



SAN FRANCISCO PLANNING DEPARTMENT

Executive Summary Conditional Use

HEARING DATE: FEBRUARY 25, 2016

Date: February 18, 2016
Case No.: **2014.0284 CUA/VAR**
Project Address: **1567 California Street**
Zoning: Polk Street NCD (Neighborhood Commercial District)
80-A Height and Bulk District
Block/Lot: 0595/013
Project Sponsor: Cyrus Sanandaji
c/o 1567 California, LLC
185 Berry Street, Suite 1200
San Francisco, CA 94107
Staff Contact: Claudine Asbagh – (415) 575-9165
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PROJECT DESCRIPTION

The project proposes to demolish the existing 11,746 square foot commercial building and associated surface parking lot, merge the three lots, and construct a new 108,389 square foot, 80-foot tall, 7-story building, with approximately 63 dwelling units and 8,000 square feet of ground-floor retail. The project includes a residential unit mix of approximately 2 studios, 37 one-bedroom units, 15 two-bedroom units, and 9 three-bedroom units. The project will satisfy the requirements of the Inclusionary Affordable Housing Program by providing two affordable units on-site and through payment of the Affordable Housing Fee for the remainder of the requirement. Open space is provided through a mix of private and common usable open spaces including a 1,510 square foot roof deck at the fifth floor and a 2,822 square foot roof deck at the seventh floor. The proposed project includes 72 Class 1 bicycle parking spaces, 6 Class 2 bicycle parking spaces, and 40 off-street parking spaces and one car-share space located within a below-grade garage accessible off of California Street.

SITE DESCRIPTION AND PRESENT USE

The Project is located on three lots at the intersection of California and Polk Streets, Block 0645, Lots 014, 014A, and 015. The development site contains 135.6 feet of frontage along the south side of California Street, and 137.6 feet of frontage along the east side of Polk Street. The project site is located within the Polk Street Neighborhood Commercial District and the 80-A Height and Bulk District. The site measures 18,625 square feet and is currently occupied by a two-story commercial building with four retail spaces and an associated surface parking lot.

SURROUNDING PROPERTIES AND NEIGHBORHOOD

The property is located at the intersection of Polk and California Streets, within the Polk Street NCD, and within half a block of the Van Ness Avenue Area Plan. The immediate vicinity is predominantly mixed-use in nature with residential uses situated above ground-floor commercial. Ground floor commercial

spaces are generally occupied by convenience and specialty uses, as well as numerous entertainment uses such as restaurants and bars. The intersecting streets adjacent to the Polk corridor tend to be more residential in character, with commercial uses interspersed on selected blocks.

The existing development in the area surrounding the Project site is varied in scale and intensity. Buildings along Polk Street and eastward on California are generally lower, ranging from single-story commercial buildings to mixed-use buildings up to four stories in height. West of the project site towards the Van Ness corridor, building heights range from two to seven stories. Building heights on the subject block range from one to five stories. The adjacent properties to the south are developed with a 5-story mixed use building with residential units over two ground floor commercial spaces, and a single story office building. The adjacent property to the east of the subject site is occupied by a single story commercial building.

ENVIRONMENTAL REVIEW

On December 2, 2015 the Project was determined to be exempt from the California Environmental Quality Act (“CEQA”) as a Class 32 Categorical Exemption under CEQA as described in the determination contained in the Planning Department files for this Project.

HEARING NOTIFICATION

TYPE	REQUIRED PERIOD	REQUIRED NOTICE DATE	ACTUAL NOTICE DATE	ACTUAL PERIOD
Classified News Ad	20 days	February 5, 2016	February 5, 2016	20 days
Posted Notice	20 days	February 5, 2016	February 5, 2016	20 days
Mailed Notice	20 days	February 5, 2016	February 5, 2016	20 days

PUBLIC COMMENT/ COMMUNITY OUTREACH

To date, the Department has received two communications in opposition to the proposal from the property owner of 1561 California Street and 31 communications in support of the proposal. Copies of correspondence have been included in the Commissioner packets. The project sponsor has conducted outreach to the adjacent neighbors and interested community groups, including the Lower Polk Neighborhood Association (LPNA), the Middle Polk Neighborhood Association (MPNA), the Lower Polk Business Collaborative, the Housing Action Coalition (HAC), SPUR, and the Polk Street Merchants Association.

ISSUES AND OTHER CONSIDERATIONS

- Conditional Use Authorization: The project requires Conditional Use Authorization to develop on a lot greater than 10,000 square feet in size, and to exceed the bulk limitations of the 80-A Height and Bulk District. The subject property is a relatively large lot compared to other properties in the vicinity. Given the dimensions of the lot, strict adherence to bulk limits would severely constrain the building envelope and could result in fewer dwelling units or less variety in unit types at a location that is appropriate for infill development. The project site is located in an area that is eclectic in terms of development scale and architectural character, with no prevailing style establishing a dominant visual pattern for the immediate neighborhood.

The project also requires a Conditional Use Authorization to create a garage entry on California Street. The project has frontages along both California and Polk Streets; however the Department prefers the garage entry be located on California Street to help minimize conflicts with other street and sidewalk users as well as to preserve the character of Polk Street.

- Rear Yard Modification: The project provides a rear yard of 25 feet where 34 feet (25%) is required. The project, therefore, requires a Rear Yard Modification from the Zoning Administrator to address the Planning Code requirements for the required rear yard (Planning Code Section 134).
- Parking Reduction: The project provides 41 parking spaces where 63 are required. The project requires a Parking Reduction from the Zoning Administrator for minimum parking requirements (Planning Code Section 161(g)).

REQUIRED COMMISSION ACTION

In order for the project to proceed, the Commission must grant Conditional Use authorization to approve development on a lot size greater than 10,000 square feet within the Polk Street NCD, to grant bulk exceptions, and to allow a garage entrance on California Street.

BASIS FOR RECOMMENDATION

- The project adds 63 dwelling units to the City's housing stock and commercial services to the Polk Street NCD.
- The residents will add to the customer base of the area, support the economic viability of the surrounding commercial establishments, and activate the sidewalks within the Polk and California Street commercial corridors.
- Public transit and neighborhood-serving commercial establishments are abundant in the area. Residents are able to walk or utilize transit to commute and satisfy convenience needs without reliance on the private automobile.
- The project has been designed with alternating facade treatments, sculpting of the upper stories, and a well-defined pedestrian realm to reduce the apparent bulk of the development and to complement the pattern of existing development in the area.
- The project is necessary and desirable, is compatible with the surrounding neighborhood, and would not be detrimental to persons or adjacent properties in the vicinity.

RECOMMENDATION:	Approval with Conditions
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Attachments:

Draft Motion
CEQA Categorical Exemption
Block Book Map
Sanborn Map
Aerial Photograph
Zoning Map
Project Sponsor Submittal Package
Correspondence Regarding Project

Attachment Checklist

- | | |
|---|---|
| <input checked="" type="checkbox"/> Executive Summary | <input checked="" type="checkbox"/> Project sponsor submittal |
| <input checked="" type="checkbox"/> Draft Motion | Drawings: <u>Existing Conditions</u> |
| <input checked="" type="checkbox"/> Environmental Determination | <input checked="" type="checkbox"/> Check for legibility |
| <input checked="" type="checkbox"/> Zoning District Map | Drawings: <u>Proposed Project</u> |
| <input checked="" type="checkbox"/> Height & Bulk Map | <input checked="" type="checkbox"/> Check for legibility |
| <input checked="" type="checkbox"/> Parcel Map | 3-D Renderings (new construction or significant addition) |
| <input checked="" type="checkbox"/> Sanborn Map | <input checked="" type="checkbox"/> Check for legibility |
| <input checked="" type="checkbox"/> Aerial Photo | |
| <input type="checkbox"/> Context Photos | |
| <input type="checkbox"/> Site Photos | |
| | <input checked="" type="checkbox"/> Housing Documents |
| | <input checked="" type="checkbox"/> Inclusionary Affordable Housing Program: Affidavit for Compliance |

Exhibits above marked with an "X" are included in this packet

Planner's Initials



SAN FRANCISCO PLANNING DEPARTMENT

Subject to: (Select only if applicable)

- Affordable Housing (Sec. 415)
- Jobs Housing Linkage Program (Sec. 413)
- Downtown Park Fee (Sec. 412)
- First Source Hiring (Admin. Code)
- Child Care Requirement (Sec. 414)
- Other

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Planning Commission Draft Motion

HEARING DATE: FEBRUARY 25, 2016

Date: February 18, 2016
Case No.: **2014.0284CUA**
Project Address: **1567 California Street**
Zoning: Polk Street NCD
 80-A Height and Bulk Districts
Block/Lot: 0645/014, 014A, and 015
Project Sponsor: Cyrus Sanandaji
 c/o 1567 California, LLC
 185 Berry Street, Suite 1200
 San Francisco, CA 94107
Staff Contact: Claudine Asbagh – (415) 588-6377
Claudine.Asbagh@sfgov.org
Recommendation: Approve with conditions

ADOPTING FINDINGS RELATING TO THE APPROVAL OF CONDITIONAL USE AUTHORIZATION PURSUANT TO PLANNING CODE SECTIONS 121.1, 155, 270 AND 303 TO DEMOLISH AN EXISTING COMMERCIAL BUILDING AND ASSOCIATED SURFACE PARKING LOT, AND CONSTRUCT A 7-STORY, 80-FOOT TALL, APPROXIMATELY 108,389 GROSS SQUARE FOOT MIXED-USE BUILDING CONTAINING 63 RESIDENTIAL DWELLING UNITS, 8,000 SQUARE FEET OF GROUND FLOOR COMMERCIAL SPACE, BELOW-GRADE PARKING FOR 40 VEHICLES, ONE CAR SHARE SPACE, 72 CLASS 1 BICYCLE PARKING SPACES AND 6 CLASS 2 SPACES, 4,959 SQUARE FEET OF PRIVATE OPEN SPACE, AND 4,332 SQUARE FEET OF COMMONLY ACCESSIBLE OPEN SPACE WITHIN THE POLK STREET NEIGHBORHOOD COMMERCIAL DISTRICT, AND THE 80-A HEIGHT AND BULK DISTRICT.

PREAMBLE

On November 12, 2014, Cyrus Sanandaji (hereinafter “Project Sponsor”) filed an application with the Planning Department (hereinafter “Department”) for Conditional Use Authorization under Planning Code Sections 121.1 and 303 to demolish an existing retail building and associated surface parking lot and construct a 7-story, 80-foot tall, approximately 108,389 gross square foot mixed-use building, containing 63 residential dwelling units, 8,000 square feet of ground floor commercial space, 15,278 square feet of below-grade parking for 40 vehicles, one car share space, 72 Class 1 bicycle parking spaces and 6 Class 2 spaces,

4,959 square feet of private code-complaint open space, and 4,332 square feet of commonly accessible open space within the Polk Street NCD, and the 80-A Height and Bulk District.

On November 12, 2014, the Project Sponsor filed an application for a rear yard modification with the Zoning Administrator to allow development within the required rear yard, pursuant to Planning Code Section 134.

On February 8, 2016 the project was determined to be exempt from the California Environmental Quality Act ("CEQA") as a Class 32 Categorical Exemption under CEQA as described in the determination contained in the Planning Department files for this project.

On February 25, 2016 the Commission conducted a duly noticed public hearing at a regularly scheduled meeting on Conditional Use Application No. 2014.0284CUA.

The Commission has heard and considered the testimony presented to it at the public hearing and has further considered written materials and oral testimony presented on behalf of the applicant, Department staff, and other interested parties.

MOVED, that the Commission hereby authorizes the Conditional Use requested in Application No. 2014.0284CUA, subject to the conditions contained in "EXHIBIT A" of this motion, based on the following findings:

FINDINGS

Having reviewed the materials identified in the preamble above, and having heard all testimony and arguments, this Commission finds, concludes, and determines as follows:

1. The above recitals are accurate and constitute findings of this Commission.
2. **Site Description and Present Use.** The Project is located on three lots at the intersection of California and Polk Streets, Block 0645, Lots 014, 014A, and 015. The development site contains 135.6 feet of frontage along the south side of California Street, and 137.6 feet of frontage along the east side of Polk Street. The project site is located within the Polk Street Neighborhood Commercial District and the 80-A Height and Bulk District. The site measures 18,625 square feet and is currently occupied by a two-story commercial building with four retail spaces and an associated surface parking lot.
3. **Surrounding Properties and Neighborhood.** The property is located at the intersection of Polk and California Streets, within the Polk Street NCD, and within half a block of the Van Ness Avenue Area Plan. The immediate vicinity is predominantly mixed-use in nature with residential uses situated above ground-floor commercial. Ground floor commercial spaces are generally occupied by convenience and specialty uses, as well as numerous entertainment uses such as restaurants and bars. The intersecting streets adjacent to the Polk corridor tend to be more residential in character, with commercial uses interspersed on selected blocks.

The existing development in the area surrounding the Project site is varied in scale and intensity. Buildings along Polk Street and eastward on California are generally lower, ranging from single-story commercial buildings to mixed-use buildings up to four stories in height. West of the project site towards the Van Ness corridor, building heights range from two to seven stories. Building heights on the subject block range from one to five stories. The properties immediately south of the site are developed with a 5-story mixed use building with residential units over two ground floor commercial spaces, and a single story office building. The property immediately east of the subject site is occupied by a single story commercial building.

4. **Project Description.** The project proposes to demolish the existing 11,746 square foot commercial building and associated surface parking lot, merge the three lots, and construct a new 108,389 square foot, 80-foot tall, 7-story building, with approximately 63 dwelling units and 8,000 square feet of ground-floor retail. The project includes a residential unit mix of approximately 2 studios, 37 one-bedroom units, 15 two-bedroom units, and 9 three-bedroom units. The project will satisfy the requirements of the Inclusionary Affordable Housing Program by providing two affordable units on-site and through payment of the Affordable Housing Fee for the remainder of the requirement. Open space is provided through a mix of private and common usable open spaces including a 1,510 square foot roof deck at the fifth floor and a 2,822 square foot roof deck at the seventh floor. The proposed project includes 72 Class 1 bicycle parking spaces, 6 Class 2 bicycle parking spaces, and 40 off-street parking spaces and one car-share space located within a below-grade garage accessible off of California Street.
5. **Public Comment/ Community Outreach.** The Department has received public correspondence both in favor of and in opposition to the proposed project. Copies of correspondence have been included in the Commissioner packets. The project sponsor has conducted outreach to the adjacent neighbors and interested community groups, including the Lower Polk Neighborhood Association (LPNA), the Middle Polk Neighborhood Association (MPNA), the Lower Polk Business Collaborative, the Housing Action Coalition (HAC), SPUR, and the Polk Street Merchants Association. The primary outcome of these meetings was an increase in the number of units provided on site as well as an increase in the size and number of bedrooms offered.
6. **Planning Code Compliance:** The Commission finds that the Project is consistent with the relevant provisions of the Planning Code in the following manner:
 - A. **Basic Floor Area Ratio.** In the Polk Street NCD, Code Section 124 allows a Floor Area Ratio (FAR) of up to 2.5 to 1. The Project Site has an area of 18,625 square feet; therefore the allowable FAR would permit a building of up to 46,562 square feet of Gross Floor Area as defined in Code Section 102.9.

Pursuant to Code Section 124(b), Floor Area Ratio limits do not apply to residential uses or non-accessory off-street parking. Subtracting the area of these uses, approximately 8,001 square feet of Gross Floor Area within the Project would be subject to the allowable FAR, resulting in an FAR of .4 to 1. The Project therefore complies with the maximum allowable FAR.
 - B. **Rear Yard.** Section 134(a) (1) of the Planning Code requires a rear yard equal to 25 percent of

the lot depth to be provided at every residential level. The Project Site has a lot depth of approximately 137 feet; therefore a rear yard measuring 34 feet in depth is required.

The project does not meet the required rear yard depth per Planning Code Section 134. The Project is seeking a rear yard modification from the Zoning Administrator to address the rear yard requirement per Section 134(e) as part of the related Case No. 2014.0284VAR.

- C. **Streetscape Improvements.** Section 138.1(b) requires new construction projects with 250 feet of frontage or more to provide streetscape improvements consistent with the Better Streets Plan. Under Section 138.1(c), the Commission may also require the Project Sponsor to install additional sidewalk improvements such as lighting, special paving, seating and landscaping in accordance with the guidelines of the Downtown Streetscape Plan if it finds that these improvements are necessary to meet the goals and objectives of the General Plan.

The Project will include new street trees consistent with the requirements of Planning Code Section 138.1(c)(1). The Project will also include streetscape elements such as Class 2 bicycle parking racks and a corner bulb out consistent with Planning Code Section 138.1.

- D. **Usable Open Space.** Planning Code Section 135(d) allows residential projects in neighborhood commercial districts to provide the amount of open space required in the nearest residential district. The nearest residential district is RC-4, which requires 36 square feet of open space per unit if private, and 48 square feet of open space per unit if common. This Section specifies that an outdoor area must meet minimum requirements for area, horizontal dimensions, and exposure to light and air to be considered usable open space.

The Project includes private open space that meets minimum dimension requirements for 13 units. The project includes additional private balconies, however since they do not meet minimum requirements; they do not count towards usable open space. The project provides common open space for the remaining 50 units. Therefore, the Project is required to provide 468 square feet of private open space for 13 units and 2,400 square feet of common open space for the remaining 50 units.

Currently, the project provides 4,959 square feet of private open space via private balconies and decks. The project proposes 4,332 square feet of common open for 50 units via a fifth floor roof deck (approximately 1,510 square feet) and a seventh floor roof deck (approximately 2,822 square feet). Therefore, the project complies with the open space requirements of the Planning Code.

- E. **Dwelling Unit Exposure.** Section 140 of the Planning Code requires that at least one room of all dwelling units face onto a public street, a rear yard, or other open area that meets minimum requirements for area and horizontal dimensions. To meet exposure requirements, a public street or public alley must be at least 20 feet, a side yard or rear yard must be at least 25 feet in width, or an open area (inner or outer court) must be no less than 25 feet in every horizontal dimension for the floor at which the dwelling unit is located and the floor immediately above it, with an increase of five feet in every horizontal dimension at each subsequent floor.

All of the dwelling units face onto either Polk or California Streets or overlook a code-compliant open area. Therefore, the Project complies with the Planning Code requirements for dwelling unit exposure.

- F. **Street Frontage.** Per Planning Code Section 145.1, the treatment of the street frontages at the project shall be designed to preserve, enhance and promote attractive, clearly defined street frontages that are pedestrian-oriented, fine-grained, and which are appropriate and compatible with the buildings and uses in the NC Districts. The project requires that 60 percent of the building perimeter at the ground floor be transparent and the first 25 feet of the ground floor to be devoted to active uses. Spaces such as lobbies are considered active uses only if they do not exceed 40 feet and spaces such as restrooms, bike parking, and other service areas are not considered "active uses".

The majority of the Project's street-facing ground floor frontages along Polk Street and California Street are devoted to retail store fronts. The Project provides 14-foot ground floor ceiling heights, and 71.4 percent fenestration along Polk Street and 68.9 percent fenestration along California Street. The Project's parking garage has one access point on California Street and is situated below grade so that it is not visible from the street. Additionally, the frontage devoted to the garage door has been minimized so as not to interfere with the street-fronting active uses. Therefore, the Project complies with the Code's street frontage requirements.

- G. **Off-Street Parking.** Section 151 establishes off-street parking requirements for all uses in all districts. Pursuant to this section, one independently accessible space is required for each dwelling unit, as well as additional parking for commercial uses that exceed 5,000 square feet of occupied floor area. 150% of the required parking is permitted as accessory parking. The project proposes 63 dwelling units, and approximately 8,000 square feet of retail space (divided between six spaces, all less than 5,000 square feet). The Project therefore requires 63 independently accessible parking spaces. A project may request a reduction to the off-street parking requirements from the Zoning Administrator pursuant to Section 307(h)(2).

The Project will provide 32 residential parking spaces and 8 retail parking spaces, plus one dedicated car share space. The Project seeks a parking reduction pursuant to Planning Code Section 307(i). Conformance with the specified criteria is discussed under item #10 below.

- H. **Off-Street Loading.** Section 152 provides a schedule of required off-street freight loading spaces for all uses in districts other than C-3 or South of Market.

Pursuant to Section 152, residential uses with less than 100,000 square feet of gross floor area and retail uses with less than 10,000 square feet of gross floor area require no off-street freight loading spaces. The Project proposes a total of 8,000 square feet of retail space and approximately 97,000 square feet of residential space. No off-street loading space is required, however, a loading area in the below grade parking garage will be provided.

- I. **Bicycle Parking.** Per Planning Code Section 155.2.11, one Class 1 bicycle parking space per unit up to 100 units, and one additional space per each 4 units above 100 units are required for new construction projects. In addition, one Class 2 bicycle parking space is required per each 20 units. For the retail use, one Class 1 bicycle parking space is required for every 7,500 square feet of occupied floor area and one Class 2 space for every 2,500

square feet of occupied floor area. For the 8,001 square feet of retail space, one additional Class 1 space and three additional Class 2 spaces are required.

The Project includes 63 dwelling units and 8,000 square feet of retail use. Therefore, the Project is required to provide 64 Class 1 bicycle parking spaces and 6 Class 2 bicycle parking spaces. The Project will provide 72 Class 1 bicycle parking spaces and six Class 2 bicycle parking spaces, thus exceeding the requirement. Therefore, the Project complies with Planning Code Section 155.2.

- J. **Car Share Parking.** Section 166 of the Planning Code requires no fewer than one car share parking space for every 50-200 residential units.

The project proposes one car share space located in the below-grade garage. The car share space shall be designed to comply with the Zoning Administrator Bulletin for car share spaces.

- K. **Use and Density.** Planning Code Sections 207.4 and 723.91 permit residential uses within the Polk Street Neighborhood Commercial District at a maximum density of no less than one dwelling unit for each 400 square feet of lot area, and up to the dwelling unit density permitted in the nearest Residential District. The closest residential district is the RC-4 District, which permits one dwelling unit for each 200 square feet of lot area. Planning Code Sections 121.2 and 723.21 principally permit non-residential uses below 2,499 square feet in size.

The Project proposes a total of 63 dwelling units for the subject property. Based on the allowable density specified by Planning Code Sections 207.4 and 723.91, up to 93 dwelling units would be allowed on the subject property. The Project also proposes six separate ground-floor commercial spaces, individually ranging in size from 683 square feet up to 2,368 square feet. Each individual space is smaller than the permitted non-residential use size of 2,499 square feet. Both the residential and commercial components of the Project conform to the use and density allowed by the Planning Code.

Development of a lot greater than 10,000 square feet in size within the Polk NCD requires Conditional Use authorization. Conformance with the specified criteria is discussed under item #8 below.

- L. **Height and Bulk.** The subject property is located within an 80-A Height and Bulk District. Within this District, roof heights of buildings are limited to 80 feet. Section 260(b) allows elevator penthouses to exceed the maximum roof height by an additional 16 feet, and stair penthouses to exceed the maximum roof height by an additional 10 feet. Maximum bulk dimensions apply to portions of the building above 40 feet in height. Above 40 feet, the building may not exceed a length of 110', or a diagonal dimension of 125'.

The finished roof of the proposed Project would reach a maximum height of approximately 80 feet. In addition, structures are located on the roof that include penthouses for the elevator and stairs. The elevator penthouse is the tallest of these features, and would reach a height of 14.5 feet above the finished roof, for a maximum structure height of approximately 94.5 feet. The finished roof and the cited rooftop features comply with the applicable regulations of the Code, therefore the Project

complies with the maximum allowable height within the 80-A Height and Bulk District.

Portions of the building above the fourth floor exceed 40 feet in height; therefore the fourth through seventh floors are subject to the bulk limitations of the 80-A Height and Bulk District. This District allows a maximum building length of 110 feet, and a maximum diagonal dimension of 125 feet. The fourth floor of the building has a length of 135 feet along Polk Street and the fourth through sixth floors of the building have a length of approximately 134 feet along California Street; therefore these floors exceed the maximum permitted length dimension. The fourth floor has a diagonal dimension of approximately 185 feet, and the fifth and sixth floors have a diagonal dimension of approximately 152 feet. Therefore, these floors exceed the maximum permitted diagonal dimension. The Commission may allow the Project to exceed the specified bulk limits after considering the criteria specified in Section 271(c), through the Conditional Use Authorization process. Conformance with these criteria is discussed under item #9 below.

- M. **Shadows on Parks.** Pursuant to Section 295, no building permit authorizing the construction of any structure exceeding 40 feet in height that will cast any shade or shadow upon any property under the jurisdiction of the Recreation and Park Commission during the times of one hour after sunrise and one hour before sunset, all year round, may be issued except on prior action of the Commission pursuant to the provisions of this Section. The Commission must conduct a hearing and must disapprove the issuance of any building permit governed by the provisions of this Section if it finds that the proposed project will have any adverse impact on the use of the property under the jurisdiction of the Recreation and Park Commission because of the shading or shadowing that it will cause, unless it is determined that the impact would be insignificant.

The Planning Department conducted a preliminary shadow fan analysis and determined that the proposed project would not cast new shadow on public spaces protected under Planning Code Section 295.

- N. **Affordable Housing.** Planning Code Section 415 sets forth the requirements and procedures for the Inclusionary Affordable Housing Program. Under Planning Code Section 415.3, the current percentage requirements apply to projects that consist of ten or more units, where the first application (EE or BPA) was applied for on or after July 18, 2006. Pursuant to Planning Code Section 415.5, the Project must pay the Affordable Housing Fee (“Fee”) or provide on-site or off-site affordable units. This Fee is made payable to the Department of Building Inspection (“DBI”) for use by the Mayor’s Office of Housing and Community Development for the purpose of increasing affordable housing citywide.

The Project Sponsor will satisfy the requirements of the Inclusionary Affordable Housing Program by providing a portion of the affordable housing on-site and the remaining requirement through payment of the Affordable Housing Fee.

The Project Sponsor submitted an “Affidavit of Compliance with the Inclusionary Affordable Housing Program: Planning Code Section 415”, on October 22, 2015 indicating that 26% of the affordable housing requirement would be provided as onsite units (two one-bedroom units) and that the remaining balance of the requirement (74%) would be paid as an in lieu fee. This fee is an amount to

be established by the Mayor's Office of Housing and Community Development at a rate equivalent to an off-site requirement of 20%. If the Project becomes ineligible to meet its Inclusionary Affordable Housing Program obligation through the On-site Affordable Housing Alternative, it must pay the Affordable Housing Fee with interest, if applicable. The EE application was submitted on May, 16, 2014.

7. **Planning Code Section 303** establishes criteria for the Planning Commission to consider when reviewing applications for Conditional Use approval. On balance, the project complies with said criteria in that:

- A. The proposed new uses and building, at the size and intensity contemplated and at the proposed location, will provide a development that is necessary or desirable, and compatible with, the neighborhood or the community.

The proposed Project is necessary and desirable in that it will provide 63 new dwelling-units to the City's housing stock, including 24 units that have two or three bedrooms and are appropriate for larger households. The Project will add housing opportunities within the Polk Street NCD at a density that is suitable for an intensely-developed urban area served by ample public transit and retail services. Additionally, the project will contribute to the commercial activity of the district by replacing the existing retail frontages along Polk Street and adding new retail frontages along California Street.

The existing development in the area surrounding the Project site is varied in scale and intensity. Buildings along Polk Street and eastward on California are generally lower, ranging from single-story commercial buildings to mixed-use buildings up to four stories in height. West of the project site towards the Van Ness corridor, building heights range from two to seven stories. Building heights on the subject block range from one to five stories. While the Project is taller than some adjacent buildings, the design incorporates off-setting planes, varied facade treatments, and staggered rooflines, to divide the elevation into discrete sections that complement the surrounding built environment. The project would be compatible with the surrounding uses and buildings and be a positive contribution to the Polk Street neighborhood and community.

- B. The proposed project will not be detrimental to the health, safety, convenience or general welfare of persons residing or working in the vicinity. There are no features of the project that could be detrimental to the health, safety or convenience of those residing or working the area, in that:

- i. Nature of proposed site, including its size and shape, and the proposed size, shape and arrangement of structures;

The Project site is adequately sized to accommodate the development. Existing development in the vicinity varies in size and intensity, and the Project is generally compatible with the eclectic character of the area.

The upper stories of the Project have been sculpted to transition to the scale of adjacent properties and reduce the apparent bulk of the development. The project proposes an open outer court at the southeast corner of the site that will help buffer the new building from adjacent properties and strengthen a pattern of mid-block open space that is currently not well-defined on the subject

block. Therefore, the Project will not be detrimental to persons or adjacent properties in the vicinity.

- ii. The accessibility and traffic patterns for persons and vehicles, the type and volume of such traffic, and the adequacy of proposed off-street parking and loading;

As discussed in the Categorical Exemption, the project would not result in a significant net increase in vehicular traffic, and would not negatively affect transit services or have significant adverse impacts on pedestrians or bicyclists.

The Project would not adversely affect public transit or overburden the existing supply of parking in the neighborhood because the project site is well-served by public transit. The project site is within ½ a mile of 12 MUNI lines including the California-1, Clement-2, Jackson-3, Folsom/Pacific-12, Polk-19, Bryant-27, Geary-38 and 38L, Van Ness-47, Van Ness/Mission-49, 90-Owl, and the California Cable Car. The Project provides a limited amount of off-street parking in support of the City's transit first policies. This off-street parking is located in a below-grade garage, which is consistent with the requirements for the Polk Street NC Zoning District. The garage would be accessible from California Street, in order to minimize pedestrian or other conflicts on Polk Street. Provision of bicycle storage areas along with the close proximity to mass transit is anticipated to encourage residents, employees and visitors to use alternate modes of transportation.

- iii. The safeguards afforded to prevent noxious or offensive emissions such as noise, glare, dust and odor;

The Project includes residential and retail uses that are typical of the surrounding context, and will not introduce operational noises or odors that are detrimental, excessive, or atypical for the area. While some temporary increase in noise can be expected during construction, this noise is limited in duration and will be regulated by the San Francisco Noise Ordinance which prohibits excessive noise levels from construction activity and limits the permitted hours of work. The Project Sponsor will be required to spray the site to suppress dust during demolition, excavation, and construction; therefore, these activities should not generate significant airborne dust. The building will not exhibit an excessive amount of glazing or other reflective materials; therefore, the Project is not expected to cause offensive amounts of glare.

- iv. Treatment given, as appropriate, to such aspects as landscaping, screening, open spaces, parking and loading areas, service areas, lighting and signs;

The Project proposes a variety of common and private open spaces in the form of balconies, decks, and two landscaped roof decks. The conceptual plans show landscaping in the form of street trees and other plantings along California and Polk Streets, as well as trees and shrubs within the outer court. Parking is located within a subterranean garage that is not readily visible from the street, except for the access driveway. Conditions of approval require that, as the Project proceeds through the review of building permits, the Project Sponsor will continue to work Planning staff to refine details of lighting, signage, materials, and other aspects of the project.

- C. That the use as proposed will comply with the applicable provisions of the Planning Code and will not adversely affect the General Plan.

The Project complies with all relevant requirements and standards of the Planning Code and is consistent with objectives and policies of the General Plan as detailed below.

- D. That the use as proposed would provide development that is in conformity with the purpose of the applicable Neighborhood Commercial District.

Section 723.1 identifies the Polk Street NCD as a linear, dense mixed-use corridor that consists of residential units above ground-story commercial uses. The Project conforms to the purposes of the Polk Street NCD in that it would retain the procession of ground-level retail uses found along Polk Street and add new retail along California Street that will help create pedestrian and commercial activity in the area. Additionally, housing development is specifically encouraged on upper stories within the District.

8. **Planning Code Section 121.1** establishes criteria for the Planning Commission to consider when reviewing applications for projects within the Polk Street Neighborhood Commercial District on lots that exceed 10,000 square feet, through the Conditional Use authorization process. On balance, the project complies with said criteria in that:

- a. The mass and facade of the proposed structure are compatible with the existing scale of the district.

The existing development in the area surrounding the Project site is varied in scale and intensity. Residential and mixed-use buildings within the Van Ness corridor to the west are generally situated on large lots and are of a higher intensity than surrounding development, with several buildings to the southwest of the project site in excess of seven stories. Building heights on the subject block range from one to five stories in height. Buildings along Polk Street and along California to the east are generally lower, ranging from single-story commercial buildings to mixed-use buildings up to four stories in height.

The Project will merge three lots to create one square parcel with frontages on both California and Polk Streets. The building has been situated on the site so that the majority of its mass falls at the northwest corner adjacent to the street intersection. The building steps down as it progresses away from the intersection along California and Polk Streets, and further steps down as it progresses to an open court located at the southeast corner of the site. The use of varied window proportions, balconies, and multiple exterior materials further assists in minimizing the appearance of bulk. The Project will complement and be harmonious with the surrounding neighborhood character. At the streetscape, the facade incorporates features that anchor the building and define a pedestrian scale, such as detailing that frames the retail spaces and the entry lobby.

- b. The facade of the proposed structure is compatible with the design features of adjacent facades that contribute to the positive visual qualities of the district.

Existing buildings in the area exhibit an eclectic architectural character, with no prevailing style establishing a dominant visual pattern for the neighborhood. One-story retail commercial buildings are interspersed with multi-story, mixed-use structures.

Both the California and Polk Street façades express a high ratio of wall to glazing, similar to older residential and mixed-use buildings in the area. The building incorporates forms and detailing that are familiar to the older buildings in the area while harmonizing with newer contemporary structures. Additionally, the massing of the ground floor retail is divided into multiple tenant spaces that will help reinforce the neighborhood commercial scale of the building.

9. **Planning Code Section 271** establishes criteria for the Planning Commission to consider when reviewing application for projects that exceed the applicable bulk limits, through the Conditional Use Process. Such deviation might occur for one specified positive reasons. The Project appears to meet one of the specified reasons, in that:

- a. Achievement of a distinctly better design, in both a public and a private sense, than would be possible with strict adherence to the bulk limits, avoiding an unnecessary prescription of building form while carrying out the intent of the bulk limits and the principles and policies of the General Plan.

The Project Site is a lot that is relatively large for the District. Given the dimensions of the lot, strict adherence to bulk limits would severely constrain the building envelope and could result in an awkward building form. In addition, the number of residential units could be sharply reduced, resulting in less housing in a location that is appropriate for infill development. The Project incorporates facade variations and sculpting on upper floors to reduce the apparent bulk of the Project, as discussed in item 9(B) below.

On balance, the Project complies with the aforementioned criteria, in that:

- b. The appearance of bulk in the building, structure, or development shall be reduced by means of at least one and preferably a combination of the following factors, so as to produce the impression of an aggregate of parts rather than a single building mass:
- i. Major variations in the planes of wall surfaces, in either depth or direction, that significantly alter the mass.

The building utilizes a "block" motif as well as horizontal and vertical banding to provide variation in the façade. The Polk Street facade has been articulated to follow the slope of the street frontage by stepping down the horizontal banding above the commercial level. This stepping down of major building elements also brings the scale of the commercial level into alignment with the neighboring building on Polk Street to the Southeast. The Project employs the same articulated banding on the California Street facade to create a consistent visual pattern for the interface between the commercial and residential levels of the building.

The facade utilizes balconies, punched windows, and significant changes in plane to create a rhythm of voids and projections. These features also create depth and shadow, and help to lessen the apparent mass of the building.

- ii. Significant differences in the heights of various portions of the building, structure, or development that divide the mass into distinct elements.

As mentioned above, the building massing has been shifted towards the intersection of California and Polk Streets to help moderate the building massing. As the building progresses south and east, the massing and setbacks increase to provide a transition between lower building heights and to help preserve and define the midblock open space. The variation of stories and setbacks will help relate the scale of the building to lower buildings in the vicinity. Active retail uses will be located on the ground floor.

- iii. Differences in materials, colors, or scales of the facades that produce separate major elements.

The alternating facade treatments and changes in plane create separate major elements within the elevations of the building. The conceptual elevations show changes in materials and architectural expression that correspond with the transitions between these elements. The project will utilize wood and aluminum panels of varying color to define and accentuate the composite pieces. As the Project proceeds through the review of building permits, the Project Sponsor will continue to work the Planning staff to refine details regarding materials and colors that will express the changes in facade treatment and minimize the apparent bulk of the Project.

- iv. Compensation for those portions of the building, structure, or development that may exceed the bulk limits by corresponding reduction of other portions below the maximum bulk permitted.

The bulk limitations of the 80-A Height and Bulk District apply to portions of the structure above 40 feet in height. For the Project, this height corresponds with the upper portions of the fourth floor, as well as the fifth through sixth floors. Portions of the building above the third story along the southwestern portion of the site are substantially set back from both the street, the rear (south) and side (east) property lines. This stepping back of the structure helps break the mass of the building to help it fit into context with the surrounding buildings that are of a lower scale. As a result of these setbacks, portions of the building are reduced below the maximum bulk allowed.

- v. In cases where two or more buildings, structures, or tower are contained within a single development, a wide separation between such buildings, structures, or towers.

The Project consists of a single building; therefore, this factor does not apply.

- c. In every case the building, structure, or development shall be made compatible with the character and development of the surrounding area by means of all of the following factors:

- vi. A silhouette harmonious with natural landforms and building patterns, including the patterns produced by height limits.

The changes in plane across the street-facing elevations reduce the apparent height of the building, break the roofline of the Project, and relate to the varied scale of adjacent buildings. The silhouette of the building, therefore, does not read as a uniform mass, but rather of a series of separate blocks that appear as multiple structures. Although the project is taller than structures in the immediate area, the project conforms to the height limit for the district and buildings of seven stories or greater can

be found in the vicinity along California Street. The silhouette is generally harmonious with the building pattern of the area.

- vii. Either maintenance of an overall height similar to that of surrounding development or a sensitive transition, where appropriate, to development of a dissimilar character.

The alternating roof heights and setbacks at the fourth through seventh stories along Polk Street help moderate the building massing and provide transition to lower building heights along Polk Street. The Project provides gradual stepping away from the southeast corner of the site to provide a transition to lower structures adjacent to the site and help define the mid-block open space.

- viii. Use of materials, colors, and scales either similar to or harmonizing with those of nearby developments.

Existing buildings in the vicinity exhibit an eclectic variety of architectural character, materials, and color, with no predominant styles or materials that wholly define the visual character of the neighborhood. Both the California and Polk Street façades express a high ratio of wall to glazing, similar to older residential and mixed-use buildings in the area. The alternating facade treatments and changes in plane help transition to lower buildings and lessen the apparent scale of the project. As the Project proceeds through the review of building permits, the Project Sponsor will continue to work with Department staff to refine details regarding materials and colors that will complement the existing built environment of the area.

- ix. Preservation and enhancement of the pedestrian environment by maintenance of pleasant scale and visual interest.

The Project creates streetscape interest through the use of active, transparent retail storefronts on the California and Polk Street frontages. At the streetscape, the facade incorporates features that anchor the building and define a pedestrian scale, such as awnings and details that frame the retail spaces and the entry lobby.

- d. While the above factors must be present to a considerable degree for any bulk limit to be exceeded, these factors must be present to a greater degree where both the maximum length and the maximum diagonal dimension are to be exceeded than where only one maximum dimension is to be exceeded.

The Project Site is a lot that is relatively large for the District. The Project exceeds the allowable bulk limitations on the fourth, fifth, and sixth floors. Given the dimensions of the lot, strict adherence to bulk limits would severely constrain the building envelope. Such constraints could result in an awkward building form. In addition, the number of residential units or variety of unit types could be reduced, resulting in less housing in a location that is rich in transit and commercial services, and is highly suited to infill development. The project incorporates significant variations in facade treatments, a well-defined pedestrian realm at the streetscape, and sculpting of the upper stories that reduce the apparent size of the project and maintain a facade rhythm that is compatible with development on narrower lots in the vicinity.

10. **Planning Code Section 307(i)** establishes criteria for the Planning Commission to consider when reviewing applications that request a reduction or modification of Off-Street Parking requirements. On balance, the project complies with said criteria in that:

- a. The reduction in the parking requirement is justified by the reasonably anticipated automobile usage by residents of and visitors to the project; and

The Project provides 63 new dwelling units in a location that is well served by public transit and has abundant commercial services nearby. The Project provides a limited amount of off-street parking in support of the City's transit first policies. Provision of bicycle storage areas along with the close proximity to mass transit is anticipated to encourage residents, employees and visitors to use alternate modes of transportation.

- b. The reduction in the parking requirement will not be detrimental to the health, safety, convenience, or general welfare of persons residing in or working in the vicinity.

As discussed in the Categorical Exemption, the project would not result in a significant net increase in vehicular traffic, and would not negatively affect transit services nor have significant adverse impacts on pedestrians or bicyclists. The Project would not adversely affect public transit or overburden the existing supply of parking in the neighborhood because the project site is well-served by public transit.

