



# SAN FRANCISCO PLANNING DEPARTMENT

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## Executive Summary Initiation of Planning Code and General Plan Amendments

HEARING DATE: MARCH 26, 2015

*Date:* March 26, 2015  
*Case No.:* 2014.0925MT  
*Project:* Rincon Hill Streetscape Plan [Adoption Hearing]  
*Staff Contact:* Paul Chasan – (415) 575-9065 [paul.chasan@sfgov.org](mailto:paul.chasan@sfgov.org)  
*Reviewed by:* Joshua Switzky – (415) 558-6815 [Joshua.Switzky@sfgov.org](mailto:Joshua.Switzky@sfgov.org)  
*Recommendation:* Adopt Amendments to the Planning Code and General Plan.

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### INTRODUCTION

The Rincon Hill Streetscape Plan is a necessary document for implementing the streetscape and circulation policies in the Rincon Hill Plan of the General Plan, adopted in 2005. As such, it is the basis for General Plan consistency determinations for all streetscape and right-of-way improvements (including traffic configurations) in the Rincon Hill area, whether implemented by the public or private sectors.

The Rincon Hill Streetscape Plan is used as the basis for, and to determine the adequacy and appropriateness of, all streetscape improvements required by Sections 138.1, 309.1 and 827 of the Planning Code, mandated by the Planning Commission, or voluntarily installed. All the curblines and traffic designs described here were fully analyzed in the certified Rincon Hill Plan EIR and related area Plan approvals. The purposes of the Streetscape Plan document are to

- (1) provide a clear, easy-to-follow and detailed comprehensive plan for streetscape and circulation changes for the Rincon Hill area.
- (2) provide detailed guidelines and standards for the design of streetscapes, including curblines, landscaping, street trees, sidewalk bulbouts, lighting, paving, and street furniture.

### REQUIRED COMMISSION ACTIONS AT THIS HEARING

1. Adopt the Rincon Hill Streetscape Plan
2. Amend the Rincon Hill Area Plan to amend and remove policies to reflect completion and adoption of the Rincon Hill Streetscape Plan
3. Amend the San Francisco Planning Code to amend and remove language to reflect the adoption of the Rincon Hill Streetscape Plan

### BACKGROUND AND CONTEXT

Rincon Hill is an area transitioning from commercial and industrial area into a high-density mixed-use residential neighborhood. In 2005, the Planning Commission and Board of Supervisors adopted the Rincon Hill Area Plan, which seeks to facilitate this transition. The plan significantly increased zoning capacity on Rincon Hill, and when built-out will create housing to support roughly 10,000 new residents. Immediately to the north of Rincon Hill, is the Transbay Redevelopment Area Zone 1, which

was designed in tandem with the Rincon Hill area as one complete neighborhood centered on Folsom Street, and will add over 3,000 new housing units to those south of Folsom.

The Rincon Hill Area Plan recognized that Rincon Hill's industrial fabric lacked infrastructure such as pedestrian amenities and open space to support a thriving residential population. The Plan seeks to rectify this by recommending the construction of a series of open spaces, community facilities and streetscape improvements in the neighborhood. This new infrastructure would be largely funded by development impact fees adopted as part of the Rincon Hill Plan. The Planning Department in coordination with the Capital Planning Committee continues to identify additional resources to fully implement the plan.

The City is also in the process of working with community stakeholders to establish a Community Benefits District to ensure that future streetscape improvements are well maintained. (Note that those required to be constructed pursuant to Planning Code 138.1 are required to be maintained in perpetuity by the developer.) The proposed Community Benefits District will cover both the Rincon Hill and Transbay neighborhoods.

While the Area Plan established basic direction for the design of streets within the plan area it did not articulate the level of detail necessary for implementation or to ensure consistent, high-quality streetscapes throughout the plan area.

To rectify this, the Planning Department worked closely with the SFMTA to refine the street and circulation concepts expressed in the Area Plan and vet design details like bulbout locations, turning radii, lane widths etc. These basic changes were approved by the MTA Board in 2006. In 2007, the Planning Department in partnership with SFDPW, the SFPUC, the SFFD and the SFMTA memorialized these designs in the illustrative document you are being asked to take action on today – The Rincon Hill Streetscape Plan (RHSP). The Streetscape plan further expands the design concepts articulated in the area plan with a level of specificity (paving materials, street trees, furniture, sidewalk dimensions) adequate to ensure that the streets surrounding Rincon Hill would be designed as high-quality, pedestrian-friendly spaces made using a consistent material palette and furnishings. Policy 7.4 of the Rincon Hill Area Plan calls on the City to:

**Policy 7.4**

Pursue the adoption of the Rincon Hill Streetscape Plan by all necessary agencies and the Board of Supervisors consistent with this plan.

– *Rincon Hill Area Plan (2005), an area plan of the San Francisco General Plan*

The Department's intent was to follow with adoptions by the Planning Commission and Board of Supervisors soon afterwards. Unfortunately, in late 2007, the global recession hit and San Francisco's real estate market crashed. Several pending projects in Rincon Hill went dormant. The Streetscape Plan was never taken though final adoption by the Commission or the Board and has persisted in "draft" status since that time.

The legislation presented in this document would rectify this situation by finishing the adoption process. The proposed ordinance would also make some simple modifications to Section 138.1 of the Planning Code and to the Rincon Hill Area Plan to reflect the final adoption of the RHSP.

This legislation is timely. As the real estate market has roared back to life, there are now various active development projects in the plan area, and all are required to construct streetscape improvements. Adopting the RHSP would clarify the City's expectations for the

area to the Development Community and thus simplify the streetscape permitting process for streetscape projects in the Rincon Hill Plan Area.

## **PLAN OVERVIEW**

Broadly, the RHSP provides two types of information to articulate a vision for the area's rights-of-ways: (1) providing typical plans, sections, lane striping configurations and dimensions for each street within the plan area, and (2) defining an approved palette of materials, furnishings, plantings and street trees.

## **CHANGES SINCE THE 2006/2007 PLAN WAS DRAFTED**

**Rerouting of the 12-Folsom Muni Line off of Folsom and Harrison Streets:** When the RHSP was initially drafted, Muni's 12-Folsom bus was routed eastbound on Folsom and westbound on Harrison Street. Within the Rincon Hill Plan Area, the parking lane on the north side of Harrison Street doubled as a transit only lane during afternoon commute hours. This shared parking/transit lane precluded corner bulbs on the north side of Harrison Street. After the RHSP was initially drafted, the SFMTA rerouted the 12 Folsom so that it turned northward on Second Street, bypassing the Rincon Hill Plan Area. The rerouting of the bus from the plan area provided an opportunity to add nine corner bulbs on the north side of Harrison Street to improve pedestrian conditions and safety. These bulb-outs were subsequently evaluated by the Environmental Planning division of the Planning Department in a note to file on January 2, 2014 and deemed consistent with the adopted EIR.

**Benches:** The bench proposed in the initial draft of the RHSP did not meet ADA compliance. The Planning Department has since updated the standard benches proposed for Rincon Hill to seating options that are in compliance with the ADA.

**Folsom Street Design Process:** Folsom Street between Second Street and Spear Street is envisioned to house neighborhood-serving retail for the Rincon Hill and Transbay Plan Areas. The Office of Community Infrastructure and Investment (OCII) has been managing the redesign of Folsom Street and this stretch of Folsom Street will soon begin construction. A few proposed block dimensions in the Rincon Hill plan area were slightly modified through this process. These modifications are still within the spirit and intent of the vision established within the Rincon Hill Plan Area Plan.

**Shared Public Ways (Curbless Streets):** In 2010, after the Rincon Hill Area Plan was adopted and the Rincon Hill streetscape plan was first drafted, the City adopted the Better Streets Plan (BSP), which provides a comprehensive set of guidelines for the design of San Francisco's pedestrian realm. Amongst these were guidelines for curbless streets or "Shared Public Ways". The RHSP has been updated to reflect this policy development. Several alleys in the plan area: Guy Place, Lansing Street, Grote Place and Zeno Place have been changed from curbed alleys to Shared Public Ways in the streetscape plan.

## **ENVIRONMENTAL CLEARANCE**

The streetscape changes proposed in the Rincon Hill Area Plan were environmentally cleared in the Rincon Hill Plan EIR in 2005. On January 7<sup>th</sup>, 2015, the Environmental Planning Division of the Planning Department published a Note to File to the original Rincon Hill Plan EIR finding that despite the passing of several years since the initial EIR was adopted, the findings were still valid and the streetscape improvements proposed in the Rincon Hill Area Plan and articulated in the Rincon Hill Streetscape Plan would have not have any significant adverse impacts.

"As described in the foregoing memorandum, the program EIR for the Rincon Hill Plan EIR adequately addressed all impacts of the Rincon Hill Streetscape Plan. The current Streetscape Plan

represents a refinement to the streetscape concepts described in the Rincon Hill Plan and would not have any additional significant adverse effects not examined in the program EIR, nor has any new or additional information come to light that would alter the conclusions of the program EIR. Moreover, no substantial changes have been made to the streetscape project or Plan since certification of the FEIR, nor have there been any substantial changes in circumstances necessitating revisions to the FEIR, nor has any new information of substantial importance come to light that raises one or more of the above issues.”

*Note to File to Rincon Hill Streetscape Plan EIR, San Francisco Planning Department, January 7<sup>th</sup> 2015*

## **PUBLIC OUTREACH & ENGAGEMENT**

The original Rincon Hill Planning Process had an extensive multi-year outreach and engagement strategy. Since that time Planning Department staff has conducted occasional outreach and attended neighborhood meetings to update residents on the status of the RHSP.

<b>RECOMMENDATION:</b> <b>Adopt Amendments to the Planning Code and General Plan</b>
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### **Attachments:**

*Note: While they are included in the digital version of this Planning Commission Packet, the printed version printed version of packet did not contain a copy of the Rincon Hill Streetscape Plan or two Notes to File to the Rincon Hill Area Plan EIR that were included in the packet at the initiation hearing (see italicized files in the attachment list below). To review hard copies of these documents, please see your packet from the initiation hearing.*

Adoption Resolution

Board Ordinances and Resolutions

*RHSP Note to File\_Extra Bulbs\_2014-01-02.pdf*

*RHSP Note to File\_Original EIR Still Good\_2015-01-07.pdf*

*Rincon Hill Streetscape Master Plan 2014 Update\_2014-05-08*



# SAN FRANCISCO PLANNING DEPARTMENT

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## Draft Planning Commission Resolution

HEARING DATE: MARCH 26, 2015

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CA 94103-2479

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*Project Name:* **Adoption of the Rincon Hill Streetscape Plan and Associated  
Planning Code Amendments**

*Case Number:* **2014.0925T**

*Staff Contact:* Paul Chasan and  
[paul.chasan@sfgov.org](mailto:paul.chasan@sfgov.org),

*Reviewed by:* Joshua Switzky  
joshua.switzky@sfgov.org, 415-575-6815

*Recommendation:* **Recommend Approval**

**RECOMMENDING THAT THE BOARD OF SUPERVISORS ADOPT AMENDMENTS TO THE RINCON HILL AREA PLAN (A SUBSECTION OF THE SAN FRANCISCO GENERAL PLAN) TO REFLECT ADOPTION OF THE RINCON HILL STREETScape PLAN.**

### PREAMBLE

WHEREAS, This document acts as a companion document to Planning Commission Resolution #19330 which recommends the Planning Commission Adopt the Rincon Hill Streetscape Plan and Recommend to the Board of Supervisors amendments to the General Plan reflective of the Rincon Hill Streetscape Plan's adoption; and

WHEREAS, The findings and General Plan Consistency findings in Planning Commission Resolution #19330 mentioned above bear equal relevance to the recommended actions articulated in this document and thus serve to legitimize and justify the recommended actions in this document;

WHEREAS, The Planning Commission finds from the facts presented that the public necessity, convenience and general welfare require the proposed amendments to the Planning Code as set forth in Section 302.

**RESOLVED**, that the Planning Commission hereby recommends approval of the proposed Planning Code amendment.

**MOVED**, that the Commission hereby *adopts* this Resolution to recommend approval of the draft Ordinance to the Board of Supervisors.

Having reviewed the materials identified in the preamble above, and having heard all testimony and arguments, this Commission finds, concludes, and determines as follows:

I hereby certify that the Planning Commission ADOPTED the foregoing Resolution on March 26, 2015.

Linda Avery  
Commission Secretary

AYES:

NAYS:

ABSENT:

ADOPTED: August 12, 2010



# SAN FRANCISCO PLANNING DEPARTMENT

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## Draft Planning Commission Resolution

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*Project Name:* **Amendments to the Rincon Hill Area Plan, a subplan within the San Francisco General Plan**

*Case Number:* **2014.0925M**

*Staff Contact:* Paul Chasan and  
[paul.chasan@sfgov.org](mailto:paul.chasan@sfgov.org)

*Reviewed by:* Joshua Switzky  
joshua.switzky@sfgov.org, 415-575-6815

*Recommendation:* **Recommend Approval**

RECOMMENDING THAT THE BOARD OF SUPERVISORS ADOPT AMENDMENTS TO THE PLANNING CODE TO REFLECT ADOPTION OF THE RINCON HILL STREETScape PLAN; ADOPTING FINDINGS, INCLUDING ENVIRONMENTAL FINDINGS, PLANNING CODE SECTION 302 FINDINGS, AND FINDINGS OF CONSISTENCY WITH THE GENERAL PLAN AND THE PRIORITY POLICIES OF PLANNING CODE SECTION 101.

### PREAMBLE

WHEREAS, The Board of Supervisors adopted the Rincon Hill Plan in August of 2005; and,

WHEREAS, The Plan adopts numerous streetscape and traffic changes including, but not limited to: Increasing the sidewalk width on Spear Main, Beale, Fremont, First, and Harrison Streets; bicycle lanes on Beale and Fremont Streets; corner bulbs; and mid-blocks crosswalks on Spear, Main and Beale Streets; and

WHEREAS, The Board of Supervisors adopted the Rincon Hill Plan in August of 2005; and,

WHEREAS, The proposed changes have been considered and approved by the Rincon Hill Plan Environmental Impact Report in 2006; and,

WHEREAS, The Rincon Hill Plan converts a large number of vacant or underutilized parcels located within a five-minute walk from the financial district into a large number of housing units in mid-rise and high-rise development and that few locations in San Francisco Represent such a major opportunity; and,

WHEREAS, The Rincon Hill Plan is the culmination of extensive public planning that began in 2003, with more than 30 workshops, hearings and walking tours, input of the existing residents and business, advocates and other public agencies; including the Municipal Transportation Agency (MTA) and that resulted in a plan that balances Rincon Hill's potential to provide much-needed housing with the design requirements of a livable neighborhood; and,

WHEREAS, The streetscape changes contemplated in the Rincon Hill Streetscape Plan are necessary for the traffic and streetscape conversions articulated in the Rincon Hill Plan; were approved in the Rincon Hill Environmental Impact Report and were approved on January 26, 2006 by the Interdepartmental Staff Committee on Traffic and Transportation (ISCOTT); and,

WHEREAS Policy 7.4 of the Rincon Hill Plan Area Plan calls on the city to “Pursue the adoption of the Rincon Hill Streetscape Plan by all necessary agencies and the board of Supervisors...”, and,

WHEREAS, the Planning Department in partnership with the San Francisco Municipal Transportation Agency and the Department of Public Works led a robust public process engaging numerous community stakeholders to develop the Rincon Hill Streetscape Plan from in 2006 to and has made held several follow-up meetings in the neighborhood between 2012 and 2014; and,

WHEREAS on May 30th of 2006, the MTA Board adopted the streetscape improvements identified in the Rincon Hill Area Plan and subsequently further articulated in the Rincon Hill Streetscape Plan and under Resolution number 06-067, and

WHEREAS, on January 2<sup>nd</sup>, 2014 the Environmental Planning Division of the San Francisco Planning Department issued a Note to File to the Rincon Hill Streetscape Plan finding the streetscape proposed bulb-outs supplemental added to the Rincon Hill Streetscape Plan after it was initially drafted would result in not have a significant environmental impact; and

WHEREAS, on January 1<sup>st</sup> 2014 the Environmental Planning Division of the San Francisco Planning Department published a note to file finding the streetscape changes contemplated in the initial Rincon Hill Streetscape Plan EIR will not have any significant impact (see attachment); and,

WHEREAS, on March 3<sup>rd</sup> 2015, the MTA Board adopted Resolution Number 15-035, approving said revisions to the Draft Rincon Hill Streetscape Plan; and,

WHEREAS, on March 5<sup>th</sup> 2015, the Planning Commission adopted resolutions number 19329 initiating amendments to the San Francisco Planning Code reflecting the adoption of the Rincon Hill Streetscape Plan; and

WHEREAS, on March 5<sup>th</sup> 2015, the Planning Commission adopted resolution number 19330 initiating amendments to the San Francisco General Plan reflecting the adoption of the Rincon Hill Streetscape Plan; and

**RESOLVED**, that the Planning Commission hereby recommends approval of the proposed General Plan amendment.

**MOVED**, that the Commission hereby *adopts* this Resolution to recommend approval of the draft Ordinance to the Board of Supervisors.

## **FINDINGS**

Having reviewed the materials identified in the preamble above, and having heard all testimony and arguments, this Commission finds, concludes, and determines as follows:

1. **General Plan Compliance.** This Resolution is consistent with the following Objectives and Policies of the General Plan:

### **I. URBAN DESIGN ELEMENT (2010)**

#### **OBJECTIVE 1**

EMPHASIS OF THE CHARACTERISTIC PATTERN WHICH GIVES TO THE CITY AND ITS NEIGHBORHOODS AN IMAGE, A SENSE OF PURPOSE, AND A MEANS OF ORIENTATION.

#### **POLICY 1.5**

Emphasize the special nature of each district through distinctive landscaping and other features.

#### **POLICY 1.7**

Recognize the natural boundaries of districts, and promote connections between districts.

#### **OBJECTIVE 4**

IMPROVEMENT OF THE NEIGHBORHOOD ENVIRONMENT TO INCREASE PERSONAL SAFETY, COMFORT, PRIDE AND OPPORTUNITY

#### **POLICY 4.1**

Protect residential areas from the noise, pollution and physical danger of excessive traffic.

#### **POLICY 4.10**

Encourage or require the provision of recreation space in private development.

#### **POLICY 4.11**

Make use of street space and other unused public areas for recreation, particularly in dense neighborhoods, such as those close to downtown, where land for traditional open spaces is more difficult to assemble.

#### **POLICY 4.12**

Install, promote and maintain landscaping in public and private areas.

#### **POLICY 4.13**

Improve pedestrian areas by providing human scale and interest.

#### **POLICY 4.14**

Remove and obscure distracting and cluttering elements.

**II. TRANSPORTATION ELEMENT (2010)**

**OBJECTIVE 1**

MEET THE NEEDS OF ALL RESIDENTS AND VISITORS FOR SAFE, CONVENIENT AND INEXPENSIVE TRAVEL WITHIN SAN FRANCISCO AND BETWEEN THE CITY AND OTHER PARTS OF THE REGION WHILE MAINTAINING THE HIGH QUALITY LIVING ENVIRONMENT OF THE BAY AREA.

**POLICY 1.1**

Involve citizens in planning and developing transportation facilities and services, and in further defining objectives and policies as they relate to district plans and specific projects.

**POLICY 1.2**

Ensure the safety and comfort of pedestrians throughout the city.

**POLICY 1.3**

Give priority to public transit and other alternatives to the private automobile as the means of meeting San Francisco's transportation needs, particularly those of commuters.

**POLICY 1.6**

Ensure choices among modes of travel and accommodate each mode when and where it is most appropriate.

**OBJECTIVE 2**

USE THE TRANSPORTATION SYSTEM AS A MEANS FOR GUIDING DEVELOPMENT AND IMPROVING THE ENVIRONMENT.

**POLICY 2.4**

Organize the transportation system to reinforce community identity, improve linkages among interrelated activities and provide focus for community activities.

**OBJECTIVE 15**

ENCOURAGE ALTERNATIVES TO THE AUTOMOBILE AND REDUCED TRAFFIC LEVELS ON RESIDENTIAL STREETS THAT SUFFER FROM EXCESSIVE TRAFFIC THROUGH THE MANAGEMENT OF TRANSPORTATION SYSTEMS AND FACILITIES.

**POLICY 15.1**

Discourage excessive automobile traffic on residential streets by incorporating traffic-calming treatments.

**OBJECTIVE 18**

ESTABLISH A STREET HIERARCHY SYSTEM IN WHICH THE FUNCTION AND DESIGN OF EACH STREET ARE CONSISTENT WITH THE CHARACTER AND USE OF ADJACENT LAND.

**OBJECTIVE 23**

IMPROVE THE CITY'S PEDESTRIAN CIRCULATION SYSTEM TO PROVIDE FOR EFFICIENT, PLEASANT, AND SAFE MOVEMENT.

**POLICY 23.1**

Provide sufficient pedestrian movement space with a minimum of pedestrian congestion in accordance with a pedestrian street classification system.

**POLICY 23.2**

Widen sidewalks where intensive commercial, recreational, or institutional activity is present, sidewalks are congested, where sidewalks are less than adequately wide to provide appropriate pedestrian amenities, or where residential densities are high.

**POLICY 23.9**

Implement the provisions of the Americans with Disabilities Act and the city's curb ramp program to improve pedestrian access for all people.

**OBJECTIVE 24**

IMPROVE THE AMBIENCE OF THE PEDESTRIAN ENVIRONMENT.

**POLICY 24.3**

Install pedestrian-serving street furniture where appropriate.

**POLICY 24.5**

Where consistent with transportation needs, transform streets and alleys into neighborhood-serving open spaces or "living streets" by adding pocket parks in sidewalks or medians, especially in neighborhoods deficient in open space.

**OBJECTIVE 26**

CONSIDER THE SIDEWALK AREA AS AN IMPORTANT ELEMENT IN THE CITYWIDE OPEN SPACE SYSTEM.

**POLICY 26.1**

Retain streets and alleys not required for traffic, or portions thereof, for through pedestrian circulation and open space use.

**POLICY 26.3**

Encourage pedestrian serving uses on the sidewalk.

**OBJECTIVE 27**

ENSURE THAT BICYCLES CAN BE USED SAFELY AND CONVENIENTLY AS A PRIMARY MEANS OF TRANSPORTATION, AS WELL AS FOR RECREATIONAL PURPOSES.

**POLICY 27.1**

Expand and improve access for bicycles on city streets and develop a well-marked, comprehensive system of bike routes in San Francisco.

**POLICY 27.3**

Remove conflicts to bicyclists on all city streets.

**POLICY 27.6**

Accommodate bicycles on local and regional transit facilities and important regional transportation links wherever and whenever feasible.

**III. RINCON HILL AREA PLAN (2006)**

**4. RECREATION, OPEN SPACE, AND COMMUNITY FACILITIES**

**OBJECTIVE 4.5**

USE EXCESS STREET SPACE ON SPEAR, MAIN, AND BEALE STREETS FOR SIDEWALK WIDENINGS THAT PROVIDE USABLE OPEN SPACES AND RECREATIONAL AMENITIES.

**5. STREETS AND TRANSPORTATION**

**OBJECTIVE 5.1**

CREATE SAFE AND PLEASANT PEDESTRIAN NETWORKS WITHIN THE RINCON HILL AREA, TO DOWNTOWN, AND TO THE BAY.

**OBJECTIVE 5.2**

WIDEN SIDEWALKS, REDUCE STREET WIDTHS, AND MAKE OTHER PEDESTRIAN AND STREET IMPROVEMENTS, WHILE RETAINING THE NECESSARY SPACE FOR TRAFFIC MOVEMENTS, PER THE RINCON HILL STREETScape PLAN.

**OBJECTIVE 5.3**

PRIORITIZE PEDESTRIAN SAFETY THROUGH STREET AND INTERSECTION IMPROVEMENTS, ESPECIALLY AT INTERSECTIONS ADJACENT TO FREEWAY RAMPS, AND INTERSECTIONS WITH A HISTORY OF VEHICLE/PEDESTRIAN COLLISIONS.

**OBJECTIVE 5.5**

MANAGE PARKING SUPPLY AND PRICING TO ENCOURAGE TRAVEL BY FOOT, PUBLIC TRANSPORTATION, AND BICYCLE.

**OBJECTIVE 5.6**

IMPROVE LOCAL AND REGIONAL TRAFFIC FLOWS AND TRANSIT MOVEMENTS BY SEPARATING BRIDGE-BOUND TRAFFIC FROM LOCAL LANES IN APPROPRIATE LOCATIONS.

**OBJECTIVE 5.7**

MAINTAIN THE POTENTIAL FOR A BAY BRIDGE BICYCLE/PEDESTRIAN/MAINTENANCE PATH, AND ENSURE THAT ALL OPTIONS FOR THE PATH TOUCHDOWN AND ALIGNMENT ARE KEPT OPEN.

**OBJECTIVE 5.8**

ENCOURAGE STATE AGENCIES TO ALLOW THE RE-OPENING OF BEALE STREET UNDER THE BAY BRIDGE AS SOON AS SECURITY CONCERNS CAN BE MET.

**OBJECTIVE 5.9**

REQUIRE PRIVATE DEVELOPMENT TO CONTRIBUTE TO THE CREATION AND ON-GOING MAINTENANCE AND OPERATIONS OF SPECIAL STREETSCAPES THROUGH IN-KIND CONTRIBUTION, A COMMUNITY FACILITIES DISTRICT, AND/OR DEVELOPER FEES. POLICIES

**Policy 5.1**

Implement the Rincon Hill Streetscape Plan.

**Policy 5.2**

Significantly widen sidewalks by removing a lane of traffic on Spear, Main and Beale Streets between Folsom and Bryant Streets per the Rincon Hill Streetscape Plan in order to create new "Living Streets," with pocket park and plaza spaces for active and passive recreational use, decorative paving, lighting, seating, trees and other landscaping. See Figure 6.

**Policy 5.3**

Transform Folsom Street into a grand civic boulevard, per this plan and the Transbay Redevelopment Plan.

**Policy 5.4**

Widen sidewalks, narrow lanes and remove lanes, where feasible, on Harrison, First and Fremont Streets.

**Policy 5.5**

Separate bridge-bound traffic from local traffic and transit through physical design strategies such as planted medians.

**Policy 5.6**

Implement streetscape improvements on Guy Place and Lansing Street that prioritize pedestrian use for the entire right-of-way.

Mid-Block Pedestrian Pathways

**Policy 5.7**

Ensure the creation of a safe, inviting, and pleasant publicly accessible pedestrian/open space mid-block pathway through Assessors Blocks 3744-3748 from First Street to the Embarcadero by requiring new developments along the alignment of the proposed path to provide a publicly-accessible easement through their property.

Mid-Block Pedestrian Pathways

**Policy 5.7**

Ensure the creation of a safe, inviting, and pleasant publicly accessible pedestrian/open space mid-block pathway through Assessors Blocks 3744-3748 from First Street to the Embarcadero by requiring new developments along the alignment of the proposed path to provide a publicly-accessible easement through their property.

**Policy 7.1**

Require new development to implement portions of the streetscape plan adjacent to their development, and additional relevant in-kind contributions, as a condition of approval.

**Policy 7.4**

Pursue the adoption of the Rincon Hill Streetscape Plan by all necessary agencies and the Board of Supervisors consistent with this plan.

2. The Planning Commission finds from the facts presented that the public necessity, convenience and general welfare require the proposed amendments to the Planning Code as set forth in Section 302.

3. This Resolution is consistent with the eight General Plan priority policies set forth in Section 101.1 in that:

- A) The existing neighborhood-serving retail uses will be preserved and enhanced and future opportunities for resident employment in and ownership of such businesses will be enhanced.

