



SAN FRANCISCO PLANNING DEPARTMENT

Executive Summary Conditional Use

HEARING DATE: DECEMBER 15, 2016

Date: December 8, 2016
Case No.: **2015-004567CUA**
Project Address: **470 West Portal Avenue**
Zoning: RH-1 (D) [Residential, House, One-Family (Detached)]
40-X, (Special Sign District)
Block/Lot: 2484/008,009 and 2540/001
Project Sponsor: David Bushnell
450 Architects, Inc.
Pier 9, Suite 105
San Francisco, CA, 94111
Staff Contact: Elizabeth Jonckheer – (415) 575-8728
elizabeth.gordon-jonckheer@sfgov.org
Recommendation: **Approval with Conditions**

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PROJECT DESCRIPTION

The proposal is to expand the current San Francisco Waldorf High School campus to provide additional on-site facilities for academic and recreational uses while also modifying the current parking lot configuration and access to the main school entrance. The Project proposes a two-phase expansion of the school. Phase 1 proposes the removal of up to 37 parking spaces from the existing parking lot for the construction of a new 35-foot tall, one-story, multi-purpose gymnasium of up to 11,100 square feet along West Portal Avenue, and relocation of the parking access fronting the Muni platform on West Portal Avenue approximately 100 feet further south along West Portal Avenue. Phase 2 proposes an addition of approximately 12,800 square feet, including 11,100 square feet beneath the footprint of the existing building at the northwest portion of the site, with, five new classrooms, a performance space, storage lockers and bathrooms, and a 1,700 square foot ground floor level lobby connection between the gymnasium and the existing building. The proposed project would be designed to accommodate a potential, gradual increase in the school's enrollment by 90 for a total enrollment of up to maximum of 240 students. The project sponsor is seeking a ten-year authorization timeline for sequential construction and additional fund-raising related to the proposed project.

The proposal requires a Conditional Use Authorization for a Planned Unit Development (PUD) for a 10-year, two-phase expansion of the high school (an institutional use) in an RH-1(D) District, and modifications from the Planning Code's front setback, rear yard and Class-1 bicycle parking space requirements. Section 311-neighborhood notification was conducted in conjunction with the Conditional Use authorization process.

BACKGROUND

In 2006, the San Francisco Waldorf High School (SFWHS) obtained a Conditional Use Authorization to convert the 35 foot tall, 22,925 square foot, vacant, AT&T/Pac-Bell Directory Assistance Operating Center office building to a secondary school per Case No. 2006.0100C and Motion No.17262 (attached). The approval allowed for 12 classrooms, three dedicated art studios, one multi-purpose room, cafeteria and lounge areas, and staff offices to accommodate 200 students and 20 school employees.

SITE DESCRIPTION AND PRESENT USE

The Project Site is an irregularly shaped lot, situated within the Lakeshore neighborhood, and along the southern end of the West Portal neighborhood. The subject 72,094 square-foot property is located along west side of West Portal Avenue, between 15th Avenue and Sloat Boulevard, near the intersection of Portola Drive, Sloat Boulevard, and West Portal Avenue; Lots 008 and 009 in Assessor's Block 2484 and Lot 001 in Assessor's Block 2540. The site is comprised of an existing 23,000 square foot high school and 25,314 square foot parking lot with 61 parking spaces. The current campus includes 12 classrooms; three dedicated art studios, one multi-purpose room, staff offices, and support spaces. The school is currently comprised of 150 students and 20 full-time staff. The property is located in a RH-1(D) District and a 40-X Height and Bulk District.

SURROUNDING PROPERTIES AND NEIGHBORHOOD

The subject property is located in the Lakeshore neighborhood, at the southern boundary of West Portal Avenue, outside the West Portal Neighborhood Commercial District, which ends several blocks north of the subject property. The site slopes steeply down to the north to an intermittent stream and eucalyptus glen located on the adjacent property which is home of the ArdenWood retirement community. The property also abuts single-family residences to the west and north. The property is also bordered by a grove of trees to the north. The St. Francis Circle MUNI stop for the M-Oceanview and K-Ingleside lines is situated in the middle of the street in front of the subject property, as a result, the street narrows to two lanes and there is no street parking in front of the property. West Portal Lutheran School is located within 300-feet of the subject property.

ENVIRONMENTAL REVIEW

The Project is exempt from the California Environmental Quality Act ("CEQA") as a Class 32 categorical exemption.

HEARING NOTIFICATION

TYPE	REQUIRED PERIOD	REQUIRED NOTICE DATE	ACTUAL NOTICE DATE	ACTUAL PERIOD
Classified News Ad	20 days	November 25, 2016	November 25, 2016	20 days
Posted Notice	20 days	November 25, 2016	November 25, 2016	20 days
Mailed Notice	20 days	November 25, 2016	November 25, 2016	20 days

The Project was originally scheduled for the Planning Commission hearing of December 1, 2016 with a continuation to the hearing of December 15, 2016 due to a noticing error. The newspaper advertisement, mailed notice dates and site poster were reposted. The proposal requires a Section 311-neighborhood notification, which was conducted in conjunction with the conditional use authorization process.

PUBLIC COMMENT

On November 7, 2016, an email was received from Mary Burns, on behalf of herself and residents of Ardenwood Way, which borders the western side of the school property (email and supporting materials attached). As part of her email, Ms. Burns requested that conditions of approval regarding the subject site's landscaping program be included in the approvals for the Project, which would assure compliance with the previous Conditional Use Permit authorization for the property. Ms. Burns filed a Code Violation complaint 11852_ENF, in 2012 in regarding to the landscaping issue, which was managed by the Planning Department's enforcement staff and abated in October of 2014. Ms. Burns notes in her email that an integral part of the Ardenwood neighbors support for the school's expansion plan is the handling of the school's landscaping and grounds maintenance. The Project Sponsor met with Ms. Burns on November 9, 2016 to resolve the above-mentioned issues and developed a revised Planting and Maintenance Plan (attached) with language to be incorporated into the Conditions of Approval of the attached Motion. The Greater West Portal Neighborhood Association also sent a letter conditionally supporting the proposal based on the landscaping agreement.

Additionally, as of November 18, 2016, the Department has received one email from a residential neighbor with concerns regarding elimination of parking spaces and the street fronting location of the gymnasium. The Project Sponsor is in contact with this individual regarding these concerns. The Department has also received three public correspondences expressing support for the proposed project, including a letter from the adjacent Arden Wood care facility and a support letter signed by the manager/owners of eight businesses along West Portal Avenue.

ISSUES AND OTHER CONSIDERATIONS

- **Project Scope.** The proposed expansion would add a new multi-purpose gymnasium, entry lobby, theater, and five additional classrooms, and accommodate a maximum capacity of up to 240 students and 32 employees. The additional square footage of approximately 23,200 square feet, combined with the existing building square footage of 23,000 square feet will total approximately 46,200 square feet. The project would propose a gradual increase in student population above current student population levels (150 students).

As noted previously, the project is planned in two phases. The first phase would include construction of a new driveway access/egress point, preparation activities for construction of the new gymnasium, construction of the multi-purpose gymnasium building and construction of the new lobby area. Phase 1 will occur over about 11 months. Phase 2, including construction of the classroom and performance space addition below the existing building and a new connection to the lobby area, would require about 12 months, but would be completed approximately 10 years in the future. The two phases are not contiguous. The gradual enrollment increase would not begin until after the completion of Phase 1 of construction.

- **Conditional Use Authorization.** The proposed project requires Conditional Use Authorization from the Planning Commission for a Planned Unit Development (PUD), for a 10-year, two-phase expansion of the high school. Since the project site is larger than a half-acre, the project may seek approval as a PUD per Planning Code Section 304. Under the PUD, the Commission may grant modifications from certain Planning Code requirements for projects that produce an environment of stable and desirable character, which will benefit the occupants, the neighborhood and the City as a whole. As noted, the project requests modifications from the Planning Code requirements for front setback (Planning Code Section 132), rear yard (Planning Code Section 134) and Class-1 bicycle parking space requirements (Planning Code Section 155.2). Department staff is generally in agreement with the proposed modifications given the overall project.
- **Below Grade AT&T Equipment Vaults/AT&T Easement.** AT&T has an easement for a below-grade equipment vault on the subject property (shown on the attached reduced plans). Throughout the project coordination and review process with Planning Department staff, both the Department and the Project Sponsor team reached out to AT&T to determine if they would allow the school to place a driveway over their easement. Although the most recent correspondence from AT&T indicates that they would not permit the encroachment, the Project Sponsor team continues to reach out to AT&T through other channels, and requests that the entitlements approve two driveway design alternatives for the gymnasium project (see **Driveway Design Variants** discussion below).

The Project Sponsor would work with Planning Department on final driveway and building design at the compliance at plan stage. The approval would be subject to Department staff review and approval prior to building permit issuance. Any significant changes outside the scope of the proposed driveway designs alternatives would require Planning Commission approval of a new Conditional Use Authorization.

- **Driveway Design Variants.** The first driveway alternative assumes that the school will be able to negotiate an agreement with AT&T to cross the easement, and allows for additional support services in the gymnasium building, including equipment storage and bike storage. The second driveway alternative allows for the driveway from West Portal Avenue to stay outside the AT&T easement, which requires a smaller sized gymnasium building, with fewer building support areas.
 - Under Driveway Design Option 1, the proposed project would remove 34 parking spaces and retaining 31 spaces, two of which are ADA-accessible (one ADA-accessible van parking space and one ADA-accessible passenger vehicle space). Driveway Design Option 2 would remove 37 parking spaces and retain 28 spaces, two of which are ADA-accessible (one ADA-accessible van parking space and one ADA-accessible passenger vehicle space). Under both variants the existing parking lot would be to reduce to about 13,035 square feet. Under either scenario, the proposed project would meet Planning Code Section 151 requirements for off-street parking at one off-street space for each two classrooms (.5 spaces per classroom): 19 classrooms x .5 spaces = 10 spaces.
 - The project would include the relocation of the existing 24 foot curb cut from West Portal Avenue fronting the Muni platform approximately 100 feet further south along West Portal

Avenue. This would result in the permanent closure of the existing driveway (and curb cut) located along the west side of West Portal Avenue. Driveway Design Option 1 would be located 115 feet south of the existing curb cut to be removed, and would be 18 feet wide. Driveway Design Option 2 would be located on the existing AT&T easement -- if AT&T grants access to allow a driveway on that easement. Driveway Design 2 would be 100 feet south of the existing curb cut to be removed, and would be 18 feet wide. Vehicles ingress and egress movements would be restricted to a right-in/right-out operation at this driveway location. Egress from the driveway would be restricted to southbound travel on West Portal Avenue.

- **Front Setback Modification.** Planning Code Section 132 requires minimum front setback areas to apply to every building in all RH, RTO, and RM Districts, in order to relate the setbacks provided to the existing front setbacks of adjacent buildings. Setbacks are based on the average of adjacent buildings; up to 15 feet or 15% of the lot depth. The proposed multi-purpose gymnasium building sits at the front property line. Therefore, the school would seek an exception to this requirement.
- **Rear Yard Modification.** Planning Code Section 134 requires a 25% rear yard in the RH-1(D) District. The existing, noncomplying original building and the proposed expansion below the existing building under Phase 2 are within the required rear yard. Therefore, the school would seek an exception to this requirement.
- **Bicycle Parking Modification.** There are five Class-2 bicycle parking spaces on the campus and the project would provide additional bicycle parking spaces; however, the bicycle parking supply and type would vary depending on the driveway option. Driveway Design 1 would provide 41 Class-1 bicycle parking spaces: 18 Class-1 spaces located within proposed gymnasium and 23 spaces within an enclosure adjacent to the ADA passenger vehicle spaces, and 19 Class-2 bicycle parking spaces: 5 existing spaces located within the parking lot and 14 spaces at the pedestrian entry from West Portal Avenue. Driveway Design 2 would provide 23 Class-1 bicycle parking spaces in the enclosure adjacent to the ADA passenger vehicle spaces and 19 Class-2 spaces near the pedestrian entry from West Portal Avenue.

Per Section 155.2 of the Planning Code, the project would be required to provide a minimum of 76 Class-1 bicycle parking spaces (4 spaces per classroom) and a minimum of 19 Class-2 bicycle parking spaces (one space per classroom). Because the school would not comply with the minimum Class-1 bicycle parking spaces per the Planning Code, the school would seek an exception to these requirements of up to 53 spaces.

- **Architecture and Design.** The Planning Department's Urban Design Team (UDAT) reviewed the Project and supported the site design, multi-purpose gymnasium design and site architecture as shown on the attached plans.

REQUIRED COMMISSION ACTION

In order for the project to proceed, the Planning Commission must grant a Conditional Use Authorization and a Planned Unit Development for the expansion of a secondary school in the RH-1 Zoning District, that would include modifications to the Planning Code's front setback, rear yard and Class-1 bicycle parking space requirements, and allow a 10-year, two-phase expansion of the San Francisco Waldorf School, pursuant to Planning Code Sections 132, 134, 155.2, 209.3(h), 303 and 304.

BASIS FOR RECOMMENDATION

- The Project complies with the applicable requirements of the Planning Code.
- The Project is consistent with the objectives and policies of the General Plan.
- An increase in student enrollment is not expected to adversely impact traffic and parking in the neighborhood because the site is well served by transit alternatives.
- The Project maintains and expands an educational use, which is a use in support of families and children in San Francisco.
- The Project would ensure the viability of an educational institution that has been located in the neighborhood since 2006 and would modernize an established independent high school with a college-preparatory curriculum in math, science, arts and humanities.
- The project will make the project site and buildings accessible to visitors and students with disabilities, including two accessible parking spaces.
- The Project is desirable for, and compatible with the surrounding neighborhood.

RECOMMENDATION: Approval with Conditions
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Attachments:

Draft Motion
CEQA Environmental Determination
Commission Motion No. 17262 for Case No. 2006.0100C
Block Book Map
Sanborn Map
Aerial Photographs
Zoning District Map
Site Photographs
Public Correspondence
Landscaping Agreement -- Revised Planting and Maintenance Plan
Project Sponsor Submittal, including:

- Reduced Plans
- Site and Aerial Photos

Attachment Checklist

- | | |
|---|--|
| <input checked="" type="checkbox"/> Executive Summary | <input checked="" type="checkbox"/> Project sponsor submittal |
| <input checked="" type="checkbox"/> Draft Motion | Drawings: <u>Existing Conditions</u> |
| <input checked="" type="checkbox"/> Environmental Determination | <input checked="" type="checkbox"/> Check for legibility |
| <input checked="" type="checkbox"/> Zoning District Map | Drawings: <u>Proposed Project</u> |
| <input type="checkbox"/> Height & Bulk Map | <input checked="" type="checkbox"/> Check for legibility |
| <input checked="" type="checkbox"/> Parcel Map | 3-D Renderings (new construction or significant addition) |
| <input checked="" type="checkbox"/> Sanborn Map | <input checked="" type="checkbox"/> Check for legibility |
| <input checked="" type="checkbox"/> Aerial Photo | <input type="checkbox"/> Wireless Telecommunications Materials |
| <input checked="" type="checkbox"/> Context Photos | <input type="checkbox"/> Health Dept. review of RF levels |
| <input checked="" type="checkbox"/> Site Photos | <input type="checkbox"/> RF Report |
| | <input type="checkbox"/> Community Meeting Notice |
| | <input type="checkbox"/> Housing Documents |
| | <input type="checkbox"/> Inclusionary Affordable Housing Program: Affidavit for Compliance |

Exhibits above marked with an "X" are included in this packet

EGJ
Planner's Initials

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SAN FRANCISCO PLANNING DEPARTMENT

Subject to: (Select only if applicable)

- Affordable Housing (Sec. 415)
- Jobs Housing Linkage Program (Sec. 413)
- Downtown Park Fee (Sec. 412)
- First Source Hiring (Admin. Code)
- Child Care Requirement (Sec. 414)
- Other (TSF, Sec. 411A)

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Planning Commission Draft Motion

HEARING DATE: DECEMBER 15, 2016

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ADOPTING FINDINGS RELATING TO THE APPROVAL OF CONDITIONAL USE AUTHORIZATION FOR A PLANNED UNIT DEVELOPMENT (PUD) PURSUANT TO SECTIONS 134, 155.2 209.3(H), 303 AND 304 OF THE PLANNING CODE TO ALLOW A TWO-PHASE EXPANSION OF A SECONDARY SCHOOL (THE SAN FRANCISCO WALDORF HIGH SCHOOL) THAT WOULD INCLUDE MODIFICATIONS TO THE FRONT SETBACK AND REAR YARD REQUIREMENTS (PLANNING CODE SECTIONS 132 AND 134), AND CLASS-1 BICYCLE PARKING REQUIREMENT (PLANNING CODE SECTION 155.2), WITHIN THE RH-1 (RESIDENTIAL-HOUSE, ONE FAMILY) ZONING DISTRICT AND A 40-X HEIGHT AND BULK DISTRICT.

PREAMBLE

On December 20, 2015, David Bushnell (hereinafter “Project Sponsor”) filed an application with the Planning Department (hereinafter “Department”) for Conditional Use Authorization and a Planned Unit Development under Planning Code Section(s) 132, 134, 155.2, 209.1, 303, 304 to allow a two-phase expansion of the San Francisco Waldorf High School (SFWHS) that would include modifications to the front setback and rear yard requirements (Planning Code Section 132 and 134), and Class-1 bicycle parking requirement (Planning Code Section 155.2), within the RH-1 (Residential-House, One Family) Zoning District and a 40-X Height and Bulk District.

On December 15, 2016 the San Francisco Planning Commission (hereinafter "Commission") conducted a duly noticed public hearing at a regularly scheduled meeting on Conditional Use Application No. 2015.004567CUA.

On November 18, 2016 the project was determined to be exempt from the California Environmental Quality Act ("CEQA") as a Class 32 Categorical Exemption under CEQA as described in the determination contained in the Planning Department files for this project.

The Commission has heard and considered the testimony presented to it at the public hearing and has further considered written materials and oral testimony presented on behalf of the applicant, Department staff, and other interested parties.

MOVED, that the Commission hereby authorizes the Conditional Use requested in Application No. 2015.004567CUA, subject to the conditions contained in "EXHIBIT A" of this motion, based on the following findings:

FINDINGS

Having reviewed the materials identified in the preamble above, and having heard all testimony and arguments, this Commission finds, concludes, and determines as follows:

1. The above recitals are accurate and constitute findings of this Commission.
2. **Site Description and Present Use.** The project site at 470 West Portal Avenue is an irregularly shaped lot, situated within the Lakeshore neighborhood, and along the southern end of the West Portal neighborhood. The subject 72,094 square-foot property is located along west side of West Portal Avenue, between 15th Avenue and Sloat Boulevard, near the intersection of Portola Drive, Sloat Boulevard, and West Portal Avenue; Lots 008 and 009 in Assessor's Block 2484 and Lot 001 in Assessor's Block 2540. The site is comprised of an existing 23,000 square foot high school and 25,314 square foot parking lot with 61 parking spaces. In 2006, the San Francisco Waldorf High School (SFWHS) obtained a Conditional Use Authorization to convert the 35-foot tall, vacant, AT&T/Pac-Bell Directory Assistance Operating Center office building to a secondary school per Case No. 2006.0100C and Motion No.17262. Enrollment was authorized at a maximum of 200 students. The current campus includes 12 classrooms; three dedicated art studios, one multi-purpose room, staff offices, and support spaces. The school is currently comprised of 150 students and full-time 20 staff. The property is located in a RH-1(D) District and a 40-X Height and Bulk District.
3. **Surrounding Properties and Neighborhood.** The subject property is located in the Lakeshore neighborhood, at the southern boundary of West Portal Avenue, outside the West Portal Neighborhood Commercial District, which ends several blocks north of the subject property. The site slopes steeply down to the north to an intermittent stream and eucalyptus glen located on the adjacent property which is home of the Ardenwood retirement community. The property also abuts single-family residences to the west and north the property is also bordered by a grove of trees to the north. The St. Francis Circle MUNI stop for the M-Oceanview and K-Ingleside lines is situated in the middle of the street in front of the subject property, as a result, the street narrows

to two lanes and there is no street parking in front of the property. West Portal Lutheran School is located within 300-feet of the subject property.

4. **Project Description.** The proposal is to expand the current San Francisco Waldorf High School campus to provide additional on-site facilities for academic and recreational uses while also modifying the current parking lot configuration and access to the main school entrance. The Project proposes a two-phase expansion of the school. Phase 1 proposes the removal of up to 37 parking spaces from the existing parking lot for the construction of a new 35-foot tall, one-story, multi-purpose gymnasium of up to 11,100 square feet along West Portal Avenue, and relocation of the parking access fronting the Muni platform on West Portal Avenue approximately 100 feet further south along West Portal Avenue. Phase 2 proposes an addition of approximately 12,800 square feet, including 11,100 square feet beneath the footprint of the existing building at the northwest portion of the site, with, five new classrooms, a performance space, storage lockers and bathrooms, and a 1,700 square foot ground floor level lobby connection between the gymnasium and the existing building. The proposed project would be designed to accommodate a potential, gradual increase in the school's current enrollment (150 students) by 90 for a total enrollment of up to maximum of 240 students. The project sponsor is seeking a ten-year authorization timeline for sequential construction and additional fund-raising related to the proposed project. Phase 1 will occur over about 11 months. Phase 2, including construction of the classroom and performance space addition below the existing building and a new connection to the lobby area, would require about 12 months, but would be completed approximately 10 years in the future. The two phases are not contiguous. The gradual enrollment increase would not begin until after the completion of Phase 1 of construction.

Public Comment. On November 7, 2016, an email was received from Mary Burns, on behalf of herself and residents of Ardenwood Way, which borders the western side of the school property (email and supporting materials attached). As part of her email, Ms. Burns requested that conditions of approval regarding the subject site's landscaping program be included in the approvals for the Project, which would assure compliance with the previous Conditional Use Permit authorization for the property. Ms. Burns filed a Code Violation complaint 11852_ENF, in 2012 in regarding to the landscaping issue, which was managed by the Planning Department's enforcement staff and abated in October of 2014. Ms. Burns notes in her email that an integral part of the Ardenwood neighbors support for the school's expansion plan is the handling of the school's landscaping and grounds maintenance. The Project Sponsor met with Ms. Burns on November 9, 2016 to resolve the above-mentioned issues and developed a revised Planting and Maintenance Plan, which is incorporated into the Conditions of Approval in Exhibit A. The Greater West Portal Neighborhood Association also sent a letter conditionally supporting the proposal based on the landscaping agreement

Additionally, as of November 18, 2016, the Department has received one email from a residential neighbor with concerns regarding elimination of parking spaces and the street fronting location of the gymnasium. The Department has also received three public correspondences expressing support for the proposed project, including a letter from the adjacent Arden Wood care facility and a support letter signed by the manager/owners of eight businesses along West Portal Avenue.

5. **Planning Code Compliance:** The Commission finds that the Project is consistent with the relevant provisions of the Planning Code in the following manner:

- A. **Use.** Planning Code Section 209.1 requires Conditional Use Authorization for a school within the RH-1 (Residential House, One-Family) Zoning District. Conditional Use Authorization is also required for a Planned Unit Development pursuant to Planning Code Section 304.

The Project would expand an existing private secondary school by adding square footage to accommodate a new gymnasium and additional academic on-site facilities, with an increase in classrooms and total enrollment. The Project is requesting Conditional Use Authorization from the Planning Commission to expand the school and to approve a Planned Unit Development.

- B. **Basic Floor Area Ratio (FAR).** Planning Code Section 124 limits the building square footage to 1.0 square feet of building area for every 1 square feet of lot area, or approximately 72,094 square feet of building area for the subject site.

The proposed project would total approximately 46,900 square feet.

- C. **Front Setback.** Planning Code Section 132 requires minimum front setback areas to apply to every building in all RH, RTO, and RM Districts, in order to relate the setbacks provided to the existing front setbacks of adjacent buildings. Setbacks are based on the average of adjacent buildings; up to 15 feet or 15% of the lot depth.

The proposed multi-purpose gymnasium building sits at the front property line along West Portal Avenue. Therefore, the school would seek an exception to this requirement.

- D. **Rear Yard.** Planning Code Section 134 requires a minimum rear yard equal to 25 percent of the total lot depth of the lot to be provided opposite the West Portal Avenue frontage.

The rear yard requirements apply to every building, including schools and churches, in Residential Districts. These requirements are intended to assure the protection and continuation of established mid-block, landscaped open spaces, and maintenance of a scale of development appropriate to each district, consistent with the location of adjacent buildings. Currently, the Project does not provide a rear yard according to the requirements specified in the Planning Code. The existing, noncomplying original building and the proposed expansion below the existing building under Phase 2 are within 16 feet of the rear property line, within the required rear yard. As a result, the project sponsor is requesting a rear yard modification.

- E. **Streetscape and Pedestrian Improvements.** Planning Code Section 138.1 requires a streetscape plan, which includes elements from the Better Streets Plan, for new construction on a lot greater than a half-acre in size.

Per the Better Streets Plan, West Portal Avenue is classified as a "Residential Throughway." The minimum sidewalk width for these streets types is 12 feet and the recommended width is 15 feet, respectively. The project is constrained to 12 feet between the curb and the property line. Along the length of the proposed new gymnasium, the project intends to shift the paved walkway away from the

street, separating the sidewalk from the roadway by a new planting strip in order to improve and facilitate safe pedestrian movement along the corridor. The proposal includes an additional 2 feet of planted area between the gymnasium and the paved sidewalk, bringing the width of sidewalk area to 14 feet. The paved area at the dedicated pedestrian entry adjacent to the new gymnasium will be expanded and 12 new street trees will be provided. Additionally there will be enhanced plantings within the public ROW at the southern end of the site. Although the project requests a modification for Class 1 bicycle parking spaces, the design expansion includes bike racks, pavers, planting, and lighting, in addition to the required street trees. The project is designed to be consistent with the Better Street Plan and therefore, the Project complies with Planning Code Section 138.1.

- F. **Off-Street Parking.** Planning Code Section 151.1 states that off-street parking is not required in the RH-1 Districts. Rather, Planning Code Section 151.1 permits up to one off-street parking space for each two classrooms for secondary school uses.

AT&T has an easement for a below-grade equipment vault on the subject property. Throughout the project coordination and review process with Planning Department staff, both the Department and the Project Sponsor team reached out to AT&T to determine if they would allow the school to place a driveway over their easement. Although the most recent correspondence from AT&T indicates that they would not permit the encroachment, the Project Sponsor team continues to reach out to AT&T through other channels, and requests that the entitlements approve two driveway design alternatives for the gymnasium project.

The first driveway alternative assumes that the San Francisco Waldorf High School will be able to negotiate an agreement with AT&T to cross the easement, and allows for additional support services in the gymnasium building, including equipment storage and bike storage. The second driveway alternative allows for the driveway from West Portal to stay outside the AT&T easement, which requires a smaller sized gymnasium building, with fewer building support areas. Under Driveway Design Option 1, the proposed project would remove 34 parking spaces and retaining 31 spaces, two of which are ADA-accessible (one ADA-accessible van parking space and one ADA-accessible passenger vehicle space). Driveway Design Option 2 would remove 37 parking spaces and retain 28 spaces, two of which are ADA-accessible (one ADA-accessible van parking space and one ADA-accessible passenger vehicle space). Under both variants, the existing parking lot would be reduced to about 13,035 square feet. Under either scenario, the proposed project would meet Planning Code Section 151 requirements for off-street parking at one off-street space for each two classrooms (.5 spaces per classroom): 19 classrooms x .5 spaces = 10 spaces. Therefore, the Project complies with Planning Code Section 151.1.

- G. **Bicycle Parking.** For school uses, Planning Code Section 155.2 requires four Class 1 bicycle parking spaces for every classroom and one Class 2 bicycle parking space for every classroom.

There are five Class-2 bicycle parking spaces on the campus and the project would provide additional bicycle parking spaces; however, the bicycle parking supply and type would vary depending on the driveway option. Driveway Design 1 would provide 41 Class-1 bicycle parking spaces: 18 Class-1 spaces located within proposed gymnasium and 23 spaces within an enclosure adjacent to the ADA passenger vehicle spaces, and 19 Class-2 bicycle parking spaces: 5 existing spaces located within the

parking lot and 14 spaces at the pedestrian entry from West Portal Avenue. Driveway Design 2 would provide 23 Class-1 bicycle parking spaces in the enclosure adjacent to the ADA passenger vehicle spaces and 19 Class-2 spaces near the pedestrian entry from West Portal Avenue. Per Section 155.2 of the Planning Code, the project would be required to provide a minimum of 76 Class-1 bicycle parking spaces (4 spaces per classroom) and a minimum of 19 Class-2 bicycle parking spaces (one space per classroom). Because the school would not comply with the minimum Class-1 bicycle parking spaces per the Planning Code, the school would seek an exception to these requirements of up to 53 spaces.

Given the school's proximity to transit and current travel mode behavior, the transportation analysis provided for the Project by Nelson Nygaard dated November 4, 2016 found that the Project would sufficiently meet the bicycle parking requirements by planning to install at least 42 total bicycle parking spaces, depending on the driveway variant. However, per the Code, the project would be required to provide 90 bicycle parking spaces (76 Class 1 bicycle parking spaces and 19 Class 2 spaces – 14 new spaces plus the existing 5 spaces on site). With a gradual, potential increase in the student body population, there may be an increase in bicycle trips to/from the school on a daily basis. However, as presented, field observations noted that about 4-6 students currently bike to the school on given day. The proposed project would increase student enrollment to up to 240 students and increase employees to a total of 32 faculty/staff; the increase in bicycle trips to/from the school may correlate to an additional 2 to 3 bicycle trips, but not to a degree of bicycle activity that would warrant more spaces than are planned for the school. Moreover, although the number of bike trips may vary day to day, the provision of 90 bicycle parking spaces does not correlate with anticipated demand; therefore, providing no less than 42 total bicycle parking spaces would be adequate.

- H. Showers & Lockers.** For Institutional Uses, Planning Code Section 155.4 requires two showers and 12 clothes lockers where the Occupied Floor Area exceeds 20,000 square feet but is no greater than 50,000 square feet.

The Project complies with Section 155.4 as it will provide two showers and 16 clothes lockers.

- I. Height.** Planning Code Section 260 requires that all structures be no taller than the height prescribed in the subject height and bulk district. The proposed project is located in a 40-X Height and Bulk District, with a 40-foot height limit.

The Gymnasium addition will be 35 feet to the finished roof.

- J. Transportation Sustainability Fee.** Planning Code Section 411A is applicable to new non-residential use over 800 gross square feet.

The Project and Project Sponsor qualify for an exemption, as outlined in Planning Code Section 411A.3 (b) (7). Therefore, the Project is exempt from the Transportation Sustainability Fee.

- K. Signage.** Any proposed signage will be subject to the review and approval of the Planning Department.

6. **Planning Code Section 303** establishes criteria for the Planning Commission to consider when reviewing applications for Conditional Use approval. On balance, the project does comply with said criteria in that:

- A. The proposed use or feature, at the size and intensity contemplated and at the proposed location, will provide a development that is necessary or desirable for, and compatible with, the neighborhood or the community.

The Project is necessary and desirable for, and compatible with the neighborhood. The Project would add a new multi-purpose gymnasium, a theater, and classroom spaces and modernize an established independent high school with a college-preparatory curriculum in math, science, arts and humanities. The Project would expand the existing secondary school, thus enhancing the educational opportunities across the City, providing a unique educational opportunity that nurtures and inspires students of diverse backgrounds to achieve their highest academic and creative potential. The project will improve the campus for existing students and will allow for expanded educational opportunities, thereby improving the educational services provided to the community and the City as a whole. A large percentage of the proposed project under Phase 2 will be within the existing building footprint. The new multipurpose gymnasium will have and added benefit to provide space for community gatherings and greatly sought after space for school athletic events.

- B. Such use or feature as proposed will not be detrimental to the health, safety, convenience or general welfare of persons residing or working in the vicinity, or injurious to property, improvements or potential development in the vicinity, with respect to aspects including but not limited to the following:

- i. Nature of proposed site, including its size and shape, and the proposed size, shape and arrangement of structures;

The Project has been designed to be compatible with the existing building on-site and surrounding buildings and will be within the applicable 40 foot height limit. The new multi-purpose gymnasium will be located along West Portal Avenue, and the classrooms located below the existing building under Phase 2 would be primarily within the existing footprint. The location of the gymnasium along West Portal will provide a natural boundary separating the student activity from the street, and provide a large separation between the building mass and the adjacent residential Ardenwood properties. The neighbors support the location of the gym along West Portal based on this separation, as a building closer to the back of the existing school parking lot, at the west property line, would impede light and air to their properties. The location of the additional classrooms beneath the existing building under Phase 2 will not significantly increase the size of shape of the current building. The project will thus be compatible with the neighborhood, thereby avoiding injury to property improvements or potential development in the vicinity and promoting the general welfare of persons residing or working in the vicinity.

The Project site and buildings are accessible to visitors and students with disabilities by providing accessibility to all floors (including entries, classrooms, restrooms, and exterior spaces) of the building. In furtherance of the City's commitment to sustainability, the proposed

project would promote energy efficient building systems and lighting, resource efficiency, a green living wall along West Portal, indoor environmental quality, and other sustainable design strategies. The Project's proposed arrangement and size (less than maximum floor area ratio) do not pose any detriment to the health, safety, and convenience of persons residing or working in the vicinity of the Property.

- ii. The accessibility and traffic patterns for persons and vehicles, the type and volume of such traffic, and the adequacy of proposed off-street parking and loading;

The project site is located at the intersection of Sloat Boulevard, Junipero Serra Boulevard, St. Francis Boulevard, Portola Drive and West Portal Avenue. In the vicinity of the campus is 19th Avenue, which continues as California State Route 1. Other than these streets, the surrounding neighborhood is comprised of residential cul-de-sacs to the west, and residential streets to the south and east. To the north, West Portal Avenue serves as a medium-density retail corridor featuring a shared right-of-way with Muni light rail. Muni K and M-Lines stop directly in front of the subject property where the street narrows to two lanes. There is no street parking in the front of the property.

Pursuant to the transportation analysis provided for the Project by Nelson Nygaard, anticipated traffic patterns are expected to remain the same where currently up to 75% of the students use public transportation. Students who arrive by private automobile are typically dropped off site to avoid being directed westward on Sloat Boulevard. Of the total current student population (about 150 students), about 42 students arrive and depart via private auto, either parking at school or being dropped off; this represents a 28% auto mode share, whereas the remaining 72% utilize other modes of transportation. Many fewer students depart via private auto than are dropped off in the morning as students either use other modes to get home or depart school with student groups to participate in sports or socialize.