- c. The minimization of conflict of vehicular and pedestrian movements.

The project would provide 40 off-street parking spaces and one car share space located in a below-grade garage that is consistent with the location requirements for the Polk Street NC Zoning District. The garage would be accessible from California Street so as to minimize pedestrian or other conflicts on Polk Street.

- d. The availability of transportation modes other than the automobile.

The project site is within ½ a mile of 12 MUNI lines including the California-1, Clement-2, Jackson-3, Folsom/ Pacific-12, Polk-19, Bryant-27, Geary-38 and 38L, Van Ness-47, Van Ness/Mission-49, 90-Owl, and the California Cable Car.

- e. The pattern of land use and character of development in the vicinity.

Section 723.1 identifies the Polk Street NCD as a linear, dense mixed-use corridor that consists of residential units above ground-story commercial uses. The Project conforms to the purposes of the Polk Street NCD in that it would retain the procession of ground-level retail uses found along Polk Street and add new retail along California Street that will help create pedestrian and commercial activity in the area. Additionally, housing development is specifically encouraged on upper stories within the District.

11. **General Plan Compliance.** The Project is, on balance, consistent with the following Objectives and Policies of the General Plan:

HOUSING ELEMENT

Objectives and Policies

OBJECTIVE 1:

IDENTIFY AND MAKE AVAILABLE FOR DEVELOPMENT ADEQUATE SITES TO MEET THE CITY'S HOUSING NEEDS, ESPECIALLY PERMANENTLY AFFORDABLE HOUSING.

Policy 1.1:

Plan for the full range of housing needs in the City and County of San Francisco, especially affordable housing.

Policy 1.10:

Support new housing projects, especially affordable housing, where households can easily rely on public transportation, walking and bicycling for the majority of daily trips.

The Project proposes a broad range of studio, one-, two- and three-bedroom units. The requirements of the Inclusionary Affordable Housing Program are proposed to be met through a combination of two on-site BMR units and payment of the in-lieu fee to cover the balance of the requirement. The Project will bolster the pedestrian- and transit-orientation of the District by encouraging residents to walk and utilize transit to satisfy shopping and convenience needs.

OBJECTIVE 4:

FOSTER A HOUSING STOCK THAT MEETS THE NEEDS OF ALL RESIDENTS ACROSS LIFECYCLES.

Policy 4.1:

Develop new housing, and encourage the remodeling of existing housing, for families with children.

The Project will provide 24 units that have two or three bedrooms and are therefore appropriate for larger households.

OBJECTIVE 11:

SUPPORT AND RESPECT THE DIVERSE AND DISTINCT CHARACTER OF SAN FRANCISCO'S NEIGHBORHOODS.

Policy 11.2

Ensure housing is provided with adequate public improvements, services, and amenities.

The project site is located within a developed, urban context that is well-served by transit and other services and amenities. Approximately 12 MUNI bus lines can be accessed within a ½ mile of the subject property. A wide spectrum of commercial services can be found within the Polk Street, Pacific Avenue, and Van Ness Avenue corridors. The subject property is appropriate for infill development, and the dense, mixed-use character of the project will contribute to the vitality, activity, and walkable urban character of the area.

COMMERCE AND INDUSTRY ELEMENT

Objectives and Policies

OBJECTIVE 1:

MANAGE ECONOMIC GROWTH AND CHANGE TO ENSURE ENHANCEMENT OF THE TOTAL CITY LIVING AND WORKING ENVIRONMENT.

Policy 1.1:

Encourage development which provides substantial net benefits and minimizes undesirable consequences. Discourage development that has substantial undesirable consequences that cannot be mitigated.

The Project will have no significant adverse environmental impacts and will provide 63 new dwelling-units to the City's housing stock, including 24 units that have two or three bedrooms and are appropriate for larger households. The Project will add housing opportunities within the Polk Street NCD at a density that is suitable for an intensely-developed urban area served by ample public transit and retail services. Additionally, the project will contribute to the commercial activity of the district by replacing the existing retail frontages along Polk Street and adding new retail frontages along California Street. Because of the Project site's proximity to numerous MUNI lines and the Van Ness corridor, Project residents will be able to easily rely on public transportation rather than cars.

OBJECTIVE 6:

MAINTAIN AND STRENGTHEN VIABLE NEIGHBORHOOD COMMERCIAL AREAS EASILY ACCESSIBLE TO CITY RESIDENTS.

Policy 6.1:

Ensure and encourage the retention and provision of neighborhood-serving goods and services in the city's neighborhood commercial districts, while recognizing and encouraging diversity among the districts.

Policy 6.3:

Preserve and promote the mixed commercial-residential character in neighborhood commercial districts. Strike a balance between the preservation of existing affordable housing and needed expansion of commercial activity.

Policy 6.9:

Regulate uses so that traffic impacts and parking problems are minimized.

The Project will contribute to the mixed-use character of the Polk Street NCD by adding residential units over a base of ground-floor commercial spaces. These commercial spaces will provide for the convenience needs of area residents, as well as create employment and business ownership opportunities. The Project will bolster the pedestrian- and transit-orientation of the District by encouraging residents to walk and utilize transit to satisfy shopping and convenience needs. The proposed commercial spaces are relatively modest and are not expected to draw significant traffic from outside of the neighborhood. The Project should therefore not result in significant parking or traffic impacts.

URBAN DESIGN ELEMENT

Objectives and Policies

OBJECTIVE 1:

EMPHASIS OF THE CHARACTERISTIC PATTERN WHICH GIVES TO THE CITY AND ITS NEIGHBORHOODS AN IMAGE, A SENSE OF PURPOSE, AND A MEANS OF ORIENTATION.

Policy 1.2:

Recognize, protect and reinforce the existing street pattern, especially as it is related to topography.

The Project's height of 80 feet, with a step down to 47 feet on the southern end of the Polk Street frontage is compatible with other developments within the Polk Street NCD. As discussed above, the Project's massing, articulation, and scale are in-keeping with existing neighborhood patterns and residential uses.

OBJECTIVE 3

MODERATION OF MAJOR NEW DEVELOPMENT TO COMPLEMENT THE CITY PATTERN, THE RESOURCES TO BE CONSERVED, AND THE NEIGHBORHOOD ENVIRONMENT.

Policy 3.6:

Relate the bulk of buildings to the prevailing scale of development to avoid an overwhelming or dominating appearance in new construction.

The massing and bulk of the Project have been moderated to provide a transition between the mix of older and more modern structures found throughout the Polk Street NCD. Additionally, the Project will add 14 street trees along its Polk Street and California Street frontages which will ground the Project and provide pedestrian scale to the blocks.

TRANSPORTATION ELEMENT

Objectives and Policies

OBJECTIVE 2:

USE THE TRANSPORTATION SYSTEM AS A MEANS FOR GUIDING DEVELOPMENT AND IMPROVING THE ENVIRONMENT.

Policy 2.1:

Use rapid transit and other transportation improvements in the city and region as the catalyst for desirable development, and coordinate new facilities with public and private development.

Policy 2.2:

Reduce pollution, noise and energy consumption.

Due to the abundant transit and commercial services in the area, residents of the Project can minimize use of the private automobile to commute and meet basic needs. The Project site is suitable for accommodating dense residential development that will discourage sprawling regional development patterns that are strongly auto-oriented and contribute to greenhouse gas emissions.

12. **Planning Code Section 101.1(b)** establishes eight priority-planning policies and requires review of permits for consistency with said policies. On balance, the project does comply with said policies in that:

- A. That existing neighborhood-serving retail uses be preserved and enhanced and future opportunities for resident employment in and ownership of such businesses be enhanced.

The Project would require the removal of the existing retail businesses; however, new commercial spaces will be included in the new building that will provide local business ownership and employment opportunities. In addition, the new residents in the Project will patronize area businesses, bolstering the viability of surrounding commercial districts.

- B. That existing housing and neighborhood character be conserved and protected in order to preserve the cultural and economic diversity of our neighborhoods.

The project will not diminish the existing housing stock, and will add dwelling units in a manner that enhances the vitality of the surrounding commercial corridors.

- C. That the City's supply of affordable housing be preserved and enhanced.

The Project would add not demolish any dwelling units, and will comply with the City's Affordable Housing Program through a combination of on-site affordable units, and the payment of an Affordable Housing Fee.

- D. That commuter traffic not impede MUNI transit service or overburden our streets or neighborhood parking.

The property is located within the Polk Street NCD, and a wide variety of goods and services are available within walking distance of the subject property. In addition, the area is well served by public transit, providing connections to all areas of the City and to the larger regional transportation network.

- E. That a diverse economic base be maintained by protecting our industrial and service sectors from displacement due to commercial office development, and that future opportunities for resident employment and ownership in these sectors be enhanced.

The Project does not propose any commercial office development. The new development will include commercial establishments that will provide employment and/or business ownership opportunities for area residents.

- F. That the City achieve the greatest possible preparedness to protect against injury and loss of life in an earthquake.

The Project is designed and will be constructed to conform to the structural and seismic safety requirements of the City Building Code.

G. That landmarks and historic buildings be preserved.

There are no historic resources present on the site.

H. That our parks and open space and their access to sunlight and vistas be protected from development.

The Project will not cast shadows or impede views for parks and open spaces in the area, nor have any negative impact on existing public parks and open spaces.

13. The Project is consistent with and would promote the general and specific purposes of the Code provided under Section 101.1(b) in that, as designed, the Project would contribute to the character and stability of the neighborhood and would constitute a beneficial development.
14. The Commission hereby finds that approval of the Conditional Use authorization would promote the health, safety and welfare of the City.

DECISION

That based upon the Record, the submissions by the Applicant, the staff of the Department and other interested parties, the oral testimony presented to this Commission at the public hearings, and all other written materials submitted by all parties, the Commission hereby **APPROVES Conditional Use Application No. 2014..0284CUA** subject to the following conditions attached hereto as “EXHIBIT A” in general conformance with plans on file, dated January 21, 2016, and stamped “EXHIBIT B”, which is incorporated herein by reference as though fully set forth.

APPEAL AND EFFECTIVE DATE OF MOTION: Any aggrieved person may appeal this Conditional Use Authorization to the Board of Supervisors within thirty (30) days after the date of this Motion No. XXXXX. The effective date of this Motion shall be the date of this Motion if not appealed (After the 30-day period has expired) OR the date of the decision of the Board of Supervisors if appealed to the Board of Supervisors. For further information, please contact the Board of Supervisors at (415) 554-5184, City Hall, Room 244, 1 Dr. Carlton B. Goodlett Place, San Francisco, CA 94102.

Protest of Fee or Exaction: You may protest any fee or exaction subject to Government Code Section 66000 that is imposed as a condition of approval by following the procedures set forth in Government Code Section 66020. The protest must satisfy the requirements of Government Code Section 66020(a) and must be filed within 90 days of the date of the first approval or conditional approval of the development referencing the challenged fee or exaction. For purposes of Government Code Section 66020, the date of imposition of the fee shall be the date of the earliest discretionary approval by the City of the subject development.

If the City has not previously given Notice of an earlier discretionary approval of the project, the Planning Commission’s adoption of this Motion, Resolution, Discretionary Review Action or the Zoning Administrator’s Variance Decision Letter constitutes the approval or conditional approval of the development and the City hereby gives **NOTICE** that the 90-day protest period under Government Code Section 66020 has begun. If the City has already given Notice that the 90-day approval period has begun for the subject development, then this document does not re-commence the 90-day approval period.

I hereby certify that the Planning Commission **ADOPTED** the foregoing Motion on December 17, 2015.

Jonas P. Ionin
Commission Secretary

AYES:

NAYS:

ABSENT:

ADOPTED:

EXHIBIT A

AUTHORIZATION

This authorization is for a conditional use to the demolition of the existing buildings and new construction of an 80 foot tall, 7-story building, with approximately 63 dwelling units and 8,000 square feet of ground-floor retail and below-grade parking for 40 vehicles plus one car share space located at 1567 California Street, 1436 and 1498 Polk Street, Block 0645, and Lots 014, 014A, and 015 pursuant to Planning Code Section(s) 121.1, 155, 270 and 303 within the Polk Street Neighborhood Commercial District and a 80-A Height and Bulk District; in general conformance with plans, dated January 21, 2016, and stamped "EXHIBIT B" included in the docket for Case No. 2014.0284CUA and subject to conditions of approval reviewed and approved by the Commission on December 17, 2015 under Motion No XXXXXX. This authorization and the conditions contained herein run with the property and not with a particular Project Sponsor, business, or operator.

RECORDATION OF CONDITIONS OF APPROVAL

Prior to the issuance of the building permit or commencement of use for the Project the Zoning Administrator shall approve and order the recordation of a Notice in the Official Records of the Recorder of the City and County of San Francisco for the subject property. This Notice shall state that the project is subject to the conditions of approval contained herein and reviewed and approved by the Planning Commission on February 25, 2016 under Motion No XXXXXX.

PRINTING OF CONDITIONS OF APPROVAL ON PLANS

The conditions of approval under the 'Exhibit A' of this Planning Commission Motion No. XXXXXX shall be reproduced on the Index Sheet of construction plans submitted with the Site or Building permit application for the Project. The Index Sheet of the construction plans shall reference to the Conditional Use authorization and any subsequent amendments or modifications.

SEVERABILITY

The Project shall comply with all applicable City codes and requirements. If any clause, sentence, section or any part of these conditions of approval is for any reason held to be invalid, such invalidity shall not affect or impair other remaining clauses, sentences, or sections of these conditions. This decision conveys no right to construct, or to receive a building permit. "Project Sponsor" shall include any subsequent responsible party.

CHANGES AND MODIFICATIONS

Changes to the approved plans may be approved administratively by the Zoning Administrator. Significant changes and modifications of conditions shall require Planning Commission approval of a new Conditional Use authorization.

Conditions of Approval, Compliance, Monitoring, and Reporting

PERFORMANCE

1. **Validity.** The authorization and right vested by virtue of this action is valid for three (3) years from the effective date of the Motion. The Department of Building Inspection shall have issued a Building Permit or Site Permit to construct the project and/or commence the approved use within this three-year period.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

2. **Expiration and Renewal.** Should a Building or Site Permit be sought after the three (3) year period has lapsed, the project sponsor must seek a renewal of this Authorization by filing an application for an amendment to the original Authorization or a new application for Authorization. Should the project sponsor decline to so file, and decline to withdraw the permit application, the Commission shall conduct a public hearing in order to consider the revocation of the Authorization. Should the Commission not revoke the Authorization following the closure of the public hearing, the Commission shall determine the extension of time for the continued validity of the Authorization.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

3. **Diligent pursuit.** Once a site or Building Permit has been issued, construction must commence within the timeframe required by the Department of Building Inspection and be continued diligently to completion. Failure to do so shall be grounds for the Commission to consider revoking the approval if more than three (3) years have passed since this Authorization was approved.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

4. **Extension.** All time limits in the preceding three paragraphs may be extended at the discretion of the Zoning Administrator where implementation of the project is delayed by a public agency, an appeal or a legal challenge and only by the length of time for which such public agency, appeal or challenge has caused delay.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

5. **Conformity with Current Law.** No application for Building Permit, Site Permit, or other entitlement shall be approved unless it complies with all applicable provisions of City Codes in effect at the time of such approval.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

6. **Additional Project Authorization.** The Project Sponsor must obtain a Variance under Section 134 to allow modification of the rear yard requirement. The conditions set forth below are additional conditions required in connection with the Project. If these conditions overlap with any other requirement imposed on the Project, the more restrictive or protective condition or requirement, as determined by the Zoning Administrator, shall apply.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863,

www.sf-planning.org

DESIGN

7. **Final Materials.** The Project Sponsor shall continue to work with Planning Department on the building design. Final materials, glazing, color, texture, landscaping, and detailing shall be subject to Department staff review and approval. The architectural addenda shall be reviewed and approved by the Planning Department prior to issuance.

For information about compliance, contact the Case Planner, Planning Department at 415-558-6378, www.sf-planning.org

8. **Garbage, composting and recycling storage.** Space for the collection and storage of garbage, composting, and recycling shall be provided within enclosed areas on the property and clearly labeled and illustrated on the building permit plans. Space for the collection and storage of recyclable and compostable materials that meets the size, location, accessibility and other standards specified by the San Francisco Recycling Program shall be provided at the ground level of the buildings.

For information about compliance, contact the Case Planner, Planning Department at 415-558-6378, www.sf-planning.org

9. **Street Trees.** The Project shall comply with the requirements of Planning Code Section 138.1, subject to the Department's review and approval of a final streetscape plan.

For information about compliance, contact the Case Planner, Planning Department at 415-558-6378, www.sf-planning.org

10. **Streetscape Plan.** Pursuant to Planning Code Section 138.1, the Project Sponsor shall continue to work with Planning Department staff, in consultation with other City agencies, to refine the design and programming of the Streetscape Plan so that the plan generally meets the standards of the Better Streets Plan and all applicable City standards. The Project Sponsor shall complete final design of all required street improvements, including procurement of relevant City permits, prior to issuance of first architectural addenda, and shall complete construction of all required street improvements prior to issuance of first temporary certificate of occupancy.

For information about compliance, contact the Case Planner, Planning Department at 415-558-6378, www.sf-planning.org

11. **Rooftop Mechanical Equipment.** Pursuant to Planning Code 141, the Project Sponsor shall submit a roof plan to the Planning Department prior to Planning approval of the building permit application. Rooftop mechanical equipment, if any is proposed as part of the Project, is required to be screened so as not to be visible from any point at or below the roof level of the subject building.

For information about compliance, contact the Case Planner, Planning Department at 415-558-6378, www.sf-planning.org

12. **Transformer Vault.** The location of individual project PG&E Transformer Vault installations has significant effects to San Francisco streetscapes when improperly located. However, they may not have any impact if they are installed in preferred locations. Therefore, the Planning Department recommends the following preference schedule in locating new transformer vaults,

in order of most to least desirable:

1. On-site, in a basement area accessed via a garage or other access point without use of separate doors on a ground floor façade facing a public right-of-way;
2. On-site, in a driveway, underground;
3. On-site, above ground, screened from view, other than a ground floor façade facing a public right-of-way;
4. Public right-of-way, underground, under sidewalks with a minimum width of 12 feet, avoiding effects on streetscape elements, such as street trees; and based on Better Streets Plan guidelines;
5. Public right-of-way, underground; and based on Better Streets Plan guidelines;
6. Public right-of-way, above ground, screened from view; and based on Better Streets Plan guidelines;
7. On-site, in a ground floor façade (the least desirable location).

Unless otherwise specified by the Planning Department, Department of Public Work's Bureau of Street Use and Mapping (DPW BSM) should use this preference schedule for all new transformer vault installation requests.

For information about compliance, contact Bureau of Street Use and Mapping, Department of Public Works at 415-554-5810, <http://sfdpw.org>

13. Entertainment Commission. The Project Sponsor shall comply with the "Recommended Noise Attenuation Conditions for [Administrative Code] Chapter 116 Residential Projects," which were recommended by the Entertainment Commission on August 25, 2015. These conditions state:

- **Community Outreach:** Project Sponsor shall include in its community outreach process any businesses located within 300 feet of the proposed project that operate between the hours of 9PM-5AM. Notice shall be made in person, written or electronic form.
- **Sound Study:** Project sponsor shall conduct an acoustical sound study, which shall include sound readings taken when performances are taking place at the proximate Places of Entertainment, as well as when patrons arrive and leave these locations at closing time. Readings should be taken at locations that most accurately capture sound from the Place of Entertainment to best of their ability. Any recommendation(s) in the sound study regarding window glaze ratings and soundproofing materials including but not limited to walls, doors, roofing, etc. shall be given highest consideration by the project sponsor when designing and building the project.
- **Design Considerations:**
 - (1) During design phase, project sponsor shall consider the entrance and egress location and paths of travel at the Place(s) of Entertainment in designing the location of (a) any entrance/egress for the residential building and (b) any parking garage in the building.
 - (2) In designing doors, windows, and other openings for the residential building, project sponsor should consider the POE's operations and noise during all hours of the day and night.

- Construction Impacts: Project sponsor shall communicate with adjacent or nearby Place(s) of Entertainment as to the construction schedule, daytime and nighttime, and consider how this schedule and any storage of construction materials may impact the POE operations.
- Communication: Project Sponsor shall make a cell phone number available to Place(s) of Entertainment management during all phases of development through construction.

For information about compliance, contact the Case Planner, Planning Department at 415-558-6378, www.sf-planning.org

PARKING AND TRAFFIC

14. **Parking for Affordable Units.** All off-street parking spaces shall be made available to Project residents only as a separate “add-on” option for purchase or rent and shall not be bundled with any Project dwelling unit for the life of the dwelling units. The required parking spaces may be made available to residents within a quarter mile of the project. All affordable dwelling units pursuant to Planning Code Section 415 shall have equal access to use of the parking as the market rate units, with parking spaces priced commensurate with the affordability of the dwelling unit. Each unit within the Project shall have the first right of refusal to rent or purchase a parking space until the number of residential parking spaces are no longer available. No conditions may be placed on the purchase or rental of dwelling units, nor may homeowner’s rules be established, which prevent or preclude the separation of parking spaces from dwelling units.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

15. **Bicycle Parking.** Pursuant to Planning Code Section 155.1, the Zoning Administrator has determined that the Project shall provide no fewer than 65 Class 1 and six Class 2 bicycle parking spaces. The Zoning Administrator will use this information to consider the parking reduction request. This requirement shall not preclude the Project Sponsor from providing additional bicycle parking facilities through valet services or a self-service corral as needed by demand, particularly for conventions with a large number of local attendees.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

16. **Car Share.** Pursuant to Planning Code Section 166, no less than one car share space shall be made available, at no cost, to a certified car share organization for the purposes of providing car share services for its service subscribers. Car share facilities must be designed to meet the requirements of the Department.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org.

17. **Managing Traffic During Construction.** The Project Sponsor and construction contractor(s) shall coordinate with the Traffic Engineering and Transit Divisions of the San Francisco Municipal Transportation Agency (SFMTA), the Police Department, the Fire Department, the Planning Department, and other construction contractor(s) for any concurrent nearby Projects to manage traffic congestion and pedestrian circulation effects during construction of the Project. For information about compliance, contact Code Enforcement,

Planning Department at 415-575-6863, www.sf-planning.org

PROVISIONS

18. **First Source Hiring.** The Project shall adhere –at a minimum – to the requirements of the First Source Hiring Construction and End-Use Employment Program approved by the First Source Hiring Administrator, pursuant to Section 83.4(m) of the Administrative Code. The Project Sponsor shall comply with the requirements of this Program regarding construction work and on-going employment required for the Project.

For information about compliance, contact the First Source Hiring Manager at 415-581-2335, www.onestopSF.org

19. **Transit Impact Development Fee.** Pursuant to Planning Code Section 411, the Project Sponsor shall pay the Transit Impact Development Fee (TIDF) as required by and based on drawings submitted with the Building Permit Application. Prior to the issuance of a temporary certificate of occupancy, the Project Sponsor shall provide the Planning Director with certification that the fee has been paid.

For information about compliance, contact the Case Planner, Planning Department at 415-558-6378, www.sf-planning.org

MONITORING

20. **Enforcement.** Violation of any of the Planning Department conditions of approval contained in this Motion or of any other provisions of Planning Code applicable to this Project shall be subject to the enforcement procedures and administrative penalties set forth under Planning Code Section 176 or Section 176.1. The Planning Department may also refer the violation complaints to other city departments and agencies for appropriate enforcement action under their jurisdiction. *For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org*

21. **Revocation due to Violation of Conditions.** Should implementation of this Project result in complaints from interested property owners, residents, or commercial lessees which are not resolved by the Project Sponsor and found to be in violation of the Planning Code and/or the specific conditions of approval for the Project as set forth in Exhibit A of this Motion, the Zoning Administrator shall refer such complaints to the Commission, after which it may hold a public hearing on the matter to consider revocation of this authorization.

For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org

OPERATION

22. **Garbage, Recycling, and Composting Receptacles.** Garbage, recycling, and compost containers shall be kept within the premises and hidden from public view, and placed outside only when being serviced by the disposal company. Trash shall be contained and disposed of pursuant to garbage and recycling receptacles guidelines set forth by the Department of Public Works.

For information about compliance, contact Bureau of Street Use and Mapping, Department of Public Works at 415-554-.5810, <http://sfdpw.org>

23. **Sidewalk Maintenance.** The Project Sponsor shall maintain the main entrance to the building and all sidewalks abutting the subject property in a clean and sanitary condition in compliance with the Department of Public Works Streets and Sidewalk Maintenance Standards.
For information about compliance, contact Bureau of Street Use and Mapping, Department of Public Works, 415-695-2017, <http://sfdpw.org>
24. **Community Liaison.** Prior to issuance of a building permit to construct the project and implement the approved use, the Project Sponsor shall appoint a community liaison officer to deal with the issues of concern to owners and occupants of nearby properties. The Project Sponsor shall provide the Zoning Administrator with written notice of the name, business address, and telephone number of the community liaison. Should the contact information change, the Zoning Administrator shall be made aware of such change. The community liaison shall report to the Zoning Administrator what issues, if any, are of concern to the community and what issues have not been resolved by the Project Sponsor.
For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org
25. **Lighting.** All Project lighting shall be directed onto the Project site and immediately surrounding sidewalk area only, and designed and managed so as not to be a nuisance to adjacent residents. Nighttime lighting shall be the minimum necessary to ensure safety, but shall in no case be directed so as to constitute a nuisance to any surrounding property.
For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, www.sf-planning.org



SAN FRANCISCO PLANNING DEPARTMENT

Certificate of Determination Exemption from Environmental Review

Case No.: 2014.0284E
 Project Title: 1567 California Street/1436 & 1498 Polk Street
 Zoning: NCD (Polk Street Neighborhood Commercial District)
 80-A Height and Bulk District
 Block/Lot: 0645/014, 014A & 015
 Lot Size: 18,625 square feet (.43-acres)
 Project Sponsor: Cyrus Sanandaji c/o 1567 California, LLC
 (760) 214-8753
 Staff Contact: Lana Russell-Hurd – (415) 575-9047
Lana.Russell@sfgov.org

1650 Mission St.
 Suite 400
 San Francisco,
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415.558.6378

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415.558.6409

Planning
 Information:
415.558.6377

PROJECT DESCRIPTION:

The project site is located on a lot in San Francisco's Nob Hill Neighborhood, on a block bounded by California Street to the north, Pine Street to the south, Larkin Street to the east, and Polk Street to the west. The site currently contains a two-story, 11,746 square-foot commercial building constructed in 1916 and a surface parking lot. The proposed project entails demolition of the existing building and surface parking lot, and construction of a new approximately 108,400 gross square foot seven-story mixed-use building, 80 feet in height. The proposed building would include an approximately 15-foot-tall elevator penthouse and stair penthouse above the proposed building's 80-foot-tall roof. The proposed building would include approximately 8,000 square feet of retail uses on the ground floor, 56,800 square feet of residential uses (63 dwelling units) on the 2nd through 7th floors, and 41 off-street parking spaces in a subsurface garage.

(Continued on next page)

EXEMPT STATUS:

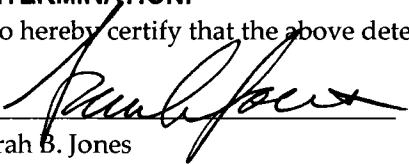
Categorical Exemption, Class 32 (California Environmental Quality Act (CEQA) Guidelines Section 15332).

REMARKS:

See next page.

DETERMINATION:

I do hereby certify that the above determination has been made pursuant to State and local requirements.


 Sarah B. Jones
 Environmental Review Officer

February 8, 2016
 Date

cc: Cyrus Sanandaji, Project Sponsor
 Claudine Asbagh, Current Planner
 Supervisor Aaron Peskin, District 3

Nob Hill and Citywide Distribution List
 Virna Byrd, M.D.F.

PROJECT DESCRIPTION (continued):

The project would require excavation up to 15 feet deep along the western side of the project site below Polk Street, and about 19 feet deep along the eastern side of the project site. The project would excavate approximately 8,000 cubic yards of soil.

The site is comprised of three lots, lot 014, 014A, and 015. Lot 014, located at 1436 Polk Street and Lot 014A located at 1498 Polk Street contain an 11,746 square foot two-story multiple-tenant commercial building constructed in 1916. Lot 015 is located at 1567 California Street and is comprised of a surface parking lot with an existing 22 foot wide curb cut on California Street.

As stated above, the proposed project entails demolition of the existing building and surface parking lot, and construction of a new seven-story mixed-use building. The proposed building would contain approximately 8,000 square feet of retail and 63 dwelling units. The parking garage would include: 8 commercial parking spaces, 32 residential parking spaces (two of which are accessible parking spaces), and one car share space. The parking garage would be accessed by the existing curb cut on California Street, which would be maintained and reduced to 15 feet. The project would include up to 72 Class I and 8 Class II bicycle parking spaces. The project would include approximately 9,300 square feet of code compliant common and private open space areas. The project would also include a living wall¹ fronting California Street.

Project Setting. The project site is located within the Polk Street Neighborhood Commercial District, a mixed-use urban area with a mixture of single- to seven-story buildings with residential units above ground-floor commercial uses. Neighboring land uses include residential, restaurants, personal services, financial services, and retail uses. The nearest six and seven-story buildings are located at 1628 California and 1635 California Street, one-block west of the project site.

The California Street Cable Car runs directly adjacent to the project site on California Street. The recently-approved Polk Streetscape² project will occur adjacent to the project site, and includes transit, pedestrian, and bicycle safety improvements; implementation of near-term improvements will begin in Spring 2015 and construction of the final Polk Streetscape project is set to begin in Summer 2016.

The project site is located one block east of Van Ness Avenue. The Van Ness Avenue Bus Rapid Transit (BRT) project³ will begin construction in early 2016 and includes transit, pedestrian, and bicycle improvements as well as utility maintenance. Bus Rapid Transit service is expected to begin in 2019.

¹ Living walls are plantings on the exterior walls of buildings that can add unique detail to a building and streetscape and serve environmental goals as well. Plants can be independent and hang from wall elements or can grow from property line planters or contained raised planter beds integrated with a building's architecture. *Guide to the San Francisco Better Streets Plan Chapter 6: Streetscape Elements*, December 2010.

² For more information visit <https://www.sfmta.com/sites/default/files/projects/2015/FAQ%20Draft%203.2.2015.pdf>.

³ For more information visit

<https://www.sfmta.com/projects-planning/projects/van-ness-corridor-transit-improvement-project#details>.

California Pacific Medical Center (CPMC) Van Ness and Geary Campus⁴ is located within ½ mile of the project site. The CPMC campus includes hospital and medical office uses. The CPMC campus is currently under construction and is expected to be completed in 2019.

Project Approvals

The proposed project would require the following approvals:

- **Conditional Use Authorization** (Planning Commission)
- **Variance Authorization** (Zoning Administrator)
- **Building Permit** (Department of Building Inspection)

The proposed project is subject to a Conditional Use Authorization for large lot size development (Planning Code Section 121.1), a bulk exception to exceed the bulk limitations of the 80-A Height and Bulk District (Planning Code Section 271) and to allow a garage entry on California Street (Planning Code Section 155 (r)(3)). The proposed project would also require a variance from the Zoning Administrator for a rear yard modification (Planning Code Section 134).

Approval Action:

The Conditional Use Authorization from the Planning Commission is the Approval Action for the proposed project. The Approval Action date establishes the start of the 30-day appeal period for this CEQA exemption determination pursuant to Section 31.04(h) of the San Francisco Administrative Code.

EXEMPT STATUS (CONTINUED):

CEQA Guidelines Section 15332, or Class 32, provides an exemption from environmental review for in-fill development projects that meet the following conditions. As discussed below, the proposed project satisfies the terms of the Class 32 exemption.

- a) The project is consistent with applicable general plan designations and policies as well as with applicable zoning designations.*

The *San Francisco General Plan*, which provides general policies to guide land use decisions, contains some policies that relate to physical environmental issues. The proposed project is consistent with applicable general plan designations and policies. The project site is in a NCD (Polk Street Neighborhood Commercial) zoning district within the Nob Hill neighborhood. The proposed project would construct approximately 8,000 square feet of retail uses on the ground floor and 56,800 square feet of residential uses on the 2nd through 7th floors; these uses are permitted or conditionally authorized within the Polk Street NCD. The property is located in the 80-A height and bulk district, which allows a maximum 80 feet of building height. The proposed building would include an approximately 15-foot-tall elevator penthouse and stair penthouse above the proposed building's 80-foot-tall roof. The proposed building, elevator, and stair penthouse would be within the permitted height limit. Thus, the proposed project would be consistent with all applicable zoning designations.

⁴ For more information visit <http://vng.cpmc2020.org/construction-activity>.

- b) *The development occurs within city limits on a site of less than five acres surrounded by urban uses.*

The approximately 18,625 square feet (0.43-acre) project site is located within a fully developed area of San Francisco. The surrounding properties encompass commercial and residential uses. The proposed project, therefore, would be properly characterized as infill development of less than five acres, completely surrounded by urban uses.

- c) *The project site has no habitat for endangered, rare or threatened species.*

The project site is within a developed urban area and occupied by an existing building, with no landscaping or groundcover. Thus, the project site has no value for rare, threatened, or endangered species.

- d) *Approval of the project would not result in any significant effects relating to traffic, noise, air quality, or water quality.*

Traffic

Based on the residential and commercial trip generation rates in the Planning Department's *Transportation Impact Analysis Guidelines for Environmental Review* (October 2002), the proposed new seven-story building would generate, 1,719 daily person-trips, of which 199 would be expected to occur during the PM peak-hour. These 199 PM peak-hour person-trips would be distributed among various modes of transportation, including 58 auto trips, 52 transit trips, 71 walking trips, and 18 other trips. The estimated 58 PM peak-hour auto trips equate to approximately 34 PM peak-hour vehicle trips (that is, 58 persons travelling in 34 vehicles during the peak hour). The existing commercial use at the project site results in 22 PM peak-hour vehicle trips. The proposed project would therefore result in a net increase of 12 PM peak-hour vehicle-trips compared to the existing condition. For context, the intersection of Polk and California Streets currently has 272 PM peak-hour vehicle trips in the north bound direction and 387 PM peak-hour vehicle-trips in the south bound direction⁵. The 12 net new PM peak-hour vehicle-trips is a small incremental increase in traffic that would not result in a significant traffic impact at the project level, and would not result in a cumulatively considerable contribution to traffic effects resulting from present and reasonably foreseeable projects in the project vicinity, including the Polk Streetscape project, Van Ness BRT, and the CPMC Hospital. Therefore, there would be no significant impact on traffic in the project area as a result of the proposed project.

Transit

The project site is located in an area well-served by transit. Twelve Muni bus routes, including the California-1, Clement-2, Jackson-3, Folsom/ Pacific-12, Polk-19, Bryant-27, Geary-38 Geary-38L, Van Ness-47, Van Ness/Mission-49, 90-Owl, and the California Cable Car are located within a ½ mile of the project site. The proposed project would generate 52 PM peak-hour transit trips. The existing transit facilities would be able to accommodate added ridership associated with the

⁵ SFMTA Traffic Count Data 1993-2013. Available at <http://www.sfmta.com/sites/default/files/adtcounets.accessible5.pdf>.

proposed project. Therefore, no significant impacts to transit would occur as a result of the proposed project.

Pedestrians

The project site is adjacent to a sidewalk on California Street and Polk Street. The proposed project would generate 123 PM peak-hour walk trips (that is, 71 PM peak-hour walk-trips and 52 PM peak-hour transit trips, which include walk trips). The proposed project would provide vehicular access to the new garage through an existing curb cut at California Street. Although the proposed project would add traffic to this curb cut, no potentially hazardous conditions would occur between pedestrians and vehicles entering and exiting the curb cut because of the minor amount of new automobile trips generated by the proposed project. The proposed project would also construct a pedestrian bulb at the intersection of California and Polk Streets to facilitate pedestrian crossings at this intersection. The increase in daily pedestrian person-trips generated by the proposed project would not substantially overcrowd sidewalks in the project vicinity or otherwise interfere with pedestrian accessibility to the site and adjoining areas. Therefore, no significant impacts related to pedestrians would occur.

Bicycles

Five bicycle routes (#10, #16, #25, #210, and #310) are within a ½ mile of the project site. Bicycle routes #25 on Polk Street and #310 on California Street are adjacent to the project site. The proposed project would providing up to 72 Class I bicycle parking spaces on the ground floor and 8 Class II bicycle parking spaces on California Street. The proposed project would generate 18 PM peak-hour other trips, some of which would be bicycle trips. The minimal increase of bicycle trips generated by the proposed project would be accommodated by the existing bicycle network and the proposed project would not create potentially hazardous conditions for bicyclists; therefore, no significant impacts related to bicyclists would occur.

Construction Traffic

Construction of the proposed project is expected to occur over the course of a 19-month period. During that time, it is anticipated that the majority of the construction-related truck traffic would use I-80, I-280, and U.S. 101 to access the project site from the East Bay, South Bay, and North Bay and from locations within the City. The addition of worker-related vehicle or transit trips would not substantially affect these roadways or local streets near the project site. Construction workers who drive to the site would cause a temporary increase in traffic volume and demand for on-street parking. However, overall construction activities would result in a small incremental increase in traffic (worker vehicles and equipment) and only slightly reduce the availability of on-street parking during working hours. Due to the temporary and limited duration and intensity, construction-related traffic impacts would not be substantial, and there would not be a significant impact on traffic in the project area as a result of the proposed project.

Parking

Public Resources Code (PRC) Section 21099(d)(1), effective January 1, 2014, provides that, "parking...impacts of a residential, mixed-use residential, or employment center project on an infill site within a transit priority area shall not be considered significant impacts on the

environment.” The project satisfies the conditions provided in the applicable PRC section.⁶ Therefore, the proposed project would not have any significant impacts related to parking, and the following discussion of parking is provide for informational purposes only.

Section 151 of the *Planning Code* generally requires one off-street parking space be provided for each dwelling unit within the Polk Street NCD. The proposed project would include 63 residential units, approximately 8,000 square feet of commercial uses, and 41 parking spaces. The parking demand generated by the proposed project has been estimated in accordance with the *Transportation Impact Analysis Guidelines* at 100 parking spaces. Therefore, the proposed project would have an estimated parking deficit of 59 spaces.

San Francisco does not consider parking supply as part of the permanent physical environment. Parking conditions are not static, as parking supply and demand varies from day to day, from day to night, from month to month, etc. Hence, the availability of parking spaces (or lack thereof) is not a permanent physical condition, but changes over time as people change their modes and patterns of travel.

Parking deficits are considered to be social effects, rather than impacts on the physical environment as defined by CEQA. Under CEQA, a project’s social impacts need not be treated as significant impacts on the environment. Environmental documents should, however, address the secondary physical impacts that could be triggered by a social impact (CEQA Guidelines Section 15131(a)). The social inconvenience of parking deficits, such as having to hunt for scarce parking spaces, is not an environmental impact, but there may be secondary physical environmental impacts, such as increased traffic congestion at intersections, air quality impacts, safety impacts, or noise impacts caused by congestion. In the experience of San Francisco transportation planners, however, the absence of a ready supply of parking spaces, combined with available alternatives to auto travel (e.g., transit service, taxis, bicycles or travel by foot) and a relatively dense pattern of urban development, induces many drivers to seek and find alternative parking facilities, shift to other modes of travel, or change their overall travel habits. Any such resulting shifts to transit service in particular, would be in keeping with the City’s “Transit First” policy. The City’s Transit First Policy, established in the City’s Charter Section 16.102 provides that “parking policies for areas well served by public transit shall be designed to encourage travel by public transportation and alternative transportation.”

Noise

Ambient noise levels in the vicinity of the project site are typical of neighborhoods in San Francisco, which are dominated by vehicular traffic, including Muni vehicles, trucks, cars, emergency vehicles, and land use activities, such as commercial businesses. An approximate doubling in traffic volumes in the area would be necessary to produce an increase in ambient noise levels. As described above, the proposed project would not double traffic volumes.

⁶ Transit-Oriented Infill Project Eligibility Checklist, 1567 California St./1436-1498 Polk St. This document (and all other documents cited in this report, unless otherwise noted) is available for review at the San Francisco Planning Department, 1650 Mission Street, Suite 400, San Francisco, CA as part of Case File 2014.0284E.

An acoustical study⁷ was conducted and noise level measurements were taken over a 24-hour period. Long-term measurements (continuous measurements with 15-minute intervals) were taken on California Street, Polk Street, and Pine Street. The noise study found the dNL to be 80 dBA on California Street, 79 dBA on Polk Street, and 69 dBA on Pine Street. The noise analysis recommended applying Sound Transmission Class requirements for exterior and insulated walls to reduce interior noise levels consistent with Title 24 of the California Building Standards Code. Through the building permit review process, the Department of Building Inspection would confirm that Title 24 standards would be met. Therefore, the proposed project would not expose persons to noise levels in excess of applicable noise standards.

