



# SAN FRANCISCO PLANNING DEPARTMENT

---

## Executive Summary Large Project Authorization

HEARING DATE: MAY 12, 2016

*Date:* May 5, 2016  
*Case No.:* **2011.1300X**  
*Project Address:* **901 16th Street & 1200 17th Street**  
*Zoning:* UMU (Urban Mixed Use) District  
48-X & 68-X Height and Bulk District  
*Block/Lot:* 3949/002; 3950/001 (1200 17th Street Site)  
3949/001A, 001, 002; 3950/001 (901 16th Street Site)  
*Project Sponsor:* Potrero Partners, LLC (Attn: Josh Smith)  
445 Virginia Avenue  
San Mateo, CA 94402  
*Staff Contact:* Chris Townes – (415) 575-9195  
[chris.townes@sfgov.org](mailto:chris.townes@sfgov.org)  
*Recommendation:* **Approval with Conditions**

1650 Mission St.  
Suite 400  
San Francisco,  
CA 94103-2479

Reception:  
**415.558.6378**

Fax:  
**415.558.6409**

Planning  
Information:  
**415.558.6377**

### PROJECT DESCRIPTION

The project proposes to merge four lots into two lots totaling approximately 3.5 acres, to demolish a surface parking lot and approximately 105,000 square feet of existing warehouse (PDR) use (formerly occupied by Cor-O-Van Moving and Storage Company), and construct two four-to-six-story mixed-use buildings. The two buildings consist of: a North Building ("16th Street Building")--a 6-story, 68-foot tall, 402,943 gross square foot (gsf) mixed-use building with 260 dwelling unit, 20,318 square feet of retail, and 263 off-street parking spaces; and, a South Building ("17th Street Building")--a 4-story, 48-foot tall, 213,009 gsf mixed-use building with 135 dwelling unit, 4,650 square feet of retail and 125 off-street parking spaces. The project would construct a publicly-accessible pedestrian alley connecting 16th Street to 17th Street along the western property line. Combined, the two new buildings would construct a total of 395 dwelling units, 24,468 gross square feet of retail space, 388 off-street parking spaces, and 455 Class 1 and 52 Class 2 bicycle parking spaces. The project would retain an existing two-story, brick historic building. The project would also include 12,219 square feet of public open space, 27,268 square feet of common open space, and 4,950 square feet of private open space.

### SITE DESCRIPTION AND PRESENT USE

The project site consists of four adjacent lots in the lower Potrero Hill neighborhood. The two lots measure 380 feet by 237 feet. The approximately 3.5-acre project site is bounded by 16th Street to the north, Mississippi Street to the east, 17th Street to the south, and residential and industrial buildings to the west. The project site currently contains four existing buildings: two metal shed industrial warehouse buildings (102,500 square feet), a vacant brick office building (1,240 square feet), and a modular office



structure (5,750 square feet). The vacant brick building was originally constructed by the Pacific Rolling Mill Co. in 1926 to house the office functions of the company's steel fabricating operation at the site, while the modular office structure was until recently occupied by Cor-O-Van Moving and Storage Company. In total, the four existing buildings possess approximately 109,500 gross square feet. Surrounding the modular office structure is an open surface parking lot which is also used for access to the University of California, San Francisco (UCSF) storage and for fleet storage of the Cor-O-Van trucks and moving vans.

## **SURROUNDING PROPERTIES AND NEIGHBORHOOD**

The project site is located in the UMU Zoning District along a transitioning industrial corridor connecting the Mission neighborhood to Mission Bay within the Showplace Square/Potrero Hill Plan Area. Directly across 16<sup>th</sup> Street to the north, is a new 453-unit, mixed-use development project (referred to as "Potrero 1010") currently under construction consisting of two, 6-story buildings with residential units above flex and retail spaces located on a triangular site. Potrero 1010 includes a new 1-acre park and a public mid-block pedestrian mews lined with active uses. The adjacent properties to the west include two 3-story, live-work buildings, as well as, a vacant 1-story auto body shop. The properties to the south, across 17<sup>th</sup> Street include a 1-story industrial building, a vacant lot and a 2-story mixed-use building with residential above a place of entertainment ("Bottom of the Hill"). Properties to the east, across Mississippi Street include 1- to 2-story industrial buildings and a 3-story, 19-unit live/work building. The elevated Interstate 280 Freeway borders the site along the eastern edge. The surrounding neighborhood includes several other one- and two-story commercial and warehouse buildings. The broader neighborhood includes a variety of light industrial uses, Whole Foods grocery store, Live Oak School, California College of the Arts (CCA), residential, live/work, and mixed-use developments, restaurants, places of entertainment and Jackson Playground Park. The project site is located approximately one mile from the 4th and Townsend Street, and 22nd Street Caltrain stations, is located along the No. 55 bus route, and is blocks from the No. 8, 10, 19, and 22 bus lines and within a half-mile of the T-Third Street Muni line. Surrounding properties to the north, west, east, and south are all zoned UMU (Urban Mixed Use). Properties further northwest are zoned PDR-1-D (Production, Distribution, Repair- 1- Design) while properties further south are zoned RH-2 (Residential-House, Two Family).

## **ENVIRONMENTAL REVIEW**

On August 12, 2015, the Department published a Draft Environmental Impact Report ("DEIR") for the Project for public review (Case No. 2011.1300E). The DEIR was available for public comment until October 5, 2015. On October 1, 2015, the Commission conducted a duly noticed public hearing at a regularly scheduled meeting to solicit comments regarding the DEIR. On April 28, 2016, the Department published a Comments and Responses document, responding to comments made regarding the DEIR for the Project.

On May 12, 2016, the Commission will review the FEIR for certification to determine if the Project is adequate, accurate and complete.

On May 12, 2016, the Commission must adopt the CEQA Findings for the FEIR, prior to the approval of the Project.



## HEARING NOTIFICATION

| TYPE               | REQUIRED PERIOD | REQUIRED NOTICE DATE | ACTUAL NOTICE DATE | ACTUAL PERIOD |
|--------------------|-----------------|----------------------|--------------------|---------------|
| Classified News Ad | 20 days         | April 22, 2016       | April 22, 2016     | 20 days       |
| Posted Notice      | 20 days         | April 22, 2016       | April 22, 2016     | 20 days       |
| Mailed Notice      | 20 days         | April 22, 2016       | April 22, 2016     | 20 days       |

The proposal also requires a Section 312-neighborhood notification which was conducted in conjunction with the required hearing notification for the Large Project Authorization.

## PUBLIC COMMENT

To date, the Department has received (15) letters of support for the Project from individuals, as well as from the Dogpatch Merchants, Potrero Hill Neighborhood House, the Bookkeeper who cite support for the project's design and program, proposed density, and relationship to the 16<sup>th</sup> Street transit corridor. In opposition, the Department has received (25) community letters, 330 community signers of a petition, and 135 comments by community signers to the petition. Issues cited include concern for the project's height, scale, and massing, lack of adequate public open space, neighborhood compatibility, lack of area and diversity of commercial uses, traffic impacts, and inadequacy of EIR.