The modest increase in employment and enrollment is expected to have a minimal impact on traffic and neighborhood parking conditions, including pick-up and drop-off conditions. Assuming similar drop-off/pick-up patterns for students that arrive/depart via private auto, the increase in 24 daily auto trips would result in about 50 vehicles entering and exiting in the morning and about 33 vehicles in the afternoon. There are currently 24 employees (16 teachers and 8 staff) at the school and once the project is completed and operational, the number of employees would increase to up to 32, at maximum. Mode split information was provided by school administration, and it is estimated that between 80% and 90% of school employees currently drive their own private vehicle and park at the school; the remaining 10% to 20% take public transit, walk or bike. By applying the same travel mode splits to the number of new employees (about 12), this would result in an increase in about 10 auto trips and 2 non-auto trips to/from the school on a daily basis. Based on these findings, the project would result in an increase in 38 new auto trips (24 from student drop-off/pick-up, 4 from student and 10 employees drive-alone/self-park trips), and 74 new non-auto trips (72 from students and 2 from employees), respectively. As the proposed drop-off lanes in both Driveway Design Variant 1 and 2 are longer than the existing lane, no queuing issues are anticipated. Observations conducted for existing conditions indicated that at maximum, two vehicles queued at the same time for student drop-off/pick-up. Based upon maximum enrollment post-

construction and the current ratio between the total number of students and the number of students currently dropped-off/picked-up, there would be a maximum of four vehicles at once.

School policy prohibits students from driving personal vehicles unless given special permission. As noted above, the proposed project will displace excess off-street parking spaces under Driveway Design Variant 1 or 2; however, these spaces are currently used for outdoor activities and storage. These functions will be provided within the proposed buildings. No off-street loading spaces will be necessary or proposed. Pursuant to Planning Code Section 152, none is required.

- iii. The safeguards afforded to prevent noxious or offensive emissions such as noise, glare, dust and odor;

The proposed project will not produce any noxious or offensive emissions, dust or odor. During construction, the General Contractor will incorporate necessary measures to ensure compliance with all necessary regulations. Once construction is completed, no loose gravel or dust will be present on the site. The current campus was designed and constructed as a green building, receiving LEED Gold Certification. The proposed project will be developed to reach equally high environmental standards. The new construction will not use reflective or glare-producing materials, and will use insulated glass and materials to mitigate sound transmission. Dust control measures will be implemented during construction, pursuant to the Construction Dust Ordinance (Article 22B of the San Francisco Health Code) and a project-specific Construction Dust Control Plan must be reviewed and approved by the San Francisco Department of Public Health (DPH). A community liaison will also be appointed by the project sponsor to address any related concerns.

With regard to construction noise, the project sponsor will adhere to the city's Noise Control Ordinance, which limits construction hours to between 7 a.m. and 8 p.m., seven days a week.

- iv. Treatment given, as appropriate, to such aspects as landscaping, screening, open spaces, parking and loading areas, service areas, lighting and signs;

The Project does not possess any additional off-street parking or loading, which is in compliance with the Planning Code. The existing parking spaces and hardscape frontage on West Portal Avenue will be removed and replaced with the proposed multi-purpose gymnasium building. The façade of the building wall will be developed as a living façade, to present a vertical landscape toward the MUNI stop (Lines K and M) and St. Francis Circle. The living façade will absorb traffic noise and screen the campus activities from the street. Parking and storage will be screened from view. Outdoor lighting will be minimized and screened to limit direct lighting from the campus onto the neighboring properties.

As noted above, along the length of the proposed new gymnasium, the project intends to shift the paved walkway away from the street, separating the sidewalk from the roadway by a new planting strip in order to improve and facilitate safe pedestrian movement along the corridor. The proposal includes an additional 2 feet of planted area between the gymnasium and the paved sidewalk, bringing the width of sidewalk area to 14 feet. The paved area at the dedicated

pedestrian entry adjacent to the new gymnasium will be expanded and 12 new street trees will be provided. Additionally there will be enhanced plantings within the public ROW at the southern end of the site. The streetscape improvements have been reviewed and endorsed by the City's Street Design Advisory Team.

Additionally, as indicated previously, the Project has developed a revised Planting and Maintenance Plan, for that landscaped area adjacent to the Ardenwood residences. This plan is incorporated into the Conditions of Approval in Exhibit A.

- C. That the use as proposed will comply with the applicable provisions of the Planning Code and will not adversely affect the General Plan.

The project complies with all relevant requirements and standards of the Planning Code and is consistent with objectives and policies of the General Plan as detailed below. This construction enhances the quality of the San Francisco Waldorf High School's existing facilities by providing modernized facilities, and allows for athletic uses that must currently be met at off-campus locations. The proposed addition is conditionally permitted within the RH-1 Zoning District.

7. **Planning Code Section 304** establishes procedures for Planned Unit Developments, which are intended for projects on sites of considerable size, including an area of not less than half-acre, developed as integrated units and designed to produce an environment of stable and desirable character, which will benefit the occupants, the neighborhood and the City as a whole. In the cases of outstanding overall design, complementary to the design and values of the surrounding area, such a project may merit a well-reasoned modification of certain provisions contained elsewhere in the Planning Code.

- A. In cases of outstanding overall design, complementary to the design and values of the surrounding area, such a project may merit a well-reasoned modification of certain provisions contained elsewhere in the Planning Code.

The new facilities proposed have been designed to be complementary to the design and values of the surrounding area. The proposed Project would develop the primary building density of the multi-purpose gymnasium building toward the street and would continue to provide significant mid-block open space toward the rear yards of the adjacent single-family residences along Ardenwood Way. The expansion of the classrooms at the lower level would be primarily within the existing building footprint with an outdoor terrace extension oriented toward the largely wooded eucalyptus glen at the rear yard of the Arden Wood retirement home on Wawona Street. The Project will be developed in a manner that is appropriately scaled to the surrounding residential buildings, while presenting a living wall façade treatment for the gymnasium that clearly defines it. Thus, the project complements the design and residential character of the surrounding area.

- B. **Modifications.** The Project Sponsor requests the following modification from the requirements of the Planning Code. These modifications are listed below, along with reference to the relevant discussion for each modification.

Front Setback: *Since the Project does not provide a code-complying front setback, the Project is seeking a modification of the front setback requirement defined in Planning Code Section 132.*

Planning Code Section 132 requires minimum front setback areas to apply to every building in all RH, RTO, and RM Districts, in order to relate the setbacks provided to the existing front setbacks of adjacent buildings. Setbacks are based on the average of adjacent buildings; up to 15 feet or 15% of the lot depth. The proposed multi-purpose gymnasium building sits at the front property line. Therefore, the school would seek an exception to this requirement. The proposed multi-purpose gymnasium would develop the primary building density of the multi-purpose gymnasium building toward the street, and has been designed to minimize disruption of adjacent residential areas. The proposed additions will be located on the existing campus and have been designed to be compatible with the existing building and surrounding commercial buildings along West Portal, and will be within the applicable 40 foot height limit. The Project would provide significant mid-block open space toward the rear yards of the adjacent single-family residences along Ardenwood Way. The proposed project is designed to be architecturally cohesive with the surrounding residential neighborhood and of a height and density appropriate to the scale of the surrounding properties; therefore, the intent of the front setback requirement would be met.

Rear Yard: *Since the Project does not provide a code-complying rear yard, the Project is seeking a modification of the rear yard requirement defined in Planning Code Section 134.*

The general purpose of rear yard setback is to provide open space for residential uses and sufficient light and air to adjacent buildings, especially residential buildings. Planning Code Section 134 requires a 25% rear yard in the RH-1(D) District. The existing, noncomplying original building and the proposed expansion below the existing building under Phase 2 are within the required rear yard. Due to its size, composition and irregular shape, the Project Site is distinguishable from most other lots regulated by the Planning Code. As noted above, the proposed Project would develop the primary building density of the multi-purpose gymnasium building toward the street and would continue to provide significant mid-block open space toward the rear yards of the adjacent single-family residences along Ardenwood Way. Therefore, the intent of the rear yard requirement would be met.

Bicycle Parking: *Since the Project would not comply with the minimum Class-1 bicycle parking spaces per Planning Code Section 155.2, the school would seek an exception to these requirements of up to 76 spaces.*

For school uses, Planning Code Section 155.2 requires four Class 1 bicycle parking spaces for every classroom and one Class 2 bicycle parking space for every classroom. There are five Class-2 bicycle parking spaces on the campus and the project would provide additional bicycle parking spaces; however, the bicycle parking supply and type would vary depending on the driveway options. Driveway Design 1 would provide 41 Class-1 bicycle parking spaces: 18 Class-1 spaces located within proposed gymnasium and 23 spaces within an enclosure adjacent to the ADA passenger vehicle spaces, and 19 Class-2 bicycle parking spaces: 5 existing spaces located within the parking lot and 14 spaces at the pedestrian entry from West Portal Avenue. Driveway Design 2 would provide 23 Class-1 bicycle parking spaces in the enclosure adjacent to the ADA passenger vehicle spaces and 19 Class-2 spaces near the pedestrian entry from West Portal Avenue. Per Section 155.2 of the Planning Code, the project would be required to provide a minimum of 76 Class-1 bicycle parking spaces (4 spaces per

classroom) and a minimum of 19 Class-2 bicycle parking spaces (one space per classroom). Because the school would not comply with the minimum Class-1 bicycle parking spaces per the Planning Code, the school would seek an exception to these requirements of up to 53 spaces.

Given the school's proximity to transit and current travel mode behavior, the transportation analysis provided for the Project by Nelson Nygaard dated November 4, 2016 found that the Project would sufficiently meet the bicycle parking requirements by planning to install at least 42 total bicycle parking spaces, depending on the driveway variant. However, per the Code, the project would be required to provide 90 bicycle parking spaces (72 Class 1 bicycle parking spaces and 19 Class 2 spaces, including the 5 existing spaces). With a gradual, potential increase in the student body population, there may be an increase in bicycle trips to/from the school on a daily basis. However, as presented, field observations noted that about 4-6 students currently bike to the school on given day. The proposed project would increase student enrollment to up to 240 students and increase employees to a total of 32 faculty/staff; the increase in bicycle trips to/from the school may correlate to an additional 2 to 3 bicycle trips, but not to a degree of bicycle activity that would warrant more spaces than are planned for the school. Moreover, although the number of bike trips may vary day to day, the provision of 90 bicycle parking spaces does not correlate with anticipated demand; therefore, providing approximately 42 total bicycle parking spaces would be more than adequate, and the intent of the bicycle-parking requirement would be met.

- C. **Criteria and Limitations** Section 304(d) establishes criteria and limitations for the authorization of PUDs over and above those applicable to Conditional Uses in general and contained in Section 303 and elsewhere in the Code. On balance, the Project complies with said criteria in that it:

- 1) Affirmatively promotes applicable objectives and policies of the General Plan;

The Project complies with the objectives and policies of the General Plan (See Below).

- 2) Provides off-street parking adequate for the occupancy proposes.

The Project provides off-street parking adequate for the proposed school additions. Under Driveway Design Option 1, the proposed project would remove 34 parking spaces and retain 31 spaces, two of which are ADA-accessible (one ADA-accessible van parking space and one ADA-accessible passenger vehicle space). Driveway Design Option 2 would remove 37 parking spaces and retain 28 spaces, two of which are ADA-accessible (one ADA-accessible van parking space and one ADA-accessible passenger vehicle space). Under both variants the existing parking lot would be reduced to about 13,035 square feet. Under either scenario, the proposed project would meet Planning Code Section 151 requirements for off-street parking at one off-street space for each two classrooms (.5 spaces per classroom): 19 classrooms x .5 spaces = 10 spaces. The Project Site is well-served by public transit, including the K and M - MUNI lines, and bus lines.

- 3) Provide open space usable by the occupants and, where appropriate, by the general public, at least equal to the open spaces required by this Code;

The Project exceeds the required amount of open space. The Project will provide open space useable by occupants totaling over 27,715 square feet. This open space includes farm use, a garden, an orchard, and two courtyards.

- 4) Be limited in dwelling unit density to less than the density that would be allowed by Article 2 of this Code for a district permitting a greater density, so that the Planned Unit Development will not be substantially equivalent to a reclassification of property;

No dwelling units are proposed.

- 5) In R Districts, include commercial uses only to the extent that such uses are necessary to serve residents of the immediate vicinity, subject to the limitations for NC-1 Districts under this Code, and in RTO Districts include commercial uses only according to the provisions of Section 230 of this Code;

The Project does not contain or propose commercial uses.

- 6) Under no circumstances be excepted from any height limit established by Article 2.5 of this Code, unless such exception is explicitly authorized by the terms of this Code. In the absence of such an explicit authorization, exceptions from the provisions of this Code with respect to height shall be confined to minor deviations from the provisions for measurement of height in Sections 260 and 261 of this Code, and no such deviation shall depart from the purposes or intent of those sections.

The Project is not requesting any exceptions to the height limits. The Project complies with the 40-X Height and Bulk District.

- 7) In NC Districts, be limited in gross floor area to that allowed under the floor area ratio limit permitted for the district in Section 124 and Article 7 of this Code;

The Project is not located within a NC District.

- 8) In NC Districts, not violate the use limitations by story set forth in Article 7 of this Code; and

The Project is not located within a NC District.

- 9) In RTO and NCT Districts, include the extension of adjacent alleys or streets onto or through the site, and/or the creation of new publicly-accessible streets or alleys through the site as appropriate, in order to break down the scale of the site, continue the surrounding existing pattern of block size, streets and alleys, and foster beneficial pedestrian and vehicular circulation.

The Project is not located in a RTO or NCT District.

- 10) Provide Street trees as per the requirements of Section 138.1 of the Code.

Per Planning Code Section 138.1(c) (1), the Department of Public Works is responsible for reviewing and guiding any new street trees present on the project site.

- 11) Provide landscaping and permeable surfaces in any required setbacks in accordance with Section 132 (g) and (h).

The Project is not subject to the requirements of Planning Code Section 132(g) and (h); however, the Project does provide new streetscape elements, including new street trees, new bicycle parking spaces, new sidewalk paving.

8. **General Plan Compliance.** The Project is, on balance, consistent with the following Objectives and Policies of the General Plan:

GENERAL PLAN – COMMERCE & INDUSTRY ELEMENT

OBJECTIVE 1:

MANAGE ECONOMIC GROWTH AND CHANGE TO ENSURE ENHANCEMENT OF THE TOTAL CITY LIVING AND WORKING ENVIRONMENT.

Policy 1.1

Encourage development which provides substantial net benefits and minimizes undesirable consequences. Discourage development which has substantial undesirable consequences that cannot be mitigated.

The Project will have substantial environmental effects and will result in economic and social benefits. The proposed school will be a net-zero energy campus. The design incorporates passive strategies to capture heat and light to minimize resource demands. Additionally, where roofs are not used for green space, solar arrays will be employed to generate electricity to cover any required power needs for San Francisco Waldorf High School. The Project will also provide positive fiscal and employment benefits. The school faculty, staff, students, and parents will likely patronize local businesses. The Project will provide additional full- and part-time employment. The Project will contribute an established school to the neighborhood and the City, where students will receive a high-quality education.

OBJECTIVE 7:

ENHANCE SAN FRANCISCO'S POSITION AS A NATIONAL AND REGIONAL CENTER FOR GOVERNMENTAL, HEALTH, AND EDUCATIONAL SERVICES.

Policy 7.2

Encourage the extension of needed health and educational services, but manage expansion to avoid or minimize disruption of adjacent residential areas.

The proposed project has been designed to minimize disruption of adjacent residential areas. The proposed additions will be located on the existing campus and have been designed to be compatible with the existing building and surrounding buildings and will be within the applicable 40 foot height limit.

The proposed increase in student enrollment is critical to fulfilling the school's mission to increase tuition accessibility to students of all backgrounds from the Bay Area. Increased enrollment will also provide greater educational opportunities to a diverse body of students drawn from the community, thereby improving the educational services provided to the City as a whole.

Policy 7.3

Promote the provision of adequate health and educational services to all geographical districts and cultural groups in the city.

The Project will enhance the educational services available to residents of the local area neighborhoods as well as the City at large. The San Francisco Waldorf High School will continue to provide tuition assistance and outreach to a socially and economically diverse community.

GENERAL PLAN – HOUSING ELEMENT

OBJECTIVE 11:

SUPPORT AND RESPECT THE DIVERSE AND DISTINCT CHARACTER OF SAN FRANCISCO'S NEIGHBORBORHOODS.

Policy 11.8:

Consider a neighborhood's character when integrating new uses, and minimize disruption caused by expansion of institutions into residential areas.

The Project will minimize disruption by expanding the school with a gymnasium at the frontage along West Portal Avenue on the existing campus, and with a Phase 2 addition within the footprint of the existing building.

GENERAL PLAN – TRANSPORTATION ELEMENT

OBJECTIVE 2:

USE THE TRANSPORTATION SYSTEM AS A MEANS FOR GUIDING DEVELOPMENT AND IMPROVING THE ENVIRONMENT.

Policy 2.5:

Provide incentives for the use of transit, carpools, and vanpools, walking and bicycling and reduce the need for new or expanded automobile and automobile parking facilities.

The San Francisco Waldorf High School would encourage use of alternative means of transportation, including bicycling, public transit and carpools. The potential for increased traffic due to the school activity will be minimized through a well-planned and monitored traffic management plan for drop-off and pick-up of students.

Given the school's proximity to transit and current travel mode behavior, the transportation analysis provided for the Project by Nelson Nygaard dated November 4, 2016 found that the Project would sufficiently meet bicycle parking demands by planning to install at least 42 total bicycle parking spaces, depending on the driveway variant.

COMMUNITY SAFETY ELEMENT

Hazard Mitigation

Objectives and Policies

OBJECTIVE 2:

REDUCE STRUCTURAL AND NON-STRUCTURAL HAZARDS TO LIFE SAFETY, MINIMIZE PROPERTY DAMAGE AND RESULTING SOCIAL, CULTURAL AND ECONOMIC DISLOCATIONS RESULTING FROM FUTURE DISASTERS.

The proposed project would comply with all required Building and Fire Code provisions to ensure life safety in case of future disasters.

URBAN DESIGN ELEMENT

OBJECTIVE 1:

EMPHASIS OF THE CHARACTERISTIC PATTERN WHICH GIVES TO THE CITY AND ITS NEIGHBORHOODS AS IMAGE, A SENSE OF PURPOSE, AND A MEANS OF ORIENTATION.

Policy 1.2:

Protect and reinforce the existing street pattern, especially as it is related to topography.

Policy 1.3:

Recognize that buildings, when seen together, produce a total effect that characterizes the city and its districts.

The Project would improve the appearance of the neighborhood. The addition of the new gymnasium with its living façade will transform an existing parking lot to provide a landmark and a new identity for St. Francis Circle demonstrating the potential to creatively provide green space and sustainable building within the urban environment.

OBJECTIVE 4:

IMPROVEMENT OF THE NEIGHBORHOOD ENVIRONMENT TO INCREASE PERSONAL SAFETY, COMFORT, PRIDE AND OPPORTUNITY.

NEIGHBORHOOD ENVIRONMENT

Visual Amenity

Policy 4.10:

Encourage or require the provision of recreation space in private development.

The proposed project is designed to be architecturally cohesive with the surrounding residential neighborhood and of a height and density appropriate to the scale of the surrounding properties. The project would provide a means for the San Francisco Waldorf High School community to participate in home athletic events strengthening, school spirit and identity.

Policy 4.12:

Install, promote and maintain landscaping in public and private areas.

While there is no usable open space requirement for institutional uses in an R District, the project will provide approximately 27,715 square feet of open space.

9. **Planning Code Section 101.1(b)** establishes eight priority-planning policies and requires review of permits for consistency with said policies. On balance, the project does comply with said policies in that:

- A. That existing neighborhood-serving retail uses be preserved and enhanced and future opportunities for resident employment in and ownership of such businesses be enhanced.

No neighborhood-serving retail uses exist on the site. In addition, the Project would not directly affect any nearby neighbor-serving retail uses.

- B. That existing housing and neighborhood character be conserved and protected in order to preserve the cultural and economic diversity of our neighborhoods.

The expansion to an existing school building has been designed to be sensitive to the surrounding neighborhood character. Overall, the school use is beneficial and supports children and families in the City.

- C. That the City's supply of affordable housing be preserved and enhanced,

No designated affordable housing is created or removed as part of this Project; therefore, the Project will not affect the City's supply of affordable housing.

- D. That commuter traffic not impede MUNI transit service or overburden our streets or neighborhood parking.

The Project will not generate significant commuter traffic that would impede Muni transit service or overburden streets or neighborhood parking. The majority of students and faculty commute to school on public transit. Staff members would monitor and manage the pick-up and drop-off process in order to ensure no traffic disruptions and promote the orderly flow of traffic.

- E. That a diverse economic base be maintained by protecting our industrial and service sectors from displacement due to commercial office development, and that future opportunities for resident employment and ownership in these sectors be enhanced.

The Project does not include commercial office development, and the Project site does not possess any industrial or service sector businesses. Rather, the Project is expected to create new job opportunities for faculty/staff, thus providing future opportunity for resident employment.

- F. That the City achieve the greatest possible preparedness to protect against injury and loss of life in an earthquake.

The Project is designed and will be constructed to conform to the structural and seismic safety requirements of the City Building Code. This proposal will not impact the property's ability to withstand an earthquake.

- G. That landmarks and historic buildings be preserved.

A landmark or historic building does not occupy the Project site.

- H. That our parks and open space and their access to sunlight and vistas be protected from development.

The Project will have no negative impact on existing parks and open spaces. The Project does not have an impact on open spaces.

10. The Project is consistent with and would promote the general and specific purposes of the Code provided under Section 101.1(b) in that, as designed, the Project would contribute to the character and stability of the neighborhood and would constitute a beneficial development.
11. The Commission hereby finds that approval of the Conditional Use authorization would promote the health, safety and welfare of the City.

DECISION

That based upon the Record, the submissions by the Applicant, the staff of the Department and other interested parties, the oral testimony presented to this Commission at the public hearings, and all other written materials submitted by all parties, the Commission hereby **APPROVES Conditional Use Application No. 2015-004567CUA**, subject to the following conditions attached hereto as "EXHIBIT A" in general conformance with plans on file, dated November 15, 2016, and stamped "EXHIBIT B", which is incorporated herein by reference as though fully set forth.

APPEAL AND EFFECTIVE DATE OF MOTION: Any aggrieved person may appeal this Conditional Use Authorization to the Board of Supervisors within thirty (30) days after the date of this Motion No. 19757. The effective date of this Motion shall be the date of this Motion if not appealed (After the 30-day period has expired) OR the date of the decision of the Board of Supervisors if appealed to the Board of Supervisors. For further information, please contact the Board of Supervisors at (415) 554-5184, City Hall, Room 244, 1 Dr. Carlton B. Goodlett Place, San Francisco, CA 94102.

Protest of Fee or Exaction: You may protest any fee or exaction subject to Government Code Section 66000 that is imposed as a condition of approval by following the procedures set forth in Government Code Section 66020. The protest must satisfy the requirements of Government Code Section 66020(a) and must be filed within 90 days of the date of the first approval or conditional approval of the development referencing the challenged fee or exaction. For purposes of Government Code Section 66020, the date of imposition of the fee shall be the date of the earliest discretionary approval by the City of the subject development.

If the City has not previously given Notice of an earlier discretionary approval of the project, the Planning Commission's adoption of this Motion, Resolution, Discretionary Review Action or the Zoning Administrator's Variance Decision Letter constitutes the approval or conditional approval of the development and the City hereby gives **NOTICE** that the 90-day protest period under Government Code Section 66020 has begun. If the City has already given Notice that the 90-day approval period has begun for the subject development, then this document does not re-commence the 90-day approval period.

I hereby certify that the Planning Commission **ADOPTED** the foregoing Motion on December 15, 2016.

Jonas P. Ionin
Commission Secretary

AYES:

NAYS:

ABSENT:

ADOPTED: December 15, 2016

EXHIBIT A

AUTHORIZATION

This authorization is for a Conditional Use Authorization for a Planned Unit Development (PUD) to allow a two-phase expansion to a private secondary school (d.b.a. The San Francisco Waldorf High School), that would include modifications to the front setback and rear yard requirements (Planning Code Section 132 and 134), and Class-1 Bicycle Parking requirement (Planning Code Section 155.2), located at 470 West Portal Avenue, Lots 008 and 009 in Assessor's Block 2484 and Lot 001 in Assessor's Block 2540, pursuant to Planning Code Section(s) 132, 134, 155.2, 209.1, 303 and 304 within the Residential-House One Family (RH-1) Zoning District and a 40-X Height and Bulk District; in general conformance with plans, dated November 15, 2016, and stamped "EXHIBIT B" included in the docket for Case No. **2015-004567CUA** and subject to conditions of approval reviewed and approved by the Commission on **December 15, 2016** under Motion No **XXXXX**. This authorization and the conditions contained herein run with the property and not with a particular Project Sponsor, business, or operator.

RECORDATION OF CONDITIONS OF APPROVAL

Prior to the issuance of the building permit or commencement of use for the Project the Zoning Administrator shall approve and order the recordation of a Notice in the Official Records of the Recorder of the City and County of San Francisco for the subject property. This Notice shall state that the project is subject to the conditions of approval contained herein and reviewed and approved by the Planning Commission on **December 15, 2016 under Motion No XXXXX**.

PRINTING OF CONDITIONS OF APPROVAL ON PLANS

The conditions of approval under the 'Exhibit A' of this Planning Commission Motion No. **XXXXX** shall be reproduced on the Index Sheet of construction plans submitted with the site or building permit application for the Project. The Index Sheet of the construction plans shall reference to the Conditional Use authorization and any subsequent amendments or modifications.

SEVERABILITY

The Project shall comply with all applicable City codes and requirements. If any clause, sentence, section or any part of these conditions of approval is for any reason held to be invalid, such invalidity shall not affect or impair other remaining clauses, sentences, or sections of these conditions. This decision conveys no right to construct, or to receive a building permit. "Project Sponsor" shall include any subsequent responsible party.

CHANGES AND MODIFICATIONS

Changes to the approved plans may be approved administratively by the Zoning Administrator. Significant changes and modifications of conditions shall require Planning Commission approval of a new Conditional Use authorization.

Conditions of Approval, Compliance, Monitoring, and Reporting

PERFORMANCE

1. **Validity.** The authorization and right vested by virtue of this action is valid for the duration of the phased development as described above under Finding No. 4 - Project Description until 2026 or ten (10) years from the effective date of the Motion. The Department of Building Inspection shall have issued a Building Permit or Site Permit to construct the project and/or commence the approved use within this ten-year period.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

2. **Expiration and Renewal.** Should a Building or Site Permit be sought after the ten (10) year period has lapsed, the project sponsor must seek a renewal of this Authorization by filing an application for an amendment to the original Authorization or a new application for Authorization. Should the project sponsor decline to so file, and decline to withdraw the permit application, the Commission shall conduct a public hearing in order to consider the revocation of the Authorization. Should the Commission not revoke the Authorization following the closure of the public hearing, the Commission shall determine the extension of time for the continued validity of the Authorization.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

3. **Diligent pursuit.** Once a site or Building Permit has been issued, construction must commence within the timeframe required by the Department of Building Inspection and be continued diligently to completion. Failure to do so shall be grounds for the Commission to consider revoking the approval if more than ten (10) years have passed since this Authorization was approved.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

4. **Extension.** All time limits in the preceding three paragraphs may be extended at the discretion of the Zoning Administrator where implementation of the project is delayed by a public agency, an appeal or a legal challenge and only by the length of time for which such public agency, appeal or challenge has caused delay.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

5. **Conformity with Current Law.** No application for Building Permit, Site Permit, or other entitlement shall be approved unless it complies with all applicable provisions of City Codes in effect at the time of such approval.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

STUDENT ENROLLMENT

6. **Enrollment.** Allowed enrollment for the San Francisco Waldorf High School shall be increased from 200 students to up to a total of 240 students upon issuance of the first certificate of occupancy for the multi-purpose gymnasium.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org.

BICYCLE PARKING

7. **Bicycle Parking.** The project shall **provide no less than 42 bicycle parking spaces** – as delineated per Driveway Design 2, with 23 Class-1 bicycle parking spaces in the enclosure adjacent to the ADA passenger vehicle spaces and 19 Class-2 spaces near the pedestrian entry from West Portal Avenue. If feasible, alternate Driveway Design 1 would provide a total of 60 bicycle parking spaces, with 41 Class-1 bicycle parking spaces: 18 Class-1 spaces located within proposed gymnasium and 23 spaces within an enclosure adjacent to the ADA passenger vehicle spaces, and 19 Class-2 bicycle parking spaces: 5 existing spaces located within the parking lot and 14 spaces at the pedestrian entry from West Portal Avenue.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org.

OPERATION

8. **Garbage, Recycling, and Composting Receptacles.** Garbage, recycling, and compost containers shall be kept within the premises and hidden from public view, and placed outside only when being serviced by the disposal company. Trash shall be contained and disposed of pursuant to garbage and recycling receptacles guidelines set forth by the Department of Public Works.

For information about compliance, contact Bureau of Street Use and Mapping, Department of Public Works at 415-554-5810, <http://sfdpw.org>.

9. **Sidewalk Maintenance.** The project sponsor shall maintain the entrances to the multi-purpose gymnasium, relocated main school entrance, and all sidewalks abutting these buildings in a clean and sanitary condition in compliance with the Department of Public Works Streets and Sidewalk Maintenance Standards.

For information about compliance, contact Bureau of Street Use and Mapping, Department of Public Works, 415-695-2017, <http://sfdpw.org>.

10. **Community Liaison.** Prior to issuance of a building permit to construct the project and implement the approved use, the project sponsor shall appoint a community liaison officer to deal with the issues of concern to owners and occupants of nearby properties. The project sponsor shall provide the Zoning Administrator with written notice of the name, business address, and telephone number of the community liaison. Should the contact information change, the Zoning Administrator shall be made aware of such change. The community liaison shall report to the Zoning Administrator what issues, if any, are of concern to the community and what issues have not been resolved by the Project Sponsor.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org.

DESIGN – COMPLIANCE AT PLAN STAGE

11. **Final Driveway Location and Resultant Building Design.** The Project Sponsor shall work with the Planning Department on the final driveway location per Driveway Design Variant Option 1 (encroachment onto AT&T easement) or Driveway Design Variant Option 2 (outside of AT&T easement), and the final building design. The approval shall be subject to Department staff review and approval prior to building permit issuance. Any significant changes outside the scope of the proposed driveway designs alternatives would require Planning Commission approval of a new Conditional Use Authorization.

For information about compliance, contact the Case Planner, Planning Department at 415-575-8728, www.sf-planning.org.

12. **Final Materials.** The Project Sponsor shall continue to work with Planning Department on the building design. Final materials, glazing, color, texture, landscaping, and detailing shall be subject to Department staff review and approval. The architectural addenda shall be reviewed and approved by the Planning Department prior to issuance.

For information about compliance, contact the Case Planner, Planning Department at 415-575-8728, www.sf-planning.org.

13. **Garbage, composting and recycling storage.** Space for the collection and storage of garbage, composting, and recycling shall be provided within enclosed areas on the property and clearly labeled and illustrated on the building permit plans. Space for the collection and storage of recyclable and compostable materials that meets the size, location, accessibility and other standards specified by the San Francisco Recycling Program shall be provided at the ground level of the buildings.

For information about compliance, contact the Case Planner, Planning Department at 415-575-8728, www.sf-planning.org.

14. **Showers and Clothes Lockers.** Pursuant to Planning Code Section 155.4, the Project shall provide no fewer than **two** showers and **twelve** clothes lockers.

For information about compliance, contact the Case Planner, Planning Department at 415-558-8728, www.sf-planning.org.

15. **Transformer Vaults.** The location of individual project PG&E Transformer Vault installations has significant effects to San Francisco streetscapes when improperly located. However, they may not have any impact if they are installed in preferred locations. Therefore, the Planning Department recommends the following preference schedule in locating new transformer vaults, in order of most to least desirable:

- a. On-site, in a basement area accessed via a garage or other access point without use of separate doors on a ground floor façade facing a public right-of-way;
- b. On-site, in a driveway, underground;
- c. On-site, above ground, screened from view, other than a ground floor façade facing a public right-of-way;

- d. Public right-of-way, underground, under sidewalks with a minimum width of 12 feet, avoiding effects on streetscape elements, such as street trees; and based on Better Streets Plan guidelines;
- e. Public right-of-way, underground; and based on Better Streets Plan guidelines;
- f. Public right-of-way, above ground, screened from view; and based on Better Streets Plan guidelines;
- g. On-site, in a ground floor façade (the least desirable location).

Unless otherwise specified by the Planning Department, Department of Public Work's Bureau of Street Use and Mapping (DPW BSM) should use this preference schedule for all new transformer vault installation requests.

For information about compliance, contact Bureau of Street Use and Mapping, Department of Public Works at 415-554-5810, <http://sfdpw.org>

16. **Rooftop Mechanical Equipment.** Pursuant to Planning Code 141, the Project Sponsor shall submit a roof plan to the Planning Department prior to Planning approval of the building permit application. Rooftop mechanical equipment, if any is proposed as part of the Project, is required to be screened so as not to be visible from any point at or below the roof level of the subject building.

For information about compliance, contact the Case Planner, Planning Department at 415-575-8728, www.sf-planning.org.

17. **Streetscape Plan.** Pursuant to Planning Code Section 138.1, the Project Sponsor shall continue to work with Planning Department staff, in consultation with other City agencies, to refine the design and programming of the Streetscape Plan so that the plan generally meets the standards of the Better Streets Plan and all applicable City standards. The Project Sponsor shall complete final design of all required street improvements, including procurement of relevant City permits, prior to issuance of first architectural addenda, and shall complete construction of all required street improvements prior to issuance of first temporary certificate of occupancy.

For information about compliance, contact the Case Planner, Planning Department at 415-575-8728, www.sf-planning.org.

DESIGN – COMPLIANCE WITH LANDSCAPE PLANTING AND GROUNDS MAINTENANCE PLAN

18. **Creation of a Landscape and Grounds Maintenance Plan.** Within three months of the date of Conditional Use Authorization approval, the Project Sponsor shall submit for Planning Department approval a Landscape and Grounds Maintenance Plan (Plan) for 470 West Portal Avenue. During development of the Plan and before submitting it, the Project Sponsor shall consult with neighbors, including residents of Ardenwood Way, and shall include with the submitted Plan all comments received from neighbors. Planning Department shall use reasonable efforts to approve the Plan, after any modifications it may require, within three months after submittal, so that implementation will begin promptly. If and when the Project Sponsor updates or otherwise revises the Plan, on its own initiative or because the Planning Department requires this due to changes in standards, conditions or compliance issues, the Project Sponsor also shall consult with its neighbors. If a revised Plan is submitted to the Planning Department, all comments received from neighbors shall be included.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org.

19. **Purpose of the Landscape and Grounds Maintenance Plan, Standards and Phasing.** The Plan shall assure that the school grounds present a uniformly attractive, neat, and clean appearance.
- a. The Plan shall assure that the school grounds, including the area outside the fence bordering on Saint Francis Circle and the western slope adjoining Ardenwood Way parcels, present a uniformly attractive, neat, and clean appearance. The Plan shall assure screening of the western and northern sides of school buildings from neighboring properties. The Plan shall conform to applicable City requirements and policies regarding plantings, such as those concerning native species and drought tolerance, and shall be consistent with the school's LEED status. Neighboring parcel owners shall not place compostable yard refuse on school property without prior written agreement.
 - b. The Plan, and any revisions, shall reflect design, methods, staffing and time schedules for both installation and maintenance consistent with prevailing professional standards in the Bay Area and suited to an institutional parcel in an RH-1(D) zoning district. Because student or faculty involvement in landscape development or maintenance may be associated with the instructional program of the school, the Plan shall not preclude such involvement, but the Plan shall not rely solely on student and faculty effort for implementation.
 - c. The Plan may be organized in phases to reflect the near-term and longer-term building improvements and revisions recognized in the Conditional Use approval. The Plan in all phases shall meet the requirements of subsections (a) and (b), and construction and alteration activities shall interfere with or delay landscaping installation and maintenance as little as possible.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org.