Construction Noise

Daily construction would occur during the working hours of 7:00 a.m. to 3:00 p.m. Monday through Friday. Deliveries and staging of materials on site may occur on Saturdays. The main sources of construction noise on this project would be engines from construction equipment, typically diesel engines. Noise generated by the demolition process would vary for the breaking down of materials on site and the earthmoving excavation and off-haul process. Mobile equipment would move about the site with bulldozers, loaders, graders and trucks. The large equipment movements would vary day to day until the foundation system is completed. The construction of the main building structure would include a mobile crane, a man lift for floor access, concrete mixer trucks, and pumps which would be more stationary around the site perimeter. The daily variations in noise beyond the site would diminish as the building envelope is closed up and the perimeter walls complete. Delivery trucks would be the main source of noise over the remaining phases of construction.

Although some increase in noise would be associated with the construction phase of the project, such occurrences would be limited to certain hours of day and would be temporary and intermittent in nature. Construction noise is regulated by the San Francisco Noise Ordinance (Article 29 of the City Police Code). Section 2907 of the Police Code requires that noise levels from individual pieces of construction equipment, other than impact tools, not exceed 80 decibels (dBA) at a distance of 100 feet from the source. Impact tools (such as jackhammers and impact wrenches) must have both intake and exhaust muffled to the satisfaction of the Director of Public Works. Section 2908 of the Police Code prohibits construction work between 8:00 p.m. and 7:00 a.m. if noise would exceed the ambient noise level by 5 dBA at the project property line, unless a special permit is authorized by the Director of Public Works.

Operational Noise

The proposed project includes the addition of new residences, commercial activities, and the construction of common and private open spaces, which would generate some additional noise that may be considered an annoyance by occupants of nearby properties. Section 2909 of the San Francisco Noise Ordinance regulates residential and commercial property noise limits. Residential noise is limited to no more than 5 dBA above the ambient noise level. Commercial

⁷ Charles M. Salter Associates, Inc., 1567 California Street San Francisco California, Environmental Noise Study, CSA Project Number: 15-0051. April 9, 2015.

noise is limited to no more than 8 dBA above the local ambient noise level at any point outside of the property plane. The Department of Public Health may investigate and take enforcement action on any noise complaints received from the proposed project. Therefore, no significant noise impacts are expected.

Air Quality

In accordance with the state and federal Clean Air Acts, air pollutant standards are identified for the following six criteria air pollutants: ozone, carbon monoxide (CO), particulate matter (PM), nitrogen dioxide (NO₂), sulfur dioxide (SO₂) and lead. These air pollutants are termed criteria air pollutants because they are regulated by developing specific public health- and welfare-based criteria as the basis for setting permissible levels. The Bay Area Air Quality Management District (BAAQMD) in their *CEQA Air Quality Guidelines* (May 2011), has developed screening criteria to determine if projects would violate an air quality standard, contribute substantially to an air quality violation, or result in a cumulatively considerable net increase in criteria air pollutants within the San Francisco Bay Area Air Basin. If a proposed project meets the screening criteria, then the project would result in less-than-significant criteria air pollutant impacts. A project that exceeds the screening criteria may require a detailed air quality assessment to determine whether criteria air pollutant emissions would exceed significance thresholds. The proposed project would not exceed criteria air pollutant screening levels for operation or construction due to the relatively limited scale of development.⁸

In addition to criteria air pollutants, individual projects may emit toxic air contaminants (TACs). TACs collectively refer to a diverse group of air pollutants that are capable of causing chronic (i.e., of long-duration) and acute (i.e., severe but short-term) adverse effects to human health, including carcinogenic effects. In response to growing concerns of TACs and their human health effects, the San Francisco Board of Supervisors approved a series of amendments to the San Francisco Building and Health Codes, generally referred to as the Enhanced Ventilation Required for Urban Infill Sensitive Use Developments or Health Code, Article 38 (Ordinance 224-14, effective December 8, 2014)(Article 38). The purpose of Article 38 is to protect the public health and welfare by establishing an Air Pollutant Exposure Zone and imposing an enhanced ventilation requirement for all urban infill sensitive use development within the Air Pollutant Exposure Zone. Projects within the Air Pollutant Exposure Zone require special consideration to determine whether the project's activities would expose sensitive receptors to substantial air pollutant concentrations or add emissions to areas already adversely affected by poor air quality.

The proposed project is not within an Air Pollutant Exposure Zone. Therefore, the proposed project would not result in a significant impact with respect to siting new sensitive receptors in areas with substantial levels of air pollution. The proposed project would require construction activities for the approximate 19-week construction phase. However, construction emissions would be temporary and variable in nature and would not be expected to expose sensitive receptors to substantial air pollutants. Furthermore, the proposed project would be subject to,

⁸ Bay Area Air Quality Management District, *CEQA Air Quality Guidelines*, Updated May 2011. Table 3-1.

and comply with, California regulations limiting idling to no more than five minutes,⁹ which would further reduce nearby sensitive receptors' exposure to temporary and variable TAC emissions. Therefore, construction period TAC emissions would not result in a significant impact with respect to exposing sensitive receptors to substantial levels of air pollution.

The proposed project would include a backup emergency generator. Emergency generators are regulated by the BAAQMD through its New Source Review (Regulation 2, Rule 5) permitting process. The project applicant would be required to obtain applicable permits to operate the emergency generator from the BAAQMD. Although emergency generators are intended only to be used in periods of power outages, monthly testing of the generator would be required. The BAAQMD limit testing to no more than 50 hours per year. Additionally, as part of the permitting process, the BAAQMD would limit the excess cancer risk from any facility to no more than ten per one million population and requires any source that would result in an excess cancer risk greater than one per one million population to install Best Available Control Technology for Toxics (BACT). Therefore, the proposed project would not result in significant air quality impacts.

Water Quality

The proposed project would involve 5,000 square feet or more of the ground surface disturbance; thus the project would require a Stormwater Control Plan. The project resides in a combined sewer area and has been determined to trigger compliance with the Stormwater Design Guidelines (SDG). As per the requirements of the SDG, the project must achieve LEED Sustainable Sites (SS) c6.1, "Stormwater Design: Quantity Control." Therefore this project must implement a stormwater management approach that reduces existing stormwater runoff flow rate and volume by 25 percent for a two-year 24-hour design storm. The project would minimize disruption of natural hydrology by implementing Low Impact Design approaches such as reduced impervious cover, reuse of stormwater, or increased infiltration. The proposed project's living wall is a feature that may be used in the Stormwater Control Plan to slow stormwater runoff. The project would not generate wastewater or result in discharges that would have the potential to degrade water quality or contaminate public water supply. Project-related wastewater and stormwater would flow to the City's combined sewer system and would be treated to standards contained in the City's National Pollutant Discharge Elimination System (NPDES) Permit prior to discharge. Therefore, the proposed project would not result in significant water quality impacts.

- e) *The site can be adequately served by all required utilities and public services.*

The project site is located in a dense urban area where all public services and facilities are available. The proposed project would be connected with existing drinking water, electric, gas, waste, and wastewater services. The project would receive police and fire protection services.

⁹ California Code of Regulations, Title 13, Division 3, § 2485. This regulation applies to on-road heavy duty vehicles and not off-road equipment.

Prior to receiving a building permit, the project would be reviewed by the City to ensure compliance with City and State fire and building code regulations concerning building standards and fire protection. The proposed project would not result in a substantial increase in intensity of use or demand for utilities or public services that would necessitate any expansion of public utilities or public service facilities.

DISCUSSION OF ENVIRONMENTAL ISSUES:

Guidelines Section 15300.2, subdivision (c), provides that a categorical exemption shall not be used for an activity where there is a reasonable possibility that the activity will have a significant effect on the environment due to unusual circumstances. As discussed above, the proposed project would not have a significant effect on traffic, noise, air quality and water quality. In addition, the proposed project would not have a significant effect on the environment due to unusual circumstances for other environmental topics, including those discussed below.

Aesthetics. Public Resources Code (PRC) Section 21099(d)(1), effective January 1, 2014, provides that, "aesthetics...impacts of a residential, mixed-use residential, or employment center project on an infill site within a transit priority area shall not be considered significant impacts on the environment." The project satisfies the conditions provided in the applicable PRC Section¹⁰. The following discussion of aesthetics is provided for informational purposes only.

The visual character of the project site and its vicinity is urban and mixed, with a variety of residential and commercial land uses ranging from single-story to seven-story structures. The proposed building would be taller than existing conditions, but would conform to the City's 80-foot height limit. The proposed project would intensify and change the use of the site, but would not change or be inconsistent with the mixed-use visual character of surrounding development. The proposed project's architectural design has been reviewed by the Planning Department's Urban Design Advisory Team and their comments have been addressed.

Hazardous Materials. The project site was previously used as a paint store and dry cleaners. Therefore, the project is subject to Article 22A of the Health Code, also known as the Maher Ordinance, which is administered and overseen by the Department of Public Health (DPH). The Maher Ordinance requires the project sponsor to retain the services of a qualified professional to prepare a Phase I Environmental Site Assessment (ESA) that meets the requirements of Health Code Section 22.A.6. A Phase I¹¹ was conducted and found the project site had recognized environmental conditions in connection with the site. Based on that information, the project sponsor conducted soil sampling and soil gas borings and analysis as part of a Phase II¹² investigation. Soil samples detected concentrations of PCEs, which were below standard levels. None of the soil samples had detectable concentrations of VOCS. The Phase II did not recommend any further investigation of the project site. The project applicant is enrolled in the Maher program and would be required to remediate potential soil and groundwater contamination in

¹⁰ Transit-Oriented Infill Project Eligibility Checklist, 1567 California St./1436-1498 Polk St.

¹¹ Partner Engineering and Science, Inc. *Phase I Environmental Assessment Report, 1436 & 1498 Polk Street and 1567 California Street, San Francisco, California, 94109. Partner Project No. 13-110720.1.* November 5, 2013.

¹² Partner Engineering and Science, Inc. *Phase II Subsurface Investigation Report, 1436 & 1498 Polk Street and 1567 California Street, San Francisco, California, 94109. Partner Project No. 13-110720.3.* February 10th, 2014.

accordance with Article 22A of the Health Code. Therefore, no environmental impacts involving hazardous materials would be associated with the proposed project.

Historic Architectural Resources. The proposed project includes the demolition of an existing structure constructed more than 45 years ago. A property may be considered a historic resource if it meets any of the criteria related to (1) events, (2) persons, (3) architecture, or (4) prehistory that make it eligible for listing in the California Register of Historical Resources, or if it is considered a contributor to a potential historic district.

The project site was previously evaluated¹³ for historic resources as part of a prior development proposal. A Planning Department Preservation Planner found that the building constructed in 1916 does not have any particular cultural or historical value. The building was found to no longer retain architectural integrity, as the building storefronts were altered. The building was reclassified to a Category C building, which is not a historic resource. Therefore, the property was found to not be a historic resource for purposes of CEQA. Furthermore, the project site is not located within a historic district. Therefore, the proposed project would not have any significant impacts related to historic resources.

Shadow. Planning Code Section 295 requires a shadow analysis for any building over 40 feet in height. The proposed project would result in construction of a building 80 feet in height. The Planning Department conducted a preliminary shadow fan analysis and determined that the proposed project would not cast new shadow on public spaces protected under Planning Code Section 295. While shadow on private property may be a concern to nearby neighbors, it is not considered a significant impact under CEQA. Therefore, the proposed project would not have any significant impacts related to shadow.

Wind. A wind assessment was prepared for the proposed development at 1567 California Street.¹⁴ San Francisco Planning Code Section 148, Reduction of Ground-level Wind Currents in C-3 Districts, outlines wind reduction criteria for project in C-3 Districts. The Planning Code sets criteria for comfort and hazards and requires buildings to be shaped so as not to cause ground-level wind currents to exceed defined comfort and hazard criteria. The Planning Code pedestrian comfort criterion of 11 miles per hour (mph) is based on wind speeds measured and averaged over a period of one minute. In contrast, the Planning Code wind hazard criterion of 26 mph is defined by a wind speed that is measured and averaged over a period of one hour. When stated on the same time basis as the comfort criterion wind speed, the hazard criterion wind speed (26 mph averaged over one hour) is equivalent to a one-minute average of 36 mph, which is a speed where wind gusts can blow people over and are therefore hazardous. As stated above, the analysis uses the hazard criterion to determine significant effects under CEQA. In addition, the proposed project's effects related to the comfort criterion are presented for informational purposes.

The proposed development at 1567 California Street would include removal of the existing surface parking lot and structure and construction of a new seven-story, 80-foot tall building. The proposed project would include large private decks and common open space areas on the 2nd and 4th floors, and on the roof.

¹³ CEQA Categorical Exemption Determination 1498 Polk Street Case No. 2012.0068E, December 2, 2011.

¹⁴ RWDI, 1567 California Street, San Francisco CA, Pedestrian Wind Assessment, RWDI #1500234, May 19, 2015.

The results of the wind assessment indicate that wind speeds on the sidewalks along California and Polk Streets are predicted to be higher than those that currently exist. However, wind speeds at the building sidewalks and entrances are predicted to comply with the wind hazard criterion due to the limited height of the proposed building and recessed and sheltered building entrances. The potential wind speeds at the private decks and the common use areas are predicted to be higher than desired for passive usage, especially on the west deck on the 4th floor. However, wind control measures such as glass railings and landscaping would provide improved wind conditions at an acceptable level.

Since the completion of the wind assessment, the project's design has been slightly updated as follows: the height on the northeast corner of the building on California Street has been slightly lowered and the height in the southwest corner of the building on Polk Street has slightly increased. As noted, these changes were found to not warrant any further review¹⁵ and would not cause any significant changes to the wind comfort conditions as described in the wind assessment.

In sum, the proposed project would not exceed the wind hazard criterion on the surrounding pedestrian areas and would not alter wind in a manner that substantially affects public areas. Therefore, no significant wind impacts would occur.

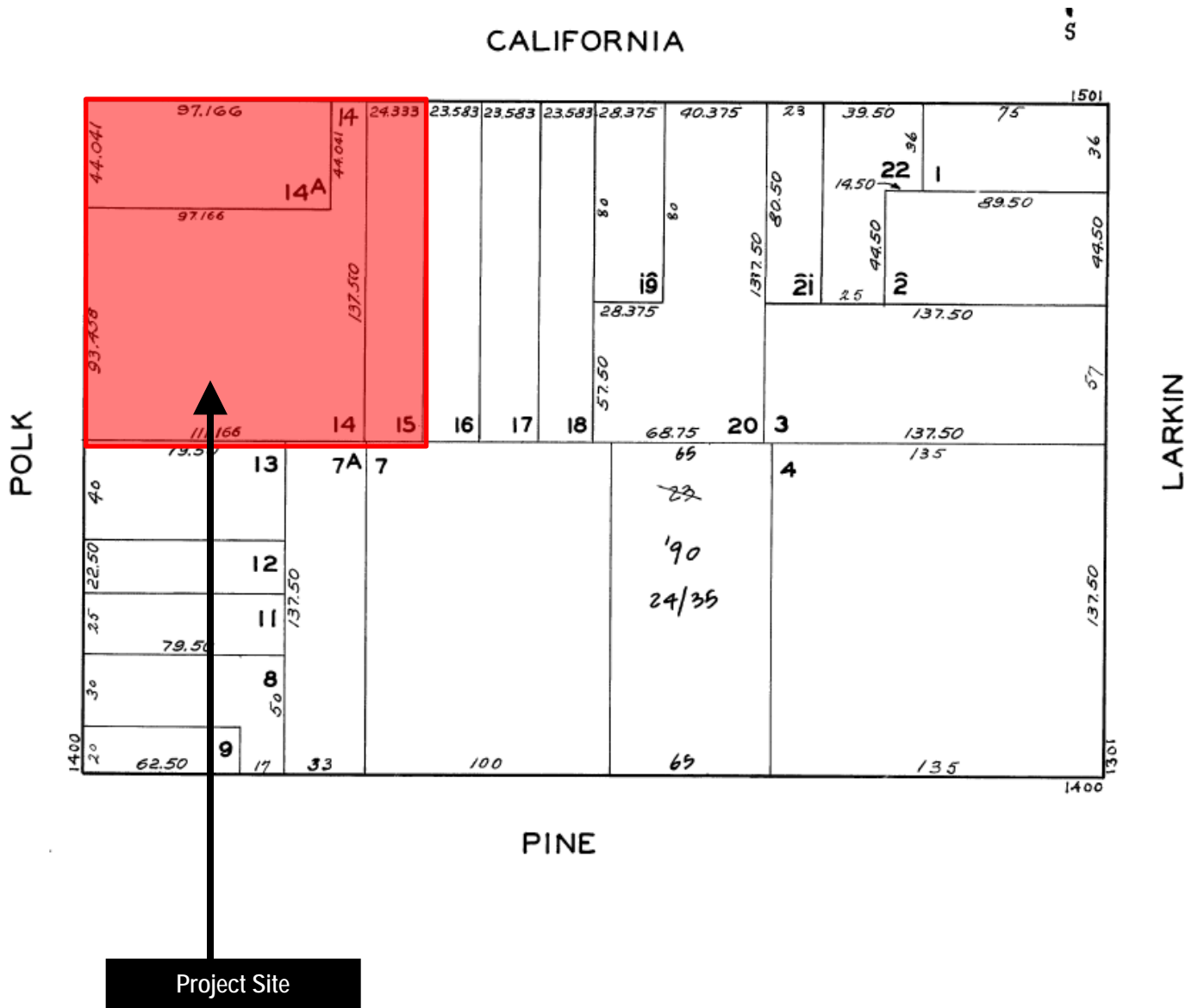
Public Notice and Comment. A "Notification of Project Receiving Environmental Review" was mailed on February 6th, 2015 to community organizations, tenants of the affected property and properties adjacent to the project site, and those persons who own property within 300 feet of the project site. Overall concerns and issues raised by the public in response to the notice were taken into consideration and incorporated into this Certificate of Exemption as appropriate for CEQA analysis. Comments regarding physical environmental effects were related to height incompatibility with the existing neighborhood character, lack of a transition to a taller building, loss of light, shadow, and wind effects, architectural design, increase in waste on the street, safety concerns, insufficient parking, and cumulative projects, including; Polk Streetscape project, Van Ness BRT, and the new CPMC Hospital traffic effects. The issues raised in these comments that are subject to CEQA review are addressed above.

Other issues that were raised by members of the public, including: real estate values, reduced quality of life, social impacts, and degradation of the middle class on Polk Street. Since these concerns do not relate to physical environmental effects, they are outside the scope of CEQA and are not addressed in this document. Comments that relate to economic, financial, and legal concerns may be considered by City decision-makers during their deliberations on whether to approve, modify, or disapprove the proposed project.

Conclusion. The proposed project satisfies the criteria for exemption under the above-cited classification. In addition, none of the CEQA Guidelines Section 15300.2 exceptions to the use of a categorical exemption applies to the proposed project. For the above reasons, the proposed project is appropriately exempt from environmental review.

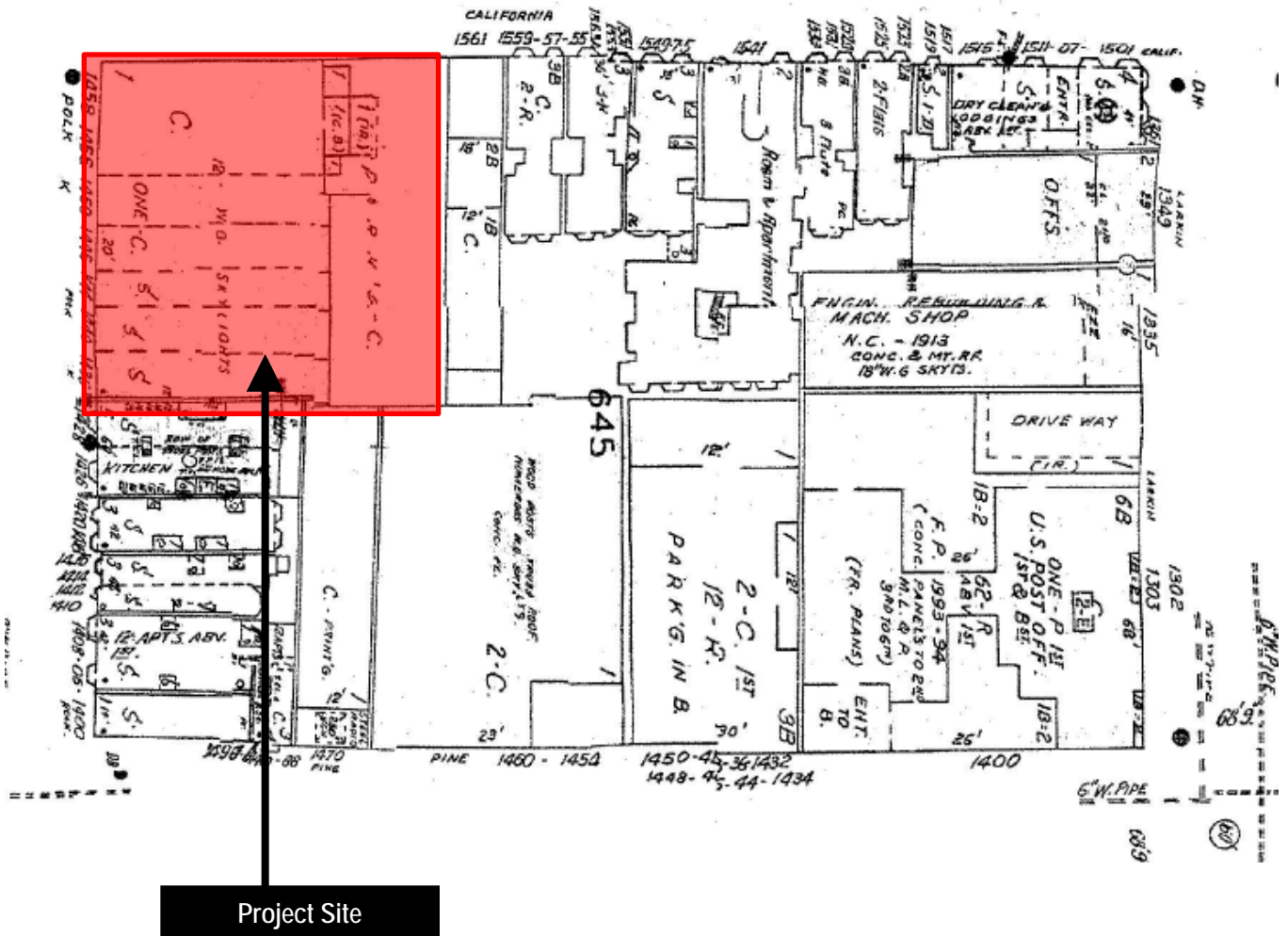
¹⁵ RWDL, 1567 California Street Pedestrian Wind Assessment Email, February 3, 2016.

Parcel Map



Case No. 2014.0284CUA/VAR
 1567 California Street
 Conditional Use Authorization/ Variance

Sanborn Map*

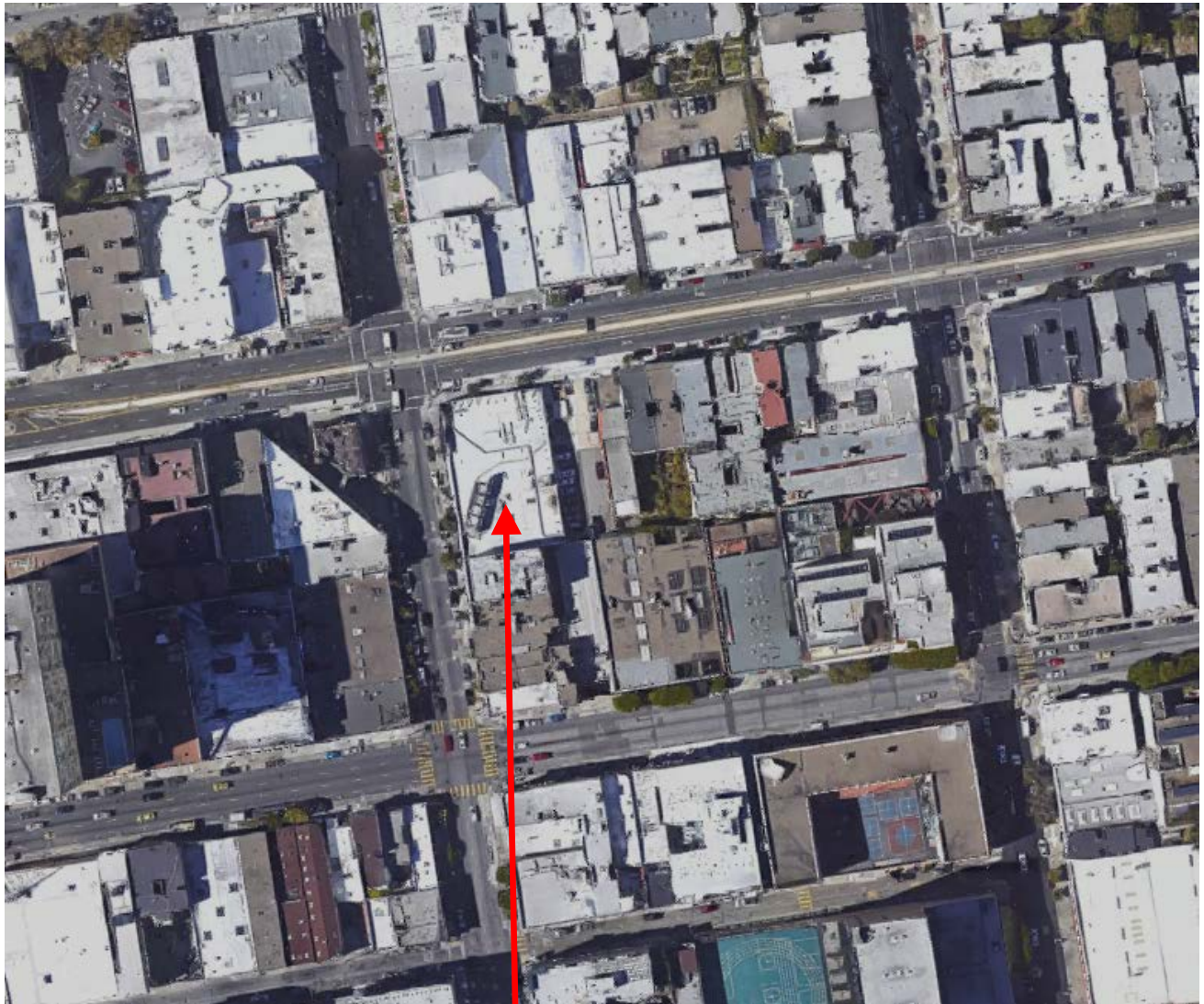


*The Sanborn Maps in San Francisco have not been updated since 1998, and this map may not accurately reflect existing conditions.



Case No. 2014.0284CUA/VAR
 1567 California Street
 Conditional Use Authorization/ Variance

Aerial Photo

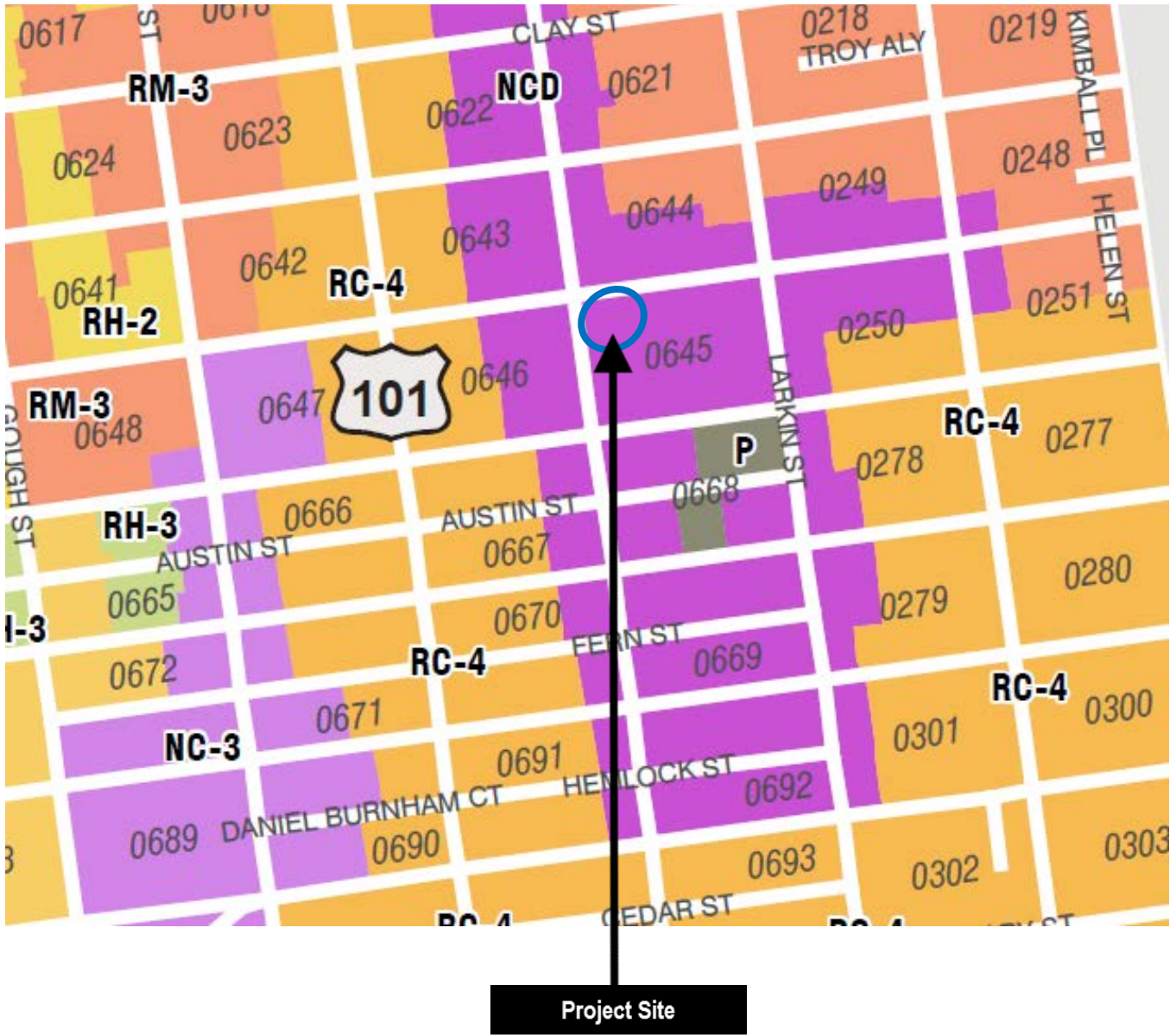


Project Site

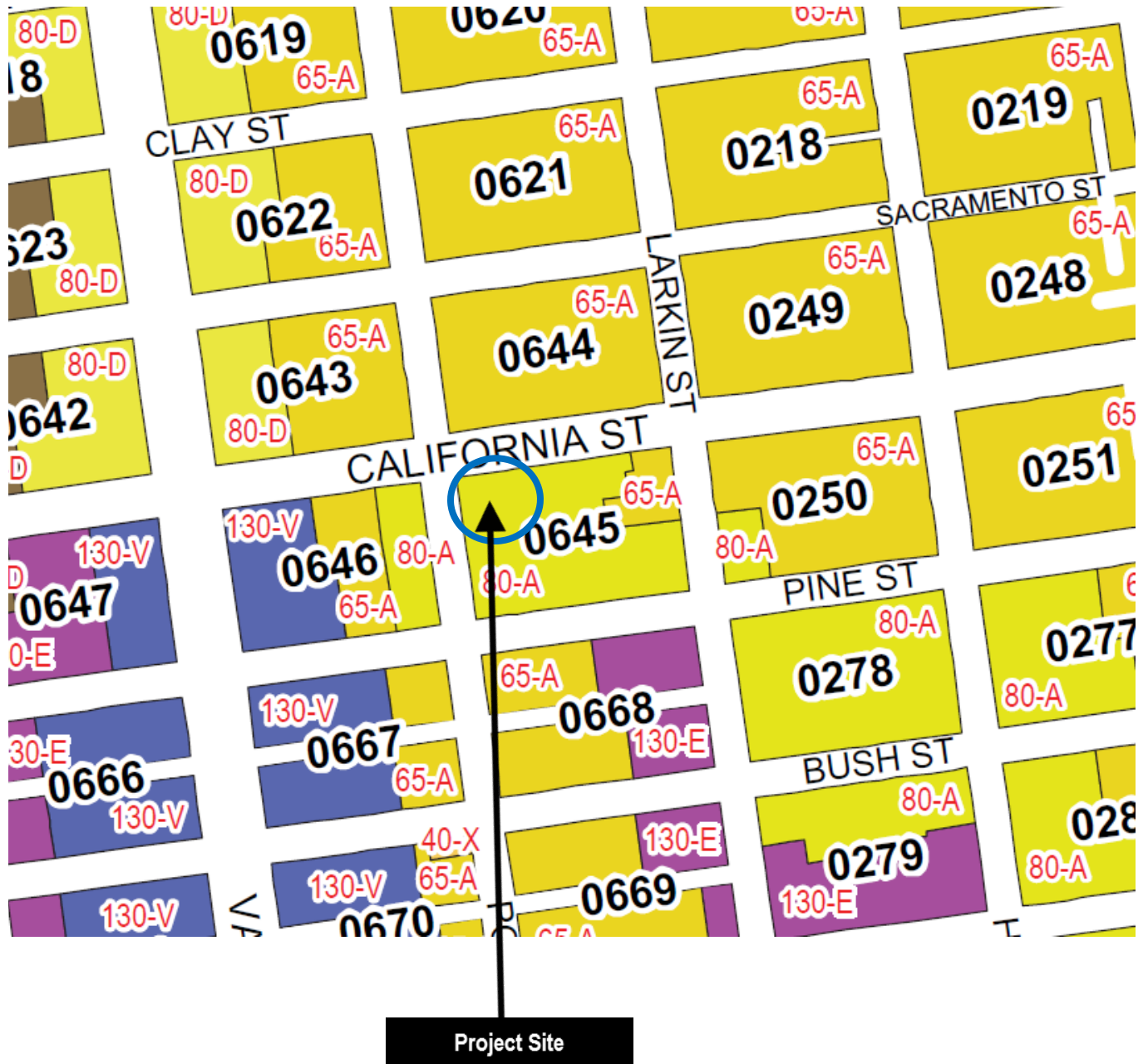


Case No. 2014.0284CUA/VAR
1567 California Street
Conditional Use Authorization/ Variance

Zoning District Map



Height and Bulk Map



Case No. 2014.0284CUA/VAR
1567 California Street
Conditional Use Authorization/ Variance

REUBEN, JUNIUS & ROSE, LLP

February 5, 2016

President Rodney Fong
San Francisco Planning Commission
1650 Mission Street, Suite 400
San Francisco, CA 94103

**Re: 1567 California Street
Planning Case No. 2014.0284X
Hearing Date: February 25, 2016
Our File No.: 7892.01**

Dear President Fong and Commissioners:

This office represents Cyrus Sanandaji c/o 1567 California, LLC, the Project Sponsor of a proposed mixed-use development (the "Project") at 1567 California Street, Assessor's Block 0645, Lots 014, 014A, and 015 (the "Property" or "Project Site"). The property is located at the southeast corner of California and Polk Streets in Nob Hill and within the Polk Street Neighborhood Commercial District. The Project Sponsor proposes to demolish an existing retail structure and associated surface parking lot and construct an attractive new mixed-use building containing 63 residential units and up to six ground floor retail spaces along California and Polk Streets.

The Sponsor respectfully requests that the Planning Commission approve the Project and grant a Conditional Use Authorization to allow this mixed-use Project. We look forward to presenting the Project to you on February 11, 2016.

A. Surrounding Neighborhood

The Property presently contains four single story retail spaces and an associated surface parking lot. The lots immediately across the street from the site on California Street are occupied by a Chase Bank and the former Lumiere Theater, which is currently used as a small restaurant and a clothing retailer. Directly across the street from the site on Polk Street is the multi-level Chelsea Square shopping center, which includes a grocery store, a bagel shop, and a seafood restaurant. Directly adjacent to the Project site on Polk Street is a 5-story mixed use building with residential units over two ground floor commercial spaces. The buildings in the vicinity of the Project Site generally range from one to seven stories and contain a mix of commercial and residential uses, as well as a mix of older and more modern architectural styles.

James A. Reuben | Andrew J. Junius | Kevin H. Rose | Daniel A. Frattin | John Kevin
Jay F. Drake | Lindsay M. Petrone | Sheryl Reuben¹ | Tuija I. Catalano | Thomas Tunny
David Silverman | Melinda A. Sarjapur | Mark H. Loper | Jody Knight | Stephanie L. Haughey
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B. Project Description

The Project proposes to demolish the existing retail structures and associated surface parking lot and construct a 7-story mixed-use development. The Project would include 63 residential units comprised of 37 one-bedroom units, 15 two-bedroom units, nine three-bedroom units, and two studios. Six ground floor retail spaces along Polk and California Street will provide neighborhood commercial opportunities and will activate the street along both Project frontages.

A below-grade parking garage will provide 32 residential parking spaces, eight retail parking spaces, and one car share space. The vehicular access will be located on California Street in order to utilize the existing curb cut. Residential lobby access will also be located on California Street, and a residential storage and bicycle parking area will be located on the ground floor as well. At least 65 Class 1 bicycle parking spaces and six Class 2 bicycle parking spaces will be provided. Open space at the Project will be provided through a mix of private and common usable open spaces, including 7,122 square feet of private decks, a 1,510 square foot fifth floor landscaped terrace, and a 2,822 square foot common roof deck.

The Project has been consciously designed for the context of the neighborhood, which includes a mix of early and mid century buildings. The Project will put an underutilized property on a prominent corner to more productive use and will help anchor the intersection by adding new retail frontages. The modern design includes numerous balconies, setbacks and terraces, as well as several exterior colors and a living wall panel that will break up the massing of the new structure. The retail level of the building will consist of large glass walls to connect the retail spaces with the sidewalk on both frontages. With the parking below-ground, there will be no blank walls or unsightly parking at street level. The result is a building that is harmonious with the intersection's existing blend of older and more modern structures.

C. Summary of Project Benefits

The Project will provide significant benefits to the City, including the following:

- **Provides smart infill development.** The Project will construct an attractive new mixed-use residential and retail development on an underutilized site that is within easy walking distance public transit. MUNI lines 1, 27, 19, 47, 49 are within one to two blocks of the Project Site. The Property also sits along the California cable car line, and within five to six blocks of the 38, 38R, 2, and 3 MUNI lines.
- **Adds 63 new dwelling units to the City's housing stock.** San Francisco is suffering from a housing shortage that has resulted in pent-up demand and an

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undesirable escalation in rental and for-sale housing prices. The Project would construct 63 new dwelling units, 24 of which would be family-sized units.

- **Eliminates a surface parking lot.** The Project would eliminate the type of surface parking that is discouraged by current Planning Department policy and replace it with an attractive, architecturally interesting development and landscaping, including new street trees.
- **Creates jobs.** In the short-term, the Project will create construction jobs. In the long-term, the new ground floor retail spaces will create full and part-time positions, many of which are anticipated to be filled by local residents.
- **Minimizes the visual impact of parking.** The proposed parking is inside the building below grade and not visible from the street. In addition, the Project utilizes the existing curb cut on California Street and does not add any additional curb cuts.
- **Provides a strong corner lot presence.** The Project is set on a corner lot and uses detailing that includes setbacks, balconies, terraces, and a mix of materials to provide architectural interest.
- **Beautifies the area.** The Project would improve the appearance of the neighborhood by replacing a surface parking lot and several unaesthetic commercial spaces with a well-designed residential building that has a strong connection to street-level activities and large windows that add to the character and walkability of the neighborhood. The Project will also add street trees on Polk and California Streets which will enhance the pedestrian experience on those streets.

D. Required Project Approvals. The Project requires Conditional Use Authorization pursuant to Planning Code Sections 303 and 121.1 for the development of a lot greater than 9,999 square feet in the Polk Street NCD zoning district. Conditional Use Authorization is also required pursuant to Sections 155 and 271 for the parking entrance on California Street and in order to deviate from bulk limitations on the California Street facade. The Project also requires a Variance from Planning Code Section 134 and 140 for the rear yard and dwelling unit exposure, as explained below.

- **Development of Large Lots.** Planning Code Sections 723.11 and 121.1 require Conditional Use Authorization for the development of a lot that is

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greater than 9,999 square feet. The Project Site is 18,631 square feet. Therefore, the Project Sponsor is seeking Conditional Use Authorization to allow for the development of a lot greater than 9,999 square feet.