*The proposed Ordinance would not have a negative impact on neighborhood serving retail uses and will not impact opportunities for resident employment in and ownership of neighborhood-serving retail.*

- B) The existing housing and neighborhood character will be conserved and protected in order to preserve the cultural and economic diversity of our neighborhoods.

*The proposed Ordinance would not have a negative effect on housing or neighborhood character. The modifications proposed would impose minimal impact on the existing housing and neighborhood character.*

- C) The City's supply of affordable housing will be preserved and enhanced.

*The proposed Ordinance would not have an adverse effect on the City's supply of affordable housing. The ordinance provides a path for persons with a disability to remain in their homes.*

- D) The commuter traffic will not impede MUNI transit service or overburden our streets or neighborhood parking.

*The proposed Ordinance would not impede Muni transit service or overburden our streets or neighborhood parking.*

- E) A diverse economic base will be maintained by protecting our industrial and service sectors from displacement due to commercial office development. And future opportunities for resident employment and ownership in these sectors will be enhanced.

*The proposed Ordinance would not cause displacement of the industrial or service sectors due to office development, and future opportunities for resident employment or ownership in these sectors would not be impaired.*

- F) The City will achieve the greatest possible preparedness to protect against injury and loss of life in an earthquake.

*The proposed Ordinance would not have an impact on City's preparedness against injury and loss of life in an earthquake.*

- G) That landmark and historic buildings will be preserved.

*The proposed Ordinance would not have a negative impact on the City's Landmarks and historic buildings as any new modifications would be added under the guidance of local law and policy protecting historic resources, when appropriate.*

- H) Parks and open space and their access to sunlight and vistas will be protected from development.

*The proposed Ordinance would not have an impact on the City's parks and open space and their access to sunlight and vistas.*

I hereby certify that the Planning Commission ADOPTED the foregoing Resolution on March 26<sup>th</sup> 2015.

Linda Avery  
Commission Secretary

AYES:

NAYS:

ABSENT:

ADOPTED: August 12, 2010

1 [Planning Code Amendments – Rincon Hill Streetscape Master Plan]

2  
3 **Ordinance amending Planning Code Section 138.1 to acknowledge approval of the**  
4 **Rincon Hill Streetscape Master Plan, and making findings under the California**  
5 **Environmental Quality Act and findings of consistency with the General Plan and**  
6 **priority policies of Planning Code Section 101.1.**

7 NOTE: **Unchanged Code text and uncodified text** are in plain Arial font.  
8 **Additions to Codes** are in *single-underline italics Times New Roman font*.  
9 **Deletions to Codes** are in *strikethrough italics Times New Roman font*.  
10 **Board amendment additions** are in double-underlined Arial font.  
11 **Board amendment deletions** are in ~~strikethrough Arial font~~.  
12 **Asterisks (\* \* \* \*)** indicate the omission of unchanged Code  
13 subsections or parts of tables.

14 Be it ordained by the People of the City and County of San Francisco:

15 Section 1. Findings.

16 (a) In companion legislation regarding General Plan amendments related to the  
17 Rincon Hill Streetscape Master Plan, a copy of which is in Clerk of the Board File No.  
18 \_\_\_\_\_, the Board of Supervisors adopted various findings, including findings under  
19 the California Environmental Quality Act (CEQA).

20 (b) For purposes of this ordinance, the Board adopts the CEQA Findings set forth in  
21 the ordinance on file with the Clerk of the Board in File No. \_\_\_\_\_. Said CEQA  
22 Findings are incorporated herein by reference.

23 (c) After a duly noticed public hearing on \_\_\_\_\_, 2015, in Motion No.  
24 \_\_\_\_\_, the Planning Commission initiated amendments to the Planning Code in  
25 regard to the Rincon Hill Streetscape Master Plan. Said Motion is on file with the Clerk of the  
Board in File No. \_\_\_\_\_.

1 (d) After a duly noticed public hearing on \_\_\_\_\_, 2015, in Resolution No.  
2 \_\_\_\_\_, the Planning Commission recommended that the Board of Supervisors  
3 approve Planning Code amendments related to the Rincon Hill Streetscape Master Plan. In  
4 this Resolution, the Planning Commission found, pursuant to Planning Code Section 302, that  
5 the Planning Code amendments will serve the public necessity, convenience, and general  
6 welfare. Said Resolution is on file with the Clerk of the Board in File No. \_\_\_\_\_ and  
7 incorporated herein by reference. The Board hereby adopts the Planning Code Section 302  
8 findings set forth in Planning Commission Resolution No. \_\_\_\_\_ as its own.

9 (e) The Board of Supervisors finds that the Planning Code amendments in this  
10 ordinance, are, on balance, in conformity with the General Plan, and the eight priority policies  
11 of Planning Code Section 101.1 for the reasons set forth in Planning Commission Resolution  
12 No. \_\_\_\_\_. The Board hereby adopts these findings as its own.

13  
14  
15 Section 2. The Planning Code is hereby amended by amending Section 138.1, to read  
16 as follows:

17 **SEC. 138.1. STREETScape AND PEDESTRIAN IMPROVEMENTS.**

18 \* \* \* \*

19 (d) Neighborhood Streetscape Plans. In addition to the requirements listed in  
20 Subsection 138.1(c), the Planning Department in coordination with other city agencies, and  
21 after a public hearing, may adopt streetscape plans for particular streets, neighborhoods, and  
22 districts, containing standards and guidelines to supplement the Better Streets Plan.  
23 Development projects in areas listed in this subsection that propose or are required through  
24 this section to make pedestrian and streetscape improvements to the public right-of-way shall  
25

1 conform with the standards and guidelines in the applicable neighborhood streetscape plan in  
2 addition to those found in the Better Streets Plan.

3 (1) Downtown Streetscape Plan.

4 (A) In any C-3 District sidewalk paving as set forth in the Downtown  
5 Streetscape Plan shall be installed by the applicant under the following conditions:

6 (i) Any new construction;

7 (ii) The addition of floor area equal to 20 percent or more of an existing  
8 building.

9 (B) In accordance with the provisions of Section 309 of the Planning Code  
10 governing C-3 Districts, when a permit is granted for any project abutting a public sidewalk in  
11 a C-3 District, the Planning Commission may impose additional requirements that the  
12 applicant install sidewalk improvements such as benches, bicycle racks, lighting, special  
13 paving, seating, landscaping, and sidewalk widening in accordance with the guidelines of the  
14 Downtown Streetscape Plan if it finds that these improvements are necessary to meet the  
15 goals and objectives of the General Plan of the City and County of San Francisco. In making  
16 this determination, the Planning Commission shall consider the level of street as defined in  
17 the Downtown Streetscape Plan.

18 (C) If a sidewalk widening or a pedestrian street improvement is used to meet  
19 the open space requirement, it shall conform to the guidelines of Section 138.

20 (D) The Planning Commission shall determine whether the streetscape  
21 improvements required by this Section may be on the same site as the building for which the  
22 permit is being sought, or within 900 feet, provided that all streetscape improvements are  
23 located entirely within the C-3 District.

24  
25 (2) Rincon Hill Streetscape Plan.

1           (4) In the Rincon Hill Downtown Residential Mixed Use (RH-DTR) and Folsom  
2 and Main Residential/Commercial Special Use Districts, the boundaries of which are shown in  
3 Section Map No. 1 of the Zoning Map, for all frontages abutting a public sidewalk, the project  
4 sponsor is required to install sidewalk widening, street trees, lighting, decorative paving,  
5 seating and landscaping in accordance with the approved Streetscape Master Plan of the  
6 Rincon Hill Area Plan, ~~developed by the Planning Department and approved by the Board of~~  
7 ~~Supervisors~~ for: (A) any new construction; or (B) the addition of floor area equal to 20 percent  
8 or more of an existing building.

9           ~~(B) Prior to approval by the Board of Supervisors of a Streetscape Plan for Rincon Hill, the~~  
10 ~~Planning Commission, through the procedures of Section 309.1, shall require an applicant to install~~  
11 ~~sidewalk widening, street trees, lighting, decorative paving, seating, and landscaping in keeping with~~  
12 ~~the intent of the Rincon Hill Area Plan of the General Plan and in accordance with this section of the~~  
13 ~~Planning Code.~~

14  
15           \* \* \* \*

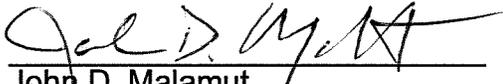
16  
17           Section 3. Effective Date. This ordinance shall become effective 30 days after  
18 enactment. Enactment occurs when the Mayor signs the ordinance, the Mayor returns the  
19 ordinance unsigned or does not sign the ordinance within ten days of receiving it, or the Board  
20 of Supervisors overrides the Mayor's veto of the ordinance.

21  
22           Section 4. Scope of Ordinance. In enacting this ordinance, the Board of Supervisors  
23 intends to amend only those words, phrases, paragraphs, subsections, sections, articles,  
24 numbers, punctuation marks, charts, diagrams, or any other constituent parts of the Municipal  
25 Code that are explicitly shown in this ordinance as additions, deletions, Board amendment

1 additions, and Board amendment deletions in accordance with the "Note" that appears under  
2 the official title of the ordinance.

3  
4  
5 APPROVED AS TO FORM:  
6 DENNIS J. HERRERA, City Attorney

7 By:

  
8 John D. Malamut  
9 Deputy City Attorney

n:\landlas2015\9690391\01001185.doc

1 [General Plan Amendments – Rincon Hill Streetscape Master Plan]

2  
3 **Ordinance amending the Rincon Hill Area Plan, a subsection of the San Francisco**  
4 **General Plan, to incorporate the Rincon Hill Streetscape Master Plan and make other**  
5 **technical amendments, and making findings under the California Environmental**  
6 **Quality Act and findings of consistency with the General Plan and priority policies of**  
7 **Planning Code Section 101.1.**

8 NOTE: **Unchanged Code text and uncodified text** are in plain Arial font.  
9 **Additions to Codes** are in *single-underline italics Times New Roman font*.  
10 **Deletions to Codes** are in ~~*strikethrough italics Times New Roman font*~~.  
11 **Board amendment additions** are in double-underlined Arial font.  
12 **Board amendment deletions** are in ~~strikethrough Arial font~~.  
13 **Asterisks (\* \* \* \*)** indicate the omission of unchanged Code  
14 subsections or parts of tables.

15 Be it ordained by the People of the City and County of San Francisco:

16 Section 1. Findings.

17 (a) Section 4.105 of the San Francisco Charter provides that the Planning Commission  
18 shall periodically recommend to the Board of Supervisors, for approval or rejection, proposed  
19 amendments to the General Plan.

20 (b) On \_\_\_\_\_, the Board of Supervisors received from the Planning  
21 Department proposed General Plan amendments related to the Rincon Hill Streetscape  
22 Master Plan, a component of the Rincon Hill Area Plan, which is a subsection of General Plan  
23 (the "Rincon Hill Streetscape Amendments"). That letter and related documents are on file  
24 with the Clerk of the Board in File No. \_\_\_\_\_.

25 (c) The Rincon Hill Plan was the culmination of extensive public planning that began in  
2003, with more than 30 workshops, hearings and walking tours, input of the existing

1 residents and business, advocates and other public agencies, and resulted in a plan that  
2 balances Rincon Hill's potential to provide much-needed housing with the design  
3 requirements of a livable neighborhood.

4 (d) Policy 7.4 of the Rincon Hill Plan Area Plan calls on the City to "[p]ursue the  
5 adoption of the Rincon Hill Streetscape Plan by all necessary agencies and the board of  
6 Supervisors..."

7 (e) The Planning Department in partnership with the San Francisco Municipal  
8 Transportation Agency (MTA) and the Department of Public Works led a robust public process  
9 engaging numerous community stakeholders to develop the Rincon Hill Streetscape Plan  
10 from \_\_\_ to \_\_\_.

11 (f) The Rincon Hill Streetscape Master Plan is in compliance with various established  
12 San Francisco policies related to the design of streets and the public realm, including the  
13 Transit First Policy, the Better Streets Policy, the Complete Streets Policy, and the SFPUC  
14 Stormwater Design Guidelines.

15 (g) On \_\_\_\_\_, 2015 the MTA Board adopted the streetscape  
16 improvements identified in the Rincon Hill Streetscape Master Plan in Resolution No.  
17 \_\_\_\_\_, a copy of which is on file with the Clerk of the Board in File No.  
18 \_\_\_\_\_.

19 (h) On May 5, 2005, after a duly noticed public meeting, the Planning Commission  
20 certified the Final Environmental Impact Report (EIR) for the Rincon Hill Area Plan by Motion  
21 No. 17007 and found that the Final EIR reflected the independent judgment and analysis of  
22 the City and County of San Francisco, is adequate, accurate and objective, contains no  
23 significant revisions to the Draft EIR, and the content of the report and the procedures through  
24 which the Final EIR was prepared, publicized and reviewed comply with the provisions of the  
25 California Environmental Quality Act (CEQA) (California Public Resources Code Sections

1 21000 et seq.), the CEQA Guidelines (14 Cal. Code Regs. Sections 15000 et seq.) and  
2 Chapter 31 of the San Francisco Administrative Code. Copies of the Planning Commission  
3 Motion and Final EIR are on file with the Clerk of the Board in File No. 050862 and are  
4 incorporated herein by reference.

5 (i) The project evaluated in the Final EIR included amendments to the General Plan  
6 related to the Rincon Hill Area Plan. This Project also included references to the Rincon Hill  
7 Streetscape Master Plan that was not completed at the time the City adopted the Rincon Hill  
8 Area Plan. The subject of this ordinance is adoption and implementation of the Rincon Hill  
9 Streetscape Master Plan.

10 (j) At the same hearing during which the Planning Commission certified the Final EIR,  
11 the Planning Commission adopted CEQA Findings with respect to the approval of the  
12 proposed Rincon Hill Area Plan and other actions in Motion No. 17008.

13 (k) The Board of Supervisors in Ordinance No. 205-05 adopted the Rincon Hill Area  
14 Plan and adopted CEQA Findings adopted by the Planning Commission with respect to the  
15 approval of the Rincon Hill Area Plan, including a mitigation monitoring and reporting program  
16 and a statement of overriding considerations. This ordinance and related materials are in the  
17 Clerk of the Board in File No. 050862. These and any and all other documents referenced in  
18 this ordinance have been made available to the Board of Supervisors and may be found in  
19 either the files of the Planning Department, as the custodian of records, at 1650 Mission  
20 Street in San Francisco, or with the Clerk of the Board in File Nos. 050862 and  
21 \_\_\_\_\_, which are located at 1 Dr. Carlton B. Goodlett Place, San Francisco, and are  
22 incorporated herein by reference.

23 (l) For purposes of this ordinance, the Board of Supervisors has reviewed and  
24 considered the Final EIR and the environmental documents on file referred to herein. The  
25 Board of Supervisors also has reviewed and considered the CEQA Findings adopted by the

1 Planning Commission in support of the approval of the Rincon Hill Streetscape Master Plan  
2 (the "CEQA Findings for the Rincon Hill Streetscape"), including the mitigation monitoring and  
3 reporting program, and hereby adopts as its own and incorporates the CEQA Findings for the  
4 Rincon Hill Streetscape contained in Planning Commission Motion No. \_\_\_\_\_ by  
5 reference as though such findings were fully set forth in this Ordinance.

6 (m) The Board of Supervisors endorses the implementation of the mitigation measures  
7 identified in the CEQA Findings for the Rincon Hill Streetscape including those for  
8 implementation by other City Departments and recommends for adoption those mitigation  
9 measures that are enforceable by agencies other than City agencies, all as set forth in the  
10 CEQA Findings for the Rincon Hill Streetscape, including the mitigation monitoring and  
11 reporting program contained in the referenced Findings.

12 (n) The Board of Supervisors finds that no substantial changes have occurred in the  
13 Rincon Hill Area Plan as proposed for amendment under this ordinance that will require  
14 revisions in the Final EIR due to the involvement of new significant environmental effects or a  
15 substantial increase in the severity of previously identified significant effects, no substantial  
16 changes have occurred with respect to the circumstances under which the Rincon Hill Area  
17 Plan are undertaken which will require major revisions to the Final EIR due to the involvement  
18 of new environmental effects or a substantial increase in the severity of effects identified in the  
19 Final EIR and no new information of substantial importance to the Rincon Hill Area Plan as  
20 proposed for amendment has become available which indicates that (1) the Rincon Hill Area  
21 Plan will have significant effects not discussed in the Final EIR, (2) significant environmental  
22 effects will be substantially more severe, (3) mitigation measure or alternatives found not  
23 feasible which would reduce one or more significant effects have become feasible or (4)  
24 mitigation measures or alternatives which are considerably different from those in the Final  
25 EIR would substantially reduce one or more significant effects on the environment.

1 (o) Section 4.105 of the City Charter further provides that if the Board of Supervisors  
2 fails to act within 90 days of receipt of the proposed General Plan amendments, then the  
3 proposed amendments shall be deemed approved.

4 (p) San Francisco Planning Code Section 340 provides that the Planning Commission  
5 may initiate an amendment to the General Plan by a resolution of intention, which refers to,  
6 and incorporates by reference, the proposed General Plan amendments. Section 340 further  
7 provides that Planning Commission shall adopt the proposed General Plan amendments after  
8 a public hearing if it finds from the facts presented that the public necessity, convenience, and  
9 general welfare require the proposed amendment or any part thereof. If adopted by the  
10 Commission in whole or in part, the proposed amendments shall be presented to the Board of  
11 Supervisors, which may approve or reject the amendments by a majority vote.

12 (q) After a duly noticed public hearing on \_\_\_\_\_, 2015, in Motion No.  
13 \_\_\_\_\_, the Planning Commission initiated amendments to the General Plan in  
14 regard to the Rincon Hill Streetscape Master Plan. Said Motion is on file with the Clerk of the  
15 Board in File No. \_\_\_\_\_.

16 (r) After a duly noticed public hearing on \_\_\_\_\_, 2015, in Resolution No.  
17 \_\_\_\_\_, the Planning Commission recommended that the Board of Supervisors  
18 approve the Rincon Hill Streetscape Amendments. In this Motion, the Planning Commission  
19 found, pursuant to Planning Code Section 340, that the Rincon Hill Streetscape Amendments  
20 will serve the public necessity, convenience, and general welfare. Said Resolution is on file  
21 with the Clerk of the Board in File No. \_\_\_\_\_ and incorporated herein by reference.  
22 The Board hereby adopts the Planning Code Section 340 findings set forth in Planning  
23 Commission Resolution No. \_\_\_\_\_ as its own.

24 (s) The Board of Supervisors finds that the Rincon Hill Streetscape Amendments are,  
25 on balance, in conformity with the General Plan, as it is proposed for amendment by this

1 ordinance, and the eight priority policies of Planning Code Section 101.1 for the reasons set  
2 forth in Planning Commission Resolution No. \_\_\_\_\_. The Board hereby adopts  
3 these findings as its own.  
4

5 Section 2. The Board of Supervisors hereby approves the Rincon Hill Streetscape  
6 Amendments set forth below as an amendment to the Rincon Hill Area Plan portion of the  
7 General Plan and directs the Planning Department, if necessary, to update the General Plan's  
8 Land Use Index to reflect these Amendments. In addition, the Planning Department shall  
9 update Map 9 (Rincon Hill Streetscape) of the Rincon Hill Area Plan to reflect the map  
10 included in Planning Commission Resolution No. \_\_\_\_\_ and on file with the Clerk of  
11 the Board in File No. \_\_\_\_\_. The Rincon Hill Streetscape Amendments are as  
12 follows:

13 **Policy 5.1**

14 Implement the Rincon Hill Streetscape Plan.

15 ~~A comprehensive streetscape plan is proposed for Rincon Hill. This plan~~ The Rincon Hill  
16 Streetscape Master Plan calls for extensive sidewalk widenings, tree plantings, street  
17 furniture, and the creation of new public spaces along streets throughout the district.  
18 The plan ~~will~~ describes specific curb and sidewalk changes and roadway lane  
19 configurations. New development ~~will in the plan area is~~ required to implement portions  
20 of the streetscape plan as a condition of approval, and to pay into a community  
21 facilities district that will enable the City to implement and maintain those portions of the  
22 Streetscape Plan not put in place by new projects. ~~The proposed Streetscape Plan will be~~  
23 ~~separately approved by the Municipal Transportation Authority, the Department of Public~~  
24 ~~Works, the Planning Commission, and the Board of Supervisors.~~

25 \* \* \* \* \*





# SAN FRANCISCO PLANNING DEPARTMENT

**MEMO**

**DATE:** January 7, 2015  
**TO:** File  
**FROM:** Michael Jacinto, Environmental Planning  
**RE:** Case No. 2014.0925E, Rincon Hill Streetscape Plan

1650 Mission St.  
Suite 400  
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CA 94103-2479

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In August 2005, the Board of Supervisors (BOS) adopted the Rincon Hill Plan and established a fee program to fund public improvements identified in the Plan. The Rincon Hill Plan is an element of the City's *General Plan* containing policies addressing land use; housing; urban design; recreation, open space and community facilities; streets and transportation; and preservation. Together these policies form a blueprint guiding Rincon Hill's transition from a former industrial district to a full-service, mixed-use urban neighborhood.

This memorandum summarizes the components of the Rincon Hill Plan germane to the proposed Rincon Hill Streetscape Master Plan (Streetscape Master Plan, March 2014)<sup>1</sup>; discusses the Rincon Hill Plan's environmental review in relation to planned street, sidewalk and public realm improvements in general; and describes why no subsequent environmental review for the proposed Streetscape Master Plan, Case No. 2014.0925EMT, is currently required. The Rincon Hill Plan Final EIR (hereafter "Rincon Hill EIR" or "EIR") serves as the basis for this determination and is incorporated in this memorandum by reference.<sup>2</sup>

## RINCON HILL PLAN

### Location

The Rincon Hill Plan area is situated within the northeast portion of San Francisco, south of the Financial District and north of the South Beach neighborhood.<sup>3</sup> The irregularly-

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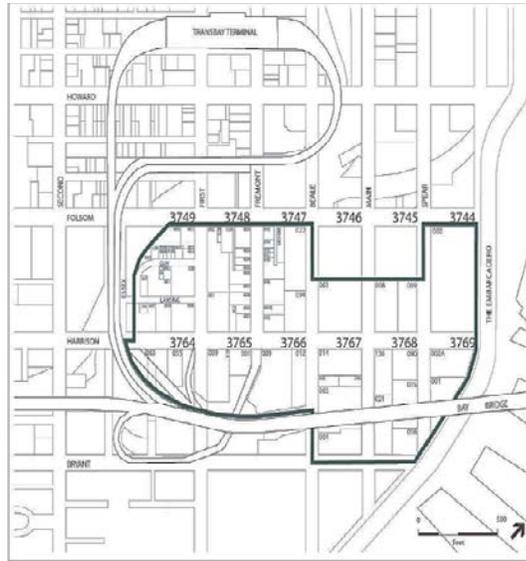
<sup>1</sup> The *Rincon Hill Streetscape Plan* (March, 2014) is available in Case File No. 2014.0925E for public review at the Planning Department 1650 Mission Street, Suite 400 in San Francisco. The document is also available on the internet at <http://www.sfplanning.org/>

<sup>2</sup> The *Rincon Hill Plan Environmental Impact Report*, certified by the City Planning Commission on May 5, 2005 (Motion No. 17007) is available for public review in Case File No. 2000.1081E at the Planning Department 1650 Mission Street, Suite 400 in San Francisco. The document is also available on the internet at <http://www.sf-planning.org/sfceqadocs>

<sup>3</sup> By convention, Folsom Street and streets parallel to it, including Harrison and Bryant Streets are considered east-west streets, while Steuart Street and streets parallel to it are considered north-south streets. The street grid in Rincon Hill and more broadly in the South of Market Area in general does not conform to

shaped Plan area is bounded generally by Folsom Street, Steuart Street, The Embarcadero, Bryant Street, Beale Street, the Bay Bridge approach, and the Transbay Terminal ramps (see Map 1, inset). It contains 55 acres of land, is subdivided into over 70 parcels, and consists of portions of Assessor Blocks 3749, 3764, 3765, and 3766, and all of Assessor's Blocks 3744, 3745, 3746, 3747, 3748, 3767, 3768, and 3769.

**Figure 1 Rincon Hill Plan Area**



### **Plan Goals and Objectives**

The Rincon Hill Plan includes area-wide policies, zoning controls and financing mechanisms to foster a dynamic, mixed-use neighborhood. An overarching goal of the Plan is to increase housing production in a former industrial area, which the Plan facilitates by permitting high-density, high-rise residential buildings that would be moderated by policies balancing residential density with livability. For example, Plan policies address solar access and air through tower separation requirements; others call for the creation of attractive and livable streets and open spaces in a neighborhood envisioned as easily accessible to local shops and services. Rincon Hill Plan policies also call for improving the public realm by providing local amenities and enhanced open spaces to serve new residents and visitors to the district. The Plan's policies and objectives, which focus primarily on housing, urban design, traffic and circulation and the public realm are summarized below:

- **Housing:** through attendant Planning Code and General Plan amendments, the Plan created capacity for 2,220 new housing units, including affordable units. When the Plan was adopted in 2005 a number of applications (“pipeline projects”) were under review at the Planning Department. Thus, housing in the development pipeline at the time the Plan was adopted is added to new capacity created by the Plan, yielding a total of 5,530 residential units at build out.

---

the cardinal directions (north, south, east and west), but the cardinal directions may be used in this document for ease of description.

- **Urban design:** addresses the spatial and functional characteristics of new buildings, streets, sidewalks and other physical infrastructure comprising the built environment. The Rincon Hill Plan's policies shape the character of neighborhood streets to reflect their primary use and function. On commercial streets such as Folsom Street serving both Rincon Hill and Transbay neighborhoods, the Plan requires transparent storefronts at the ground-floor of buildings to encourage a pedestrian-friendly commercial streetscape; along streets intended to be more residential in character, the Plan requires building setbacks and stoops as transitional landscaping elements separating the public street from the private residence. The Plan also prescribes detailed bulk and massing regulations to ensure that residential towers would mark the foot of the Bay Bridge and appear slender on the City's skyline. Standards for tower bulk and spacing maximize light and air within the district, and preserve views to and from downtown and the Bay Bridge.
- **Public Realm:** The Plan implements the Rincon Hill Community Infrastructure Impact Fee, which assesses an \$11 per square foot fee to pay for widened tree-lined sidewalks on Main, Beale, Spear, Fremont, Harrison, and First Streets, public pedestrian mid-block pathways, a community center at the Sailor's Union for the Pacific Building, and new neighborhood parks. The fee has generated an excess of \$22 million to date for community infrastructure.
- **Traffic and Circulation:** The Plan acknowledges the traffic and circulation impacts associated with the intensification of residential uses in Rincon Hill and calls for improvements to area traffic flow, including traffic-calming on residential streets, planted medians, and signage to smooth neighborhood and regional traffic movement. The Plan requires all parking to be underground, and limits residential parking to up to a maximum of 1 car per 2 units "by right", or up to 1 car per unit if parking spaces are stored with mechanical stackers or valet.