20. **Monitoring and Compliance of Landscape Plan after Approval.** The Project Sponsor shall install landscaping as shown on Preliminary Planting Plan included in Exhibit B, submitted to the Planning Department on November 16, 2016, as revised to be consistent with Condition Nos. 18 and 19, and shall demonstrate to Planning Department compliance staff that said landscaping is established and maintained according to the Plan, per Condition Nos. 18 and 19. The Project Sponsor shall permit the Planning Department to conduct site inspections a minimum of one time per year as necessary to ensure that landscaping is maintained for the life of the project. Violation of these conditions shall be subject to the enforcement procedures and administrative penalties set forth under Planning Code Section 176 or Section 176.1. The Planning Department may also refer the violation complaints to other city departments and agencies for appropriate enforcement action under their jurisdiction.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org.

PARKING AND TRAFFIC - GENERAL

21. **Parking Requirement.** Pursuant to Planning Code Section 151, the Project shall provide **ten** independently accessible off-street parking spaces.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org.

22. **Managing Traffic During Construction.** The Project Sponsor and construction contractor(s) shall coordinate with the Traffic Engineering and Transit Divisions of the San Francisco Municipal Transportation Agency (SFMTA), the Police Department, the Fire Department, the Planning Department, and other construction contractor(s) for any concurrent nearby Projects to manage traffic congestion and pedestrian circulation effects during construction of the Project. *For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org.*

PARKING AND TRAFFIC - CIRCULATION MEMORANDUM IMPROVEMENT MEASURES

23. **Transportation Demand Management (TDM) Coordinator.** The Project Sponsor shall appoint a Transportation Demand Management (TDM) Coordinator that shall:

- Be responsible for preparing and modifying current Drop-Off/Pick-Up Plan and distributing the Plan to faculty, staff, parents, guardians, etc. on an annual basis;
- Establish goals and policies for student/staff transportation mode split (auto/transit/bike/walk) and regularly monitor the progress toward said goals;
- Periodically survey students, staff and parents to determine travel patterns, reasons for travel choices, barriers and potential opportunities for change;
- Discuss transportation options with new students and/or parents of students to ensure all options are understood;
- Encourage carpooling among students who must drive or be driven to school;
- Encourage a buddy system as needed to teach new students how to use transit, bike or walk to school;
- Provide incentives as needed to reach above goals for transportation mode choice, including subsidized transit passes
- Expand bicycle offerings to include repair station, secure parking, and maintenance services;
- Ensure adequate coverage of staff to monitor student arrival/departures including auto drop-off/pick-up situation and students' safe use of transit; and discourage illegal crossings or illegal parking in the school lot.
- Encourage and facilitate the use of alternative transportation modes by all attendees of evening special events (e.g. open houses, sports games) at the school; make alternative parking arrangements with nearby facilities (e.g. Scottish Rite) in the rare case of overflow parking demand.
- Provide parents with *Multimodal Access Guide* to describe how to reach the school by walking, bicycling, and transit. The guide should be provided to all new, incoming student's parents and redistributed every year to all students and faculty/staff. The guide may include:
 - A detailed map of nearby transit facilities (stops and routes) in vicinity of the school;
 - A detailed map of bicycle routes in the vicinity of the school; and
 - Provide online links and phone numbers to transit providers that serve the school.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org.

24. Parking Lot Signage/Designated Loading Area for Drop-offs and Pick-ups. The Project Sponsor shall install Parking Lot Signage/Designated Loading Areas for Drop-off and Pick-ups in the following manner:

- Stripe parking spaces and install signage in the new parking lot to clearly designate parking space types, including staff, student, parent loading, school vans, etc.
- Install signage in the parking lot directing cars to a designated drop-off/pick-up area and a route through the parking lot to exit the lot.
- Allocate a minimum 60' zone for parent loading (drop-offs and pick-ups) on the western frontage of the new gymnasium building. This will accommodate up to three loading vehicles.
- Install signage to restrict any parent/guardian from parking in a ADA-space during student drop-off and pick-up activities. An initial warning and/or notification shall be issued to any parent/guardian in direct violation and subsequent warnings and notifications shall be administered for repeat offenders. These ADA spaces are to be solely utilized by persons with disabilities and a placard that is easily visible by on-site school staff.
- School staff shall monitor and manage the loading zone during peak times to prevent queues.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org.

25. Modify Student Parking Policy. The Project Sponsor shall modify current parking policies for students. Currently, students that live outside of San Francisco are permitted to drive and park their vehicle in the school lot. Modification to this policy would restrict any students from driving and parking in the lot, unless there is a substantiated reason for driving/parking in the lot (e.g., health condition and/or injury that prevent them from walking a specified distance). Such efforts would reduce any foreseeable parking demand associated with student parkers. Furthermore, parking supply would increase to allow for available spaces for visitors and/or designate a few spaces in the lot strictly for parents waiting to pick up students.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org.

26. Pedestrian Path through Parking Lot. It is assumed that some students, staff or visitors may cut through the parking lot to access points west and south of campus, as well as bus stops on Sloat Boulevard. The Project Sponsor shall incorporate a pedestrian pathway through the parking lot and/or adjacent to the parking lot driveway into the designs to ensure a safe path for pedestrians in that area. The pedestrian path shall include signage clearly indicating the path of travel. Additionally, the path shall be used to direct students and staff to use the correct crosswalk to safely access the Muni Metro stop.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org.

27. New Mid-Block Crosswalk to Muni Metro Station. The school shall petition the San Francisco Municipal Transportation Agency (SFMTA) to remove the existing barriers and install a crosswalk on the north side of the Muni Metro station to cross West Portal Avenue from the

Project's proposed new pedestrian entrance, as feasible. Field observations noted a considerable amount of students hopping over the barriers and jaywalking across West Portal Avenue, as opposed to walking down the platform to the crosswalk location to walk across West Portal Avenue. The crosswalk may be signalized; in the absence of signal, crosswalk designations in the roadway and signs indicating that automobiles must yield to students would be an improvement to pedestrian safety and accessibility. The presence of a new crosswalk at this location would eliminate the propensity of students to cross West Portal Avenue from the light rail platform unsafely and dangerously. Additionally, the school shall station a staff member at the crosswalk during peak arrival and departure times to require students to use the crosswalk.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org.

28. **Prohibit On-Street Drop-Off/Pick Up Activities.** As an effort to maintain a high level of safety for persons walking to the campus and reduce any potential traffic impacts or constrained on-street parking conditions, the Project Sponsor shall enforce the prohibition of allowing any drop-off activities along adjacent streets (e.g., West Portal Avenue, Sloat Boulevard), and shall require all drop-off activities to occur within the campus parking lot. Such information shall be posted on the school's website and/or included in the existing Drop-Off/Pick-Up Management Plan. School staff shall also monitor the street and discourage any on-street loading.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org.

29. **Provide Adequate Sight Distance at Curb Cut.** To ensure safe vehicle maneuvering while exiting the relocated school driveway, no vegetation or removable or permanent structures shall be emplaced along the west side of West Portal Avenue to provide drivers with adequate line-of-sight of oncoming vehicles from southbound West Portal Avenue. Such actions would reduce and/or eliminate potential conflicts between exiting vehicles and oncoming traffic and pedestrians walking along the west side of West Portal Avenue.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org.

30. **Update School Website.** The Project Sponsor shall develop a "Transportation Choices" webpage on the school's website. Webpage to include but not limited to directions to the school from various origin locations (e.g., north of school, east of school, south of school, etc.); maps, schedules and detailed information of nearby Muni bus routes and light rail lines; parking procedures for drop-off/pick-up activities or visitor purposes; maps of bicycle routes and on-site bicycle parking locations; etc.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org.

31. **Establish a Transit Pass Subsidy Program.** In an effort to reduce auto trips and related short-/long-term parking demand, and to continue to encourage use of transit to travel to/from the school on a daily basis, the Project Sponsor shall offer a Transit Subsidy Program that issues monthly BART and Muni transit passes to employees. For example, the school shall provide pre-loaded Clipper Cards for employees.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org.

32. **Provide Transit and Active Transportation Incentive Programs for Employees.** In an effort to reduce auto trips associated with current and future employees, the Project Sponsor shall provide free and/or subsidized Muni and BART transit passes (in form of a Clipper Card, for example) to encourage transit use and shift employees' primary mode of transportation from auto to bicycle. Other incentives could be financial (in the form a monthly check or debit card) to reward employees who bike and/or walk to school on a daily basis.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org.

33. **Construction Management Plan.** The project sponsor and construction contractor(s) shall develop a detailed *Construction Management Plan*. The *Construction Management Plan* would, at a minimum, include the following provisions:

- Circulation routes shall be developed to minimize impacts on local street circulation, as appropriate. In the event of parking and/or travel lane closures, flaggers or signs or both shall be used to guide vehicles through or around the construction zone. Roadside construction safety protocols shall be implemented.
- Truck routes shall be identified. Haul routes that minimize truck traffic on local roadways and residential streets shall be used to the extent possible.
- Sufficient staging areas shall be developed for trucks accessing construction zones so as to minimize disruption of access to adjacent land uses, particularly at entries to the project site.
- Construction vehicle movement shall be controlled and monitored by on-site inspectors enforcing standard construction specifications.
- Truck trips shall be scheduled outside the peak morning and evening commute hours, to the extent possible.
- All equipment and materials shall be stored in designated contractor staging areas on or next to the worksite, such that vehicle, pedestrian, and bicycle traffic obstruction is minimized.
- Construction shall be coordinated with facility owners or administrators of police and fire stations (including all fire protection agencies) and transit stations or stops. Emergency service vehicles shall be given priority for access.
- The contractor shall be encouraged to reduce the number of construction workers' vehicle trips by facilitating the use of public transportation and minimizing construction worker parking availability.
- The contractor shall coordinate with other contractor(s) for projects in the vicinity and share information regarding schedule, duration of activities, vehicle routing and detouring (if applicable), staging of vehicles, etc.
- The contractor shall provide regularly-updated information (typically in the form of website, news articles, on-site posting, etc.) regarding project construction and schedule, as well as contact information for specific construction inquiries or concerns.
- During Phase 1 of the construction period, passenger loading (student drop-off/pick-up) shall take place that the passenger loading zone at 19th Avenue and Sloat Boulevard (see Appendix D). This shall be communicated to students and parents, and monitored as necessary by school staff.

- The school shall continue to discourage students and staff from driving to school during the construction period. Staff parking will be available at the nearby Scottish Rite (10 spaces will be leased); otherwise, staff will be reimbursed for the use of taxis or other rideshare vehicles.

It is noted that the construction management plan shall be reviewed by the SFMTA, TASC, and other City agencies as appropriate to adequately address issues of circulation (traffic, pedestrians, and bicycle), safety, parking and other project construction in the area.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org.

34. **Construction Notification from the School Administration.** Prior to construction and approval of Construction Management Plan, the school shall notify all faculty/staff, parents, students and visitors of construction activities and include detailed information pertaining to schedule, access to school, loading facilities, potential detouring and to include a notice to have faculty/staff, students, parents, visitors, etc. avoid parking at the campus and to utilize other modes of transportation. Such information shall be posted on the school's website and the school should provide contact information for anyone who has questions or concerns regarding construction information. On-site monitoring of students traveling to/from the school in the morning and afternoon periods shall also be conducted by faculty/staff to ensure student safety while walking in and around the school during construction activities.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org.

PROVISIONS

35. **First Source Hiring.** The Project shall adhere to the requirements of the First Source Hiring Construction and End-Use Employment Program approved by the First Source Hiring Administrator, pursuant to Section 83.4(m) of the Administrative Code. The Project Sponsor shall comply with the requirements of this Program regarding construction work and on-going employment required for the Project.

For information about compliance, contact the First Source Hiring Manager at 415-581-2335, www.onestopSF.org.

MONITORING - AFTER ENTITLEMENT

36. **Enforcement.** Violation of any of the Planning Department conditions of approval contained in this Motion or of any other provisions of Planning Code applicable to this Project shall be subject to the enforcement procedures and administrative penalties set forth under Planning Code Section 176 or Section 176.1. The Planning Department may also refer the violation complaints to other city departments and agencies for appropriate enforcement action under their jurisdiction.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org.

37. **Revocation due to Violation of Conditions.** Should implementation of this Project result in complaints from interested property owners, residents, or commercial lessees which are not resolved by the Project Sponsor and found to be in violation of the Planning Code and/or the

specific conditions of approval for the Project as set forth in Exhibit A of this Motion, the Zoning Administrator shall refer such complaints to the Commission, after which it may hold a public hearing on the matter to consider revocation of this authorization.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org.

OPERATION

38. **Garbage, Recycling, and Composting Receptacles.** Garbage, recycling, and compost containers shall be kept within the premises and hidden from public view, and placed outside only when being serviced by the disposal company. Trash shall be contained and disposed of pursuant to garbage and recycling receptacles guidelines set forth by the Department of Public Works.

For information about compliance, contact Bureau of Street Use and Mapping, Department of Public Works at 415-554-.5810, <http://sfdpw.org>.

39. **Sidewalk Maintenance.** The Project Sponsor shall maintain the main entrance to the building and all sidewalks abutting the subject property in a clean and sanitary condition in compliance with the Department of Public Works Streets and Sidewalk Maintenance Standards.

For information about compliance, contact Bureau of Street Use and Mapping, Department of Public Works, 415-695-2017, <http://sfdpw.org>.

40. **Community Liaison.** Prior to issuance of a building permit to construct the project and implement the approved use, the Project Sponsor shall appoint a community liaison officer to deal with the issues of concern to owners and occupants of nearby properties. The Project Sponsor shall provide the Zoning Administrator with written notice of the name, business address, and telephone number of the community liaison. Should the contact information change, the Zoning Administrator shall be made aware of such change. The community liaison shall report to the Zoning Administrator what issues, if any, are of concern to the community and what issues have not been resolved by the Project Sponsor.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org.

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ATTACHMENTS

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SAN FRANCISCO PLANNING DEPARTMENT

Certificate of Determination Exemption from Environmental Review

Case No.: 2014.0678ENV
 Project Address: 470 West Portal Avenue
 Zoning: RH-1(D) – (Residential - House, One Family-Detached)
 40-X Height and Bulk District
 Block/Lot: 2484/001, 2484/008, and 2484/009
 Lot Size: 72,094 square feet
 Project Sponsor: David Bushnell, 450 Architects, (415) 546-0450
 Staff Contact: Don Lewis – (415) 575-9168
don.lewis@sfgov.org

1650 Mission St.
 Suite 400
 San Francisco,
 CA 94103-2479

Reception:
415.558.6378

Fax:
415.558.6409

Planning
 Information:
415.558.6377

PROJECT DESCRIPTION:

The project site consists of three adjacent lots located on the west side of West Portal Avenue between 15th Avenue and Sloat Boulevard in the Lakeshore neighborhood. The project site is occupied by a 35-foot-tall, three-story building approximately 23,000 square feet in size with an adjacent surface parking lot with 61 spaces. The existing building was constructed in 1970, is currently occupied by a high school ("San Francisco Waldorf School"). The school has been in operation since 2006 and includes 12 classrooms, three dedicated art studios, one multi-purpose room, staff offices, and support spaces. The school is currently comprised of 150 students and 20 school employees (faculty and staff members). The school currently uses half of the existing parking lot for outdoor games and physical education activities. The proposed project involves the expansion of the existing school to provide additional on-site facilities and modification of the parking lot and access to the main school entrance.

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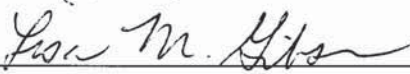
EXEMPT STATUS:

Categorical Exemption, Class 32 (State CEQA Guidelines Section 15332)

(Continued on next page)

DETERMINATION:

I do hereby certify that the above determination has been made pursuant to State and local requirements.


 Lisa M. Gibson
 Acting Environmental Review Officer

11/10/16
 Date

cc: David Bushnell, Project Sponsor
 Elizabeth Gordon Jonckheer, Current Planner
 Supervisor Yee, District 7 (via Clerk of the Board)

Distribution List
 Virna Byrd, M.D.F.

PROJECT DESCRIPTION (continued):

The proposed project involves the removal of up to 37 parking spaces for the construction of a new 35-foot-tall, one-story, 11,800-square-foot, multi-purpose gymnasium which would include a classroom and storage area. The proposed gymnasium would feature a “green wall” on its West Portal Avenue façade. The proposed project also includes an addition of approximately 11,700 square feet beneath the existing building footprint to include four new classrooms, a performance space (“Black Box Theater”), storage lockers, showers, and bathrooms. The proposed project, which would create a total of five new classrooms, would accommodate an increase of 90 students and 12 employees, for a total enrollment of up to 240 students with 32 employees. In addition, a 1,700-square-foot lobby would be constructed between the proposed gymnasium and the existing building.

The project would include the relocation of the existing 24-foot-wide curb cut from West Portal Avenue to approximately 100 feet south along West Portal Avenue. This would result in the permanent closure of the existing driveway (and curb cut). The project sponsor proposes two driveway variants: Driveway Design Variant 1 would be located 100 feet south of the existing curb cut, and would be 20 feet wide; and Driveway Design Variant 2, which would be located on an existing AT&T easement if AT&T grants access, would be 116 feet south of the existing curb cut, and would be 20 feet wide. The proposed number of surface parking spaces to be removed would vary depending on the location of the new driveway. Under Driveway Design Variant 1, the proposed project would remove 35 parking spaces and would retain 30 spaces. Under Driveway Design Variant 2, the proposed project would remove 37 parking spaces and would retain 28 spaces.

The project site contains zero Class-1 bicycle parking spaces and five Class-2 bicycle parking spaces. The proposed project would add 14 Class-2 bicycle parking spaces, for a total of 19 bicycle parking spaces at the pedestrian entry from West Portal Avenue. The number of proposed Class-1 bicycle parking spaces would depend on the driveway design. Driveway Design Variant 1 would provide 41 Class-1 bicycle parking spaces while Driveway Design Variant 2 would provide 23 Class-1 bicycle parking spaces.

The proposed project would provide landscaping on the project site and a new pathway is proposed on the project site near its Sloat Boulevard frontage. The proposed project would remove approximately eight trees, and would plant approximately 20 trees. Along the length of the proposed gymnasium, the project would install a two-foot-wide planting strip to separate the sidewalk from West Portal Avenue. The proposed addition to the existing building would require new mechanical equipment which could be located on top of the existing building. If the new mechanical equipment is installed on top of the existing building, the equipment would be located at least 15 feet from the property line and would include acoustic screening that would extend at least five feet above the top of the mechanical equipment.

The proposed project includes several measures to improve circulation, as provided in Attachment A. The project sponsor would appoint a Transportation Demand Management Plan coordinator to guide pick-up and drop-off procedures and to develop multimodal strategies for parents to encourage the use of alternative modes of transportation to the school. Other measures include installation of parking lot signage, petitioning the San Francisco Municipal Transportation Authority (SFMTA) to install a new mid-block crosswalk to the adjacent Muni Metro station, the enforcement of prohibiting on-street drop-off/pick-up activities, and the development of a Construction Management Plan.

Implementation of the project is planned in two phases. The first phase would include the construction of the gymnasium building, including the modifications to the parking lot and driveway, and would take approximately 11 months. The second phase would include the classroom/performance space addition, including the construction of the 1,700-square-foot lobby, and would take approximately 12 months. The proposed project would require up to 12 feet of excavation for the classroom/performance space addition, resulting in approximately 3,030 cubic yards of soil disturbance. The proposed gymnasium would be supported on a mat foundation and the addition would be supported by drilled piers.

Project Approvals

The proposed project would require the following approvals:

- **Planned Unit Development Authorization** (*Planning Commission*). The proposed project would require Conditional Use authorization for intensification and expansion of an existing school use in a Residential District. The project site qualifies for a Planned Unit Development (PUD) since the subject lot is greater than one half acre). As part of the PUD, the project sponsor is seeking modifications to the front yard, rear yard, and Class-1 bicycle parking requirements with a ten-year authorization timeline. The approval of the PUD authorization would be the Approval Action for the project. The Approval Action date establishes the start of the 30-day appeal period for this CEQA exemption determination pursuant to Section 31.04(h) of the San Francisco Administrative Code.
- **Building Permit** (*Department of Building Inspection*) (DBI). The proposed project would require approval from DBI for the proposed construction at the project site.

Project Setting. The project site consists of three adjacent lots located on the west side of West Portal Avenue between 15th Avenue and Sloat Boulevard in the Lakeshore neighborhood. The project site is located at the intersection of Sloat Boulevard, Junipero Serra Boulevard, Saint Francis Boulevard, Portola Drive and West Portal Avenue. In the vicinity of the school is 19th Avenue, which continues as California State Route 1. The surrounding neighborhood is comprised of residential cul-de-sacs to the west, and residential streets to the south and east. Immediately to northwest of the project site is an urban forest that is part of the 7-acre convalescent/nursing home property (“Arden Wood”) that fronts on Wawona Street. The nearest noise-sensitive receptors to the school are residences located along Ardenwood Way. The distance from the proposed gymnasium to the nearest residential property line is approximately 120 feet. West Portal Avenue serves as a medium-density retail corridor featuring a shared right-of-way with Muni light rail. Within 250 feet of the project site, the San Francisco Municipal Railway (Muni) operates the following bus lines: K, KT, M, 23, 57, and 91. There are bicycle facilities along nearby streets including bicycle lanes along Portola Drive, Sloat Boulevard, and St. Francis Boulevard. The surrounding uses near the project site include residential, commercial, and institutional uses. All of the surrounding parcels are zoned RH-1(D) and within a 40-X height and bulk district. There are no known projects in the vicinity that could combine with the proposed project to result in cumulative impacts.

EXEMPT STATUS (continued):

CEQA Guidelines Section 15332, or Class 32, provides an exemption from environmental review for in-fill development projects that meet the following conditions. As discussed below, the proposed project satisfies the terms of the Class 32 exemption.

- a) *The project is consistent with applicable general plan designations and policies as well as with applicable zoning designations.*

The San Francisco General Plan establishes objectives and policies to guide land use decisions related to the physical development of San Francisco and is composed of ten elements, each of which addresses a particular topic that applies citywide: air quality; arts; commerce and industry; community facilities; community safety; environmental protection; housing; recreation and open spaces; transportation; and urban design. The proposed project would be consistent with the San Francisco General Plan and with applicable zoning designations and policies. The project site is located within the RH-1(D) zoning district, where the existing school use is permitted with Conditional Use authorization, which was approved in 2006. The proposed expansion of the existing school would require a new Conditional Use authorization. In the RH-1(D) district, one off-street parking space is required for every two classrooms. The overall parking requirement for 19 classrooms would be ten parking spaces. The proposed project would retain 28 to 30 parking spaces. Therefore, the number of parking spaces exceeds the minimum parking requirement of the Planning Code. The project site is located within a 40-X height and bulk district, and the proposed 35-foot-tall gymnasium building would comply with the height and bulk district. Per Section 155.2 of the Planning Code, the project would be required to provide a minimum of 76 Class-1 bicycle parking spaces (four spaces per classroom) and a minimum of 19 Class-2 bicycle parking spaces (one space per classroom). The project proposes 19 Class-2 bicycle parking spaces and either 23 or 41 Class-1 bicycle parking spaces depending on which driveway design variant is selected. Because the school may not comply with the minimum Class-1 bicycle parking spaces, the school would seek an exception to these requirements. Development of lots that have an area of no less than one half acre qualify for authorization as a Planned Unit Development (PUD) pursuant to Section 304 of the Planning Code. The objective of the PUD process is to allow well-reasoned modifications to certain code provisions for sites of considerable size that are developed as integrated units and designed to produce a desirable development which will benefit the occupants, the neighborhood, and the City as a whole. The project site measures 1.7 acres (72,094 square feet) in size, and the project sponsor is seeking modifications to the front yard, rear yard, and Class-1 bicycle parking requirements through the PUD process. The proposed project would be consistent with applicable general plan zoning designations.

- b) *The development occurs within city limits on a site of less than five acres surrounded by urban uses.*

The 72,094-square-foot (1.7 acres) project site is located within a developed area of San Francisco. Surrounding uses near the project site include residential, commercial, and institutional uses. The proposed project, therefore, would be properly characterized as in-fill development of less than five acres, surrounded by urban uses.

c) *The project site has no habitat for endangered, rare or threatened species.*

The project site is within a developed urban area and occupied by an existing building with a surface parking lot. The project site is surrounded by residential, commercial, and institutional uses, and an urban forest is located immediately to the northwest of the project site. The project site is within a developed urban area of San Francisco with no significant riparian corridors, estuaries, marshes, wetlands, or any other potential wildlife habitat that might contain endangered, rare, or threatened species. Thus, the project site has no value as habitat for rare, threatened, or endangered species.

d) *Approval of the project would not result in any significant effects relating to traffic, noise, air quality, or water quality.*

Transportation

A school circulation memorandum¹ was prepared by a consultant to analyze transportation impacts associated with the proposed project. The following discussion summarizes the results from this analysis. On March 3, 2016, in anticipation of the future certification of revised CEQA Guidelines pursuant to Senate Bill 743, the San Francisco Planning Commission adopted State Office of Planning and Research's recommendation in the *Revised Proposal on Updates to the CEQA Guidelines on Evaluating Transportation Impacts in CEQA*² to use the Vehicle Miles Traveled (VMT) metric instead of automobile delay to evaluate the transportation impacts of projects (Resolution 19579). (Note: the VMT metric does not apply to the analysis of impacts on non-automobile modes of travel such as riding transit, walking, and bicycling.) Accordingly, this categorical exemption does not contain a separate discussion of automobile delay (i.e., traffic) impacts. Instead, a VMT and induced automobile travel impact analysis is provided.

VMT and Induced Vehicle Travel

Many factors affect travel behavior. These factors include density, diversity of land uses, design of the transportation network, access to regional destinations, distance to high-quality transit, development scale, demographics, and transportation demand management. Typically, low-density development at great distance from other land uses, located in areas with poor access to non-private vehicular modes of travel, generate more automobile travel compared to development located in urban areas, where a higher density, mix of land uses, and travel options other than private vehicles are available.

Given these travel behavior factors, San Francisco has a lower VMT ratio than the nine-county San Francisco Bay Area region. In addition, some areas of the City, expressed geographically through transportation analysis zones (TAZs), have lower VMT ratios than other areas of the City. The Planning Department has prepared a Geographic Information System database (the Transportation Information map) with current and projected 2040 per capita VMT figures for all TAZs in the City, in addition to regional daily average figures.³

A project would have a significant effect on the environment if it would cause substantial additional VMT. The State Office of Planning and Research's (OPR) [Revised Proposal on Updates to the CEQA](#)

¹ Nelson/Nygaard, 470 West Portal Avenue (San Francisco Waldorf High School Expansion) Transportation Memorandum, November, 2016. This document (and all other documents cited in this report, unless otherwise noted), is available for review at 1650 Mission Street, Suite 400, San Francisco, CA, as part of Case No. 2014.0678ENV.

² This document is available online at: https://www.opr.ca.gov/s_sb743.php.

³ San Francisco Planning Department *Transportation Information Map*, accessed November 1, 2016 at: <http://sftransportationmap.org>.

[Guidelines on Evaluating Transportation Impacts in CEQA](#)⁴ (“Proposed Transportation Impact Guidelines”) recommend screening criteria to identify types, characteristics, or locations of projects that would not result in significant impacts to VMT. If a project meets one of the three screening criteria provided (Map-Based Screening, Small Projects, or Proximity to Transit Stations), then it is presumed that VMT impacts would be less than significant for the project and a detailed VMT analysis is not required. Map-Based Screening is used to determine if a project site is located within a TAZ in the City that exhibits low levels of VMT; Small Projects are projects that would generate fewer than 100 vehicle trips per day; and the Proximity to Transit Stations criterion includes projects that are within a half mile of an existing major transit stop, have a floor area ratio (FAR) of greater than or equal to 0.75, vehicle parking that is less than or equal to that required or allowed by the Planning Code without conditional use authorization, and are consistent with the applicable Sustainable Communities Strategy.

According to the Transportation Information Map, the existing average daily VMT per capita for office (used to approximate school uses)⁵ is 12.1 for the transportation analysis zone the project site is located in, 430. This is 25 percent below the existing regional average daily VMT per employee of 16.2. Given the project site is located in an area where existing VMT is more than 15 percent below the existing regional average, the school would not result in substantial additional VMT. Future 2040 average daily VMT per employee is 10.9 for the transportation analysis zone, 430. This is 25 percent below the future 2040 regional average daily VMT per employee of 14.5.

The proposed project is not a transportation project. However, the proposed project would remove an existing 24-foot-wide driveway on West Portal Avenue and standard sidewalk and curb dimensions would be restored. A new 20-foot-wide driveway would be located south of the existing curb cut along West Portal Avenue. The project would also create bicycle parking and other pedestrian and bicycle safety and accessibility measures. These features fit within the general types of projects that would not substantially induce automobile travel, and the impacts would be less than significant.

Trip Generation

The proposed project would result in approximately 74 non-auto daily trips (including transit, bike, and walk trips) and 38 auto daily trips, which represents 24 trips associated with student drop-off/pick-up activities and 14 drive-alone/self-park trips (10 employees and four students).⁶ The project sponsor has agreed to implement several improvement measures that would further reduce less-than-significant impacts to circulation, and parking (see Attachment A).

Transit

The project site is located in an area well-served by transit. Within one-quarter mile of the project site, the San Francisco Municipal Railway (Muni) operates the following lines: 23-Monterey, 57-Park Merced, 91-Owl, K-Owl, KT-Ingleside/Third Street, and M-Ocean View. The Saint Francis Circle Station, which is served by the M-Ocean View and KT-Ingleside/Third Street light rail lines, is located directly across West Portal Avenue from the project site. The 23-Monterey stops at the intersection of Sloat Boulevard and

⁴ Governor’s Office of Planning and Research, *Revised Proposal on Updates to the CEQA Guidelines on Evaluating Transportation Impacts in CEQA*, January 20, 2016. Accessed November 1, 2016 at: https://www.opr.ca.gov/docs/Revised_VMT_CEQA_Guidelines_-_Proposal_January_20_2016.pdf.

⁵ Per the San Francisco Planning Department, Eligibility Checklist: CEQA Section 21099 – Modernization of Transportation, K-12 schools should be treated as office for screening and analysis. This is based on the SF-CHAMP model.

⁶ Travel demand resulting from the project was based on the school circulation memorandum.

West Portal Avenue, directly south of the project site. The 57-Park Merced stops at the intersection of West Portal Avenue and Sloat Boulevard, directly east of the project site.

The proposed project would result in approximately 53 new daily transit trips. These new transit trips by new students and employees would be distributed across several Muni lines and would not result in exceedance in ridership beyond current capacity standards. Therefore, the proposed project would not result in any new significant transit impacts or contribute considerably to any cumulative transit impacts.

It is noted that students illegally access and depart the school by crossing West Portal Avenue at mid-block and walk to the Muni light rail platform. As the main entrance to the school would remain in the same general area, it is expected that students would continue to access Muni light rail by crossing West Portal Avenue. The project sponsor has agreed to petition SFMTA to install a new mid-block crossing to the Muni Metro landing. (See Attachment A for further details.)

Loading

According to the school circulation memo, current observation of internal vehicle circulation within the parking lot indicated that there were no substantial queues during drop-off/pick-up activities nor were there any indications of vehicle spillover onto West Portal Avenue. The project would include the relocation of the parking lot driveway on West Portal Avenue to 100 to 116 feet farther south along West Portal Avenue. As such, private vehicles parking at the school or performing drop-off/pick-up activities would continue to access the parking lot from West Portal Avenue. The project would not result in a substantial amount of additional vehicle movements in and out of the school driveway (about 38 new vehicle trips). The proposed driveway and new parking lot configuration would be able to accommodate new vehicle trips. Vehicle circulation patterns entering and exiting the parking lot would not result in any considerable queues or reduced traffic flows along southbound West Portal Avenue. Furthermore, as detailed in the school circulation memo and Attachment A, the project sponsor would install signage designating the drop-off/pick-up area and would prohibit on-street drop-offs/pick-ups.

Pedestrians

The proposed project would result in an increase in about 18 new walk trips to and from the school on a daily basis. The increase in daily pedestrian person-trips generated by the project would not substantially overcrowd sidewalks in the project vicinity or otherwise interfere with pedestrian accessibility to the site and adjoining areas. Furthermore, as detailed in the school circulation memo and Attachment A, the project sponsor would implement improvement measures to enhance pedestrian safety during both construction and operation, provide more efficient access to nearby transit facilities, improve internal circulation during drop-off/pick-up activities, and designate a primary transportation coordinator to ensure that circulation conditions are monitored accordingly.

Bicycle

There are no bicycle routes on West Portal Avenue in front of the project site; however, there are bicycle facilities along nearby streets, including Portola Drive, Sloat Boulevard, and Saint Francis Boulevard. The project site currently contains five bicycle parking spaces. The project would provide 41 Class-1 and 19 Class-2 bicycle parking spaces under Driveway Design Variant 1 and 23 Class-1 bicycle parking spaces and 19 Class-2 spaces under Driveway Design Variant 2. The proposed project would result in approximately two to three new bicycle trips. The increase of bicycle trips generated by the proposed project would be accommodated by the existing bicycle network, and the amount of proposed bicycle

spaces would meet the demand. The proposed project would not create potentially hazardous conditions for bicyclists and no significant impacts related to bicycling or bicycle facilities would occur.

Parking

The project site currently contains 61 parking spaces and field observations observed that 22 spaces (36 percent) were occupied in the morning and 31 spaces (51 percent) were occupied in the afternoon. Due to the surplus of on-site parking, the school currently reserves half of the parking lot for outdoor games and physical education activities. The proposed project involves the removal of up to 37 parking spaces for construction of the gymnasium. The proposed project could create a deficiency in parking of approximately 13 spaces during peak demand, and a potential spillover in parking onto residential streets may occur. The school circulation memo reported that there are a total of 158 available on-street parking spaces during the weekday morning period and a total of 116 available spaces during the weekday afternoon period. Of the total on-street availability, there are about 64 Residential Parking Permit (RPP) designated spaces available in the morning and 37 RPP designated spaces available in the afternoon. In the event that there is temporal spillover in parking due to lack of available parking on the project site, the school may coordinate with SFMTA to obtain RPP stickers for long-term parkers (e.g., employees and students), as there is ample supply of public on-street parking (and RPP-designated parking) in the project vicinity.

As detailed in the school circulation memo and Attachment A, the project sponsor has agreed to address parking management strategies for the project and to encourage monitoring of parking during student drop-off/pick-up activities and to reduce any potential spillover or related parking constraints within the school lot and nearby streets. The sponsor has agreed to appoint a Transportation Demand Management coordinator to encourage and communicate information about the use of alternative modes and to administer subsidized transit passes and vanpool programs. These measures would help the project meet its parking demand; however, even without such features the proposed project is not expected to result in a substantial parking deficit that could create hazardous conditions or significant delays affecting traffic, transit, bicycles or pedestrians.

