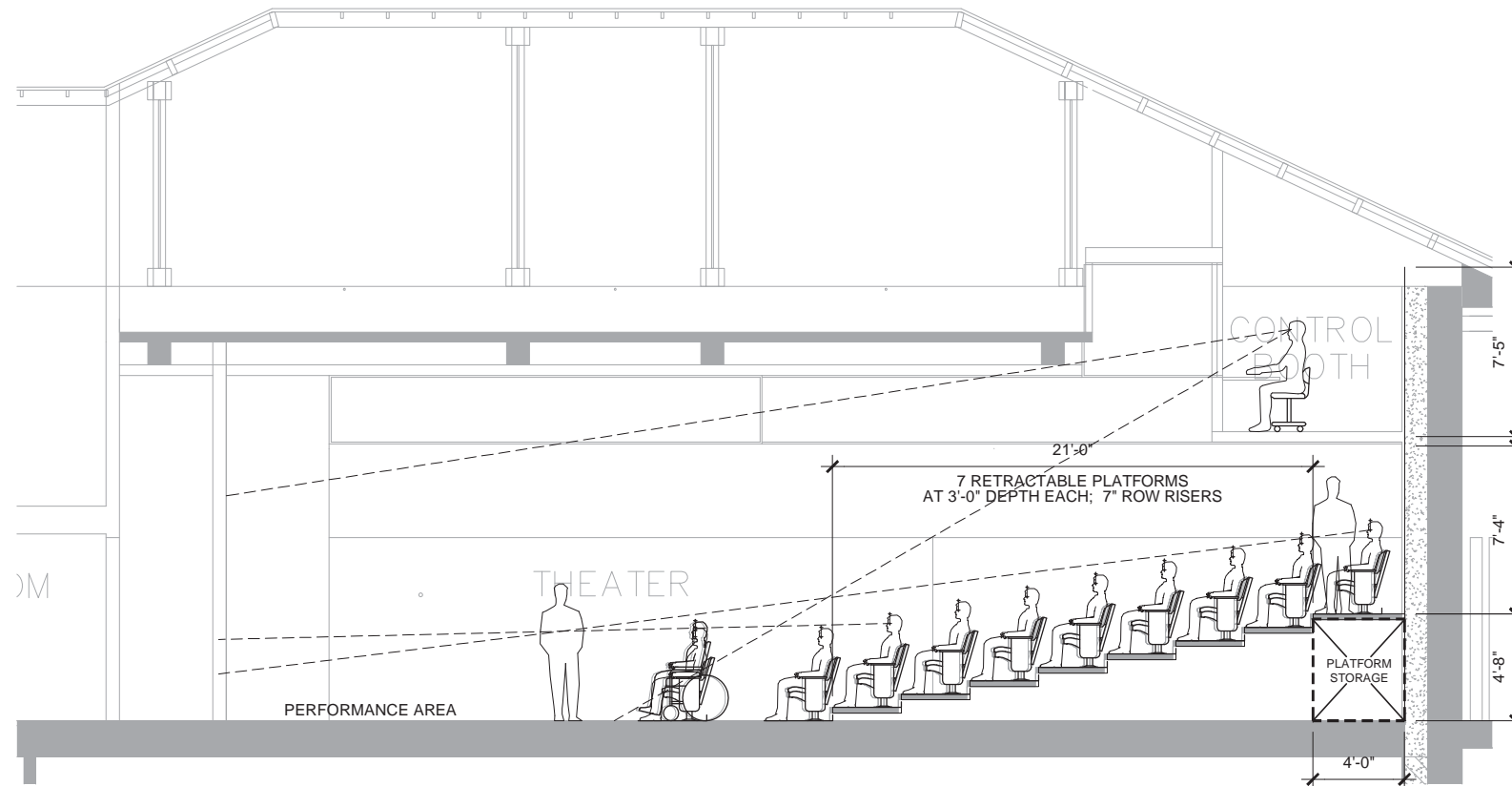
DATE: 06.03.15

JOB: 0906

DRAWN: APF

SCALE: AS NOTED

ISSUE	DATE
50% DD SET	03.09.15
PROGRESS	05.13.15



SEATING TOTALS

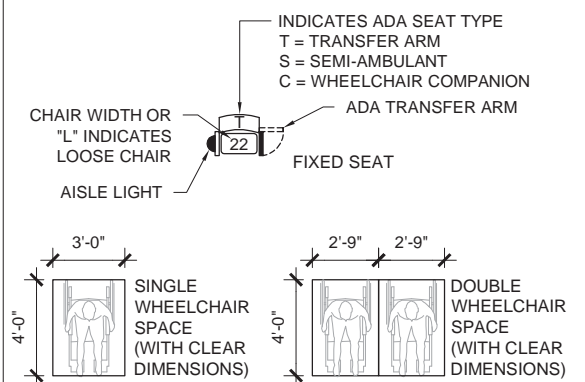
LOOSE SEATS	79
WHEELCHAIRS	4
OVERALL TOTAL	83

1 THEATRICAL SEATING CENTERLINE SECTION
 1/2" = 1'-0"

GENERAL NOTES

- SEATING LAYOUT INDICATES DESIGN INTENT ONLY. FINAL LAYOUT ON SHOP DRAWINGS INCLUDING CHAIR DIMENSIONS AND QUANTITIES ARE THE SOLE RESPONSIBILITY OF THE THEATRICAL SEATING CONTRACTOR.
- CLEAR AISLE DIMENSIONS INDICATED ON THE DRAWINGS ARE TO BE FOLLOWED WHEN PREPARING SHOP DRAWINGS. THESE DIMENSIONS ARE BASED ON PROJECT CODE REQUIREMENTS.
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- AISLE LIGHT TRANSFORMER(S) SUPPLIED BY SEATING CONTRACTOR AND INSTALLED BY DIVISION 26.

SYMBOL KEY



TS5.1

THEATRICAL SEATING CENTERLINE SECTION