### **Streetscape Master Plan**

The Rincon Hill Streetscape Master Plan is a detailed street and open space plan that reflects the public realm and circulation policies in the Rincon Hill Plan in addition to the requirements of Planning Code Sections 309.1 and 827. The Streetscape Master Plan would involve implementing the following:

- applying “Living Streets” treatments on Spear, Main, and Beale Streets that would both calm traffic and provide substantial open space amenities. Traffic calming is intended to facilitate a pleasant and safe residential, pedestrian, and bicycling environment. Leveraging newly landscaped streetscapes as pleasant, multi-functional spaces for pedestrians is intended to address the existing deficit of open space and relative lack of greenery in the Plan area.
- improving pedestrian conditions at intersections, particularly near freeway ramps;
- widening narrow sidewalks on Fremont, First, and Harrison Streets to the greatest extent feasible; and
- separating bridge-bound traffic from local traffic on First Street and segregating local traffic and peak hour transit lanes on Harrison Street.

The Plan provides details for individual streetscape elements, including dimensional, material, functional, construction and procedural requirements. Detailed descriptions of the following streets are included in the Streetscape Master Plan for: Harrison Street, Spear Street, Main Street, Beale Street, Fremont Street, First Street, Guy Place, Lansing Street and Grote Place. The Plan articulates current roadway configuration, foreseeable changes to the street segment as envisioned by the Rincon Hill Plan and includes dimensioned drawings illustrating the changes in context. Further, the plan contains guidance on a variety of other design elements that pertain to corner bulbouts and curb extensions; street trees and vegetation (e.g., “living street open space panels”); street furnishings and amenities (e.g., bicycle racks, benches, trash receptacles and the like) as well as street lighting, paving and utilities. The design elements that are applied to specific locations in the Plan area are illustrated in **Table 1**, below.

## **ENVIRONMENTAL REVIEW**

The California Environmental Quality Act (CEQA) requires state and local agencies, such as the San Francisco Planning Department to identify the significant environmental impacts of their actions and to avoid or mitigate those impacts, if feasible. The following is a summary of the Rincon Hill Plan’s environmental review, including a discussion of the public realm and street network changes analyzed in the Plan, and the reasons why no subsequent review is necessary for the Streetscape Master Plan.

**Table 1: Public Realm and Street Network Improvements**

Street Segment	Side	Cross Streets	Corner Bulbs	Mid Block Bulbs	Sidewalk Widening	Proposed Sidewalk Width <sup>(1)</sup>	Special Sidewalk Paving	Street Trees	Sidewalk Landscaping	Permeable Paving	Street Furniture <sup>(2)</sup>	Living Street Pod
Harrison Street	N	Essix - Embarcadero	Yes, all corners except at Embarcad	N	Y	12'	Y	Y	Y	M	Y	N
Harrison Street	S	Essix - Embarcadero	Yes, all corners	N	Y	12	Y	Y	Y	M	Y	N
Spear Street	E	Folsom - Harrison	Yes, both sides of block	Y	Y	15'	Y	Y	Y	M	Y	N
Spear Street	W	Folsom - Harrison	Yes, Folsom corner only	Y	Y	31.5'	Y	Y	Y	M	Y	Y
Spear Street	E	Harrison - Embarcadero	Yes, Harrison side only <sup>(4)</sup>	N	Y	15'	Y	Y	Y	M	Y	N
Spear Street	W	Harrison - Embarcadero	Yes, Harrison side only <sup>(4)</sup>	N	Y	31.5'	Y	Y	Y	M	Y	Y
Main Street	E	Folsom - Harrison	Yes, Harrison side only	Y	Y	15'	Y	Y	Y	M	Y	N
Main Street	W	Folsom - Harrison	Yes, Folsom side only	N	Y	28.5'	Y	Y	Y	M	Y	Y
Main Street	E	Harrison - Bryant	Yes, Bryant side only	Y	Y	15'	Y	Y	Y	M	Y	N
Main Street	W	Harrison - Bryant	Yes, Harrison side only	Y	Y	28.5'	Y	Y	Y	M	Y	Y
Beale Street	E	Folsom - Bryant	Yes, Bryant Side only	Y <sup>(5)</sup>	Y	24'	Y	Y	Y	M	Y	Y
Beale Street	W	Folsom - Bryant	Yes, Folsom side only	N	Y	15'	Y	Y	Y	M	Y	N
Fremont Street	E	Folsom - Harrison	Yes, both sides of block	N	Y	15'	Y	Y	Y	M	Y	N
Fremont Street	W	Folsom - Harrison	Yes, both sides of block	N	Y	15'	Y	Y	Y	M	Y	N
First Street	E	Folsom - Harrison	Yes, both sides of the block	N	Y	10'-15' <sup>(3)</sup>	Y	Y	Y	M	Y	N
First Street	W	Folsom - Harrison	Yes, Folsom side only, none at HARR	N	Y	10'	Y	Y	Y	M	Y	N
First Street	E	South of Harrison	Yes, at Harrison <sup>(6)</sup>	N	Y		Y	Y	Y	M	Y	N
First Street	W	South of Harrison	None	N	Y		Y	Y	Y	M	Y	N

**Notes**

- 1 Measured from property line to curb face
- 2 Benches, Bollards, Tree Grates, Bike Racks, Garbage Cans Etc
- 3 1st Street from Folsom to Harrison sidewalk width: East side of the street shall be 15 feet to face of curb, transitioning to 10 feet south of Lansing Street.
- 4 Street deadends at Embarcadero, so there are no potential corner bulb-outs at the south side of the block.
- 5 Bulb is located between Street and Harrison Street overpass
- 6 Street deadends mid-block south of Harrison Street, no corner bulb-outs possible on the south side of this street segment

**Relationship to the Certified Rincon Hill Plan EIR**

The Planning Department prepared and circulated a draft environmental impact report (DEIR) for the Rincon Hill Plan on September 25, 2004. This EIR analyzed the physical environmental impacts of implementing the plan. It also considered the plan’s effects in combination with reasonably foreseeable future projects in order to disclose potential cumulative impacts. The Planning Commission certified the EIR on May 5, 2005.

The Rincon Hill Plan EIR is a program EIR. A program EIR may be “prepared for a series of related actions that are characterized as one large project or program” (CEQA Guidelines §15168). The EIR notes that:

while the EIR analysis is based on assumed development and activity that could occur pursuant to the Rincon Hill Plan, individual projects that may be proposed in the future under the Rincon Hill Plan would undergo project-level environmental review to determine whether they could generate further impacts specific to their site, time and configuration. The proposed plan that is the subject of this EIR is not a development proposal, and, while it would facilitate construction of certain projects, the only specific improvements proposed are in the area of streetscape enhancement and open space. Therefore, the EIR's analysis of physical changes in the environment is based on the assumptions about future development that could occur under the plan (p. S-2).

Therefore, the EIR approaches the analysis of environmental impacts based on a description of the anticipated physical changes that plan adoption could permit. The EIR reviewed three plan variants at an equal level of detail. The main differences in these plan alternatives relate to the number, height and separation of towers that the plan could ultimately allow.

Regarding the streetscape improvements, the EIR clarifies that:

Other than controls relating to height and bulk, and location and number of towers, the controls and zoning changes in the proposed Rincon Hill Plan would apply uniformly to all options; that is, permitted uses, maximum residential density, open space, parking and loading requirements, and proposed street changes and public realm improvements would be the same under all options, regardless of the height and bulk controls ultimately implemented (p. S-2)

### **Public Realm Improvements Identified and Analyzed in the Rincon Hill EIR**

The Rincon Hill Plan's streetscape and public realm components are described in the Rincon Hill Plan EIR as follows:

A comprehensive streetscape and open space plan is proposed under all options, with sidewalk widenings, tree plantings, street furniture and the creation of new public spaces along streets throughout the district, intended to create an inviting and vibrant public realm. Under the Draft Plan, new development would be required to implement portions of the streetscape plan as a condition of approval, with potential additional funding from a neighborhood benefit district for both the streetscape plan and for parks and open space and other public amenities.

Seventy-five square feet of usable open space would be required for every dwelling unit, and one square foot of public open space would be required per 50 square feet of non-residential uses. The Draft Plan calls for open space funds collected as part of an assessment district, Mello Roos district, or other means to be directed to purchasing and improving as public open space the parcel adjacent to the Fremont Street off-ramp at Harrison Street, and the implementation of the sidewalk treatments along Plan area streets. Additional open space would be created along Essex Street, including the hillside and useable space at the top of the hill along Guy Place and Lansing Street. Mid-block pedestrian pathways, proposed in the existing Rincon Hill Area Plan, would continue to be included, with some alterations.

The Draft Plan calls for a number of changes in the configuration of streets to shift the street environment from one built almost entirely around vehicular movement to one that is more accessible and accommodating to pedestrians. On First Street, the Plan calls for retaining four lanes but narrowing the easternmost lane by up to 5 feet and widening the adjacent sidewalk, and building raised landscaped medians south of Lansing Street, where there are currently striped medians, to prevent drivers from jumping center-lane bridge queues by using side lanes meant for local access.

Fremont Street would lose one southbound lane, for a resulting configuration of one southbound and two northbound lanes; the east sidewalk would be widened. On Harrison Street, the eastbound lane is to be narrowed from 18 to 12 feet, to add space to the north sidewalk and realign the westbound lanes. The Draft Plan proposes “soft-hit” stanchions between the two northernmost lanes to prevent drivers from jumping center-lane bridge queues by using side lanes meant for local access.

Main, Beale, and Spear Streets would be converted to “living streets,” with two-way operation (one 11-foot lane in each direction), priority given to pedestrian activity and open space over auto needs, a parking lane on either side, widened sidewalks, street trees and pocket open spaces. Guy Place and Lansing Street would be converted to pedestrian-priority streets, but no change is proposed to the number of lanes or allowable auto access.

The Draft Plan encourages walking as the primary way that people living in Rincon Hill and Transbay will move about due to the proximity to downtown, regional transit hubs at the Transbay Terminal, Muni Metro and BART below Market Street, and the Ferry Building, and the anticipated development of a neighborhood retail center focused on Folsom Street. However, the Plan also

recognizes that there is limited transit service between the Rincon Hill/Transbay neighborhood and the rest of San Francisco, and therefore recommends evaluation of a series of potential Muni improvements achievable in the short term to link the Plan area with the remainder of the City, including potentially extending certain Muni lines, increasing service on others, and creating late night service to the area. No funding has been identified for these changes. (pp. S-6 and S-7)

Activities which relate to and follow the specific plan must be examined in light of the program EIR to determine if additional limited environmental analysis is warranted. Later activities which have been adequately analyzed under the program EIR will not require additional environmental documentation. If an activity may result in additional effects, or new mitigation measures are needed, a subsequent or supplemental EIR, or negative declaration must be prepared (CEQA Guidelines §15162 and 15163). As indicated in Table 1, the current Streetscape Plan does not differ from the description of the types of improvements to the public realm envisioned in the Rincon Hill Plan and studied in the Rincon Hill Plan EIR.

### **Consideration of Cumulative Effects**

Cumulative impacts refers to two or more individual effects which, when combined together, are considerable or which compound or increase other environmental impacts. The individual effects may be changes resulting from a single project or a number of separate projects. (CEQA Guidelines §15355) The EIR considered implementation of the plan in combination with other potential projects and assessed whether the components of the Rincon Hill Plan could result in considerable cumulative impacts. The cumulative impact from several projects is the change in the environment which results from the incremental impact of the project when added to those of other closely related past, present, and reasonably foreseeable probable future projects. Cumulative impacts can result from individually minor but collectively significant projects taking place over a period of time. (CEQA Guidelines §15355(a)(b))

The EIR considered the approximately 2,200 new residential units that plan policies and Planning Code amendments would permit in the Plan area, in addition to the approximately 1,565 units that were under construction in Rincon Hill and the 1,595 units approved but not yet under construction (e.g. pipeline projects) for a total of 5,350 residential units. The Plan EIR considered the potential for cumulative impacts by analyzing physical changes in the environment that may be caused by new housing,

commercial uses, public facilities and infrastructure improvements foreseeable under the plan. At the time of Plan adoption, a specific Rincon Hill Streetscape Plan had been contemplated though not prepared. The type, character and location of the infrastructure elements required to implement the public realm improvements are articulated in Section 5 of the Plan in specific objectives and policies related to reconfiguration of streets, pedestrian and bicycle improvements, transit service, and parking management. These policies were also considered in the EIR's background studies and impact analyses.

Additionally as part of the Plan's enabling legislation, the Board of Supervisors adopted Planning Code Section 138.1(D)(i) and (ii), which set forth the requirements and parameters directing project sponsors to install streetscape improvements for any new construction; for construction equal to 20 percent or more of an existing building; or for alterations greater than 50 percent of the existing square footage of a building. The Planning Code further indicates that prior to the adoption of a Streetscape Plan for Rincon Hill, those aforementioned streetscape elements shall be "in keeping with the intent of the Rincon Hill Area Plan... and in accordance with... the Planning Code" (Section 138.1(2)(ii)).

## **Environmental Impacts**

The EIR found that implementation of the Plan would result in project-specific significant unavoidable impacts on the environment by: 1) adding traffic and changing street configurations which will cause significant unavoidable adverse traffic impacts at the Fremont/Harrison, First/Market, Embarcadero/Folsom intersections; 2) contributing considerably to significant cumulative traffic impacts at the First/Market and First/Folsom intersections (EIR Impacts C.1a through C.1c); and 3) encouraging and facilitating the loss of the Union Oil Company Building and potentially the Edwin W. Tucker Company Building, the 375 Fremont Street building, as well as other architectural resources in the Plan Area, resulting in potentially significant adverse impacts on historic architectural resources (EIR Impact I.2a through I.2d).

The Rincon Hill EIR states that the Plan would

mitigate the traffic and circulation impacts by providing extensive pedestrian, traffic-calming and other streetscape improvements that will make it attractive to residents to make as many daily trips as possible on foot, by bicycle or on transit. A comprehensive program of new public infrastructure is necessary to mitigate the impacts of the proposed new development and to provide these basic community improvements to the area's growing residential population.

## **Mitigation Measures**

The Rincon Hill EIR identifies mitigation measures to reduce, where feasible, significant environmental impacts that implementation of the Plan could cause. Mitigation measures have been identified for the following topical areas: construction *noise; traffic and circulation* (primarily lane reductions, signal timing, turning restricts); abating construction-related dust and reducing the severity of regional air pollutants (by providing services and infrastructure to encourage great transit use); *wind; hazardous materials*; as well as *archeological resources* and *historic architectural resources*.

The implementation of the streetscape project would not change the type, number or intensity of impacts that were not already disclosed in the Rincon Hill EIR. All mitigation measures identified for potentially significant impacts in the environmental impact report would be required in the current context except for cases in which measures have been superseded by adopted ordinance. In this case, mitigation measures related to dust suppression, operational air quality, and hazardous materials have subsequently been superseded by legislation requiring the same or greater levels of environmental protection as called for by the EIR's mitigation. No new mitigation is required.

## **Approval and Adoption**

On May 5, 2005, the Planning Commission certified the Rincon Hill Plan EIR as complete (Motion No. 17007). On August 2, 2005 the Board of Supervisors approved the plan and on August 19, 2005 the Mayor signs the Rincon Hill Plan into law.

## **CONCLUSION**

As described in the foregoing memorandum, the program EIR for the Rincon Hill Plan EIR adequately addressed all impacts of the Rincon Hill Streetscape Plan. The current Streetscape Plan represents a refinement to the streetscape concepts described in the Rincon Hill Plan and would not have any additional significant adverse effects not examined in the program EIR, nor has any new or additional information come to light that would alter the conclusions of the program EIR. Moreover, no substantial changes have been made to the streetscape project or Plan since certification of the FEIR, nor have there been any substantial changes in circumstances necessitating revisions to the FEIR, nor has any new information of substantial importance come to light that raises one or more of the above issues.

San Francisco Administrative Code Section 31.19(c)(1) states that a modified project must be reevaluated and that "If, on the basis of such reevaluation, the Environmental Review Officer determines, based on the requirements of CEQA, that no additional environmental review is necessary, this determination and the reasons therefore shall be noted in writing in the case record, and no further evaluation shall be required by this Chapter." For the reasons articulated above, this memorandum provides sufficient documentation that no further environmental review is required for the Rincon Hill Streetscape Plan.



# SAN FRANCISCO PLANNING DEPARTMENT

**MEMO**

**DATE:** January 2, 2014

**TO:** Rincon Hill Plan  
Environmental Impact Report, case 2000.1081E

**FROM:** Greg Riessen, Environmental Planning

**RE:** Note to File: Modifications to Harrison Street

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## Background

The Rincon Hill Plan and its associated streetscape plan were finalized in 2006. One of the goals of the streetscape plan has been to implement corner bulb-outs to calm traffic and reduce pedestrian crossing distances at intersections throughout the Rincon Hill area.

At the time of the report preparation in 2006, the parking lane on the north side of Harrison Street between the Embarcadero and First Street was utilized as a curbside transit-only lane during the PM peak period, which served the Muni 12 Folsom transit route. Due to this tow-away lane, the original draft of the Rincon Hill streetscape plan did not include corner bulb-outs on the north side of Harrison Street between Spear and 1<sup>st</sup> streets

In early 2010, the SFMTA rerouted the 12 Folsom line out of the Rincon Hill area, and the route no longer runs along Harrison Street east of 2<sup>nd</sup> Street. The peak-hour transit-only lane has been converted into a standard parking lane.

Because the north curb of Harrison Street no longer accommodates a tow-away lane, and is now a standard parking lane, the Rincon Hill streetscape plan has been amended to add corner bulb-outs where feasible.

## Additional Bulb-outs

The amended plan calls for an additional seven bulb-outs to be installed on the north side of Harrison Street. These bulb-outs are proposed at:

<b>Table 1: Bulb Location</b>
Harrison Street at Spear Street – NE corner
Harrison Street at Spear Street – NW corner
Harrison Street at Main Street – NE corner
Harrison Street at Main Street – NW corner
Harrison Street at Fremont Street – NE corner
Harrison Street at Fremont Street – NW corner
Harrison Street at First Street – NE corner

These bulbouts would be required to meet Department of Public Works design standards. Also the bulbouts would be reviewed by the San Francisco Fire Department (through the Transportation Advisory Staff Committee) to ensure that emergency vehicle access is not hindered. SFMTA would also review the bulbouts to ensure that traffic and transit operations are not affected. Overall, the impacts would be similar to the impacts discussed for the other bulbouts that were proposed in the Rincon Hill Plan.

**Parking Analysis**

The bulb-outs would be approximately 6’ wide. In total, each bulb-out would be approximately 33’ long, which includes both the area within the crosswalk (where parking is not permitted) and the area in front or behind the crosswalk (where parking is usually permitted). Assuming that a parked vehicle, including maneuvering space, requires 20’ of curb, each corner bulb-out would conservatively remove approximately one parking space.

It should be noted that fire hydrants are installed at many of the proposed corner bulb-out locations. Fire hydrants typically have a 10’ – 15’ no-parking zone fronting the hydrant, which is about half of a parking space.

Moreover, one of the proposed bulb-out locations on the northeast corner of Harrison Street at Fremont Street currently does not have any on-street parking. Adding a bulb-out at this location would not result in a loss of any parking spaces.

Considering the above, the following table summarizes the approximate net loss of parking spaces adding bulb-outs at each of the proposed bulb-out locations.

<b>Table 2: Parking Removal for Bulb-outs</b>		
<b>Bulb Location</b>	<b>Approximate Parking Spaces Removed</b>	<b>Notes</b>
Harrison Street at Spear Street – NE corner	1	Fire hydrant
Harrison Street at Spear Street – NW corner	1	Fire hydrant
Harrison Street at Main Street – NE corner	1	Fire hydrant
Harrison Street at Main Street – NW corner	1	Fire hydrant
Harrison Street at Fremont Street – NE corner	0	No Parking spaces
Harrison Street at Fremont Street – NW corner	1	Fire hydrant
Harrison Street at First Street, on the – NE corner	2	

Factoring existing fire hydrants and parking configurations, these bulb-outs would necessitate the removal of approximately **seven** on-street parking spaces.

### **Parking Impacts**

The project would have a significant effect on the environment if it would result in a substantial unmet parking demand that could create hazardous conditions or significant delays affecting traffic, transit, bicycles or pedestrians and where particular characteristics of the project or its site demonstrably render use of other modes infeasible.

The proposed sidewalk bulb-outs would not generate any new demand for vehicular parking spaces. However, the project would remove approximately seven on-street parking spaces. This minor reduction in parking supply, relative to the supply of on-street and off-street parking in the project vicinity would be negligible. It should be noted that when the former transit lane was in operation, these seven spaces were not available during the PM peak demand period.

In summary, the corner bulb-outs would not result in a substantial loss that would create hazardous conditions or significant delays affecting traffic, transit, bicycles or pedestrians. Therefore, impacts related to parking would be **less than significant**.

# Rincon Hill

## STREETSCAPE MASTER PLAN



SAN FRANCISCO  
PLANNING  
DEPARTMENT

# DRAFT

San Francisco Planning Department  
March 2015

# Rincon Hill

## STREETSCAPE MASTER PLAN

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# 1.0 Introduction



## STREETS IN RINCON HILL

The new Rincon Hill Plan was adopted by the city and incorporated into the General Plan in August 2005. The Rincon Hill Plan contains a robust plan and detailed policies for streetscape and traffic changes as an integral part of the neighborhood's development. Besides being traffic-ways, some quite key to the city's regional traffic flows, the streets are an important part of the open space system in a very dense urban environment with limited opportunity for parks. These streets must also accommodate safe and gracious pedestrian and bicycle movement within the neighborhood. The key underlying goals that have shaped the Rincon Hill Streetscape and Traffic Plan are:

- Create "Living Streets" on Spear, Main, and Beale Streets, including calmed traffic and significant open space amenities. The calming of traffic is intended to facilitate a pleasant and safe residential, pedestrian, and bicycling environment, and the creation of lushly-landscaped streets with usable open space is necessary to augment the deficit of open green space in this dense urban area.
- Improve pedestrian conditions at intersections, particularly near freeway ramps.
- Widen narrow sidewalks on Fremont, First, and Harrison Streets to the greatest extent feasible.
- Separate bridge-bound traffic from local traffic on First Street and from local traffic and peak hour transit lanes on Harrison Street.

## APPROVAL PROCESS

All of the street and traffic changes described in this Plan were analyzed and covered by the Environmental Impact Report (EIR) of the Rincon Hill Plan, which was certified by the Planning Commission in 2005 prior to adoption of the Plan, favorably recommended by ISCOTT in January 2006 and approved by the MTA Board of Directors on May 30, 2006. This document was approved by the Planning Commission on XXXXXXX XXXXXX and the Board of Supervisors on XXXXXXX XX, 20XX.

## PURPOSE OF THE DOCUMENT

This document is necessary to implement the streetscape and circulation policies adopted in the Rincon Hill Plan of the General Plan, adopted in 2005. As such, this document is the basis for General Plan consistency determinations for all streetscape and right-of-way improvements (including traffic configurations) in the Rincon Hill area, whether implemented by the public or private sectors. This Rincon Hill Streetscape Plan is used as the basis for, and to determine the adequacy and appropriateness of, all streetscape improvements required by Section 309.1 and 827 of the Planning Code, mandated by the Planning Commission, or voluntarily installed. All the curblin and traffic designs described here were fully analyzed and adopted in the Rincon Hill Plan EIR and Plan approvals. The purposes of this document are to

- (1) provide a clear, easy-to-follow and detailed comprehensive plan for streetscape and circulation changes for the Rincon Hill area.

- (2) provide detailed guidelines and standards for the design of streetscapes, including curblines, landscaping, street trees, sidewalk bulbouts, lighting, paving, and street furniture.

## RELATIONSHIP TO TRANSBAY REDEVELOPMENT AREA

The Transbay Redevelopment area sits just to the north of Rincon Hill, on the north side of Folsom Street. The Planning Department and Redevelopment Agency have coordinated the planning of these two adjacent areas so that they will be built out as one coherent high-density residential neighborhood, and policies and controls have been coordinated for all relevant issues, including land use, building pattern, and streetscape design. The Transbay Redevelopment Project Area Streetscape and Open Space Concept Plan, approved by the Redevelopment Commission in November 2006, generally reflects the same basic configurations and streetscape standards as contained in this document. The details contained in this, the Rincon Hill Streetscape Plan, are the requirements and guidelines for Rincon Hill, but one can refer to the Transbay document for additional context and information purposes. Copies of the Transbay Streetscape document may be downloaded from the Redevelopment Agency's website at: [http://www.sfgov.org/sitelsfra\\_page.asp?id=5583](http://www.sfgov.org/sitelsfra_page.asp?id=5583).

## ORGANIZATION OF THE DOCUMENT

This document has two main sections:

- (1) **Individual Streets.** These pages outline the detailed streetscape and circulation design adopted for each street in Rincon Hill. The text includes a general descrip-

# 1.0 Introduction



tion of the present right-of-way configuration and approved Rincon Hill Plan configuration, as well as a detailed accounting of all the curblines and bulbout locations and measurements. Both cross-sections and plan views are included to show the organization of the street and placement of streetscape elements. Where appropriate, references are given to other pages in the document where details may be found on related specifications.

- (2) **Streetscape Element Standards and Implementation Requirements.** This section provides details for individual streetscape elements, including any dimensional, material, functional, construction or procedural requirements.

## STREETSCAPE PLAN IMPLEMENTATION

The streetscape improvement show in this document will be implemented over time incrementally, through multiple mechanisms and funding sources:

1. **Developer Requirements:** Per planning code Section 138.1 (c)(2), developments exceeding certain size thresholds described therein must build out the streetscape improvements, including sidewalk widening and all elements as a basic zoning requirement.
2. **Developer In-kind Construction:** In lieu of paying some or all of required Rincon Hill impact fees, projects can propose to build streetscape improvements in excess of what is required by Planning Code Section 138.1
3. **City Construction:** Using available funds from some combination of impact fees an infrastructure financing district (IFD), or other funds (e.g. grants, general fund), the City would undertake improvements

All descriptions of physical elements in this document are required to be built out as specified herein, including dimensions, materials, installation methods, and locations. Some minor variation may be necessary or desirable due to unique or unforeseen circumstances, as well as to accommodate piecemeal and gradual buildout of the district's streetscapes over time. All streetscape implementation is subject to the approval and Plan consistency finding of the Planning Department. The Department of Public Works is the permitting agency for improvements within the public right-of-way and all applications and plan submissions must meet DPW submittal requirements. All technical specifications not described in this document must meet pertinent City standards and are subject to detailed design review and approval by DPW and other relevant agencies.

All existing streetscape elements, including traffic signals, parking meters, signage, and utility boxes must be relocated to conform to the alignments and configurations described in this Streetscape Plan.

All of the specific curblines and traffic changes have been approved in detail by the MTA Board of Directors on May 30, 2006 in Resolution 06-066. All changes to curblines must be legislated by the Board of Supervisors, and this is typically done when construction drawings are completed and coordinated with the Department of Public Works Bureau of Street Use and Mapping (DPW BSM). Project sponsors implementing these new sidewalk and curblines must apply through DPW BSM and legislation will be submitted to the Board. Because the MTA Board and Planning Commission have already approved the changes in concept via this Streetscape Plan, the curblines legislation process is merely procedural, but necessary to implement an incremental build-out of streetscapes across the neighborhood.

## UTILITIES AND VAULTS

### New

It is Project Sponsor's responsibility to ensure minimal impact or interference from any utilities (e.g. sidewalk vaults for electric power transformers or switches) with required streetscape treatments, particularly street tree planting and planter bed landscaping. The location and design of electric and other utility servicing needs must be considered in the architectural design phase of the project. Any sidewalk vaults must be placed either wholly within the clear walking sidewalk surface between the building edge and the inner edge of landscaping beds and tree basins or in naturally occurring breaks in planter beds as described for each street in this document. The preferred location for electric vaults is within the driving or walking surface of driveways, alleyways or walkways on the project property. Proposals that require significant elimination of street trees or landscaping due to utilities will not be considered favorably and approval will be delayed.