San Francisco does not consider parking supply as part of the permanent physical environment. Parking conditions are not static, as parking supply and demand varies from day to day, from day to night, from month to month, etc. Hence, the availability of parking spaces (or lack thereof) is not a permanent physical condition, but changes over time as people change their modes and patterns of travel.

Parking deficits are considered to be social effects, rather than impacts on the physical environment as defined by CEQA. Under CEQA, a project's social impacts need not be treated as significant impacts on the environment. Environmental documents should, however, address the secondary physical impacts that could be triggered by a social impact (CEQA Guidelines Section 15131(a)). The social inconvenience of parking deficits, such as having to hunt for scarce parking spaces, is not an environmental impact, but there may be secondary physical environmental impacts, such as increased traffic congestion at intersections, air quality impacts, safety impacts, or noise impacts caused by congestion. In the experience of San Francisco transportation planners, however, the absence of a ready supply of parking spaces, combined with available alternatives to auto travel (e.g., transit service, taxis, bicycles or travel by foot) and a relatively dense pattern of urban development, induces many drivers to seek and find alternative parking facilities, shift to other modes of travel, or change their overall travel habits. Any such resulting shifts to transit service in particular, would be in keeping with the City's "Transit First" policy. The City's Transit First Policy, established in the City's Charter Section 16.102 provides that "parking policies for

areas well served by public transit shall be designed to encourage travel by public transportation and alternative transportation.” Therefore, no significant parking impacts are expected.

Construction

All construction staging is planned to be contained within the school site, including access for vehicles, materials, and deliveries to avoid impacts to on-street parking, street closures, transit operations, and sidewalk closures. Parking for employees, students, and visitors would be prohibited on the project site during construction. The school would coordinate with SFMTA to seek temporary RPP parking permits for employees and students during the construction period. In addition, the school has an agreement with the San Francisco Scottish Rite Masonic Center⁷ to lease 10 parking spaces for use by school staff and to use its 45-foot-long passenger loading white curb on 19th Avenue at Sloat Boulevard for student drop-off/pick-up. The white zone can accommodate up to three vehicles at a time. As stated above, there are a total of 158 available on-street parking spaces during the weekday morning period and a total of 116 available spaces during the weekday afternoon period. Of the total on-street availability, there are about 64 RPP-designated spaces available in the morning and 37 RPP-designated spaces available in the afternoon. As such, in the event that the school is able to obtain the necessary temporary RPP stickers for existing/future employees and students, there is adequate on-street parking capacity to accommodate this temporary increase in parked vehicles during the construction period.

In light of the above, the proposed project would not result in a significant impact on the transportation and circulation system or result in a cumulative transportation impact.

Noise

Noise is regulated by the San Francisco Noise Ordinance (Noise Ordinance), which is codified in Article 29 of the San Francisco Police Code. Article 29 establishes property line and other limits for fixed noise sources and also regulates construction noise. Under Section 2909(b), fixed noise sources from commercial properties (such as the existing school use) are limited to 8 dBA⁸ above ambient levels and Section 2909 (d) also establishes that such noise not exceed an interior daytime (7 a.m. to 10 p.m.) noise limit of 55 dBA or nighttime noise limit (10 p.m. to 7 a.m.) of 45 dBA at the nearest residential receptor. While the limits in the Noise Ordinance only apply to fixed noise sources (e.g. mechanical equipment), and not to noise from the variety of school-related noise activity, the Planning Department uses the criteria in the Noise Ordinance for determining the significance of noise impacts. Specifically, the Department undertakes a two-step analysis that considers first whether noise from a proposed project would exceed the property line noise limits of 8 dBA above ambient per Section 2909(b). If the project does not result in noise in excess of the property line noise limits established in the Noise Ordinance, generally, no further analysis is required. If a project could exceed the property line noise limits, a second analysis is conducted to determine if the noise would meet the daytime or nighttime interior noise limits in Section 2909(d). The requirements of the Noise Ordinance are designed to prevent sleep disturbance, protect public health, and prevent the acoustical environment from progressive deterioration. Therefore, if noise generated by project operations meets either the property line noise limits or limits established in

⁷ The Scottish Rite facility includes an auditorium, a banquet hall, and a parking garage with approximately 80 spaces.

⁸ The standard method used to quantify environmental noise involves evaluating the sound with an adjustment to reflect the fact that human hearing is less sensitive to low-frequency sound than to mid-and high-frequent sound. This measurement adjustment is called “a” weighting, and the data are reported in A-weighted decibel (dBA). A -10dB (decibel) increase in noise level is generally perceived to be twice as loud.

Section 2909(d), the project would not result in a significant noise impact. Section 2907 of Article 29 of the Police Code regulates construction noise and is the basis for determining the significance of construction-related noise impacts.

Operational Noise

An acoustical study⁹ was conducted for the proposed project and long-and short-term noise measurements were taken. Ambient noise levels at the project site at the residential property line were determined to be 54 dBA. The study analyzed the loudest events that would occur in the proposed gymnasium and theater, as well as from mechanical noise.

As indicated in the acoustical study, noise from the proposed gymnasium and theater, which is estimated to be 50 dBA at the neighboring residential property line, would not exceed 8 dBA above ambient noise levels (62 dBA) and would meet the criteria of Section 2909(b). Furthermore, the acoustical study indicates that none of the foreseeable events at the proposed gymnasium or theater would exceed the 55 dBA daytime limit (between the hours of 7 a.m. to 10 p.m.) of Section 2909(d). No nighttime (10 p.m. to 7 a.m.) activities are proposed or anticipated; therefore, the proposed project would also meet the nighttime noise limits of Section 2909(d).

Mechanical noise from fans and heat pumps was also analyzed as part of the acoustical study. The results of the mechanical noise analysis (given the assumed equipment and operating conditions) indicate that sound levels at the residential property line would be below the 62 dBA criteria of Section 2909(b) and the 55 dBA interior residential noise limit established for fixed mechanical equipment in Section 2909(d) of Article 29 of the Police Code.

Construction Noise

Although increase in noise during the 23-month construction phase of the project would occur, construction noise would be limited to certain hours of day and would be temporary and intermittent in nature. Construction noise is also regulated by the San Francisco Noise Ordinance and Section 2907 of the Police Code requires that noise levels from individual pieces of construction equipment, other than impact tools, not exceed 80 dBA at a distance of 100 feet from the source. Impact tools (such as jackhammers and impact wrenches) must have both intake and exhaust muffled to the satisfaction of the Director of Public Works. Section 2908 of the Police Code prohibits construction work between 8:00 p.m. and 7:00 a.m. if noise would exceed the ambient noise level by 5 dBA at the project property line, unless a special permit is authorized by the Director of Public Works. The proposed project would be required to comply with these requirements during construction. Therefore, the proposed project would result in less than significant construction noise impacts.

In light of the above, the proposed project would not result in individually or cumulatively significant noise impacts.

Air Quality

In accordance with the state and federal Clean Air Acts, air pollutant standards are identified for the following six criteria air pollutants: ozone, carbon monoxide (CO), particulate matter (PM), nitrogen dioxide (NO₂), sulfur dioxide (SO₂) and lead. These air pollutants are termed criteria air pollutants because they are regulated by developing specific public health- and welfare-based criteria as the basis for setting permissible levels. The Bay Area Air Quality Management District (BAAQMD) in its CEQA

⁹ Wilson Ihrig, *San Francisco Waldorf High School Expansion, Noise Study*, March 21, 2016.

Air Quality Guidelines (May 2011), has developed screening criteria to determine if projects would violate an air quality standard, contribute substantially to an air quality violation, or result in a cumulatively considerable net increase in criteria air pollutants within the San Francisco Bay Area Air Basin. If a proposed project meets the screening criteria, then the project would result in less-than-significant criteria air pollutant impacts. A project that exceeds the screening criteria may require a detailed air quality assessment to determine whether criteria air pollutant emissions would exceed significance thresholds. The proposed project, which would construct an 11,800-square-foot gymnasium and an addition of 11,700 square feet to the existing 23,000-square-foot high school building, would not exceed criteria air pollutant screening levels for operation or construction.¹⁰

In addition to criteria air pollutants, individual projects may emit toxic air contaminants (TACs). TACs collectively refer to a diverse group of air pollutants that are capable of causing chronic (i.e., of long-duration) and acute (i.e., severe but short-term) adverse effects to human health, including carcinogenic effects. In response to growing concerns of TACs and their human health effects, the San Francisco Board of Supervisors approved a series of amendments to the San Francisco Building and Health Codes, generally referred to as the Enhanced Ventilation Requirements for Urban Infill Sensitive Use Developments or Health Code, Article 38 (Ordinance 224-14, effective December 8, 2014)(Article 38). The purpose of Article 38 is to protect the public health and welfare by establishing an Air Pollutant Exposure Zone and imposing an enhanced ventilation requirement for all urban infill sensitive use development within the Air Pollutant Exposure Zone. Projects within the Air Pollutant Exposure Zone require special consideration to determine whether the project's activities would expose sensitive receptors to substantial air pollutant concentrations or add emissions to areas already adversely affected by poor air quality.

The proposed project is not within an Air Pollutant Exposure Zone. Therefore, the proposed project would not result in a significant impact with respect to siting new sensitive receptors in areas with substantial levels of air pollution. The proposed project would require construction activities for the approximate 23-month construction phase. However, construction emissions would be temporary and variable in nature and would not be expected to expose sensitive receptors to substantial air pollutants. Furthermore, the proposed project would be subject to, and comply with, California regulations limiting idling to no more than five minutes,¹¹ which would further reduce nearby sensitive receptors' exposure to temporary and variable TAC emissions. Therefore, construction period TAC emissions would not result in a significant impact with respect to exposing sensitive receptors to substantial levels of air pollution. In conclusion, the proposed project would not result in significant air quality impacts.

Water Quality

The proposed project would not generate wastewater or result in wastewater discharges that would have the potential to degrade water quality or contaminate a public water supply. Project-related wastewater and stormwater would flow to San Francisco's combined sewer system and would be treated to standards contained in San Francisco's National Pollutant Discharge Elimination System Permit for the Southeast Water Pollution Control Plant prior to discharge. In 2013, the San Francisco Public Utilities Commission (SFPUC) adopted the Construction Site Runoff Ordinance (Public Works Code, Ordinance

¹⁰ Bay Area Air Quality Management District, CEQA Air Quality Guidelines, Updated May 2011. Table 3-1. Criteria air pollutant screening sizes for a high school is 311,000 square feet for operational and 277,000 square feet for construction.

¹¹ California Code of Regulations, Title 13, Division 3, § 2485 (on-road) and § 2449(d)(2) (off-road).

260-13) which requires all construction sites, regardless of size to implement Best Management Practices (BMPs) to prevent construction site runoff discharges into the combined or separate sewer systems. Further, construction sites that disturb 5,000 square feet or more of ground surface, such as the proposed project, are required to apply for a Construction Site Runoff Control Permit from the SFPUC and submit an Erosion and Sediment Control Plan which includes BMPs to prevent stormwater runoff and soil erosion during construction. Therefore, the proposed project would not result in significant water quality impacts.

e) The site can be adequately served by all required utilities and public services.

The project site is located in a dense urban area where all public services and facilities are available. The proposed project would be connected to existing water, electric, gas, and wastewater services. Prior to receiving a building permit, the project would be reviewed by the Department of Building Inspection (DBI), the San Francisco Fire Department (SFFD), the San Francisco Public Utilities Commission (SFPUC), and Public Works (DPW) to ensure compliance with City and State regulations concerning building standards, fire protection, sewer connections, and hydrology. Therefore, the proposed project would be adequately served by all required utilities and public services.

DISCUSSION OF ENVIRONMENTAL ISSUES:

CEQA Guidelines Section 15300.2 establishes exceptions to the application of a categorical exemption for a project. None of the established exceptions applies to the proposed project.

Guidelines Section 15300.2, subdivision (b), provides that a categorical exemption shall not be used where the cumulative impact of successive projects of the same type in the same place, over time, is significant. As discussed previously under Project Setting, there are no cumulative projects in the vicinity that could combine with the proposed project to result in cumulative effects on the environment. Therefore, there is no possibility of a significant cumulative effect on the environment due to the proposed project.

Guidelines Section 15300.2, subdivision (c), provides that a categorical exemption shall not be used for an activity where there is a reasonable possibility that the activity will have a significant effect on the environment due to unusual circumstances. As discussed above, the proposed project would not have a significant effect on traffic, noise, air quality and water quality. In addition, the proposed project would not have a significant effect on the environment due to unusual circumstances for other environmental topics, including those discussed below.

CEQA Guidelines Section 15300.2, subdivision (e), provides that a categorical exemption shall not be used for a project located on a site which is included on any list compiled pursuant to Section 65962.5 of the Government Code. The project site is not located on such a list.

OTHER ENVIRONMENTAL TOPICS:Aesthetics

Design and aesthetics are by definition subjective, and open to interpretation by decision-makers and members of the public. A proposed project would be considered to have a significant adverse effect on visual quality only if it would cause a substantial and demonstrable negative change. The proposed project would not cause such change. As described above, the proposed envelope of the gymnasium meets Planning Code requirements for the RH-1(D) zoning district and the 40-X height and bulk district. The project would intensify and change the use of the site and would be visible from residential buildings within the project site vicinity. The project would not degrade or obstruct scenic views from public areas viewable by a substantial number of people or substantially degrade the visual quality in the project vicinity. While private views may be lost or obstructed, this is common and expected in an urban setting. Therefore, the project would not result in a significant impact on aesthetics.

Biological Resources

The project site is surrounded by residential, commercial, and institutional uses, and an urban forest is located immediately to the northwest of the project site. There are no riparian corridors, estuaries, marshes, or wetlands in the project vicinity that could be affected by the proposed project. The proposed construction of a 35-foot-tall gymnasium building would require the removal of eight trees. The project would include new landscaping and vegetation including 21 new trees. The proposed project's height would not substantially interfere with the movement of any resident or migratory wildlife species and the proposed project would be required to comply with Planning Code Section 139 standards for bird-safe buildings.

Migratory and residential birds often nest in ornamental and/or street trees in urban environments. Although birds that nest in urban environments are generally habituated to higher levels of noise and human activity than birds in less developed areas, project-related construction activities and noise could disrupt nesting activities. Most species of nesting birds and their nests and eggs are protected by state Fish and Game Code Sections 3505 and 3503.5 and the federal Migratory Bird Treaty Act (MBTA) which makes it unlawful to harm migratory birds and their nests. To ensure compliance with the Fish and Game Code and the MBTA, the project would implement the following measures, or their equivalent, to ensure compliance with state and federal regulations protecting migratory birds:

- Vegetation removal activities for the proposed project could be conducted during the nonbreeding season (i.e., September through February) to avoid impacts to nesting birds. If other timing restrictions make it impossible to avoid the nesting season, preconstruction surveys shall be conducted for work scheduled during the breeding season (March through August).
- Preconstruction surveys are typically conducted by a qualified ornithologist, authorized by the California Department of Fish and Wildlife (CDFW), to determine if any birds are nesting in or in the vicinity of the vegetation to be removed. The preconstruction survey is typically conducted within 15 days prior to the start of the work from March through May (since there is higher potential for birds to initiate nesting during this period) and within 30 days prior to the start of work from June through August.

- If an active nest is found close enough to the construction area to be disturbed by these activities, the qualified biologist, in consultation with CDFW, typically determine the extent of a construction-free buffer zone to be established around the nest until the young have fledged.

As described above, the proposed project would not conflict with any local policies or ordinances protecting biological resources; affect any rare, threatened, or endangered species; or diminish habitat for rare, threatened or endangered species. For these reasons, the proposed project is not anticipated to have a significant effect on biological resources.

Hazards

Article 22A of the Health Code, also known as the Maher Ordinance, is administered and overseen by the Department of Public Health (DPH). The Maher area includes properties throughout the City where there is potential to encounter hazardous materials, primarily industrial zoning districts, sites with industrial uses or underground storage tanks, sites with historic bay fill, and sites in close proximity to freeways or underground storage tanks. The over-arching goal of the Maher Ordinance is to protect public health and safety by requiring appropriate handling, treatment, disposal and when necessary, mitigation of contaminated soils that are encountered in the building construction process. Projects that disturb 50 cubic yards or more of soil that are located on sites with potentially hazardous soil or groundwater are subject to this ordinance.

The proposed project is located within the Article 22A (Maher) area and would involve excavation up to approximately 12 feet below ground surface and approximately 3,030 cubic yards of soil disturbance. Therefore, the project is subject to the Maher Ordinance. In compliance with the Maher Ordinance, the project sponsor submitted a Maher Application and a Phase I Environmental Site Assessment (ESA)¹² to DPH.¹³ According to the Phase I ESA, the historical use of the project site was either vacant land or a single-family dwelling until the construction of the current building on the project site in 1970. The project site was depicted as undeveloped vacant land in the 1913-1915, 1948-1950, and 1966 Sanborn Maps. Based on the 1935 aerial photograph, a residential structure and natural vegetation covered the project site. The residential structure was no longer present and the project site was overgrown with natural vegetation as depicted in aerial photographs from 1948, 1958 and 1969. Pacific Bell/SBC Communications occupied the existing building on the project site for general office functions and a call center from 1972 to 2006. The Waldorf High School has occupied the project site since 2006.

In 1982, SBC Communications installed an underground storage tank (UST) to store diesel fuel to power its emergency generator. The UST was replaced in 1998 with a 4,000-gallon UST. No spills, leaks or any violations were reported for the installation or operation of the UST. In October 2005, under the oversight of DPH, the UST was removed from the project site. No petroleum hydrocarbon odors or discoloration were noted in the soils and groundwater was not encountered. Following the removal of the UST, excavation and stockpile samples were collected and sampled. Low levels of diesel were detected in the samples collected from the soil stockpiles. The excavated area was subsequently backfilled with clean imported fill material and the stockpiled soil was transported off-site for disposal. The Phase I ESA reported that the current, historic and surrounding use of the project site has a very low probability of impacting its soil or groundwater resources and there was no evidence of dumping or landfill activities,

¹² All West, *Phase I Environmental Site Assessment Update*, 470 West Portal Avenue, San Francisco, CA, March 31, 2006.

¹³ Csarina Tabora, SFDPH, email to Don Lewis, 470 West Portal Avenue, October 20, 2016

large scale hazardous material storage or use on the project site in any of the photographs or historical documents reviewed.

Since the project site is located in the Maher area and the proposed project would require more than 50 cubic yards of soil disturbance, the proposed project would be required to remediate potential soil contamination described above in accordance with Article 22A of the Health Code. Therefore, the proposed project would not result in a significant hazard to the public or the environment through the release of hazardous materials.

Public Notice and Comment. On December 1, 2015, the Planning Department mailed a "Notification of Project Receiving Environmental Review" to community organizations, tenants of properties adjacent to the project site, and those persons who own property within 300 feet of the project site. One member of the public was concerned that the removal of off-street parking spaces for the new gymnasium, and any special events at the new gymnasium, would result in the reduction of street parking in the project vicinity. The school anticipates about five evening events per year (Founder's Night, two open houses, and two theater/music performances). Sports events are also likely to be hosted in the new gymnasium, including about eight home volleyball games per season and 12 twelve basketball games per season. These events are likely to generate approximately 20 to 80 attendees each. The school would continue to encourage the use of public transportation to and from special events. Given that all of these events occur in the evening, conflicts with staff parking are not anticipated and thus the parking lot would be available for use by attendees. For any events where additional parking is needed, the school will coordinate with the Scottish Rite to offer overflow parking 0.3 miles away from the school. As discussed in the "Parking" section above, the proposed project is not expected to result in a substantial parking deficit that could create hazardous conditions or significant delays affecting traffic, transit, bicycles or pedestrians.

Conclusion. The proposed project satisfies the criteria for exemption under the above-cited classification(s). In addition, none of the CEQA Guidelines Section 15300.2 exceptions to the use of a categorical exemption applies to the proposed project. For the above reasons, the proposed project is appropriately exempt from environmental review.

ATTACHMENT A



SAN FRANCISCO WALDORF HIGH SCHOOL TRANSPORTATION MANAGEMENT PLAN 470 WEST PORTAL AVENUE, SAN FRANCISCO

San Francisco Waldorf School is currently planning an expansion to the existing academic facility at 470 West Portal Avenue in San Francisco. The expansion includes the construction of a multi-purpose gymnasium and an addition of approximately 11,700 square feet beneath the existing building footprint to include new classrooms, a performance space, storage lockers, showers, and bathrooms. The expansion would create a total of six new classrooms, for a total enrollment of up to 240 students and 32 employees.

To help manage vehicle circulation immediately surrounding the school site, especially during the student drop-off and pick-up periods, San Francisco Waldorf School will implement a comprehensive suite of circulation and transportation demand management strategies. This Transportation Management Plan (TMP) provides transportation-related measures that the San Francisco Waldorf School will commit to implementing at its campus at 470 West Portal Avenue.

Recommended Improvement Measures

A number of improvement measures that would aid in further reducing less-than-significant impacts to traffic/circulation and parking are described below.

Improvement Measure #1: Appoint a Transportation Demand Management (TDM) Coordinator

- Responsible for preparing and modifying current Drop-Off/Pick-Up Plan and distributing the Plan to faculty, staff, parents, guardians, etc. on an annual basis;
- Establishing goals and policies for student/staff transportation mode split (auto/transit/bike/walk) and regularly monitor the progress toward said goals;
- Periodically survey students, staff and parents to determine travel patterns, reasons for travel choices, barriers and potential opportunities for change;
- Discuss transportation options with new students and/or parents of students to ensure all options are understood;

- Encourage carpooling among students who must drive or be driven to school;
- Encourage a buddy system as needed to teach new students how to use transit, bike or walk to school;
- Provide incentives as needed to reach above goals for transportation mode choice, including subsidized transit passes;
- Expand bicycle offerings to include repair station, secure parking, and maintenance services;
- Ensure adequate coverage of staff to monitor student arrival/departures including auto drop-off/pick-up situation and students' safe use of transit; and discourage illegal crossings or illegal parking in the school lot.
- Encourage and facilitate the use of alternative transportation modes by all attendees of evening special events (e.g. open houses, sports games) at the school; make alternative parking arrangements with nearby facilities (e.g. Scottish Rite) in the rare case of overflow parking demand.
- Provide parents with *Multimodal Access Guide* to describe how to reach the school by walking, bicycling, and transit. The guide should be provided to all new, incoming student's parents and redistributed every year to all students and faculty/staff. The guide may include:
 - A detailed map of nearby transit facilities (stops and routes) in vicinity of the school;
 - A detailed map of bicycle routes in the vicinity of the school; and
 - Provide online links and phone numbers to transit providers that serve the school.
- Please refer to Appendix G - TDM Checklist.

Improvement Measure #2: Install Parking Lot Signage/Designated Loading Area for Drop-offs and Pick-ups

- Stripe parking spaces and install signage in the new parking lot to clearly designate parking space types, including staff, student, parent loading, school vans, etc.
- Install signage in the parking lot directing cars to a designated drop-off/pick-up area and a route through the parking lot to exit the lot
- Allocate a minimum 60' zone for parent loading (drop-offs and pick-ups) on the western frontage of the new gymnasium building. This will accommodate up to three loading vehicles.
- Install signage to restrict any parent/guardian from parking in a ADA-space during student drop-off and pick-up activities. An initial warning and/or notification shall be issued to any parent/guardian in direct violation and subsequent warnings and notifications shall be administered for repeat offenders. These ADA spaces are to be solely utilized by persons with disabilities and a placard that is easily visible by on-site school staff.
- School staff will continue to monitor and manage the loading zone during peak times to prevent queues.

Improvement Measure #3: Modify Student Parking Policy

The school may consider modifying current parking policies for students. Currently, students that live outside of San Francisco are permitted to drive and park their vehicle in the school lot. Modification to this policy could restrict any students from driving and parking in the lot, unless there is a substantiated reason for driving/parking in the lot (e.g., health condition and/or injury that prevent them from walking a specified distance). Such efforts would reduce any foreseeable parking demand associated with student parkers. Furthermore, parking supply would increase to allow for available spaces for visitors and/or designate a few spaces in the lot strictly for parents waiting to pick up students.

Improvement Measure #4: Pedestrian Path through Parking Lot

It is assumed that some students, staff or visitors may cut through the parking lot to access points west and south of campus, as well as bus stops on Sloat Boulevard. This improvement measure would incorporate a pedestrian

pathway through the parking lot and/or adjacent to the parking lot driveway into the designs to ensure a safe path for pedestrians in that area. The pedestrian path would include signage clearly indicating the path of travel. Additionally, the path should be used to direct students and staff to use the correct crosswalk to safely access the Muni Metro stop.

Improvement Measure #5: New Mid-Block Crosswalk to Muni Metro station

The school should consider petitioning the San Francisco Municipal Transportation Agency (SFMTA) to remove the existing barriers and install a crosswalk on the north side of the Muni Metro station to cross West Portal Avenue from the project's proposed new pedestrian entrance. Field observations noted a considerable amount of students hopping over the barriers and jaywalking across West Portal Avenue, as opposed to walking down the platform to the crosswalk location to walk across West Portal Avenue. The crosswalk may be signalized; in the absence of signal, crosswalk designations in the roadway and signs indicating that automobiles must yield to students would be an improvement to pedestrian safety and accessibility. The presence of a new crosswalk at this location would eliminate the propensity of students to cross West Portal Avenue from the light rail platform unsafely and dangerously. Additionally, the school should station a staff member at the crosswalk during peak arrival and departure times to require students to use the crosswalk.

Improvement Measure #6: Prohibit On-Street Drop-Off/Pick-Up Activities

As an effort to maintain a high level of safety for persons walking to the campus and reduce any potential traffic impacts or constrained on-street parking conditions, the school should enforce the prohibition of allowing any drop-off activities along adjacent streets (e.g., West Portal Avenue, Sloat Boulevard), and should require all drop-off activities to occur within the campus parking lot. Such information could be posted on the school's website and/or included in the existing Drop-Off/Pick-Up Management Plan. School staff should also monitor the street and discourage any on-street loading.

Improvement Measure #7: Provide Adequate Sight Distance at Curb Cut

To ensure safe vehicle maneuvering while exiting the relocated school driveway, it is recommended that no vegetation or removable or permanent structures be emplaced along the west side of West Portal Avenue to provide drivers with adequate line-of-sight of oncoming vehicles from southbound West Portal Avenue. Such actions would reduce and/or eliminate potential conflicts between exiting vehicles and oncoming traffic and pedestrians walking along the west side of West Portal Avenue.

Improvement Measure #8: Update School Website

Develop a "Transportation Choices" webpage on the school's website. Webpage to include but not limited to directions to the school from various origin locations (e.g., north of school, east of school, south of school, etc.); maps, schedules and detailed information of nearby Muni bus routes and light rail lines; parking procedures for drop-off/pick-up activities or visitor purposes; maps of bicycle routes and on-site bicycle parking locations; etc.

Improvement Measure #9: Establish Transit Pass Subsidy Program

In an effort to reduce auto trips and related short-/long-term parking demand, and to continue to encourage use of transit to travel to/from the school on a daily basis, the school could offer a Transit Subsidy Program that issues monthly BART and Muni transit passes to employees. For example, the school could provide pre-loaded Clipper Cards for employees.

Improvement Measure #10: Provide Transit and Active Transportation Incentive Program for Employees

In an effort to reduce auto trips associated with current and future employees, it is recommended that the school provide free and/or subsidized Muni and BART transit passes (in form of a Clipper Card, for example) to encourage

transit use and shift employees' primary mode of transportation from auto to bicycle. Other incentives could be financial (in the form a monthly check or debit card) to reward employees who bike and/or walk to school on a daily basis.

Improvement Measure #11: Construction Management Plan

The project sponsor and construction contractor(s) shall develop a detailed *Construction Management Plan*. The *Construction Management Plan* would, at a minimum, include the following provisions:

- Circulation routes shall be developed to minimize impacts on local street circulation, as appropriate. In the event of parking and/or travel lane closures, flaggers or signs or both shall be used to guide vehicles through or around the construction zone. Roadside construction safety protocols shall be implemented.
- Truck routes shall be identified. Haul routes that minimize truck traffic on local roadways and residential streets shall be used to the extent possible.
- Sufficient staging areas shall be developed for trucks accessing construction zones so as to minimize disruption of access to adjacent land uses, particularly at entries to the project site.
- Construction vehicle movement shall be controlled and monitored by on-site inspectors enforcing standard construction specifications.
- Truck trips shall be scheduled outside the peak morning and evening commute hours, to the extent possible.
- All equipment and materials shall be stored in designated contractor staging areas on or next to the worksite, such that vehicle, pedestrian, and bicycle traffic obstruction is minimized.
- Construction shall be coordinated with facility owners or administrators of police and fire stations (including all fire protection agencies) and transit stations or stops. Emergency service vehicles shall be given priority for access.
- The contractor shall be encouraged to reduce the number of construction workers' vehicle trips by facilitating the use of public transportation and minimizing construction worker parking availability.
- The contractor shall coordinate with other contractor(s) for projects in the vicinity and share information regarding schedule, duration of activities, vehicle routing and detouring (if applicable), staging of vehicles, etc.
- The contractor shall provide regularly-updated information (typically in the form of website, news articles, on-site posting, etc.) regarding project construction and schedule, as well as contact information for specific construction inquiries or concerns.
- During Phase 1 of the construction period, passenger loading (student drop-off/pick-up) shall take place that the passenger loading zone at 19th Avenue and Sloat Boulevard (see Appendix D). This shall be communicated to students and parents, and monitored by school staff. Staff will be available to ensure queues at the passenger loading zone do not extend into the bus stop or into 19th Avenue traffic. Staff will also assist students getting to school (i.e. walking along Sloat Blvd route and going through crosswalks).
- The school shall continue to discourage students and staff from driving to school during the construction period. Staff parking will be available at the nearby Scottish Rite (10 spaces will be leased); otherwise, staff will be reimbursed for the use of taxis or other rideshare vehicles. See email correspondence with Scottish Rite in Appendix H.

It is noted that the construction management plan shall be reviewed by the SFMTA, TASC, and other City agencies as appropriate to adequately address issues of circulation (traffic, pedestrians, and bicycle), safety, parking and other project construction in the area.

Improvement Measure #12: Construction Notification from School Administration

Prior to construction and approval of *Construction Management Plan*, the school should notify all faculty/staff, parents, students and visitors of construction activities and include detailed information pertaining to schedule, access to school, loading facilities, potential detouring and to include a notice to have faculty/staff, students, parents, visitors, etc. avoid parking at the campus and to utilize other modes of transportation. Such information could be posted on the school's website and the school should provide contact information for anyone who has questions or concerns regarding construction information. On-site monitoring of students traveling to/from the school in the morning and afternoon periods should also be conducted by faculty/staff to ensure student safety while walking in and around the school during construction activities.

San Francisco Waldorf School commits to implementing the above measures at its located at 470 West Portal Avenue in San Francisco, California.



Lisa Anderson
High School Administration Coordinator
San Francisco Waldorf School

11-17-16
Date

PLANNING COMMISSION

**Case No. 2006.0100C
470 West Portal Avenue
Assessor's Block 2540, Lot 001
And Assessor's Block 2484,
Lots 008 and 009
Motion No. 17262
Page 1 of 10**

Subject to:

- Inclusionary Housing
- Childcare Requirement
- Park Fund
- Art Fund
- Public Open Space Fund
- Jobs Housing Linkage Program
- Transit Impact Development Fee
- First Source Hiring
- Other: Rincon Hill ComMUNity Infrastructure Fee and the SOMA Stabilization Fee

**SAN FRANCISCO
PLANNING COMMISSION
MOTION NO. 17262**

ADOPTING FINDINGS RELATING TO THE APPROVAL OF A CONDITIONAL USE AUTHORIZATION PURSUANT TO PLANNING CODE SECTION 209.3(h) TO ALLOW A PRIVATE SECONDARY SCHOOL (SAN FRANCISCO WALDORF HIGH SCHOOL) TO LOCATE WITHIN A VACANT OFFICE BUILDING AT 470 WEST PORTAL AVENUE, LOT 001 IN ASSESSOR'S BLOCK 2540, AND LOTS 008 AND 009 IN ASSESSOR'S BLOCK 2484, LOCATED WITHIN A RH-1(D) [RESIDENTIAL, HOUSE, ONE-FAMILY, (DETACHED)] DISTRICT, AND A 40-X HEIGHT AND BULK DISTRICT.

Preamble

On February 9, 2006, David Bushnell of 450 Architects, authorized agent (hereinafter "Project Sponsor,") made an application (hereinafter "Application") for Conditional Use authorization pursuant to Section 209.3(h) of the Planning Code for the property at 470 West Portal Avenue, Lots 008 and 009 in Assessor's Block 2484 and Lot 001 in Assessor's Block 2540 (hereinafter "Subject Property") to convert a vacant office building into a private secondary school operated by San Francisco Waldorf High School, in general conformity with plans filed with the Application and labeled "Exhibit B", located within located in a RH-1(D) [Residential-House, One-Family (Detached)] District and a 40-X Height and Bulk District.

On June 8, 2006, the San Francisco Planning Commission (hereinafter "Commission") conducted a duly noticed public hearing at a regularly scheduled meeting on Conditional Use Application No. 2006.0100C.

The proposed Conditional Use application was determined by the San Francisco Planning Department (hereinafter "Department") to be Categorically Exempt, Class 1 and Class 12 [State CEQA Guidelines Sections 15301(a) and 15332]

The Commission has heard and considered the testimony presented to it at the public hearing and has further considered written materials and oral testimony presented on behalf of the applicant, Department staff, and other interested parties.

MOVED, that the Commission hereby authorizes the Conditional Use requested in Case No. 2006.0100C subject to the conditions contained in EXHIBIT A, attached hereto and incorporated herein by reference thereto, based on the following findings:

Findings

Having reviewed all the materials identified in the recitals above, and having heard oral testimony and arguments, this Commission finds, concludes and determines as follows:

1. The above recitals are accurate and also constitute findings of this Commission.
2. **Site Condition.** The project site is located at 470 West Portal Avenue, near where Portola Drive, Sloat Boulevard, and West Portal Avenue intersect, Lots 008 and 009 in Assessor's Block 2484 and Lot 001 in Assessor's Block 2540, located in a RH-1(D) District and a 40-X Height and Bulk District. The subject 72,094 square-foot property is occupied by a 22,925 square-foot, two-story office building and 61 off-street parking spaces. The building was previously used by Pac-Bell as a Directory Assistance Operating Center but it has been vacant since August of 2005. The building is approximately 35-feet in height.
3. **Proposal.** The project would convert the office building to the San Francisco Waldorf High School. The conversion would create 12 classrooms, three dedicated arts studios, one multi-purpose room, cafeteria and lounge areas, and staff offices to accommodate 200 students and 20 school employees. With the exception of some Building Code and ADA requirements for fire stairs, and an elevator at the rear of the building, the building envelope would not be altered and the square-footage of the building would remain the same. Other exterior changes include adding skylights on the roof and new windows along the northeast side of the building
4. **Surrounding Properties and Neighborhood.** The subject property is located at the southern boundary of West Portal Avenue, outside the Neighborhood Commercial District, which ends several blocks north of the subject property. The property abuts single-family residences to the west and north. The property is also bordered by a grove of trees to the north. There is a MUNI stop for the M-Oceanview and K-Ingleside lines in the middle of the street in front of the subject property, as a result, the street narrows to two lanes and there is no street parking in front of the property. West Portal Lutheran School is located within 300-feet of the subject property.
5. **Neighborhood Response.** Planning Department staff has received letters of support from both the West Portal Avenue Association and the Greater West Portal Neighborhood Association as well as one neighbor. The Department has also received several letters and phone calls from neighbor opposing the project for the following reasons: 1) high school students would be disruptive to the neighborhood, 2) the loading and unloading of students will impact traffic on West Portal Ave. and 3) an EIR has not been commissioned or the project.
6. **Parking.** Section 151 of the Code contains the schedule of required off-street parking

spaces. For a secondary school, either public or private, it requires one off-street parking space for each two classrooms. The proposed project, containing 12 classrooms, would require 6 parking spaces. The project will provide a total of 61 parking spaces which is above the amount that would be permitted as accessory. Conditional use authorization is not required for the parking because it is a pre-existing situation.