- **Bulk Limitations.** The Property is located within an 80-A height and bulk district, which limits bulk above 40 feet to 110 feet in length and 125 feet diagonally. Planning Code Section 271(b) allows for deviations from bulk limitations with Planning Commission approval. Although the Project technically exceeds the bulk limitations on its California Street frontage, it does incorporate significant sculpting at the seventh floor and features several balconies, a mix of materials, and a living wall panel that break up the facade and provide visual interest.
- **California Street Garage Entrance.** Planning Code Section 155(r)(3) states that garage entries are not permitted on California Street except with Conditional Use Authorization. The Project does include a California Street entrance to the below grade parking garage, but this entrance utilizes an existing curb cut. No new curb cuts will be added by the Project. Because Polk Street is a retail thoroughfare with a bicycle lane that runs past the Project Site, the Project's California Street frontage is a more appropriate place for the garage entrance.
- **Rear Yard.** The Project requires a Variance from the Zoning Administrator because it does not meet the required rear yard depth per Planning Code Section 134 because a 25% rear yard is not provided. However, the Project provides 7,122 square feet of usable private outdoor space and 4,332 sq ft square feet of common open space, which is more than would be provided were the Project to instead provide a code compliant rear yard.
- **Parking Reduction.** The Project requires an administrative reduction from the parking requirements set forth in Planning Code Section 151, which requires one parking space per dwelling unit plus one space per each 500 square feet of retail space above 5,000 square feet. The Project will include 41 parking spaces (32 residential, one car share, and eight retail). However, a parking reduction is appropriate based on the pedestrian character of the neighborhood, the Project's proximity to public transit options, and the abundant bicycle parking provided on site.

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E. Community Outreach

The Project Sponsor has conducted a number of community meetings to present the Project and hear neighborhood input, including the following:

- Nob Hill Association – Project was well received by membership
- Lower Polk Neighborhood Association – Letter of support received
- Middle Polk Neighborhood Association – Project Sponsor addressed membership concerns
- Polk Merchants Association – Project Sponsor addressed membership concerns
- Community Leadership Alliance – Project Sponsor addressed membership concerns
- SPUR Review Board – Letter of support received
- Housing Action Coalition – Letter of support received
- San Francisco Building Trades Council – Letter of support received
- Marlow HOA Board – Letter of support received

The Project Sponsor also met with multiple individual community members and neighborhood association representatives. To date, the Project has received 29 letters of support. The Project Sponsor has gone out of its way to ensure the Project is compatible with the neighborhood and incorporates community input, and as a result, the Project will be a positive addition to the neighborhood.

F. Conclusion

The Project would create an attractive new mixed-use residential and retail building on an underutilized site that is currently occupied by ground floor retail and an associated surface parking lot. The Project would add new ground floor retail spaces that will activate the adjacent street frontages, and will plant new street trees to enhance the pedestrian environment on Polk and California Streets. In addition, the Project will add 63 desirable new housing units, including 24 family size units, to the City's housing stock and would contribute to the City's affordable housing program by paying an in lieu fee. For these reasons, we urge you to support this Project.

Very truly yours,

REUBEN, JUNIUS & ROSE, LLP


John Kevlin

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REUBEN, JUNIUS & ROSE, LLP

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Enclosures

cc: Vice-President Dennis Richards
Commissioner Michael Antonini
Commissioner Rich Hillis
Commissioner Christine Johnson
Commissioner Kathrin Moore
Commissioner Cindy Wu
Jonas P. Ionin – Commission Secretary
John Rahaim – Planning Director
Scott Sanchez – Zoning Administrator
Claudine Asbaugh – Project Planner

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Residences | 1567 California, San Francisco
63 New Residential Units



DRAWING INDEX

GENERAL

- G0.00 COVER SHEET
- G0.01 PLANNING DATA
- G0.02 AREA CALCULATIONS
- G0.03 GREEN BUILDING CHECKLIST
- G0.04 REAR YARD DIAGRAM
- G0.05 COMM. STREET FRONTAGE SEC 145.1

CIVIL

- C1.00 SITE SURVEY

ARCHITECTURAL

- A1.00 SITE PLAN
- A2.01 STORY P1
- A2.02 STORY 1
- A2.03 STORY 2
- A2.04 STORY 3
- A2.05 STORY 4
- A2.06 STORY 5
- A2.07 STORY 6
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- A7.00 ELEVATIONS
- A7.01 ELEVATIONS W/LIVING WALL
- A7.02 ELEVATIONS
- A7.03 ELEVATIONS
- A7.04 ELEVATIONS
- A8.00 BUILDING SECTIONS
- A8.01 BUILDING SECTIONS
- A8.02 BUILDING SECTIONS

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REVISIONS

NO.	DATE	ISSUE
△	09/23/15	NOPDR #1 RESPONSE
	01/14/16	NOPDR #2 RESPONSE
	01/21/16	NOPDR #3 RESPONSE

ISSUE

09/23/15 NOPDR #1 RESPONSE

OWNER

1567 California, LLC
1567 California St
San Francisco, CA 94109

PROJECT NAME

1567 California
San Francisco, CA 94109

BLOCK/LOT

0645 / 14 / 14A / 15

SCALE

AS NOTED

DRAWN BY

RG, MA, TG, JB

CHECKED BY

RG

PROJECT NO.

201504

DATE

1/21/16

COVER SHEET


DRAWING NO. 1 of 47

G0.00

Green Building: Site Permit Submittal

BASIC INFORMATION:

These facts, plus the primary occupancy, determine which requirements apply. For details, see AB 093 Attachment A Table 1.

Project Name THE CALIFORNIAN	Block/Lot 0645 / 14 / 14A / 15	Address 1567 California St San Francisco, CA 94109
Gross Project Area 108,389 SQ FT	Primary Occupancy RESIDENTIAL	Number of occupied floors 7 FLOORS AND BASEMENT
Design Professional/Applicant: Sign & Date 		

Instructions:

As part of application for site permit, this form acknowledges the specific green building requirements that apply to a project under San Francisco Green Building Code, California Title 24 Part 11, and related codes. Attachment C3, C4, C5, C6, C7, or C8 will be due with the applicable addendum. To use the form:

(a) Provide basic information about the project in the box at left. This info determines which green building requirements apply.

AND

(b) Indicate in one of the columns below which type of project is proposed. If applicable, fill in the blank lines below to identify the number of points the project must meet or exceed. A LEED or GreenPoint checklist is not required to be submitted with the site permit application, but using such tools as early as possible is strongly recommended.

Solid circles in the column indicate mandatory measures required by state and local codes. For projects applying LEED or GreenPoint Rated, prerequisites of those systems are mandatory. See relevant codes for details.

ALL PROJECTS, AS APPLICABLE

Construction activity stormwater pollution prevention and site runoff controls - Provide a construction site Stormwater Pollution Prevention Plan and implement SFPUC Best Management Practices.	●
Stormwater Control Plan: Projects disturbing ≥5,000 square feet must implement a Stormwater Control Plan meeting SFPUC Stormwater Design Guidelines	●
Water Efficient Irrigation - Projects that include ≥ 1,000 square feet of new or modified landscape must comply with the SFPUC Water Efficient Irrigation Ordinance.	●
Construction Waste Management - Comply with the San Francisco Construction & Demolition Debris Ordinance	●
Recycling by Occupants: Provide adequate space and equal access for storage, collection and loading of compostable, recyclable and landfill materials. See Administrative Bulletin 088 for details.	●

GREENPOINT RATED PROJECTS

Proposing a GreenPoint Rated Project (Indicate at right by checking the box.)	
Base number of required Greenpoints:	75
Adjustment for retention / demolition of historic features / building:	
Final number of required points (base number +/- adjustment)	
GreenPoint Rated (i.e. meets all prerequisites)	●
Energy Efficiency: Demonstrate a 10% energy use reduction compared to Title 24, Part 6 (2013).	●
Meet all California Green Building Standards Code requirements (CalGreen measures for residential projects have been integrated into the GreenPoint Rated system.)	●

Notes

- 1) New residential projects of 4 or more occupied floors must use the "New Residential High-Rise" column. New residential with 3 or fewer occupied floors must use the "New Residential Low Rise" column.
- 2) LEED for Homes Mid-Rise projects must meet the "Silver" standard, including all prerequisites. The number of points required to achieve Silver depends on unit size. See LEED for Homes Mid-Rise Rating System to confirm the base number of points required.

LEED PROJECTS

Type of Project Proposed (Indicate at right)	New Large Commercial	New Low Rise Residential	New High Rise Residential	Large First Time Commercial Interior	Commercial Major Alteration	Residential Major Alteration
Overall Requirements:						
LEED certification level (includes prerequisites):	GOLD	SILVER	SILVER	GOLD	GOLD	GOLD
Base number of required points:	60	2	50	60	60	60
Adjustment for retention / demolition of historic features / building:				n/a		
Final number of required points (base number +/- adjustment)				50		
Specific Requirements: (n/r indicates a measure is not required)						
Construction Waste Management – 75% Diversion AND comply with San Francisco Construction & Demolition Debris Ordinance - LEED MR 2, 2 points	●	●	●	●	Meet C&D ordinance only	●
Energy Use Comply with California Title-24 Part 6 (2013) and meet LEED minimum energy performance (LEED EA p2)	●	LEED prerequisite	●	●	LEED prerequisite only	
Renewable Energy or Enhanced Energy Efficiency Effective 1/1/2012: Generate renewable energy on-site ≥1% of total annual energy cost (LEED EA2), OR Demonstrate at least 10% energy use reduction (compared to Title 24 Part 6 2013), OR Purchase Green-E certified renewable energy credits for 35% of total electricity use (LEED EA6).	●	n/r	n/r	n/r	n/r	n/r
Enhanced Commissioning of Building Energy Systems LEED EA 3	●	Meet LEED prerequisites				
Water Use - 30% Reduction LEED WE 3, 2 points	●	Meet LEED prerequisite	●	Meet LEED prerequisite		
Enhanced Refrigerant Management LEED EA 4	●	n/r	n/r	●	●	n/r
Indoor Air Quality Management Plan LEED IEQ 3.1	●	CalGreen 4.504.1	CalGreen 4.504.1	CalGreen 5.504.3	CalGreen 5.504.3	CalGreen 4.504.1
Low-Emitting Materials LEED IEQ 4.1, 4.2, 4.3, and 4.4	●	●	●	●	●	●
Bicycle parking: Provide short-term and long-term bicycle parking for 5% of total motorized parking capacity each, or meet San Francisco Planning Code Sec 155, whichever is greater, or meet LEED credit SSC4.2.	●	See San Francisco Planning Code 155		●	See San Francisco Planning Code 155	
Designated parking: Mark 8% of total parking stalls for low-emitting, fuel efficient, and carpool/van pool vehicles.	●			●	n/r	n/r
Water Meters: Provide submeters for spaces projected to consume more than 1,000 gal/day, or more than 100 gal/day if in building over 50,000 sq. ft.	●	n/r	n/r	●	(addition only)	n/r
Air Filtration: Provide at least MERV-8 filters in regularly occupied spaces of mechanically ventilated buildings (or LEED credit IEQ 5).	●	n/r	n/r	●	●	n/r
Air Filtration: Provide MERV-13 filters in residential buildings in air-quality hot-spots (or LEED credit IEQ 5). (SF Health Code Article 38 and SF Building Code 1203.5)	n/r	●	●	n/r	n/r	●
Acoustical Control: wall and roof-ceilings STC 50, exterior windows STC 30, party walls and floor-ceilings STC 40.	●	See CBC 1207		●	(envelope alteration & addition only)	n/r

OTHER APPLICABLE NON-RESIDENTIAL PROJECTS

Requirements below only apply when the measure is applicable to the project. Code references below are applicable to New Non-Residential buildings. Corresponding requirements for additions and alterations can be found in Title 24 Part 11, Division 5.7. Requirements for additions or alterations apply to applications received July 1, 2012 or after. ³	Other New Non-Residential	Addition ≥1,000 sq ft OR Alteration ≥\$200,000 ³
Type of Project Proposed (Check box if applicable)		
Energy Efficiency: Comply with California Energy Code, Title 24, Part 6 (2013).	●	●
Bicycle parking: Provide short-term and long-term bicycle parking for 5% of total motorized parking capacity each, or meet San Francisco Planning Code Sec 155, whichever is greater (or LEED credit SSC4.2).	●	●
Fuel efficient vehicle and carpool parking: Provide stall marking for low-emitting, fuel efficient, and carpool/van pool vehicles; approximately 8% of total spaces.	●	●
Water Meters: Provide submeters for spaces projected to consume >1,000 gal/day, or >100 gal/day if in buildings over 50,000 sq. ft.	●	Addition only
Indoor Water Efficiency: Reduce overall use of potable water within the building by 20% for showerheads, lavatories, kitchen faucets, wash fountains, water closets, and urinals.	●	●
Commissioning: For new buildings greater than 10,000 square feet, commissioning shall be included in the design and construction of the project to verify that the building systems and components meet the owner's project requirements. OR for buildings less than 10,000 square feet, testing and adjusting of systems is required.	●	(Testing & Balancing)
Protect duct openings and mechanical equipment during construction	●	●
Adhesives, sealants, and caulks: Comply with VOC limits in SCAQMD Rule 1168 VOC limits and California Code of Regulations Title 17 for aerosol adhesives.	●	●
Paints and coatings: Comply with VOC limits in the Air Resources Board Architectural Coatings Suggested Control Measure and California Code of Regulations Title 17 for aerosol paints.	●	●
Carpet: All carpet must meet one of the following: 1. Carpet and Rug Institute Green Label Plus Program, 2. California Department of Public Health Standard Practice for the testing of VOCs (Specification 01350), 3. NSF/ANSI 140 at the Gold level, 4. Scientific Certifications Systems Sustainable Choice, OR 5. California Collaborative for High Performance Schools EQ 2.2 and listed in the CHPS High Performance Product Database AND carpet cushion must meet Carpet and Rug Institute Green Label, AND indoor carpet adhesive & carpet pad adhesive must not exceed 50 g/L VOC content.	●	●
Composite wood: Meet CARB Air Toxics Control Measure for Composite Wood	●	●
Resilient flooring systems: For 80% of floor area receiving resilient flooring, install resilient flooring complying with the VOC-emission limits defined in the 2009 Collaborative for High Performance Schools (CHPS) criteria or certified under the Resilient Floor Covering Institute (RFCI) FloorScore program.	●	●
Environmental Tobacco Smoke: Prohibit smoking within 25 feet of building entries, outdoor air intakes, and operable windows.	●	●
Air Filtration: Provide at least MERV-8 filters in regularly occupied spaces of mechanically ventilated buildings.	●	●
Acoustical Control: Wall and roof-ceilings STC 50, exterior windows STC 30, party walls and floor-ceilings STC 40.	●	(envelope alteration & addition only)
CFCs and Halons: Do not install equipment that contains CFCs or Halons.	●	●
Additional Requirements for New A, B, I, OR M Occupancy Projects 5,000 - 25,000 Square Feet		
Construction Waste Management - Divert 75% of construction and demolition debris AND comply with San Francisco Construction & Demolition Debris Ordinance.	●	Meet C&D ordinance only
Renewable Energy or Enhanced Energy Efficiency Effective January 1, 2012: Generate renewable energy on-site equal to ≥1% of total annual energy cost (LEED EA2), OR demonstrate a 10% energy use reduction compared to Title 24 Part 6 (2013), OR purchase Green-E certified renewable energy credits for 35% of total electricity use (LEED EA6).	●	n/r

Attachment C-2:

Green Building:

Site Permit Submittal

Version: July 1, 2014



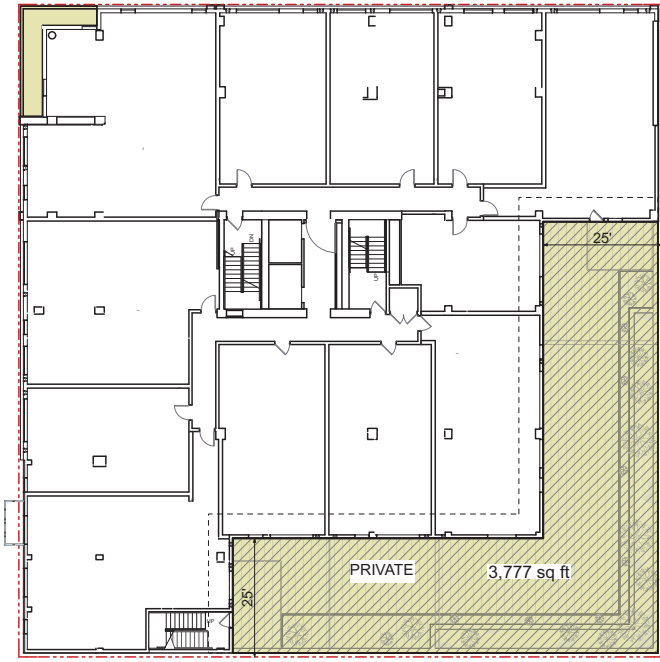
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660 THIRD STREET
SAN FRANCISCO, CA 94107
415.774.3333
info@rg-architecture.com

ISSUE
MEP PRELIMINARY BID SET 1/2/16

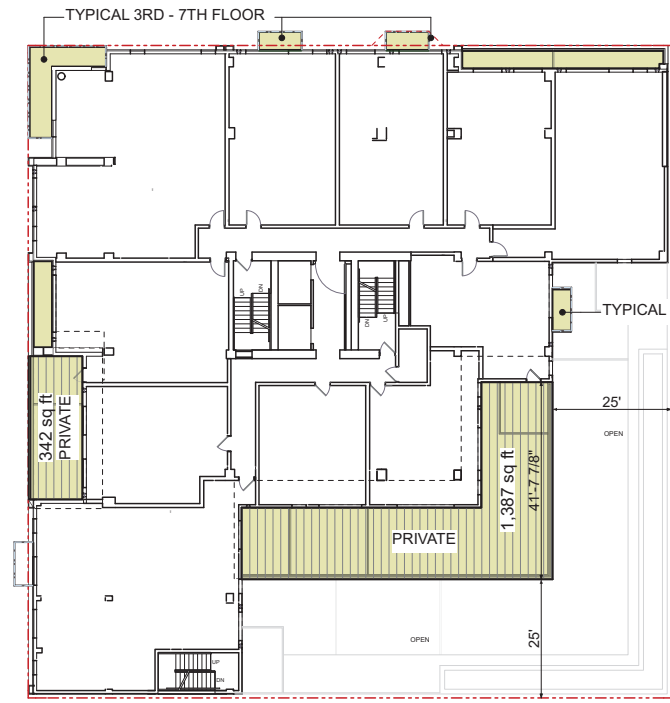
San Francisco, CA 94109

1567 California

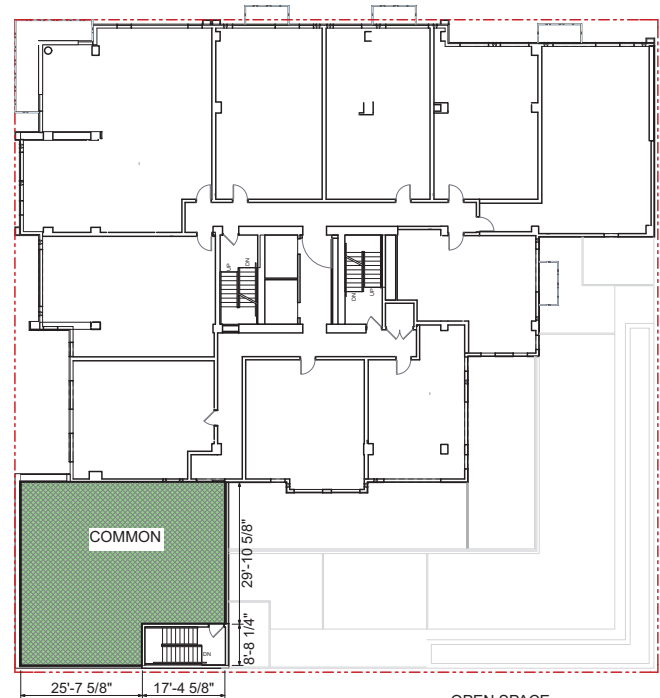
BLOCK/LOT
0645 / 14 / 14A / 15



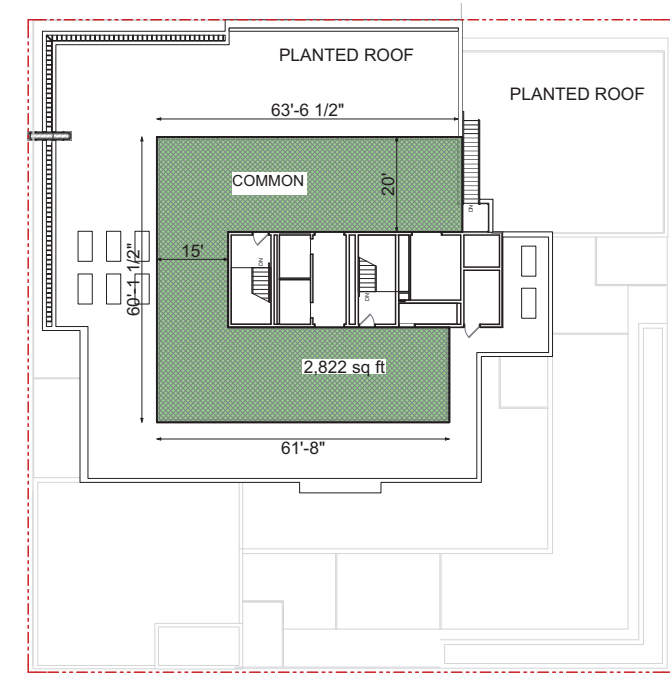
OPEN SPACE
THIS LEVEL = 3,777 sq ft / 20.2% LOT AREA



OPEN SPACE
THIS LEVEL = 1729 sq ft / 9.2% LOT AREA



OPEN SPACE
THIS LEVEL = 1,510 sq ft / 8.1% LOT AREA



OPEN SPACE
THIS LEVEL = 2,822 sq ft / 15.1% LOT AREA

GENERAL NOTES

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SHEET NOTES

OPEN SPACE & USEABLE OPEN
SPACE AREA TOTAL CALCULATIONS

STORY	OPEN SPACE TYPE	AREA
1		
2	PRIVATE	3777 sq ft
3		
4	PRIVATE	1729 sq ft
5	COMMON	1510 sq ft
6		
7		
ROOF	COMMON	2822 sq ft
TOTAL	PRIVATE	5506 sq ft
TOTAL	COMMON	4332 sq ft
TOTAL		9838 sq ft

LOT AREA = 18,625 SQ FT
TOTAL OPEN SPACE 9,838 sq ft = 52.8%

REVISIONS

NO.	DATE	ISSUE
1	09/23/15	NOPDR #1 RESPONSE
2	01/14/16	NOPDR #2 RESPONSE
3	01/21/16	NOPDR #3 RESPONSE

ISSUE
09/23/15 NOPDR #1 RESPONSE

OWNER
1567 California, LLC
1567 California St
San Francisco, CA 94109

PROJECT NAME
1567 California
San Francisco, CA 94109

BLOCK/LOT
0645 / 14 / 14A / 15

SCALE
AS NOTED
DRAWN BY
RG, MA, TG, JB
CHECKED BY
RG

PROJECT NO.
201504
DATE
1/21/16

REAR YARD DIAGRAM

DRAWING NO. 5 of 47

G0.04

KEY

	OPEN SPACE, PRIVATE
	QUALIFYING OPEN SPACE, PRIVATE
	REAR YARD EQUIVALENT CONTRIBUTING TO STORY
	COMMON USEABLE OPEN AREA



COMMERCIAL FRONTAGE WALL AREA = 2,417 sq ft
 FENESTRATION = 1,727 sq ft = 71.4%

WEST ELEVATION
 SCALE: 1/8" = 1'-0" 2



COMMERCIAL FRONTAGE WALL AREA = 1,334 sq ft
 FENESTRATION = 921 sq ft = 68.9%

NORTH ELEVATION
 SCALE: 1/8" = 1'-0" 1

GENERAL NOTES

SHEET NOTES

KEY

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REVISIONS
 NO. DATE ISSUE

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ISSUE

09/23/15 NOPDR #1 RESPONSE

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PROJECT NAME

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BLOCK/LOT

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SCALE

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PROJECT NO.

201504

DATE

1/21/16

**COMM. STREET
 FRONTAGE SEC 145.1**

DRAWING NO. 6 of 47

G0.05

ZONING INFORMATION

Listed below are Setback, Height, and Floor Space Area Restrictions as disclosed by applicable Zoning or Building Codes unless "None" is stated below. The source of this information is Laura Linch, with City of San Francisco, 415-558-6377

Zoning Designation: "NCD" Neighborhood Commercial District
 Building Height: 80 Feet
 Minimum Lot Area: N/A
 Building Setbacks: N/A
 Parking Requirements: (Retail) One for each 500 sq ft of occupied floor area

FLOOD ZONE

A field survey was not conducted to determine the flood zone areas. Any flood zone lines distinguishing between flood areas are graphically plotted from FEMA Flood Insurance Rate Maps (FIRM). A flood elevation certificate may be needed to determine or verify the location of the flood areas. The subject property's community does participate in the program. It is determined that the subject property resides in an area not mapped by FEMA and Special Flood Hazard Zone is undetermined. The Flood Insurance Rate Program was contacted on 10/30/2013 by telephone or email (www.fema.gov)

UTILITY NOTE

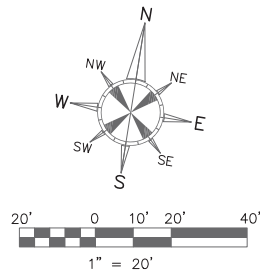
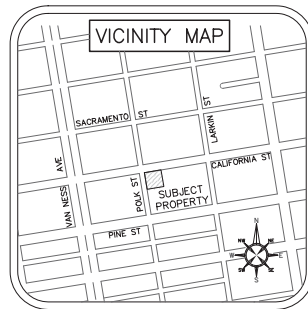
{11a} The aboveground utilities shown have been located from field survey information only. The surveyor makes no guarantee that the utilities shown comprise all such utilities in the area, either in service or abandoned. The surveyor further certifies that they are located as accurately as possible from the field information obtained.

MISCELLANEOUS NOTES

- N1 Property has direct access to California Street which is a public right-of-way.
- N2 The addresses of 1436 and 1498 Polk Street were observed by the surveyor posted on property as of date of survey.
- N3 The basis of bearings of this survey is based on the center line of California Street, shown as East, per plat.
- N4 The table below describes the type and number of parking stalls entirely within the property boundary. Stalls that are partially within the boundary are listed under the heading "partial". Partial stalls are not counted in the total.

PARKING				
REGULAR	HANDICAPPED	TRAILER	PARTIAL	TOTAL
16	0	0	0	16

- N5 There was no observable evidence of earth moving work, building construction or building additions within recent months.
- N6 There were no changes in street right-of-way lines either completed or proposed, and available from controlling jurisdiction or evidence of recent street or sidewalk construction repairs.
- N7 There was no observable evidence of site use as a solid waste dump, sump, sanitary landfill or cemetery.
- N8 The subject property is located at the intersection of California Street and Polk Street.
- N9 No evidence of potential wetlands was observed on the subject property at the time the survey was conducted, nor have we received any documentation of any wetlands being located on the subject property.
- N10 This survey map correctly represents the facts at the time of the survey.
- N11 There are no discrepancies between the boundary lines of the property as shown on this survey map and as described in the legal description presented in the title commitment.
- N12 The boundary lines of the property are contiguous with the boundary lines of all adjoining streets, highways, right-of-ways and easements, public or private, as described in their most recent respective legal descriptions of record.
- N13 The subject property (APN# 0645-014, 0654-014A and 0654-015; Owner: Lu Li Hwa) Contains: 18,631 sq. ft. or 0.4277 acres, more or less.



SYMBOL LEGEND

MONUMENT SET (AS DESCRIBED)	MONUMENT FOUND (AS DESCRIBED)	LIST OF ABBREVIATIONS
○ - BUSH	○ - CONCRETE	U/S - UNDERGROUND
○ - GAS METER	○ - ELEC. PEDESTAL	AC - HANDICAPPED CONC. - CONCRETE
○ - SIGNAL LIGHT	○ - FIRE HYDRANT	RET. - RETAINING APPROX. - APPROXIMATE
○ - PIPELINE MARK	○ - POWER MANHOLE	SAN. - SANITARY ASPAL. - ASPHALT
○ - MAIL BOX	○ - POWER POLE	TPP. - TYPICAL
○ - TELE. PED.	○ - WATER VALVE	BLD. - BUILDING
○ - SAN. SER. MAN.	○ - TRM. SER. MAN.	PEL. - PEDESTAL PLANTER
○ - GUY WIRE	○ - PULL BOX	C/R - GAS REGULATOR TRANS. - TRANSFORMER
○ - CURB WALK	○ - GUARD POST	FC - CENTER LINE
○ - MAIL COVER	○ - LIGHT POLE	RF - RIGHT-OF-WAY
○ - TV PED.	○ - STOCKADE FCE	FCB - FENCE
○ - CLEAN OUT	○ - MONITOR WELL	BLVD. - BOULEVARD
○ - PARKING METER	○ - WATER METER	BLK. - BUILDING SETBACK
○ - ELEC. TRANS.	○ - HANDCAP.	ESK. - ESKIMU
○ - UNDERGROUND TANK ACCESS	○ - GAS VALVE	A/S - AUTO-SPRINKLER
○ - AUTO SPRINKLER	○ - TRAFFIC CONTROL BOX	DET. - DETAIL
○ - STORM SEWER	○ - FIRE ROSE	PL - PLANT DIMENSION
○ - SANITARY SEWER	○ - WATER LINE	P - TOP OF RW
○ - FIBER OPTIC	○ - OVERHEAD ELECTRIC	PL - FLOW LINE
○ - G.S. - GROUND LEVEL SPOT ELEVATION	○ - P.M. - POINT OF MEASUREMENT	RD - ROAD DIMENSION
○ - B.M. - TOP OF BUILDING SPOT ELEVATION		

SURVEY RELATED ITEMS CORRESPONDING TO SCHEDULE B TITLE COMMITMENT

According to Preliminary Report Order Number 0224031163-CB, bearing an effective date of September 26, 2013, there are no Schedule "B" items pertaining to the subject property.

STATEMENT OF ENCROACHMENTS

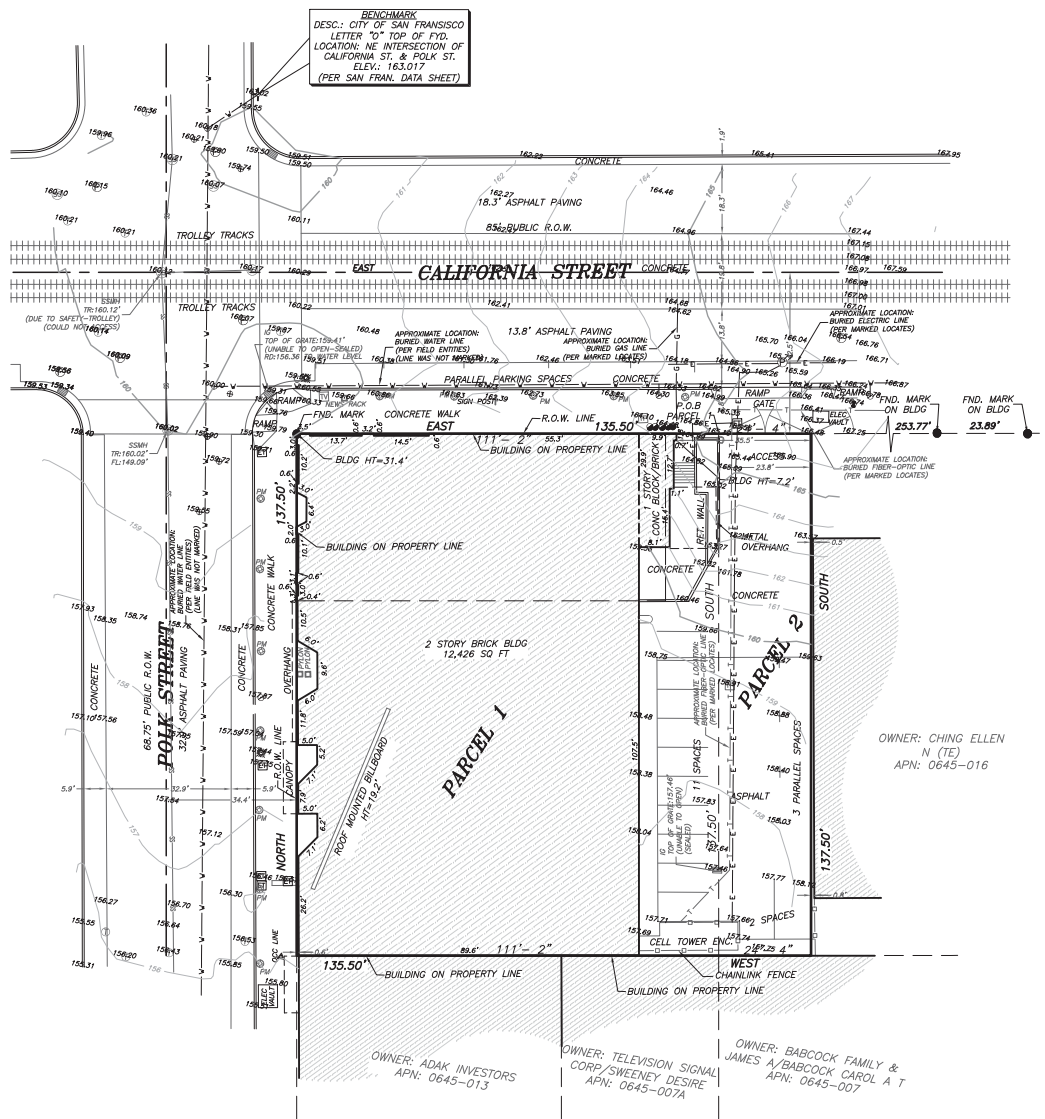
No visible encroachments or observed evidence of encroachments onto or over subject property's boundary line as of date of survey.

TITLE LEGAL DESCRIPTION

PARCEL I:
 BEGINNING at the point of intersection of the southerly line of California Street and the Easterly line of Polk Street; running thence Easterly along the Southerly line of California Street 111 feet and 2 inches; thence at a right angle Southerly 137 feet and 6 inches; thence at a right angle Westerly 111 feet and 2 inches to the Easterly line of Polk Street; thence Northerly along the Easterly line of Polk Street 137 feet and 6 inches to the point of beginning.
 BEING part of Western Addition Block No. 15.
 APN: Lot 014 & 014A; Block 0645

PARCEL II:
 BEGINNING at a point on the Southerly line of California Street, distant thereon 111 feet and 2 inches Easterly from the easterly line of Polk Street; running thence Easterly along said line of California Street 24 feet and 4 inches; thence at a right angle Southerly 137 feet and 6 inches; thence at a right angle, Westerly 24 feet and 4 inches; thence at a right angle Northerly 137 feet and 6 inches to the point of beginning.
 BEING a portion of Western Addition Block No. 15.
 APN: Lot 015; Block 0645

The above described parcel is the same land described in Old Republic Title Company's Preliminary Report Order No. 0224031163-CB, bearing an effective date of September 26, 2013.



ALTA/ACSM LAND TITLE SURVEY
 FOR
Polk Street
 PARTNER PROJECT NUMBER 13-110720.2
 PROPERTY ADDRESS: 1436 and 1498 Polk Street and 1567 California Street, San Francisco, California 94109
 ALTA SURVEY BASED AND RELIED ON OLD REPUBLIC TITLE COMPANY PRELIMINARY REPORT ORDER No. 0224031163-CB, BEARING AN EFFECTIVE DATE OF SEPTEMBER 26, 2013

CERTIFICATION

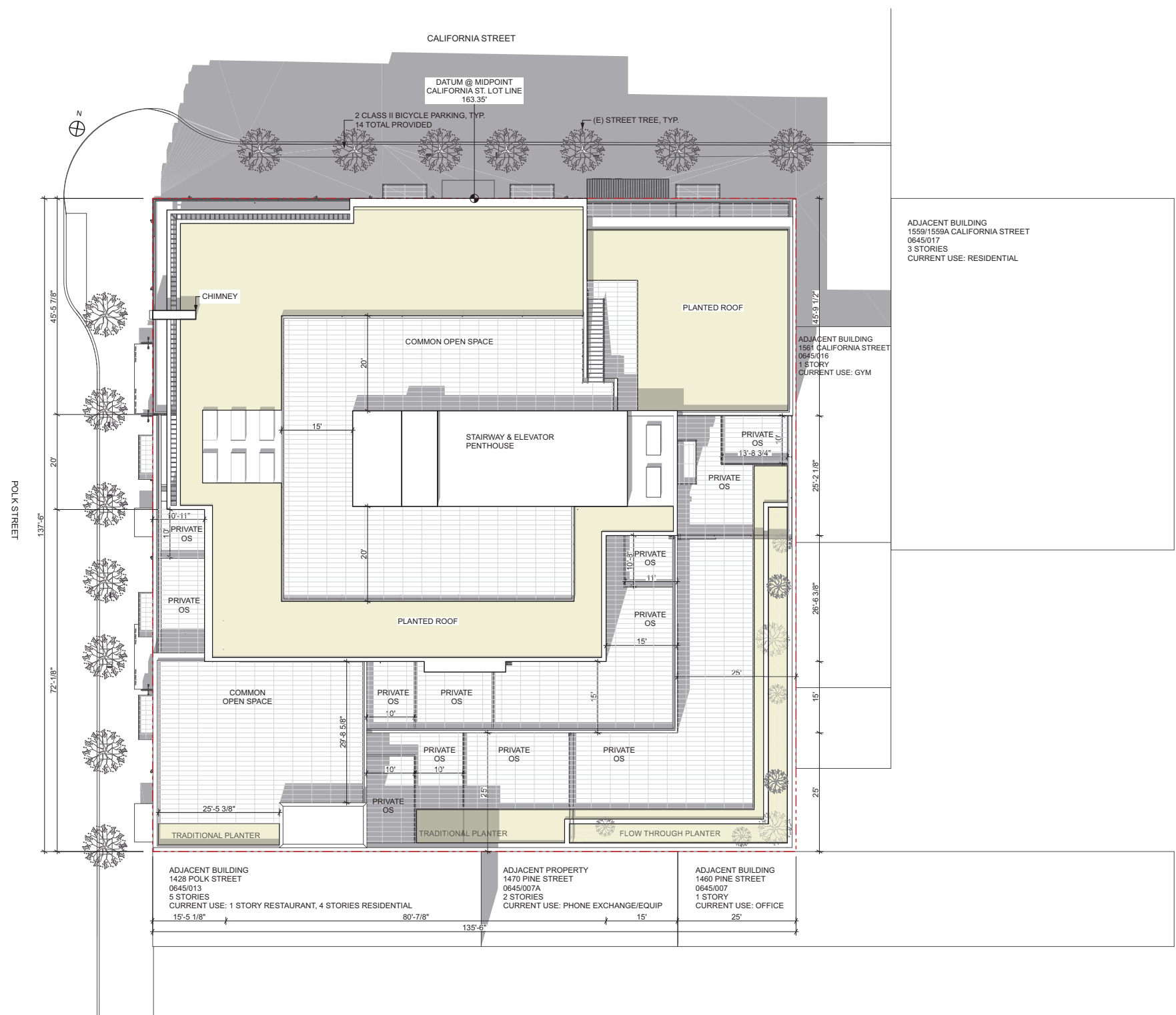
To: Partner Engineering and Science, Inc.
 This is to certify that this map or plat and the survey on which it is based were made in accordance with the 2011 Minimum Standard Detail Requirements for ALTA/ACSM Land Title Surveys, jointly established and adapted by ALTA and NSPS, and includes items 2, 3, 4, 5, 6b, 7a, 7b1, 7c, 8, 9, 11a, 11b, 13, 14, 16, 17, 18, 19 and 22 of Table A thereof. The field work was completed on 11/01/2013.
 Date of Plat or Map:

PRELIMINARY

KEVIN B. BRONSON
 RPLS No.: 8523
 IN THE STATE OF CALIFORNIA
 DATE OF SURVEY: 11/01/2013
 DATE OF LAST REVISION:
 9/11/2015

Survey Prepared By:
 The Bronson Co.
 6206 Epps Drive
 Winton, CA

PARTNER
 Engineering and Science, Inc.
 30505 BAINBRIDGE ROAD
 SUITE 190
 SOLON, OHIO 44139
 T 440-987-1001
 jdavenport@partneresi.com
 http://www.partneresi.com



GENERAL NOTES

SHEET NOTES

GRADE PLANE CALCULATIONS
 CALIFORNIA STREET:
 167.56 + 174.1' * 2 = 170.85'
 AVERAGE SLOPE OF CURB = 4.80%
 POLK STREET*
 163.5' + 167.6' * 2 = 165.55'
 AVERAGE SLOPE OF CURB = 2.98%



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 mail@rg-architecture.com



REVISIONS

NO.	DATE	ISSUE
▲ 09/23/15	NOPDR #1 RESPONSE	
01/14/16	NOPDR #2 RESPONSE	
01/21/16	NOPDR #3 RESPONSE	

ISSUE

09/23/15 NOPDR #1 RESPONSE

OWNER

1567 California, LLC
 1567 California St
 San Francisco, CA 94109

PROJECT NAME

1567 California
 San Francisco, CA 94109

BLOCK/LOT

0645 / 14 / 14A / 15

SCALE

AS NOTED

DRAWN BY

RG, MA, TG, JB

CHECKED BY

RG

PROJECT NO.