### Existing

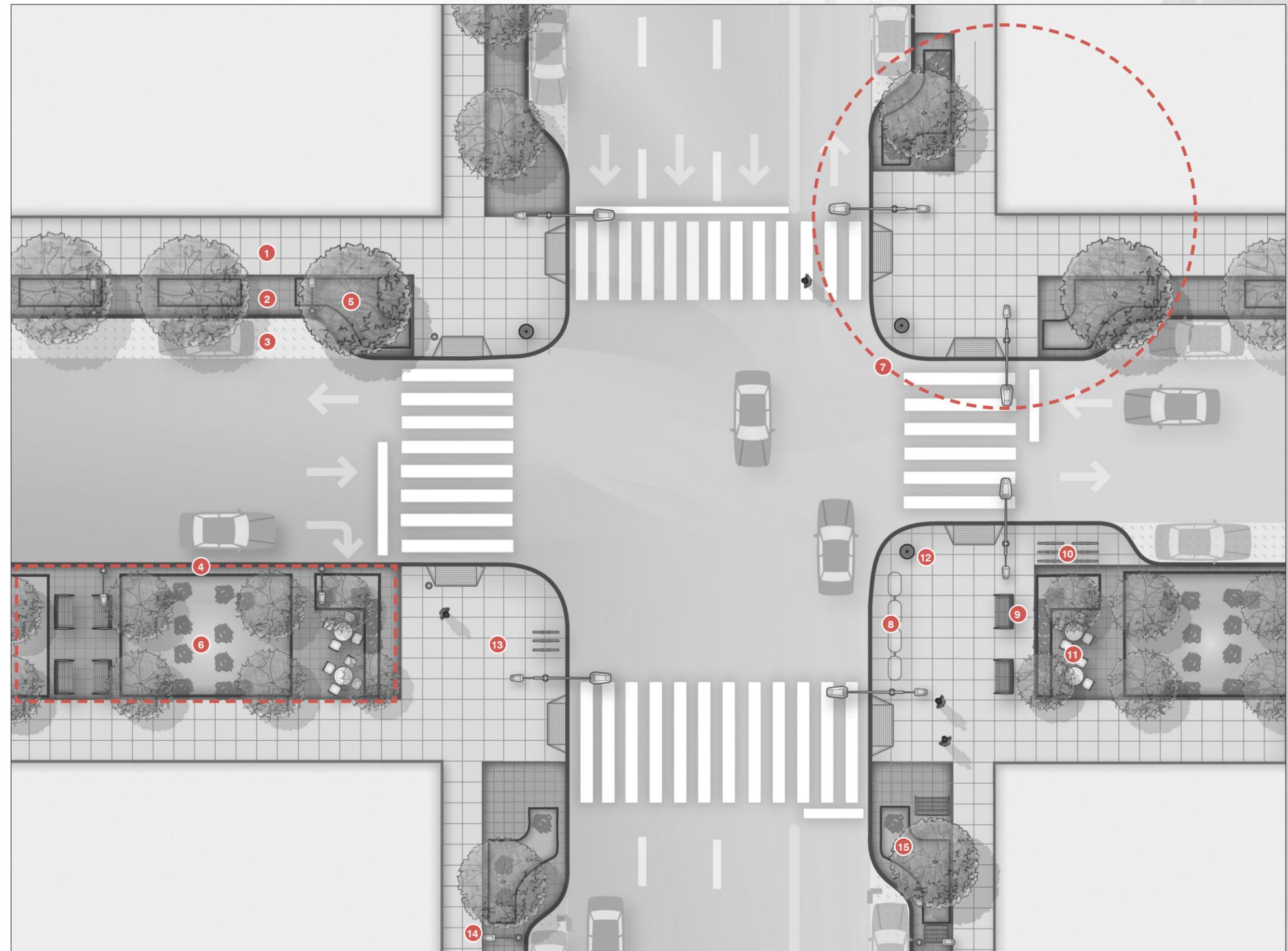
There are numerous sub-grade utilities and vaults (water, sewer, power, telecommunications) within the existing right-of-ways. The implementation of the curblines and other streetscape elements required in this document will, in some cases, require some relocation or alteration of existing utilities. Per requirements of DPW, PUC, or other agencies, project sponsors are required to carry out any and all utility relocations or modifications as necessary. Any variation from the curblines and standards contained in this document proposed by project sponsors in order to avoid modifications of existing utilities may only be considered and approved in consultation with and at the discretion of the Planning Department.

# 1.0 Street Plans

The diagram at right, along with the associated key below, is intended to help identify streetscape features for all subsequent street plans shown on pages 3 - 19.

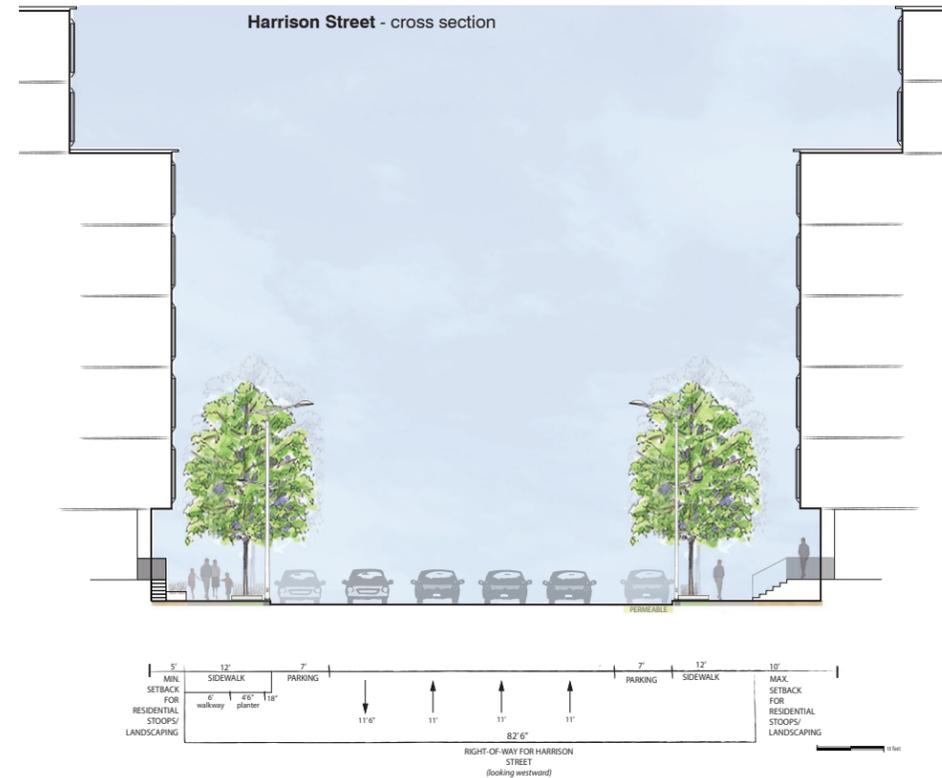
## KEY

1. 3' x 3' saw-cut concrete
2. 6" x 6" granite pavers
3. Permeable pavers
4. Living Street Open Space Panel
5. Street Tree
6. Understory plantings
7. 6' x 15' Bulbout
8. Newspaper rack
9. Benches
10. Bike rack
11. Cafe seating
12. Trash can
13. Traffic/Pedestrian light
14. Pedestrian light
15. Planter



# 1.1 Harrison Street

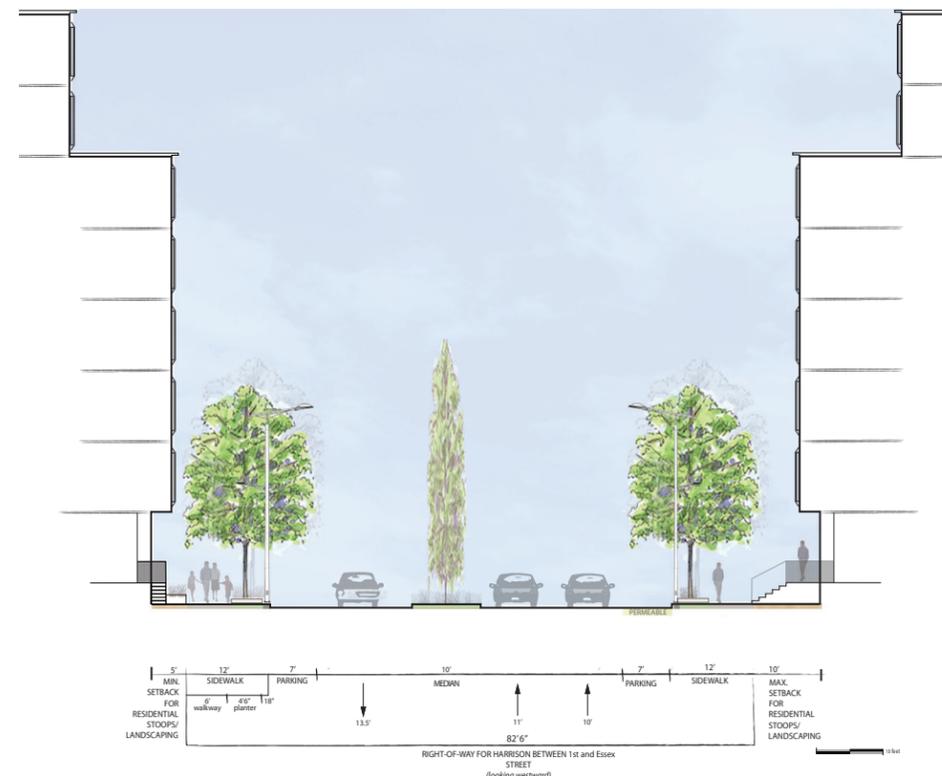
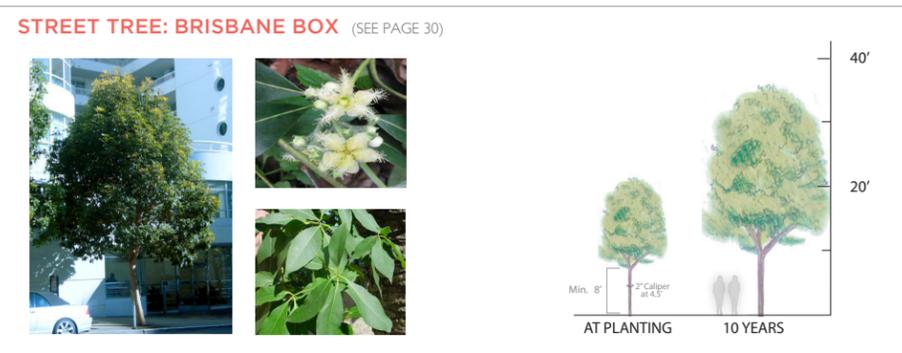
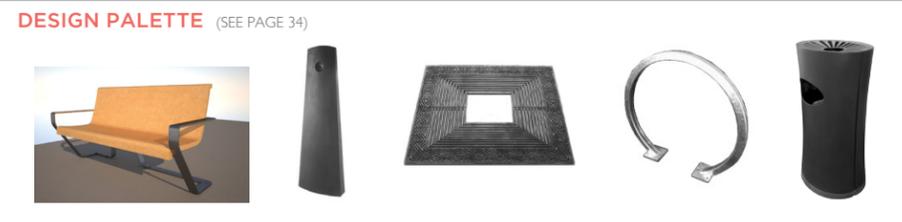
Harrison Street is a fairly heavily trafficked and auto-dominated street associated with three Bay Bridge ramps: two on-ramps (at Essex and at First Street) and one off-ramp (at Fremont Street). Westbound afternoon peak hour traffic feeding the First Street on-ramp is particularly heavy. The pedestrian realm is currently bleak, with narrow 8' sidewalks (and narrower in some places). However, traffic lanes are excessively wide, especially the much more lightly used eastbound lane, which allows some marginal room for widening sidewalks. Several major developments, including some ground floor residential townhouses, will line Harrison west of the Beale Street overpass. Additionally, the primary site identified for a public park on Rincon Hill sits along Harrison Street, just east of the Fremont Street off-ramp, making improvements to the pedestrian realm and safety imperative.



**Roadway:**  
*Current:* Three traffic lanes westbound, one eastbound. Curbside parking on both sides.  
*RH Plan:* (Embarcadero to Essex) All lanes narrowed. Curbside parking lane on both sides.  
 (First to Essex) Eliminate one westbound lane for a total of two lanes westbound and one eastbound. Create a 10'-wide landscaped median.

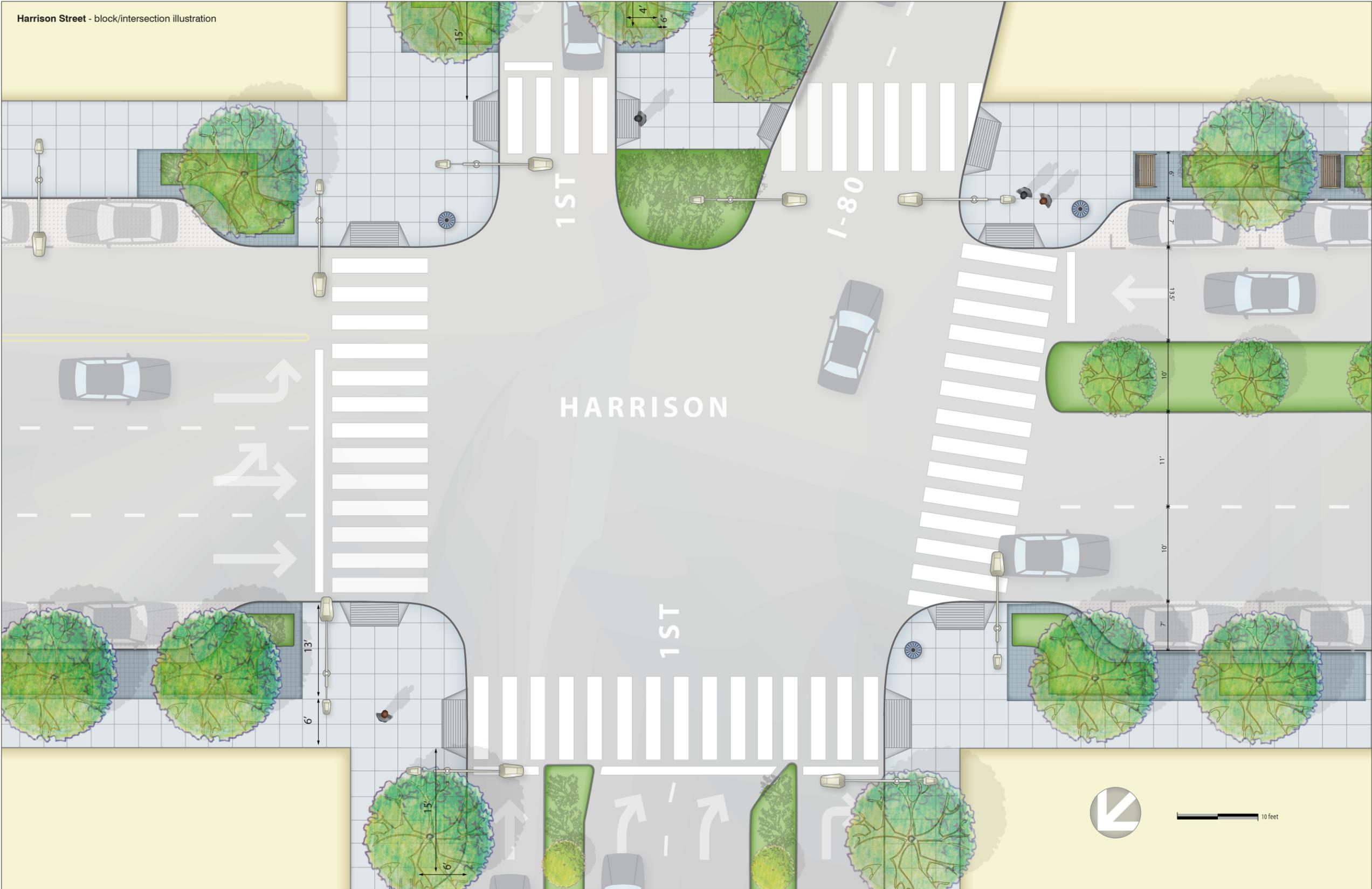
**Sidewalks:**  
 Both sides of the street shall be 12 feet to face of curb.

**Bulbouts:**  
 All corners all corners at all intersections. except SW corner at Fremont Street.



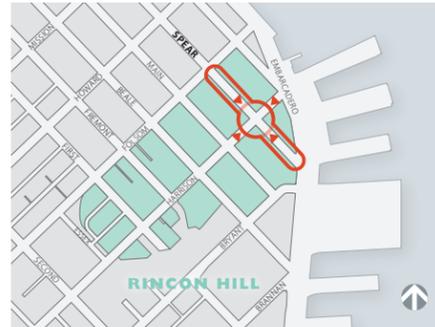
Harrison Street - block/intersection illustration





# 1.2 Spear Street

The Rincon Hill Plan contains explicit policies to narrow the width of the trafficways on Spear, Main and Beale Streets south of Folsom Street by reducing the number of traffic lanes and their width, allowing for one lane in each direction at all times but the peak hour, and transforming them into “Living Streets.” The primary goal of Living Streets is to prioritize pedestrian activity and usable open space over traffic and to calm traffic.



The basic design strategy of the Living Streets is to significantly widen the pedestrian space on one side of each street in order to create sufficient space for open space amenities such as pocket parks, seating areas, community gardens, dog runs, public art, and the like. This proposal is coordinated as “one neighborhood” with the Transbay area, just across Folsom Street, so that these Living Streets will form linear parks stretching from

Mission Street through both districts to the Embarcadero. Rincon Hill will be a very dense neighborhood and opportunities for traditional “park” space are highly limited; the Living Streets will fill part of this need.

A mid-block crosswalk will also be created to allow pedestrians to cross safely on these long blocks and connect to a system of interior mid-block paths.

### DESIGN PALETTE (SEE PAGE 34)



### STREET TREE: LITTLE LEAF LINDEN (SEE PAGE 29)



### Roadway:

*Current:* Three lanes southbound. Curbside parking both sides, with perpendicular parking south of Harrison.

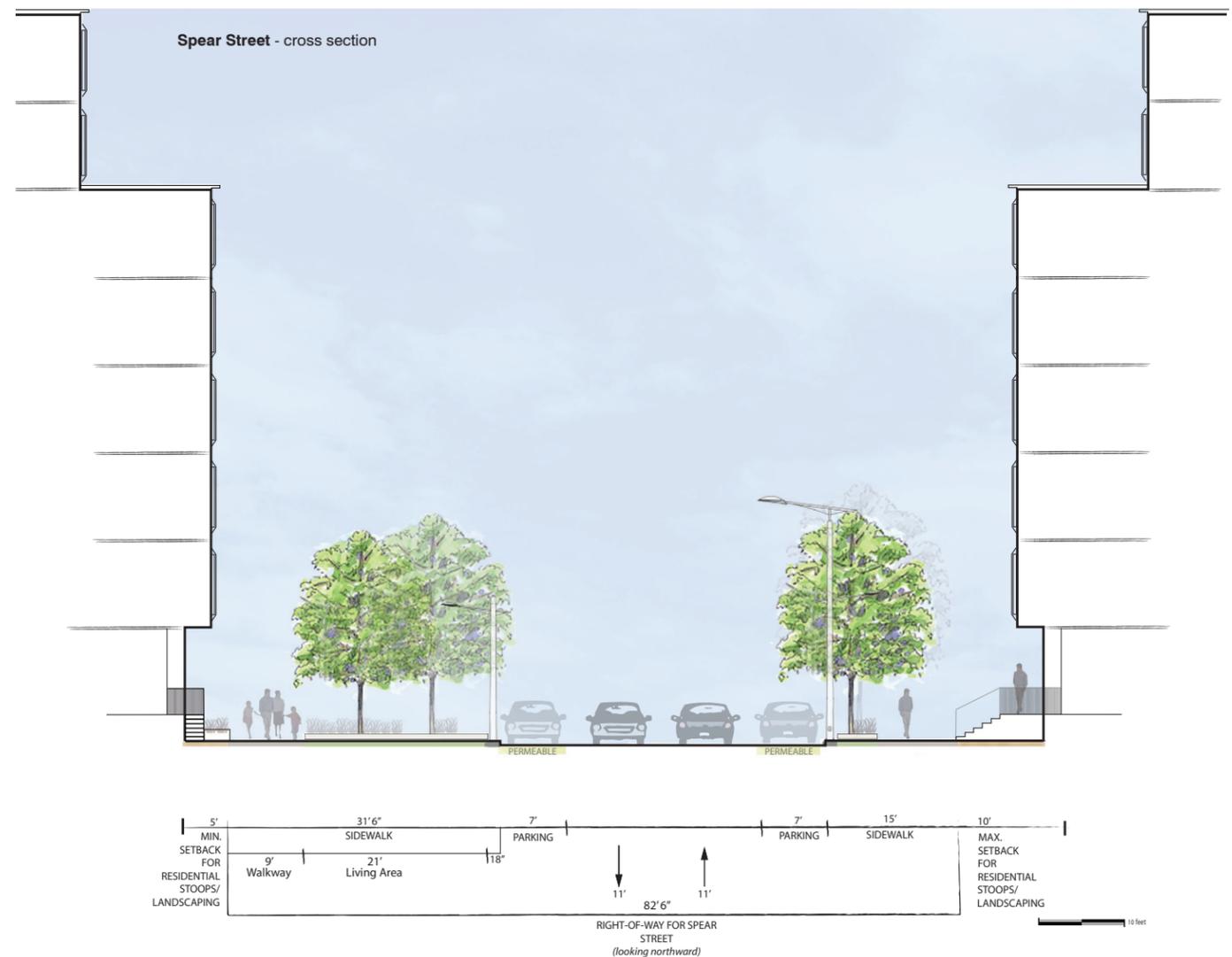
*RH Plan:* One lane each direction. Curbside parking both sides, all parallel. Permanent curbside right-turn pocket 100' in length in lieu of parking and bulb-out southbound at Harrison.

### Sidewalks:

West side shall be 31 feet 6 inches to face of curb.  
East side shall be 15 feet to face of curb.

### Bulbouts:

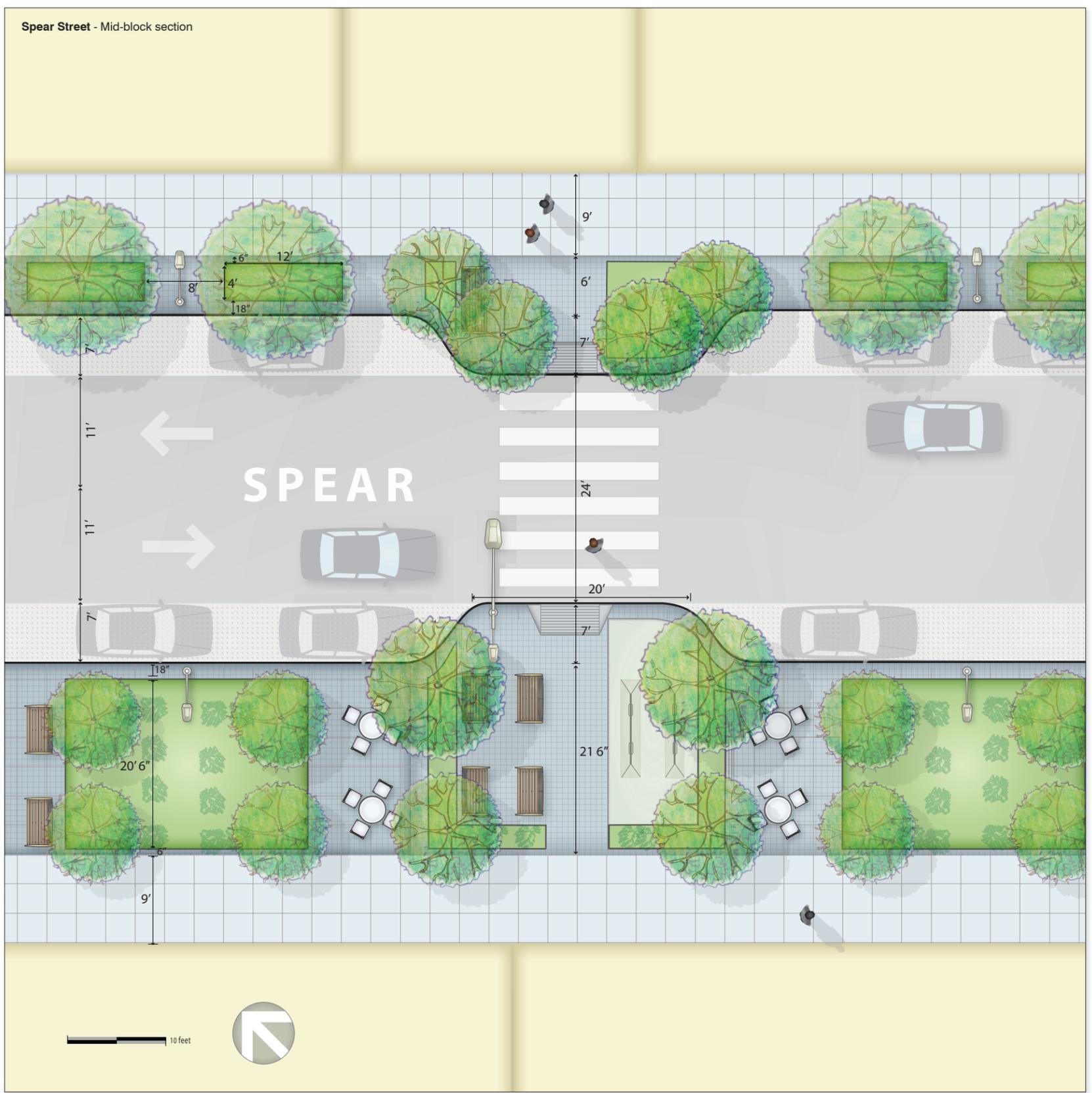
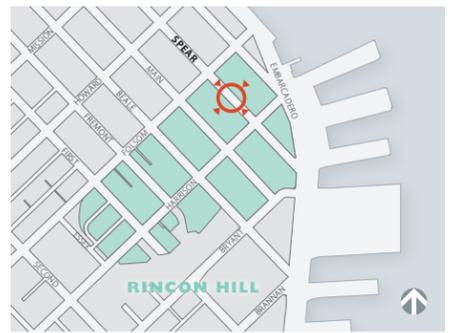
All corners except west side from Harrison Street northerly.  
Mid-block; both sides, from 250 feet to 280 feet south of Folsom Street



Spear Street - block/intersection illustration

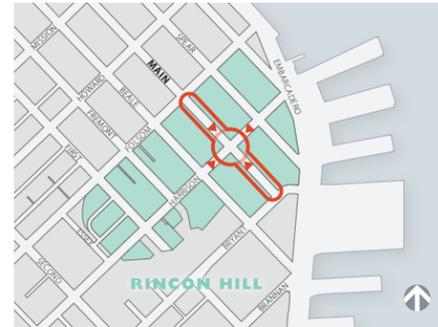


# 1.2 Spear Street



# 1.3 Main Street

Main Street will have an almost identical Living Street configuration to Spear Street, with a couple small, but notable differences. Main Street features heavier southbound peak hour freeway-bound traffic which turns east on Harrison. To allow the sidewalk and open space to be created while maintaining greater capacity in the peak hour when it is needed, a southbound towaway curbside lane will be created.



**Roadway:**

*Current:* Two lanes southbound and one northbound. Curbside parking both sides, with perpendicular parking south of Harrison.