7. **Planning Code Section 209.3(h).** Section 209.3(h) of the Code allows secondary schools (an institutional use) in RH-1(D) Districts only upon the approval of a conditional use authorization by the Commission.

8. **Planning Code Section 206.1, RH-1(D) Districts: One-Family (Detached Dwellings).** These districts are characterized by lots of greater width and area than in other parts of the City, and by single-family houses with side yards. The structures are relatively large, but rarely exceed 35 feet in height. Ground level open space and landscaping at the front and rear are usually abundant. Much of the development has been in sizable tracts with similarities of building style and narrow streets following the contours of hills. In some cases private covenants have controlled the nature of development and helped to maintain the street areas.

9. **Planning Code Section 303(c)**
Section 303(c) of the Planning Code. Under the provisions of Planning Code Section 303(c) the Planning Commission may approve an application for conditional use if the facts presented are such to establish:

- (1) That the proposed use or feature, at the size and intensity contemplated and at the proposed location, will provide a development that is necessary or desirable for, and compatible with, the neighborhood or the community;

The project would enroll 125 students with a maximum capacity of 200 students and employ approximately 20 faculty and staff. At full occupancy, the previous office use housed 140 employees, so the proposed use is similar in intensity to the previous use. The project is necessary and desirable because it would provide an additional choice in education to neighborhood and city residents and it provides adaptive reuse of an existing building. Furthermore, there are a limited number of suitable sites available for institutional uses such as an independent school.

- (2) That such use or feature as proposed will not be detrimental to the health, safety, convenience or general welfare of persons residing or working in the vicinity, or injurious to property, improvements or potential development in the vicinity, with respect to aspects including but not limited to the following:
 - (a) The nature of the proposed site, including its size and shape, and the proposed size, shape and arrangement of the structure;

No modifications are proposed to the project site or to the arrangement of the structure on the site.

- (b) The accessibility and traffic patterns for persons and vehicles, the type and volume of such traffic, and the adequacy of proposed off-street parking and loading;

MUNIs K and M-Lines stop in the middle of the street in front of the subject property and the street narrows to two lanes. There is no street parking in front of the site. An analysis at Waldorf's students and faculty at its current campus on Valencia Street found that 34% used transit, 34% were dropped off by or drove a private automobile, and 28% carpooled. The school expects the transit mode to increase at the new site because it is well served by transit. The students that continue to arrive by private automobile will be able to queue within the existing parking lot. The conditions of approval will further help mitigate impacts on traffic

- (c) The safeguards afforded to prevent noxious or offensive emissions such as noise, glare, dust, and odor;

The noise, glare, dust, and odor generated by the occupants of the proposed use would be considered common and generally acceptable in an urban environment, and would not be considered a significant impact.

- (d) Treatment given, as appropriate, to such aspects as landscaping, screening, open spaces, parking and loading areas, service areas, lighting and signs;

No change is proposed for the parking, open spaces, loading areas, and service areas on the site.

- (3) That such use or feature as proposed will comply with the applicable provisions of the Planning Code and will not adversely affect the General Plan.

How the project supports the objectives and policies of the General Plan is outlined in Finding 10 of this Motion.

- 10. General Plan Conformity.** The Project affirmatively promotes the objectives and policies of the General Plan as follows.

COMMERCE AND INDUSTRY ELEMENT

OBJECTIVE 7: ENHANCE SAN FRANCISCO'S POSITION AS A NATIONAL AND REGIONAL CENTER FOR GOVERNMENTAL, HEALTH, AND EDUCATIONAL SERVICES

POLICY 2: Encourage the extension of needed health and education services, but manage expansion to avoid or minimize disruption of adjacent residential areas.

The Project will allow for additional choices in educational options to neighborhood and city residents and allow for a modest increase in student population at the school should others want to attend.

POLICY 3: Promote the provision of adequate health and education services to all geographic districts and cultural groups in the city.

The Project would enhance the educational services available to residents of the local area neighborhoods as well as the city at large.

RESIDENCE ELEMENT

OBJECTIVE 12: TO PROVIDE A QUALITY OF LIVING ENVIRONMENT

POLICY 3: Minimize disruption caused by expansion of institutions into residential areas.

The Project will allow a school to locate within a residential District in a vacant property that is suitable for an institutional use. As a result, additional educational services would be provided for the local neighborhood and community at large.

TRANSPORTATION ELEMENT

OBJECTIVE 2: USE THE TRANSPORTATION SYSTEM AS A MEANS FOR GUIDING DEVELOPMENT AND IMPROVING THE ENVIRONMENT

POLICY 2.5: Provide incentives for the use of transit, car pools, van pools, walking and bicycling and reduce the need for new or expanded automobile and automobile parking facilities.

This authorization includes conditions that encourage the use of alternative means of transportation, including public transit, bicycles and car pools. The school has stated that 34% of its students and faculty arrive by transit to its current location on Valencia Street and this number is expected to increase because this site is better served by transit.

OBJECTIVE 33: CONTAIN AND LESSEN THE TRAFFIC AND PARKING IMPACT OF INSTITUTIONS ON SURROUNDING RESIDENTIAL AREAS

POLICY 33.2: Protect residential neighborhoods from the parking impacts of nearby traffic generators.

The Project includes all of the parking required by Code and more than the maximum allowed as accessory under the Code. The project would provide 677% of the parking required for this use including that which is permitted as accessory. The excess parking represents an opportunity for the school to self contain its own traffic.

11. Planning Code Section 101.1. Planning Code Section 101.1 establishes eight priority-planning policies and requires review of permits for consistency with said policies. The project complies with said policies in that:

- (1) That existing neighborhood-serving retail uses be preserved and enhanced and future opportunities for resident employment in and ownership of such businesses enhanced;

The Project will bring new activity and street life to the local area, including new customers, supporting and enhancing local neighborhood-serving business.

- (2) That existing housing and neighborhood character be conserved and protected in order to preserve the cultural and economic diversity of our neighborhoods;

The proposal is for the adaptive reuse of a vacant office building. The proposed school is a more appropriate use within a residential district than an office use.

The school actively promotes economic diversity, as a result, 30% of its students receive tuition assistance.

- (3) That the City's supply of affordable housing be preserved and enhanced;

The project would not affect the City's supply of affordable housing.

- (4) That commuter traffic not impede MUNI transit service or overburden our streets or neighborhood parking;

A traffic survey conducted by the school showed that more than half of its students and faculty take transit or carpool to its current campus. This trend is expected to continue at the proposed campus as it is encouraged by the philosophy of the school..

- (5) That a diverse economic base be maintained by protecting our industrial and service sectors from displacement due to commercial office development, and

that future opportunities for resident employment and ownership in these sectors be enhanced;

This project will not affect industrial or service sector uses or related employment opportunities. Ownership of industrial or service sector businesses will not be affected by this project.

- (6) That the City achieve the greatest possible preparedness to protect against injury and loss of life in an earthquake;

Internally the building would be brought up to Building Code for the proposed occupancy change.

- (7) That landmarks and historic buildings be preserved; and,

This project will not have any impacts on landmarks or historic buildings.

- (8) That our parks and open space and their access to sunlight and vistas be protected from development.

This project will have no negative impact on existing parks and open spaces. The existing landscape on the site will be retained.

DECISION

That based upon the Record, the submissions by the Applicant, the staff of the Department, and other interested parties, the oral testimony presented to the Commission at the public hearing, and all other written materials submitted by all parties, the Commission hereby **APPROVES** Conditional Use Application No. **2006.0100C** subject to the following conditions attached hereto as **Exhibit A** which is incorporated herein by reference as though fully set forth.

I hereby certify that the foregoing Motion was ADOPTED by the Planning Commission on June 8, 2006.

Linda Avery
Commission Secretary

AYES: Commissioners Dwight S. Alexander, William L. Lee, Michael J. Antonini,
Christina Olague

NAYS: None

ABSENT: Commissioners Bradford Bell, Kevin Hughes, Sue Lee

ADOPTED: June 8, 2006

**EXHIBIT A
CONDITIONS OF APPROVAL**

This approval is for the conversion of a vacant 22,925 square-foot office building into a secondary school operated by the San Francisco Waldorf High School, located in a RH-1(D) [Residential-House, One-Family (Detached)] District and a 40-X Height and Bulk District., in general conformance with plans dated February 9, 2006 and stamped "Exhibit B."

1. Enrollment for a school at the Project Site shall be limited to 200 students. Any increase in enrollment beyond 200 students at the Project Site shall require approval of a new or amended conditional use authorization by the Commission.
2. The Project shall be equipped with sufficient outdoor and indoor trash receptacles to avoid litter problems in the surrounding neighborhood.
3. Noise and light shall be contained within the premises so as not to be a nuisance to nearby residents or neighbors. Project lighting shall be directed onto the property so as not to directly illuminate adjacent properties. Only non-reflective glass shall be used on the building exterior.
4. The Applicant shall provide attendants or monitors to supervise and direct traffic and parking adjacent to the Project campus during primary drop-off and pick-up times before and after school is in session to discourage double parking and promote the orderly flow of traffic.
5. The Applicant shall establish a program to reduce vehicle usage by students and faculty and encourage transit and alternative means of transportation. Such programs should include an advertised system of internally coordinated car pools, incentives and information regarding public transit, and encouragement of the use of bicycles. Information on such a program and advisement of the sensitivity of parking and drop-off/pick-up loading in the area shall be included in student/parent and employee information packages. The Applicant shall submit a Vehicle Usage Reduction Program to the Department for review and approval prior to the issuance of a site or building permit. The Applicant shall submit an annual report on the Vehicle Usage Reduction Program to the Department until the Zoning Administrator determines that it is no longer necessary.
6. The Applicant shall take all reasonable measures to prevent loitering by students (and possible associated nuisances) during break times or before and after classes in adjacent residential areas.
7. The Applicant shall appoint a community liaison officer to deal with issues of concern to neighbors related to the operation of this Project. The name and telephone number of the community liaison shall be reported to the Zoning Administrator.

8. Should implementation of this project result in complaints from neighborhood residents which are not resolved by the Project Sponsor and are subsequently reported to the Zoning Administrator and found to be in violation of the Planning Code and/or the specific Conditions of Approval for the Project as set forth in Exhibit A of this Motion, the Zoning Administrator shall report such complaints to the Planning Commission which may thereafter hold a public hearing on the matter in accordance with the hearing notification and conduct procedures as set forth in Sections 174, 306.3 and 396.4 of the Code to consider revocation of this Conditional Use Authorization.
9. Should the monitoring of Conditions of Approval contained in Exhibit A of this Motion be required, the Project Sponsor or successor=s shall pay fees as established in Planning Code Section 351(f)(2).
10. Prior to the issuance of a building or site permit, the Zoning Administrator shall approve and order the recordation of a notice in the Official Records with the Office of the Recorder of the City and County of San Francisco, which notice shall state that construction of the Project has been authorized by and is subject to the conditions of this Motion. From time to time, after the recordation of such notice, the Zoning Administrator shall affirm in writing the extent to which the conditions of this Motion have been satisfied.

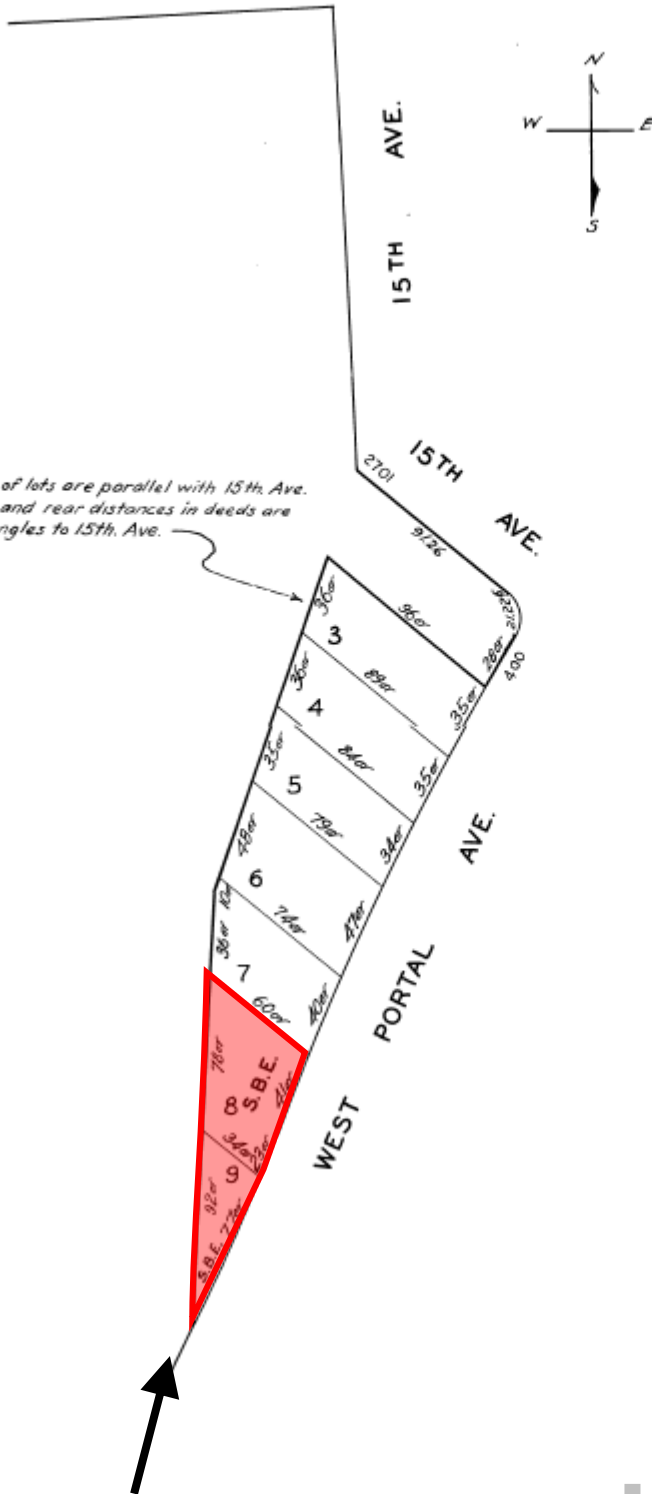
Parcel Map

2484

SUNSET BLK. 1255

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CITY & COUNTY ASSESSOR 1995

WAWONA



Note:
Side lines of lots are parallel with 15th. Ave.
Frontages and rear distances in deeds are
at right angles to 15th. Ave.

SUBJECT PROPERTY



Conditional Use Authorization Hearing
Case Number 2015-004567CUA
470 West Portal Avenue
Block 2484 Lots 008 & 009

Parcel Map

© COPYRIGHT SAN FRANCISCO
CITY & COUNTY ASSESSOR 1968
lot8 into lots93to101 for 2008 roll
lot93 into lots102/104 for 2011 roll

SHEET 1

2540

(Enclosure & portion of Block 2540 from the City of Albany 1847 to 1850)

ARDEN WOOD

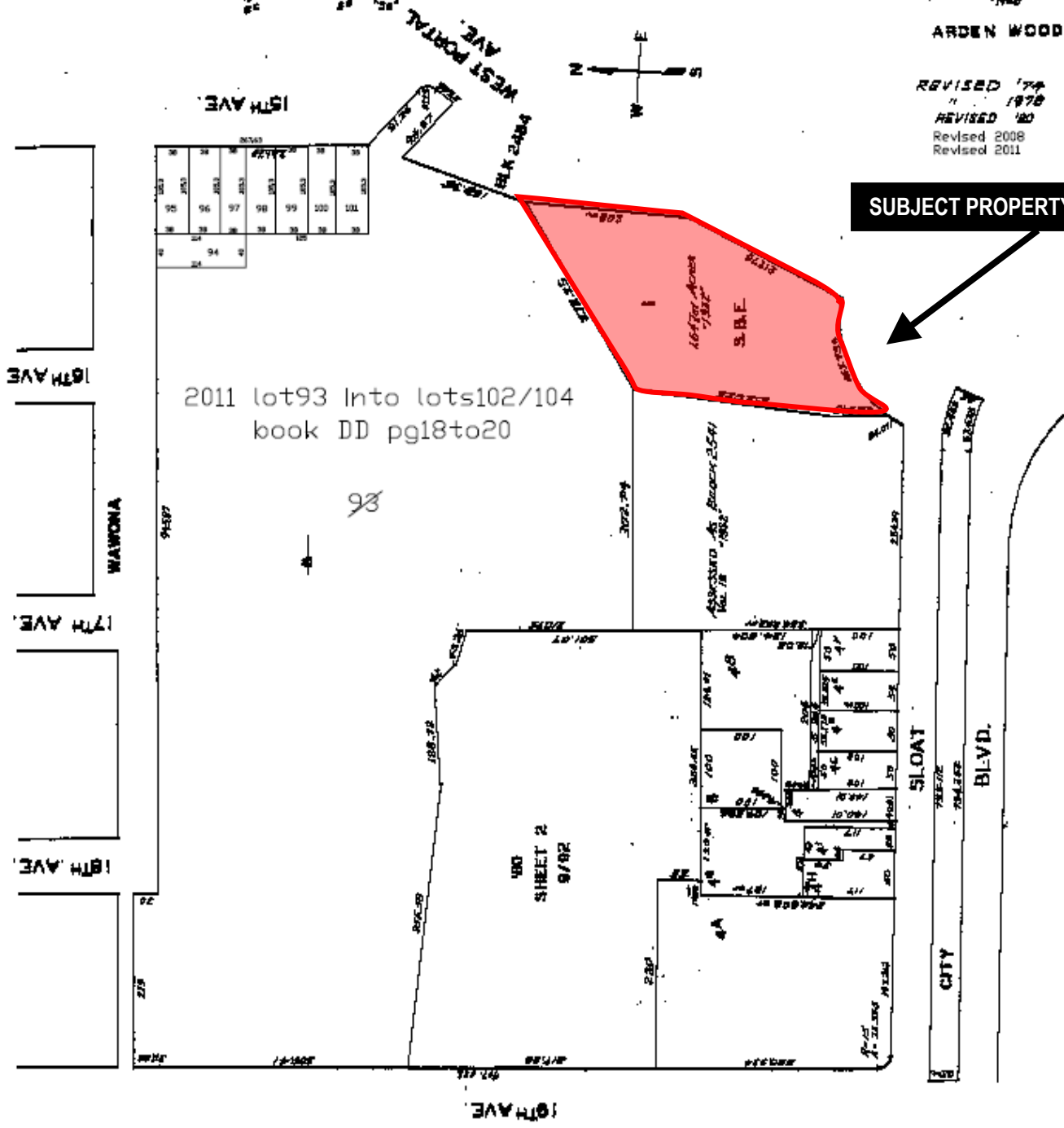
SCALE: 1 IN. = 100 FT.

REVISED '74
" 1978
REVISED '80
Revised 2008
Revised 2011

LOT MERGED
LSTS INTO LOTS
BLK 2517



SUBJECT PROPERTY

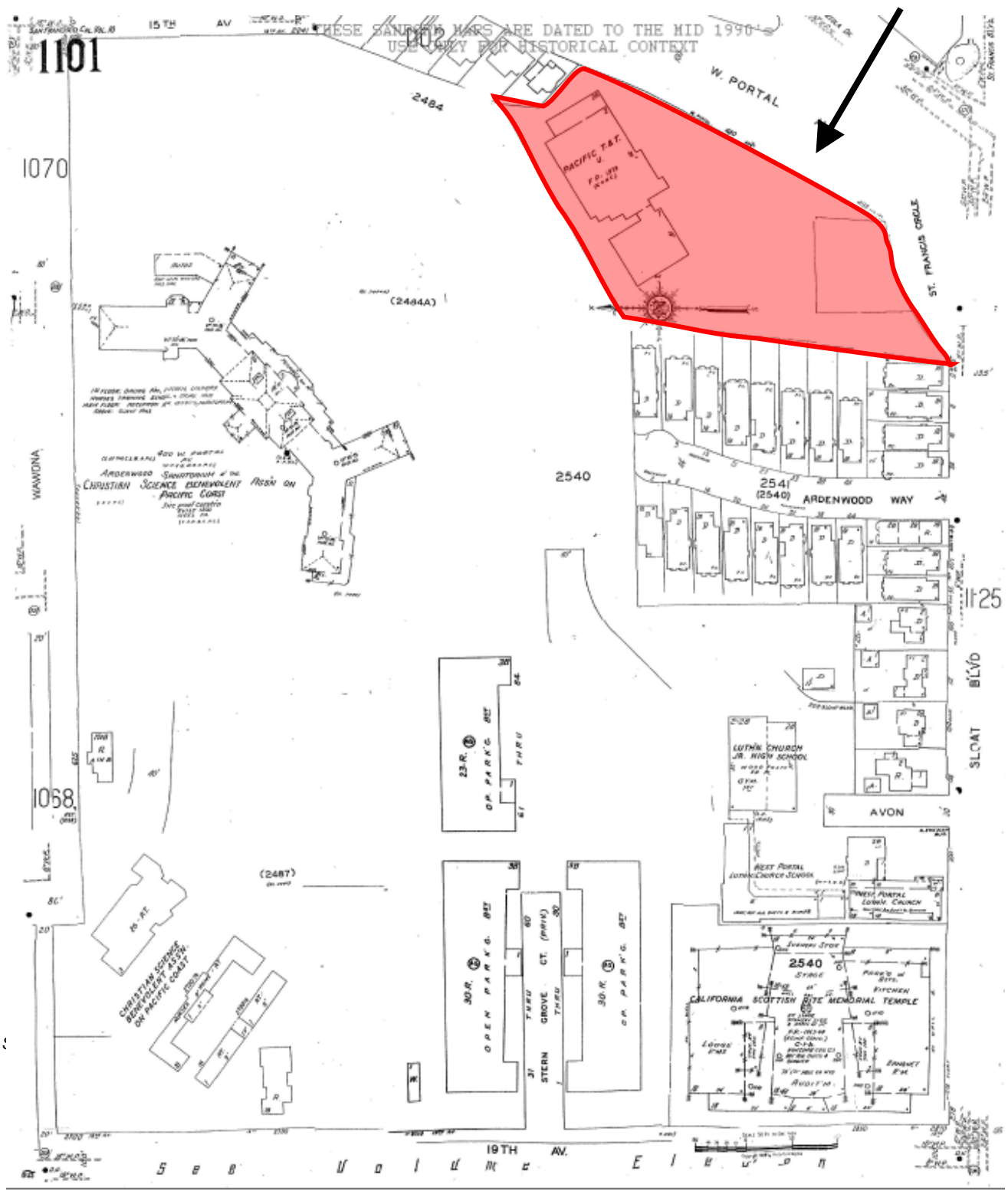


Conditional Use Authorization Hearing
Case Number 2015-004567CUA
470 West Portal Avenue
Block 2540 Lot 001



Sanborn Map*

SUBJECT PROPERTY



*The :



Conditional Use Authorization Hearing
Case Number 2015-004567CUA
470 West Portal Avenue
Block 2484 Lots 008 & 009/Block 2540 Lot 001

Aerial Photo



SUBJECT PROPERTY



Conditional Use Authorization Hearing
Case Number 2015-004567CUA
470 West Portal Avenue
Block 2484 Lots 008 & 009/Block 2540 Lot 001

Aerial Photo (looking west)



SUBJECT PROPERTY



Conditional Use Authorization Hearing
Case Number 2015-004567CUA
470 West Portal Avenue
Block 2484 Lots 008 & 009/Block 2540 Lot 001

Aerial Photo (looking north)

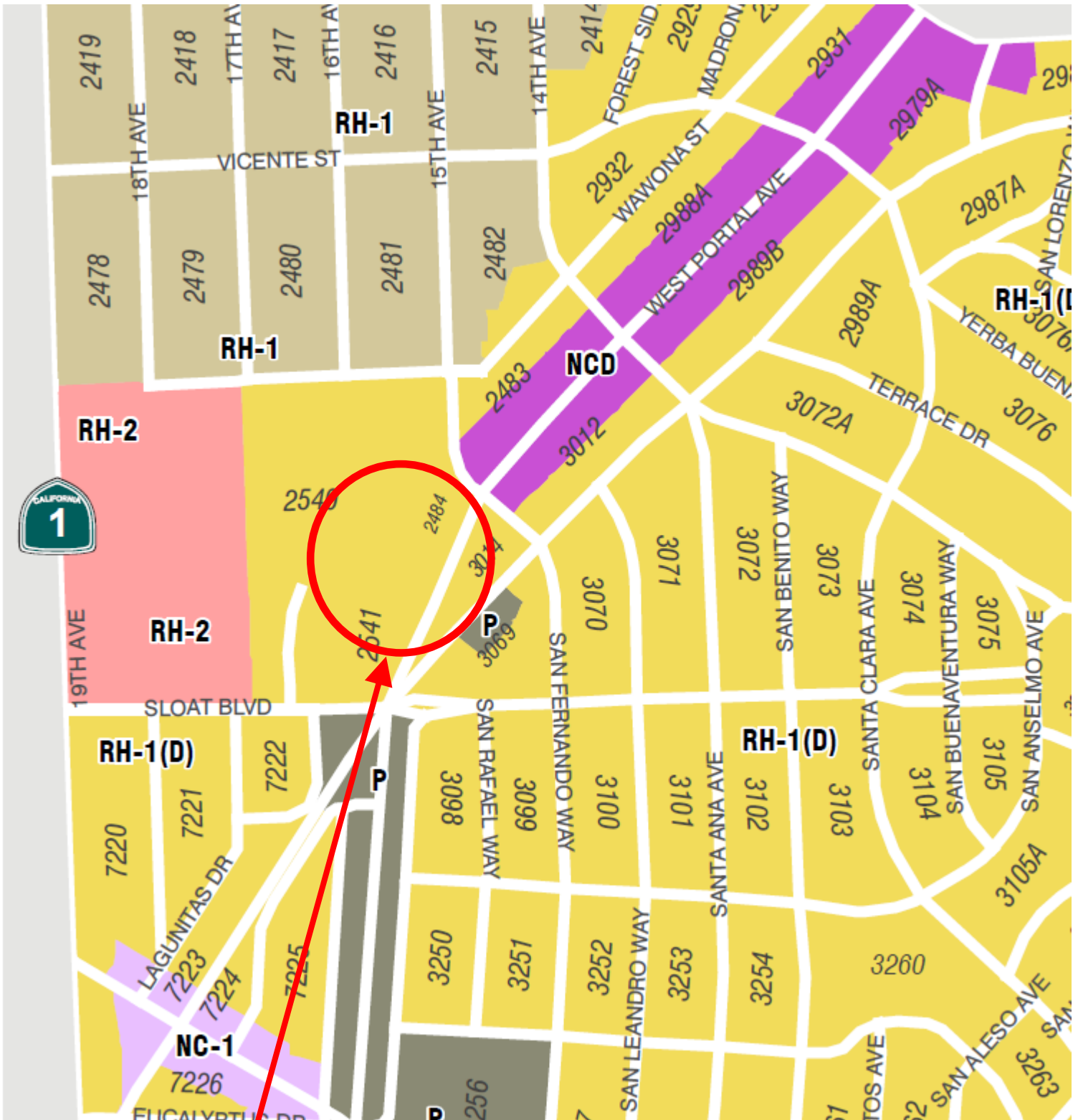


SUBJECT PROPERTY



Conditional Use Authorization Hearing
Case Number 2015-004567CUA
470 West Portal Avenue
Block 2484 Lots 008 & 009/Block 2540 Lot 001

Zoning Map



SUBJECT PROPERTY



Conditional Use Authorization Hearing
Case Number 2015-004567CUA
470 West Portal Avenue
Block 2484 Lots 008 & 009/Block 2540 Lot 001

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Gordon-Jonckheer, Elizabeth (CPC)

From: John Mitchell <JMitchell@ardenwood.org>
Sent: Thursday, November 17, 2016 11:43 AM
To: Gordon-Jonckheer, Elizabeth (CPC)
Subject: SUPPORT LETTER FOR WALDORF HS GYMNASIUM PROJECT, 11-17-16
Attachments: SUPPORT LETTER FOR WALDORF HS GYMNASIUM PROJECT, 11-17-16.pdf

Dear Ms. Jonckheer,

As neighbors, we are pleased to offer our full support for the proposed Waldorf High School Gymnasium project located at St. Francis Circle. I've attached a letter clearly stating our support for your review.

Please don't hesitate to contact me directly with any questions at this email or by phone at (415) 379-2200.

Very sincerely,



John W. Mitchell
Executive Director / CEO
Arden Wood, Inc.
445 Wawona Street
San Francisco, CA 94116
Direct: 415-379-2200
Toll free: (800) 767-0003 x2200
www.ardenwood.org



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November 17, 2016

Ms. Elizabeth Gordon Jonckheer
Preservation Planner/Current Planner, Southwest Quadrant
Planning Department, City and County of San Francisco
1650 Mission Street, Suite 400
San Francisco, California 94103-2480

Dear Ms. Jonckheer,

We are long-time and long-term neighbors of the San Francisco Waldorf High School located at St. Francis Circle, and we are in favor of their plan to build a gymnasium. Athletics are an important component of a well-rounded education, especially during the teen years, and of a balanced lifestyle at every age. The value of a gymnasium is unquestionable, and we are happy to fully support this project.

Our relationship with the SF Waldorf High School is warm and neighborly. For the last 5 years, the students and faculty have spent many hours restoring a sizable area on our property to its natural habitat. During the first week of school every year since 2011, the 9th grade students spend their afternoons working on our property. Their curriculum also includes a yearlong class, called Habitat Restoration; a large part of the class involves hands-on experience, i.e., learning through working on our property. In acknowledgement of their efforts, we host a special barbeque for the students and staff every spring to meet and thank them in person. One year, the senior class used our property and building to film a segment of their senior movie project.

We have always found the Waldorf High School students to be respectful and kind to our residents. There is plenty of goodwill between our very different organizations—we are good neighbors. As such, we are pleased to support the building of a gymnasium.

Very best regards,

John W. Mitchell
Executive Director / CEO

Gordon-Jonckheer, Elizabeth (CPC)

From: Lisa Anderson <landerson@sfwaldorf.org>
Sent: Thursday, November 17, 2016 4:53 PM
To: Gordon-Jonckheer, Elizabeth (CPC)
Cc: David Bushnell
Subject: Support for SF Waldorf Project
Attachments: SF Gym support.pdf

Follow Up Flag: Flag for follow up
Flag Status: Flagged

Dear Elizabeth, Yesterday I took a walk down West Portal to check in with the neighborhood merchants. It was between 10:00am and 11:00am. Many merchants were not open yet but I was able to speak with the manager or owner of 8 businesses. In each case they were supportive of our plans to build a gym. I asked them to sign the attached document confirming their support.

The merchants who have signed are;

Armstrong Flooring
Greenhouse Cafe
Manor Cafe
Eezy Freezy
Submarine Center
Papenhausen Hardware
West Portal News
Goat Hill Pizza

I will continue this neighborhood outreach and update you with the results. -

Thank you for your help with our project.

--

Lisa Anderson
High School Administrative Coordinator
San Francisco Waldorf High School
470 West Portal Avenue
San Francisco, CA 94127

Tel: 415-431-2736, ext. 111






Fax: 415-431-1712

E-mail: landerson@sfwaldorf.org

Web: <http://www.sfwhs.org>

We the undersigned are in support of the San Francisco Waldorf School expansion plans. The addition of a gym on site will strengthen the athletic program at the school.

The school has been a good neighbor and their students are a vital and welcome part of our community.

1. Steve Balestreri Armstrong Carpet & Linoleum Co
369 West Portal Ave SF 
2. Queenia Leung 329 West Portal Ave
Greenhouse Cafe 
3. Mike Gonzalez Manor Coffee Shop 
4. Maureen Stoss Eezy Freezy 
5. Rafik Zadoo Mang _____
sub center 
6. Matthew Rogers OWNER —
Popenhansen Hardware 
7. Issa Haddad Westport al _____
to market 
8. Shayon Bariani Goathill pizza 

Gordon-Jonckheer, Elizabeth (CPC)

From: Paul Peterson <peterson314@gmail.com>
Sent: Tuesday, November 15, 2016 4:00 PM
To: Gordon-Jonckheer, Elizabeth (CPC)
Subject: Support for Proposed SF Waldorf Gym

Dear Ms. Jonckheer,

My wife Martha and I are the owner-occupants at 45 Ardenwood Way. As immediate neighbors of the SF Waldorf School, we are writing to support the school's current plan to build a gym on the eastern edge of its property.

The school has been responsive to all our concerns, and we feel that the school is a good neighbor. We particularly appreciate that the school changed the original plan by moving the gym from the western property edge to the eastern edge and by re-designing the gym to fit in the reduced space available.

If you have any questions feel free to email us or call [571.366.0859](tel:571.366.0859).

Sincerely,

Paul Peterson

Gordon-Jonckheer, Elizabeth (CPC)

From: Thomas Kanaley <tkanaley@yahoo.com>
Sent: Sunday, November 20, 2016 5:01 PM
To: Gordon-Jonckheer, Elizabeth (CPC)
Cc: Mary Burns
Subject: Waldorf School
Attachments: Waldorf Letter.pdf

Dear Ms. Gordon-Jonckheer:

Attached is a letter from the Greater West Portal Neighborhood Association in conditional support of the Waldorf School application. The support is conditional on Waldorf preparing and complying with a professional landscaping plan, as proposed by the neighbors and previously promised by Waldorf.

Thank you for your attention to this.

Tom Kanaley
President, 2016-17
Greater West Portal Neighborhood Association



Greater West Portal Neighborhood Association

Families working to improve their neighborhood

November 14, 2016

Planning Department
City and County of San Francisco
1650 Mission Street, Suite 400
San Francisco, CA 94103

RE: Waldorf School at 470 West Portal Ave. Expansion and Gardening

Dear Elizabeth Gordon Jonckheer,

The Greater West Portal Neighborhood Association (GWPNA) formed in 1974 and is an active philanthropic non-profit corporation representing nearly 2,200 homes in the West Portal area.

We conditionally support the San Francisco Waldorf High School plans for the new gymnasium and reconfiguration of their driveway at 470 West Portal, provided they include a written commitment to the planning Department and GWPNA that they will perform regular, professional quality landscaping and maintenance to their grounds. We voted to support this at our November 2nd General Membership meeting.

Sincerely,

A handwritten signature in black ink, appearing to read "TK", is written over a horizontal line.