Copies of this correspondence have been included in the Commission packets

## ISSUES AND OTHER CONSIDERATIONS

- Large Project Authorization Exceptions: As part of the Large Project Authorization (LPA), the Commission may grant exceptions from certain Planning Code requirements for projects that exhibit outstanding overall design and are complementary to the design and values of the surrounding area. The proposed project requests modifications from: 1) Rear Yard (Planning Code Section 134); 2) Permitted Obstructions Over the Street (Planning Code Section 136); 3) Parking/Loading Entrance Width (Planning Code Section 145.1); 4) Off-Street Parking (Planning Code Section 151.1); 5) Off-Street Loading (Planning Code Section 152.1); 6) Horizontal Mass Reduction (Planning Code Section 270.1); and, 7) Flexible Units-Modification to the Accessory Use Provisions for Dwelling Units (Planning Code Section 329(D)(10) and 803.3(B)(1)(C)). Planning Department staff is generally in agreement with the proposed modifications given the overall project and its outstanding and compatible design with the exception of the exception for off-street parking. The Department does not support the exception to off-street parking, since the amount of off-street parking is contrary to the City's Transit First Policy.
- Interim Controls-Additional Design Standards for LPAs within the Showplace Square, Potrero Hill and Central Waterfront Area Plans: Board of Supervisor's File No. 151281, effective February, 19, 2016, established three additional interim design controls for Large Project Authorizations within the Showplace Square, Potrero Hill, and Central Waterfront Area Plans. These controls include the following:
  - An awareness of urban patterns, and harmonizes visual and physical relationships between existing buildings, streets, open spaces, natural features, and view corridors.



- An awareness of neighborhood scale and materials, and renders building facades with texture, detail and depth; and
- A modulation of buildings vertically and horizontally, with rooftops and facades designed to be seen from multiple vantage points.

In accordance with this new legislation, the Project has been evaluated in relation to the controls and determined to be in conformance.

- **Inclusionary Affordable Housing:** The project has elected the on-site affordable housing alternative, identified in Planning Code Section 415.6 and 419.3 for the 901 16<sup>th</sup> Street Building and payment of an in-lieu fee for the 1200 17<sup>th</sup> Street Building. The entire project is located within the UMU Zoning District. 901 16<sup>th</sup> Building is located within the 68-X Height and Bulk District and is subject to the Tier B Affordable Housing Program Requirements, which requires 16% of the total number of units to be designated as part of the inclusionary affordable housing program. 1200 17<sup>th</sup> Street Building is located within the 48-X Height and Bulk District and is subject to the Tier A Affordable Housing Program Requirements, which requires 14.4% of the total number of units to be designated as part of the inclusionary affordable housing program or an Affordable Housing Fee based on 23% of the total number of units. The 901 16<sup>th</sup> Street Building contains 260 dwelling units and the Project Sponsor will fulfill this requirement by providing 42 affordable units on-site, which will be available for rent. The 1200 17<sup>th</sup> Street Building contains 135 dwelling units and the Project Sponsor will fulfill this requirement by providing 42 affordable units on-site, which will be available for rent. As part of the project, the Project Sponsor has entered into a Costa-Hawkins Agreement with the City.

## **REQUIRED COMMISSION ACTION**

In order for the project to proceed, the Commission must grant a Large Project Authorization pursuant to Planning Code Section 329 to allow the new construction of a two, four-to-six-story mixed-use buildings with a total of 395 dwelling units, 24,468 gross square feet of retail space, and 388 off-street parking spaces, and to allow exceptions to the requirements for rear yard (Planning Code Section 134), permitted obstructions over the street (Planning Code Section 136), parking/loading entrance width (Planning Code Section 145.1), off-street parking (Planning Code Section 151.1), off-street loading (Planning Code Section 152.1), horizontal mass reduction (Planning Code Section 270.1), and flexible units-modification of the accessory use provisions for dwelling units (Planning Code Section 329(D)(10) and 803.3(B)(1)(C)).

## **BASIS FOR RECOMMENDATION**

The Department believes this project is approvable for the following reasons:

- The Project complies with the applicable requirements of the Planning Code.
- The Project is consistent with the objectives and policies of the General Plan and the Potrero Hill Area Plan
- The Project is located within the UMU zoning district where residential and ground floor commercial uses are principally permitted.
- The Project produces a new mixed-use residential development with significant site updates, including landscaping and common open space.



- The Project is compatible with and respects the existing neighborhood character, and provides an appropriate massing and scale.
- The Project complies with the First Source Hiring Program.
- The Project adds 395 new dwelling units to the City's housing stock.
- The Project will fully utilize the Eastern Neighborhoods Area Plan controls, and will pay the appropriate development impact fees.

|  |
|--|
| <b>RECOMMENDATION:</b> <b>Approval with Conditions</b> |
|--|

**Attachments:**

Draft Motion- Large Project Authorization  
Parcel Map  
Sanborn Map  
Zoning Map  
Height & Bulk Map  
Aerial Photograph  
Site Photos  
Major Projects Map  
Project Sponsor Submittal  
Architectural Drawings  
Public Correspondence  
Community Plan Exemption



Attachment Checklist:

- |   |   |
|---|---|
| <input checked="" type="checkbox"/> Executive Summary           | <input checked="" type="checkbox"/> Project sponsor submittal   |
| <input checked="" type="checkbox"/> Draft Motion                | Drawings: <u>Existing Conditions</u>  |
| <input checked="" type="checkbox"/> Environmental Determination | <input checked="" type="checkbox"/> Check for legibility  |
| <input checked="" type="checkbox"/> Major Projects Map          | Drawings: <u>Proposed Project</u>   |
| <input checked="" type="checkbox"/> Height & Bulk Map           | <input checked="" type="checkbox"/> Check for legibility  |
| <input checked="" type="checkbox"/> Parcel Map                  | 3-D Renderings (new construction or significant addition)   |
| <input checked="" type="checkbox"/> Sanborn Map                 | <input checked="" type="checkbox"/> Check for legibility  |
| <input checked="" type="checkbox"/> Aerial Photo                | <input type="checkbox"/> Wireless Telecommunications Materials  |
| <input checked="" type="checkbox"/> Context Photos              | <input type="checkbox"/> Health Dept. review of RF levels   |
| <input checked="" type="checkbox"/> Site Photos                 | <input type="checkbox"/> RF Report  |
| <input checked="" type="checkbox"/> Public Correspondence       | <input type="checkbox"/> Community Meeting Notice   |
|   | <input checked="" type="checkbox"/> Housing Documents   |
|   | <input checked="" type="checkbox"/> Inclusionary Affordable Housing Program: Affidavit for Compliance |