*RH Plan:* One lane each direction. Curbside parking both sides, all parallel. Permanent curbside right turn-pockets 100' in length in lieu of parking and bulb-outs: northbound at Folsom; southbound at Harrison; northbound at Harrison; and southbound at Bryant. Curbside parking lane westside between Folsom and Harrison becomes towaway no-stopping afternoon peak hour southbound traffic lane.

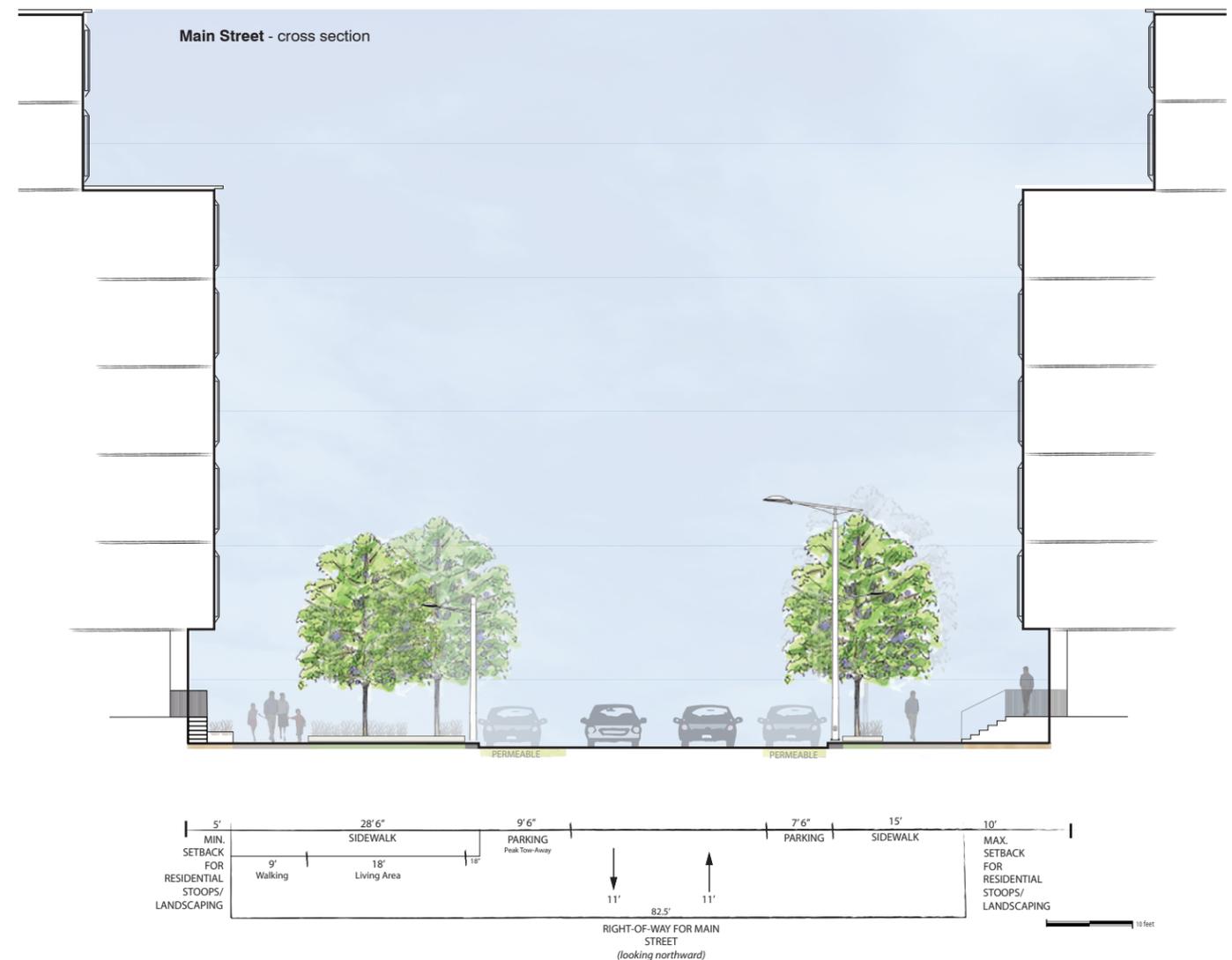
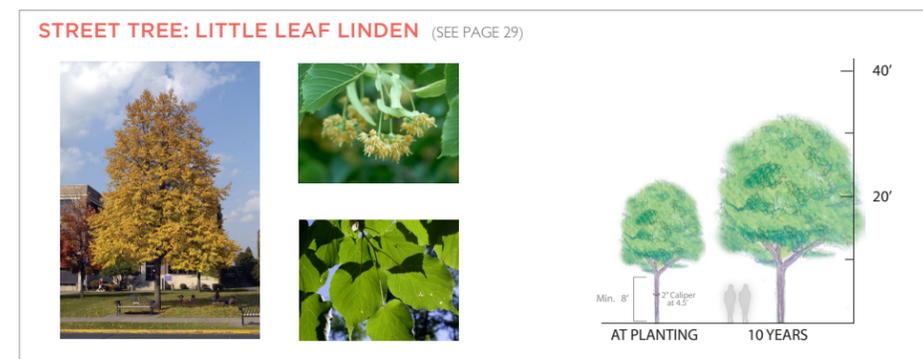
**Sidewalks:**

West side shall be 28.5 feet to face of curb. East side shall be 15 feet to face of curb.

**Bulbouts:**

All corners except: east side from Folsom Street southerly; west side from Harrison Street northerly; east side of Harrison Street southerly, west side from Bryant Street northerly.

Mid-block; east side, from 250 feet to 280 feet south of Folsom Street; both sides, from 250 to 280 feet south of Harrison Street.

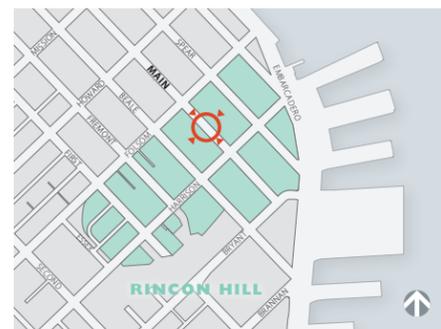




1.3 MAIN STREET

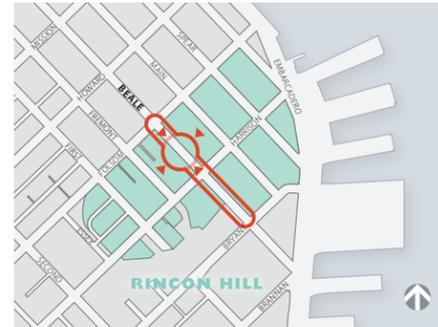


# 1.3 Main Street



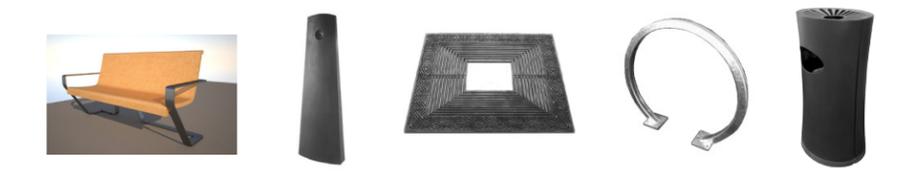
# 1.4 Beale Street

Main Street will also have an almost identical Living Street configuration to Spear and Main Street, with a couple small, but notable differences. Beale Street does not intersect with Harrison Street but rather passes under it. This presents several opportunities and additional demands on Beale Street. First, it provides the only practical access from the Financial District to the Bryant Street carpool-only on-ramp to the Bay Bridge, allowing bridge-bound vehicles to avoid traffic queues on Main and Harrison Street. Second, it is a reasonably direct southbound bicycle route south through Rincon Hill to South Beach. Additionally, the Bay Bridge anchorage is adjacent to the roadway south of Harrison Street. Due to heightened security concerns for protecting the bridge anchorage, a new security wall extending out into the existing sidewalk was built by Caltrans around the anchorage. To accommodate growing carpool traffic, the road width is sufficiently wide to allow a second



southbound peak hour lane as a curbside towaway lane should it be necessary in the future. A southbound bicycle lane between Folsom and Bryant is also included. (Note: After September 11, 2001, Beale Street was closed to all public access between Folsom and Bryant. It has since been re-opened after security measures were put in place, and the traffic striping was adjusted to partially conform to the Rincon Hill Plan).

### DESIGN PALETTE (SEE PAGE 34)



### STREET TREE: LITTLE LEAF LINDEN (SEE PAGE 29)



### Roadway:

*Pre-2001:* Three lanes southbound.

*Current:* One lane each direction, southbound bicycle lane. Curbside parking both sides between Folsom and approximately Harrison, parallel west side and perpendicular east side. No parking south of northern line of Bay Bridge either side. Permanent curbside right turn-pockets 100' in length in lieu of parking: northbound at Folsom; southbound at Bryant.

*RH Plan:* One lane each direction, southbound bicycle lane. Curbside parking both sides, all parallel. Permanent curbside right turn-pockets 100' in length in lieu of parking and bulb-outs: northbound at Folsom; southbound at Bryant.

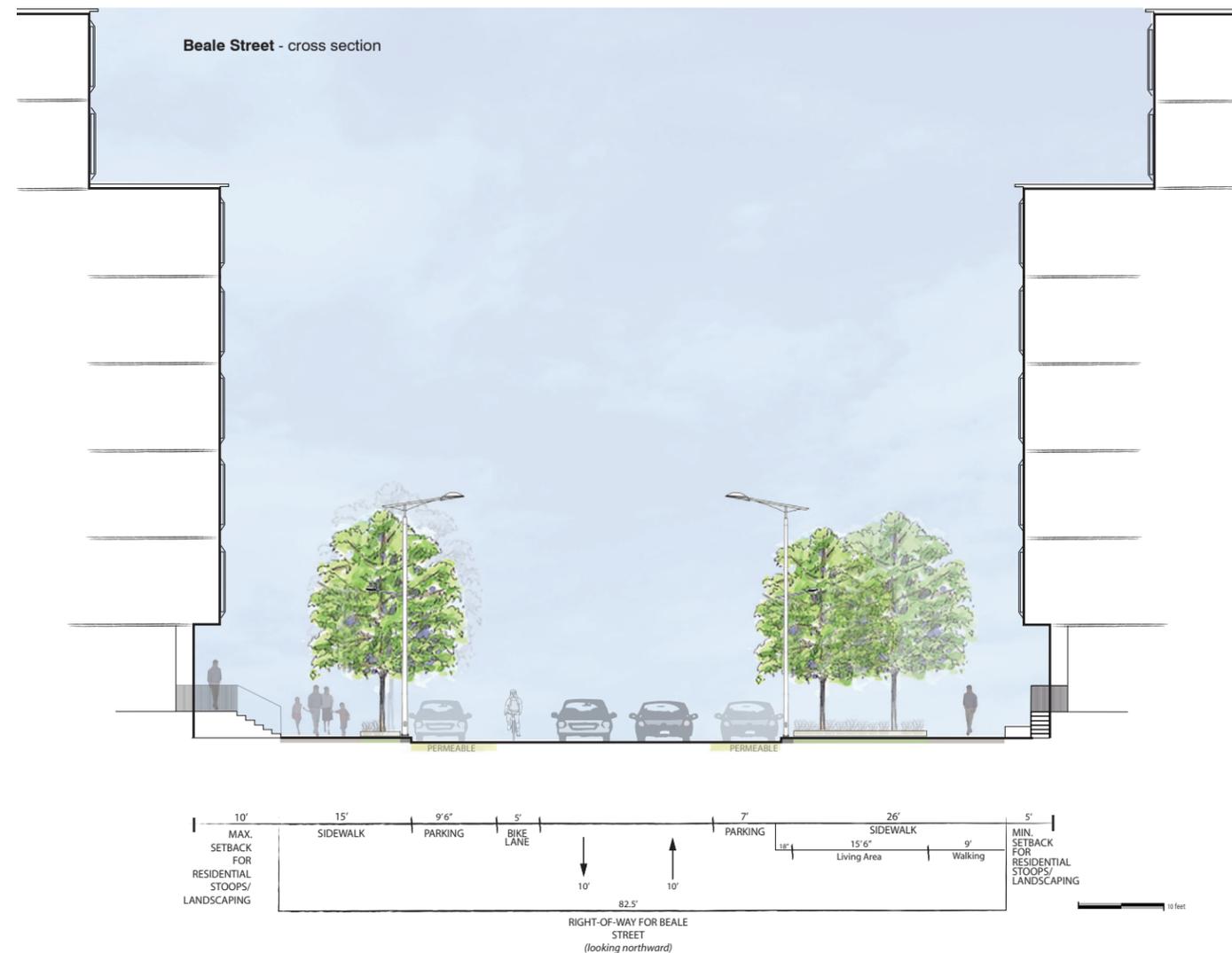
### Sidewalks:

West side shall be 15 feet to face of curb. East side shall be 24 feet to face of curb.

### Bulbouts:

All corners except: east side from Folsom Street southerly; west side from Bryant Street northerly;

Mid-block; east side, from 250 feet to 280 feet south of Folsom Street.

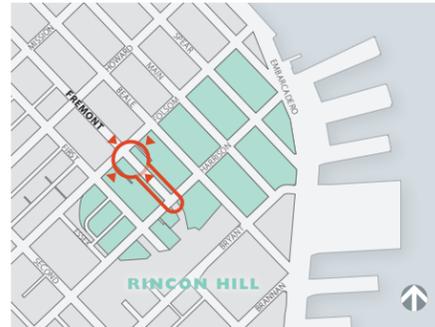


# 1.4 Beale Street



# 1.5 Fremont Street

While there is an off-ramp feeding directly onto Fremont Street northbound, there is relatively light traffic on Fremont Street between Harrison and Folsom Streets, and therefore excess capacity. This street will see major land use transformation, with approximately 750 housing units on this one block, including numerous ground floor townhouses on both sides of the street.



### Roadway:

*Current:* Two traffic lanes each direction, except the southbound direction narrows to one lane at Harrison Street. Curbside parking on both sides.

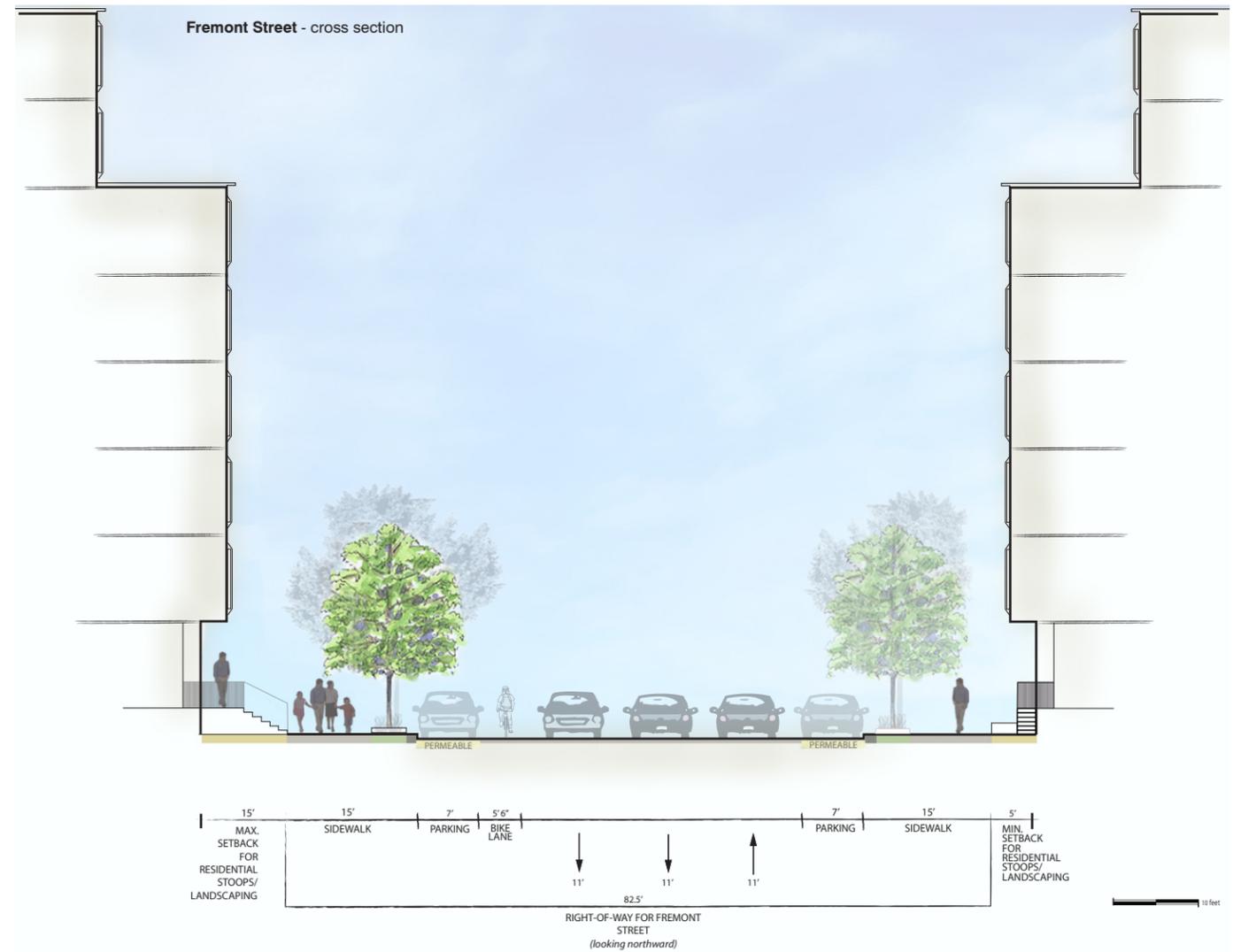
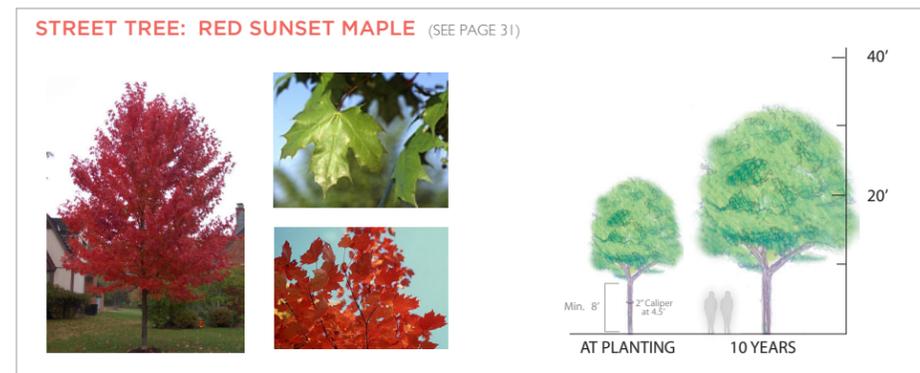
*RH Plan:* One lane southbound and two northbound. One southbound (uphill) bicycle lane. Curbside parking on both sides.

### Sidewalks:

Both sides of the street shall be 15 feet to face of curb.

### Bulbouts:

All corners (both sides from Folsom Street southerly; both sides from Harrison Street northerly)





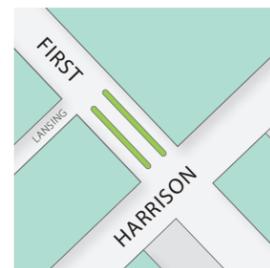
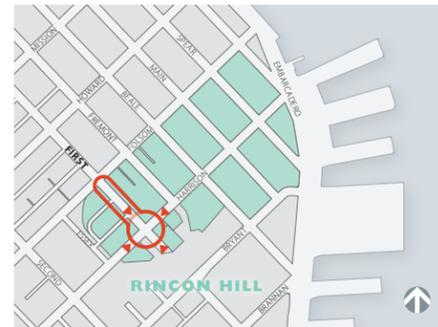
Fremont Street - block/intersection illustration

Note: Folsom Street streetscape improvements including sidewalk widths, paving and all other elements must be coordinated with the Redevelopment Agency and Planning Department for consistency with the Transbay Redevelopment Plan, and Transit Center District Plan.



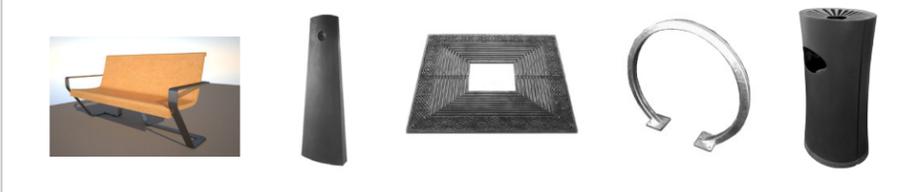
# 1.6 First Street

First Street's primary function is as a feeder to the Bay Bridge. Between Folsom and Harrison there is little opportunity to widen sidewalks significantly or eliminate traffic lanes. The east sidewalk at the north half of the block was widened during the Rincon Hill planning process. To improve pedestrian crossing at Harrison Street, beautify and soften the street environment, and facilitate local-traffic flow in the outer lanes, landscaped medians are included at the southern end of the block, roughly between Lansing and Harrison Streets, where there are currently painted medians only.

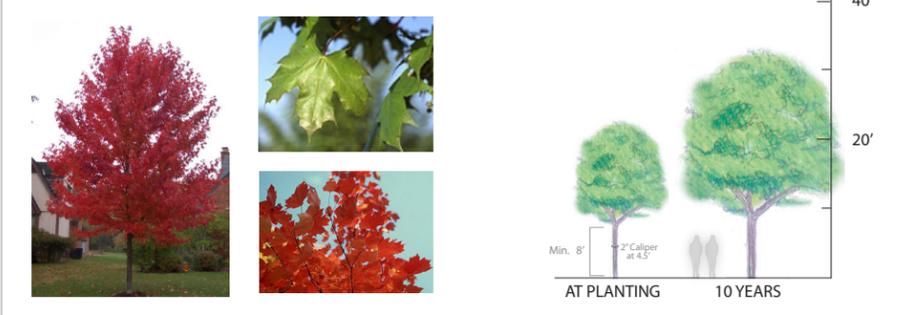


The topography of Rincon Hill is such that First Street terminates at the top of the hill, just south of Harrison Street. This stub end is to be narrowed to the minimum necessary to serve development at the top of the hill, and the remainder converted into landscaped open space.

### DESIGN PALETTE (SEE PAGE 34)



### STREET TREE: RED SUNSET MAPLE AND LOMBARDY POPLAR (SEE PAGE 32)



### Roadway:

*Current:* (Folsom to Harrison) Four traffic lanes southbound. Curbside parking on both sides, except south of Lansing Street.

(Harrison to end) One lane each direction. Perpendicular parking both side.

*RH Plan:* (Folsom to Harrison) Four traffic lanes southbound. Curbside parking on both sides, except south of Lansing Street.

(Harrison to end). One lane each direction. No on-street parking.

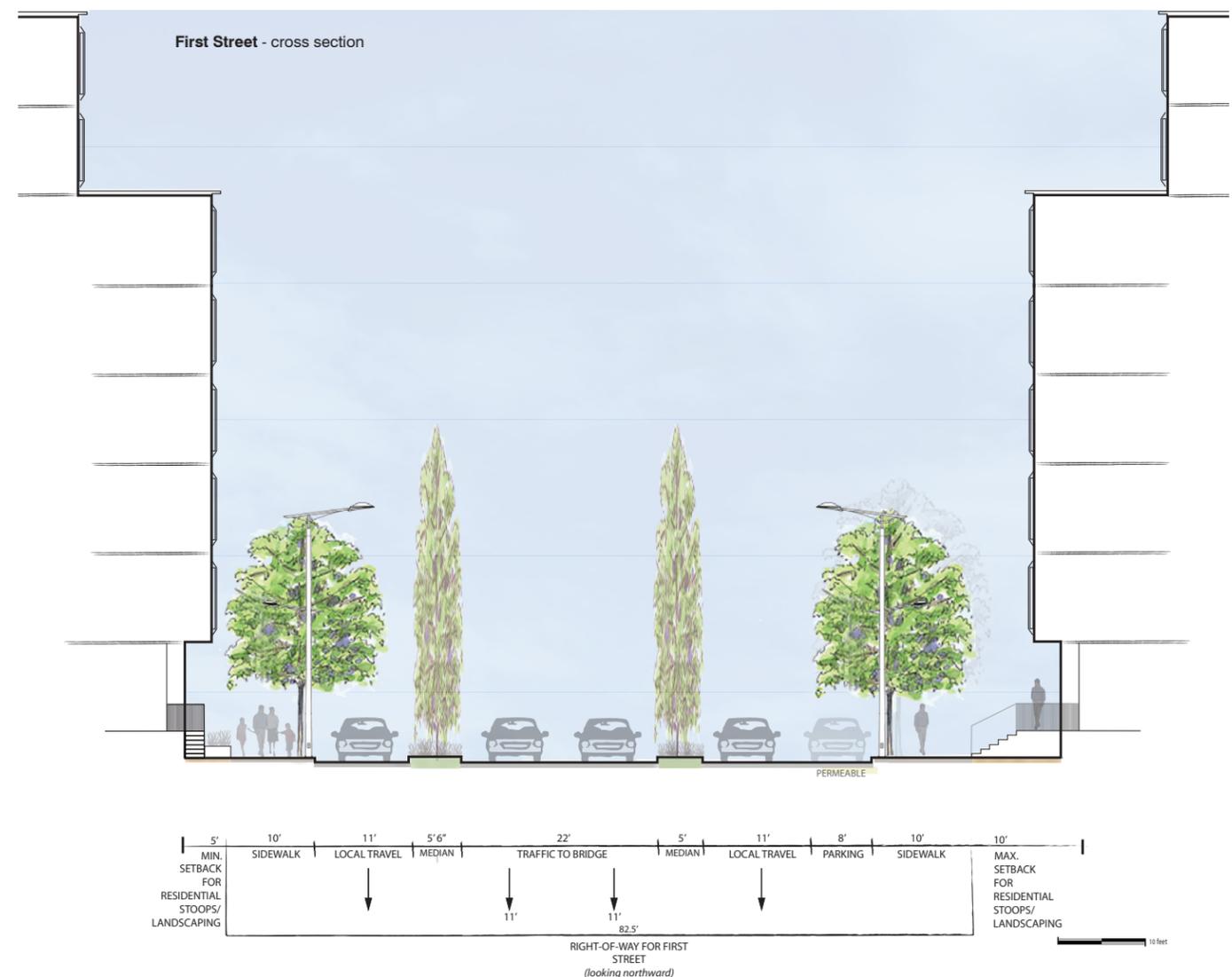
### Sidewalks:

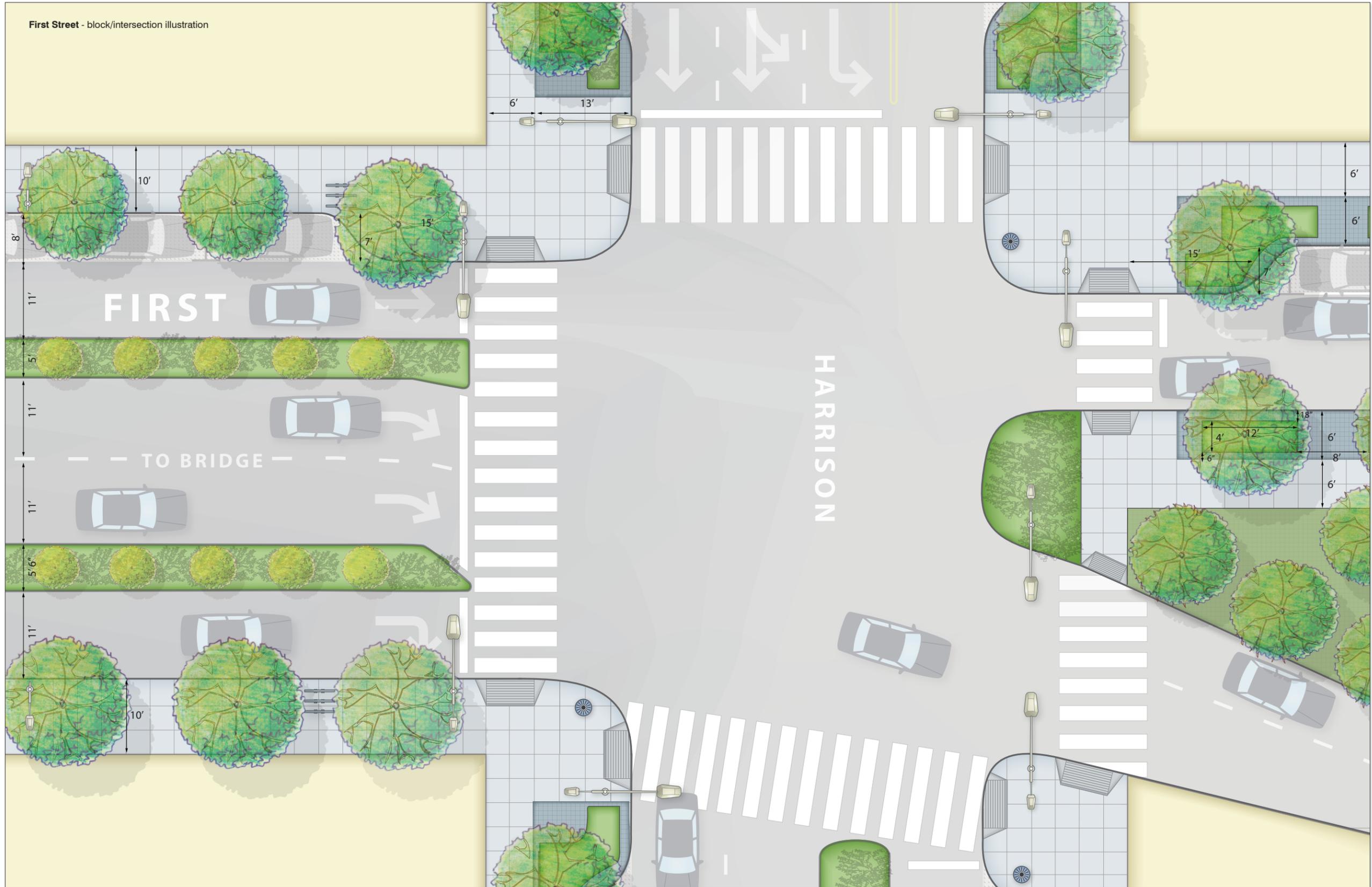
(Folsom to Harrison) East side of the street shall be 15 feet to face of curb, transitioning to 10 feet south of Lansing Street. West side shall be 10 feet.