201504

DATE

1/21/16

SITE PLAN

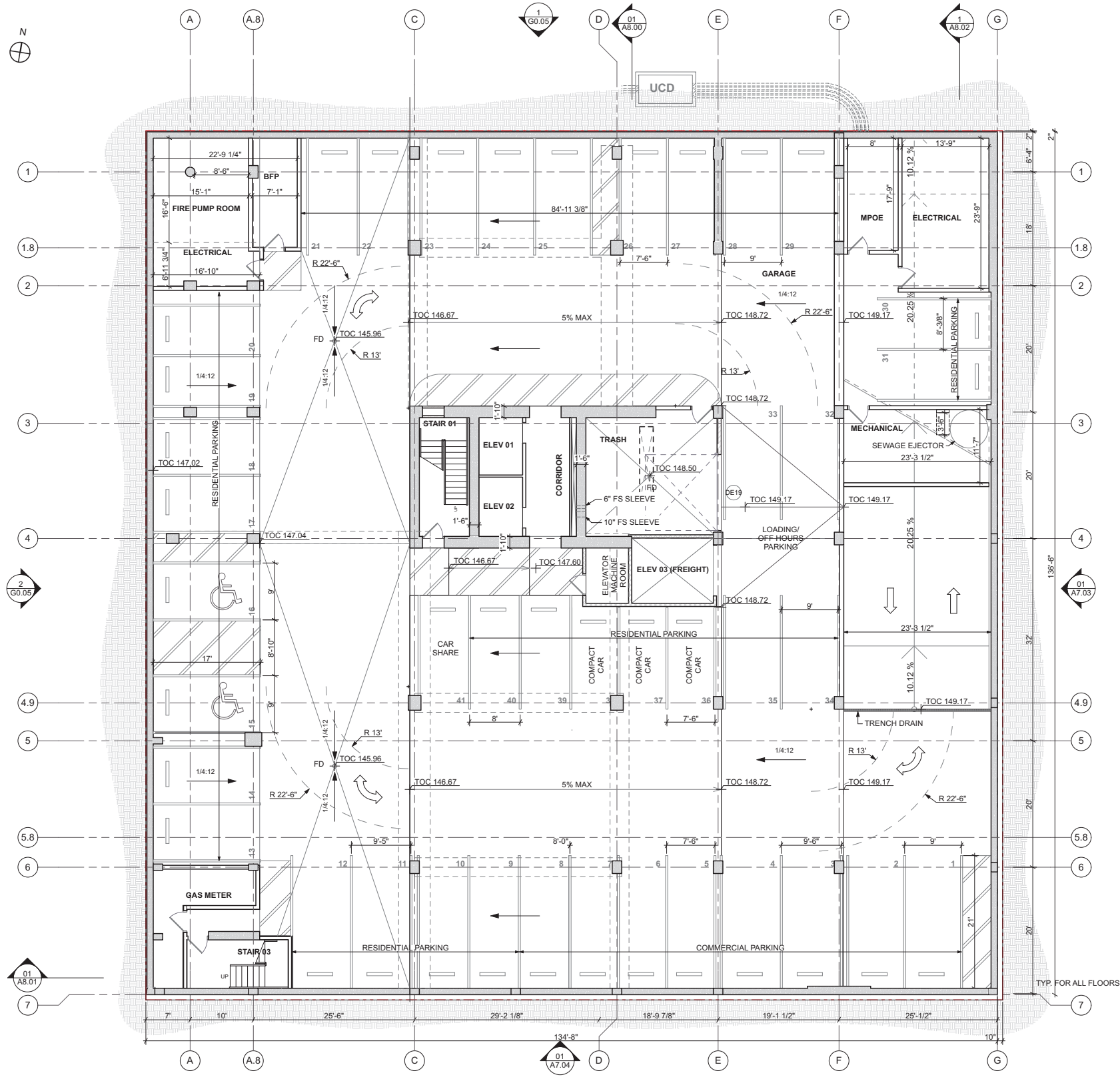
DRAWING NO. 8 of 47

A1.00

KEY



TOTAL GROSS EXT.
18,360 sq ft



GENERAL NOTES

SHEET NOTES

DIRECT ALL FLOOR DRAINS
IN GARAGE TO SAND & OIL
INTERCEPT

KEY

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SCALE

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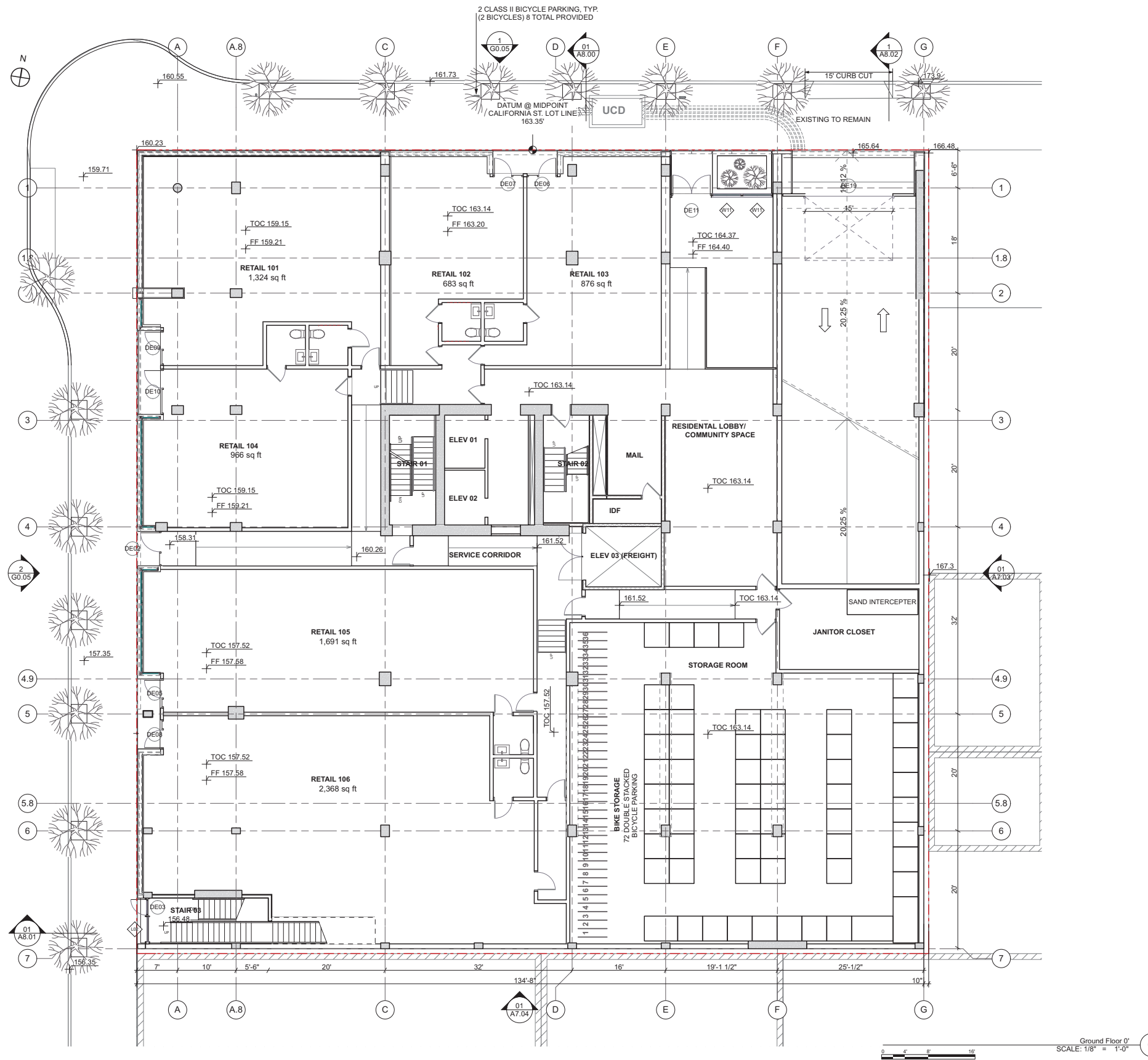
STORY P1

DRAWING NO. 9 of 47

A2.01

Garage -14'
SCALE: 1/8" = 1'-0"

TOTAL GROSS EXT.
17,917 sq ft



GENERAL NOTES

SHEET NOTES

KEY

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REVISIONS

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1	09/23/15	NOPDR #1 RESPONSE
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3	01/21/16	NOPDR #3 RESPONSE

ISSUE

09/23/15 NOPDR #1 RESPONSE

OWNER
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San Francisco, CA 94109

BLOCK/LOT
0645 / 14 / 14A / 15

SCALE
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RG
PROJECT NO.
201504
DATE
1/21/16

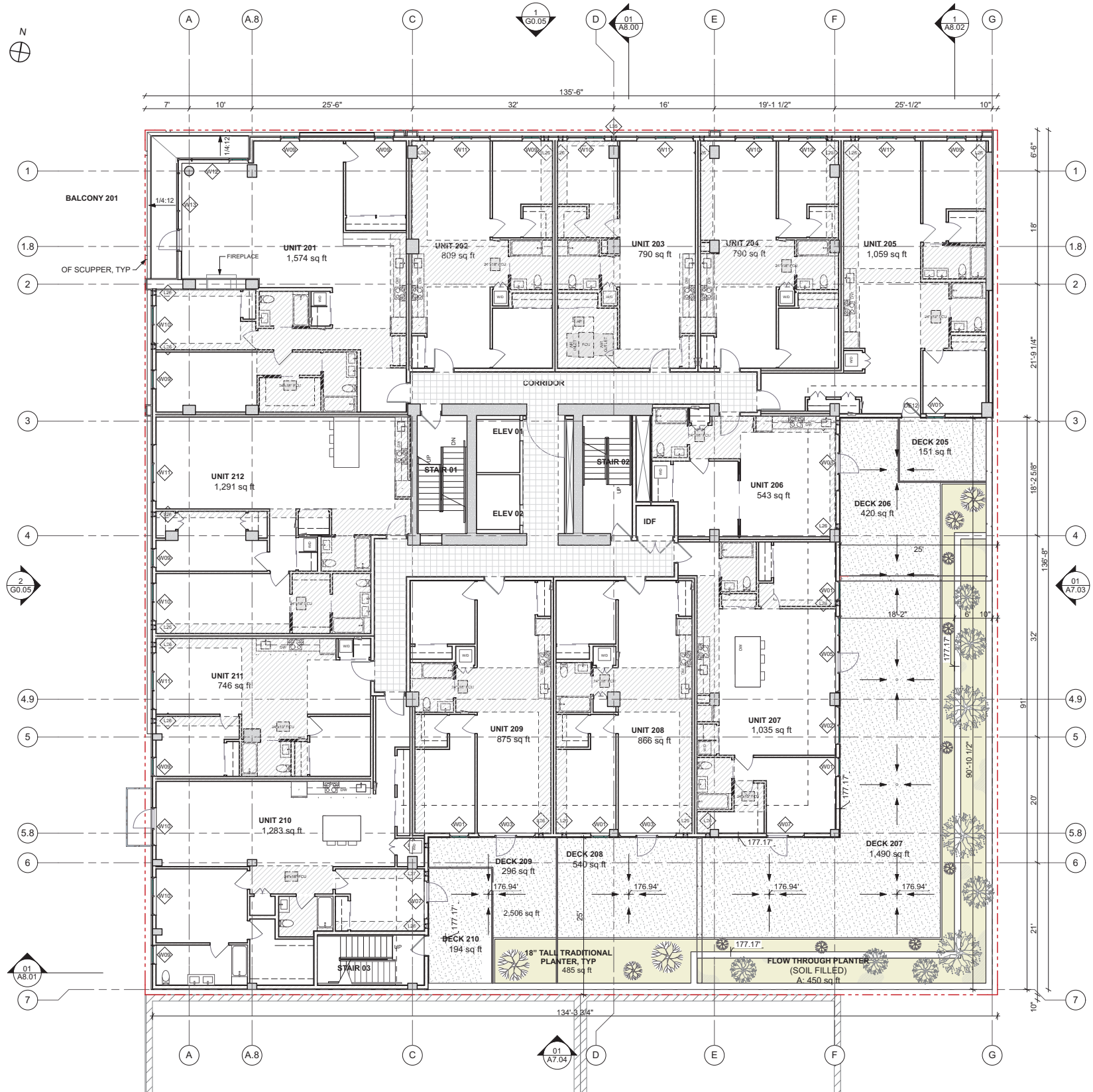
STORY 1

DRAWING NO. 10 of 47

A2.02

Ground Floor 0'
SCALE: 1/8" = 1'-0"

TOTAL GROSS EXT.
14,260 sq ft



GENERAL NOTES

SHEET NOTES

KEY

2nd Floor +14'-0"
SCALE: 1/8" = 1'-0" 01

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REVISIONS

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ISSUE
09/23/15 NOPDR #1 RESPONSE

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BLOCK/LOT
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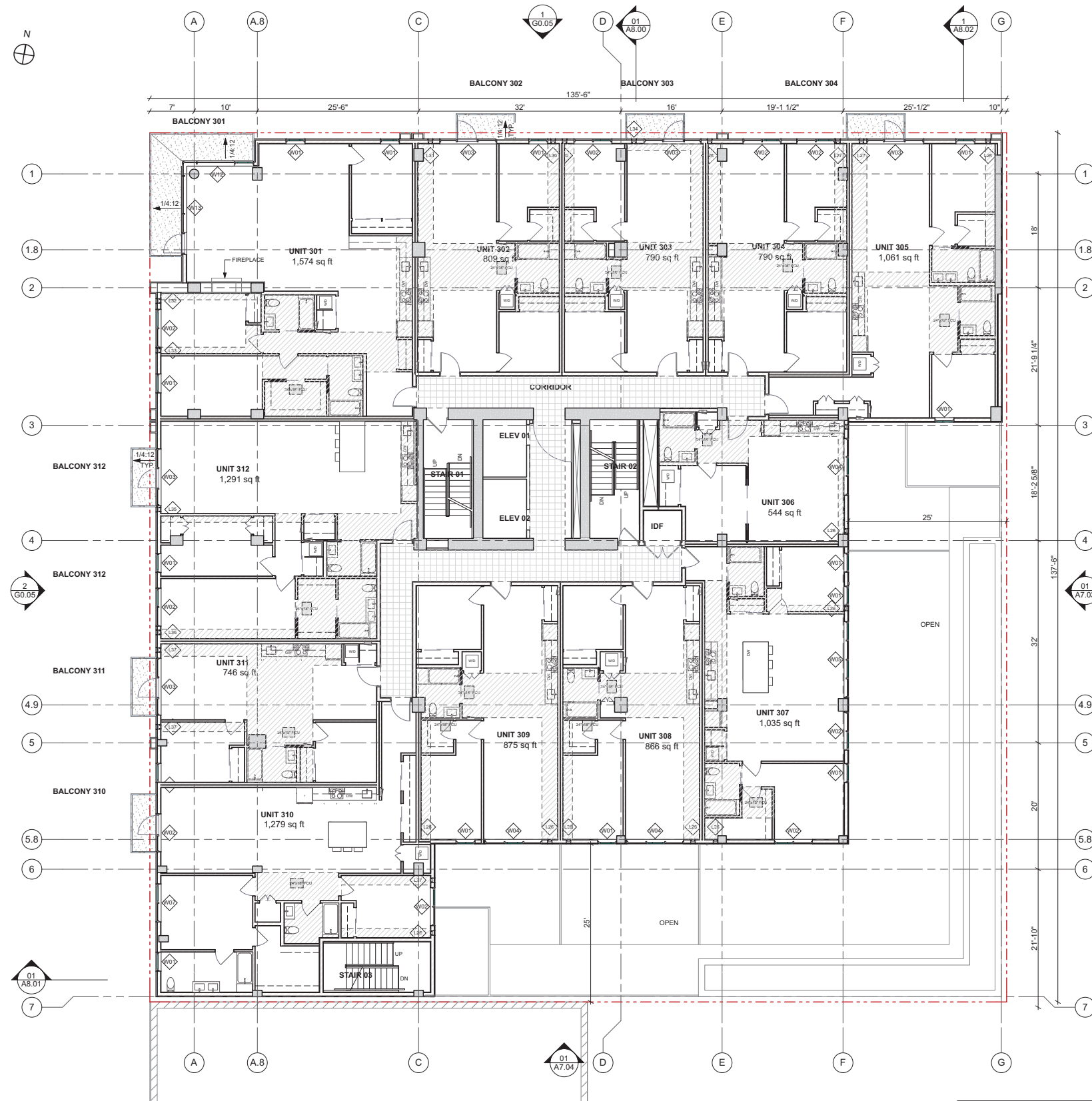
SCALE
AS NOTED
DRAWN BY
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RG
PROJECT NO.
201504
DATE
1/21/16

STORY 2

DRAWING NO. 11 of 47

A2.03

TOTAL GROSS EXT.
14,266 sq ft



3rd Floor +25'-0"
SCALE: 1/8" = 1'-0" 01

GENERAL NOTES

SHEET NOTES

KEY

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09/23/15 NOPDR #1 RESPONSE
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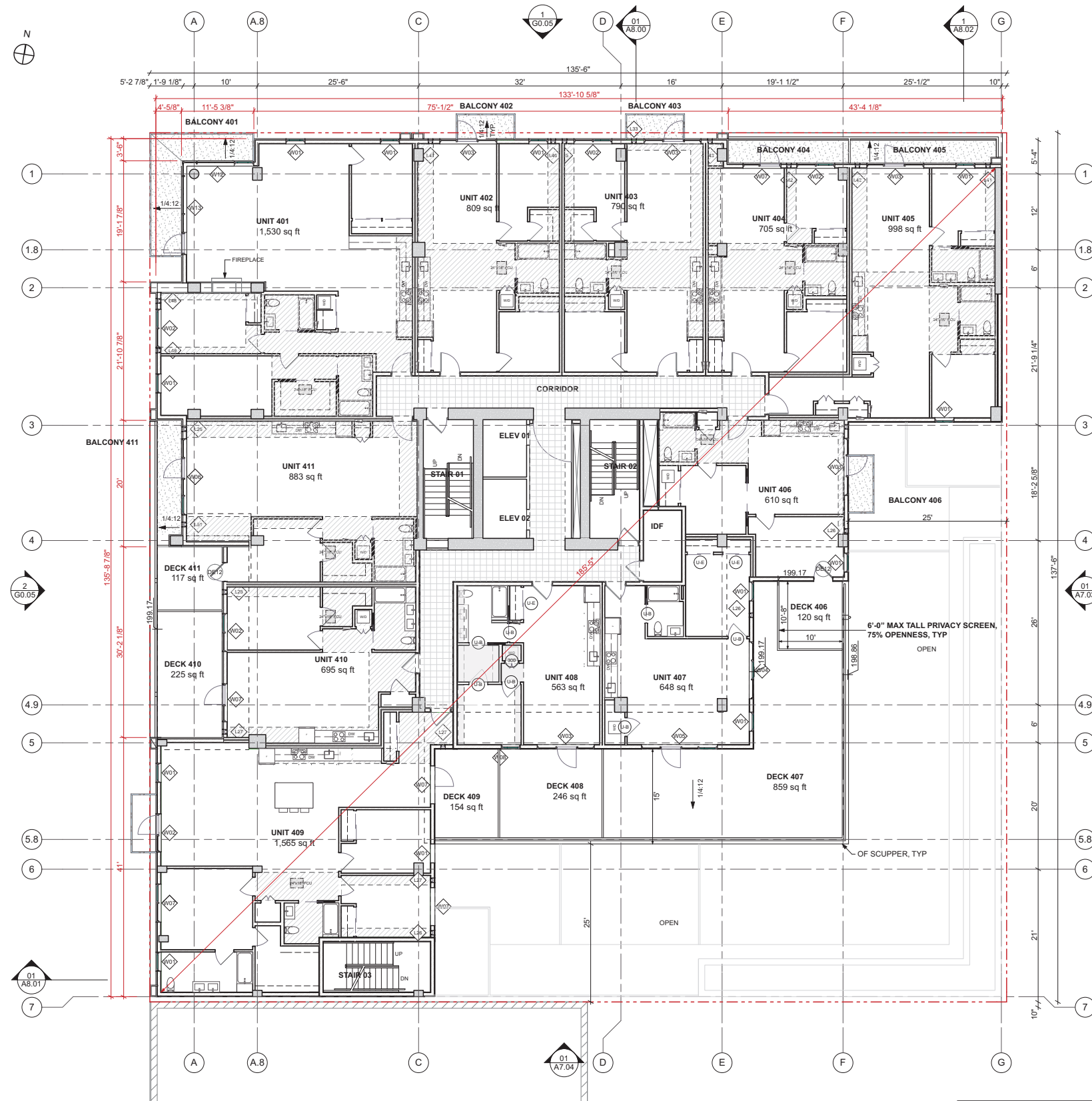
SCALE
AS NOTED
DRAWN BY
RG, MA, TG, JB
CHECKED BY
RG
PROJECT NO.
201504
DATE
1/21/16

STORY 3

DRAWING NO. 12 of 47

A2.04

TOTAL GROSS EXT.
12,331 sq ft



4th Floor +36'-0"
SCALE: 1/8" = 1'-0" 01

GENERAL NOTES

SHEET NOTES

KEY

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BLOCK/LOT
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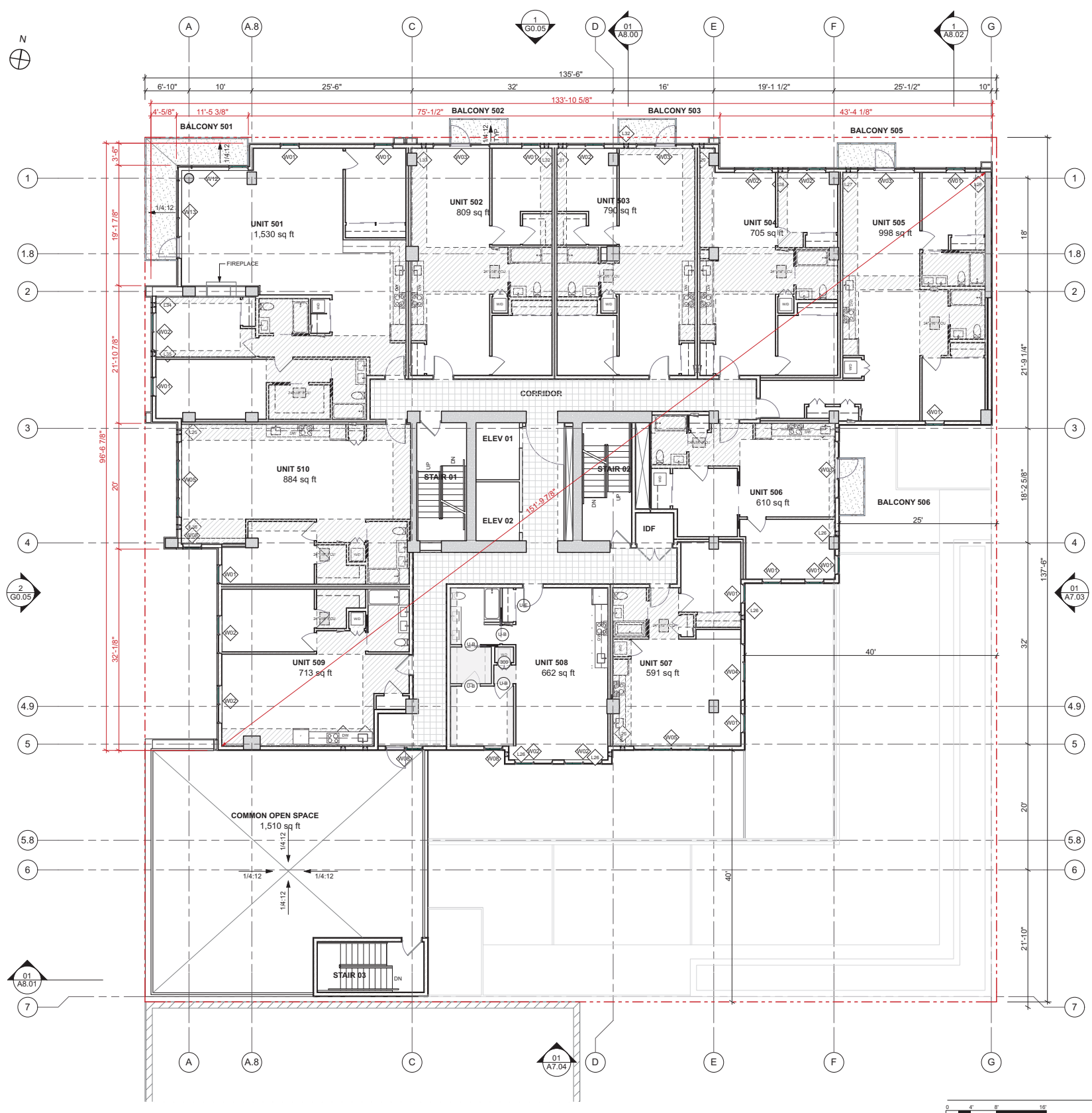
SCALE
AS NOTED
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CHECKED BY
RG
PROJECT NO.
201504
DATE
1/21/16

STORY 4

DRAWING NO. 13 of 47

A2.05

TOTAL GROSS EXT.
10,614 sq ft



5th Floor +47'-0"
SCALE: 1/8" = 1'-0"

GENERAL NOTES

SHEET NOTES

KEY

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01/21/16	NOPDR #3 RESPONSE	

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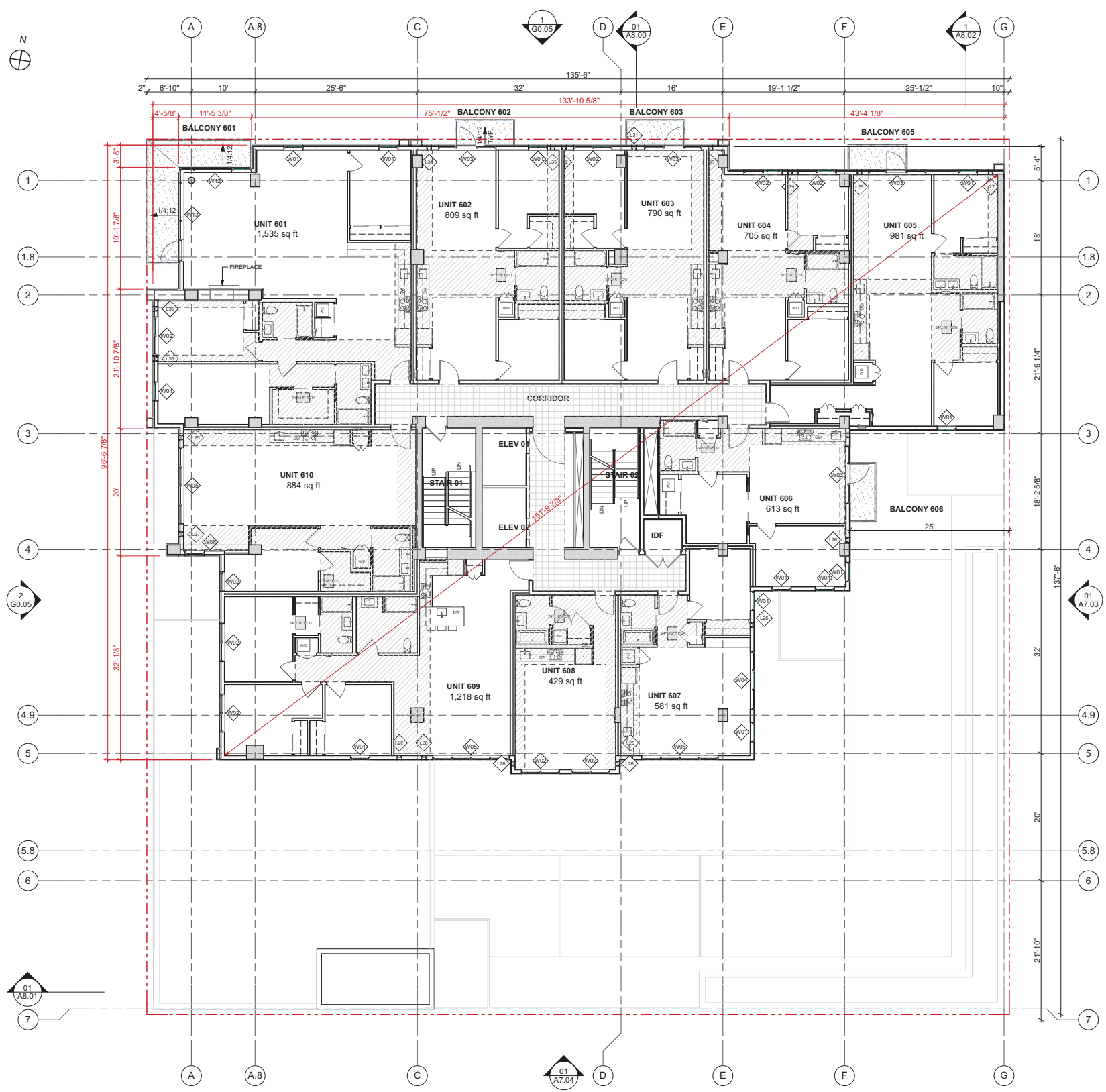
SCALE
AS NOTED
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CHECKED BY
RG
PROJECT NO.
201504
DATE
1/21/16

STORY 5

DRAWING NO. 14 of 47

A2.06

TOTAL GROSS EXT.
10,615 sq ft



6th Floor +58'-0"
SCALE: 1/8" = 1'-0"

GENERAL NOTES

SHEET NOTES

KEY

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BLOCK/LOT
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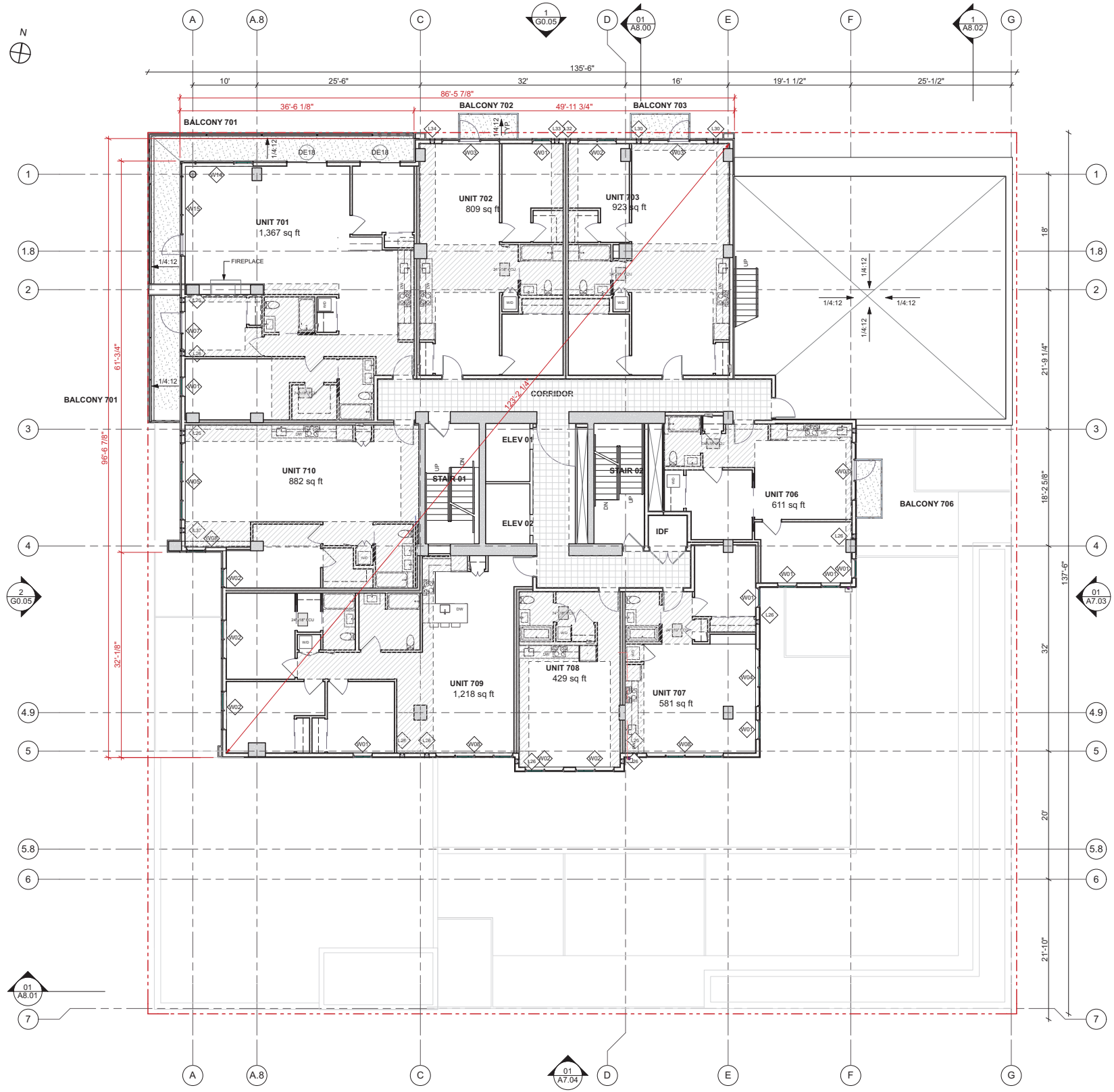
SCALE
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RG
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201504
DATE
1/21/16

STORY 6

DRAWING NO. 15 of 47

A2.07

TOTAL GROSS EXT.
8,735 sq ft



7th Floor +69'-0"
SCALE: 1/8" = 1'-0"

GENERAL NOTES

SHEET NOTES

KEY

rg-architecture

560 THIRD STREET
SAN FRANCISCO, CA 94107
415.699.3640
mail@rg-architecture.com



REVISIONS

NO.	DATE	ISSUE
09/23/15	NOPDR #1 RESPONSE	
01/14/16	NOPDR #2 RESPONSE	
01/21/16	NOPDR #3 RESPONSE	

ISSUE

09/23/15 NOPDR #1 RESPONSE

OWNER
1567 California, LLC
1567 California St
San Francisco, CA 94109

PROJECT NAME
1567 California
San Francisco, CA 94109

BLOCK/LOT
0645 / 14 / 14A / 15

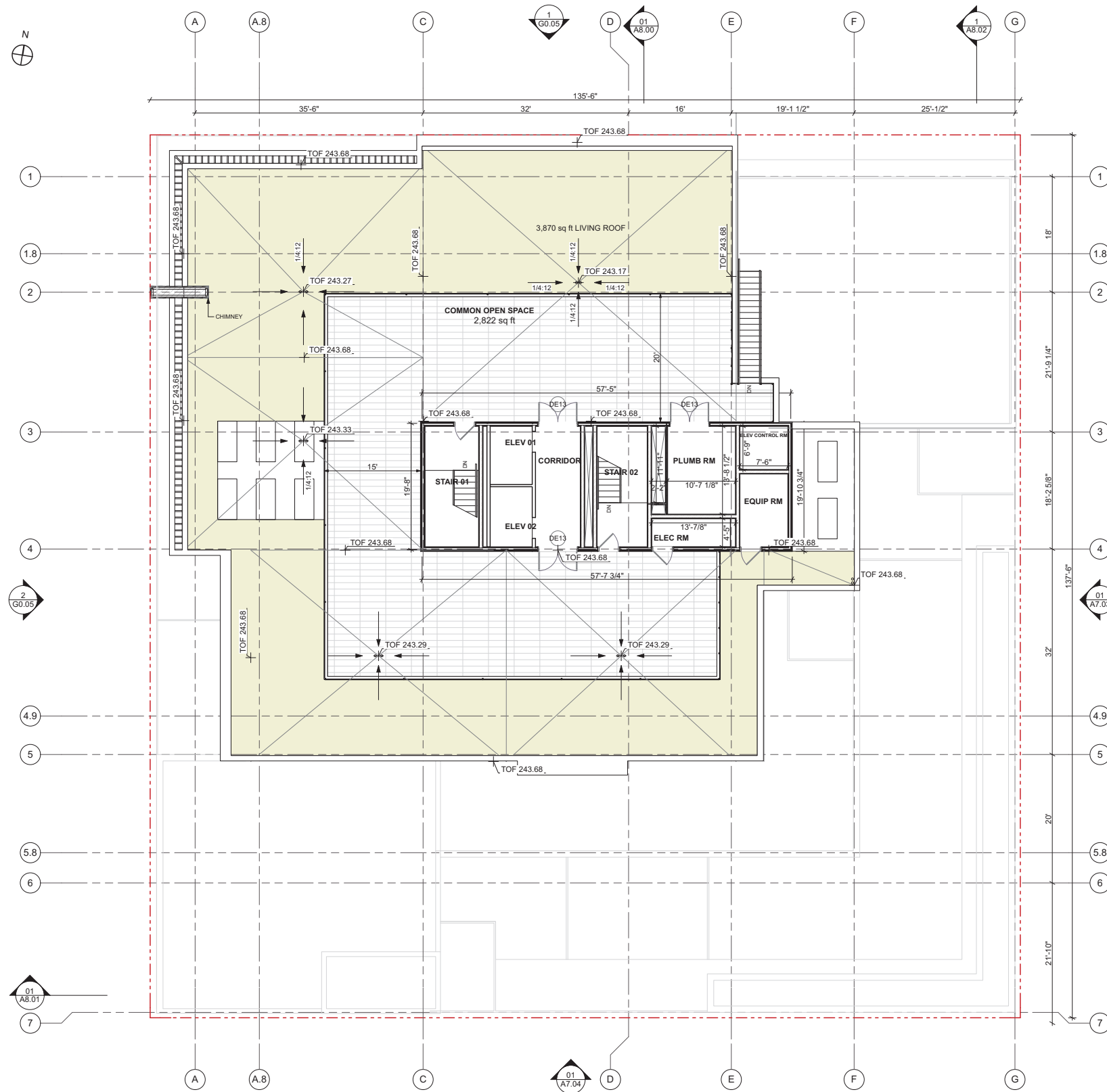
SCALE
AS NOTED
DRAWN BY
RG, MA, TG, JB
CHECKED BY
RG
PROJECT NO.
201504
DATE
1/21/16

STORY 7

DRAWING NO. 16 of 47

A2.08

TOTAL GROSS EXT.
1,165 sq ft



Roof Level +80'-0"
SCALE: 1/8" = 1'-0"

GENERAL NOTES

SHEET NOTES

ROOF PENTHOUSE CALCULATIONS
1,182.84 sq ft + 10,251.30 sq ft = 11.5%

KEY

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REVISIONS

NO.	DATE	ISSUE
01	09/23/15	NOPDR #1 RESPONSE
02	01/14/16	NOPDR #2 RESPONSE
03	01/21/16	NOPDR #3 RESPONSE

ISSUE

09/23/15 NOPDR #1 RESPONSE

OWNER

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1567 California St
San Francisco, CA 94109

PROJECT NAME

1567 California
San Francisco, CA 94109

BLOCK/LOT

0645 / 14 / 14A / 15

SCALE

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CHECKED BY

RG

PROJECT NO.