Exhibits above marked with an "X" are included in this packet

\_\_\_\_\_CT\_\_\_\_\_  
Planner's Initials





# SAN FRANCISCO PLANNING DEPARTMENT

---

## Planning Commission Motion No. XXXXX

HEARING DATE: May 12, 2016

*Hearing Date:* May 12, 2016  
*Case No.:* 2011.1300E  
*Project Address:* 901 16<sup>th</sup> Street and 1200 17<sup>th</sup> Street  
*Zoning:* UMU (Urban Mixed Use) Zoning District  
48-X Height and Bulk District (1200 17<sup>th</sup> Street Site);  
68-X Height and Bulk District (901 16<sup>th</sup> Street Site)  
*Block/Lot:* 3949/001, 001A, 002, & 3950/001  
*Project Sponsor:* Josh Smith for Potrero Partners, LLC – (650) 348-3232  
jsmith@waldendevlopment.com  
*Staff Contact:* Chris Townes – (415) 575-9195  
[christopher.townes@sfgov.org](mailto:christopher.townes@sfgov.org)

1650 Mission St.  
Suite 400  
San Francisco,  
CA 94103-2479

Reception:  
**415.558.6378**

Fax:  
**415.558.6409**

Planning  
Information:  
**415.558.6377**

ADOPTING ENVIRONMENTAL FINDINGS PURSUANT TO THE CALIFORNIA ENVIRONMENTAL QUALITY ACT, INCLUDING FINDINGS OF FACT, FINDINGS REGARDING SIGNIFICANT IMPACTS AND SIGNIFICANT AND UNAVOIDABLE IMPACTS, EVALUATION OF MITIGATION MEASURES AND ALTERNATIVES, AND A STATEMENT OF OVERRIDING CONSIDERATIONS RELATED TO APPROVALS FOR THE PROJECT, LOCATED AT 901 16<sup>TH</sup> STREET AND 1200 17<sup>TH</sup> STREET, TO MERGE FOUR LOTS INTO TWO LOTS, DEMOLISH TWO WAREHOUSES AND A MODULAR OFFICE STRUCTURE, PRESERVE THE BRICK OFFICE BUILDING, AND CONSTRUCT TWO NEW MIXED USE BUILDINGS ON SITE. THE "16<sup>TH</sup> STREET BUILDING" AT 901 16<sup>TH</sup> STREET WOULD CONSIST OF A NEW SIX-STORY, APPROXIMATELY 402,943 GROSS SQUARE FOOT RESIDENTIAL MIXED USE BUILDING WITH 260 DWELLING UNITS AND 20,138 GROSS SQUARE FEET OF RETAIL ON THE NORTHERN LOT. THE "17<sup>TH</sup> STREET BUILDING" AT 1200 17<sup>TH</sup> STREET WOULD CONSIST OF A NEW FOUR-STORY, APPROXIMATELY 213,509 GROSS SQUARE FEET RESIDENTIAL MIXED USE BUILDING WITH 135 DWELLING UNITS AND 4,650 GROSS SQUARE FEET ON THE SOUTHERN LOT. THE HISTORIC BRICK OFFICE BUILDING WOULD BE REHABILITATED FOR RETAIL OR RESTAURANT USE. COMBINED, THE TWO NEW BUILDINGS WOULD CONTAIN A TOTAL OF 395 DWELLING UNITS AND APPROXIMATELY 24,968 GROSS SQUARE FEET OF RETAIL SPACE, WITH A TOTAL OF 389 VEHICULAR PARKING SPACES, 455 OFF-STREET BICYCLE PARKING SPACES, AND APPROXIMATELY 14,669 SQUARE FEET OF PUBLIC OPEN SPACE, 33,149 SQUARE FEET OF COMMON OPEN SPACE SHARED BY PROJECT OCCUPANTS, AND 3,114 SQUARE FEET OF OPEN SPACE PRIVATE TO UNITS.

### PREAMBLE

On June 19, 2014, Potrero Partners, LLC (Attn: Josh Smith) (hereinafter "Project Sponsor"), filed Application No. 2011.1300EX (hereinafter "Application") with the Planning Department (hereinafter "Department") for a Large Project Authorization to construct one six-story building and one four-story building, referred to as the "16<sup>th</sup> Street" and "17<sup>th</sup> Street" Buildings



(approximately 616,452 gross square feet and 395 dwelling units total) with ground floor retail and open space at 901 16<sup>th</sup> Street and 1200 17<sup>th</sup> Street (Block 3949/Lots: 001, 001A, 002, and Block 3950/Lots 001) in San Francisco, California.

On August 12, 2015, the Department published a Draft Environmental Impact Report (“DEIR”) for the Project for public review (Case No. 2011.1300E). The DEIR was available for public comment until October 5, 2015. On October 1, 2015, the Commission conducted a duly noticed public hearing at a regularly scheduled meeting to solicit comments regarding the DEIR. On April 28, 2016, the Department published a Comments and Responses document, responding to comments made regarding the DEIR for the Project.

On May 12, 2016, the Commission certified the FEIR for the Project as adequate, accurate and complete.

On May 12, 2016, at a duly noticed public hearing at a regularly scheduled meeting, the Commission adopted findings, including a statement of overriding considerations and a Mitigation, Monitoring and Reporting Program.

MOVED, that the Commission hereby adopts the Project findings required by the California Environmental Quality Act, attached hereto as Attachment A including a statement of overriding considerations and adopts the Mitigation, Monitoring and Reporting Program, included as Exhibit 1 to Attachment B.

I hereby certify that the foregoing Motion was ADOPTED by the Planning Commission at its regular meeting of May 12, 2016.

Jonas Ionin  
Commission Secretary

AYES:

NOES:

ABSENT:

EXCUSED:

ACTION: Adoption of CEQA Findings



## Attachment A

# California Environmental Quality Act Findings

### PREAMBLE

In determining to approve the project described in Section I, below, (the "Project"), the San Francisco Planning Commission (the "Commission") makes and adopts the following findings of fact and decisions regarding the Project description and objectives, significant impacts, significant and unavoidable impacts, mitigation measures and alternatives, and a statement of overriding considerations, based on substantial evidence in the whole record of this proceeding and pursuant to the California Environmental Quality Act, California Public Resources Code Section 21000 et seq. ("CEQA"), particularly Section 21081 and 21081.5, the Guidelines for Implementation of CEQA, 14 California Code of Regulations Section 15000 et seq. ("CEQA Guidelines"), Section 15091 through 15093, and Chapter 31 of the San Francisco Administrative Code ("Chapter 31"). The Commission adopts these findings in conjunction with the Approval Actions described in Section I(c), below, as required by CEQA.