(Harrison to end) 12 feet both sides.

### Bulbouts:

All corner except west side from Harrison Street northerly.





First Street - block/intersection illustration

FIRST

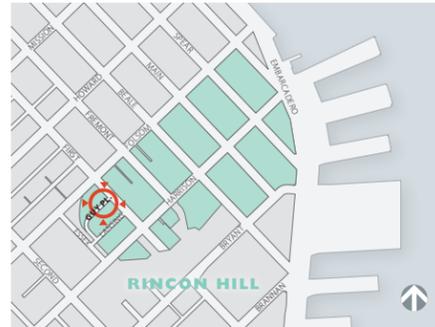
TO BRIDGE

HARRISON



# 1.7 Guy Place

Guy Place and Lansing Street are narrow streets (35' wide) that form a continuous semi-loop connecting to the west side of First Street, between Folsom and Harrison Streets. A public staircase descends from the west end of Lansing Street down to Essex Street. These streets see only light traffic serving buildings directly on these streets, as they connect only to First Street, but the right-of-way width limits the width of the narrow sidewalks. The streets shall be designed to encourage pedestrian use for the entire street width, particularly in the use of special paving across the entire roadway, as well as street tree planting in between parked cars. The street should be designed as a single-surface "shared street" without curbs pursuant to the Better Streets Plan guidelines. Additionally, raised crosswalks across the mouth of the streets at First Street will define a threshold into which vehicles enter a mostly pedestrian environment.



**Roadway:**

Current: One travel lane. Curbside parallel parking one side.

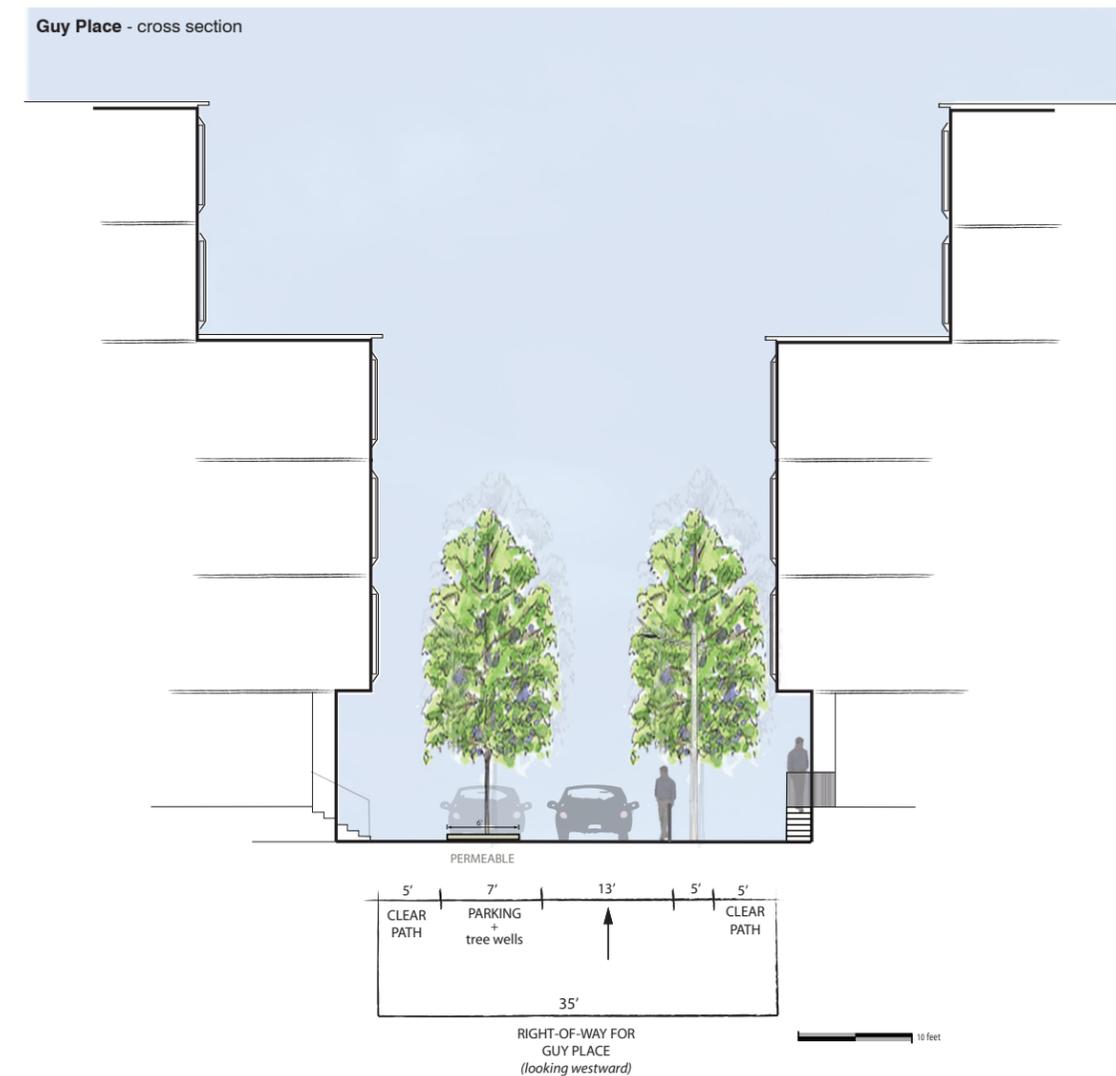
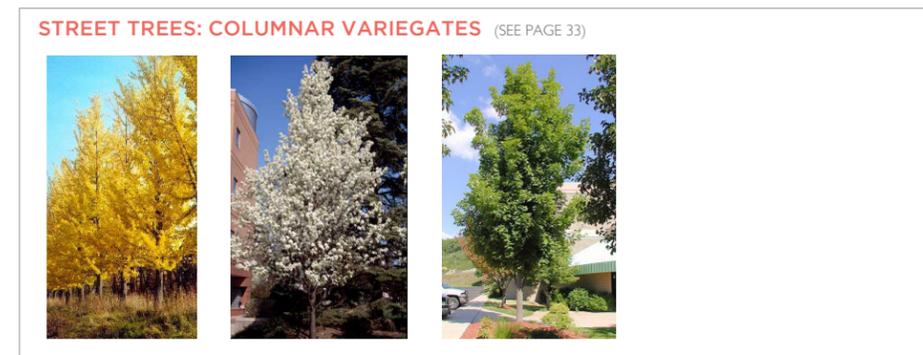
RH Plan: No change.

**Sidewalks:**

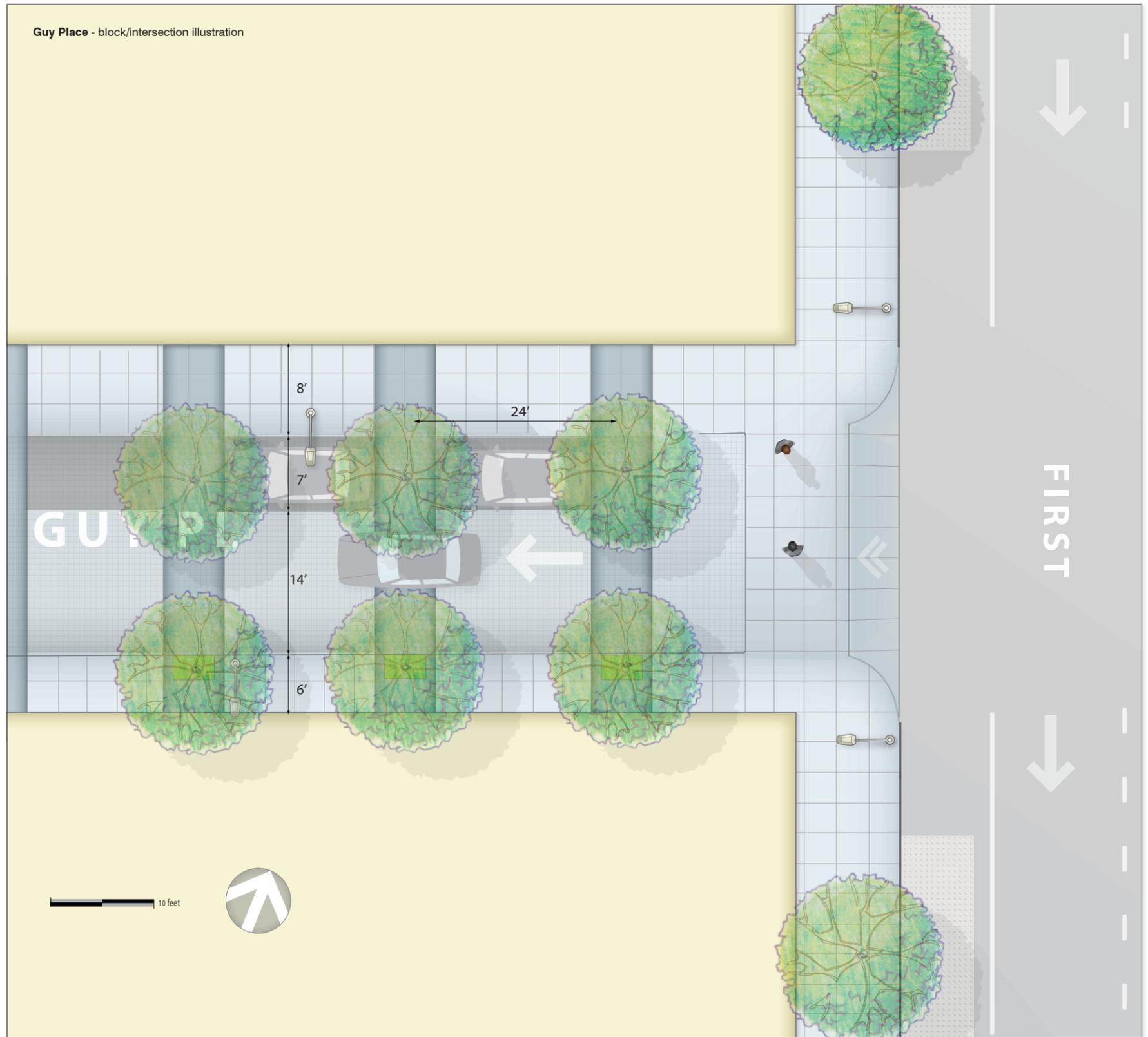
The protected pedestrian area adjacent to parking shall be 6 feet in width, the other protected pedestrian area shall be 9 feet to face of curb.

**Bulbouts:**

None.

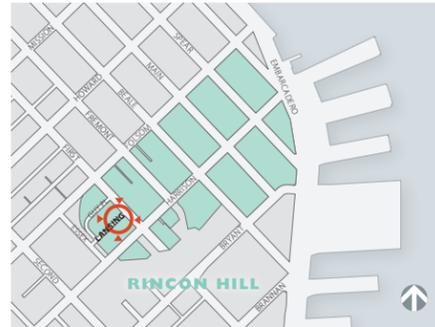


Guy Place - block/intersection illustration



# 1.8 Lansing Street

Guy Place and Lansing Street are narrow streets (35' wide) that form a continuous semi-loop connecting to the west side of First Street, between Folsom and Harrison Streets. A public staircase descends from the west end of Lansing Street down to Essex Street. These streets see only light traffic serving uses directly on these streets, as they connect only to First Street, but the right-of-way width limits the width of the narrow sidewalks. The streets shall be designed to encourage pedestrian use for the entire street width, particularly in the use of special paving across the entire roadway, as well as street tree planting in between parked cars. Additionally, raised crosswalks across the mouth of the streets at First Street will define a threshold into which vehicles enter a mostly pedestrian environment.



**Roadway:**

*Current: One travel lane. Curbside parallel parking one side.*

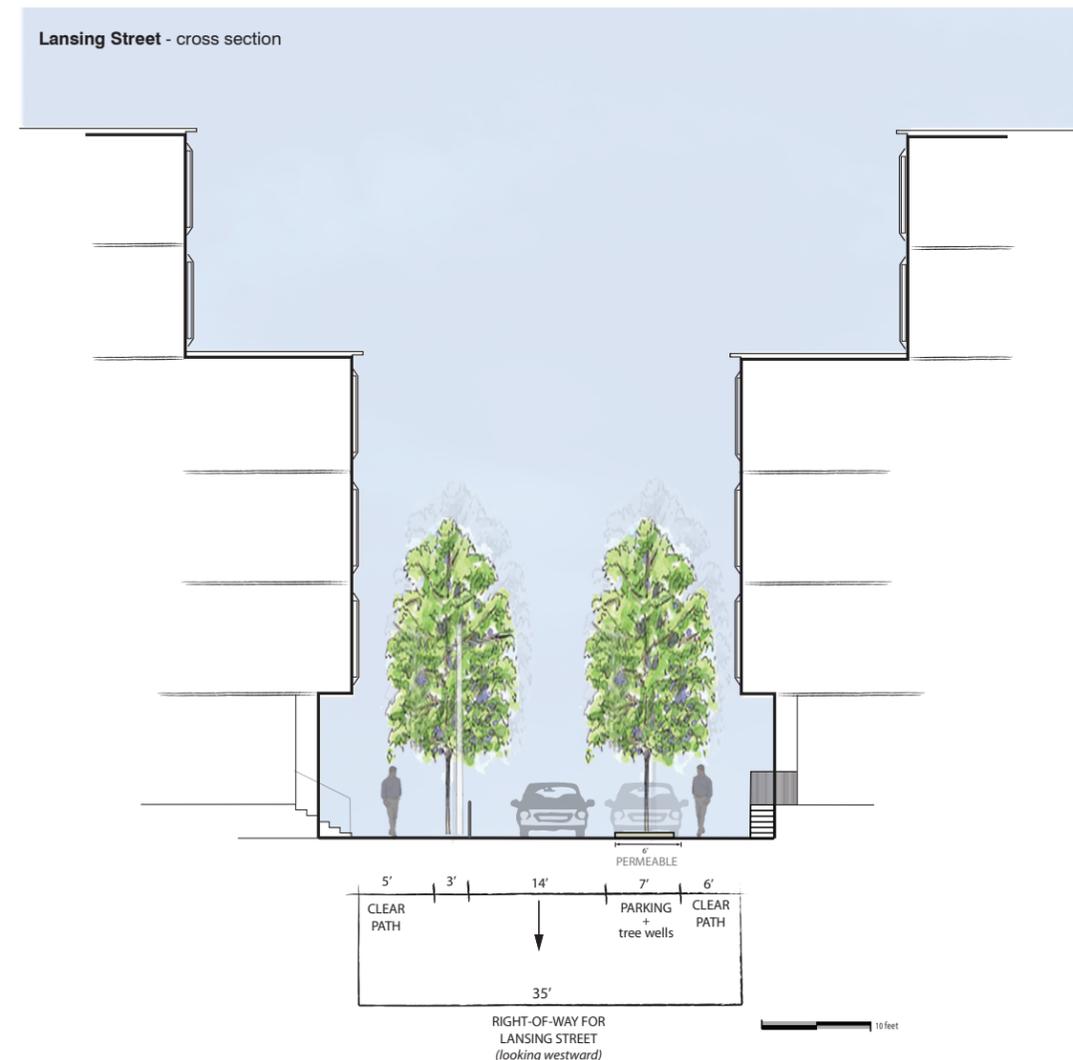
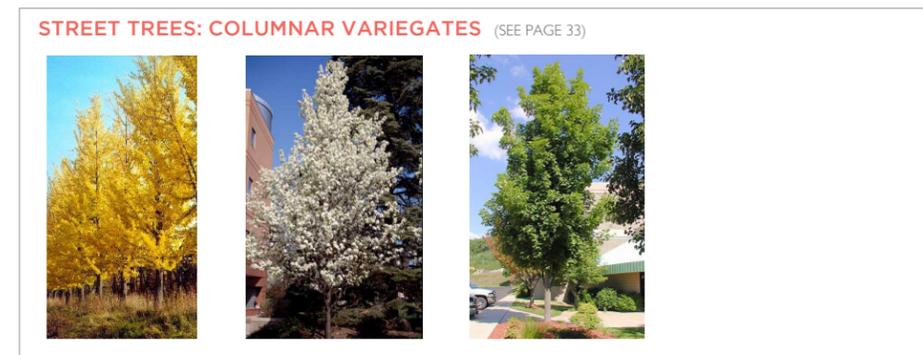
*RH Plan: Maintain existing pedestrian zone and travel lane dimensions but convert to street to Shared Public Way (curbless street).*

**Pedestrian-Safe Zones (sidewalks):**

The sidewalk adjacent to curb parking ("outer sidewalk") shall be 6 feet to face of curb, the other sidewalk shall be 8 feet to face of curb.

**Bulbouts:**

None.

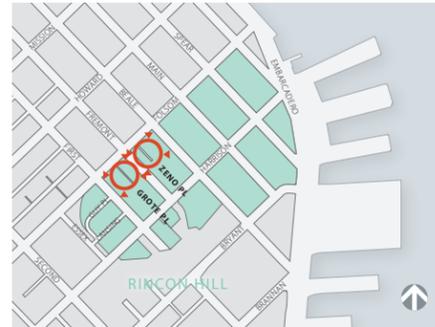






# 1.10 Grote Place and Zeno Place

Grote Place and Zeno Place are narrow alleys (12.5' and 17.5' wide respectively) that extend about halfway into their blocks. Because of their constrained width, lack of space for cars to turn around. Zeno Place has insufficient space to safely handle two-way traffic. Accommodating motorized vehicles on these streets, especially if not accessing parking garages, raises significant design challenges. The streets shall be designed to encourage pedestrian use for the entire street width, particularly in the use of special paving across the entire roadway, as well as street trees and landscaping areas. If vehicular access to these alleys is deemed infeasible, they shall be designed as pedestrian only plazas.



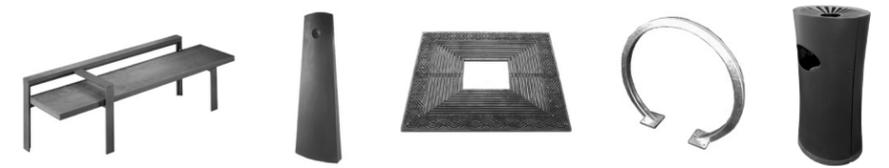
**Roadway:**  
Current: One travel lane.

*RH Plan:* Possible pedestrian only depending on future development.

**Sidewalks:**  
Street shall be designed to be curbsless to encourage pedestrian use of full ROW, except Zeno Place should have protected pedestrian-only area on one side.

**Bulbouts:**  
None.

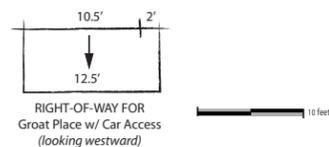
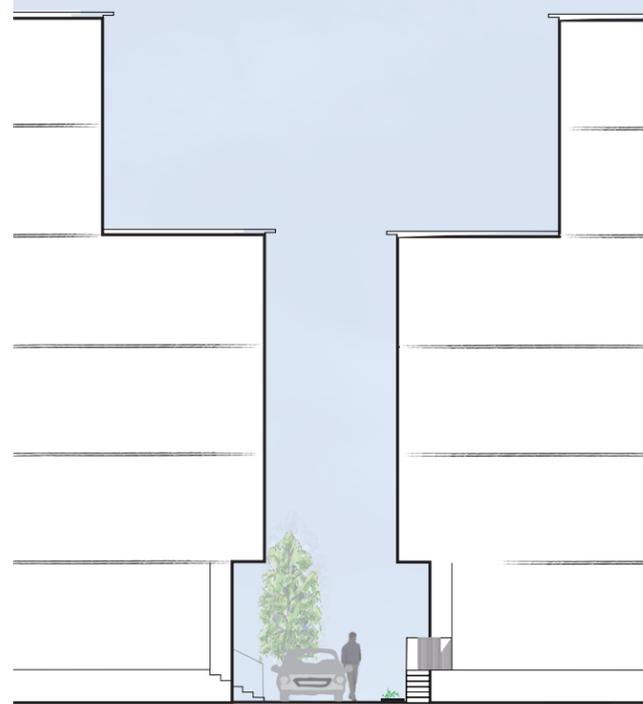
**DESIGN PALETTE** (SEE PAGE 34)



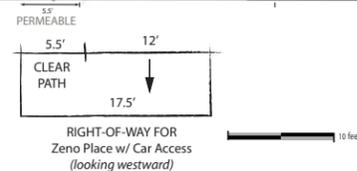
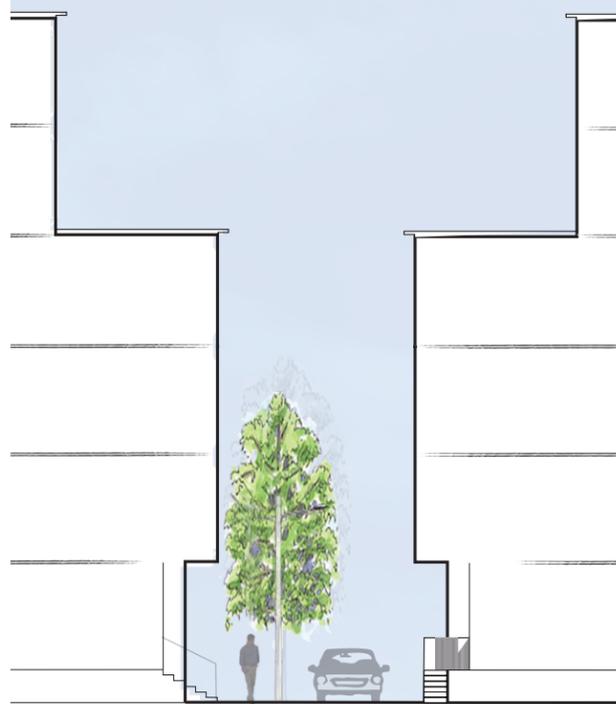
**STREET TREES: COLUMNAR VARIEGATES** (SEE PAGE 33)



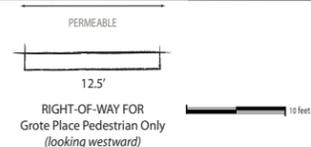
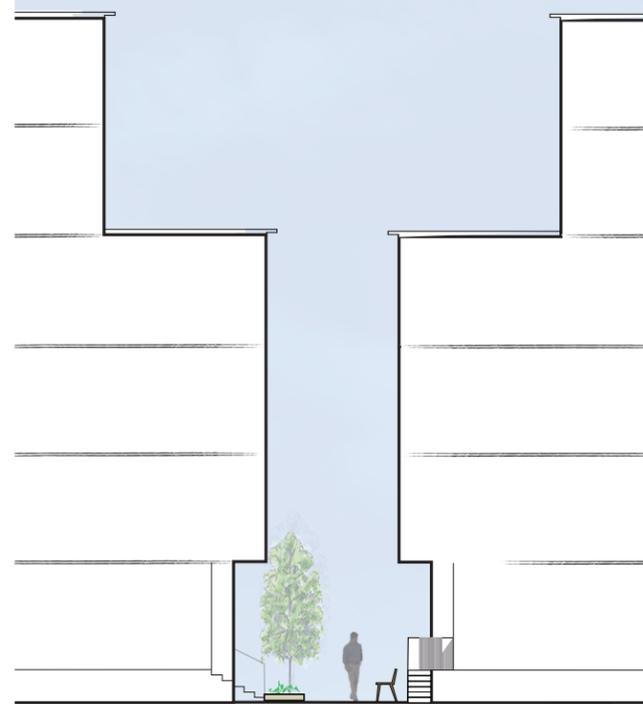
**Grote Place - one way traffic cross section**



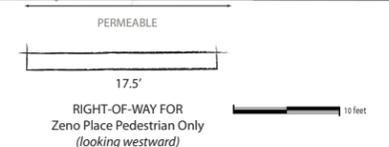
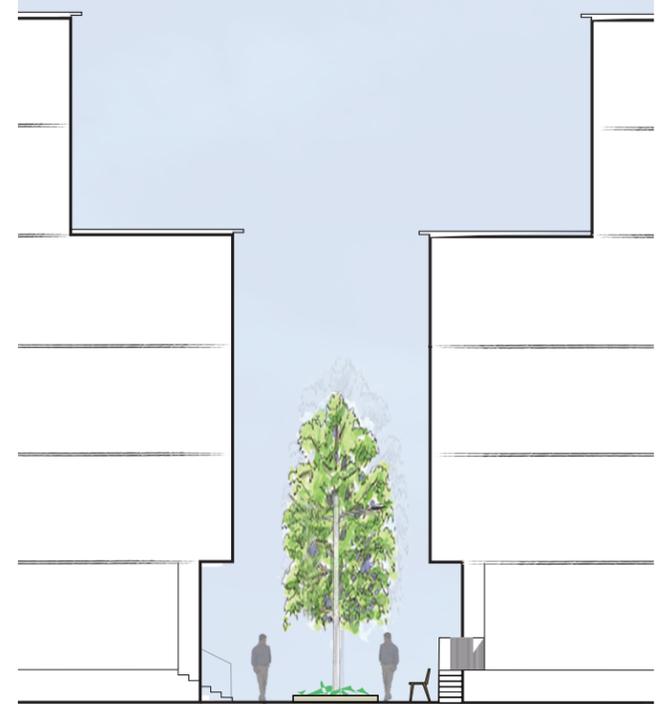
**Zeno Place - one way traffic cross section**