201504

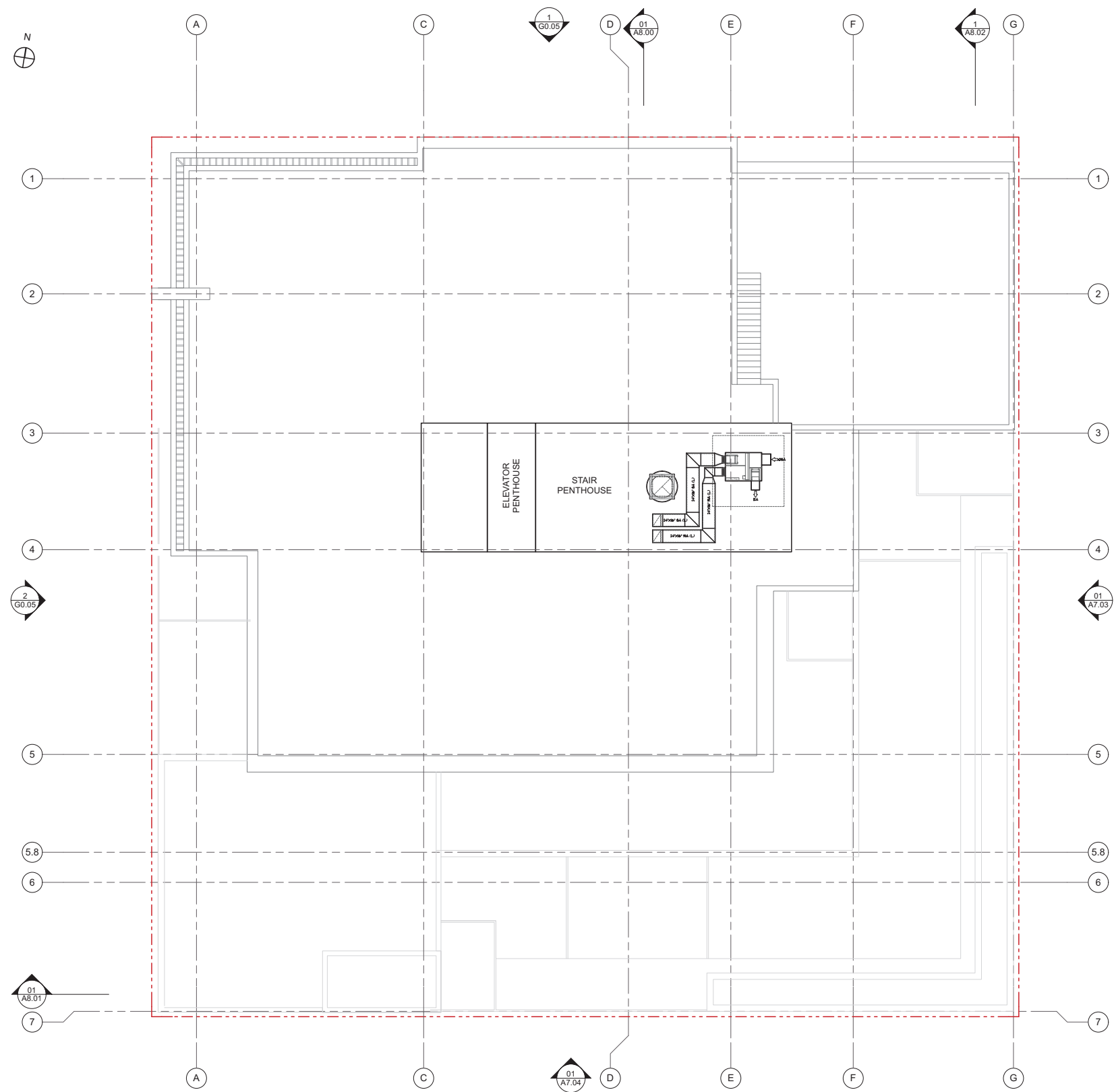
DATE

1/21/16

ROOF

DRAWING NO. 17 of 47

A2.09



GENERAL NOTES

SHEET NOTES

KEY

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REVISIONS
NO. DATE ISSUE

1	09/23/15	NOPDR #1 RESPONSE
2	01/14/16	NOPDR #2 RESPONSE
3	01/21/16	NOPDR #3 RESPONSE

ISSUE
09/23/15 NOPDR #1 RESPONSE

OWNER
1567 California, LLC
1567 California St
San Francisco, CA 94109

PROJECT NAME
1567 California
San Francisco, CA 94109

BLOCK/LOT
0645 / 14 / 14A / 15

SCALE
AS NOTED
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RG, MA, TG, JB
CHECKED BY
RG

PROJECT NO.
201504
DATE
1/21/16

PENTHOUSE ROOF

DRAWING NO. 18 of 47

A2.10

Bulkhead Roof Level
SCALE: 1/8" = 1'-0"



GENERAL NOTES

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SHEET NOTES

ID	DESCRIPTION
01	POWDER COATED, INTEGRAL COLOR ALUM. WALL PANEL SYSTEM
02	WOOD VENEER COMPOSITE PANEL SYSTEM
03	ALUM. STOREFRONT WINDOWS
04	GLASS GUARDRAIL
05	PAINTED STEEL & GLASS AWNING
06	POWDER COATED ALUM. LOUVER TO MATCH WINDOWS
07	POWDER COATED ALUM. FRAME WINDOWS, TYP.
08	BACKLIT TRANSLUCENT GLASS PANELS & S.STL STAND OFFS
09	PLATE GLASS ENTRY
10	VENTILATED POWDER-COATED ALUM. FRAME OVERHEAD GARAGE DOOR
11	POWDER COATED ALUM. ENTRY AWNING & MARQUEE w/WOOD SLAT INFILL
12	MTL. GUARDRAIL, PTD
13	CEMENT PLASTER, PTD
14	FORMED CONCRETE
15	LIVING WALL

REVISIONS

NO.	DATE	ISSUE
△ 09/23/15	NOPDR #1 RESPONSE	
01/14/16	NOPDR #2 RESPONSE	
01/21/16	NOPDR #3 RESPONSE	

ISSUE
 09/23/15 NOPDR #1 RESPONSE

OWNER
 1567 California, LLC
 1567 California St
 San Francisco, CA 94109

PROJECT NAME
1567 California
 San Francisco, CA 94109

BLOCK/LOT
 0645 / 14 / 14A / 15

SCALE
 AS NOTED
 DRAWN BY
 RG, MA, TG, JB
 CHECKED BY
 RG
 PROJECT NO.
 201504
 DATE
 1/21/16

ELEVATIONS

DRAWING NO. 40 of 47

A7.00





GENERAL NOTES

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14	FORMED CONCRETE
15	LIVING WALL

REVISIONS

NO.	DATE	ISSUE
01	09/23/15	NOPDR #1 RESPONSE
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03	01/21/16	NOPDR #3 RESPONSE

ISSUE

09/23/15 NOPDR #1 RESPONSE

OWNER

1567 California, LLC
1567 California St
San Francisco, CA 94109

PROJECT NAME

1567 California
San Francisco, CA 94109

BLOCK/LOT

0645 / 14 / 14A / 15

SCALE

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RG

PROJECT NO.

201504

DATE

1/21/16

**ELEVATIONS W/
LIVING WALL**

DRAWING NO. 41 of 47

A7.01

CALIFORNIA STREET ELEVATION
SCALE: 1/8" = 1'-0"

01



GENERAL NOTES

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SHEET NOTES

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13	CEMENT PLASTER, PTD
14	FORMED CONCRETE
15	LIVING WALL

REVISIONS

NO.	DATE	ISSUE
△ 09/23/15	NOPDR #1 RESPONSE	
01/14/16	NOPDR #2 RESPONSE	
01/21/16	NOPDR #3 RESPONSE	

ISSUE
 09/23/15 NOPDR #1 RESPONSE

OWNER
 1567 California, LLC
 1567 California St
 San Francisco, CA 94109

PROJECT NAME
1567 California
 San Francisco, CA 94109

BLOCK/LOT
 0645 / 14 / 14A / 15

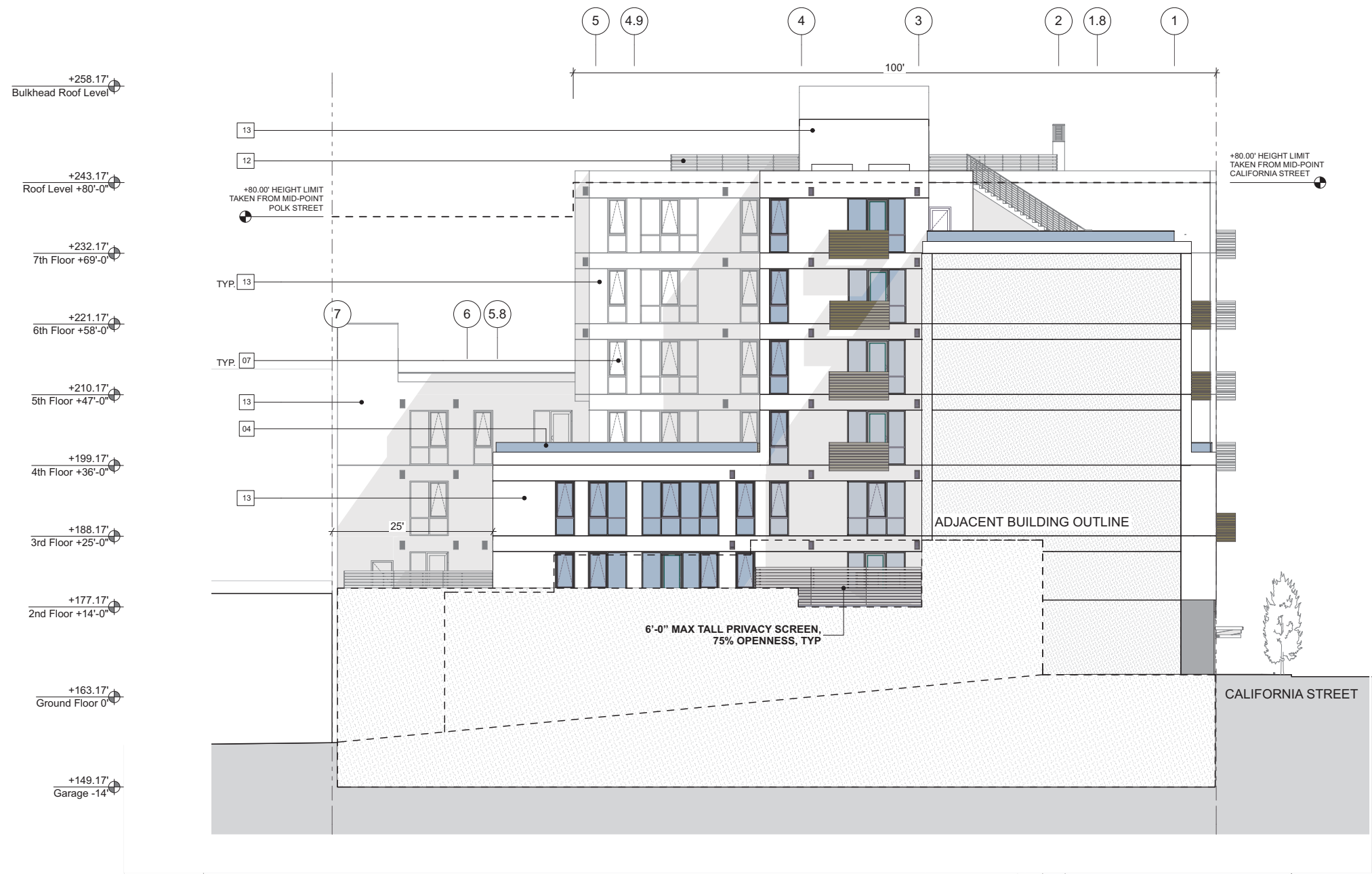
SCALE
 AS NOTED
DRAWN BY
 RG, MA, TG, JB
CHECKED BY
 RG
PROJECT NO.
 201504
DATE
 1/21/16

ELEVATIONS

DRAWING NO. 42 of 47

A7.02

POLK STREET ELEVATION
 SCALE: 1/8" = 1'-0" 01



GENERAL NOTES

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SHEET NOTES

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12	MTL. GUARDRAIL, PTD
13	CEMENT PLASTER, PTD
14	FORMED CONCRETE
15	LIVING WALL

REVISIONS

NO.	DATE	ISSUE
△	09/23/15	NOPDR #1 RESPONSE
	01/14/16	NOPDR #2 RESPONSE
	01/21/16	NOPDR #3 RESPONSE

ISSUE

09/23/15 NOPDR #1 RESPONSE

OWNER

1567 California, LLC
 1567 California St
 San Francisco, CA 94109

PROJECT NAME

1567 California
 San Francisco, CA 94109

BLOCK/LOT

0645 / 14 / 14A / 15

SCALE

AS NOTED

DRAWN BY

RG, MA, TG, JB

CHECKED BY

RG

PROJECT NO.

201504

DATE

1/21/16

ELEVATIONS

DRAWING NO. 43 of 47

A7.03

EAST ELEVATION
 SCALE: 1/8" = 1'-0"
 01



GENERAL NOTES

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SHEET NOTES

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15	LIVING WALL

REVISIONS

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△ 09/23/15		NOPDR #1 RESPONSE
01/14/16		NOPDR #2 RESPONSE
01/21/16		NOPDR #3 RESPONSE

ISSUE

09/23/15 NOPDR #1 RESPONSE

OWNER

1567 California, LLC
 1567 California St
 San Francisco, CA 94109

PROJECT NAME

1567 California
 San Francisco, CA 94109

BLOCK/LOT

0645 / 14 / 14A / 15

SCALE

AS NOTED

DRAWN BY

RG, MA, TG, JB

CHECKED BY

RG

PROJECT NO.

201504

DATE

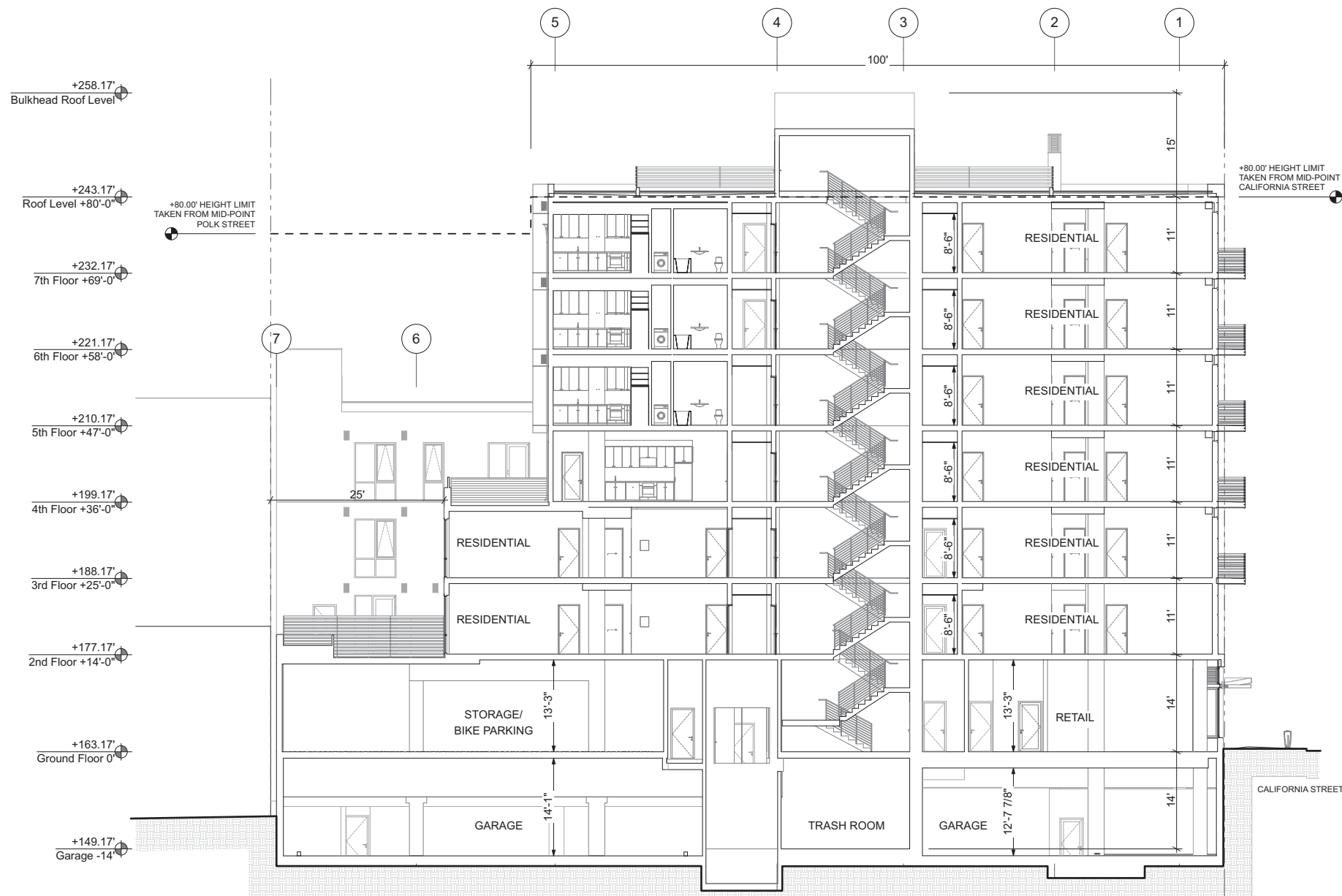
1/21/16

ELEVATIONS

DRAWING NO. 44 of 47

A7.04





GENERAL NOTES

SHEET NOTES

KEY

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REVISIONS	NO.	DATE	ISSUE
▲	09/23/15	NOPDR #1 RESPONSE	
	01/14/16	NOPDR #2 RESPONSE	
	01/21/16	NOPDR #3 RESPONSE	

ISSUE
09/23/15 NOPDR #1 RESPONSE

OWNER
1567 California, LLC
1567 California St
San Francisco, CA 94109

PROJECT NAME
1567 California
San Francisco, CA 94109

BLOCK/LOT
0645 / 14 / 14A / 15

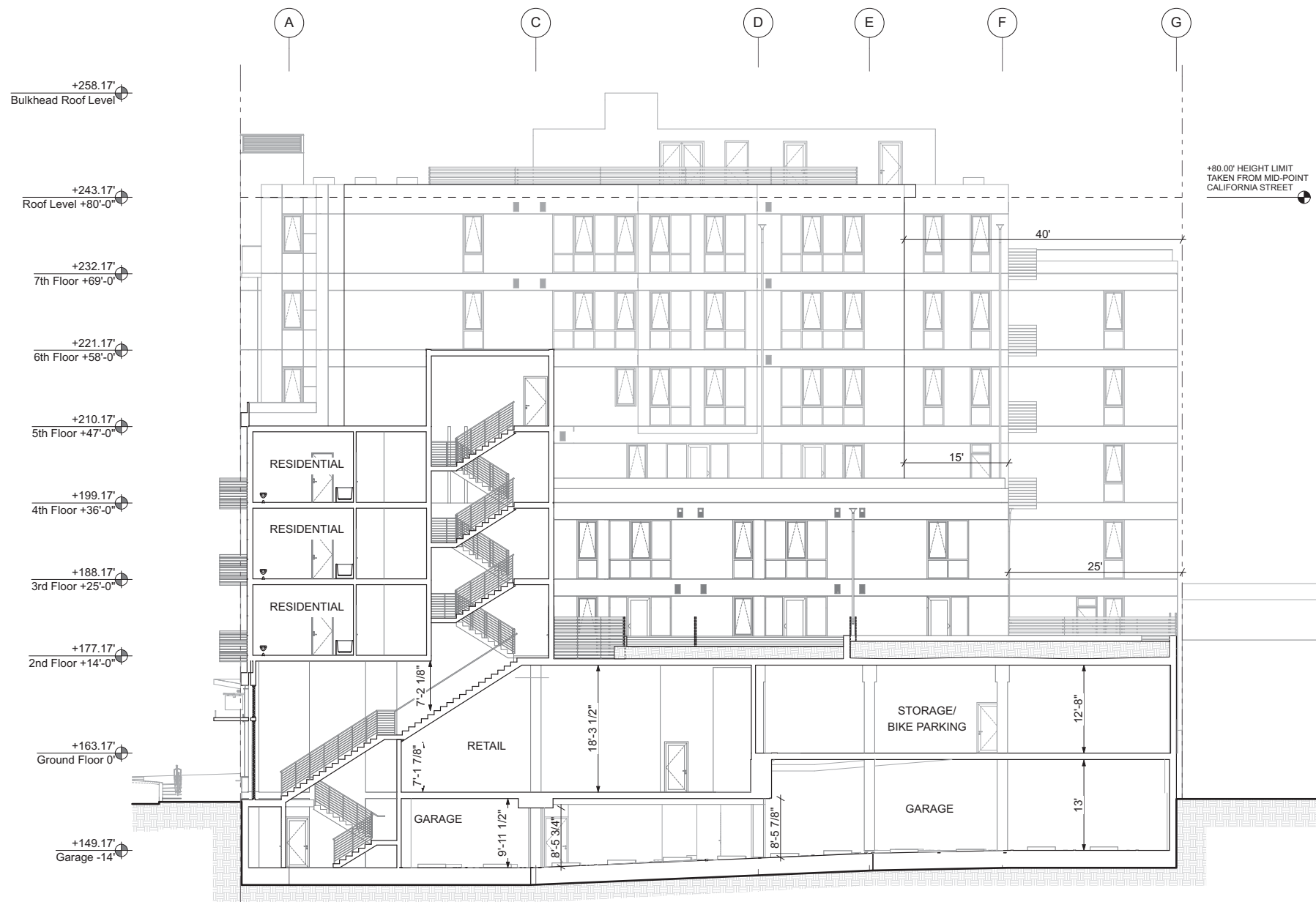
SCALE
AS NOTED
DRAWN BY
RG, MA, TG, JB
CHECKED BY
RG
PROJECT NO.
201504
DATE
1/21/16

BUILDING SECTIONS

DRAWING NO. 45 of 47

A8.00

SECTION A 01
SCALE: 1/8" = 1'-0"



+80.00' HEIGHT LIMIT
TAKEN FROM MID-POINT
CALIFORNIA STREET

GENERAL NOTES

SHEET NOTES

KEY



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SAN FRANCISCO, CA 94107
415.699.3640
mail@rg-architecture.com



REVISIONS

NO.	DATE	ISSUE
△	09/23/15	NOPDR #1 RESPONSE
	01/14/16	NOPDR #2 RESPONSE
	01/21/16	NOPDR #3 RESPONSE

ISSUE

09/23/15 NOPDR #1 RESPONSE

OWNER

1567 California, LLC
1567 California St
San Francisco, CA 94109

PROJECT NAME

1567 California
San Francisco, CA 94109

BLOCK/LOT

0645 / 14 / 14A / 15

SCALE

AS NOTED

DRAWN BY

RG, MA, TG, JB

CHECKED BY

RG

PROJECT NO.

201504

DATE

1/21/16

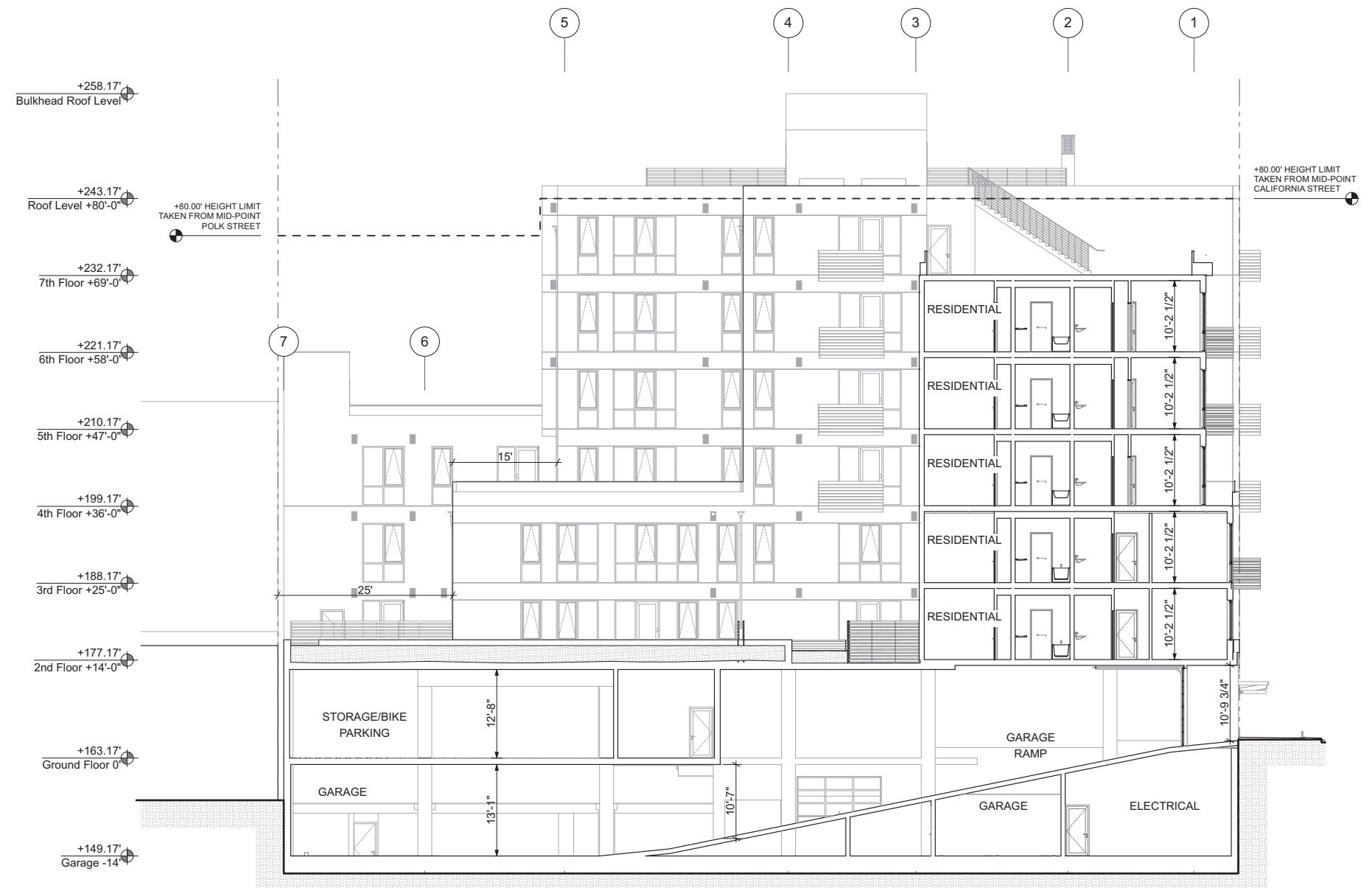
BUILDING SECTIONS

DRAWING NO. 46 of 47

A8.01



01



GENERAL NOTES

SHEET NOTES

KEY

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REVISIONS	NO.	DATE	ISSUE
▲	09/23/15	NOPDR #1 RESPONSE	
	01/14/16	NOPDR #2 RESPONSE	
	01/21/16	NOPDR #3 RESPONSE	

ISSUE
 09/23/15 NOPDR #1 RESPONSE

OWNER
 1567 California, LLC
 1567 California St
 San Francisco, CA 94109

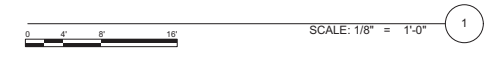
PROJECT NAME
1567 California
 San Francisco, CA 94109

BLOCK/LOT
 0645 / 14 / 14A / 15

SCALE
 AS NOTED
 DRAWN BY
 RG, MA, TG, JB
 CHECKED BY
 RG
 PROJECT NO.
 201504
 DATE
 1/21/16

BUILDING SECTIONS

DRAWING NO. 47 of 47
A8.02





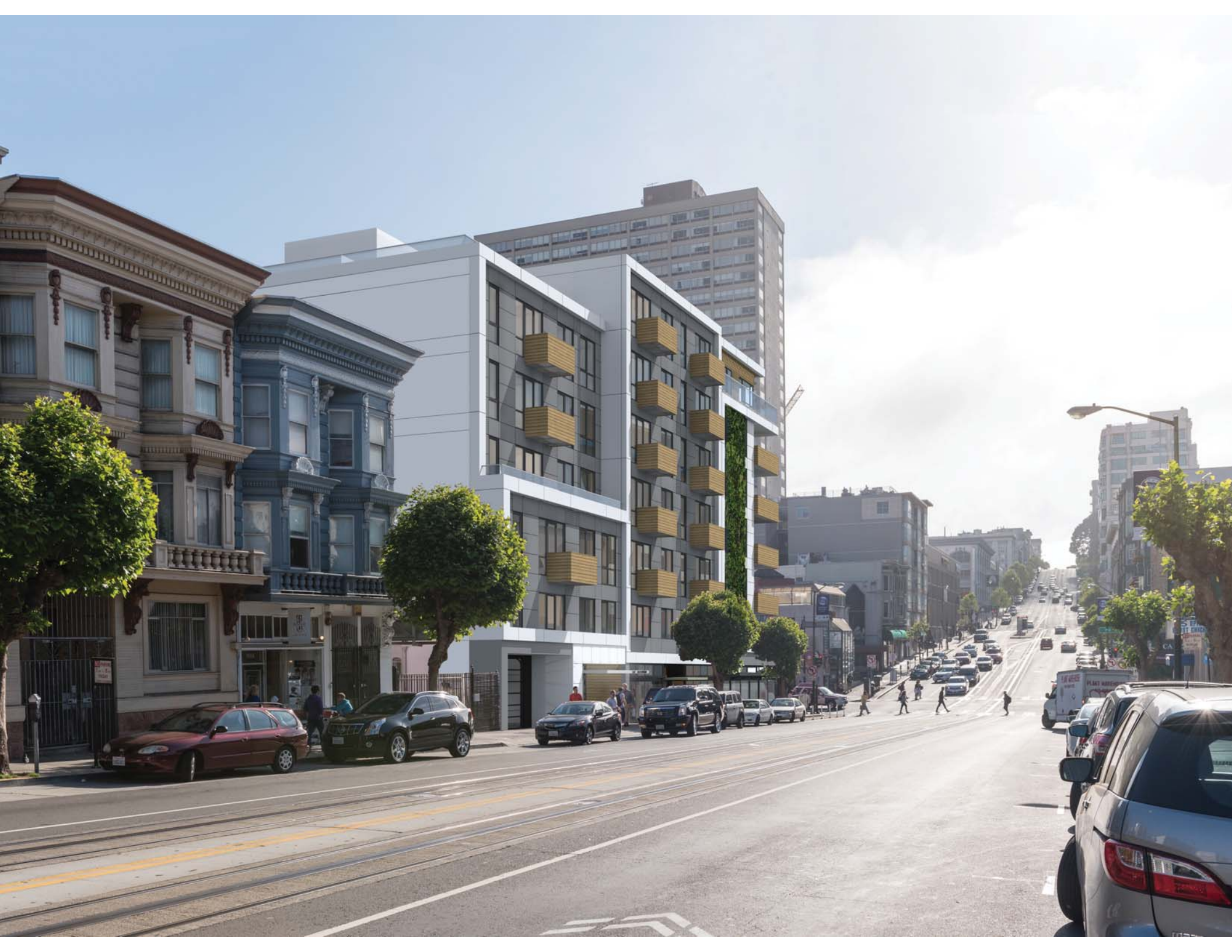
Golden Veggie Market
Fresh Produce
Organic Produce
Quality Groceries
Quality Meat
Buy Fresh
Buy Local
1475 POLK ST.
(415) 771-9099

U.S.

SAN FRANCISCO MUNICIPAL RAILWAY

Polk

CALIFORNIA







1567 CALIFORNIAN

FOOD

Cafe Street Market



SHOP SMALL
EVERY DAY IN THE BAY
@SHOPSMALL

CHASE

JACOBS
415-474-2500

NEW VILLAGE CAFE

DRS. HURA & HURA, OPTOMETRISTS

DARBA
Pakistani and Indian
FOOD TO GO
LUNCH - DINNER





POLK ST

SPECIAL BEER
SANGRIA
COFFEE
DESSERT

Affidavit for Compliance with the Inclusionary Affordable Housing Program: Planning Code Section 415

October 22, 2015

Date

I, Cyrus Sanandaji, do hereby declare as follows:

a. The subject property is located at (address and block/lot):

1567 California Street / 1436 Polk Street / 1498 Polk Street

Address

0645 / 14, 14A & 15

Block / Lot

b. The proposed project at the above address is subject to the Inclusionary Affordable Housing Program, Planning Code Section 415 et seq.

The Planning Case Number and/or Building Permit Number is:

2014.0284CV

Planning Case Number

N/A

Building Permit Number

This project requires the following approval:

- Planning Commission approval (e.g. Conditional Use Authorization, Large Project Authorization)
- This project is principally permitted.

The Current Planner assigned to my project within the Planning Department is:

Claudine Asbagh

Planner Name

Is this project within the Eastern Neighborhoods Plan Area?

- Yes (if yes, please indicate Tier) _____
- No

This project is exempt from the Inclusionary Affordable Housing Program because:

- This project uses California Debt Limit Allocation Committee (CDLAC) funding.
- This project is 100% affordable.

c. This project will comply with the Inclusionary Affordable Housing Program by:

Payment of the Affordable Housing Fee prior to the first site or building permit issuance (Planning Code Section 415.5).

Combination of
both

On-site or Off-site Affordable Housing Alternative (Planning Code Sections 415.6 and 416.7).

d. If the project will comply with the Inclusionary Affordable Housing Program through an **On-site** or **Off-site Affordable Housing Alternative**, please fill out the following regarding how the project is eligible for an alternative and the accompanying unit mix tables on page 4.

- Ownership.** All affordable housing units will be sold as ownership units and will remain as ownership units for the life of the project.
- Rental.** Exemption from Costa Hawkins Rental Housing Act.² The Project Sponsor has demonstrated to the Department that the affordable units are not subject to the Costa Hawkins Rental Housing Act, under the exception provided in Civil Code Sections 1954.50 though one of the following:
 - Direct financial contribution from a public entity.
 - Development or density bonus or other public form of assistance.
 - Development Agreement with the City. The Project Sponsor has entered into or has applied to enter into a Development Agreement with the City and County of San Francisco pursuant to Chapter 56 of the San Francisco Administrative Code and, as part of that Agreement, is receiving a direct financial contribution, development or density bonus, or other form of public assistance.

e. The Project Sponsor acknowledges that failure to sell the affordable units as ownership units or to eliminate the on-site or off-site affordable ownership-only units at any time will require the Project Sponsor to:

- (1) Inform the Planning Department and the Mayor’s Office of Housing and, if applicable, fill out a new affidavit;
- (2) Record a new Notice of Special Restrictions; and
- (3) Pay the Affordable Housing Fee plus applicable interest (using the fee schedule in place at the time that the units are converted from ownership to rental units) and any applicable penalties by law.

f. The Project Sponsor must pay the Affordable Housing Fee in full sum to the Development Fee Collection Unit at the Department of Building Inspection for use by the Mayor’s Office of Housing prior to the issuance of the first construction document, with an option for the Project Sponsor to defer a portion of the payment to prior to issuance of the first certificate of occupancy upon agreeing to pay a deferral surcharge that would be deposited into the Citywide Affordable Housing Fund in accordance with Section 107A.13.3 of the San Francisco Building Code.

g. I am a duly authorized officer or owner of the subject property.

I declare under penalty of perjury under the laws of the State of California that the foregoing is true and correct. Executed on this day in:

San Francisco

 Location

10/22/2015

 Date



 Signature

Cyrus Sanandaji

 Name (Print), Title

cc: Mayor’s Office of Housing
 Planning Department Case Docket
 Historic File, if applicable
 Assessor’s Office, if applicable

(760) 214-8753

 Contact Phone Number

2 California Civil Code Section 1954.50 and following.

Unit Mix Tables

NUMBER OF ALL UNITS IN PRINCIPAL PROJECT:					
Total Number of Units	SRO	Studios	One-Bedroom Units	Two-Bedroom Units	Three-Bedroom Units
63	0	3	34	16	10

If you selected an On-site or Off-Site Alternative, please fill out the applicable section below:

- On-site Affordable Housing Alternative (Charter Section 16.110 (g) and Planning Code Section 415.6): calculated at 12% of the unit total.

NUMBER OF AFFORDABLE UNITS TO BE LOCATED ON-SITE					
Total Affordable Units	SRO	Studios	One-Bedroom Units	Two-Bedroom Units	Three-Bedroom Units

- Off-site Affordable Housing Alternative (Planning Code Section 415.7): calculated at 20% of the unit total.

NUMBER OF AFFORDABLE UNITS TO BE LOCATED OFF-SITE					
Total Affordable Units	SRO	Studios	One-Bedroom Units	Two-Bedroom Units	Three-Bedroom Units

Area of Dwellings in Principal Project (in sq. feet)	Off-Site Project Address				
Area of Dwellings in Off-Site Project (in sq. feet)					
Off-Site Block/Lot(s)	Motion No. (if applicable)			Number of Market-Rate Units in the Off-site Project	

- Combination of payment of a fee, on-site affordable units, or off-site affordable units with the following distribution:

Indicate what percent of each option would be implemented (from 0% to 99%) and the number of on-site and/or off-site below market rate units for rent and/or for sale.

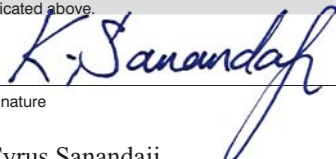
1. Fee 74 % of affordable housing requirement.
 2. On-Site 26 % of affordable housing requirement.

NUMBER OF AFFORDABLE UNITS TO BE LOCATED ON-SITE					
Total Affordable Units	SRO	Studios	One-Bedroom Units	Two-Bedroom Units	Three-Bedroom Units
2	0	0	2	0	0

3. Off-Site _____ % of affordable housing requirement.

NUMBER OF AFFORDABLE UNITS TO BE LOCATED OFF-SITE					
Total Affordable Units	SRO	Studios	One-Bedroom Units	Two-Bedroom Units	Three-Bedroom Units

Area of Dwellings in Principal Project (in sq. feet)	Off-Site Project Address				
Area of Dwellings in Off-Site Project (in sq. feet)					
Off-Site Block/Lot(s)	Motion No. (if applicable)			Number of Market-Rate Units in the Off-site Project	

CONTACT INFORMATION AND DECLARATION OF SPONSOR OF PRINCIPAL PROJECT	CONTACT INFORMATION AND DECLARATION OF SPONSOR OF OFF-SITE PROJECT (IF DIFFERENT)
Company Name Presidio Bay Capital, LLC	Company Name
Print Name of Contact Person Cyrus Sanandaji	Print Name of Contact Person
Address 1160 Battery Street, Suite 250	Address
City, State, Zip San Francisco, CA 94111	City, State, Zip
Phone, Fax (760) 214-8753	Phone, Fax
Email cyrus@presidiobay.com	Email
I hereby declare that the information herein is accurate to the best of my knowledge and that I intend to satisfy the requirements of Planning Code Section 415 as indicated above.	I hereby declare that the information herein is accurate to the best of my knowledge and that I intend to satisfy the requirements of Planning Code Section 415 as indicated above.
	
Signature	Signature
<u>Cyrus Sanandaji</u>	
Name (Print), Title	Name (Print), Title



**SAN FRANCISCO
PLANNING
DEPARTMENT**

AFFIDAVIT FOR FIRST SOURCE HIRING PROGRAM

Administrative Code

Chapter 83

1650 Mission Street, Suite 400 • San Francisco CA 94103-2479 • 415.558.6378 • <http://www.sfplanning.org>

Section 1: Project Information

PROJECT ADDRESS		BLOCK/LOT(S)	
1567 California Street		0645/014, 014A, 015	
BUILDING PERMIT APPLICATION NO.	CASE NO. (IF APPLICABLE)	MOTION NO. (IF APPLICABLE)	
	2014.0284X		
PROJECT SPONSOR	MAIN CONTACT	PHONE	
1567 California, LLC	Cyrus Sanandaji	(760) 214-8753	
ADDRESS			
185 Berry Street, Suite 1200			
CITY, STATE, ZIP		EMAIL	
San Francisco, CA 94107		cyrus@presidiobay.com	
ESTIMATED RESIDENTIAL UNITS	ESTIMATED SQ FT COMMERCIAL SPACE	ESTIMATED HEIGHT/FLOORS	ESTIMATED CONSTRUCTION COST
63 units	8,001 sq. ft.	80 ft./7 floors	\$29 million
ANTICIPATED START DATE			

Section 2: First Source Hiring Program Verification

CHECK ALL BOXES APPLICABLE TO THIS PROJECT	
<input type="checkbox"/>	Project is wholly Residential
<input type="checkbox"/>	Project is wholly Commercial
<input checked="" type="checkbox"/>	Project is Mixed Use
<input checked="" type="checkbox"/>	A: The project consists of ten (10) or more residential units;
<input type="checkbox"/>	B: The project consists of 25,000 square feet or more gross commercial floor area.
<input type="checkbox"/>	C: Neither 1A nor 1B apply.
NOTES:	
<ul style="list-style-type: none"> If you checked C, this project is NOT subject to the First Source Hiring Program. Sign Section 4: Declaration of Sponsor of Project and submit to the Planning Department. If you checked A or B, your project IS subject to the First Source Hiring Program. Please complete the reverse of this document, sign, and submit to the Planning Department prior to any Planning Commission hearing. If principally permitted, Planning Department approval of the Site Permit is required for all projects subject to Administrative Code Chapter 83. For questions, please contact OEWD's CityBuild program at CityBuild@sfgov.org or (415) 701-4848. For more information about the First Source Hiring Program visit www.workforcedevelopmentsf.org If the project is subject to the First Source Hiring Program, you are required to execute a Memorandum of Understanding (MOU) with OEWD's CityBuild program prior to receiving construction permits from Department of Building Inspection. 	