These findings are organized as follows:

**Section I** provides a description of the proposed project at 901 16th Street / 1200 17th Street, the environmental review process for the Project, the City approval actions to be taken, and the location and custodian of the record.

**Section II** lists the Project's less-than-significant impacts that do not require mitigation.

**Section III** identifies potentially significant impacts that can be avoided or reduced to less-than-significant levels through mitigation and describes the disposition of the mitigation measures.

**Section IV** identifies significant project-specific or cumulative impacts that would not be eliminated or reduced to a less-than-significant level and describes any applicable mitigation measures as well as the disposition of the mitigation measures. The Final EIR identified mitigation measures to address these impacts, but implementation of the mitigation measures will not reduce the impacts to a less than significant level.

Sections III and IV set forth findings as to the mitigation measures proposed in the Final EIR. (The Draft EIR and the Comments and Responses document together comprise the Final EIR, or "FEIR.") Attachment B to the Planning Commission Motion contains the Mitigation Monitoring and Reporting Program ("MMRP"), which provides a table setting forth each mitigation measure listed in the Final Environmental Impact Report that is required to reduce a significant adverse impact.

**Section V** identifies the project alternatives that were analyzed in the EIR and discusses the reasons for their rejection.



**Section VI** sets forth the Planning Commission's Statement of Overriding Considerations pursuant to CEQA Guidelines Section 15093.

The MMRP for the mitigation measures that have been proposed for adoption is attached with these findings as **Attachment B** to this Motion. The MMRP is required by CEQA Section 21081.6 and CEQA Guidelines Section 15091. Attachment B provides a table setting forth each mitigation measure listed in the FEIR that is required to reduce a significant adverse impact. Attachment B also specifies the agency responsible for implementation of each measure and establishes monitoring actions and a monitoring schedule. The full text of the mitigation measures is set forth in Attachment B.

These findings are based upon substantial evidence in the entire record before the Commission. The references set forth in these findings to certain pages or sections of the Draft Environmental Impact Report ("Draft EIR" or "DEIR") or the Comments and Responses document ("C&R") in the Final EIR are for ease of reference and are not intended to provide an exhaustive list of the evidence relied upon for these findings.

## **I. PROJECT DESCRIPTION AND PROCEDURAL BACKGROUND**

### **A. Project Description**

The Project Site consists of four adjacent lots in the lower Potrero Hill neighborhood (Assessor's block/lot: 3949/001, 001A, 002, and 3950/001). The approximately 3.5-acre Project Site is bounded by 16th Street to the north, Mississippi Street to the east, 17th Street to the south, and residential and industrial buildings to the west. The Project Site currently contains four existing buildings: two metal shed industrial warehouse buildings, a vacant brick office building, and a modular office structure. The vacant brick building was originally constructed by the Pacific Rolling Mill Co. in 1926 to house the office functions of the company's steel fabricating operation at the site, while the modular office structure is currently occupied by Cor-O-Van Moving and Storage Company. In total, the four existing buildings on the Project Site amount to approximately 109,500 gsf of building space. Surrounding the modular office structure is an open surface parking lot which is also used for access to the University of California, San Francisco (UCSF) storage and for fleet storage of the Cor-O-Van trucks and moving vans. The Project Site is within the Urban Mixed-Use (UMU) Zoning District. Per the San Francisco General Plan (General Plan), UMU is a land use designation intended to promote a vibrant mix of uses while maintaining the characteristics of this formerly industrial-zoned area. The site is located within the Showplace Square/Potrero Plan Area of the Eastern Neighborhoods Rezoning and Area Plan.

The proposed Project would merge the four lots into two lots, demolish the two warehouses and the modular office structure, and preserve the brick office building. Two new buildings would be constructed on site. The "16th Street Building" at 901 16th Street would consist of a new six-story, 68-foot tall (excluding rooftop projections of up to 82 feet), approximately 402,943 gross square foot (gsf) residential mixed use building with 260 dwelling units and 20,318 gsf of retail on the northern lot. The "17th Street Building" at 1200 17th Street would consist of a new four-story 48-foot tall (excluding rooftop projections of up to 52 feet), approximately 213,509 gsf residential mixed use building with 135 dwelling units and 4,650 gsf of retail on the southern lot.

Additionally, the historic brick office building would be rehabilitated for retail or restaurant use, which would generally involve retaining and rehabilitating the outer walls and features and renovating the



interior non-historic improvements. The proposed Project would also construct a new publicly accessible pedestrian alley along the entirety of its western property line.

Combined, the two new buildings would contain a total of 395 dwelling units and 24,968 gsf of retail space, in addition to a total of 389 vehicular parking spaces and 455 off-street bicycle parking spaces. The proposed Project would include 14,669 square feet of public open space, 33,149 square feet of common open space shared by Project occupants, and 3,114 square feet of open space private to units.

## **B. Project Objectives**

The Project Sponsor has developed the following objectives for the proposed Project:

- ▶ Redevelop a large underutilized site into a development with a mix of ground floor retail uses along 16th Street and 17th Street, residential dwelling units, and substantial open space amenities.
- ▶ Create a mixed-use project consistent with the Urban Mixed Use (UMU) zoning and the Showplace Square/Potrero Area Plan's policies that encourage a mix of land uses by providing both residential uses and community-serving retail uses on the site.
- ▶ Build a substantial number of residential dwelling units on the site to contribute to the City's General Plan Housing Element goals and ABAG's Regional Housing Needs Allocation for the City and County of San Francisco.
- ▶ Create a project that is consistent with the site's 48-X and 68-X height and bulk districts and is compatible with existing and contemplated development in the immediate vicinity.
- ▶ Incorporate open space for the use of project residents in an amount equal to or greater than required by the UMU zoning.
- ▶ Preserve and integrate the historic brick office building into the development, while removing the obsolete metal shed warehouses.
- ▶ Develop a financially feasible project capable of providing a market-based return on investment and sufficient to satisfy both equity capital investment and debt financing providers.

## **C. Project Approvals**

The Project requires the following Planning Commission approvals:

- ▶ Planning Commission Certification of the EIR
- ▶ Findings of General Plan and Priority Policies consistency
- ▶ Large Project Authorization, which includes exceptions to the following Planning Code standards:
  - Planning Code Section 134 for the required rear yard
  - Planning Code Section 152.1 for the required loading zones



- Planning Code Section 151.1 for the off-street parking
- Planning Code Section 145.1 for the parking/loading entrance width
- Planning Code Section 136 for the projecting bay dimension

***Actions by Other City Departments and State Agencies***

- ▶ Demolition, grading, building and occupancy permits (Department of Building Inspection)
- ▶ Approval of Color Curb Program for all proposed changes in curb cuts, parking and loading zones, and Class 2 bicycle parking, as well as all crosswalk markings and pedestrian signage required (San Francisco Municipal Transportation Agency)
- ▶ Approval of Lot Merger and Condominium Map to merge and re-subdivide the separate lots that comprise the Project Site and the sidewalk widening plans (San Francisco Department of Public Works)
- ▶ Approval of Site Mitigation Plan and Enhanced Ventilation Plan, as well as Soil Management Plan, Air Monitoring Plan, and Dust Control Plan for construction-period activities (San Francisco Department of Public Health)
- ▶ Issuance of permits for installation and operation of emergency generator (Bay Area Air Quality Management District)

**D. Environmental Review**

The Project is within the Eastern Neighborhoods Area Plan area, the environmental impacts of which were examined in the Eastern Neighborhoods Program EIR (Eastern Neighborhoods PEIR). The Planning Commission (hereafter referred to as "Commission") certified the Eastern Neighborhoods PEIR on August 7, 2008.