Thomas Kanaley, President

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Gordon-Jonckheer, Elizabeth (CPC)

From: Riseman James <jriseman@yahoo.com>
Sent: Wednesday, November 16, 2016 10:18 PM
To: Yee, Norman (BOS); Low, Jen (BOS); Gordon-Jonckheer, Elizabeth (CPC)
Subject: Conditional Use Hearing for 470 West Portal Ave/ Waldorf School

Hi Elizabeth,

I received a mailer about the upcoming Planning Commission hearing for the Waldorf School (470 West Portal Ave). Can you send me any more information on their plans? They hosted a neighborhood gathering several months ago where we discussed their development plans. Many concerns were raised (like eliminating parking spaces, putting a high wall adjacent to the sidewalk, etc), and I hope they're addressing these concerns in their latest plans.

Supervisor Yee and Jen, copying you on this so that this project is in your sights. Have you heard anything about this upcoming Planning Commission hearing on December 1? Do you know approximately what time it'll take place? I'd like to attend, but I can't free up more than a couple hours that day.

Regards,

James Riseman
1650 Portola Dr.

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Gordon-Jonckheer, Elizabeth (CPC)

From: Mary Burns <maryfburns@att.net>
Sent: Monday, November 07, 2016 11:45 AM
To: Gordon-Jonckheer, Elizabeth (CPC)
Cc: 'Stu Gardiner'; 'Mary Burns'
Subject: Waldorf School Building Extension Project
Attachments: PLANNING CODE VIOLATION COMPLAINT FORM for Waldorf School Complaint 6-14-12.doc; Waldorf School - Planting and Maintenance Plan.pdf; Comments on Waldorf Landscape Plan 14 June 2013.doc

November 7, 2016

Elizabeth Gordon Jonckheer
Planner/Preservation Specialist, Southwest Quadrant
Planning Department, City and County of San Francisco
1650 Mission Street, Suite 400
San Francisco, California 94103

RE: San Francisco Waldorf High School Building Project – Landscaping Requirement

Dear Ms. Gordon Jonckheer:

On behalf of ourselves and other residents of Ardenwood Way, which borders the western side of the San Francisco Waldorf High School property at 470 West Portal Avenue, we request that the Planning Department include as an element of Planning Commission approval of the school's proposed project a landscaping program that assures compliance with the *existing* Conditional Use Permit for this property. An integral part of our support for the school's expansion plan—support which has been consistent since the school acquired the property in 2005, assuming of course that the final plan presented for Commission approval is acceptable—is curing the persistent insufficiency of the school's landscaping and grounds maintenance. Because the proposed project may be on the Commission's agenda as soon as December 1, this letter and attachments present the basis for this requirement.

Subsections 10, 11 and 13 of the Conditional Use Permit approved in 1971 for the construction of the existing building (adopted under Planning Commission Resolution No. 6671 and incorporated in the school's CUP of 2006) address the installation and maintenance of landscaping for the parcel. As explained in the attached Code Violation Complaint we filed in 2012, the school has failed to comply with these obligations since it acquired the property. In response to the complaint, Code Enforcement staff required the school to submit a landscaping plan. The document submitted by the school in 2013 as a plan is attached. Also attached is our response to that submittal, explaining why it was inadequate to meet the school's CUP obligations and proposing a process to develop and approve an satisfactory plan. (We provide that response for its explanation of the deficiencies of the school's plan, and not necessarily as a proposed process for developing an adequate successor plan.)

Unfortunately, Planning staff did not require any improvement to the school's submittal. In the three years since, the school has continued to perform only sporadic maintenance, much of it relying on student labor and some of which has destroyed valuable plants, and has done little new landscaping. The result is that the condition of the grounds continues to detract from the visual quality of the well-maintained surrounding neighborhood, including the school's low hillside bordering our homes on Ardenwood Way which is an ugly mess of overgrown wild grasses and weeds most of the time.

Our fundamental concern is that the school will continue largely to ignore its landscaping obligations unless the Commission includes compliance as a condition of approving the building expansion project. The *ad hoc* code

enforcement process has proven inadequate, and the school should not benefit from its new project while ignoring its existing obligations to the community. That is why we are asking you, as the staff planner assigned to this project, to examine this problem and to include the following items in the Commission resolution approving the project:

- Require that Waldorf High School promptly prepare and implement a written landscaping and grounds maintenance plan, after consultation with neighbors including residents of Ardenwood Way and approval by Planning staff;
- Specify that this plan reflect landscaping design, standards and methods suitable for an institutional parcel in an RH-1(D) zone, and require implementation by sufficient qualified, trained and supervised landscaping workers instead of relying significantly on student and faculty efforts. Note that we do not object to student or faculty involvement in landscape development or maintenance, but this must be supplemental to an assured level of effort and quality by professional landscapers.
- Direct Planning staff to monitor, at least annually and with consultation with neighbors, adequate compliance with the landscaping and maintenance plan for not less than 10 years, to end this recurring problem.

Please contact us if there is more we can do or provide to help advance this proposal. We believe the school has given limited attention and resources to landscaping because it has not made, and has not had to make, this obligation an adequate budget and program priority. The result is an inward focus that ignores basic needs and expectations of the community that surrounds it. The Planning Department has the opportunity, through the approval process for the school's building project, to restore the school's relationship with its neighborhood to one of mutual trust and respect. We hope you will take that opportunity.

We would appreciate it if you will confirm that you received this email and attachments.

Sincerely,

Mary F. Burns

(Former president of the Greater West Portal Neighborhood Association, and SFMTA Citizens Advisory Committee)

Stuart K. Gardiner

(Former Planning Commissioner, City of Berkeley)

21 Ardenwood Way
San Francisco, CA 94132
415-566-4669
maryburns@att.net
stu.gardiner@att.net

cc: Lisa Anderson, Waldorf High School
Supervisor Norman Lee
Thomas Kanaly, President, GWPNA
Ardenwood neighbors

Gordon-Jonckheer, Elizabeth (CPC)

From: Mary Burns <maryburns@att.net>
Sent: Thursday, November 10, 2016 11:24 AM
To: Gordon-Jonckheer, Elizabeth (CPC)
Cc: 'Lisa Anderson'; david@450architects.com
Subject: Update on Landscaping Issue at S.F. Waldorf High School

Dear Ms. Gordon-Jonckheer,

Both we and the Waldorf High School are now engaged in an effort to agree promptly on language to be included in the Conditional Use Permit associated with the school's expansion project that will address the concerns raised in our email/letter to you of November 7, 2016.

Yesterday we met with representatives of the school at their initiative. We had a productive discussion of the historical, current and future conditions and possibilities for landscaping design, installation and maintenance of the property. The school will take the next step and send us its proposal for CUP language, along with possible exhibit(s), one that we expect will be responsive to the language proposed in our November 7 email.

We will keep you advised of the status of this effort. Please let us know if you have any thoughts or recommendations at any time.

Sincerely,

Mary F. Burns
Stuart K. Gardiner

21 Ardenwood Way
San Francisco, CA 94132
415.566.4669

cc: Lisa Anderson, S.F. Waldorf High School
David Bushnell, project architect

Gordon-Jonckheer, Elizabeth (CPC)

From: Mary Burns <maryburns@att.net>
Sent: Thursday, November 17, 2016 6:48 PM
To: Gordon-Jonckheer, Elizabeth (CPC)
Cc: david@450architects.com; 'Lisa Anderson'; 'Stu Gardiner'
Subject: FW: 470WP landscaping conditions: Final Language
Attachments: Waldorf HS CUA language FINAL.DOCX

Follow Up Flag: Follow up
Flag Status: Flagged

Dear Elizabeth – as you can see below, David Bushnell intended to send this to you, and thought he did. I just noticed that your email address isn't included! Attached is the final language we have all agreed upon.

Thanks for all your help!

Mary F. Burns
San Francisco, California

From: David Bushnell [<mailto:david@450architects.com>]
Sent: Thursday, November 17, 2016 2:41 PM
Cc: 'Alex Gunst'; 'Mary Burns'; 'Stu Gardiner'; 'Lisa Anderson'
Subject: 470WP landscaping conditions: Final Language

Hi Elizabeth,

Please find the attached conditions of approval language regarding the landscaping agreed on by the SFWHS and Ardenwood Way neighbors.

Thank you for helping us to develop a language that is mutually beneficial for the school and neighboring residents.

Best regards,
David

David Bushnell, AIA, LEED AP
Principal

450 architects, inc.
9 pier, suite 105
the embarcadero
san francisco, california 94111
t: 415.546.0450
www.450architects.com

From: Lisa Anderson [<mailto:landerson@sfwaldorf.org>]
Sent: Thursday, November 17, 2016 1:44 PM
To: David Bushnell <david@450architects.com>; Alex Gunst <a.gunst@poundmgt.com>; Mary Burns

<maryburns@att.net>; Stu Gardiner <stu.gardiner@att.net>

Subject: Final Language

Hi David, Here is the final agreed upon language for the landscaping Element of the conditional use permit.

Please copy Mary and Stu when you send this to Elizabeth. Thank you all for your time and effort to get this completed. -Lisa

--

Lisa Anderson
High School Administrative Coordinator
San Francisco Waldorf High School
470 West Portal Avenue
San Francisco, CA 94127

Tel: 415-431-2736, ext. 111

Fax: 415-431-1712

E-mail: landerson@sfwaldorf.org

Web: <http://www.sfwhs.org>

Proposed Language for Landscaping Element of Conditional Use Authorization
San Francisco Waldorf High School Expansion Project

1. Creation – Within three months of the date of CUA approval, San Francisco Waldorf High School (“Project Sponsor”) shall submit for Planning Department approval a landscape and grounds-maintenance plan (“Plan”) for 470 West Portal Avenue. During development of the Plan and before submitting it, the Project Sponsor shall consult with neighbors, including residents of Ardenwood Way, and shall include with the submitted Plan all comments received from neighbors. Planning Department shall use reasonable efforts to approve the Plan, after any modifications it may require, within three months after submittal, so that implementation will begin promptly. If and when the Project Sponsor updates or otherwise revises the Plan, on its own initiative or because the Planning Department requires this due to changes in standards, conditions or compliance issues, the Project Sponsor also shall consult with its neighbors. If a revised Plan is submitted to the Planning Department, all comments received from neighbors shall be included.

2. Purpose, standards and phases

(a) The Plan shall assure that the school grounds, including the area outside the fence bordering on Saint Francis Circle and the western slope adjoining Ardenwood Way parcels, present a uniformly attractive, neat, and clean appearance. The Plan shall assure screening of the western and northern sides of school buildings from neighboring properties. The Plan shall conform to applicable City requirements and policies regarding plantings, such as those concerning native species and drought tolerance, and shall be consistent with the school’s LEED status. Neighboring parcel owners shall not place compostable yard refuse on school property without prior written agreement.

(b) The Plan, and any revisions, shall reflect design, methods, staffing and time schedules for both installation and maintenance consistent with prevailing professional standards in the Bay Area and suited to an institutional parcel in an RH-1(D) zone. Because student or faculty involvement in landscape development or maintenance may be associated with the instructional program of the school, the Plan shall not preclude such involvement, but the Plan shall not rely on student and faculty effort for implementation.

(c) The Plan may be organized in phases to reflect the near-term and longer-term building improvements and revisions recognized in this CUA. The Plan in all phases shall meet the requirements of subsections (a) and (b), and construction and alteration activities shall interfere with or delay landscaping installation and maintenance as little as possible.

3. Monitoring and Compliance of Landscape Plan after Approval – The Project Sponsor shall install landscaping as shown on the exhibit to the Plan titled the Preliminary Planting Plan, submitted to the Planning Department on November 16, 2016, as revised to be consistent with [these Conditions], and shall demonstrate to Planning Department compliance staff that said landscaping is established and maintained according to the Plan, per Conditions XX through XX. The Project Sponsor shall permit the Planning Department to conduct site inspections a minimum of one time per year as necessary to ensure that landscaping is maintained for the life of the project. Violation of these conditions shall be subject to the enforcement procedures and

administrative penalties set forth under Planning Code Section 176 or Section 176.1. The Planning Department may also refer the violation complaints to other city departments and agencies for appropriate enforcement action under their jurisdiction.

For information about compliance, contact Code Enforcement, Planning Department at [415-575-6863](tel:415-575-6863), www.sfplanning.org.

Exhibit A – Immediate Actions for Western Slope Bordering Ardenwood Way

The Project Sponsor shall take the following steps immediately upon CUA approval. These actions address current deficiencies only and shall be superseded in the Plan to the extent the Plan includes elements better suited to achieving its purpose.

1. Hillside and Building screening:

On the west side, the Project Sponsor shall install limited hillside screening at the lowest landscape terrace to partially obscure views of the upper terraces. This screening shall be accomplished with planting that will allow glimpses through to the garden areas and shall not be an impenetrable mass. The selection of plant materials shall be determined by the Project Sponsor. The Project Sponsor shall gradually (within 3 years) replace the volunteer Acacia trees in the west side slope with more manageable tree species consistent with maintaining adequate screening of the School buildings during the transition and afterwards. The latter tree replacement shall be done in such a way as to allow for succession planting.

2. Weed control:

The Project Sponsor shall keep the weeds on the slope facing the residential neighbors (west side of the property) and on all areas along and outside the school fencing mown at least twice monthly.

3. Site maintenance:

The Project Sponsor shall augment existing landscaping and maintain the site by hiring professional maintenance personnel who will provide twice-monthly service.

San Francisco Waldorf High School Expansion

470 WEST PORTAL AVE, SAN FRANCISCO, CA 94127

CONDITIONAL USE PERMIT

SYMBOLS	SCOPE OF WORK	BUILDING AREA INFORMATION	LIST OF DRAWINGS
<p>SYMBOLS</p> <p>EXTERIOR ELEVATION / ELEVATION SERIES</p> <p>INTERIOR ELEVATION / ELEVATION SERIES</p> <p>BUILDING SECTION</p> <p>DETAIL SECTION</p> <p>DETAIL CALLOUT</p> <p>VERTICAL DATUM DIMENSION</p> <p>REVISION CLOUD</p> <p>LINETYPES</p> <p>--- DEMOLISHED</p> <p>----- OVERHEAD</p> <p>----- CONC CONTROL JT</p> <p>----- CENTERLINE</p> <p>----- HIDDEN</p> <p>⊕ NORTH NORTH ARROW</p> <p>11 CEILING TAG</p> <p>SAMPLE ROOM NAME ROOM TAG: Room Name and Room Number</p> <p>11 DOOR TAG</p> <p>11 WINDOW TAG</p> <p>11 WALL TAG</p> <p>ALIGN ALIGN</p> <p>--- RATED WALL 1 HR.</p> <p>----- FIRE LANE</p> <p>●●●● PATH OF TRAVEL, ACCESSIBLE OR EGRESS AS NOTED</p> <p>--- FENCE LINE, CHAIN LINK</p>	<p>SCOPE OF WORK</p> <p>The proposed alteration to the existing San Francisco Waldorf High School will expand the size and occupancy of the existing 23,000 square foot campus located at 470 West Portal Avenue, at the north end of Sloat Boulevard. The current campus includes 12 classrooms, 3 dedicated art studios, multi-purpose room, staff offices, and support spaces to accommodate 200 students and 30 school employees. The proposed project would be implemented in Phases.</p> <p>The proposed project would add a new multi-purpose gymnasium, entry lobby, performance space, 5 additional classrooms, and enroll a maximum capacity of 240 students and 32 staff. The additional square footage of approximately 23,900 square feet, combined with the existing building square footage of 23,000 square feet will total approximately 46,900 square feet.</p> <p>The first phase of the project would remove up to 37 surface parking spaces within the existing parking lot to allow construction of a new 35 foot tall, 11,100 square foot, multi-purpose gymnasium and classroom addition. This phase will also include the relocation of the existing driveway approximately 100 feet south of the current location along West Portal Avenue.</p> <p>The second phase of the project would include an 11,100 square foot addition at the northwest portion of the site beneath the existing building footprint, including 5 new classrooms and a performance space. This phase also includes a new 1,700 square foot lobby connection between the Phase 1 gymnasium addition and the existing building.</p> <p>San Francisco Waldorf High School authorized by Case No. 2006.0100CE</p>	<p>BUILDING AREA INFORMATION</p> <p>BLOCK/LOT: 2484 / 001, 008, 009</p> <p>ZONING: RH - 1 (D)</p> <p>HEIGHT LIMIT: 40 - X</p> <p>CONSTRUCTION: Type 2B</p> <p>OCCUPANCY: GROUP E/350</p> <p>PROPOSED HEIGHT: 35' - 0"</p> <p>LOT AREA: 72,094 square feet</p> <p>LOT DIMENSION: irregular/varies 220 x 530 feet</p> <p>F.A.R.: 72,094 sf x 1.8 = 129,770 sf</p> <p>PROPOSED BUILDING AREA:</p> <p>PHASE 1: GYM 8,800</p> <p>MEZZANINE 1,200</p> <p>CLASSROOM 1,100</p> <p>TOTAL: 11,100</p> <p>PHASE 2: LOWER-LEVEL ADDITION 11,100</p> <p>LOBBY 1,700</p> <p>TOTAL: 12,800</p> <p>EXISTING BUILDING AREA: 23,000 SF</p> <p>TOTAL ADDED AREA: 23,900</p> <p>TOTAL NEW BUILDING: 46,900</p> <p>PARKING</p> <p>REQUIRED PARKING: (SF PLANNING CODE, TABLE 151)</p> <p>19 CLASSROOMS x 0.5 SPACES/CLASSROOM = 10 SPACES</p> <p>EXISTING ON-SITE PARKING SPACES: 65</p> <p>PROPOSED PARKING:</p> <p>OPTION 1:</p> <p>(E) PARKING SPACES TO BE REMOVED: 34</p> <p>TOTAL PARKING SPACES: 31</p> <p>OPTION 2:</p> <p>(E) PARKING SPACES TO BE REMOVED: 37</p> <p>TOTAL PARKING SPACES: 28</p> <p>REQUIRED ACCESSIBLE SPACES (ADA):</p> <p>TOTAL NUMBER OF PARKING SPACES: 26-50</p> <p>MIN. REQUIRED ACCESSIBLE SPACES: 2 (1 CAR + 1 VAN)</p> <p>PROPOSED ACCESSIBLE SPACES: 2</p> <p>BICYCLE PARKING</p> <p>REQUIRED BICYCLE PARKING:</p> <p>PER SECTION 155.2 SF PLANNING CODE BICYCLE PARKING IS CALCULATED BASED ON THE NUMBER OF CLASSROOMS. SF WALDORF HIGH SCHOOL PROPOSES 19 SECONDARY EDUCATION CLASSROOMS.</p> <p>CLASS 1 BICYCLE PARKING SPACES, SECONDARY SCHOOL (TABLE 155.2.20): 19 CLASSROOMS x 4 SPACES/CLASSROOM = 76 SPACES</p> <p>CLASS 2 BICYCLE PARKING SPACES, SECONDARY SCHOOL (TABLE 155.2.20): 19 CLASSROOMS x 1 SPACE/CLASSROOM = 19 SPACES</p> <p>PROPOSED BICYCLE PARKING:</p> <p>OPTION 1:</p> <p>CLASS 1: 41</p> <p>CLASS 2: 19</p> <p>OPTION 2:</p> <p>CLASS 1: 23</p> <p>CLASS 2: 19</p> <p>SHOWERS/LOCKERS</p> <p>REQUIRED SHOWERS/LOCKERS: (SF PLANNING CODE SEC. 155.4)</p> <p>SHOWERS: 2</p> <p>LOCKERS: 12</p> <p>PROPOSED SHOWERS/LOCKERS:</p> <p>SHOWERS: 2</p> <p>LOCKERS: 16</p>	<p>LIST OF DRAWINGS</p> <p>ARCHITECTURAL</p> <p>A0.0 COVER SHEET</p> <p>S0.1 SURVEY</p> <p>A0.1 EXISTING PHOTOS</p> <p>A0.2 EXISTING PHOTOS</p> <p>A0.3 EXISTING PLOT PLAN</p> <p>A1.0 SITE PLAN, EXISTING</p> <p>A1.2 SITE PLAN PHASE I, OPTION 1</p> <p>A1.3 SITE PLAN PHASE I, OPTION 2</p> <p>A1.4 SITE PLAN PHASE II, OPTION 1</p> <p>A1.5 SITE PLAN PHASE II, OPTION 2</p> <p>A2.0 LOWER LEVEL PLAN</p> <p>A2.1 FIRST FLOOR PLAN</p> <p>A2.2 SECOND FLOOR PLAN</p> <p>A2.3 ENLARGED GYM PLAN PHASE I, OPTION 1</p> <p>A2.4 ENLARGED GYM PLAN PHASE II, OPTION 1</p> <p>A2.5 ENLARGED GYM PLAN PHASE II, OPTION 2</p> <p>A3.0 EAST ELEVATION</p> <p>A3.1 WEST ELEVATION</p> <p>A3.2 NORTH ELEVATION</p> <p>A3.3 SOUTH ELEVATION</p> <p>A3.4 EAST & WEST ELEVATIONS PHASE I</p> <p>A3.5 NORTH & SOUTH ELEVATIONS PHASE I</p> <p>A4.0 NORTH & WEST SECTIONS PHASE II</p> <p>A5.1 BIRDS EYE PERSPECTIVE OPTION 1</p> <p>A5.2 STREET VIEW PERSPECTIVE</p> <p>A5.3 COURTYARD VIEW PERSPECTIVE</p> <p>L1.0 PLANTING PLAN</p>
<p>APPLICABLE CODES</p>	<p>PROJECT TEAM</p>	<p>BICYCLE PARKING</p>	<p>VICINITY MAP</p>
<p>California Code of Regulations, Title 24, 2013 edition.</p> <p>2013 CALIFORNIA GREEN BUILDING STANDARDS CODE</p> <p>2013 CALIFORNIA BUILDING CODE, Title 24, Part 2, 2009 International Building Code w/ California amendments</p> <p>2013 CALIFORNIA RESIDENTIAL CODE, Title 24, Part 2.5</p> <p>2013 CALIFORNIA ELECTRICAL CODE, Title 24, Part 3, 2008 National Electrical Code w/ California amendments</p> <p>2013 CALIFORNIA MECHANICAL CODE, Title 24, Part 4, 2009 Uniform Mechanical Code w/ California amendments</p> <p>2013 CALIFORNIA FIRE CODE, Title 24, Part 9, 2009 International Fire Code w/ California amendments</p> <p>2013 CALIFORNIA ENERGY CODE, Title 24, Part 6</p> <p>2013 CALIFORNIA ADMINISTRATIVE CODE, Title 24, Part 1</p> <p>2013 CALIFORNIA REFERENCED STANDARDS CODE, Title 24, Part 12</p> <p>Title 19, C.C.R., Public Safety, Division 1: State Fire Marshal Regulations</p> <p>NFPA 72, National Fire Alarm Code, 2010 Edition (as amended by SFM)</p> <p>Building Code Requirements for Structural Concrete & Commentary, ACI 318-08 & ACI 318R-08.</p> <p>American Institute of Steel Construction, Construction Manual - 13th Addition, AISC 360-05</p>	<p>ARCHITECT</p> <p>450 Architects, Inc. 9 Pier, Suite 105 San Francisco, CA 94111</p> <p>Contact: David Bushnell T: 415-546-0450 E: david@450architects.com</p> <p>CIVIL ENGINEER</p> <p>Coffman Engineers, Inc. 1939 Harrison Street, Ste. 320 Oakland, California 94612</p> <p>Contact: Roy Worthen T: 510-251-9578 E: worthen@coffman.com</p> <p>STRUCTURAL ENGINEER</p> <p>Strandberg Engineering 560 3rd Street San Francisco, CA 94107</p> <p>Contact: David Strandberg T: 415-778-8726 E: david@strandbergeng.com</p> <p>MEP CONSULTANT</p> <p>EDesignC 582 Market St., Ste. 400 San Francisco, CA 94104</p> <p>Contact: Rosanna Lerma T: 415-963-4303 E: rosanna@edesignc.com</p> <p>STREET TREES REQUIRED</p> <p>PROVIDING 11 NEW STREET TREES, 1 PER 20' - 0" PER DPW DIRECTOR'S ORDER 165-95</p>	<p>REQUIRED BICYCLE PARKING:</p> <p>PER SECTION 155.2 SF PLANNING CODE BICYCLE PARKING IS CALCULATED BASED ON THE NUMBER OF CLASSROOMS. SF WALDORF HIGH SCHOOL PROPOSES 19 SECONDARY EDUCATION CLASSROOMS.</p> <p>CLASS 1 BICYCLE PARKING SPACES, SECONDARY SCHOOL (TABLE 155.2.20): 19 CLASSROOMS x 4 SPACES/CLASSROOM = 76 SPACES</p> <p>CLASS 2 BICYCLE PARKING SPACES, SECONDARY SCHOOL (TABLE 155.2.20): 19 CLASSROOMS x 1 SPACE/CLASSROOM = 19 SPACES</p> <p>PROPOSED BICYCLE PARKING:</p> <p>OPTION 1:</p> <p>CLASS 1: 41</p> <p>CLASS 2: 19</p> <p>OPTION 2:</p> <p>CLASS 1: 23</p> <p>CLASS 2: 19</p> <p>SHOWERS/LOCKERS</p> <p>REQUIRED SHOWERS/LOCKERS: (SF PLANNING CODE SEC. 155.4)</p> <p>SHOWERS: 2</p> <p>LOCKERS: 12</p> <p>PROPOSED SHOWERS/LOCKERS:</p> <p>SHOWERS: 2</p> <p>LOCKERS: 16</p>	<p>VICINITY MAP</p>



COVER SHEET

A0.0

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EXISTING PARKING LOT



BELOW EXISTING BUILDING - PROPOSED FUTURE PERFORMANCE SPACE



BELOW EXISTING BUILDING - PROPOSED LOWER LEVEL FUTURE CLASSROOM SPACE



VIEW FROM WEST PORTAL AVENUE LOOKING SOUTH



EXISTING PHOTOS

A0.1

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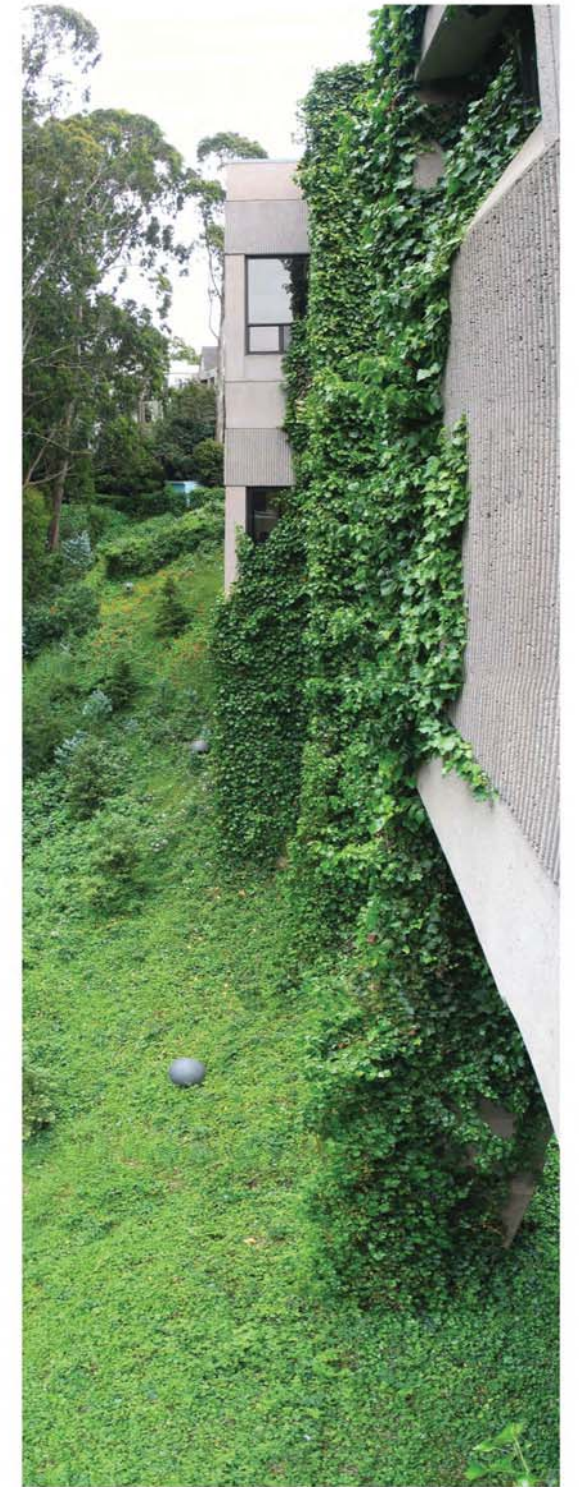
EXISTING BUILDING ENTRY



CLASSROOM



CLASSROOM, WOODEN WINDOWS TO EUCALYPTUS GROVE



WEST SIDE OF EXISTING BUILDING



EXISTING SITE FROM WEST PORTAL



EXISTING PHOTOS

A0.2

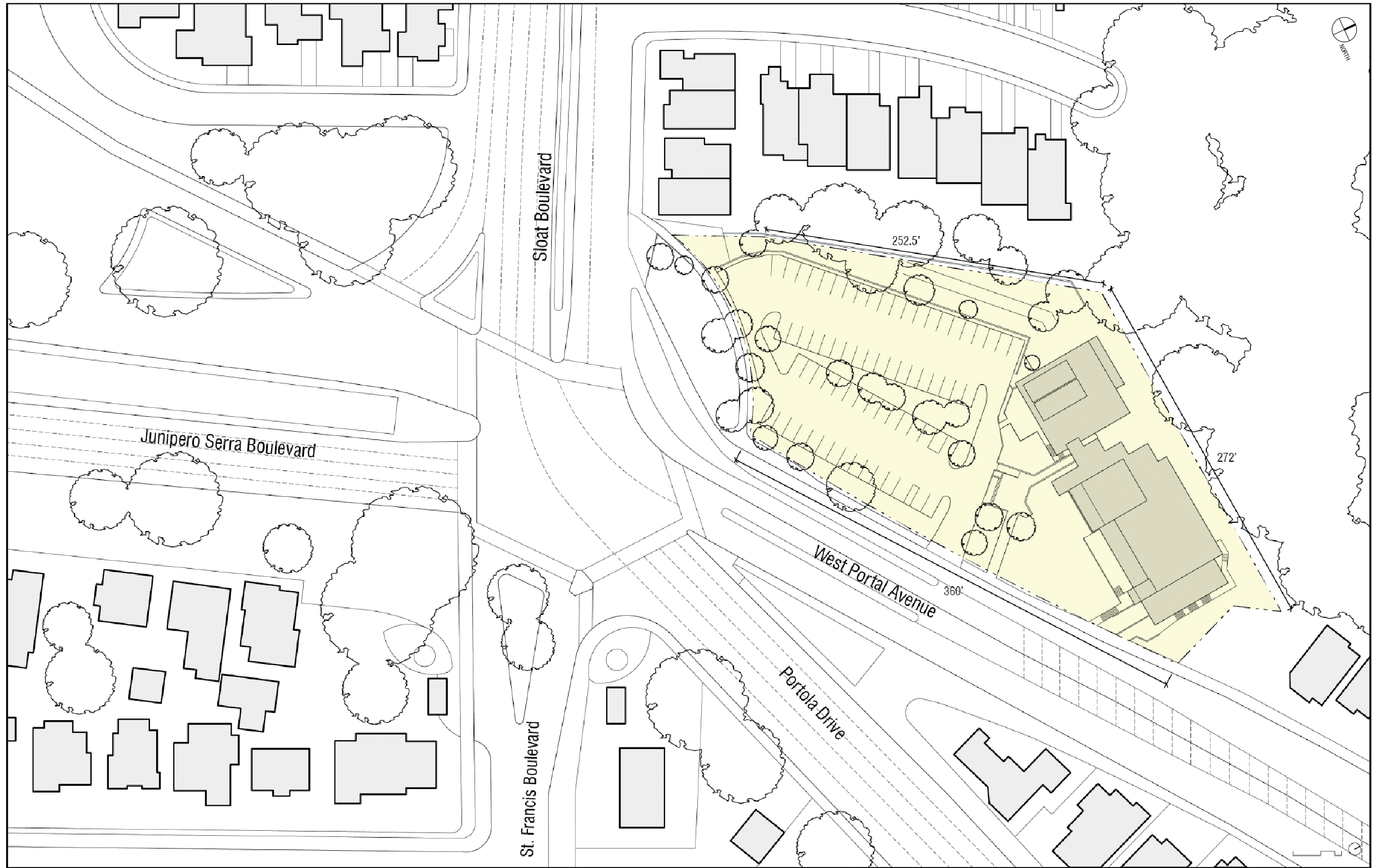
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EXISTING PLOT PLAN

A0.3

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LEGEND

- ① MUNI
- ② LANDSCAPING AREA
- ③ (E) PARKING
- ④ CONC. PATIO
- ⑤ BRICK PAVING AREA
- ⑥ (E) AT&T EASEMENT
- ⑦ (E) AT&T VAULT ACCESS HATCH
- ⑧ BRICK PAVER WALKWAY
- ⑨ DRIVEWAY ENTRANCE
- ⑩ FENCING

SITE PLAN, EXISTING

A1.0

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LEGEND

- ① (N) GYMNASIUM
- ② (E) CONCRETE RETAINING WALL
- ③ (N) SLIDING GATE
- ④ (E) PARKING TO REMAIN
- ⑤ (N) PAVING AREA
- ⑥ (E) AT&T EASEMENT
- ⑦ (E) AT&T VAULT ACCESS HATCH
- ⑧ (E) AREA DRAIN
- ⑨ (N) CURB & WALKWAY AT (E) DRIVEWAY
- ⑩ (N) CURB CUT DRIVEWAY PER DPW REQ.
- ⑪ (E) BRICK PATIO TO REMAIN
- ⑫ (E) BRICK PAVER WALKWAY TO REMAIN
- ⑬ (E) LANDSCAPING AREA TO REMAIN
- ⑭ (N) WHEEL STOP
- ⑮ (N) PARKING SPACES / STRIPING
- ⑯ (E) LIGHTING TO REMAIN
- ⑰ (E) BUS SHELTER
- ⑱ (E) MUNI PLATFORM
- ⑲ (N) SITE STORAGE
- ⑳ (N) ADA PARKING SIGN
- ㉑ (N) FIRE SPRINKLER BACK FLOW PREVENTER
- ㉒ (N) LANDSCAPING AREA
- ㉓ (E) TRANSFORMER VAULT BELOW TO REMAIN SEE ELECTRICAL DWGS.
- ㉔ (E) UNDERGROUND CONDUIT TO REMAIN
- ㉕ (E) UNDERGROUND CONDUIT TO BE REMOVED
- ㉖ (E) UNDERGROUND CONDUIT TO ABANDONED
- ㉗ (E) CONCRETE STAIR TO REMAIN
- ㉘ (E) TRENCHING FOR GAS PIPE SEE CIVIL DWGS.
- ㉙ (E) TRANSFORMER / SWITCH GEAR
- ㉚ (N) VAN PARKING
- ㉛ (N) RETAINING WALL
- ㉜ (E) DASHED LINE OF (E) CONCRETE TO BE REMOVED
- ㉝ (E) CONC. BRIDGE TO REMAIN
- ㉞ (E) BRICK PAVERS TO REMAIN
- ㉟ (E) STUB OUT FOR KITCHEN
- ㊱ (E) LANDSCAPING TO BE IMPROVED, SEE PLANTING PLAN