Continued...

Section 3: First Source Hiring Program – Workforce Projection

Per Section 83.11 of Administrative Code Chapter 83, it is the developer's responsibility to complete the following information to the best of their knowledge.

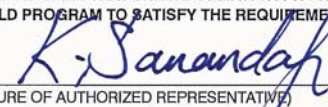
Provide the estimated number of employees from each construction trade to be used on the project, indicating how many are entry and/or apprentice level as well as the anticipated wage for these positions.

Check the anticipated trade(s) and provide accompanying information (Select all that apply):

TRADE/CRAFT	ANTICIPATED JOURNEYMAN WAGE	# APPRENTICE POSITIONS	# TOTAL POSITIONS	TRADE/CRAFT	ANTICIPATED JOURNEYMAN WAGE	# APPRENTICE POSITIONS	# TOTAL POSITIONS
Abatement Laborer		0	0	Laborer	\$ 65.00	5	20
Boilermaker		0	0	Operating Engineer	\$ 125.00	1	5
Bricklayer		0	0	Painter	\$ 90.00	3	12
Carpenter	\$ 90.00	4	20	Pile Driver		0	0
Cement Mason	\$ 68.00	5	15	Plasterer	\$ 84.00	2	10
Drywall/Latherer	\$ 85.00	5	25	Plumber and Pipefitter	\$ 125.00	2	10
Electrician	\$ 130.00	5	20	Roofer/Water proofer	\$ 80.00	3	12
Elevator Constructor	\$ 150.00	2	8	Sheet Metal Worker	\$ 110.00	3	12
Floor Coverer	\$ 105.00	2	10	Sprinkler Fitter	\$ 104.00	2	10
Glazier	\$ 127.00	4	20	Taper	\$ 85.00	3	15
Heat & Frost Insulator	\$ 90.00	1	5	Tile Layer/ Finisher	\$ 84.00	2	8
Ironworker	\$ 110.00	8	25	Other:			0
TOTAL:			198	TOTAL:			114

- Will the anticipated employee compensation by trade be consistent with area Prevailing Wage? YES NO
- Will the awarded contractor(s) participate in an apprenticeship program approved by the State of California's Department of Industrial Relations? YES NO
- Will hiring and retention goals for apprentices be established? YES NO
- What is the estimated number of local residents to be hired? _____

Section 4: Declaration of Sponsor of Principal Project

PRINT NAME AND TITLE OF AUTHORIZED REPRESENTATIVE	EMAIL	PHONE NUMBER
Cyrus Sanandaji, Authorized Representative	cyrus@presidiobay.com	(760) 214-8753
I HEREBY DECLARE THAT THE INFORMATION PROVIDED HEREIN IS ACCURATE TO THE BEST OF MY KNOWLEDGE AND THAT I COORDINATED WITH OEWD'S CITYBUILD PROGRAM TO SATISFY THE REQUIREMENTS OF ADMINISTRATIVE CODE CHAPTER 83.		
		12/07/2015
(SIGNATURE OF AUTHORIZED REPRESENTATIVE)		(DATE)

FOR PLANNING DEPARTMENT STAFF ONLY: PLEASE EMAIL AN ELECTRONIC COPY OF THE COMPLETED AFFIDAVIT FOR FIRST SOURCE HIRING PROGRAM TO OEWD'S CITYBUILD PROGRAM AT CITYBUILD@SFGOV.ORG

Cc: Office of Economic and Workforce Development, CityBuild
 Address: 1 South Van Ness 5th Floor San Francisco, CA 94103 Phone: 415-701-4848
 Website: www.workforcedevelopmentsf.org Email: CityBuild@sfgov.org

October 21, 2014

Kevin Guy
San Francisco Planning Department
City and County of San Francisco
1650 Mission Street, Suite 400
San Francisco, CA 94103

Re: Construction on 1567 California St., San Francisco

As a follow-up to our conversation on October 15, 2014, I currently own the building at 1561 – 1565 California St., which is next to the proposed new construction. My commercial/retail property is 1 story and set back from the driveway by about 30 feet.

In general I am not opposed to construction at 1567 California St.; however, I have several concerns with the current proposal of a new seven-story mixed-use building with retail shops at ground level and 47 dwelling units on levels 2 through 7. If these concerns are not adequately addressed, I am afraid this development will have a major impact on the value of my existing property.

In particular, I am concerned that:

- without an appropriate setback for the proposed building, my building (set back by 30 feet by city code when built in 1975) will lose visibility from the street, making it more difficult for me as a landlord and any future tenant to fully maximize our return on investment;
- there will not be a suitable transition between the taller building and my lower, one story building and that a building of that height will have a major impact on the amount of sunlight that will reach my property;
- the proposed underground parking will not be sufficient to alleviate the additional parking congestion -- already a major headache in this area; and
- the height of the new development will not be compatible with the individual neighborhood character and the height and scale of the adjacent buildings.

Please take this under consideration when evaluating the approval for that project. Feel free to contact me should you need any additional information.

Thank you,



Ellen Ching
Owner
1561-1565 California St.
San Francisco, CA 94109

Contact info:
Ellen Ching
3707 Brunswick Court
South San Francisco, CA 94080
ecclc@aol.com
Cell phone: (415) 608-6838

LETTER OF SUPPORT FOR NEW DEVELOPMENT AT 1567 CALIFORNIA STREET

Case Number: 2014-0284E (1567 California St. 1436-14498 Polk St)

Merchant Name: STEPHEN CORNELL

Merchant Address: 1563 - POLK.

Lana Russell (CPC)

San Francisco Planning Department

1650 Mission Street, Suite 400

San Francisco, CA 94103

I am a small business owner located approximately _____ blocks away from the proposed mixed-use development to be constructed at 1567 California Street. After meeting with representatives of the developer, Presidio Bay Ventures, I have seen the proposed design and understand that the project is to contain approximately 47-50 residential units combined with a ground floor space dedicated towards retail and dining purposes.

I would like to express my unconditional support for this development. As a business owner, I feel that this project will prove extremely beneficial for the neighborhood by contributing the following:

1. Additional 100-150 residents of varying demographics who will contribute to the diversity and makeup of the Polk Street.
2. Increased pedestrian traffic from both the residents of the project and visitors from other neighborhoods who are likely to provide increased business.
3. Reduction of loitering and vagrancy that will improve the safety of the prominent Polk and California intersection and encourage more visitors to our businesses.
4. Promotion of small and local businesses via the retail ground floor and the introduction of new dining options.
5. Retail 'arcade' that will encourage pedestrians to congregate around indoor/outdoor spaces and create a positive atmosphere.
6. 10 additional bicycle parking spaces on Polk Street and 1 car share space available for public use.
7. Large living wall facing California Street that will serve as the "gateway" between Middle and Lower Polk Street. This will give tourists on the cable car a reason to slow down and stop to patronize Polk Street businesses. Currently the cable car quickly passes through without slowing down.

I look forward to welcoming the residents and merchants of 1567 California to the community.

Sincerely,

Phone Number:

Email:

CC: MOED Kevin Guy, SF Planning
SF Planning Director Planning Commission
Supervisor Christensen

LETTER OF SUPPORT FOR NEW DEVELOPMENT AT 1567 CALIFORNIA STREET

Case Number: 2014-0284E (1567 California St. 1436-14498 Polk St)

Merchant Name: FRANK'S SHOE REPAIR

Merchant Address: 1619 POLK STREET SAN FRANCISCO CA 94109

Lana Russell (CPC)

San Francisco Planning Department

1650 Mission Street, Suite 400

San Francisco, CA 94103

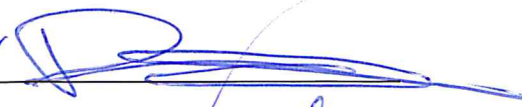
I am a small business owner located approximately _____ blocks away from the proposed mixed-use development to be constructed at 1567 California Street. After meeting with representatives of the developer, Presidio Bay Ventures, I have seen the proposed design and understand that the project is to contain approximately 47-50 residential units combined with a ground floor space dedicated towards retail and dining purposes.

I would like to express my unconditional support for this development. As a business owner, I feel that this project will prove extremely beneficial for the neighborhood by contributing the following:

1. Additional 100-150 residents of varying demographics who will contribute to the diversity and makeup of the Polk Street.
2. Increased pedestrian traffic from both the residents of the project and visitors from other neighborhoods who are likely to provide increased business.
3. Reduction of loitering and vagrancy that will improve the safety of the prominent Polk and California intersection and encourage more visitors to our businesses.
4. Promotion of small and local businesses via the retail ground floor and the introduction of new dining options.
5. Retail 'arcade' that will encourage pedestrians to congregate around indoor/outdoor spaces and create a positive atmosphere.
6. 10 additional bicycle parking spaces on Polk Street and 1 car share space available for public use.
7. Large living wall facing California Street that will serve as the "gateway" between Middle and Lower Polk Street. This will give tourists on the cable car a reason to slow down and stop to patronize Polk Street businesses. Currently the cable car quickly passes through without slowing down.

I look forward to welcoming the residents and merchants of 1567 California to the community.

Sincerely,

415 775 1694 

Phone Number:

Email: FRANK.SOBREPAIN@comcast.net

CC: MOED

Kevin Guy, SF Planning

SF Planning Director

Planning Commission

Supervisor Christensen

LETTER OF SUPPORT FOR NEW DEVELOPMENT AT 1567 CALIFORNIA STREET

Case Number: 2014-0284E (1567 California St. 1436-14498 Polk St)

Merchant Name: Angalee Thai massage

Merchant Address: 1617 Polk St

Lana Russell (CPC)

San Francisco Planning Department

1650 Mission Street, Suite 400

San Francisco, CA 94103

I am a small business owner located approximately _____ blocks away from the proposed mixed-use development to be constructed at 1567 California Street. After meeting with representatives of the developer, Presidio Bay Ventures, I have seen the proposed design and understand that the project is to contain approximately 47-50 residential units combined with a ground floor space dedicated towards retail and dining purposes.

I would like to express my unconditional support for this development. As a business owner, I feel that this project will prove extremely beneficial for the neighborhood by contributing the following:

1. Additional 100-150 residents of varying demographics who will contribute to the diversity and makeup of the Polk Street.
2. Increased pedestrian traffic from both the residents of the project and visitors from other neighborhoods who are likely to provide increased business.
3. Reduction of loitering and vagrancy that will improve the safety of the prominent Polk and California intersection and encourage more visitors to our businesses.
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7. Large living wall facing California Street that will serve as the "gateway" between Middle and Lower Polk Street. This will give tourists on the cable car a reason to slow down and stop to patronize Polk Street businesses. Currently the cable car quickly passes through without slowing down.

I look forward to welcoming the residents and merchants of 1567 California to the community.

Sincerely,

Phone Number:
Email:

Angalee
angalee.thai@att.net
415-351-1900

CC: MOED

Kevin Guy, SF Planning

SF Planning Director

Planning Commission

Supervisor Christensen

LETTER OF SUPPORT FOR NEW DEVELOPMENT AT 1567 CALIFORNIA STREET

Case Number: 2014-0284E (1567 California St. 1436-14498 Polk St)

Merchant Name: Polk A Dot Consignment

Merchant Address: 1553 POLK ST. 94109

Lana Russell (CPC)

San Francisco Planning Department

1650 Mission Street, Suite 400

San Francisco, CA 94103

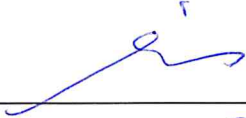
I am a small business owner located approximately _____ blocks away from the proposed mixed-use development to be constructed at 1567 California Street. After meeting with representatives of the developer, Presidio Bay Ventures, I have seen the proposed design and understand that the project is to contain approximately 47-50 residential units combined with a ground floor space dedicated towards retail and dining purposes.

I would like to express my unconditional support for this development. As a business owner, I feel that this project will prove extremely beneficial for the neighborhood by contributing the following:

1. Additional 100-150 residents of varying demographics who will contribute to the diversity and makeup of the Polk Street.
2. Increased pedestrian traffic from both the residents of the project and visitors from other neighborhoods who are likely to provide increased business.
3. Reduction of loitering and vagrancy that will improve the safety of the prominent Polk and California intersection and encourage more visitors to our businesses.
4. Promotion of small and local businesses via the retail ground floor and the introduction of new dining options.
5. Retail 'arcade' that will encourage pedestrians to congregate around indoor/outdoor spaces and create a positive atmosphere.
6. 10 additional bicycle parking spaces on Polk Street and 1 car share space available for public use.
7. Large living wall facing California Street that will serve as the "gateway" between Middle and Lower Polk Street. This will give tourists on the cable car a reason to slow down and stop to patronize Polk Street businesses. Currently the cable car quickly passes through without slowing down.

I look forward to welcoming the residents and merchants of 1567 California to the community.

Sincerely,



Phone Number:

415-872-9080

Email:

polkadotconsignmentsf@yahoo.com

CC: MOED

Kevin Guy, SF Planning

SF Planning Director

Planning Commission

Supervisor Christensen

LETTER OF SUPPORT FOR NEW DEVELOPMENT AT 1567 CALIFORNIA STREET

Case Number: 2014-0284E (1567 California St. 1436-14498 Polk St)

Merchant Name: LusciousWear

Merchant Address: 1410 Polk Street, San Francisco, CA 94109

Lana Russell (CPC)

San Francisco Planning Department

1650 Mission Street, Suite 400

San Francisco, CA 94103

I am a small business owner located approximately same blocks away from the proposed mixed-use development to be constructed at 1567 California Street. After meeting with representatives of the developer, Presidio Bay Ventures, I have seen the proposed design and understand that the project is to contain approximately 47-50 residential units combined with a ground floor space dedicated towards retail and dining purposes.

I would like to express my unconditional support for this development. As a business owner, I feel that this project will prove extremely beneficial for the neighborhood by contributing the following:

1. Additional 100-150 residents of varying demographics who will contribute to the diversity and makeup of the Polk Street.
2. Increased pedestrian traffic from both the residents of the project and visitors from other neighborhoods who are likely to provide increased business.
3. Reduction of loitering and vagrancy that will improve the safety of the prominent Polk and California intersection and encourage more visitors to our businesses.
4. Promotion of small and local businesses via the retail ground floor and the introduction of new dining options.
5. Retail 'arcade' that will encourage pedestrians to congregate around indoor/outdoor spaces and create a positive atmosphere.
6. 10 additional bicycle parking spaces on Polk Street and 1 car share space available for public use.
7. Large living wall facing California Street that will serve as the "gateway" between Middle and Lower Polk Street. This will give tourists on the cable car a reason to slow down and stop to patronize Polk Street businesses. Currently the cable car quickly passes through without slowing down.

I look forward to welcoming the residents and merchants of 1567 California to the community.

Sincerely,

Emma Smith

Phone Number: 415 440 0172

Email: ESMITH@LusciousWear.COM

CC: MOED Kevin Guy, SF Planning

SF Planning Director Planning Commission

Supervisor Christensen

LETTER OF SUPPORT FOR NEW DEVELOPMENT AT 1567 CALIFORNIA STREET

Case Number: 2014-0284E (1567 California St. 1436-14498 Polk St)

Merchant Name: Utsuwa Floral Design

Merchant Address: 1288 Polk, St. SF, CA 94109

Lana Russell (CPC)

San Francisco Planning Department

1650 Mission Street, Suite 400

San Francisco, CA 94103

I am a small business owner located approximately _____ blocks away from the proposed mixed-use development to be constructed at 1567 California Street. After meeting with representatives of the developer, Presidio Bay Ventures, I have seen the proposed design and understand that the project is to contain approximately 47-50 residential units combined with a ground floor space dedicated towards retail and dining purposes.

I would like to express my unconditional support for this development. As a business owner, I feel that this project will prove extremely beneficial for the neighborhood by contributing the following:

1. Additional 100-150 residents of varying demographics who will contribute to the diversity and makeup of the Polk Street.
2. Increased pedestrian traffic from both the residents of the project and visitors from other neighborhoods who are likely to provide increased business.
3. Reduction of loitering and vagrancy that will improve the safety of the prominent Polk and California intersection and encourage more visitors to our businesses.
4. Promotion of small and local businesses via the retail ground floor and the introduction of new dining options.
5. Retail 'arcade' that will encourage pedestrians to congregate around indoor/outdoor spaces and create a positive atmosphere.
6. 10 additional bicycle parking spaces on Polk Street and 1 car share space available for public use.
7. Large living wall facing California Street that will serve as the "gateway" between Middle and Lower Polk Street. This will give tourists on the cable car a reason to slow down and stop to patronize Polk Street businesses. Currently the cable car quickly passes through without slowing down.

I look forward to welcoming the residents and merchants of 1567 California to the community.

Sincerely,



Phone Number: 415 - 447-8476

Email: info@utsuwafd.com

CC: MOED

Kevin Guy, SF Planning

SF Planning Director

Planning Commission

Supervisor Christensen

LETTER OF SUPPORT FOR NEW DEVELOPMENT AT 1567 CALIFORNIA STREET

Case Number: 2014-0284E (1567 California St. 1436-14498 Polk St)

Merchant Name: polk st produce.

Merchant Address: 1334 polk st.

Lana Russell (CPC)

San Francisco Planning Department

1650 Mission Street, Suite 400

San Francisco, CA 94103

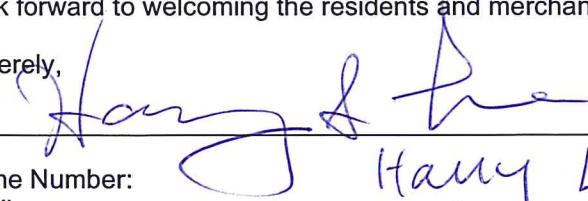
I am a small business owner located approximately _____ blocks away from the proposed mixed-use development to be constructed at 1567 California Street. After meeting with representatives of the developer, Presidio Bay Ventures, I have seen the proposed design and understand that the project is to contain approximately 47-50 residential units combined with a ground floor space dedicated towards retail and dining purposes.

I would like to express my unconditional support for this development. As a business owner, I feel that this project will prove extremely beneficial for the neighborhood by contributing the following:

1. Additional 100-150 residents of varying demographics who will contribute to the diversity and makeup of the Polk Street.
2. Increased pedestrian traffic from both the residents of the project and visitors from other neighborhoods who are likely to provide increased business.
3. Reduction of loitering and vagrancy that will improve the safety of the prominent Polk and California intersection and encourage more visitors to our businesses.
4. Promotion of small and local businesses via the retail ground floor and the introduction of new dining options.
5. Retail 'arcade' that will encourage pedestrians to congregate around indoor/outdoor spaces and create a positive atmosphere.
6. 10 additional bicycle parking spaces on Polk Street and 1 car share space available for public use.
7. Large living wall facing California Street that will serve as the "gateway" between Middle and Lower Polk Street. This will give tourists on the cable car a reason to slow down and stop to patronize Polk Street businesses. Currently the cable car quickly passes through without slowing down.

I look forward to welcoming the residents and merchants of 1567 California to the community.

Sincerely,



 Hany Lee
 415. 447-2801

Phone Number:
Email:

CC: MOED
SF Planning Director
Supervisor Christensen

Kevin Guy, SF Planning
Planning Commission

LETTER OF SUPPORT FOR NEW DEVELOPMENT AT 1567 CALIFORNIA STREET

Case Number: 2014-0284E (1567 California St. 1436-14498 Polk St)

Merchant Name: Ibrahim AlHay

Merchant Address: 1234 Polk St S.F. CA 94109

Lana Russell (CPC)

San Francisco Planning Department

1650 Mission Street, Suite 400

San Francisco, CA 94103

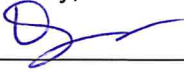
I am a small business owner located approximately _____ blocks away from the proposed mixed-use development to be constructed at 1567 California Street. After meeting with representatives of the developer, Presidio Bay Ventures, I have seen the proposed design and understand that the project is to contain approximately 47-50 residential units combined with a ground floor space dedicated towards retail and dining purposes.

I would like to express my unconditional support for this development. As a business owner, I feel that this project will prove extremely beneficial for the neighborhood by contributing the following:

1. Additional 100-150 residents of varying demographics who will contribute to the diversity and makeup of the Polk Street.
2. Increased pedestrian traffic from both the residents of the project and visitors from other neighborhoods who are likely to provide increased business.
3. Reduction of loitering and vagrancy that will improve the safety of the prominent Polk and California intersection and encourage more visitors to our businesses.
4. Promotion of small and local businesses via the retail ground floor and the introduction of new dining options.
5. Retail 'arcade' that will encourage pedestrians to congregate around indoor/outdoor spaces and create a positive atmosphere.
6. 10 additional bicycle parking spaces on Polk Street and 1 car share space available for public use.
7. Large living wall facing California Street that will serve as the "gateway" between Middle and Lower Polk Street. This will give tourists on the cable car a reason to slow down and stop to patronize Polk Street businesses. Currently the cable car quickly passes through without slowing down.

I look forward to welcoming the residents and merchants of 1567 California to the community.

Sincerely,



Phone Number:

Email: poetral Polk @ Email . com
415 531 7814

CC: MOED

Kevin Guy, SF Planning

SF Planning Director

Planning Commission

Supervisor Christensen

LETTER OF SUPPORT FOR NEW DEVELOPMENT AT 1567 CALIFORNIA STREET

Case Number: 2014-0284E (1567 California St. 1436-14498 Polk St)

Merchant Name: Super Burrito

Merchant Address: 1200 Polk St

Lana Russell (CPC)

San Francisco Planning Department

1650 Mission Street, Suite 400

San Francisco, CA 94103

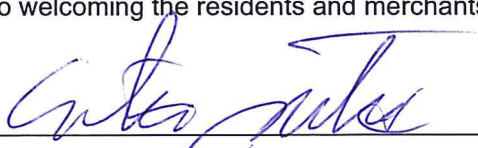
I am a small business owner located approximately _____ blocks away from the proposed mixed-use development to be constructed at 1567 California Street. After meeting with representatives of the developer, Presidio Bay Ventures, I have seen the proposed design and understand that the project is to contain approximately 47-50 residential units combined with a ground floor space dedicated towards retail and dining purposes.

I would like to express my unconditional support for this development. As a business owner, I feel that this project will prove extremely beneficial for the neighborhood by contributing the following:

1. Additional 100-150 residents of varying demographics who will contribute to the diversity and makeup of the Polk Street.
2. Increased pedestrian traffic from both the residents of the project and visitors from other neighborhoods who are likely to provide increased business.
3. Reduction of loitering and vagrancy that will improve the safety of the prominent Polk and California intersection and encourage more visitors to our businesses.
4. Promotion of small and local businesses via the retail ground floor and the introduction of new dining options.
5. Retail 'arcade' that will encourage pedestrians to congregate around indoor/outdoor spaces and create a positive atmosphere.
6. 10 additional bicycle parking spaces on Polk Street and 1 car share space available for public use.
7. Large living wall facing California Street that will serve as the "gateway" between Middle and Lower Polk Street. This will give tourists on the cable car a reason to slow down and stop to patronize Polk Street businesses. Currently the cable car quickly passes through without slowing down.

I look forward to welcoming the residents and merchants of 1567 California to the community.

Sincerely,



Phone Number:

Email:

415 771 9700

CC: MOED

SF Planning Director

Supervisor Christensen

Kevin Guy, SF Planning

Planning Commission

LETTER OF SUPPORT FOR NEW DEVELOPMENT AT 1567 CALIFORNIA STREET

Case Number: 2014-0284E (1567 California St. 1436-14498 Polk St)

Merchant Name: Broad! Butter MKT

Merchant Address: 888 ofarrell st S.F CA 94109

Lana Russell (CPC)

San Francisco Planning Department

1650 Mission Street, Suite 400

San Francisco, CA 94103

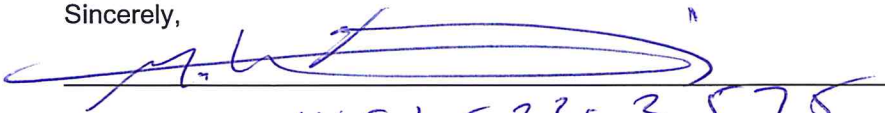
I am a small business owner located approximately _____ blocks away from the proposed mixed-use development to be constructed at 1567 California Street. After meeting with representatives of the developer, Presidio Bay Ventures, I have seen the proposed design and understand that the project is to contain approximately 47-50 residential units combined with a ground floor space dedicated towards retail and dining purposes.

I would like to express my unconditional support for this development. As a business owner, I feel that this project will prove extremely beneficial for the neighborhood by contributing the following:

1. Additional 100-150 residents of varying demographics who will contribute to the diversity and makeup of the Polk Street.
2. Increased pedestrian traffic from both the residents of the project and visitors from other neighborhoods who are likely to provide increased business.
3. Reduction of loitering and vagrancy that will improve the safety of the prominent Polk and California intersection and encourage more visitors to our businesses.
4. Promotion of small and local businesses via the retail ground floor and the introduction of new dining options.
5. Retail 'arcade' that will encourage pedestrians to congregate around indoor/outdoor spaces and create a positive atmosphere.
6. 10 additional bicycle parking spaces on Polk Street and 1 car share space available for public use.
7. Large living wall facing California Street that will serve as the "gateway" between Middle and Lower Polk Street. This will give tourists on the cable car a reason to slow down and stop to patronize Polk Street businesses. Currently the cable car quickly passes through without slowing down.

I look forward to welcoming the residents and merchants of 1567 California to the community.

Sincerely,



Phone Number: (415) 533-3575
Email:

CC: MOED Kevin Guy, SF Planning
SF Planning Director Planning Commission
Supervisor Christensen

LETTER OF SUPPORT FOR NEW DEVELOPMENT AT 1567 CALIFORNIA STREET

Case Number: 2014-0284E (1567 California St. 1436-14498 Polk St)

Merchant Name: Streets

Merchant Address: 634 Polk St

Lana Russell (CPC)

San Francisco Planning Department

1650 Mission Street, Suite 400

San Francisco, CA 94103

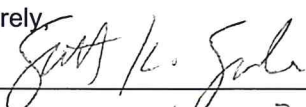
I am a small business owner located approximately 4 blocks away from the proposed mixed-use development to be constructed at 1567 California Street. After meeting with representatives of the developer, Presidio Bay Ventures, I have seen the proposed design and understand that the project is to contain approximately 47-50 residential units combined with a ground floor space dedicated towards retail and dining purposes.

I would like to express my unconditional support for this development. As a business owner, I feel that this project will prove extremely beneficial for the neighborhood by contributing the following:

1. Additional 100-150 residents of varying demographics who will contribute to the diversity and makeup of the Polk Street.
2. Increased pedestrian traffic from both the residents of the project and visitors from other neighborhoods who are likely to provide increased business.
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6. 10 additional bicycle parking spaces on Polk Street and 1 car share space available for public use.
7. Large living wall facing California Street that will serve as the "gateway" between Middle and Lower Polk Street. This will give tourists on the cable car a reason to slow down and stop to patronize Polk Street businesses. Currently the cable car quickly passes through without slowing down.

I look forward to welcoming the residents and merchants of 1567 California to the community.

Sincerely,



Phone Number: 415-373-2168

Email: Streets.sf@att.net

CC: MOED

SF Planning Director

Supervisor Christensen

Kevin Guy, SF Planning

Planning Commission

644 Polk

LETTER OF SUPPORT FOR NEW DEVELOPMENT AT 1567 CALIFORNIA STREET

Case Number: 2014-0284E (1567 California St. 1436-14498 Polk St)

Merchant Name: California Produce

Merchant Address: 986 Polk St

Lana Russell (CPC)

San Francisco Planning Department

1650 Mission Street, Suite 400

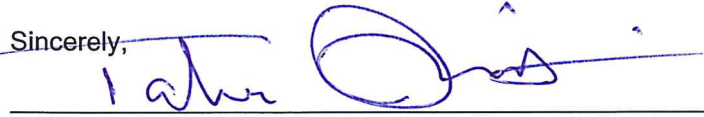
San Francisco, CA 94103

I am a small business owner located approximately 4 blocks away from the proposed mixed-use development to be constructed at 1567 California Street. After meeting with representatives of the developer, Presidio Bay Ventures, I have seen the proposed design and understand that the project is to contain approximately 47-50 residential units combined with a ground floor space dedicated towards retail and dining purposes.

I would like to express my unconditional support for this development. As a business owner, I feel that this project will prove extremely beneficial for the neighborhood by contributing the following:

1. Additional 100-150 residents of varying demographics who will contribute to the diversity and makeup of the Polk Street.
2. Increased pedestrian traffic from both the residents of the project and visitors from other neighborhoods who are likely to provide increased business.
3. Reduction of loitering and vagrancy that will improve the safety of the prominent Polk and California intersection and encourage more visitors to our businesses.
4. Promotion of small and local businesses via the retail ground floor and the introduction of new dining options.
5. Retail 'arcade' that will encourage pedestrians to congregate around indoor/outdoor spaces and create a positive atmosphere.
6. 10 additional bicycle parking spaces on Polk Street and 1 car share space available for public use.
7. Large living wall facing California Street that will serve as the "gateway" between Middle and Lower Polk Street. This will give tourists on the cable car a reason to slow down and stop to patronize Polk Street businesses. Currently the cable car quickly passes through without slowing down.

I look forward to welcoming the residents and merchants of 1567 California to the community.

Sincerely, 

Phone Number: _____
Email: californiaproduce@gmail.com

CC: MOED Kevin Guy, SF Planning
SF Planning Director Planning Commission
Supervisor Christensen

LETTER OF SUPPORT FOR NEW DEVELOPMENT AT 1567 CALIFORNIA STREET

Case Number: 2014-0284E (1567 California St. 1436-14498 Polk St)

Merchant Name: Irving Pizza

Merchant Address: 928 Geary SF CA 94109

Lana Russell (CPC)

San Francisco Planning Department

1650 Mission Street, Suite 400

San Francisco, CA 94103

I am a small business owner located approximately _____ blocks away from the proposed mixed-use development to be constructed at 1567 California Street. After meeting with representatives of the developer, Presidio Bay Ventures, I have seen the proposed design and understand that the project is to contain approximately 47-50 residential units combined with a ground floor space dedicated towards retail and dining purposes.

I would like to express my unconditional support for this development. As a business owner, I feel that this project will prove extremely beneficial for the neighborhood by contributing the following:

1. Additional 100-150 residents of varying demographics who will contribute to the diversity and makeup of the Polk Street.
2. Increased pedestrian traffic from both the residents of the project and visitors from other neighborhoods who are likely to provide increased business.
3. Reduction of loitering and vagrancy that will improve the safety of the prominent Polk and California intersection and encourage more visitors to our businesses.
4. Promotion of small and local businesses via the retail ground floor and the introduction of new dining options.
5. Retail 'arcade' that will encourage pedestrians to congregate around indoor/outdoor spaces and create a positive atmosphere.
6. 10 additional bicycle parking spaces on Polk Street and 1 car share space available for public use.
7. Large living wall facing California Street that will serve as the "gateway" between Middle and Lower Polk Street. This will give tourists on the cable car a reason to slow down and stop to patronize Polk Street businesses. Currently the cable car quickly passes through without slowing down.

I look forward to welcoming the residents and merchants of 1567 California to the community.

Sincerely,



Phone Number: 415-235-1574
Email:

CC: MOED

SF Planning Director

Supervisor Christensen

Kevin Guy, SF Planning

Planning Commission

LETTER OF SUPPORT FOR NEW DEVELOPMENT AT 1567 CALIFORNIA STREET

Case Number: 2014-0284E (1567 California St 1436-1498 Polk Street)

Resident Name: Tina Bantay Jones

Resident Address: 3000 Webster St SF, CA 94123

Lana Russell (CPC)

SF Planning Department

1650 Mission Street Suite 400

San Francisco, CA 94103

I am a current District Three resident and live approximately ____ blocks away from the proposed mixed-use development to be constructed at 1567 California Street. After meeting with representatives of the developer, Presidio Bay Ventures, I have seen the proposed design and understand that the project is to contain approximately 47-50 residential units combined with a ground floor space dedicated towards retail and dining purposes.

I would like to express my unconditional support for this development. As a resident, I feel that this project will prove extremely beneficial for the neighborhood by contributing the following:

1. Additional 100-150 residents of varying demographics who will contribute to the diversity and makeup of the Polk Street.
2. Increased housing stock that will help alleviate rising prices by bringing more units onto the market
3. Increased pedestrian traffic from both the residents of the project and visitors from other neighborhoods who patronize the project's retail stores.
4. Reduction of loitering and vagrancy that will improve the safety of the prominent Polk and California intersection.
5. Promotion of small and local businesses via the retail ground floor and the introduction of new dining options.
6. Retail 'arcade' that will encourage pedestrians to congregate around indoor/outdoor spaces and create a positive atmosphere.
7. 10 additional bicycle parking spaces on Polk Street and 1 car share space available for public use.
8. Large living wall facing California Street that will serve as the "gateway" between Middle and Lower Polk Street. This will give tourists on the cable car a reason to slow down and stop to patronize Polk Street businesses. Currently the cable car quickly passes through without slowing down.

I look forward to welcoming the residents and merchants of 1567 California to the community.

Sincerely, Tina Bantay Jones

Phone Number: 415 497 2422

Email: tbantay@comcast.net

CC: MOED

SF Planning Director

SF Planning Commission

Planner Kevin Guy

Supervisor Julie Christensen

LETTER OF SUPPORT FOR NEW DEVELOPMENT AT 1567 CALIFORNIA STREET

Case Number: 2014-0284E (1567 California St. 1436-14498 Polk St)

Merchant Name: VAPOR ROOM COOP

Merchant Address: 935 Geary Blvd. SF CA 94109

Lana Russell (CPC)

San Francisco Planning Department

1650 Mission Street, Suite 400

San Francisco, CA 94103

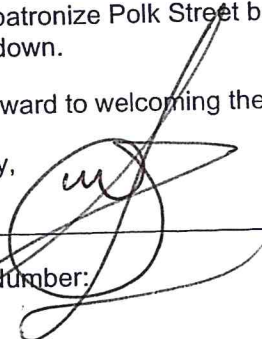
I am a small business owner located approximately 5 blocks away from the proposed mixed-use development to be constructed at 1567 California Street. After meeting with representatives of the developer, Presidio Bay Ventures, I have seen the proposed design and understand that the project is to contain approximately 47-50 residential units combined with a ground floor space dedicated towards retail and dining purposes.

I would like to express my unconditional support for this development. As a business owner, I feel that this project will prove extremely beneficial for the neighborhood by contributing the following:

1. Additional 100-150 residents of varying demographics who will contribute to the diversity and makeup of the Polk Street.
2. Increased pedestrian traffic from both the residents of the project and visitors from other neighborhoods who are likely to provide increased business.
3. Reduction of loitering and vagrancy that will improve the safety of the prominent Polk and California intersection and encourage more visitors to our businesses.
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I look forward to welcoming the residents and merchants of 1567 California to the community.

Sincerely,



Martin Olive

Phone Number:

Email:

CC: MOED
SF Planning Director
Supervisor Christensen

Kevin Guy, SF Planning
Planning Commission

LETTER OF SUPPORT FOR NEW DEVELOPMENT AT 1567 CALIFORNIA STREET

Case number: 2014-0284E (1567 California St. 1436-1498 Polk St.)

Resident Name: Jenny Sharma

Resident Address: 935 Geary Blvd

Lana Russell (CPC)

San Francisco Planning Department

1650 Mission Street Suite 400

San Francisco, CA 94103

I am a current resident of the Polk Street community and live approximately ____ blocks away from the proposed mixed-use development to be constructed at 1567 California Street. After meeting with representatives of the developer, Presidio Bay Ventures, I have seen the proposed design and understand that the project is to contain approximately 47-50 residential units combined with a ground floor space dedicated towards retail and dining purposes.

I would like to express my unconditional support for this development. As a resident, I feel that this project will prove extremely beneficial for the neighborhood by contributing the following:

1. Additional 100-150 residents of varying demographics who will contribute to the diversity and makeup of the Polk Street.
2. Increased housing stock that will help alleviate rising prices by bringing more units onto the market
3. Increased pedestrian traffic from both the residents of the project and visitors from other neighborhoods who patronize the project's retail stores.
4. Reduction of loitering and vagrancy that will improve the safety of the prominent Polk and California intersection.
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I look forward to welcoming the residents and merchants of 1567 California to the community.

Sincerely, Jenny Sharma

Phone Number:

Email:

jenny@gallerydaily.com

CC: MOED

San Francisco Planning Director

Supervisor Christensen

Planner Kevin Guy

SF Planning Commission

LETTER OF SUPPORT FOR NEW DEVELOPMENT AT 1567 CALIFORNIA STREET

Case Number: 2014-0284E (1567 California St 1436-1498 Polk Street)

Resident Name: James Duncan

Resident Address: 935 Geary St San Fran 94109

Lana Russell (CPC)

SF Planning Department

1650 Mission Street Suite 400

San Francisco, CA 94103

I am a current District Three resident and live approximately ____ blocks away from the proposed mixed-use development to be constructed at 1567 California Street. After meeting with representatives of the developer, Presidio Bay Ventures, I have seen the proposed design and understand that the project is to contain approximately 47-50 residential units combined with a ground floor space dedicated towards retail and dining purposes.

I would like to express my unconditional support for this development. As a resident, I feel that this project will prove extremely beneficial for the neighborhood by contributing the following:

1. Additional 100-150 residents of varying demographics who will contribute to the diversity and makeup of the Polk Street.
2. Increased housing stock that will help alleviate rising prices by bringing more units onto the market
3. Increased pedestrian traffic from both the residents of the project and visitors from other neighborhoods who patronize the project's retail stores.
4. Reduction of loitering and vagrancy that will improve the safety of the prominent Polk and California intersection.
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8. Large living wall facing California Street that will serve as the "gateway" between Middle and Lower Polk Street. This will give tourists on the cable car a reason to slow down and stop to patronize Polk Street businesses. Currently the cable car quickly passes through without slowing down.

I look forward to welcoming the residents and merchants of 1567 California to the community.

Sincerely, Jim Duncan

Phone Number:

Email: jidunca@hotmail.com

CC: MOED

SF Planning Director

SF Planning Commission

Planner Kevin Guy

Supervisor Julie Christensen

LETTER OF SUPPORT FOR NEW DEVELOPMENT AT 1567 CALIFORNIA STREET

Case Number: 2014-0284E (1567 California St. 1436-14498 Polk St)

Merchant Name: Lem-Roy Properties I DO, LLC

Merchant Address: 933 Geary ST SF 94109

Lana Russell (CPC)

San Francisco Planning Department

1650 Mission Street, Suite 400

San Francisco, CA 94103

I am a small business owner located approximately 5 blocks away from the proposed mixed-use development to be constructed at 1567 California Street. After meeting with representatives of the developer, Presidio Bay Ventures, I have seen the proposed design and understand that the project is to contain approximately 47-50 residential units combined with a ground floor space dedicated towards retail and dining purposes.

I would like to express my unconditional support for this development. As a business owner, I feel that this project will prove extremely beneficial for the neighborhood by contributing the following:

1. Additional 100-150 residents of varying demographics who will contribute to the diversity and makeup of the Polk Street.
2. Increased pedestrian traffic from both the residents of the project and visitors from other neighborhoods who are likely to provide increased business.
3. Reduction of loitering and vagrancy that will improve the safety of the prominent Polk and California intersection and encourage more visitors to our businesses.
4. Promotion of small and local businesses via the retail ground floor and the introduction of new dining options.
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7. Large living wall facing California Street that will serve as the "gateway" between Middle and Lower Polk Street. This will give tourists on the cable car a reason to slow down and stop to patronize Polk Street businesses. Currently the cable car quickly passes through without slowing down.

I look forward to welcoming the residents and merchants of 1567 California to the community.

Sincerely,

Phone Number: 415-640-0964

Email: gary@sc49square.realtygroup.com

CC: MOED

SF Planning Director

Supervisor Christensen

Kevin Guy, SF Planning

Planning Commission

LETTER OF SUPPORT FOR NEW DEVELOPMENT AT 1567 CALIFORNIA STREET

Case Number: 2014-0284E (1567 California St. 1436-14498 Polk St)

Merchant Name: 49 Square Realty Group, Inc

Merchant Address: 933 Geary ST SF 94109

Lana Russell (CPC)

San Francisco Planning Department

1650 Mission Street, Suite 400

San Francisco, CA 94103

I am a small business owner located approximately 5 blocks away from the proposed mixed-use development to be constructed at 1567 California Street. After meeting with representatives of the developer, Presidio Bay Ventures, I have seen the proposed design and understand that the project is to contain approximately 47-50 residential units combined with a ground floor space dedicated towards retail and dining purposes.