Section 15183 of the CEQA Guidelines provides an exemption from environmental review for projects that are consistent with the development density established by existing zoning, community plan, or general plan policies for which an EIR has been certified, except as may be necessary to examine whether an Project-specific effects are peculiar to the Project or Project Site. Under this exemption, examination of environmental effects shall be limited to those effects that: a) are peculiar to the Project or parcel on which the Project would be located; b) were not analyzed as significant effects in the prior EIR for the underlying zoning or plan; c) are potentially significant off-site or cumulative impacts that were not discussed in the underlying EIR; or d) were previously identified as significant effects in the underlying EIR, but that have been determined to have a more severe adverse impact than that discussed in the underlying EIR.

Because this Project is within the Showplace Square/Potrero Plan Area, a community plan exemption ("CPE") Checklist was prepared for the Project to analyze whether it would result in peculiar, Project-specific environmental effects that were not sufficiently examined in the Eastern Neighborhoods PEIR. The CPE Checklist (Appendix A to the Draft EIR) concluded that, with the exception of transportation and circulation and historic architectural resources the proposed Project would not result in any new



significant environmental impacts or impacts of greater severity than were analyzed in the Eastern Neighborhoods PEIR.

Thus, the Department determined that a focused Environmental Impact Report (hereinafter “EIR”) should be prepared and published a NOP with a CPE Checklist under the Eastern Neighborhoods PEIR on February 11, 2015. Topics analyzed in the EIR were Transportation and Circulation and Historic Architectural Resources.

On August 12, 2015, the Department published the Draft Environmental Impact Report (hereinafter “DEIR”) and provided public notice in a newspaper of general circulation of the availability of the DEIR for public review and comment and of the date and time of the Planning Commission public hearing on the DEIR; this notice was mailed to the Department’s list of persons requesting such notice.

Notices of availability of the DEIR and of the date and time of the public hearing were posted near the Project Site by the Project Sponsor on August 12, 2015.

On August 12, 2015, copies of the DEIR were mailed or otherwise delivered to a list of persons requesting it, to those noted on the distribution list in the DEIR, to adjacent property owners, and to government agencies, the latter both directly and through the State Clearinghouse.

Notice of Completion was filed with the State Secretary of Resources via the State Clearinghouse on August 12, 2015.

The Commission held a duly advertised public hearing on the DEIR on September 17, 2015, at which opportunity for public comment was given, and public comment was received on the DEIR. The period for commenting on the EIR ended on September 28, 2015.

The Department prepared responses to comments on environmental issues received during the 45 day public review period for the DEIR, prepared revisions to the text of the DEIR in response to comments received or based on additional information that became available during the public review period, and corrected clerical errors in the DEIR. This material was presented in a Responses to Comments document, published on April 28, 2016, distributed to the Commission and all parties who commented on the DEIR, and made available to others upon request at the Department.

A Final Environmental Impact Report (hereinafter “FEIR”) has been prepared by the Department, consisting of the DEIR, any consultations and comments received during the review process, any additional information that became available, and the Responses to Comments document all as required by law. The CPE Checklist is included as Appendix A to the DEIR and is incorporated by reference thereto.

Project EIR files have been made available for review by the Commission and the public. These files are available for public review at the Department at 1650 Mission Street, Suite 400, and are part of the record before the Commission.

On May 12, 2016, the Commission reviewed and considered the FEIR and found that the contents of said report and the procedures through which the FEIR was prepared, publicized, and reviewed comply with the provisions of CEQA, the CEQA Guidelines, and Chapter 31 of the San Francisco Administrative Code. The FEIR was certified by the Commission on May 12, 2016 by adoption of its Motion No. XXXXX.



## **E. Content and Location of Record**

The record upon which all findings and determinations related to the adoption of the proposed Project are based include the following:

- The FEIR, and all documents referenced in or relied upon by the FEIR, including the CPE Checklist prepared under the Eastern Neighborhoods PEIR;
- All information (including written evidence and testimony) provided by City staff to the Planning Commission relating to the FEIR, the proposed approvals and entitlements, the Project, and the alternatives set forth in the FEIR;
- All information (including written evidence and testimony) presented to the Planning Commission by the environmental consultant and subconsultants who prepared the FEIR, or incorporated into reports presented to the Planning Commission;
- All information (including written evidence and testimony) presented to the City from other public agencies relating to the project or the FEIR;
- All applications, letters, testimony, and presentations presented to the City by the Project Sponsor and its consultants in connection with the Project;
- All information (including written evidence and testimony) presented at any public hearing or workshop related to the Project and the EIR;
- The MMRP; and,
- All other documents comprising the record pursuant to Public Resources Code Section 21167.6(e).

The public hearing transcripts and audio files, a copy of all letters regarding the FEIR received during the public review period, the administrative record, and background documentation for the FEIR are located at the Planning Department, 1650 Mission Street, 4th Floor, San Francisco. The Planning Department, Jonas P. Ionin, is the custodian of these documents and materials.

## **F. Findings about Environmental Impacts and Mitigation Measures**

The following Sections II, III and IV set forth the Commission's findings about the FEIR's determinations regarding significant environmental impacts and the mitigation measures proposed to address them. These findings provide the written analysis and conclusions of the Commission regarding the environmental impacts of the Project and the mitigation measures included as part of the FEIR and adopted by the Commission as part of the Project. These findings do not attempt to describe the full analysis of each environmental impact contained in the FEIR. Instead, a full explanation of these environmental findings and conclusions can be found in the FEIR, and these findings hereby incorporate by reference the discussion and analysis in the FEIR supporting the determination regarding the project impact and mitigation measures designed to address those impacts. In making these findings, the Commission ratifies, adopts and incorporates in these findings the determinations and conclusions of the FEIR relating to environmental impacts and mitigation measures, except to the extent any such



determinations and conclusions are specifically and expressly modified by these findings, and relies upon them as substantial evidence supporting these findings.