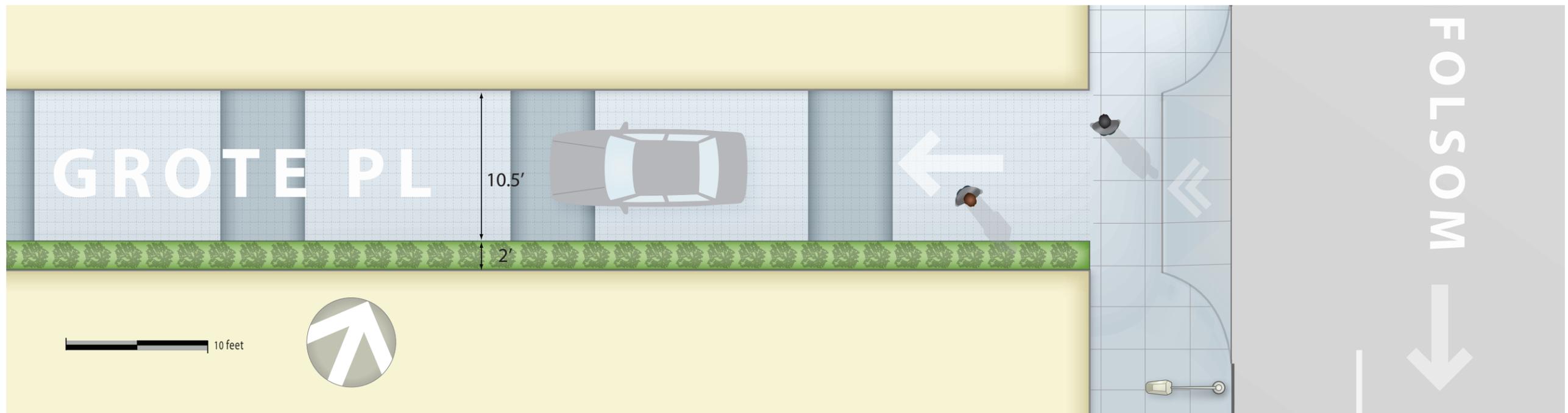
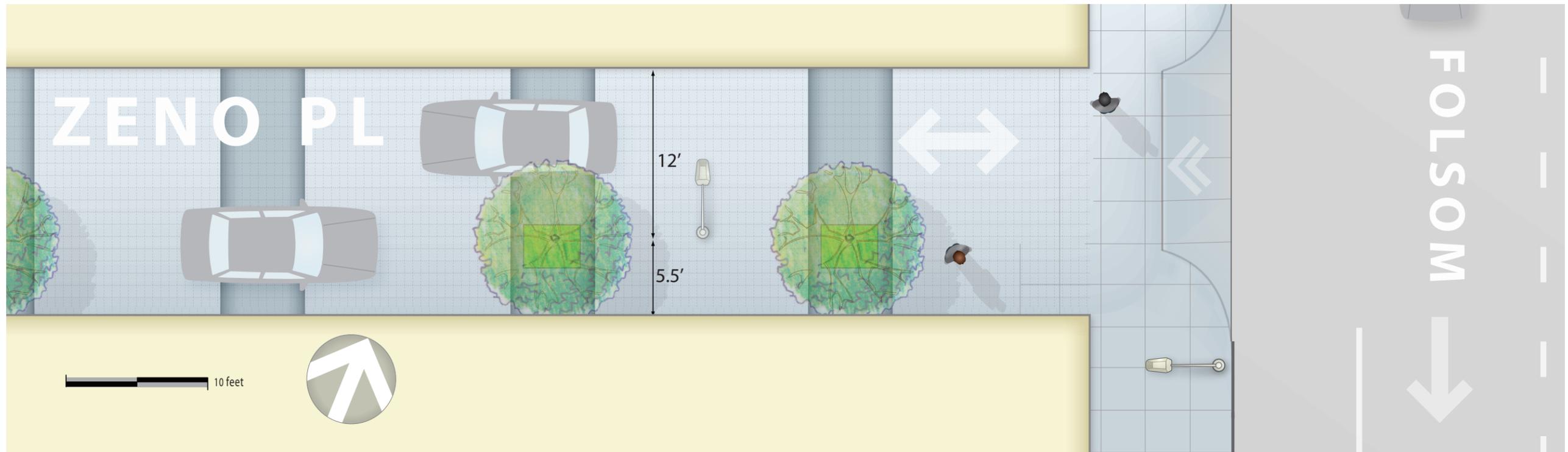
**Grote Place - pedestrian only cross section**



**Zeno Place - pedestrian only cross section**



# 1.10 Grote Place and Zeno Place - Car Traffic



# 1.10 Grote Place and Zeno Place – Pedestrian Only



# 2.0 Corner Bulbout/Curb Extension Design

Most corners in the Plan area must be built with “corner bulbouts.” Corner bulbouts shall be built in all corner locations except where curbside turn lanes are necessary and in locations where curb parking lanes become peak hour towaway lanes for transit and auto traffic (e.g. north side of Harrison Street, west side of Main Street). Additionally, bulbouts are

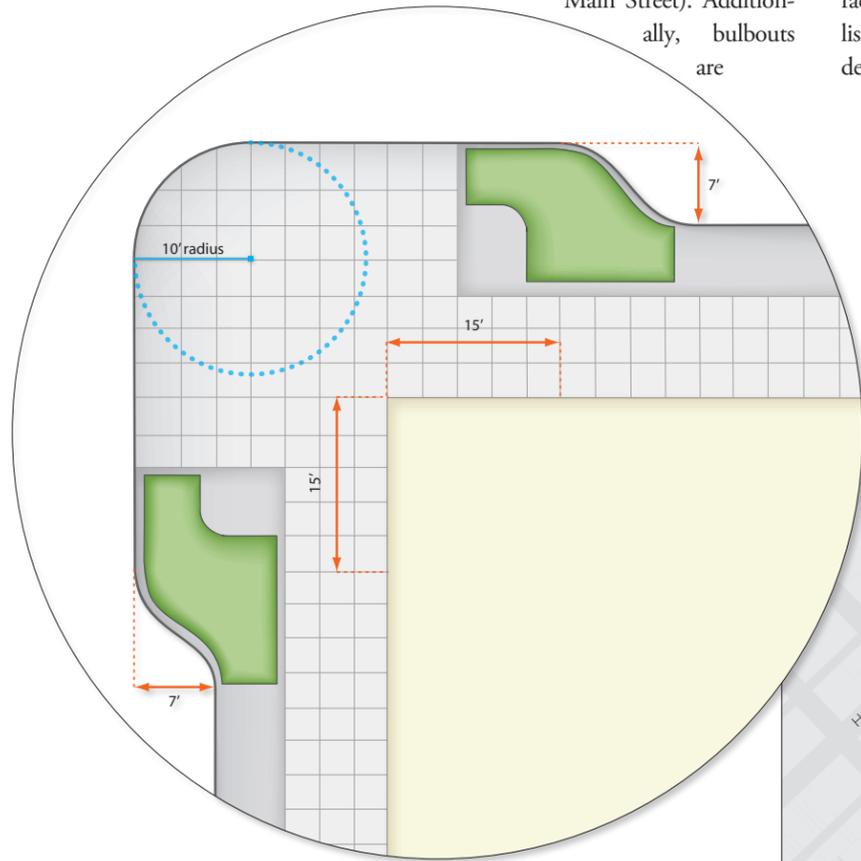
required where mid-block crosswalks are located and at some bus stops. Bulbouts in the Rincon Hill Plan Area will be longer in length than typical San Francisco bulbouts. This additional length creates space for amenities like bike parking or greening. Other proposed bulbout dimensions such as depth and corner radii should be built in to the standards established in the **Better Streets Plan**. Following are design standards for bulbouts:

- Bulbouts shall extend 7’ from the sidewalk curbline.
- Corner bulbouts must have a corner radius of 10’.

- Corner bulbouts should extend inward along the block for 15 feet along the property line. See diagram.

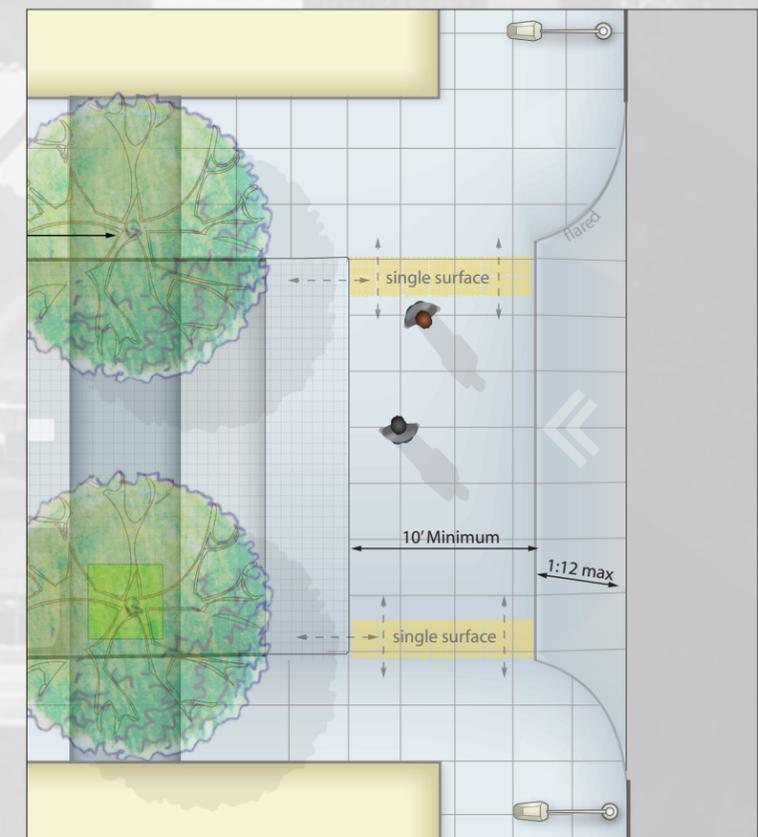
- Mid-block bulbouts shall be 30’ in length.

Landscaping should be maximized on bulbouts. Wherever possible, planters should wrap around the trailing curved edge of the bulbout to help visually narrow the roadway and draw drivers’ attention to the extended curbline. The extra spaces created by bulbouts are also key locations for placing pedestrian amenities such as bicycle racks, waste receptacles, newsracks, and additional seating.



## RAISED CROSSWALKS

Raised crosswalks must be used where alleys that have vehicular access (Guy, Lansing, Zeno, Grote, and any newly created alleys) intersect with primary streets. The sidewalk level portion of the raised crosswalk shall be at least 10’ wide and shall be designed for a continuous walking surface along the primary street at sidewalk level. Roadway ramp transitions shall be 10%.



# 3.0 Living Street Open Space Panels

## LIVING STREET DESIGN ON SPEAR, MAIN AND BEALE STREETS

The widened side of Spear, Main and Beale Streets will function as linear parks, stretching from Mission Street all the way through Transbay and Rincon Hill to the Embarcadero on the south. These spaces must actively contribute to the open space in the neighborhood, providing public amenities and open space opportunities. They are not intended to be simply visual show gardens or visual patches of green, but actual usable and inhabitable pockets of open space in this very dense neighborhood.

### OPEN SPACE PANELS

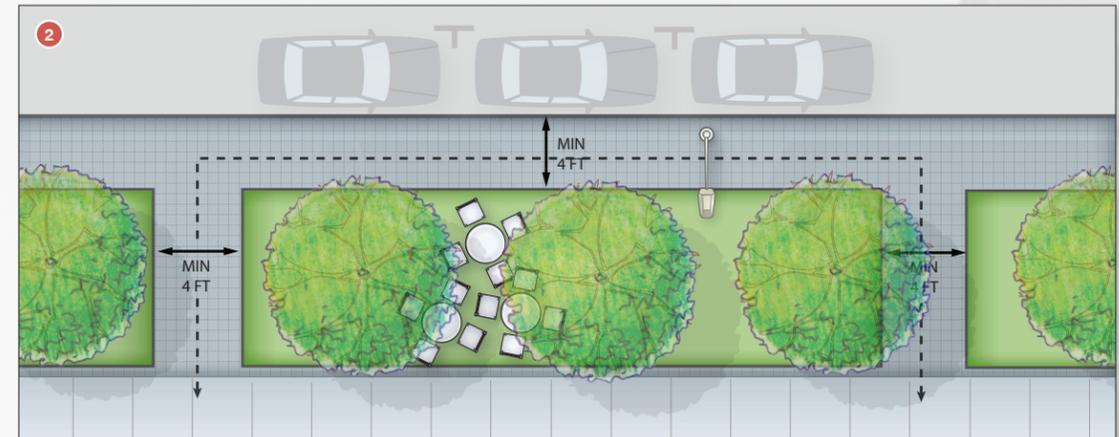
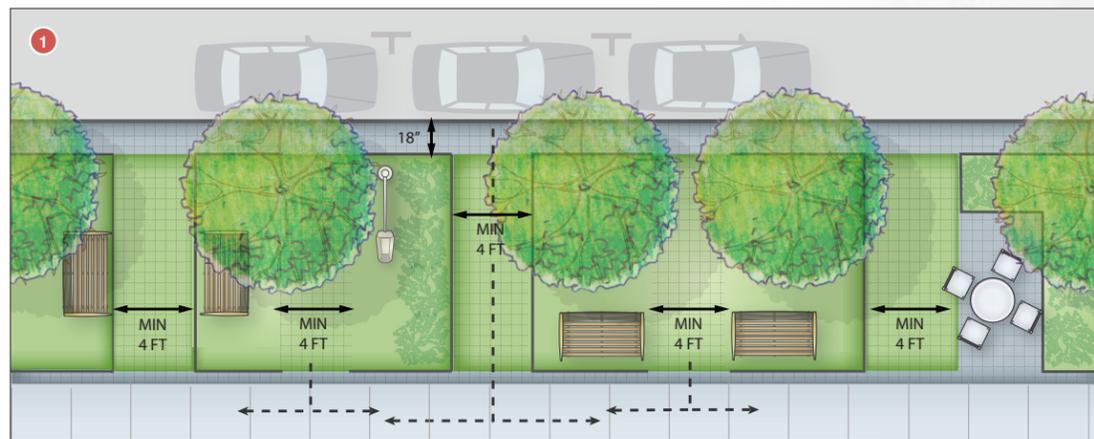
Though discussed as “linear parks,” the open space strip shall be designed not as a unified park strip with continuous paths and unified continuous design, but rather a linked linear necklace of unique open space panels, or modules. This modular structure is designed to both provide variety and practically reflect the necessity of breaking the open space multiple times per block for driveway and other access. The design and uses for these panels are flexible and open for proposal and interpretation. Designs must foster and encourage active use by area residents and visitors – they should be welcoming and encourage informal use, while de-emphasizing overly-manicured and high-maintenance showpieces. Following are suggestions for open space panels:

- seating
- café tables (for immediately adjacent commercial uses)
- public art/sculpture
- play structures
- lawn
- dog runs
- community garden
- gaming (e.g. chess tables)
- ecological/educational displays
- community bulletin board

A diversity of panels on each street is desirable. A continuous row of the same repeated module (e.g. all lawn or all similar seating arrangements) would be both aesthetically and functionally monotonous.

The panel structure allows and expects evolution of individual spaces over time. As the neighborhood evolves and tastes or needs change, the design of individual panels can evolve and be refreshed (as opposed to the more static nature of a unified singular linear park design).

Panels should minimize hardscape and maximize permeability and landscaping, though balance landscaping with inhabitable open space.



### PANEL DIMENSIONS AND SPECIFICATIONS

The width of each module varies according to the specific street: 22’6” on Spear, 19’6” on Main, and 17’ on Beale. The length of each module may and will vary according to the designs proposed and influenced by the location of driveways, loading zones, crosswalks, and the like. Recommended lengths are 15’ minimum and 40’ maximum.

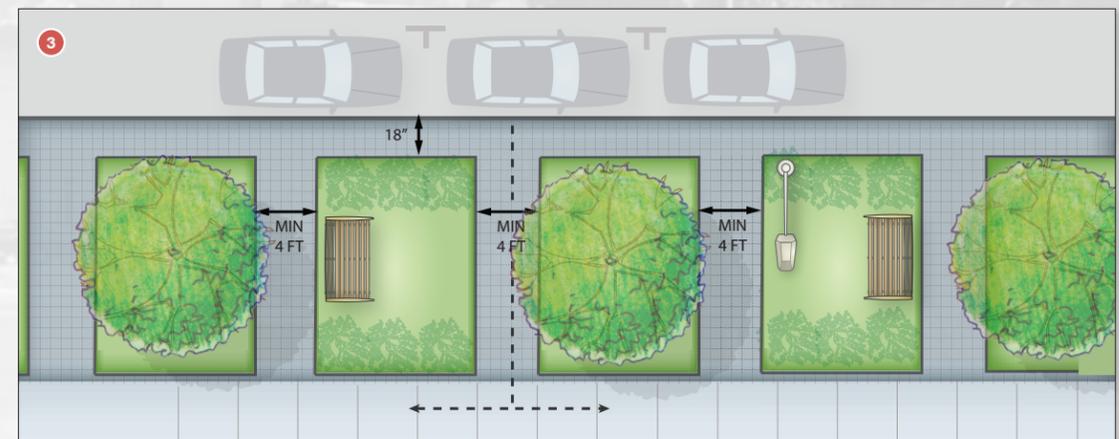
Where curbside parking exists, ADA-accessible pathways must be provided. This may take one of three forms:

- 1 It is possible to provide an accessible pathway (using appropriate clearances and walking surfaces) through a panel, incorporating this space into the panel’s design.

- 2 Alternatively, where multiple panels are fused together without breaks, a 4’-wide walk along the curb can be provided connecting to the nearest pathway around the panels.

- 3 A minimum 4’-wide gap between open space panels, centered on the parking space, to connect the curb parking to the primary walkway/sidewalk.

The first form is preferable. Where ADA accessible paths cannot be integrated into the design of the panels, the second form should be chosen. The third form, shown below, should be used only as a last result. However, specific designs will be evaluated on their individual proposals.



# 4.0 Street Trees and Understory Plantings

## EXISTING TREES

Existing street trees are very spotty except where recent new development has installed street trees in front of their buildings. Below is a rough inventory of the 224 existing street trees within the plan area boundary.

As the plan for many of the streets in the district calls for widening sidewalks, maintaining some existing street trees is not desirable or practical because of the new configurations of walkways, street trees, landscaping, and other sidewalk elements. Most of the existing trees to be removed were planted within the past 10 years. Approximately 84 trees will likely be removed or relocated over the course of the implementation of the Streetscape Plan, and a total of approximately 1290 new trees will be planted to the neighborhood upon final buildout, for a net gain of 1206 trees over the life of the Plan.



## NEW TREES

The box at right lists the required street tree species and cultivars for each street in the district. Project sponsors must use the primary tree species and cultivar indicated unless it is unavailable, in which case the alternative selection may be used. Botanical names are given in italics, specific cultivars (if any) follow in plain text with single quotes, and common names are given in parentheses.

## TREE SELECTION AND PLANTING SPECIFICATIONS

Basic requirements for street trees in Rincon Hill are established in Planning Code Section 138(c)(1). Some of these requirements are reprinted here and augmented with additional specifications.

### SIZE

Recommended nursery-grown container sizes are 48" box for all street trees except for 36" boxes on alleys and mid-block paths. All new street trees must have a minimum 2" caliper at approximately 4.5 feet above sidewalk grade and branch a minimum of 8 feet above sidewalk grade. Trees must be planted in a sidewalk opening of at least 16 square feet.

### STRUCTURAL SOILS

Trees must be planted in basins with structural soils and a minimum soil depth of 3'6". This basin must provide nutrient-rich soils, free from overly-compacted soils, and generally be conducive to tree root development. Where multiple adjacent trees are being planted on a block face, trees shall be planted in a continuous soil-filled trench parallel to the curb, such that the basin for each tree is connected below the sidewalk.

### IRRIGATION

All street trees are to receive automatic irrigation, including trees set within tree grates.

### LOCATION

Planning Code Section 138.1 requires every newly constructed or significantly modified building to plant street trees at a rate of one tree for every 20 feet of street frontage. In Rincon Hill street trees must be planted in the ground at all feasible locations per the spacing pattern required for the particular street per this document illustrated on pages 24-28. Street trees may not be omitted from the pattern for any reason, such as in front of the lobby or signage of a particular building or business. In the case that sub-sidewalk utility vaults preclude the planting of any particular street trees, the project sponsor shall work with the Planning Department to propose an above-grade planter or pedestrian amenity appropriate for the specific sidewalk condition and width.

## RH STREET TREE ANALYSIS 7.07.2007

STREET	COUNT	REMOVE	KEEP	
				Incorrect Species*
Spear	43	22	21 <sup>1</sup>	all
Main	21	6	15 <sup>1</sup>	all
Beale	29	5	24 <sup>4</sup>	all
Fremont	11	11 <sup>3</sup>	0	all
First	24	1 <sup>2</sup>	23	all
Harrison	47	4 <sup>2</sup>	43	26
Folsom	10	0	10	0
Guy	14	10	4	all <sup>4</sup>
Lansing	25	25	0	all <sup>4</sup>
Essex	0	0	0	n/a
<b>TOTAL</b>	<b>224</b>	<b>84</b>	<b>140</b>	

a. Existing trees are different species than those called for in this Plan.  
 1. To be removed if required for construction. Could remain as secondary planting row.  
 2. Trees to be removed are dead as of 7.07.  
 3. Existing trees are in above-grade planters.  
 4. Alleys - majority currently upright juniper cultivars.

Folsom and Harrison Streets:  
*Lophostemon conferta* (Brisbane Box)

Spear, Main, and Beale Streets:  
*Tilia cordata* 'Greenspire' (Little Leaf Linden)  
 alternative: *Liquidambar styraciflora* 'Rotundiloba' (Liquidambar/Fruitless Sweetgum)

Fremont, First, and Essex Streets:  
*Acer rubrum* 'Red Sunset' (Red Maple)  
 alternative: *Acer x Freemanii* 'Autumn Blaze' (Freeman Maple)

First Street Center Medians  
*Populus nigra* 'Italica' (Lombardy Poplar)

\*Guy Place, Lansing Street, Zeno Place, Grote Place, and mid-block pedestrian paths:  
*Pyrus calleryana* 'Chanticleer' (Columnar Ornamental Pear)  
*Acer rubrum* 'Bowhall' (Columnar Red Maple)  
*Ginkgo biloba* 'Princeton Sentry' (Columnar Ginkgo)

\* For alleys and paths, the selected street tree may vary from alley to alley, but each alley/path shall be planted consistently with only one species.



Currently, RH streets have few, if any, street trees.



## UNDERSTORY PLANTINGS

At-grade landscaping in planters is a key component of greening and softening the streetscape in the district. Extensive planters are required on most streets. In addition to providing color and natural relief from the hard cityscape at pedestrian level, planters along the sidewalk edge buffer pedestrians from traffic and parked cars, as well as serve valuable ecological functions by collecting, filtering, and slowing sidewalk stormwater runoff. The Streetscape Plan's goal is to maximize permeable surface and greenery wherever possible.

Plantings should be as exuberant as possible, with significant seasonal or year-round color. A diversity of plantings and species is encouraged to create heterogeneity and a casual, informal feeling consistent with a residential neighborhood. Developments that are landscaping extensive sidewalk frontages or multiple consecutive planting beds are strongly encouraged to avoid repetitive or homogenous treatments. Boxy or rigid evergreen hedges or bushes, such as Japanese Boxwood, should be avoided, except in limited usage, such as on the wide

parkway side of Spear, Main, or Beale Streets for the purpose of creating intimate sitting or activity areas. Recommended plant types include flowering plants and grasses, including Flax, Phormium, Sedge, Carex, Hemerocallis (Daylilies), and other drought tolerant species. Landscape architects are encouraged to meet and confer with the DPW Bureau of Urban Forestry to review species proposed for each specific streetscape implementation.

## PLANTER DESIGN

Planters are required on almost all sidewalks in Rincon Hill. Planter dimensions are given for each street on those street's respective sections of the document.

## LOCATION

Planters meeting the minimum dimensional standards must be located at all feasible locations per the spacing pattern and dimensional standards required for the particular street per this document. In general, planters may not be omitted from the pattern, such as in front of a particular business or building entrance. The Planning Department may permit up to

two street trees to be placed in tree grates in lieu of planters in front of a building with a particularly high volume of curb-side drop-off activity and an official white curb loading zone.

## GRADE

All planting beds should be designed to allow sidewalk stormwater runoff to filter through planting beds. Planting beds should be flush or slightly depressed from sidewalk grade.

## EDGING

Planter edging features are encouraged and may be incorporated along the perimeter of the planter. The edging feature must be permeable to allow water to flow into and through the planter. Edging features should not be higher than 18" above grade, and may consist of ornamental railings or other materials such as decorative stone, brick, or concrete. If constructed of a non-permeable material such as stone, brick, or concrete, the edging must be significantly perforated at sidewalk grade at regular intervals to allow runoff to flow through the planter.



# 4.1 Street Trees and Understory Plantings

## SPEAR, MAIN, & BEALE STREETS - Living Streets



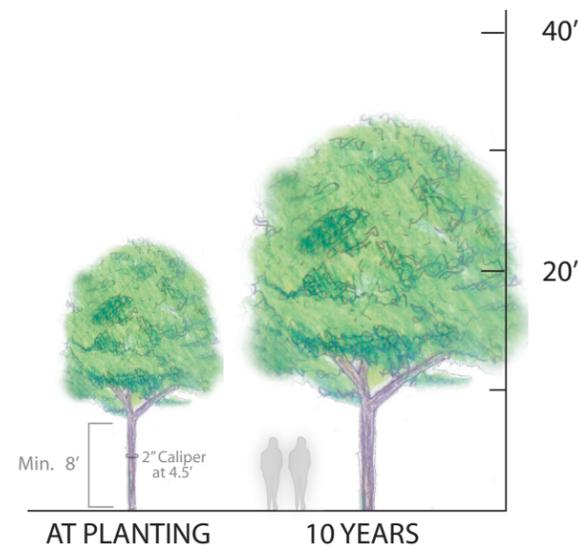
**TILIA CORDATA 'GREENSPIRE' (LITTLE LEAF LINDEN)**

**Character:**  
Pyramidal in youth, ovate when mature; deciduous; dense and compact branching; branches are upright and spreading.

**Size:**  
Height: 40' – 50'  
Spread: 35'

**Flower//Bark:**  
Small, yellow or light cream flowers in drooping clusters during summer months. Ridged, grey-brown bark.

**Planting Specifications:**  
New street trees must have a minimum 2" caliper at 4.5' above sidewalk grade and branch at a minimum of 8' above sidewalk grade. Trees are to be planted every 20' in sidewalk openings of at least 16 square feet, and shall not be closer than 25' to an intersection approach or 10' from the far side of the intersection. Trees shall be planted in a continuous, connected soil-filled trench of structural soils to a depth of at least 3' 6".