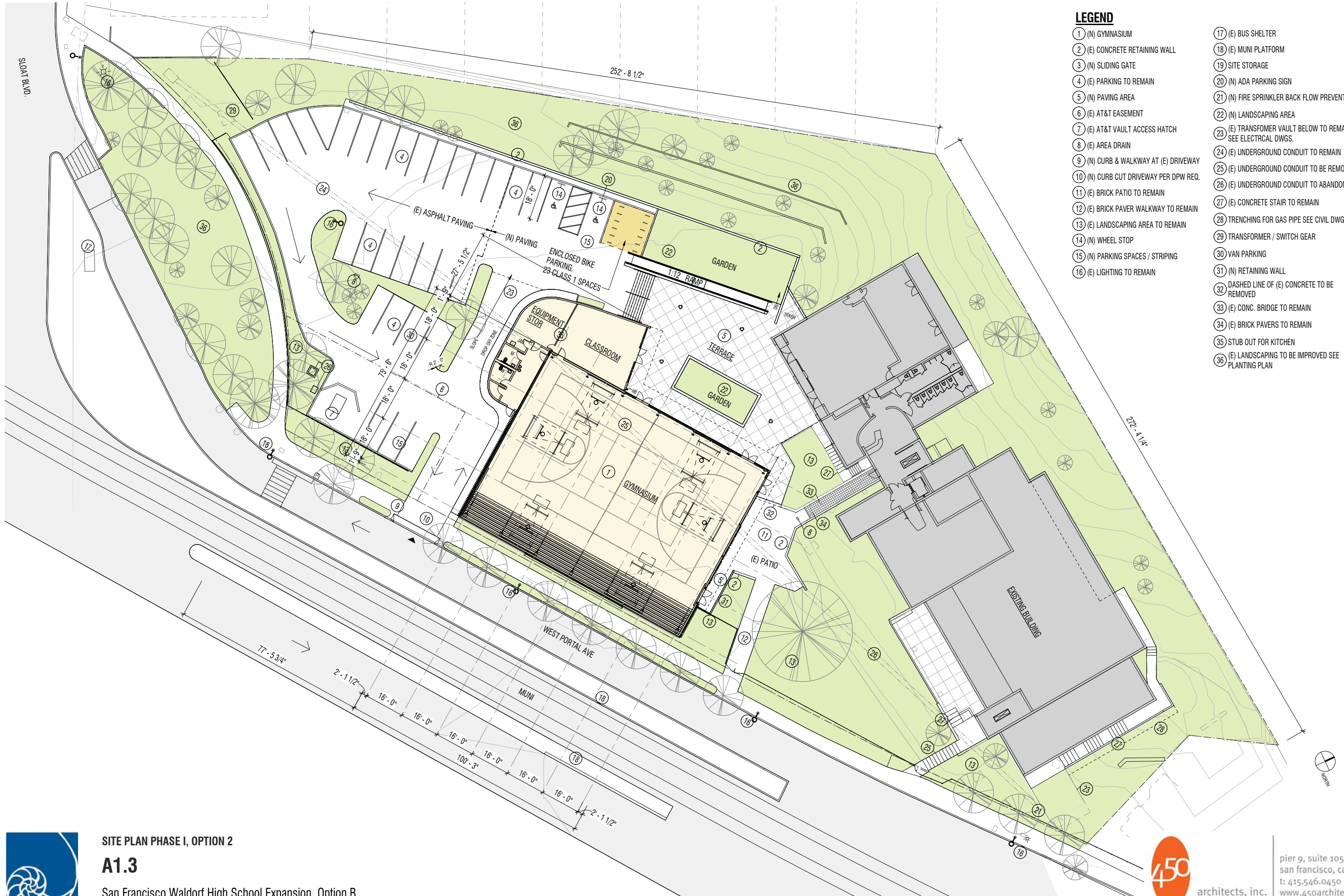
SITE PLAN PHASE I, OPTION 1

A1.2

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LEGEND

- ① (N) GYMNASIUM
- ② (E) CONCRETE RETAINING WALL
- ③ (N) SLIDING GATE
- ④ (E) PARKING TO REMAIN
- ⑤ (N) PAVING AREA
- ⑥ (E) AT&T EASEMENT
- ⑦ (E) AT&T VAULT ACCESS HATCH
- ⑧ (E) AREA DRAIN
- ⑨ (N) CURB & WALKWAY AT (E) DRIVEWAY
- ⑩ (N) CURB CUT DRIVEWAY PER DPW REQ.
- ⑪ (E) BRICK PATIO TO REMAIN
- ⑫ (E) BRICK PAVER WALKWAY TO REMAIN
- ⑬ (E) LANDSCAPING AREA TO REMAIN
- ⑭ (N) WHEEL STOP
- ⑮ (N) PARKING SPACES / STRIPING
- ⑯ (E) LIGHTING TO REMAIN
- ⑰ (E) BUS SHELTER
- ⑱ (E) MUNI PLATFORM
- ⑲ SITE STORAGE
- ⑳ (N) ADA PARKING SIGN
- ㉑ (N) FIRE SPRINKLER BACK FLOW PREVENTER
- ㉒ (N) LANDSCAPING AREA
- ㉓ (E) TRANSFORMER VAULT BELOW TO REMAIN SEE ELECTRICAL DWGS.
- ㉔ (E) UNDERGROUND CONDUIT TO REMAIN
- ㉕ (E) UNDERGROUND CONDUIT TO BE REMOVED
- ㉖ (E) UNDERGROUND CONDUIT TO ABANDONED
- ㉗ (E) CONCRETE STAIR TO REMAIN
- ㉘ TRENCHING FOR GAS PIPE SEE CIVIL DWGS.
- ㉙ TRANSFORMER / SWITCH GEAR
- ㉚ VAN PARKING
- ㉛ (N) RETAINING WALL
- ㉜ DASHED LINE OF (E) CONCRETE TO BE REMOVED
- ㉝ (E) CONC. BRIDGE TO REMAIN
- ㉞ (E) BRICK PAVERS TO REMAIN
- ㉟ STUB OUT FOR KITCHEN
- ㊱ (E) LANDSCAPING TO BE IMPROVED SEE PLANTING PLAN

SITE PLAN PHASE I, OPTION 2

A1.3

San Francisco Waldorf High School Expansion, Option B

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- LEGEND**
- ① (N) GYMNASIUM
 - ② (E) CONCRETE RETAINING WALL
 - ③ (N) TREES
 - ④ (E) PARKING TO REMAIN
 - ⑤ (N) PERMEABLE PAVING AREA
 - ⑥ (E) AT&T EASEMENT
 - ⑦ (E) AT&T VAULT ACCESS HATCH
 - ⑧ TRANSFORMER / SWITCH GEAR
 - ⑨ (N) CURB & WALKWAY AT (E) DRIVEWAY
 - ⑩ (N) CURB CUT DRIVEWAY PER DPW REQ.
 - ⑪ (N) FIRE SPRINKLER BACKFLOW PREVENTER
 - ⑫ (N) LANDSCAPING AREA
 - ⑬ (E) LANDSCAPING AREA TO REMAIN
 - ⑭ (N) STAIR
 - ⑮ (N) PERMEABLE ACCESSIBLE ENTRYWAY
 - ⑯ (E) LIGHTING TO REMAIN
 - ⑰ (E) BUS SHELTER
 - ⑱ (E) MUNI PLATFORM
 - ⑲ SITE STORAGE & BIKE PARKING
 - ⑳ ADA PARKING SIGN
 - ㉑ (E) SIDEWALK TO REMAIN
 - ㉒ SHADED AREA OF (E) BUILDING TO REMAIN NO WORK
 - ㉓ WHEEL STOP
 - ㉔ (E) UNDERGROUND CONDUIT TO REMAIN
 - ㉕ (E) UNDERGROUND CONDUIT TO BE REMOVED BENEATH (N) BUILDING
 - ㉖ (E) UNDERGROUND CONDUIT TO BE ABANDONED
 - ㉗ (E) LANDSCAPING TO BE IMPROVED SEE PLANTING PLAN
 - ㉘ (N) FENCE

SITE PLAN PHASE II, OPTION 1
A1.4
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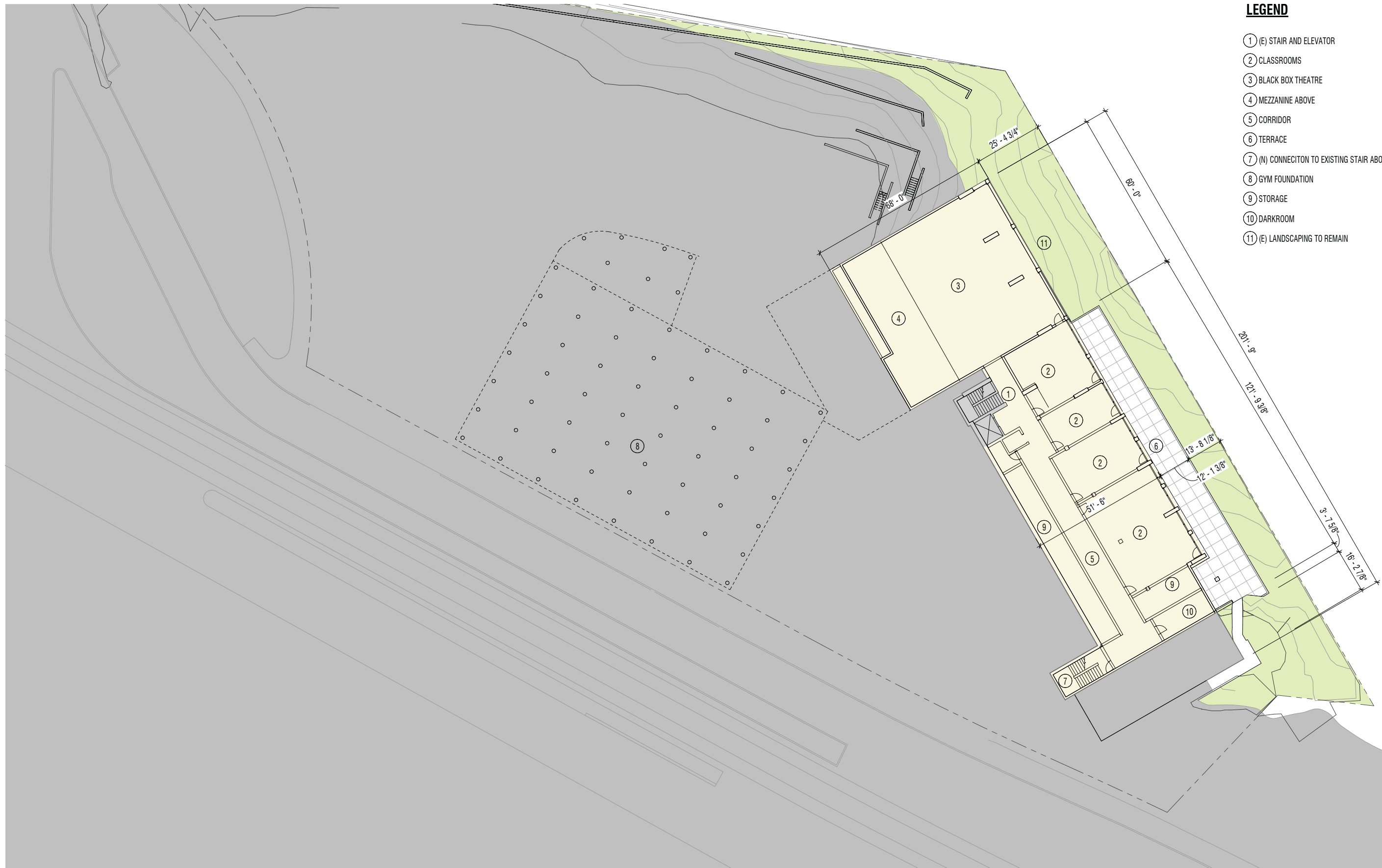
- LEGEND**
- ① (N) GYMNASIUM
 - ② (E) CONCRETE RETAINING WALL
 - ③ NOT USED
 - ④ (E) PARKING TO REMAIN
 - ⑤ (N) PERMEABLE PAVING AREA
 - ⑥ (E) AT&T EASEMENT
 - ⑦ (E) AT&T VAULT ACCESS HATCH
 - ⑧ TRANSFORMER / SWITCH GEAR
 - ⑨ (N) CURB & WALKWAY AT (E) DRIVEWAY
 - ⑩ (N) CURB CUT DRIVEWAY PER DPW REQ.
 - ⑪ (N) FIRE SPRINKLER BACKFLOW PREVENTER
 - ⑫ (N) LANDSCAPING AREA
 - ⑬ (E) LANDSCAPING AREA TO REMAIN
 - ⑭ (N) STAIR
 - ⑮ (N) PERMEABLE ACCESSIBLE ENTRYWAY
 - ⑯ (E) LIGHTING TO REMAIN
 - ⑰ (E) BUS SHELTER
 - ⑱ (E) MUNI PLATFORM
 - ⑲ NOT USED
 - ⑳ ADA PARKING SIGN
 - ㉑ (E) SIDEWALK TO REMAIN
 - ㉒ SHADED AREA OF (E) BUILDING TO REMAIN NO WORK
 - ㉓ WHEEL STOP
 - ㉔ (E) UNDERGROUND CONDUIT TO REMAIN
 - ㉕ (E) UNDERGROUND CONDUIT TO BE REMOVED BENEATH (N) BUILDING
 - ㉖ (E) UNDERGROUND CONDUIT TO BE ABANDONED
 - ㉗ (N) SIDEWALK



SITE PLAN PHASE II, OPTION 2
A1.5
 San Francisco Waldorf High School Expansion, Option B
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LEGEND

- ① (E) STAIR AND ELEVATOR
- ② CLASSROOMS
- ③ BLACK BOX THEATRE
- ④ MEZZANINE ABOVE
- ⑤ CORRIDOR
- ⑥ TERRACE
- ⑦ (N) CONNECITON TO EXISTING STAIR ABOVE
- ⑧ GYM FOUNDATION
- ⑨ STORAGE
- ⑩ DARKROOM
- ⑪ (E) LANDSCAPING TO REMAIN



LOWER LEVEL PLAN

A2.0

San Francisco Waldorf High School Expansion

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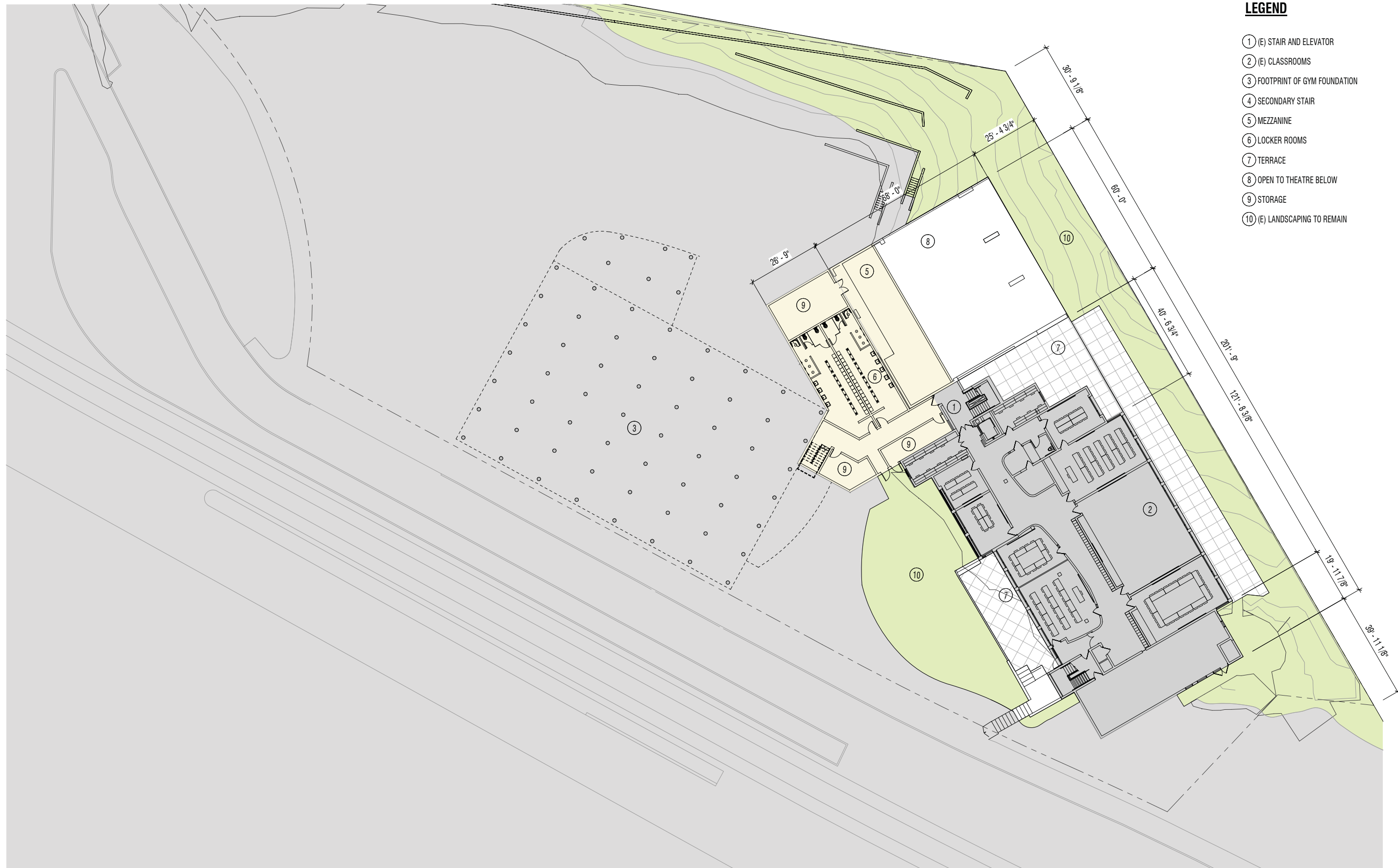


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LEGEND

- ① (E) STAIR AND ELEVATOR
- ② (E) CLASSROOMS
- ③ FOOTPRINT OF GYM FOUNDATION
- ④ SECONDARY STAIR
- ⑤ MEZZANINE
- ⑥ LOCKER ROOMS
- ⑦ TERRACE
- ⑧ OPEN TO THEATRE BELOW
- ⑨ STORAGE
- ⑩ (E) LANDSCAPING TO REMAIN



FIRST FLOOR PLAN

A2.1

San Francisco Waldorf High School Expansion

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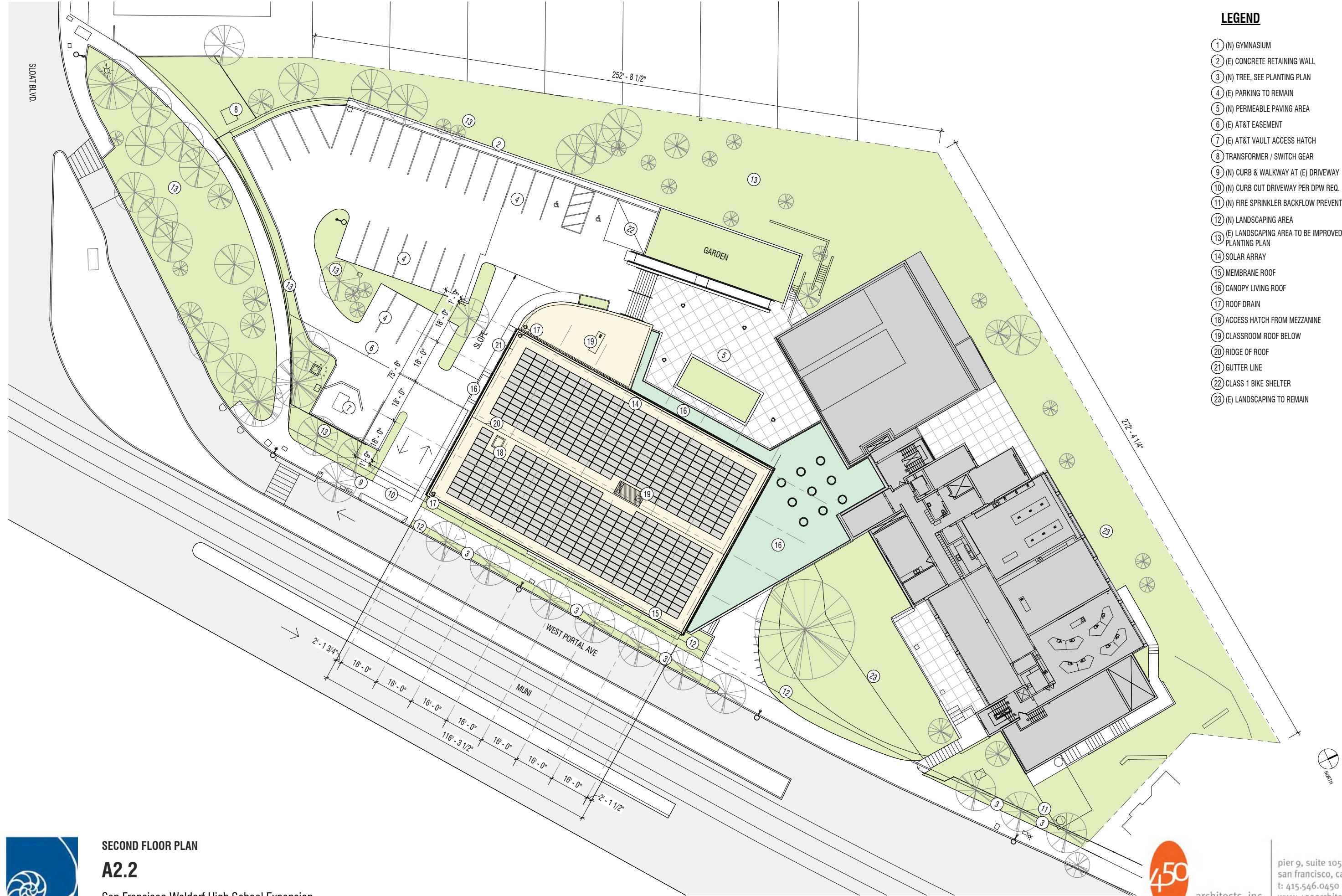


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LEGEND

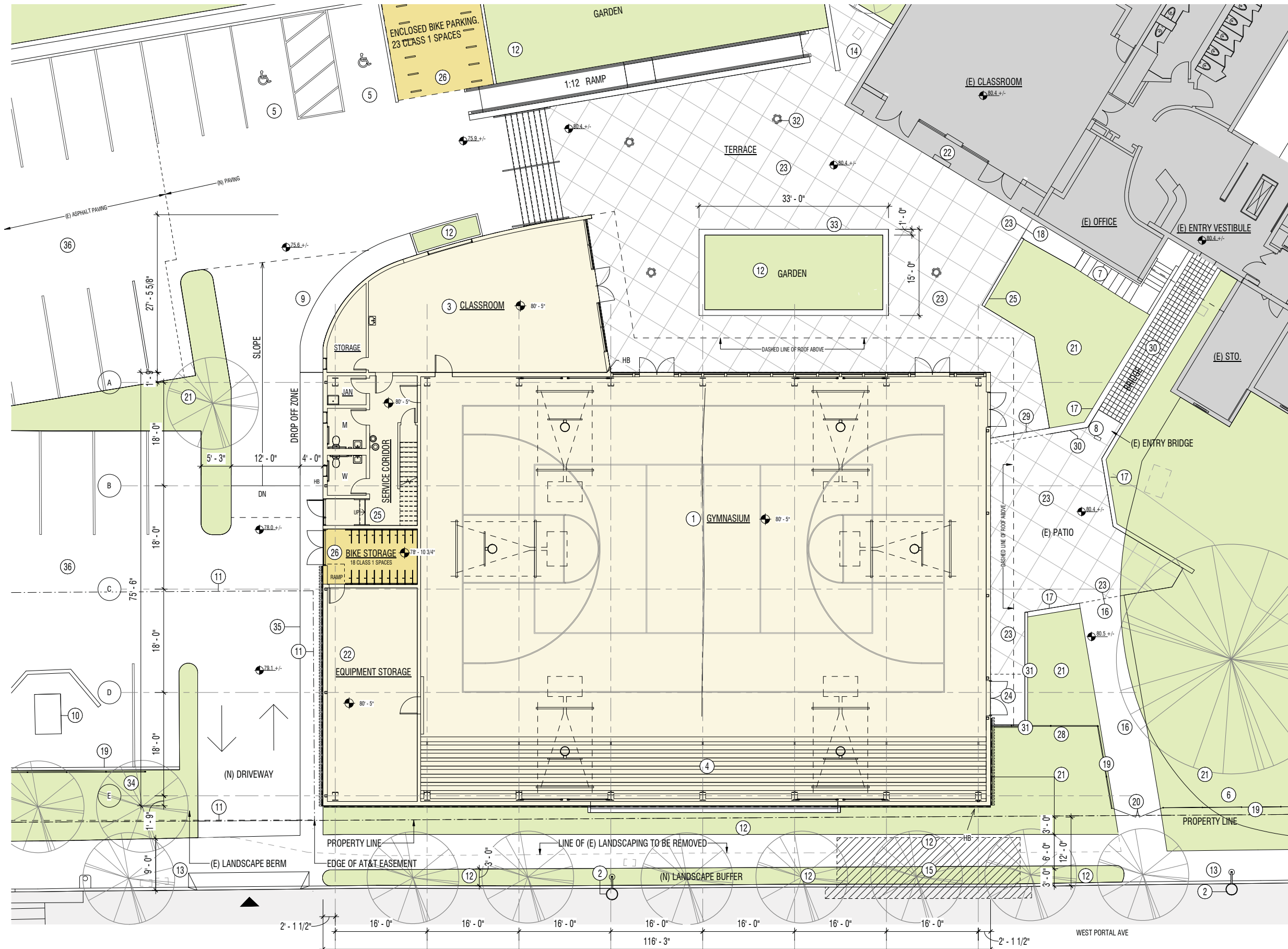
- ① (N) GYMNASIUM
- ② (E) CONCRETE RETAINING WALL
- ③ (N) TREE, SEE PLANTING PLAN
- ④ (E) PARKING TO REMAIN
- ⑤ (N) PERMEABLE PAVING AREA
- ⑥ (E) AT&T EASEMENT
- ⑦ (E) AT&T VAULT ACCESS HATCH
- ⑧ TRANSFORMER / SWITCH GEAR
- ⑨ (N) CURB & WALKWAY AT (E) DRIVEWAY
- ⑩ (N) CURB CUT DRIVEWAY PER DPW REQ.
- ⑪ (N) FIRE SPRINKLER BACKFLOW PREVENTER
- ⑫ (N) LANDSCAPING AREA
- ⑬ (E) LANDSCAPING AREA TO BE IMPROVED SEE PLANTING PLAN
- ⑭ SOLAR ARRAY
- ⑮ MEMBRANE ROOF
- ⑯ CANOPY LIVING ROOF
- ⑰ ROOF DRAIN
- ⑱ ACCESS HATCH FROM MEZZANINE
- ⑲ CLASSROOM ROOF BELOW
- ⑳ RIDGE OF ROOF
- ㉑ GUTTER LINE
- ㉒ CLASS 1 BIKE SHELTER
- ㉓ (E) LANDSCAPING TO REMAIN



SECOND FLOOR PLAN
A2.2
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LEGEND

- ① SPRUNG HARDWOOD FLOOR
- ② (E) LIGHTING TO REMAIN
- ③ STUB OUT FOR PHASE II KITCHEN
- ④ TELESCOPE SEATING
- ⑤ (N) ACCESSIBLE PARKING SPACES
- ⑥ FIRE HYDRANT, SEE CIVIL DWGS
- ⑦ (E) CONC. STAIR TO REMAIN
- ⑧ (E) AREA DRAIN
- ⑨ (N) CURB TO FOLLOW SLOPE
- ⑩ (E) AT&T VAULT ACCESS HATCH
- ⑪ (E) AT&T EASEMENT (DASHED)
- ⑫ (N) LANDSCAPING
- ⑬ (E) SIDEWALK TO REMAIN
- ⑭ (E) TRASH AREA
- ⑮ (N) CURB & SIDEWALK AT (E) DRIVEWAY
- ⑯ (E) BRICK PAVER WALKWAY TO REMAIN
- ⑰ (E) CONC. GUARDRAIL TO REMAIN
- ⑱ (E) PAVING TO REMAIN
- ⑲ (E) CHAINLINK FENCE TO REMAIN
- ⑳ (E) GATE TO REMAIN
- ㉑ (E) LANDSCAPING TO BE REMAIN
- ㉒ EQUIPMENT STORAGE
- ㉓ (N) CONC. PAVERS
- ㉔ (N) CONC. PAVING
- ㉕ (E) CONC. CURB TO REMAIN
- ㉖ BIKE STORE, CLASS 1 TYPE SPACES
- ㉗ NOT USED
- ㉘ (N) CHAINLINK FENCE TO MATCH (E) ADJ.
- ㉙ LINE OF (E) CONC. GUARDRAIL TO REMAIN
- ㉚ (N) PLASTER FINISH & SIGNAGE AT (E) CONC. BALUSTRADE
- ㉛ (N) CONC. RETAINING WALL
- ㉜ (N) AREA DRAIN
- ㉝ 18" HIGH CONC. PLANTER WALL
- ㉞ (E) LANDSCAPING TO BE IMPROVED SEE PLANTING PLAN
- ㉟ (N) SIDEWALK
- ㊱ (E) PARKING



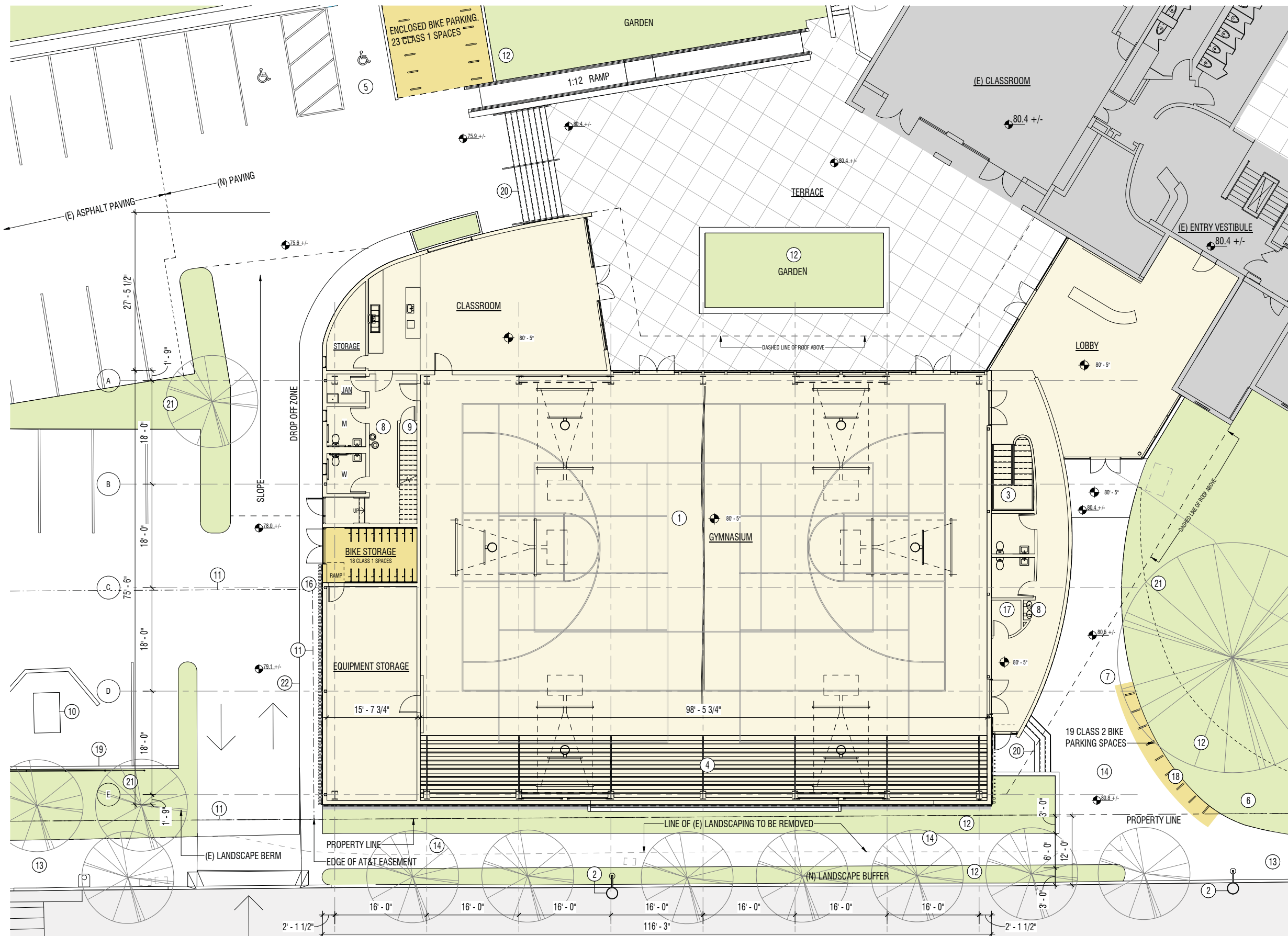
ENLARGED GYM PLAN PHASE I, OPTION 1

A2.3

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LEGEND

- ① SPRUNG HARDWOOD FLOOR
- ② (E) LIGHTING TO REMAIN
- ③ STAIR TO LOCKERROOM BELOW
- ④ TELESCOPE SEATING
- ⑤ (N) ACCESSIBLE PARKING SPACES
- ⑥ FIRE HYDRANT, SEE CIVIL DWGS
- ⑦ (N) SLOPED ENTRANCE WALKWAY
- ⑧ DRINKING FOUNTAINS
- ⑨ STAIR TO UPPER MEZZANINE
- ⑩ (E) AT&T VAULT ACCESS HATCH
- ⑪ (E) AT&T EASEMENT (DASHED)
- ⑫ (N) LANDSCAPING
- ⑬ (E) SIDEWALK TO REMAIN
- ⑭ (N) TREE
- ⑮ (N) CURB & SIDEWALK AT (E) DRIVEWAY
- ⑯ WATER CISTERN TANKS
- ⑰ STORE ROOM
- ⑱ (N) CLASS 2 BIKE PARKING SPACES
- ⑲ (E) CHAINLINK FENCE TO REMAIN
- ⑳ (N) STAIR
- ㉑ (E) LANDSCAPING AREA TO REMAIN
- ㉒ (N) SIDEWALK

ENLARGED GYM PLAN PHASE II, OPTION 1

A2.4

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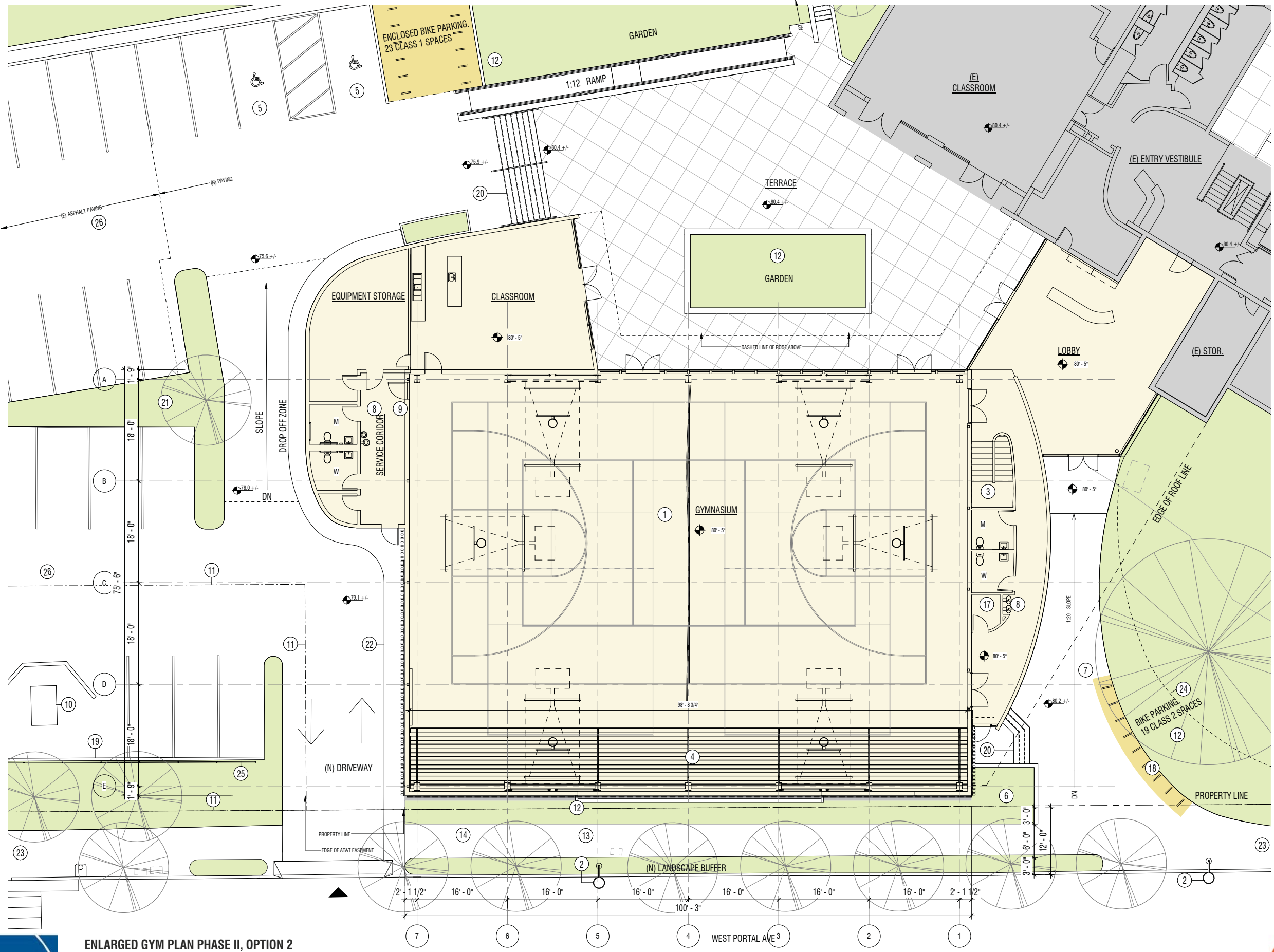
OPTION 1 - ENLARGED EQUIPMENT STORAGE WITH INTERNAL CLASS 1 BIKE PARKING.