In making these findings, the Commission has considered the opinions of staff and experts, other agencies, and members of the public. The Commission finds that (i) the determination of significance thresholds is a judgment decision within the discretion of the City and County of San Francisco; (ii) the significance thresholds used in the FEIR are supported by substantial evidence in the record, including the expert opinion of the FEIR preparers and City staff; and (iii) the significance thresholds used in the FEIR provide reasonable and appropriate means of assessing the significance of the adverse environmental effects of the Project. Thus, although, as a legal matter, the Commission is not bound by the significance determinations in the FEIR (see Public Resources Code, Section 21082.2, subdivision (e)), the Commission finds them persuasive and hereby adopts them as its own.

As set forth below, the Commission adopts and incorporates the applicable mitigation measures found in the Eastern Neighborhoods PEIR and all of the mitigation measures set forth in the Project FEIR, which are set forth in the attached MMRP, to reduce the significant and unavoidable impacts of the Project. The Commission intends to adopt the mitigation measures proposed in the FEIR as well as the applicable mitigation measures proposed in the Eastern Neighborhoods PEIR. Accordingly, in the event a mitigation measure recommended in the FEIR or Eastern Neighborhoods PEIR has inadvertently been omitted in these findings or the MMRP, such mitigation measure is hereby adopted and incorporated in the findings below by reference. In addition, in the event the language describing a mitigation measure set forth in these findings or the MMRP fails to accurately reflect the mitigation measures in the FEIR or Eastern Neighborhoods PEIR due to a clerical error, the language of the policies and implementation measures as set forth in the FEIR or Eastern Neighborhoods PEIR shall control. The impact numbers and mitigation measure numbers used in these findings reflect the information contained in the FEIR and Eastern Neighborhoods PEIR.

In Sections II, III and IV below, the same findings are made for a category of environmental impacts and mitigation measures. Rather than repeat the identical finding to address each and every significant effect and mitigation measure, the initial finding obviates the need for such repetition because in no instance is the Commission rejecting the conclusions of the FEIR or the Eastern Neighborhoods PEIR or the mitigation measures recommended in the FEIR or in the Eastern Neighborhoods PEIR for the Project.

These findings are based upon substantial evidence in the entire record before the Planning Commission. The references set forth in these findings to certain pages or sections of the EIR or responses to comments in the Final EIR are for ease of reference and are not intended to provide an exhaustive list of the evidence relied upon for these findings.

## **II. LESS-THAN-SIGNIFICANT IMPACTS**

The CPE Checklist (Appendix A to the DEIR) and/or the Final EIR found that implementation of the Project would result in less-than-significant impacts in the following environmental topic areas: Land Use and Land Use Planning (with the exception of significant and unavoidable impacts due to the cumulative loss of PDR (Production, Distribution, and Repair), as further discussed in Section IV herein); Aesthetics; Population and Housing; Cultural Resources; Greenhouse Gas Emissions; Wind and Shadow; Recreation; Utilities and Service Systems; Public Services; Biological Resources; Geology and Soils; Hydrology and Water Quality; Mineral and Energy Resources; Agriculture and Forest Resources.



Note: Senate Bill (SB) 743 became effective on January 1, 2014. Among other things, SB 743 added § 21099 to the Public Resources Code and eliminated the requirement to analyze aesthetics and parking impacts for certain urban infill projects under CEQA. The proposed Project meets the definition of a mixed-use residential project on an infill site within a transit priority area as specified by Public Resources Code § 21099. Accordingly, the FEIR did not discuss the topic of Aesthetics, which are no longer considered in determining the significance of the proposed Project's physical environmental effects under CEQA. The FEIR nonetheless provided visual simulations for informational purposes. Similarly, the FEIR included a discussion of parking for informational purposes. This information, however, did not relate to the significance determinations in the FEIR.

### **III. FINDINGS OF SIGNIFICANT IMPACTS THAT CAN BE AVOIDED OR REDUCED TO A LESS-THAN-SIGNIFICANT LEVEL THROUGH MITIGATION AND THE DISPOSITION OF THE MITIGATION MEASURES**

CEQA requires agencies to adopt mitigation measures that would avoid or substantially lessen a project's identified significant impacts or potential significant impacts if such measures are feasible. The findings in this section concern four potential impacts and mitigation measures proposed in the Eastern Neighborhoods PEIR and the CPE Checklist for this Project and four potential impacts and mitigation measures proposed in the FEIR. These mitigation measures are included in the MMRP. A copy of the MMRP is included as Attachment B to the Planning Commission Motion adopting these findings. The CPE Checklist found that one mitigation measure proposed in the Eastern Neighborhoods PEIR would be required for this Project to avoid any potential adverse effect from the proposed Project on accidentally discovered buried or submerged historical resources as defined in CEQA Guidelines Section 15064.5(a)(c). The CPE Checklist also found that four mitigation measures identified in the Eastern Neighborhoods PEIR would be required for this Project to eliminate or reduce to a less-than-significant level potential noise impacts, as set forth below. The CPE Checklist also found that two mitigation measures identified in the Eastern Neighborhoods PEIR would be required for this Project to eliminate or reduce to a less-than-significant level potential air quality impacts, as set forth below. The CPE Checklist also found that one mitigation measure identified in the Eastern Neighborhoods PEIR would be required for this Project to eliminate or reduce to a less-than-significant level potential hazardous materials impacts, as set forth below.

The Project Sponsor has agreed to implement the following mitigation measures to address the potential archeological resource, noise, air quality, hazardous materials impacts identified in the CPE Checklist and FEIR. As authorized by CEQA Section 21081 and CEQA Guidelines Section 15091, 15092, and 15093, based on substantial evidence in the whole record of this proceeding, the Planning Commission finds that, unless otherwise stated, the Project will be required to incorporate mitigation measures identified in the FEIR and the Eastern Neighborhoods PEIR into the Project to mitigate or to avoid significant or potentially significant environmental impacts. Except as otherwise noted, these mitigation measures will reduce or avoid the potentially significant impacts described in the Final EIR, and the Commission finds that these mitigation measures are feasible to implement and are within the responsibility and jurisdiction of the City and County of San Francisco to implement or enforce.

Additionally, the required mitigation measures are fully enforceable and are included as conditions of approval in the Planning Commission's Large Project Authorization under Planning Code Section 329 and also will be enforced through conditions of approval in any building permits issued for the Project by the San Francisco Department of Building Inspection. With the required mitigation measures, these



Project impacts would be avoided or reduced to a less-than-significant level. The Planning Commission finds that the mitigation measures presented in the MMRP are feasible and shall be adopted as conditions of project approval.