I would like to express my unconditional support for this development. As a business owner, I feel that this project will prove extremely beneficial for the neighborhood by contributing the following:

1. Additional 100-150 residents of varying demographics who will contribute to the diversity and makeup of the Polk Street.
2. Increased pedestrian traffic from both the residents of the project and visitors from other neighborhoods who are likely to provide increased business.
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I look forward to welcoming the residents and merchants of 1567 California to the community.

Sincerely,

Phone Number: 415-640-0964

Email: customer.service@49squarerealtygroup.com

CC: MOED
SF Planning Director
Supervisor Christensen

Kevin Guy, SF Planning
Planning Commission



SPUR

San Francisco | San Jose | Oakland

July 1, 2015

Kabir Seth
Presidio Bay Ventures
185 Berry St, Suite 1200
San Francisco, CA 94107

Re: Proposed Development at 1567 California Street

Dear Mr. Seth:

On behalf of the members of the SPUR Project Review Committee, we would like to thank you for bringing the proposed development at 1567 California Street to our group for review and consideration at our April 2015 meeting. Thank you for also submitting additional information for SPUR’s review after the meeting.

The mission of the SPUR Project Review Committee is to consider projects that are of citywide importance and to evaluate them according to criteria related to land use, public realm interface, building design and environmental effects. In all cases, we are seeking a combination of excellent planning and design solutions that will ensure the positive contribution of each project to a safe, visually appealing, and vibrant urban setting for the people who live and work in San Francisco.

After reviewing and discussing the 1567 California Street project, we provide the following comments for your information and action.

Land Use

Located at the corner of California and Polk streets, the proposed project is for mixed-use development in a transit-rich neighborhood. The existing one- and two-story buildings at 1436-1498 Polk Street are currently occupied by small retail tenants who will be given the option to return after construction is complete.

The project sponsor proposes a 7-story building with 51 residential units, including approximately 8,500 square feet of retail space and 12,500 square feet of common open space. The residential units are primarily large two- and three-bedroom rental units, with condo-mapping in place. The ground-floor retail space will have ceiling heights ranging from 14 to 20 feet, reflecting the Polk Street grade change. The proposed height of the building meets the 80-foot limit for this parcel.

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The project sponsor is proposing to move the bulk of the retail use to California Street in an effort to encourage a more active and pedestrian friendly streetscape and to tempt pedestrians to explore lower Polk Street. Commercial units will be small — 400 square feet to 2,500 square feet — to focus on non-formula retail tenants in an effort to maintain the neighborhood's existing retail scale. The committee is in agreement with these goals and encourages the project sponsor to allow for as much individualization as possible during the design and build-out of tenant improvements.

The project sponsor proposes 51 residential units, considerably less than the allowed 90 units, with the intention of attracting families to the project. We understand there are discussions underway that would shrink unit square footage and increase the project's density in order to price the units at a more reasonable level. SPUR is supportive of the higher density. We believe that larger (by bedroom count) units are needed in the city and appreciate the focus on a family-friendly unit mix, so we hope that remains the intention. We appreciate that between the fourth floor deck and roof deck there will be over 3,000 square feet of family-friendly amenities and common area, including picnic tables, child-safe flooring, dog runs, bocce ball courts, barbecue pits, etc. and 9,000 square feet of landscaped open space.

After our meeting, the project sponsor received a determination by the City affirming this project's eligibility to provide affordable units on-site. We understand you are pursuing next steps in the process to get an exemption from Costa Hawkins. The committee appreciates the effort to accommodate the affordable units on-site.

Public Realm Interface and the Promotion of a Pedestrian-Oriented Environment

The committee agrees that the additional retail space along California and the setback of the Polk Street façade may help to draw pedestrian traffic south of California into the lower Polk neighborhood. Some early concerns regarding the uniform steel and glass ground-floor treatment were mitigated by additional information from the project sponsor. The committee suggests that allowing for variety and "messiness" during the design of tenant improvements may be worth considering in the future in order to support new retail activity and provide visual interest for pedestrians.

1567 California Street is located in a transit-rich neighborhood, with direct or nearby access to the 19-Polk Street line, the 1-California line, the Van Ness corridor lines and the California Street cable car. The project sponsor proposes a 0.7:1 underground parking ratio, including 2 car share spaces, as well as 1:1 bike parking onsite and an additional 14 bike parking spaces offsite. The existing curb cut on California will serve as the entrance to the underground parking. The committee is pleased with the lower parking ratio and the minimal intrusion on the pedestrian realm as proposed.

Building & Landscape Design

The proposed building design at 1567 California Street is a modern mix of concrete, wood and composite paneling. The committee is satisfied that the proposed design is in keeping with the context of the neighborhood and is generally pleased with the overall treatment of the upper floors. The committee particularly liked the strong verticals that help to break up the massing on the upper floors and could envision a situation where the strong white horizontal band gesture could be successfully broken up as well. We were less impressed with the proposed green wall on California Street, as it doesn't seem as well integrated into the design as we would like. The California Street façade might be better served by a mural or other work of public art.

The committee urges the project sponsor to consider a solution that will more fully integrate the ground floor into the overall design, possibly by extending some of the vertical design elements of the upper stories to break up the horizontal massing of the retail base, as long as the vertical elements do not impinge on transparency at the ground floor retail.

It is unclear from the presentation how the landscape design at the street level is to be enhanced, though we appreciate the verbal assurances that the project will provide green spaces at the sidewalk that will improve the public realm and contribute toward project sustainability efforts. We suggest the project sponsor refer to the Better Streets Plans and engage the services of an experienced landscape designer if that is not already done. We look forward to hearing more about the streetscape as plans progress.

Environmental Effects

SPUR believes it is essential for projects to build environmental sustainability into their design and function. We understand that the project sponsor intends to seek a Gold LEED rating and a Delos WELL certification. Our discussions of storm-water treatment and other "green" features assured the committee that the project sponsor is committed to taking whatever steps are necessary to reach those goals. We encourage them to include sustainable systems in the earliest stages of development. We look forward to hearing more as the project progresses.

Conclusion

The SPUR Project Review Committee finds the proposed project at 1567 California Street to be an appropriate use of the site. The increased density in the residential development will enliven and enhance this transit-rich neighborhood. We are also encouraged by the project team's genuine commitment to building family-friendly residences and neighborhood-friendly retail in this established neighborhood.

We thank you for committing your time and resources to the presentation at SPUR, appreciate the fact that you have presented your proposal to us at an early stage in its development so that you may take our recommendations into consideration. We will follow further refinements of this project with great interest and invite you to keep us informed on its progress.

Please do not hesitate to contact us for questions/clarifications.

Sincerely,

Charmaine Curtis Mary Beth Sanders Reuben Schwartz
SPUR Project Review Committee Co-Chairs

cc: SPUR Board of Directors



95 Brady Street
San Francisco, CA 94103
415 541 9001
info@sfnac.org
www.sfnac.org

Mr. Cyrus Sanandaji, Managing Director
Presidio Bay Ventures
185 Berry Street, Suite 1200
San Francisco, CA 94107

July 8, 2015 (revised from June 11, 2015)

Re: 1567 California Street – Mixed-Use Development

Dear Mr. Sanandaji,

Thank you for presenting your plans for 1567 California Street to our Project Review Committee on April 22, 2015. Upon thorough review and discussion, we endorse the project. Although we have some reservations, we feel the project has merit and aligns with our mission of increasing the supply of well-designed, well-located in San Francisco, at all levels of affordability. Please read our letter, which explains how your project meets our guidelines, as well as areas suggested for improvement. Please also see our report card, which grades your project according to each guideline. We have attached a copy of our project review guidelines for your reference.

Project Description: The project proposes to build 61-65 homes in a seven-story building with over 9,000 square feet of ground floor retail, above one level of subterranean parking.

Land Use: The site is currently occupied by retail and a surface parking lot. This is an ideal location for new, higher density housing, as it is in the heart of a vibrant mixed-use commercial and transit corridor with numerous neighborhood amenities.

Density: At your presentation to our Committee, your plan was to build 51 units. But in follow-up conversation, you stated you would increase the number of homes to between 61 and 65 units. The site could accommodate up to 94 units under allowable zoning. We understand your current proposal is due to the somewhat unique rules governing local zoning; RM-3, the nearest adjacent residential zone, allows building 47 homes, while RM-4, still close your site, allows for 94. The current unit count is a compromise that has been worked out with the Planning Department. Although our members would have preferred a greater density, we understand the circumstances with which you must work.

We understand you are still in negotiations with Planning on increasing the density. We hope they will allow for the additional units, as we believe this site is an ideal location for greater density and the smaller units would make them affordable to more residents.

Mr. Cyrus Sanandaji
July 8, 2015
Page Two

Affordability: At the time of your presentation, it seemed you would pay the in-lieu fee to the Mayor's Office of Housing. We are pleased to hear you have now opted for putting the below-market-rate (BMR) units on site. This would equate to seven or eight BMR units, depending on the total unit count.

Parking and Alternative Transportation: The site is located along an active pedestrian and bicycle corridor that acts as a major connector between the eastern and northwestern areas of the City. The project would be well-served by several transit lines, including the California Street Cable Car, Muni lines along California and Polk Streets and the future Van Ness Bus Rapid Transit (BRT).

The project proposes 35 parking spaces, 15 of which will be allocated for the commercial space in the building. The parking ratio for the residential units is just over 0.3 spaces per homes. SFHAC supports this low ratio. You indicated you plan to provide a bike-parking ratio of one space per home. Because of the high demand for bike parking in the City, we encourage you to increase this ratio. We consistently hear from our members that project sponsors over estimate the need for car parking and don't plan for enough bike parking.

Preservation: There are no structures of significant cultural or historic merit on or near the site that would be affected by the proposed project.

Urban Design: Our members encourage you to create more differentiation between the retail and residential portions of the building along Polk Street, in order to respect the context of the Polk Street mixed-use corridor. Follow up renderings you've shared show improvements. We also believe the landscaping and streetscape improvements will make for a better pedestrian experience along this corridor.

Our members noted the streetscape design along California Street could do a better job of buffering the busy vehicular traffic and pedestrian activity on the sidewalk. However, you acknowledged any final designs may be left up to the Department of Public Works (DPW), which we understand is beyond your control.

We would support the design stepping back the ground floor of the building along California Street and reducing the number of curb cuts where possible. We urge you to consider creating a multi-purpose community room near the residential entry.

The project would include about 17,000 square feet of open space for the building's residents, distributed between private patio spaces and common open spaces on the 4th floor roof deck and top roof deck, which would include various amenities. Since these are larger units, it is more likely families will occupy them, and they would be well served by the space.

Mr. Cyrus Sanandaji
July 8, 2015
Page Three

Finally, we'd like to acknowledge your subsequent efforts to respond to our concerns about the urban design of your project and sharing updated renderings with our Committee Chair, which show improvements.

Environmental Features: You plan to incorporate several features that will help green the building, above the basic energy and water conservation requirements of San Francisco, which exceed those of most other cities in the country. These include implementing individual water metering into the units, which we strongly support. The building will most likely be LEED Gold Certified and also the first DELOS/Well certified multifamily project on the West Coast. The latter program is new to us, but we understand the goal is to improve the health and well being of the building's residents. Finally, you stated in follow-up discussion that you will equip the building with EPA's energy star rating system, which will monitor the building's energy usage.

Community Input: Our members believe you have been thorough in reaching out to and engaging with the surrounding the community. You have met with the local Central Business District, various merchant associates and Larkin Street Youth, amongst numerous other groups. You have also engaged various City officials with your plans. As you approach your Planning Commission hearing, we encourage you to continue to be open to any legitimate concerns that can readily be accommodated.

Thank you for presenting your plans to our Project Review Committee. We endorse the project, with the reservations noted above. Please keep us abreast of any changes and let us know how we may be of assistance.

Best,



Tim Colen
Executive Director

SFHAC Project Review Guidelines

Land Use: Housing should be an appropriate use of the site given the context of the adjacent properties and the surrounding neighborhood and should enhance neighborhood livability.

Density: The project should take full advantage of the maximum unit density and/or building envelope, allowable under the zoning rules.

Affordability: The need for affordable housing, including middle income (120-150 of Area Median Income) housing, is a critical problem and SFHAC gives special support to projects that propose creative ways to expand or improve unit affordability beyond the legally mandated requirements.

Parking and Alternative Transportation: SFHAC expects the projects it endorses to include creative strategies to reduce the need for parking, such as ample bicycle storage, provision of space for car-share vehicles on-site or nearby, un-bundling parking cost from residential unit cost, and measures to incentivize transit use. Proximity to transit should result in less need for parking.

In districts with an as-of-right maximum and discretionary approval up to an absolute maximum, SFHAC will support parking exceeding the as-of-right maximum only to the extent the Code criteria for doing so are clearly met. In districts where the minimum parking requirement is one parking space per residential unit (1:1), the SFHAC will not, except in extraordinary circumstances, support a project with parking in excess of that amount.

Preservation: If there are structures of significant historic or cultural merit on the site, their retention and/or incorporation into the project consistent with historic preservation standards is encouraged. If such structures are to be demolished, there should be compelling reasons for doing so.

Urban Design: The project should promote principles of good urban design: Where appropriate, contextual design that is compatible with the adjacent streetscape and existing neighborhood character while at the same time utilizing allowable unit density; pleasant and functional private and/or common open space; pedestrian, bicycle and transit friendly site planning; and design treatments that protect and enhance the pedestrian realm, with curb cuts minimized and active ground floor uses provided.

Projects with a substantial number of multiple bedroom units should consider including features that will make the project friendly to families with children.

Mr. Cyrus Sanandaji
July 8, 2015
Page Five

Environmental Features: SFHAC is particularly supportive of projects that employ substantial and/or innovative measures that will enhance their sustainability and reduce their carbon footprint.

Community Input: Projects for which the developer has made a good faith effort to communicate to the community and to address legitimate neighborhood concerns, without sacrificing SFHAC's objectives, will receive more SFHAC support.



San Francisco Housing Action Coalition (SFHAC) Project Report Card

Address: 1567 California Street

Project Sponsor: Presidio Bay Ventures

Date of SFHAC Review: April 22, 2015

Grading Scale:

1 = Fails to meet project review guideline criteria

2 = Meets some project review guideline criteria

3 = Meets basic project review guideline criteria

4 = Exceeds basic project review guideline criteria

5 = Goes far beyond of what is required

Criteria for SFHAC Endorsement:

1. The project must have been presented to the SFHAC Project Review Committee;
2. The project must score a minimum of 3/5 on any given guideline.

<i>Guideline</i>	<i>Comments</i>	<i>Grade</i>
Land Use	The project replaces retail and a surface parking lot with 61-65 homes above ground floor retail, with one level of subterranean parking, along an active, mixed-use corridor.	5
Density	The project faces somewhat unique zoning constraints. The site allows for up to 94 homes, but only 61-65 homes are proposed with the current plans. We would prefer greater density.	3
Affordability	The project will most likely put the below-market-rate (BMR) units on site.	3
Parking and Alternative Transportation	The project is located along a busy pedestrian and bike corridor with various transit options. We support the low car-parking ratio for the residential units. We encourage more bike parking.	4
Preservation	There are no structures of significant cultural or historic merit on or near the site that would be affected by your project.	N/A
Urban Design	We encourage more buffering between the street and sidewalk along California, if possible. Setting back the ground floor along California would be helpful. We would support increasing the design differentiation between the residential and commercial uses.	3
Environmental Features	The project targets LEED Gold and Delo Certification. The project sponsor will also implement individual water sub metering for the units and EPA's energy star rating system.	5
Community Input	The project sponsor has thoroughly engaged the surrounding community and City officials and has addressed legitimate feedback that can readily be accommodated.	5
Additional Comments	We thank the project sponsor for addressing our member's questions and concerns with thorough and prompt responses.	N/A
Final Comments	SFHAC endorses the proposed project, with the reservations noted above.	4/5



July 9, 2015
Mr. Kevin Guy
San Francisco Planning Department
1650 Mission Street, Suite 400
San Francisco, CA 94103-2414
re: case 2014-001293CUA (1567 California Street)

Dear Mr. Guy:

In reference to case 2014-001293CUA, I am writing on behalf of **Lower Polk Neighbors** [LPN], a community association made up of residents and merchants. Our association boundaries are California Street on the North, Ellis Street on the South, Hyde Street on the East and the West Side of Van Ness. 1567 California Street is situated within our neighborhood organization boundaries.

At a regular LPN meeting on May 13, 2014, we heard a presentation of this project, updating us on its current design. I want to point out that this was the Presidio Bay Ventures' THIRD presentation to our group which demonstrates a commitment to working with our neighborhood and its residents. As always, the project sponsor was both receptive to membership inquiries into and comments on the project design. At the conclusion of the presentation, we took a vote of membership attendees to support the project as it was presented to us. The results were 95% in favor and 5% opposed, indicating an extremely high level of support among member attendees for the project as presented.

The project has been responsive to neighbors in the existing building next to it and, while not all residents are ultimately happy that a taller building is going up next to them, they have all indicated that the project sponsors have engaged them in dialogue.

Overall, there are several design features that have garnered the support of the Lower Polk neighbors including:

- An appropriate response to the differences between Polk Street and California Street scales
- The inclusion of all BMR units on site (a top LPN priority)
- The inclusion of larger three bedroom units for families
- An excellent ground level response to Polk Street
- A sensitivity to the idea that this is a "gateway" to Lower Polk and adding the vertical 'fin' as part of this design intent.

- The addition of a very large green wall on California Street in honor of a local neighborhood advocate, Shell Thomas, who died while this project was being designed.

One area where some have disagreements with the project design is the ground floor design at the corner of Polk and California. LPN worked a lot with the project sponsors on creating a cut-back at the corner to open up the corner for larger crowds and better flow. The design did have that at one time but has since been replaced with a square corner. Project sponsors have indicated that they take the position that the expansion of the corner with bulb-outs will accommodate that request. However, this is ultimately not within the project sponsor's power to decide. LPN would encourage the Planning Commission to seriously consider our proposal to cut back the CA/Polk corner to open the sidewalk up there. Our disagreement with this project element from the project sponsor would be the only real area of disagreement. However, it does represent a step back from an earlier corner design we had requested and they had incorporated.

Presidio Bay Ventures has not only engaged the Lower Polk Neighbors in their project, but have become regular attendees at all of our monthly meetings. Additionally, I, as chair of the organization, met recently with Mr. Kabir Seth to discuss other matters concerning the neighborhood. This resulted in Presidio Bay Ventures meeting with and seeking information on how they could work with our District 3 Supervisor Christensen's office and the city to address the affordable housing crisis in the city. While this is at the beginning stages, it indicates a commitment to the future of San Francisco and the Lower Polk neighborhood. They should be recognized for their efforts!

In conclusion, LPN supports this project. Presidio Bay Ventures has distinguished themselves as a project sponsor who has been directly sensitive to neighborhood concerns of the Lower Polk Neighbors and has been an active partner in considering resident concerns as they have developed their project.

Thank you for your serious consideration on this matter.

With regards,

A handwritten signature in black ink, appearing to be 'Andrew Chandler', with a long horizontal line extending to the right.

Andrew Chandler, AIA
Chair
Lower Polk Neighbors

Cc: District 3 Supervisor Julie Christensen, San Francisco Board of Supervisors



MARLOW

August 7th, 2015

San Francisco Planning Commission
1650 Mission Street, STE 400
San Francisco, CA 94103

Re: Letter of Support for New Development at 1567 California Street

To the Planning Commission:

We represent the interests of the Marlow Owner's Association for The Marlow, a mixed-use condominium development located at 1788 Clay Street on the corner of Van Ness and Clay. The project sponsor (Presidio Bay Ventures) gave their formal presentation on the proposed mixed-use project at 1567 California to our organization on July 16th, 2015 and spent time discussing the details of their project, as well as responding to our questions and concerns.

Please accept this letter on behalf of the homeowners on the following signature page in Marlow as a commitment to our unconditional support for this project and the many benefits that we believe it will bring to the area's merchants, residents and long-term vitality. The project sponsor has been responsive to all of our questions and concerns regarding the project, and has taken proactive steps to help alleviate some of the neighborhood issues specific to us, as residents, that we have been trying to address with every new project.


In particular, we would like to highlight certain aspects of the project that we are especially in support of:

1. Additional 100-150 residents of varying demographics who will contribute to the diversity and makeup of the Polk Street.
2. Commitment to a curated retail space that will bring much needed amenities to the neighborhood; Presidio Bay Ventures has proactively reached out to us and other neighbors to better understand what is currently lacking and what we would like to see e.g. outdoor café, children's toy/clothing store, bookstore, etc.
3. Introduction of increased parking (located underground) that will help replace some of the spaces that have been removed as a result of the recently introduced bicycle lanes.
4. Reduction of loitering and vagrancy at that intersection that will improve the safety of the prominent Polk and California intersection and encourage local residents to walk further south along Polk than is currently desirable.
5. Retail 'arcade' that will encourage pedestrians to congregate around indoor/outdoor spaces and create a positive atmosphere.

MARLOW OWNERS' ASSOCIATION

C/O TITAN MANAGEMENT GROUP • P.O. BOX 77045 • SAN FRANCISCO • CA • 94107

415.777.5200 PHONE • 415.777.5220 FAX

- 
6. 14 additional bicycle parking spaces on Polk Street and car share space available for public use that will help Lower Polk become a 'destination' area for visitors from different neighborhoods.

In addition to this general letter of support, we are also attaching a list of support signatures from individual residents of The Marlow. We look forward to continuing our discussion with Presidio Bay Ventures and working with them as they construct their new project at 1567 California Street over the coming years.

Sincerely,

Marlow Owners Association

Arielle Rahmani, CCAM
Portfolio Manager

MARLOW OWNERS' ASSOCIATION

C/O TITAN MANAGEMENT GROUP • P.O. BOX 77045 • SAN FRANCISCO • CA • 94107

415.777.5200 PHONE • 415.777.5220 FAX



MARLOW

MARLOW OWNERS ASSOCIATION
1567 California Street Development Support Signatures Page

Name	Unit #
1. S. Markel-Fox	809
2. H. Mostofi	802
3. J. FOX	809
4. EDILMA CON HOWE	605
5. Russ Hampton	407
6. Mimi CHAN	
7. David Yungo	
8. Moe Jamil	205
9. Annie Yung Jamil	205
10. Joseph Williams II	
11. Huiying Li	410
12. Christine Wong	608
13. Allison	
14. John Akhla	
15. [Signature]	211
16. James V. [Signature]	207

Name	Unit #
15. [Signature]	
16. [Signature]	211
17. Nelson Wu	208
18. Daniel Kennedy	702
19. Elean or	801
20. ALAN CHAN	
22. Mike Scheinert	311
23. Anne Hoeg	311
24. Chris Cui	608
25. Cynthia Lin	509
26. [Signature]	409
27.	
28.	
29.	
30.	
31.	

LETTER OF SUPPORT FOR NEW DEVELOPMENT AT 1567 CALIFORNIA STREET

Case Number: 2014-0284E (1567 California St. 1436-14498 Polk St)

Merchant Name: ONE HALF - DAN BLACKWELDER

Merchant Address: 1837 POLK ST

Lana Russell (CPC)

San Francisco Planning Department

1650 Mission Street, Suite 400

San Francisco, CA 94103

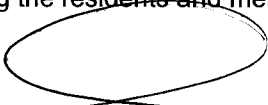
I am a small business owner located approximately _____ blocks away from the proposed mixed-use development to be constructed at 1567 California Street. After meeting with representatives of the developer, Presidio Bay Ventures, I have seen the proposed design and understand that the project is to contain approximately 47-50 residential units combined with a ground floor space dedicated towards retail and dining purposes.

I would like to express my unconditional support for this development. As a business owner, I feel that this project will prove extremely beneficial for the neighborhood by contributing the following:

1. Additional 100-150 residents of varying demographics who will contribute to the diversity and makeup of the Polk Street.
2. Increased pedestrian traffic from both the residents of the project and visitors from other neighborhoods who are likely to provide increased business.
3. Reduction of loitering and vagrancy that will improve the safety of the prominent Polk and California intersection and encourage more visitors to our businesses.
4. Promotion of small and local businesses via the retail ground floor and the introduction of new dining options.
5. Retail 'arcade' that will encourage pedestrians to congregate around indoor/outdoor spaces and create a positive atmosphere.
6. 10 additional bicycle parking spaces on Polk Street and 1 car share space available for public use.
7. Large living wall facing California Street that will serve as the "gateway" between Middle and Lower Polk Street. This will give tourists on the cable car a reason to slow down and stop to patronize Polk Street businesses. Currently the cable car quickly passes through without slowing down.

I look forward to welcoming the residents and merchants of 1567 California to the community.

Sincerely,



Phone Number:

Email:

CC: MOED

SF Planning Director

Supervisor Christensen

Kevin Guy, SF Planning

Planning Commission

Handwritten mark

LETTER OF SUPPORT FOR NEW DEVELOPMENT AT 1567 CALIFORNIA STREET

Case Number: 2014-0284E (1567 California St 1436-1498 Polk Street)

Resident Name: JEN JENDE

Resident Address: 2134 VAN NESS AVE.

Lana Russell (CPC)

SF Planning Department

1650 Mission Street Suite 400

San Francisco, CA 94103

I am a current District Three resident and live approximately ____ blocks away from the proposed mixed-use development to be constructed at 1567 California Street. After meeting with representatives of the developer, Presidio Bay Ventures, I have seen the proposed design and understand that the project is to contain approximately 47-50 residential units combined with a ground floor space dedicated towards retail and dining purposes.

I would like to express my unconditional support for this development. As a resident, I feel that this project will prove extremely beneficial for the neighborhood by contributing the following:

1. Additional 100-150 residents of varying demographics who will contribute to the diversity and makeup of the Polk Street.
2. Increased housing stock that will help alleviate rising prices by bringing more units onto the market
3. Increased pedestrian traffic from both the residents of the project and visitors from other neighborhoods who patronize the project's retail stores.
4. Reduction of loitering and vagrancy that will improve the safety of the prominent Polk and California intersection.
5. Promotion of small and local businesses via the retail ground floor and the introduction of new dining options.
6. Retail 'arcade' that will encourage pedestrians to congregate around indoor/outdoor spaces and create a positive atmosphere.
7. 10 additional bicycle parking spaces on Polk Street and 1 car share space available for public use.
8. Large living wall facing California Street that will serve as the "gateway" between Middle and Lower Polk Street. This will give tourists on the cable car a reason to slow down and stop to patronize Polk Street businesses. Currently the cable car quickly passes through without slowing down.

I look forward to welcoming the residents and merchants of 1567 California to the community.

Sincerely,

Handwritten signature of Jen Jende

Phone Number: 415 834-0300

Email: J.J@EUXINDUJN.COM

CC: MOED

SF Planning Director

SF Planning Commission

Planner Kevin Guy

Supervisor Julie Christensen

LETTER OF SUPPORT FOR NEW DEVELOPMENT AT 1567 CALIFORNIA STREET

Case Number: 2014-0284E (1567 California St. 1436-14498 Polk St)

Merchant Name: BARLOW LAM

Merchant Address: 1611 Jackson St, San Francisco Calif 94109

Lana Russell (CPC)

San Francisco Planning Department

1650 Mission Street, Suite 400

San Francisco, CA 94103

I am a small business owner located approximately 3 blocks away from the proposed mixed-use development to be constructed at 1567 California Street. After meeting with representatives of the developer, Presidio Bay Ventures, I have seen the proposed design and understand that the project is to contain approximately 47-50 residential units combined with a ground floor space dedicated towards retail and dining purposes.

I would like to express my unconditional support for this development. As a business owner, I feel that this project will prove extremely beneficial for the neighborhood by contributing the following:

1. Additional 100-150 residents of varying demographics who will contribute to the diversity and makeup of the Polk Street.
2. Increased pedestrian traffic from both the residents of the project and visitors from other neighborhoods who are likely to provide increased business.
3. Reduction of loitering and vagrancy that will improve the safety of the prominent Polk and California intersection and encourage more visitors to our businesses.
4. Promotion of small and local businesses via the retail ground floor and the introduction of new dining options.
5. Retail 'arcade' that will encourage pedestrians to congregate around indoor/outdoor spaces and create a positive atmosphere.
6. 10 additional bicycle parking spaces on Polk Street and 1 car share space available for public use.
7. Large living wall facing California Street that will serve as the "gateway" between Middle and Lower Polk Street. This will give tourists on the cable car a reason to slow down and stop to patronize Polk Street businesses. Currently the cable car quickly passes through without slowing down.

I look forward to welcoming the residents and merchants of 1567 California to the community.

Sincerely,

Retha Barlow

Phone Number: 415 977 1107

Email: barlowairbar@yahoo.com

CC: MOED

SF Planning Director

Supervisor Christensen

Kevin Guy, SF Planning

Planning Commission

LETTER OF SUPPORT FOR NEW DEVELOPMENT AT 1567 CALIFORNIA STREET

Case Number: 2014-0284E (1567 California St. 1436-14498 Polk St)

Merchant Name: Picnic

Merchant Address: 1808 Polk St SF, CA

Lana Russell (CPC)

San Francisco Planning Department

1650 Mission Street, Suite 400

San Francisco, CA 94103


I am a small business owner located approximately _____ blocks away from the proposed mixed-use development to be constructed at 1567 California Street. After meeting with representatives of the developer, Presidio Bay Ventures, I have seen the proposed design and understand that the project is to contain approximately 47-50 residential units combined with a ground floor space dedicated towards retail and dining purposes.

I would like to express my unconditional support for this development. As a business owner, I feel that this project will prove extremely beneficial for the neighborhood by contributing the following:

1. Additional 100-150 residents of varying demographics who will contribute to the diversity and makeup of the Polk Street.
2. Increased pedestrian traffic from both the residents of the project and visitors from other neighborhoods who are likely to provide increased business.
3. Reduction of loitering and vagrancy that will improve the safety of the prominent Polk and California intersection and encourage more visitors to our businesses.
4. Promotion of small and local businesses via the retail ground floor and the introduction of new dining options.
5. Retail 'arcade' that will encourage pedestrians to congregate around indoor/outdoor spaces and create a positive atmosphere.
6. 10 additional bicycle parking spaces on Polk Street and 1 car share space available for public use.
7. Large living wall facing California Street that will serve as the "gateway" between Middle and Lower Polk Street. This will give tourists on the cable car a reason to slow down and stop to patronize Polk Street businesses. Currently the cable car quickly passes through without slowing down.

I look forward to welcoming the residents and merchants of 1567 California to the community.

Sincerely,



Phone Number: 415 346 6556

Email:

CC: MOED

SF Planning Director

Supervisor Christensen

Kevin Guy, SF Planning

Planning Commission

LETTER OF SUPPORT FOR NEW DEVELOPMENT AT 1567 CALIFORNIA STREET

Case Number: 2014-0284E (1567 California St. 1436-14498 Polk St)

Merchant Name: The UPS Store 0051

Merchant Address: 1819 Polk St S.F. CA 94109

Lana Russell (CPC)

San Francisco Planning Department

1650 Mission Street, Suite 400

San Francisco, CA 94103

I am a small business owner located approximately _____ blocks away from the proposed mixed-use development to be constructed at 1567 California Street. After meeting with representatives of the developer, Presidio Bay Ventures, I have seen the proposed design and understand that the project is to contain approximately 47-50 residential units combined with a ground floor space dedicated towards retail and dining purposes.

I would like to express my unconditional support for this development. As a business owner, I feel that this project will prove extremely beneficial for the neighborhood by contributing the following:

1. Additional 100-150 residents of varying demographics who will contribute to the diversity and makeup of the Polk Street.
2. Increased pedestrian traffic from both the residents of the project and visitors from other neighborhoods who are likely to provide increased business.
3. Reduction of loitering and vagrancy that will improve the safety of the prominent Polk and California intersection and encourage more visitors to our businesses.
4. Promotion of small and local businesses via the retail ground floor and the introduction of new dining options.
5. Retail 'arcade' that will encourage pedestrians to congregate around indoor/outdoor spaces and create a positive atmosphere.
6. 10 additional bicycle parking spaces on Polk Street and 1 car share space available for public use.
7. Large living wall facing California Street that will serve as the "gateway" between Middle and Lower Polk Street. This will give tourists on the cable car a reason to slow down and stop to patronize Polk Street businesses. Currently the cable car quickly passes through without slowing down.

I look forward to welcoming the residents and merchants of 1567 California to the community.

Sincerely,



Phone Number: 415 441 9954

Email: store0051@theupsstore.com

CC: MOED

Kevin Guy, SF Planning

SF Planning Director

Planning Commission

Supervisor Christensen

LETTER OF SUPPORT FOR NEW DEVELOPMENT AT 1567 CALIFORNIA STREET

Merchant Name: uforia studios

Merchant Address: 1561 California St

To the Planning Commission:

I am a small business owner located approximately beside from the proposed mixed-use development to be constructed at 1567 California Street. After meeting with representatives of the developer, Presidio Bay Ventures, I have seen the proposed design and understand that the project is to contain approximately 47-50 residential units combined with a ground floor space dedicated towards retail and dining purposes.

I would like to express my unconditional support for this development. As a business owner, I feel that this project will prove extremely beneficial for the neighborhood by contributing the following:

1. Additional 100-150 residents of varying demographics who will contribute to the diversity and makeup of the Polk Street.
2. Increased pedestrian traffic from both the residents of the project and visitors from other neighborhoods who are likely to provide increased business.
3. Reduction of loitering and vagrancy that will improve the safety of the prominent Polk and California intersection and encourage more visitors to our businesses.
4. Promotion of small and local businesses via the retail ground floor and the introduction of new dining options.
5. Retail 'arcade' that will encourage pedestrians to congregate around indoor/outdoor spaces and create a positive atmosphere.
6. 10 additional bicycle parking spaces on Polk Street and 1 car share space available for public use.
7. Large living wall facing California Street that will serve as the "gateway" between Middle and Lower Polk Street. This will give tourists on the cable car a reason to slow down and stop to patronize Polk Street businesses. Currently the cable car quickly passes through without slowing down.

I look forward to welcoming the residents and merchants of 1567 California to the community.

Sincerely,



Phone Number: 650-329-8794

Email: sarah@uforiastudios.com

• I elect not to provide this information.

LETTER OF SUPPORT FOR NEW DEVELOPMENT AT 1567 CALIFORNIA STREET

Organization Name: Lower Polk Business Collaborative

Authorized Representative: Yahya Awadalla – President of the LPBC

To the Planning Commission:

We represent the mission and interests of the Lower Polk Business Collaborative, which is a registered association of merchants located along the Lower Polk Street corridor. The project sponsor (Presidio Bay Ventures) gave their first formal presentation on the proposed mixed-use project at 1567 California to our organization on September 24th, 2014. Subsequently, our organization was in communication with the project sponsor over email, phone calls, individual meetings and eventually a second formal presentation on April 29th, 2015.

Please accept this letter on behalf of the Lower Polk Business Collaborative as a commitment to our unconditional support for this project and the many benefits that we believe it will bring to Lower Polk's merchants, residents and long-term vitality. The project sponsor has been responsive to all of LPBC's questions and concerns regarding the project, and has taken proactive steps to help alleviate some of the neighborhood issues specific to us, as merchants, that we have been trying to address with every new project.

In particular, we would like to highlight certain aspects of the project that we are especially in support of:

1. Additional 100-150 residents of varying demographics who will contribute to the diversity and makeup of the Polk Street.
2. Increased pedestrian traffic from both the residents of the project and visitors from other neighborhoods who are likely to provide increased business to the Lower Polk merchants.
3. Introduction of increased parking (located underground) that will help replace some of the spaces that have been removed as a result of the recently introduced bicycle lanes.
4. Inclusion of all necessary infrastructure (utility, mechanical, HVAC) to serve a wide variety of potential merchants. This help prevent downtime following project completion.
5. Curation of small retail suites that will encourage local businesses
6. Reduction of loitering and vagrancy at that intersection that will improve the safety of the prominent Polk and California intersection and encourage more visitors to our businesses.
7. Retail 'arcade' that will encourage pedestrians to congregate around indoor/outdoor spaces and create a positive atmosphere.
8. 14 additional bicycle parking spaces on Polk Street and car share space available for public use that will help Lower Polk become a 'destination' area for visitors from different neighborhoods.

We look forward to welcoming the residents and merchants of 1567 California to the community.

Sincerely,



Date:

On behalf of the Lower Polk Business Collaborative

Claudine Asbagh
San Francisco Planning Commission Department

December 15, 2015

Regarding: 1567 California Street new building project

Thank you for sending me the drawings regarding the 1567 California Street new building project.

Since I am not able to attend the hearing on December 17, 2015, I am sending this email to ENSURE that my strong objections to these variances are on record. As the landowner directly adjacent to the proposed project, I feel it is very important that the planning commission be made aware of my main concerns. As I have noted to you in previous messages, my main concerns are:

1. Lack of front setback and lack of back yard space
2. Loss of privacy / security issues as a result of windows overlooking my property
3. Impact of lack of adequate parking
4. Sunlight / shadow study

1. The new building on California Street should have the front set back and should have a back yard.

My property is at 1561 California St. in San Francisco, which is right next to the 1567 California Street property. As you can see, the front of my building is set back 30 feet from the street and we also have a back yard. This was the city's requirement when we built the building from an empty lot back in 1975. I believe the city required us to have the front set back 30 feet because of the empty lot at 1567 California Street (which is next to my property and now the proposed new building project) and to have a back yard because of the back yards on the properties next to my property.

My building is a one story building. If the 80 feet high new project at 1567 California St. is built next to my building without the front set back from the street and without a back yard, my building will be hidden with no natural sunlight and fresh air in the front and back. Therefore, I request that the city's planning staff for the 1567 California Street new building project also follow the required ordinance to have the front of the new building on California Street set back 30 feet and also have a back yard as I followed the ordinance in 1975.

It should be noted that my building will be the only building on this block that has an actual setback from the street. I clearly feel my property has been unfairly discriminated against and that my property value will be severely impacted.

These requested variances will change the character of my neighborhood and have a direct impact on the value of my property. I see this as unfairly favoring the new builder while not providing any benefit to the existing landlords on the street.

2. No windows facing my building on California Street

I request that the side of the new building next to my building on California Street should not have any windows facing my building. This is not only to protect my airspace right but also to prevent tenants from throwing trash onto the roof of my building. More importantly, I want to prevent people from coming onto the roof of my building through their windows.

As I mentioned, the new structure will significantly overwhelm my existing building. I strongly request that the building design be modified such that there are no or minimal windows on the side that overlooks my property. This will provide some degree of privacy and security/safety for my building and will allow the use of my backyard without feeling like any activity there is on display. At a minimum, the design of the new building should be required to provide some degree of privacy for my property.

3. Lack of adequate parking

The design of this building as indicated in the environmental overview indicates that there will be less than half the number of parking spaces as required for the proposed population. That means that a substantial number of cars will be forced to find additional street parking on a regular basis. This will have a direct impact on the quality of life for existing residents / patrons in the area. I strongly urge that the plans be changed to increase available parking, either by adding more parking or by utilizing the already existing parking lot.

4. Impact of structure on sunlight / shadow on my property

Despite the environmental report's representation that building an 80 feet high building right on my property line will not adversely impact the amount of sunlight on public space, the analysis did not seriously consider the sunlight and shadow on my property. I strong request that you and the community carefully reexamine that portion of the report before accepting its input.

Thank you for considering my concerns.

I sincerely hope the city's planning staff will take into serious consideration my concerns and make no variance.

Please acknowledge that this email is received and delivered to the persons in charge of issuing the new building permit.

Ellen Ching



Building Owner

1561 California St.

San Francisco, CA 94109

ecclc@aol.com

SHEET METAL WORKERS' LOCAL UNION No. 104

WEST BAY DISPATCH OFFICE

PHONE (415) 621-2930



FAX (415) 621-2554

1939 MARKET STREET, SUITE A, SAN FRANCISCO, CA 94103

February 12, 2016

Claudine Asbagh
San Francisco Planning Department
1650 Mission Street, Ste. 400
San Francisco, CA 94103

RE: 1567 California Street, San Francisco (Case #2014.0284CUA/VAR)

Dear Ms. Asbagh,

The Sheet Metal Workers' Local Union No. 104 supports Presidio Bay Ventures' proposed project at 1075 Market Street, San Francisco. We believe this project will be a benefit to the local community and the City of San Francisco for the following reasons:

- Stimulate the local economy by providing local union construction jobs
- Employ State of California certified building trades apprentices during construction, ensuring career pathways for our local youth
- Enhance the neighborhood by replacing a former boxing gym and thrift store with a mixed-use residential project
- Provide much needed housing for the City by adding approximately 63 residential units

For all of the aforementioned reasons, we urge the Planning Commission to approve this project as proposed.

Respectfully,

A handwritten signature in blue ink, appearing to read "Danny Campbell".

Danny Campbell
Business Development Representative