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- LEGEND**
- ① SPRUNG HARDWOOD FLOOR
 - ② (E) LIGHTING TO REMAIN
 - ③ STAIR TO LOCKE ROOM BELOW
 - ④ TELESCOPE SEATING
 - ⑤ (N) ACCESSIBLE PARKING SPACES
 - ⑥ FIRE HYDRANT, SEE CIVIL DWGS
 - ⑦ (N) SLOPED ENTRANCE WALKWAY
 - ⑧ DRINKING FOUNTAINS
 - ⑨ STAIR TO UPPER MEZZANINE
 - ⑩ (E) AT&T VAULT ACCESS HATCH
 - ⑪ (E) AT&T EASEMENT (HATCHED)
 - ⑫ (N) LANDSCAPING
 - ⑬ (N) SIDEWALK
 - ⑭ (N) TREE
 - ⑮ (N) CURB & SIDEWALK AT (E) DRIVEWAY
 - ⑯ WATER CISTERN TANKS
 - ⑰ STORE ROOM
 - ⑱ (N) CLASS 2 BIKE PARKING SPACES
 - ⑲ (E) CHAINLINK FENCE TO REMAIN
 - ⑳ (N) STAIR
 - ㉑ (E) LANDSCAPING AREA TO REMAIN
 - ㉒ (N) SIDEWALK
 - ㉓ (E) SIDEWALK TO REMAIN
 - ㉔ (N) FENCE
 - ㉕ (E) FENCE TO REMAIN
 - ㉖ (E) PARKING

ENLARGED GYM PLAN PHASE II, OPTION 2

A2.5

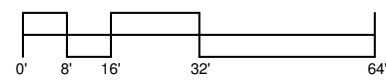
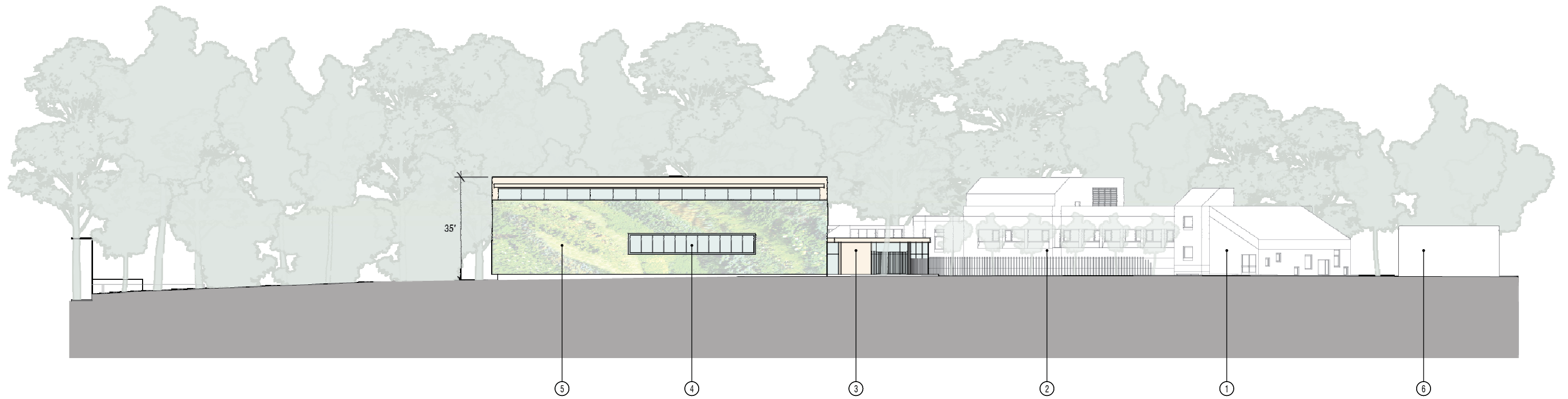
San Francisco Waldorf High School Expansion, Option B
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LEGEND

- ① (E) SCHOOL BUILDING
- ② ORCHARD
- ③ ENTRY LOBBY
- ④ STREET WINDOW
- ⑤ LIVING WALL
- ⑥ RESIDENCE 260 WEST PORTAL AVE



EAST ELEVATION

A3.0

San Francisco Waldorf High School Expansion

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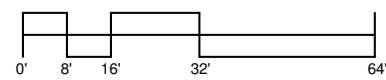
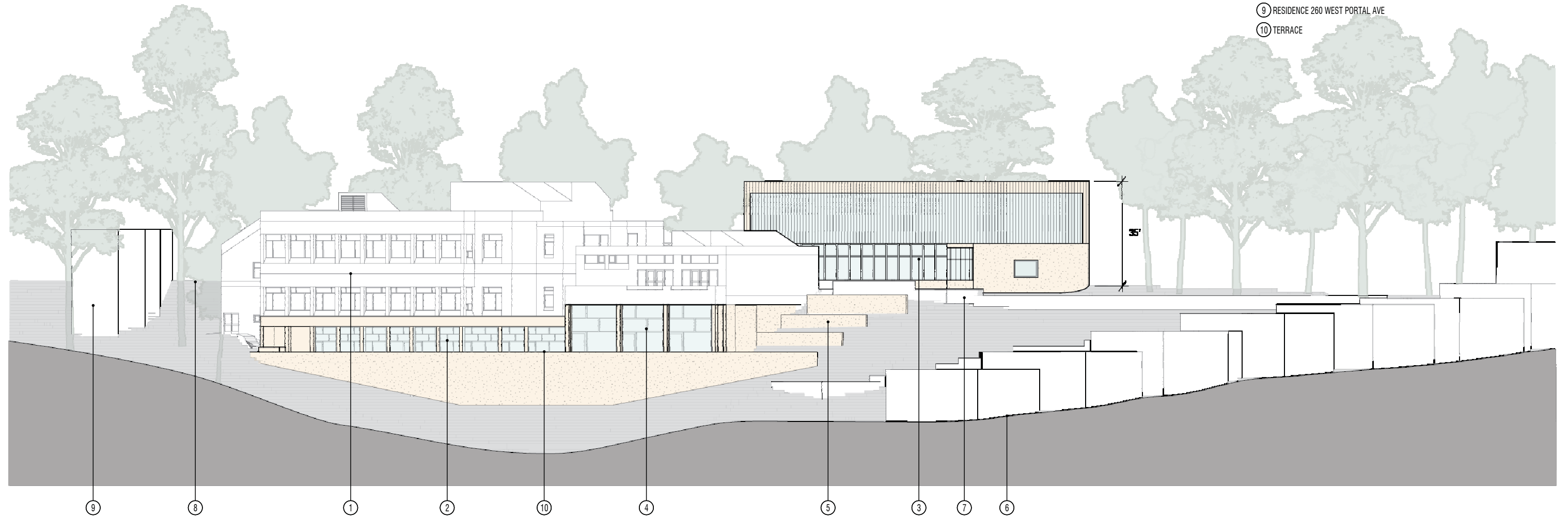


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LEGEND

- ① (E) SCHOOL BUILDING
- ② CLASSROOM ADDITION
- ③ GYM
- ④ PERFORMANCE SPACE
- ⑤ TERRACED GARDENS
- ⑥ ARDENWOOD WAY
- ⑦ EXISTING RETAINING WALL AT PARKING
- ⑧ WEST PORTAL AVE, BEYOND
- ⑨ RESIDENCE 260 WEST PORTAL AVE
- ⑩ TERRACE



WEST ELEVATION

A3.1

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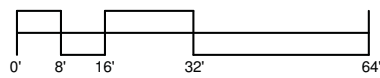
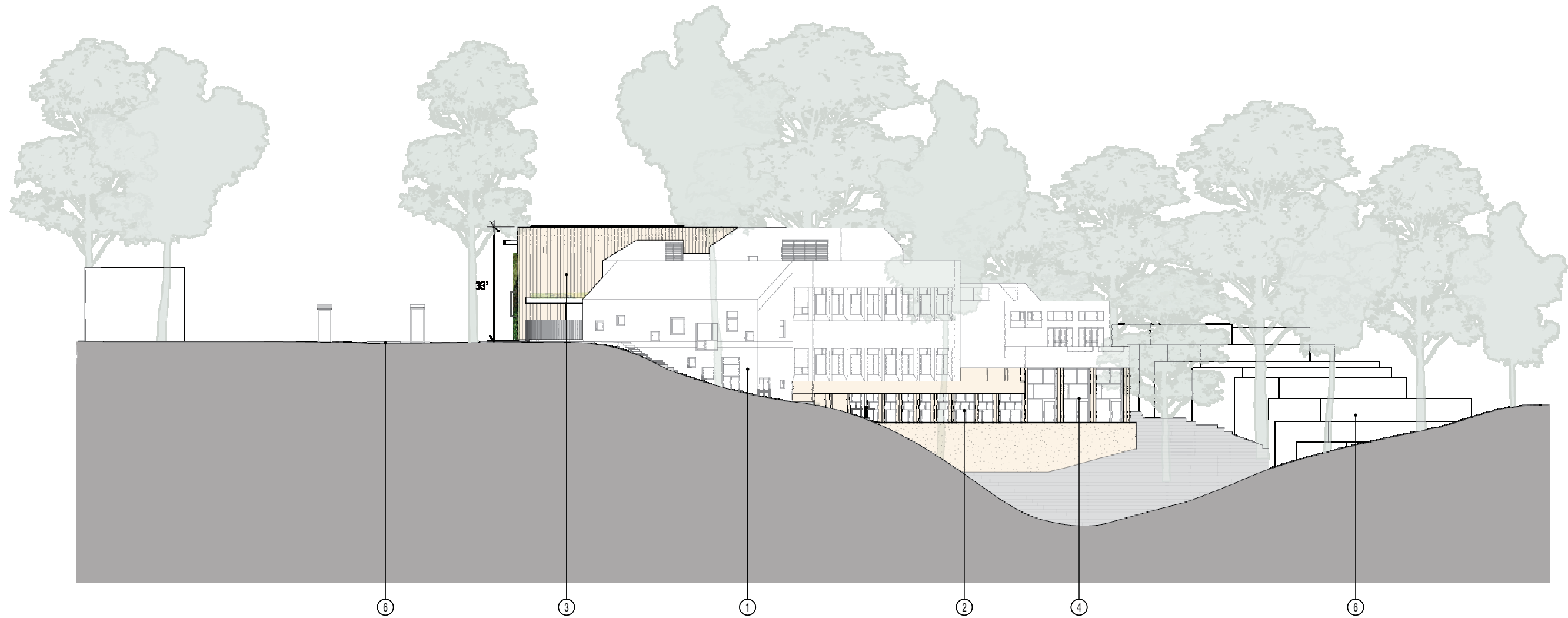


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LEGEND

- ① (E) SCHOOL BUILDING
- ② CLASSROOM ADDITION
- ③ GYM
- ④ PERFORMANCE SPACE
- ⑤ ARDENWOOD WAY RESIDENCES
- ⑥ MUNI PLATFORM



NORTH ELEVATION

A3.2

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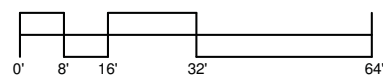


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LEGEND

- ① (E) SCHOOL BUILDING
- ② CLASSROOM ADDITION
- ③ GYM
- ④ ARDENWOOD WAY RESIDENCES
- ⑤ MUNI PLATFORM
- ⑥ PARKING
- ⑦ LANDSCAPE AREA
- ⑧ PROPERTY LINE FENCE



SOUTH ELEVATION

A3.3

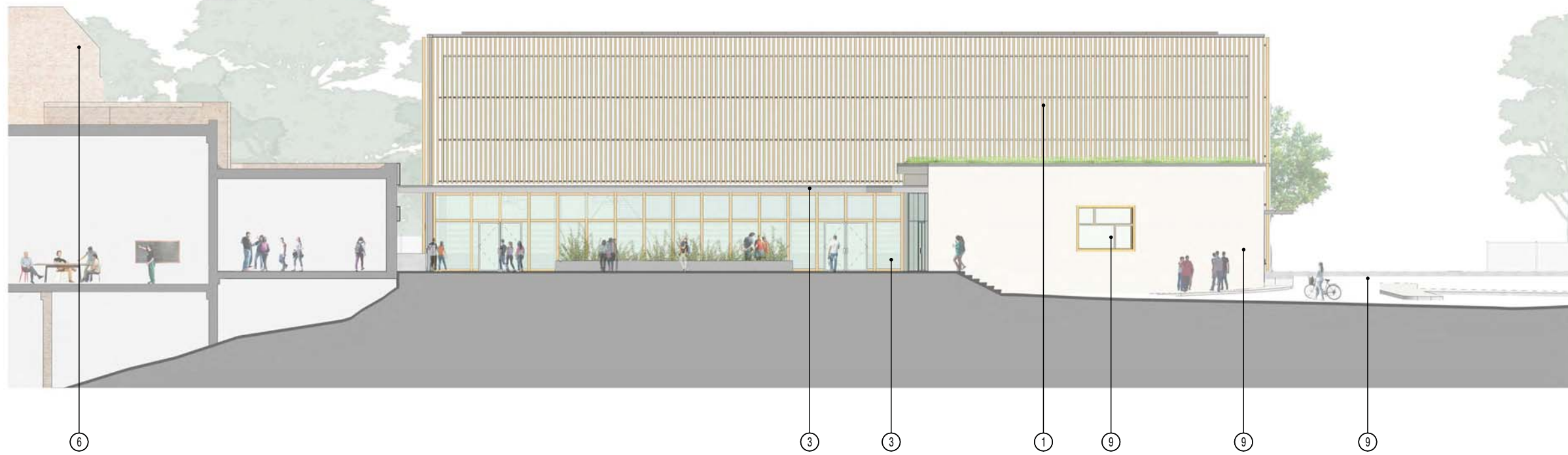
San Francisco Waldorf High School Expansion

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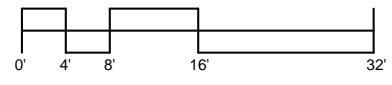


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- LEGEND**
- ① LIVING WALL
 - ② WOODEN BATTEN SCREEN OVER ALUMINUM FRAMED WINDOW WALL
 - ③ CONCRETE BASE
 - ④ WOOD BAY WINDOW
 - ⑤ NEW CLASSROOM PAINTED STUCCO
 - ⑥ CANOPY WITH METAL FACIA
 - ⑦ CLERESTORY WINDOW ALUMINIUM FRAMED
 - ⑧ METAL SUN SHADE
 - ⑨ NEW DRIVEWAY
 - ⑩ EXISTING POURED IN PLACE FLUTED CONCRETE



EAST & WEST ELEVATIONS PHASE I

A3.4

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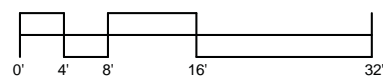


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LEGEND

- ① WOODEN BATTENS SCREEN OVER STEEL SHELL
- ② CONCRETE BASE
- ③ WOOD STOREFRONT TO MATCH EXISTING BUILDING
- ④ PAINTED STUCCO
- ⑤ METAL FACIA
- ⑥ EXISTING POURED IN PLACE, CONCRETE FLUTED
- ⑦ EXISTING CLASSROOM
- ⑧ ENCLOSED CLASS 1, BIKE PARKING AREA
- ⑨ MUNI PLATFORM
- ⑩ EXISTING OPEN SPACE FOR FUTURE CLASSROOMS + PERFORMANCE SPACE
- ⑪ NEW CLASSROOM PAINTED STUCCO
- ⑫ PHASE II PERFORMING ARTS SPACE EXPANSION



NORTH & SOUTH ELEVATIONS PHASE I

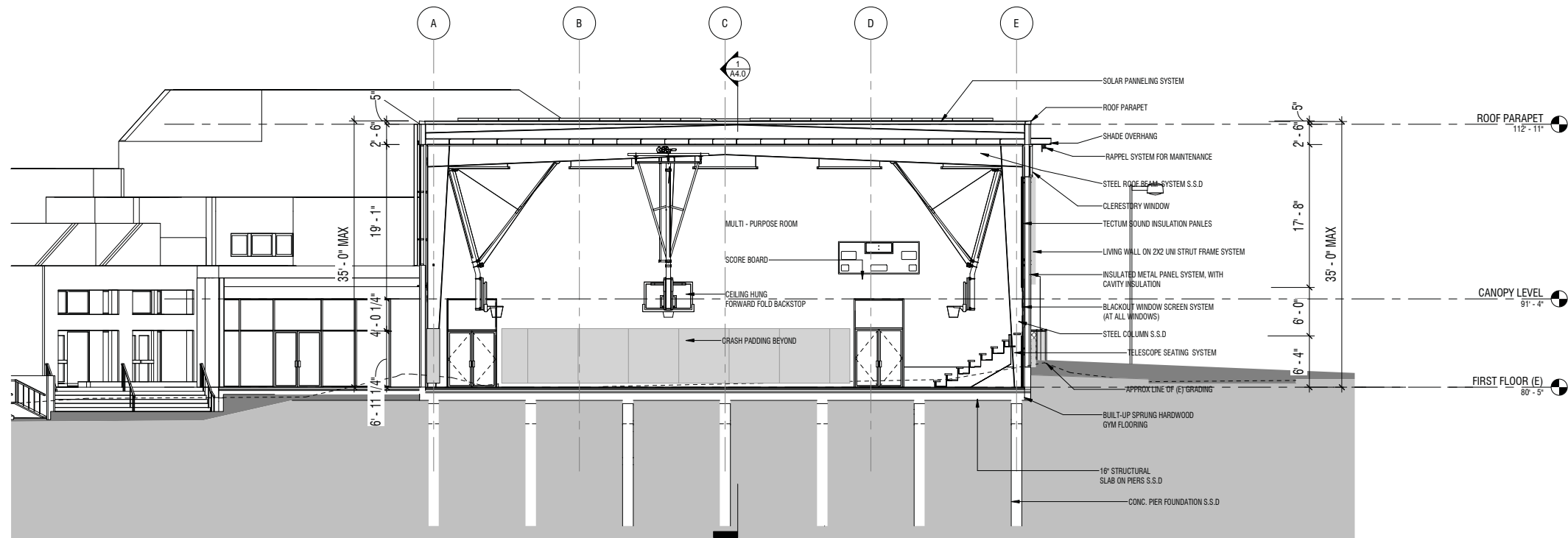
A3.5

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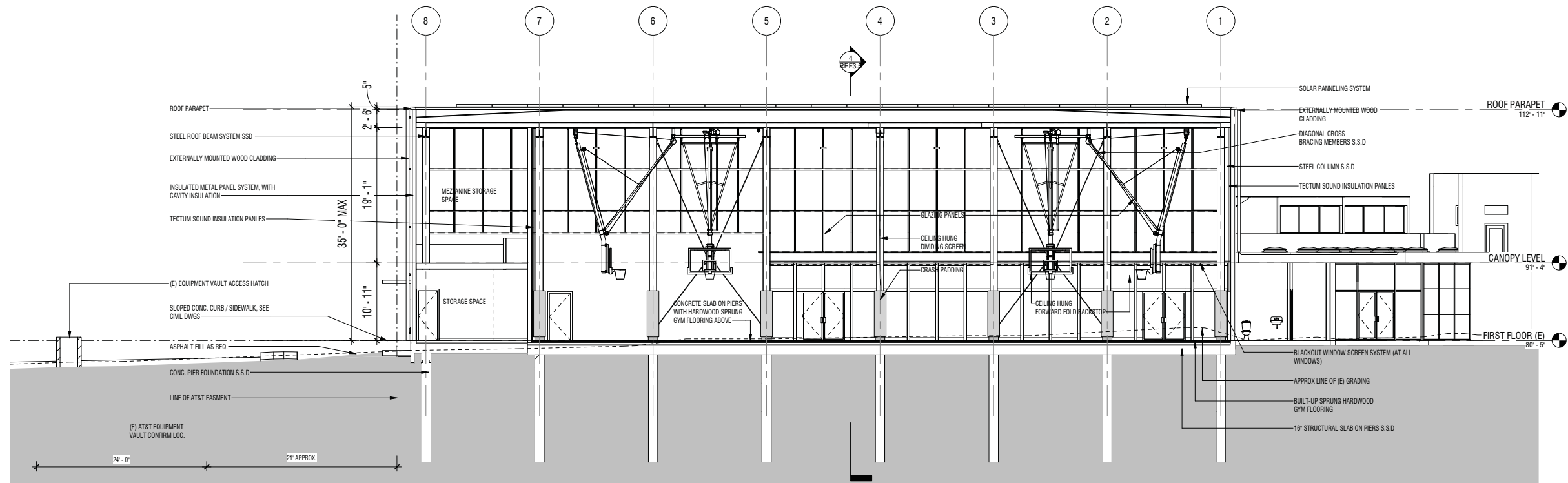


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2 NORTH TRANSVERSE SECTION PHASE II
1/8" = 1'-0"



1 WEST LONGITUDINAL SECTION PHASE II
1/8" = 1'-0"



NORTH & WEST SECTIONS PHASE II

A4.0

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BIRDS EYE PERSPECTIVE OPTION 1

A5.1

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STREET VIEW PERSPECTIVE

A5.2

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COURTYARD VIEW PERSPECTIVE

A5.3

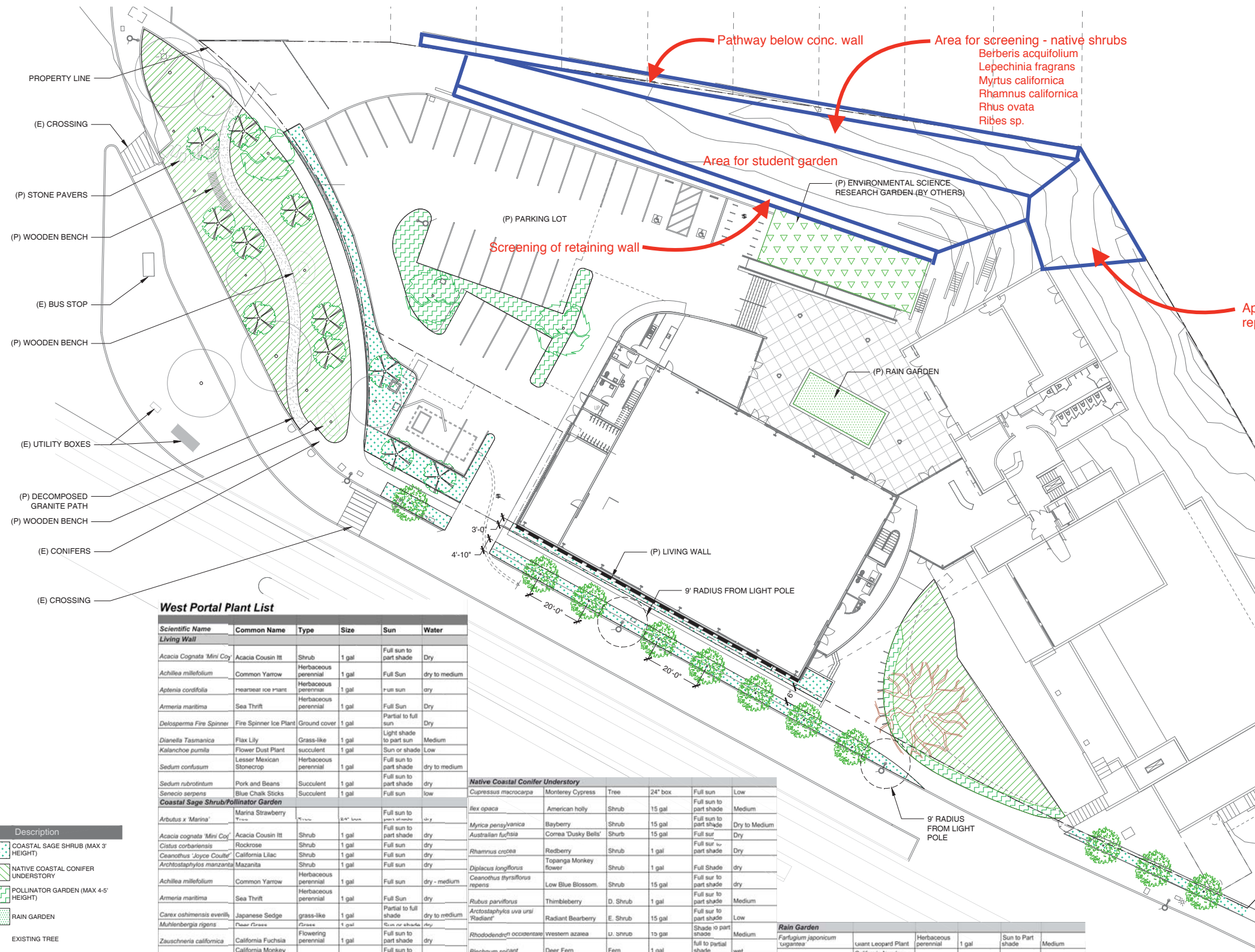
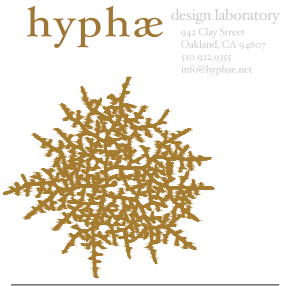
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PROJECT INFORMATION
WALDORF HIGH SCHOOL EXPANSION
 ADDRESS:
 470 WEST PORTAL AVE
 SAN FRANCISCO, CA 94127
 OWNER:
 DESIGNER:
 ERIC OLSON, PE
 eric@hyphae.net
 DRAWN BY:
 TL
 SUBMITTAL

16.11.01 SD

KEY PLAN

SCALE: 1" = 16'
PRELIMINARY PLANTING PLAN
 (SUBJECT TO FINAL APPROVAL BY PLANNING DEPT.)

West Portal Plant List

Scientific Name	Common Name	Type	Size	Sun	Water
Living Wall					
<i>Acacia Cognata 'Mini Coy'</i>	Acacia Cousin Itt	Shrub	1 gal	Full sun to part shade	Dry
<i>Achillea millefolium</i>	Common Yarrow	Herbaceous perennial	1 gal	Full Sun	dry to medium
<i>Aptenia cordifolia</i>	Heart-leaf ice plant	Herbaceous perennial	1 gal	Full sun	dry
<i>Armeria maritima</i>	Sea Thrift	Herbaceous perennial	1 gal	Full Sun	Dry
<i>Delosperma Fire Spinner</i>	Fire Spinner Ice Plant	Ground cover	1 gal	Partial to full sun	Dry
<i>Dianella Tasmanica</i>	Flax Lily	Grass-like	1 gal	Light shade to part sun	Medium
<i>Kalanchoe pumila</i>	Flower Dust Plant	succulent	1 gal	Sun or shade	Low
<i>Sedum confusum</i>	Lesser Mexican Stonecrop	Herbaceous perennial	1 gal	Full sun to part shade	dry to medium
<i>Sedum rubrotintum</i>	Pork and Beans	Succulent	1 gal	Full sun to part shade	dry
<i>Senecio serpens</i>	Blue Chalk Sticks	Succulent	1 gal	Full sun	low
Coastal Sage Shrub/Pollinator Garden					
<i>Arbutus x 'Marina'</i>	Marina Strawberry Tree	Shrub	1 gal	Full sun to part shade	dry
<i>Acacia cognata 'Mini Coy'</i>	Acacia Cousin Itt	Shrub	1 gal	Full sun to part shade	dry
<i>Cistus corbariensis</i>	Rockrose	Shrub	1 gal	Full sun	dry
<i>Ceanothus 'Joyce Coulter'</i>	California Lilac	Shrub	1 gal	Full sun	dry
<i>Archtophylos manzanita</i>	Mazanita	Shrub	1 gal	Full sun	dry
<i>Achillea millefolium</i>	Common Yarrow	Herbaceous perennial	1 gal	Full sun	dry - medium
<i>Armeria maritima</i>	Sea Thrift	Herbaceous perennial	1 gal	Full Sun	dry
<i>Carex oshimensis everittii</i>	Japanese Sedge	grass-like	1 gal	Partial to full shade	dry to medium
<i>Muhlenbergia rigens</i>	Flour Grass	Grass	1 gal	Full sun to part shade	dry
<i>Zauschneria californica</i>	California Fuchsia	Flowering perennial	1 gal	Full sun to part shade	dry
<i>Diplacis aurantiacus</i>	California Monkey Flower	Perennial	1 gal	Full sun to part shade	dry
<i>Senecio serpens</i>	Blue Chalk Sticks	Succulent	1 gal	Full sun	dry
<i>Sedum rubrotintum</i>	Pork and Beans	Succulent	1 gal	Full sun to part shade	dry
<i>Fragaria chiloensis</i>	Beach Strawberry	Ground cover	1 gal	Full sun to part shade	dry
<i>Thymus pulegioides</i>	Woolly Thyme	Ground cover	1 gal	Full sun	dry - medium
<i>Salvia spathacea</i>	Hummingbird Sage	Shrub	1 gal	Full sun	dry
<i>Salvia leucophylla</i>	Purple Sage	Shrub	1 gal	Full sun	dry
Native Coastal Conifer Understory					
<i>Cupressus macrocarpa</i>	Monterey Cypress	Tree	24" box	Full sun	Low
<i>Ilex opaca</i>	American holly	Shrub	15 gal	Full sun to part shade	Medium
<i>Myrica pensylvanica</i>	Bayberry	Shrub	15 gal	Full sun to part shade	Dry to Medium
<i>Australian fuchsia</i>	Correa 'Dusky Bells'	Shrub	15 gal	Full sun	Dry
<i>Rhamnus crocea</i>	Redberry	Shrub	1 gal	Full sun to part shade	Dry
<i>Diplacis longiflorus</i>	Topanga Monkey Flower	Shrub	1 gal	Full Shade	dry
<i>Ceanothus thyrsiflorus repens</i>	Low Blue Blossom	Shrub	15 gal	Full sun to part shade	dry
<i>Rubus parviflorus</i>	Thimbleberry	D. Shrub	1 gal	Full sun to part shade	Medium
<i>Arctostaphylos uva ursi 'Radiant'</i>	Radiant Bearberry	E. Shrub	15 gal	Full sun to part shade	Low
<i>Rhododendron occidentale</i>	Western azalea	u. shrub	10 gal	Shade to part shade	Medium
<i>Blechnum spicant</i>	Deer Fern	Fern	1 gal	full to partial shade	wet
<i>Tecomaria capensis</i>	Cape Honeysuckle	Vine	15 gal	Sun or shade	dry
<i>Lessingia flagellifolia</i>	Emerald Carpet	GC	1 gal	Full sun	dry
<i>Rubus pentanobis</i>	Creeping Raspberry	GC	1 gal	Sun to part shade	dry to medium
<i>Fragaria chiloensis</i>	Beach Strawberry	GC	1 gal	Full sun to part shade	dry to medium
<i>Delosperma cooperi</i>	Hardy iceplant	GC	1 gal	Full sun	Low
<i>Erigeron Karwinskianus</i>	Mexican Daisy	GC	1 gal	Full sun to part shade	Low
Rain Garden					
<i>Farfugium japonicum 'argentea'</i>	variant Leopard Plant	Herbaceous perennial	1 gal	Sun to Part shade	Medium
<i>Carex pansa</i>	California Meadow Sedge	Sedge	1 gal	Full sun	medium
<i>Eleocharis palustris</i>	Spike Rush	Sedge	1 gal	Full sun to part shade	Wet
<i>Juncus patens</i>	Juncus patens	Grass-like	1 gal	Sun or Shade	Low
<i>Juncus xiphioides</i>	Iris Leaved Rush	grass-like	1 gal	Full sun to part shade	dry - wet
<i>Fragaria chiloensis</i>	Beach Strawberry	Ground cover	1 gal	Full sun to part shade	dry to medium
<i>Satureja douglasii</i>	Yerba Buena	Ground cover	1 gal	shade - part shade	dry

- Legend**
- Symbol Description
 - COASTAL SAGE SHRUB (MAX 3' HEIGHT)
 - NATIVE COASTAL CONIFER UNDERSTORY
 - POLLINATOR GARDEN (MAX 4.5' HEIGHT)
 - RAIN GARDEN
 - EXISTING TREE
 - PROPOSED MONTEREY PINE
 - PROPOSED ARBUTUS X 'MARINA'
 - PROPOSED DECIDUOUS TREE
 - PROPOSED MONTEREY CYPRESS