The following mitigation measures would be required to reduce cultural, paleontological and archeological impacts, noise impacts, air quality impacts, and hazards and hazardous materials impacts identified in the Eastern Neighborhoods PEIR and FEIR to a less-than-significant level:

**Impacts to Archeological Resources (Impact CPE-1) (FEIR, Appendix A, CPE Checklist, pp. 28-30)**

The proposed Project would include demolition of existing site buildings, excavation and soil disturbance, and construction activities, which has the potential to impact archeological resources that may be present within the Project site. Project Mitigation Measure M-CP-1 / Eastern Neighborhoods PEIR Mitigation Measure J-2 (Archeological Resources Testing) requires retention of an archaeological consultant, implementation of an Archeological Testing Program, and other measures to protect archeological resources. With implementation of Project Mitigation Measure M-CP-1 / Eastern Neighborhood PEIR Mitigation Measure J-2, Impact CPE-1 is reduced to a less than significant level.

**Impacts Associated with Construction Noise, Pile-Driving (Impact CPE-2) (FEIR, Appendix A, CPE Checklist, pp. 31-32)**

The proposed Project would include demolition, excavation and construction activities that are likely to include pile-driving activities and other particularly noisy construction procedures. Project Mitigation Measure M-NO-1 / Eastern Neighborhoods PEIR Mitigation Measure F-1 (Construction Noise, Pile-Driving) requires the use of drilled piles only (not pile-driving) unless pile-driving is absolutely necessary. With implementation of Project Mitigation Measure M-NO-1 / Eastern Neighborhood PEIR Mitigation Measure F-1, Impact CPE-2 is reduced to a less than significant level.

**Impacts Associated with Construction Noise (Impact CPE-3) (FEIR, Appendix A, CPE Checklist, pp. 31-32)**

The proposed Project would include demolition, excavation and construction activities that involve potentially noisy construction procedures in proximity to sensitive land uses. Project Mitigation Measure M-NO-2 / Eastern Neighborhoods PEIR Mitigation Measure F-2 (Construction Noise) requires the submittal of site-specific noise attenuation measures prior to commencing construction. With implementation of Project Mitigation Measure M-NO-2 / Eastern Neighborhood PEIR Mitigation Measure F-2, Impact CPE-3 is reduced to a less than significant level.

**Impacts Associated with Operation-Period Noise Impacts to Sensitive Uses (Impact CPE-4) (FEIR, Appendix A, CPE Checklist, pp. 32-35)**

The proposed Project would introduce sensitive residential land uses to existing noise-generating uses in the vicinity. Project Mitigation Measure M-NO-3 / Eastern Neighborhoods PEIR Mitigation Measure F-4 (Siting of Noise-Sensitive Uses) addresses the exposure of noise-sensitive uses to existing noise-generating uses in the vicinity. With implementation of Project Mitigation Measure M-NO-3 / Eastern Neighborhood PEIR Mitigation Measure F-4, Impact CPE-4 is reduced to a less than significant level.



**Impacts Associated with Generation of Operation-Period Noise Impacts to Sensitive Uses (Impact CPE-5) (FEIR, Appendix A, CPE Checklist, pp. 34-35)**

The proposed Project would include a backup diesel generator that is considered a noise-generating source. Project Mitigation Measure M-NO-4 / Eastern Neighborhoods PEIR Mitigation Measure F-5 (Siting of Noise-Generating Uses) addresses the potential impacts to sensitive uses associated with the generation of operation-period noise. With implementation of Project Mitigation Measure M-NO-4 / Eastern Neighborhood PEIR Mitigation Measure F-5, Impact CPE-5 is reduced to a less than significant level.

**Impacts Associated with Machinery Use During Construction Activities (Impact CPE-6) (FEIR, Appendix A, CPE Checklist, pp. 35-41)**

The proposed Project would include demolition, excavation and construction activities that are likely to require off- and on-road equipment that will increase emissions exhaust and air pollutants. Project Mitigation Measure M-AQ-1 / Eastern Neighborhoods PEIR Mitigation Measure G-1 (Construction Air Quality) requires engines to meet higher emissions standards on certain types of construction equipment, thereby reducing NOx emissions. With implementation of Project Mitigation Measure M-AQ-1 / Eastern Neighborhood PEIR Mitigation Measure G-1, Impact CPE-6 is reduced to a less than significant level.

**Impacts Associated with Potential Release of Hazardous Materials During Demolition (Impact HZ-1) (FEIR, Appendix A, CPE Checklist, pp. 57-58)**

The proposed Project would include demolition of existing site buildings that may contain hazardous building materials which could result in a public health risk. Project Mitigation Measure M-HZ-1 / Eastern Neighborhoods PEIR Mitigation Measure L-1 (Hazardous Building Materials) addresses the removal and disposition of potentially hazardous materials. With implementation of Project Mitigation Measure M-HZ-1 / Eastern Neighborhood PEIR Mitigation Measure L-1, Impact CPE-HZ-1 is reduced to a less than significant level.

**IV. SIGNIFICANT IMPACTS THAT CANNOT BE AVOIDED OR REDUCED TO A LESS-THAN-SIGNIFICANT LEVEL**

Based on substantial evidence in the whole record of these proceedings, the Planning Commission finds that there are significant project-specific and cumulative impacts that would not be eliminated or reduced to an insignificant level by the mitigation measures listed in the MMRP. The FEIR identifies two significant and unavoidable impacts on transportation and circulation, and one significant and unavoidable impact on land use and land use planning with respect to cumulative loss of PDR.

The Planning Commission further finds based on the analysis contained within the FEIR, other considerations in the record, and the significance criteria identified in the FEIR, that feasible mitigation measures are not available to reduce the significant Project impacts to less-than-significant levels, and thus those impacts remain significant and unavoidable. The Commission also finds that, although measures were considered in the FEIR that could reduce some significant impacts, certain measures, as described in this Section IV below, are infeasible for reasons set forth below, and therefore those impacts remain significant and unavoidable or potentially significant and unavoidable.



Thus, the following significant impacts on the environment, as reflected in the FEIR, are unavoidable. But, as more fully explained in Section VI, below, under Public Resources Code Section 21081(a)(3) and (b), and CEQA Guidelines 15091(a)(3), 15092(b)(2)(B), and 15093, the Planning Commission finds that these impacts are acceptable for the legal, environmental, economic, social, technological and other benefits of the Project. This finding is supported by substantial evidence in the record of this proceeding.

Additionally, on September 27, 2013, Governor Brown signed SB 743, which became effective on January 1, 2014. As noted in the Draft EIR on page IV.2, Public Resources Code Section 21099 requires that the State Office of Planning and Research (OPR) develop revisions to the CEQA Guidelines establishing criteria for determining the significance of transportation impacts of projects within transit priority areas that promote the “reduction of greenhouse gas emissions, the development of multimodal transportation networks, and a diversity of land uses.” The statute provides that, upon certification and adoption of the revised CEQA Guidelines by the Secretary of the Natural Resources Agency, “automobile delay, as described solely by level of service (LOS) or similar measures of vehicular capacity or traffic congestion shall not be considered a significant impact on the environment.” In other words, LOS or any other automobile delay metric more generally shall not be used as a significance threshold under CEQA.