**ALTERNATE LIQUIDAMBAR STYRACIFLORA 'ROTUNDILOBA' (FRUITLESS SWEETGUM)**

**Character:**  
Pyramidal when young, oblong to rounded when mature; deciduous shade tree; alternate, star-shaped leaves; usually maintains a single leader.

**Size:**  
Height: 40' – 60'  
Spread: 35'

**Flower//Bark:**  
Small, non-descript flowers. Corky, deeply furrowed ridges, yellowish-brown bark.

**Planting Specifications:**  
New street trees must have a minimum 2" caliper at 4.5' above sidewalk grade and branch at a minimum of 8' above sidewalk grade. Trees are to be planted every 20' in sidewalk openings of at least 16 square feet, and shall not be closer than 25' to an intersection approach or 10' from the far side of the intersection. Trees shall be planted in a continuous, connected soil-filled trench of structural soils to a depth of at least 3' 6".



### UNDERSTORY PLANTING PALETTE

Understory plantings, such as different Carex, Hemerocallis, Koeleria, Flax, Phormium, and Sedge cultivars, are required in all planters. While the general visual theme of these plantings should be consistent, variety is encouraged and the choice of specific plantings is flexible.



# 4.2 Street Trees and Understory Plantings

## HARRISON & FOLSOM STREETS



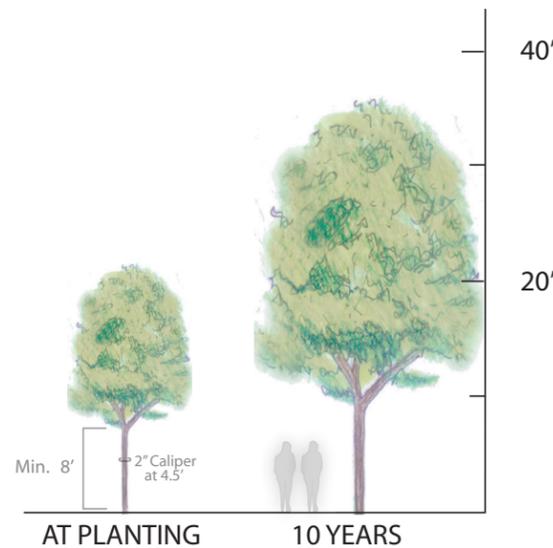
**LOPHOSTEMON CONFERTUS  
(BRISBANE BOX)**

**Character:**  
Broadleaf; evergreen; upright; oval form.

**Size:**  
Height: 35' – 40'  
Spread: 25'

**Flower//Bark:**  
Small, white, distinctive, flowers in clusters 2-4" across during summer months. Mottled, shredding, light brown or reddish bark, similar to Madrone.

**Planting Specifications:**  
New street trees must have a minimum 2" caliper at 4.5' above sidewalk grade and branch at a minimum of 8' above sidewalk grade. Trees are to be planted every 20' in sidewalk openings of at least 16 square feet, and shall not be closer than 25' to an intersection approach or 10' from the far side of the intersection. Trees shall be planted in a continuous, connected soil-filled trench of structural soils to a depth of at least 3' 6".



**UNDERSTORY PLANTING PALETTE**

Understory plantings, such as different Carex, Hemerocallis, Koeleria, Flax, Phormium, and Sedge cultivars, are required in all planters. While the general visual theme of these plantings should be consistent, variety is encouraged and the choice of specific plantings is flexible.



# 4.3 Street Trees and Understory Plantings

## FREMONT & ESSEX STREETS



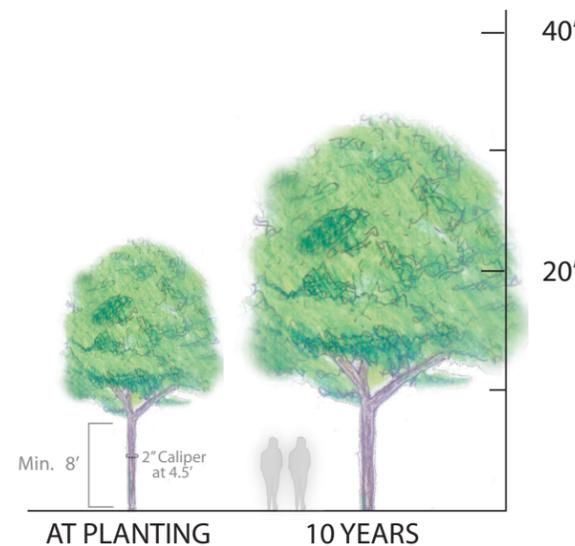
**ACER RUBRUM 'RED SUNSET' (RED MAPLE)**

**Character:**  
Symmetrical, upright ovate in youth and when mature; deciduous; branches upright and require pruning for optimal shape. Showy red foliage during fall months.

**Size:**  
Height: 40'-45'  
Spread: 25'-35'

**Flower/Bark:**  
Small, red showy flowers in spring. Reddish-grey bark, smooth.

**Planting Specifications:**  
New street trees must have a minimum 2" caliper at 4.5' above sidewalk grade and branch at a minimum of 8' above sidewalk grade. Trees are to be planted every 20' in sidewalk openings of at least 16 square feet, and shall not be closer than 25' to an intersection approach or 10' from the far side of the intersection. Trees shall be planted in a continuous, connected soil-filled trench of structural soils to a depth of at least 3' 6".



**ALTERNATE ACER FREEMANII 'AUTUMN BLAZE' (FREEMAN MAPLE)**

**Character:**  
Distinct, upright ovate form in youth and when mature; deciduous; well-defined central leader with ascending branches; rapid growth rate; not as dense as other cultivars. Showy orange-red foliage during fall months, medium-green, shiny foliage in summer.

**Size:**  
Height: 40'-50' | Spread: 30'-40'

**Flower/Bark:**  
Non-descript flowers. The bark is smooth, whitish when young, becoming furrowed with dark ridges as it ages.

**Planting Specifications:**  
New street trees must have a minimum 2" caliper at 4.5' above sidewalk grade and branch at a minimum of 8' above sidewalk grade. Trees are to be planted every 20' in sidewalk openings of at least 16 square feet, and shall not be closer than 25' to an intersection approach or 10' from the far side of the intersection. Trees shall be planted in a continuous, connected soil-filled trench of structural soils to a depth of at least 3' 6".



### UNDERSTORY PLANTING PALETTE

Understory plantings, such as different Carex, Hemerocallis, Koeleria, Flax, Phormium, and Sedge cultivars, are required in all planters. While the general visual theme of these plantings should be consistent, variety is encouraged and the choice of specific plantings is flexible.



# 4.4 Street Trees and Understory Plantings

## FIRST STREET



**ACER RUBRUM 'RED SUNSET'**  
(RED MAPLE)

**Character:**

Symmetrical, upright ovate in youth and when mature; deciduous; branches upright and require pruning for optimal shape. Showy red foliage during fall months.

**Size:**

Height: 40'-45' | Spread: 25'-35'

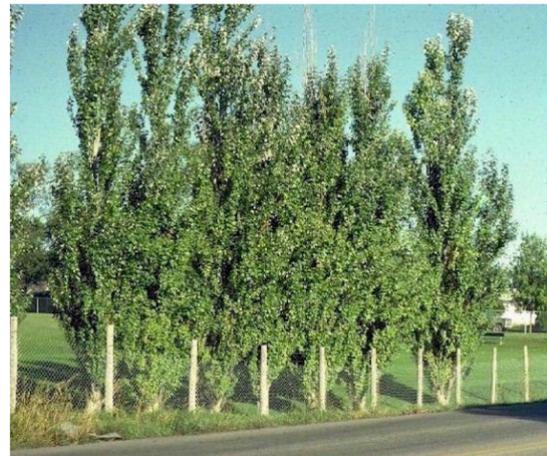
**Flower/Bark:**

Small, red showy flowers in spring. Reddish-grey bark, smooth.

**Planting Specifications:**

Red Sunset Maple shall be used for sidewalk planting

New street trees must have a minimum 2" caliper at 4.5' above sidewalk grade and branch at a minimum of 8' above sidewalk grade. Trees are to be planted every 20' in sidewalk openings of at least 16 square feet, and shall not be closer than 25' to an intersection approach or 10' from the far side of the intersection. Trees shall be planted in a continuous, connected soil-filled trench of structural soils to a depth of at least 3' 6".



**Lombardy Poplar shall be planted in the center median. No alternate species has been selected.**

**POPULUS NIGRA 'ITALICA'**  
(LOMBARDY POPLAR)

**Character:**

Very slender upright crown (column-like); deciduous, small shiny green leaves, serrated at edge; upward bending branches start close to the ground.

**Size:**

Height: 40'-60' | Spread: 10'-15'

**Flower/Bark:**

Slender, reddish to yellow-green, hanging catkins, 2 to 3 inches long, appear in early spring before the leaves. Smooth grey-green bark.

**Planting Specifications:**

Lombardy Poplar shall be planted in the center median.

Trees are to be planted every 20' along both median strips but shall not be closer than 25' to the intersection with Harrison Street or 10' from the intersection with Lansing Street. Trees shall be planted in a continuous, connected soil-filled trench of structural soils to a depth of at least 3' 6". The median shall be planted with low-growing shrubs and impervious cover shall be kept to a minimum. The median curbs shall be reinforced and include root barriers to protect the integrity of the surrounding roadway.



**ALTERNATE**  
**ACER FREEMANII 'AUTUMN BLAZE'**  
(FREEMAN MAPLE)

**Character:**

Distinct, upright ovate form in youth and when mature; deciduous; well-defined central leader with ascending branches; rapid growth rate; not as dense as other cultivars. Showy orange-red foliage during fall months, medium-green, shiny foliage in summer.

**Size:**

Height: 40'-50' | Spread: 30'-40'

**Flower/Bark:**

Non-descript flowers. The bark is smooth, whitish when young, becoming furrowed with dark ridges as it ages.

**Planting Specifications:**

New street trees must have a minimum 2" caliper at 4.5' above sidewalk grade and branch at a minimum of 8' above sidewalk grade. Trees are to be planted every 20' in sidewalk openings of at least 16 square feet, and shall not be closer than 25' to an intersection approach or 10' from the far side of the intersection. Trees shall be planted in a continuous, connected soil-filled trench of structural soils to a depth of at least 3' 6".



**UNDERSTORY PLANTING PALETTE**

Understory plantings, such as different Carex, Hemerocallis, Koeleria, Flax, Phormium, and Sedge cultivars, are required in all planters. While the general visual theme of these plantings should be consistent, variety is encouraged and the choice of specific plantings is flexible.



# 4.5 Street Trees

GUY PLACE, LANSING STREET, ZENO PLACE, GROTE PLACE, & mid-block pedestrian paths



**PYRUS CALLERYANA 'CHANTICLEER'**  
**(COLUMNAR ORNAMENTAL PEAR)**

**Character:**  
Pyramidal to columnar in youth and when mature; upright branching; oval, glossy green leaves in summer that 'dance' in breezes; attractive reddish-purple leaves in fall. Showy flowers in spring.

**Size:**  
Height: 25'-35'  
Spread: 15'

**Flower/Bark:**  
Five-petaled, creamy-white flowers in spring, showy; deeply furrowed, textured bark.

**Planting Specifications:**  
New street trees must have a minimum 2" caliper at 4.5' above sidewalk grade and branch at a minimum of 8' above sidewalk grade. Trees are to be planted every 20' in sidewalk openings of at least 16 square feet, and shall not be closer than 25' to an intersection approach or 10' from the far side of the intersection. Trees shall be planted in a continuous, connected soil-filled trench of structural soils to a depth of at least 3' 6".



**ACER RUBRUM 'BOWHALL'**  
**(COLUMNAR RED MAPLE)**

**Character:**  
Upright pyramidal, fast growth rate; deciduous; showy red-orange leaves in fall, single-trunk with upright branching; medium-textured dark green leaves in summer.

**Size:**  
Height: 45'-50'  
Spread: 18'-25'

**Flower/Bark:**  
Showy red flowers in spring; reddish-gray trunk, furrowed.

**Planting Specifications:**  
New street trees must have a minimum 2" caliper at 4.5' above sidewalk grade and branch at a minimum of 8' above sidewalk grade. Trees are to be planted every 20' in sidewalk openings of at least 16 square feet, and shall not be closer than 25' to an intersection approach or 10' from the far side of the intersection. Trees shall be planted in a continuous, connected soil-filled trench of structural soils to a depth of at least 3' 6".



**GINKGO BILOBA 'PRINCETON SENTRY'**  
**(COLUMNAR GINKGO)**

**Character:**  
Upright columnar, highly irregular picturesque branching when mature; deciduous; medium-green and unusually obovate (fan-shaped) leaves in summer, striking yellow color in fall; plant male specimens only to avoid seed dropping.

**Size:**  
Height: up to 60'  
Spread: 10'

**Flower/Bark:**  
Non-descript flowers; light brown to brownish-gray bark is deeply furrowed and becomes highly ridged with age.

**Planting Specifications:**  
New street trees must have a minimum 2" caliper at 4.5' above sidewalk grade and branch at a minimum of 8' above sidewalk grade. Trees are to be planted every 20' in sidewalk openings of at least 16 square feet, and shall not be closer than 25' to an intersection approach or 10' from the far side of the intersection. Trees shall be planted in a continuous, connected soil-filled trench of structural soils to a depth of at least 3' 6".



# 5.0 Street Furnishings & Amenities

There will be a common palette of street furnishings for Rincon Hill and Transbay. These furnishings are also described in the Transbay Redevelopment Area Streetscape and Open Space Concept Plan. The furnishings listed below must be used. However, given that manufacturers and their products come and go over time, if these furnishings are not available, a substitute comparable in aesthetics and performance may be proposed subject to the approval of the Planning Department.

**BICYCLE RACK**  
 “Welle Circular” - Square Tube  
 Manufacturer: Palmer Group  
 (www.bikeparking.com)

Bicycle racks should be installed throughout the district, at least one rack per block on each side of the street on the shorter east-west blocks (e.g. Harrison between First and Fremont Streets) and at least two on the longer north-south blocks (e.g. Fremont between Folsom and Harrison Streets). At least two bike racks should be located on each block of Folsom Street.

**TREE GRATE**  
 “Chinook” – 4’, Cast Iron  
 Manufacturer: Urban Accessories  
 (www.urbanaccessories.com)

In general, trees are to be un-grated and planted in landscaped planting beds as illustrated on the pages pertaining to each relevant street. However, there are limited locations where tree grates may be used and planting beds are not desirable or feasible in areas with high pedestrian traffic and narrower sidewalks, such as along Folsom Street. Additionally, one or two trees may be placed in grates adjacent to designated curbside loading zones. The approved grate, the Urban Accessories “Chinook” grate, is capable of being modified over time to accom-

modate the increasing trunk girth of a growing tree. There are supporting ribs for the distinctive concentric squares of the Chinook grate that can be easily scored, sawed, or ground in order to remove the innermost concentric squares and allow the tree additional space. Where tree grates are proposed, project sponsors must commit to maintaining and adjusting the tree grate over time.

**BENCHES**  
 Preferred Bench  
 “Folsom Street Custom Bench”

Manufacturer: Galanter and Jones

Contact: Office of Community Investment and Infrastructure (OCII - Successor Agency to the Redevelopment Agency)

Alternative:  
 “Knight Bench”  
 Manufacturer: Forms + Surfaces

Benches length may vary depending on the constraints of the location. Although all benches should feature backs and armrests, at least one bench in each group of benches must have armrests and a backrest of 18” minimum height.

**FOLSOM AND HARRISON STREETS AND AT TRANSIT STOPS**  
 Metal Perch Seating with Custom Back and Base  
 Manufacturer: Hess

**TRASH RECEPTACLES**  
 Dual Trash Recycling Receptacle  
 Manufacturer: Forms and Surfaces

Maximum 34” height is recommended.

**BOLLARDS**  
 “DG-5”, “DG-1” (with light incorporated)  
 Manufacturer: Urban Accessories

Minimum recommended bollard height is 3’ 6”.



**Bicycle Rack.** “Welle Circular” by Palmer Group



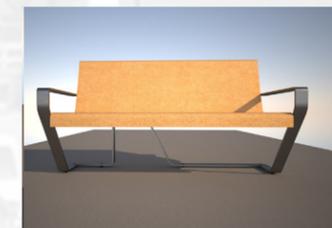
**Tree Grate.** “Chinook” 4 ft cast iron, by Urban Accessories



**Trash & Recycling.** Dual Trash Recycling Receptacle by Forms & Surfaces



**Bollard.** “DG-5” or “DG-1” (w/ light) by Urban Accessories



**Benches.** “Folsom Street Custom Bench” designed by CMG Landscape Architecture, Manufacturer: Galanter and Jones



**Benches.** “Knight Bench” by Forms + Surfaces



# 6.0 Street Lighting

One common unifying element of the public realm is the lighting scheme, whose elements include the light fixtures, illumination levels, and fixture locations. Unique light fixtures, common to Rincon Hill and Transbay, are intended to replace all of the existing street lighting in the districts, including all of the standard “Cobra” head fixtures. The fundamental principles guiding these lighting standards are:

- (1) Illumination should be oriented to the pedestrian realm, with roadway lighting serving to highlight conflict points and pedestrian crossings only at intersections and crosswalks.
- (2) The pattern of illumination and fixture placement should create a clear hierarchy and classification of streets, differentiating the function of Folsom and Harrison Streets from the more residential streets and alleys.

The City, through ordinance by the Board of Supervisors and the Mayor, have declared Rincon Hill and Transbay a unique special lighting area, due to the neighborhoods’ cohesiveness, distinctness and size.

The City has adopted the following fixtures and standards for lighting in Rincon Hill and Transbay:

**ROADWAY AND PEDESTRIAN LIGHTS:**

**Pole:** The city has commissioned Valmont Industries to manufacture a custom light pole for the Rincon Hill Streetscape Master Plan area. The light pole is available as a tall roadway light and shorter pedestrian light. Specific pole heights, luminaire arm lengths and pole spacing will vary depending on site conditions.

**Manufacturer:** Valmont Industries.  
**Luminaire:** “Lumec GPLS / GPLM”  
**Manufacturer:** Philips Lumec

Interested parties should contact SFPUC Utility Services for detailed specifications and construction standards for street lights. Current contacts are Sue Black (sblack@swater.org) and Kevin Sporer (ksporer@swater.org).

**Note:** A special streetlight configuration will be selected for Folsom Street as a special street, but this has yet to be selected. Any implementation of streetlights on Folsom will require coordination of Planning Dept, SFPUC, and SF Redevelopment Agency.

**STREET LIGHTING PATTERN:**

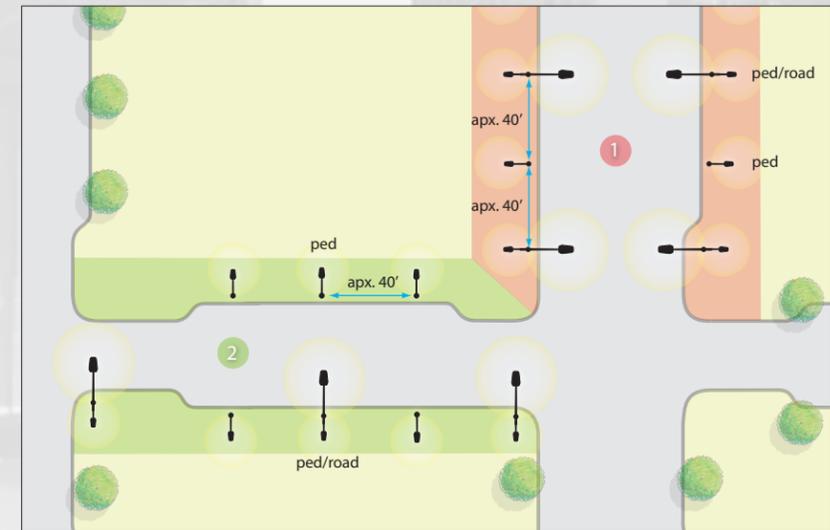
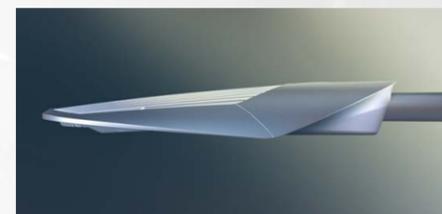
**Folsom Street:** Roadway lights, with Roadway/Pedestrian combo, four per block, spaced roughly every 75-80 feet. Roadway lights must be paired/aligned to the greatest extent feasible with roadway lights on opposite side of Folsom Street. Pedestrian lights infill midway between Roadway/Pedestrian lights (i.e. three per block). Lamping: Roadway: 100W Pedestrian: 70W.

**Spear, Main, Beale Fremont, First, Harrison Streets:** Pedestrian lights spaced every 40 feet (roughly between every other street tree), both sides of the block. One Roadway/Pedestrian combo light at each crosswalk/intersection -- one at either end of the block and one at mid-block. Lamping: Roadway: 100W Pedestrian: 70W.

**Guy Place, Lansing Street, Zeno, Grote Streets:** Alleyway light spaced 40’ apart on one side of street only. Pendant lights, suspended on a cable mounted to abutting buildings, may be substituted for pedestrian lights.

**LIGHT POLLUTION, UPLIGHTING, SUPPLEMENTAL LIGHTING**

To avoid unnecessary light pollution of the night sky and of upper level residential units, uplighting is generally not permitted, including uplighting in planters and of street trees. Luminaires with open lamps and the use of non-cutoff fixtures is prohibited. Lighting meant to supplement existing street lighting to enhance the pedestrian realm or create dramatic architectural effects (bollards, wall soffits, wall lanterns with cutoffs) should be directed downward and kept to low levels.



**1. FOLSOM STREET**

- 4 ped/road lights per block, spaced approximately every 75-80 feet; aligned
- Ped lights infill midway between ped/road lights ( three per block )

**2. SPEAR / MAIN / BEALE / FREMONT / FIRST / HARRISON STREETS**

- 1 ped/road at both block ends
- 1 ped/road light midblock
- Ped lights approximately every 40 feet, both sides of street; aligned.

# 7.0 Paving

Sidewalk paving provides the common floor that ties the public ground plane in the district together, as well as establishes “zones” of use on the sidewalk through subtle variation. Individual sidewalk paving patterns unique to a particular development are not permitted in Rincon Hill. Rather, a common vocabulary, pattern, and materials shall be used as described in this document.

## BASIC SIDEWALK

The basic sidewalk shall consist of:

- Concrete
- Light Grey color
- Light sandblast finish
- 3' x 3' scoring
- Saw-cut joints

## SIDEWALK BANDING

Bands of contrasting color and pattern are required on all streets. The pattern for each street is established on the respective pages. Materials shall be as follows:

### CURB BAND PARALLEL TO ROADWAY ON FOLSOM

- Concrete
- Medium or Dark Grey color
- Light sandblast finish
- 3' x 3' scoring
- Saw-cut joints

### CROSS-SIDEWALK BANDS PERPENDICULAR TO ROADWAY ON FOLSOM, MAIN, AND BEALE STREETS

- 4" x 4" Granite Setts or Unit Paver, or 4"x8" Unit Paver
- Dark Grey or Black

### CURB LANDSCAPING ZONE ON 12'-15' SIDEWALKS ON SPEAR, MAIN, BEALE, FREMONT, FIRST, HARRISON, AND ESSEX STREETS

- 6" x 6" Unit Paver
- Dark Grey or Black

## PARKING LANE PAVING

All on-street curbside parking lanes not used as peak-hour tow-away lanes or turning lanes should be paved with permeable unit pavers medium to dark-grey in color, designed to provide sub-surface peak-flow detention of stormwater. The specific performance measures and engineering characteristics are to be determined on a site-by-site basis in consultation with the Public Utilities Commission and the Department of Public Works.

## ALLEY PAVING (GUY PLACE, LANSING STREET, ZENO AND GROTE ALLEYS, AND ANY NEWLY CREATED ALLEYS)

Sidewalks, where present, shall be paved with the basic sidewalk pattern as described at left. Additionally, cross-sidewalk banding of a contrasting color and pattern shall extend across both sidewalks and continue across the street, perpendicular to the flow of traffic. Spacing of these bands shall be approximately every 20' aligned with tree planting.

The street surface of the alley shall be a stamped and/or colored asphalt, of a pattern and color complimentary to the cross-banding. The intent is for the alley to read as a visually uniform, cohesive surface.

The street surface of the alley shall be a stamped and/or colored asphalt, of a pattern and color complimentary to the cross-banding. The intent is for the alley to read as a visually uniform, cohesive surface from building face to building face.

## SIDEWALK VAULTS

Where sub-grade utility vaults must be located in the sidewalks, paving patterns and materials should be continued across the surface of the vaults.



BASIC 3' X 3' SIDEWALK PAVERS



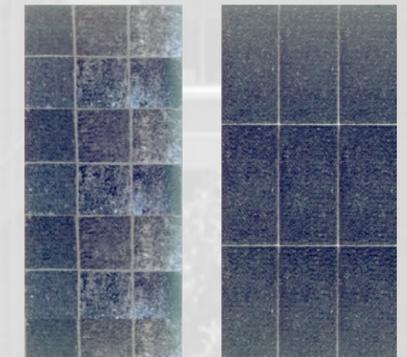
Required saw-cut joints



Paving bands



PERMEABLE PARKING PAVERS



CURB LANDSCAPING AREA

## UTILITIES

Many of the streetscape improvements proposed within this document necessitate expansion of the sidewalk area and relocation of curbs into the street.

These designs may pose conflicts with existing overhead or underground utilities. For example, overhead electrical wires may conflict with proposed street tree placement and fire hydrants and water lines may conflict with a proposed curb extension.

Project sponsors are expected to design and construct public realm improvements that are reflective of the designs articulated

in this document. City standards restrict the placement of some above ground infrastructure such as retaining walls and landscaping over certain utilities within the right-of-way. City standards also regulate the location of certain utilities within the right-of-way. For example, high-pressure fire hydrants must be located within XXX feet of the curb. Streetscape upgrades will likely necessitate the relocation of existing utilities, the costs of which will be borne by the project sponsor.

Project sponsors are encouraged to consider and analyze the location and potential

impacts local utilities may pose early on in the design process. To learn more about the City's standards and regulations concerning utilities, coordinate with the SFPUC.

See: The Better Streets Plan ([www.sfbetterstreets.org](http://www.sfbetterstreets.org)) provides guidance on design of specific streetscape features related to utility placement and relocation when installing street trees and traffic calming devices.

SFPUC Standards for the Placement of Water Facilities with Respect to Street and Sidewalk Improvements



## 8.0 Utilities

There are numerous sub-grade utilities and vaults (water, sewer, power, telecommunications) within the existing right-of-ways. The implementation of the curblines and other streetscape elements articulated in this document (e.g. required by Planning Code Section 138.1) will in some instances require some relocation or alteration of existing utilities. Per requirements of DPW, PUC or other agencies, project sponsors are required to carry out any and all utility relocations or modifications as necessary. These costs must be borne by the project sponsor. Any variation from the curblines and standards contained in this document proposed by project sponsors in order to avoid modifications of existing utilities may only be considered and approved in consultation with and at the discretion of the Planning Department.

Utility relocation costs will not typically stand as a reason for deviating from or degrading the concept designs articulated in this document. Project sponsors are encouraged to consider and analyze the location and potential impacts local utilities may pose early on in the design process. To learn more about the City's standards and regulations concerning utilities, coordinate with the SFPUC and DPW.



High Pressure (AWS) Fire Hydrant.  
Photo by Flickr user *fiveinchpixie*.



## ACKNOWLEDGEMENTS

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