Since publication of the DEIR for this Project on August 12, 2015, the California Office of Planning and Research (OPR) published for public review and comment a *Revised Proposal on Updates to the CEQA Guidelines on Evaluating Transportation Impacts in CEQA* (“proposed transportation impact guidelines”) in January 2016. OPR’s proposed transportation impact guidelines recommends that transportation impacts can be best measured using an alternative metric known as vehicle miles traveled (VMT). VMT measures the amount and distance that a project might cause people to drive, accounting for the number of passengers within a vehicle.

OPR’s proposed transportation impact guidelines provides substantial evidence that VMT is an appropriate standard to use in analyzing transportation impacts to protect environmental quality and a better indicator of greenhouse gas, air quality, and energy impacts than automobile delay. Acknowledging this, San Francisco Planning Commission Resolution 19579, adopted on March 3, 2016:

- Found that automobile delay, as described solely by LOS or similar measures of vehicular capacity or traffic congestion, shall no longer be considered a significant impact on the environment pursuant to CEQA, because it does not measure environmental impacts and therefore it does not protect environmental quality.
- Directed the Environmental Review Officer to remove automobile delay as a factor in determining significant impacts pursuant to CEQA for all guidelines, criteria, and list of exemptions, and to update the Transportation Impact Analysis Guidelines for Environmental Review and Categorical Exemptions from CEQA to reflect this change.
- Directed the Environmental Planning Division and Environmental Review Officer to replace automobile delay with VMT criteria which promote the reduction of greenhouse gas emissions, the development of multimodal transportation networks, and a diversity of land uses; and consistent with proposed and forthcoming changes to the CEQA Guidelines by OPR.



Planning Commission Resolution 19579, adopted on March 3, 2016, became effective immediately for all projects that have not received a CEQA determination and all projects that have previously received CEQA determinations, but require additional environmental analysis.

Under the VMT metric required by Planning Commission Resolution 19579, the Transportation and Circulation impacts would shift from significant to less-than-significant. As no Final CEQA determination for this Project was in place at the time Planning Commission Resolution 19579 went into effect, it would be permissible to rely only on the VMT metric in analyzing impacts of the Project. However, in recognition of the DEIR that had previously been circulated for comment, the newness of the VMT rather than LOS metric, and the fact that the public and decision-makers nonetheless may be interested in information pertaining to the automobile delay effects of this proposed Project and may desire that such information be provided as part of the environmental review process, the FEIR will continue to identify significant and unavoidable impacts to transportation and circulation based on automobile delay or traffic congestion.

Therefore, under Existing Plus Project conditions, the Project would contribute to the existing unacceptable operating conditions at three intersections (17th Street and Mississippi Street, Mariposa Street and Pennsylvania Street, and Mariposa Street and Mississippi Street). In addition, the Project (combined with past, present, and reasonably foreseeable future projects) would result in a considerable contribution to significant cumulative traffic impacts at four intersections (Mariposa Street and Mississippi Street, Mariposa Street and Pennsylvania Street, 17th Street and Mississippi Street, and 7th/16th/Mississippi Street). These impacts have been identified as significant, and no feasible mitigation measures have been identified to reduce these impacts to a less than significant level.

The FEIR identifies the following impacts for which no feasible mitigation measures were identified that would reduce these impacts to a less than significant level:

**Transportation and Circulation Impacts Associated with Level of Service at Three Study Intersections (Impact TR-2) (FEIR, IV.A.41-45)**

The proposed Project would cause a substantial increase in traffic that would substantially affect traffic operations at three of the 14 study intersections: 17th Street and Mississippi Street, Mariposa Street and Pennsylvania Street, and Mariposa Street and Mississippi Street. No feasible mitigation measures were identified that would reduce this impact to a less than significant level after consideration of several potential mitigation measures. The Project Sponsor has agreed to implement two mitigation measures, however the feasibility of each is not assured or assumed. The following mitigation measures were considered:

- **Mitigation Measure M-TR-2a (17th Street and Mississippi Street Signalization) (FEIR, IV.A.42-43):** This mitigation measure was evaluated to mitigate the poor operating conditions at the intersection of 17th Street and Mississippi Street. Under this mitigation measure, the Project Sponsor would be required to pay their fair share for the cost of design and of signalization or other similar mitigation to improve automobile delay at this intersection, as determined by the SFMTA. However, full funding of this measure has not been identified, so feasibility of implementation is not assured or assumed. Therefore, Impact TR-2 will remain significant and unavoidable with mitigation.



- **Mitigation Measure M-TR-2b (Mariposa Street and Pennsylvania Street Signalization) (FEIR, IV.A.43-44):** This mitigation measure was evaluated to mitigate the poor operating conditions at the intersection of Mariposa Street and Pennsylvania Street, and the Project Sponsor has agreed to pay their fair share for the cost of design and of signalization or other similar mitigation to improve automobile delay at this intersection, as determined by the SFMTA. However, full funding of this measure has not been identified, so feasibility of implementation is not assured or assumed. Therefore, Impact TR-2 will remain significant and unavoidable with mitigation.
- **Mitigation Measure M-TR-2c (Implement a Transportation Demand Management Plan) (FEIR, IV.A.44-45):** This mitigation measure was evaluated to mitigate the overall transportation and circulation impacts of the Project. The Project Sponsor has agreed to implement this mitigation measure, which requires preparation and implementation of a Transportation Demand Management Plan. However, this mitigation measure would not reduce volumes by the 50% required to reduce the impacts at the target intersections to a less than significant level. Therefore, Impact TR-2 will remain significant and unavoidable with mitigation.

Additionally, the Project FEIR identified a mitigation measure that would reduce impacts at the Mariposa and Mississippi Street intersection. However, the mitigation measure was found infeasible because it conflicts with SFMTA's goals and policies for the area as the considered improvements would conflict with the desired operation of this intersection. (FEIR, IV.A.43). Specifically, one option considered by SFMTA staff included the installation of turn pockets, but it was rejected because it did not improve intersection LOS to an acceptable level. Another option considered by SFMTA staff was the installation of a traffic signal. With signalization, the intersection would operate at LOS C during the Existing Plus Project weekday PM peak hour conditions. After review of this potential mitigation, SFMTA concluded that the existing all-way STOP sign-controlled intersection of Mariposa and Mississippi streets is not a desirable candidate for traffic signalization because the traffic patterns at this particular intersection are more effectively served by an all-way STOP control than by a traffic signal. The existing STOP sign on westbound Mariposa Street slows traffic on westbound Mariposa Street as it approaches Mississippi Street, where the land uses change from generally commercial to mostly residential. SFMTA does not want to encourage a substantial amount of through westbound movements on Mariposa Street west of Mississippi Street, which a traffic signal could encourage. Thus no feasible mitigation was identified for this intersection, and therefore Impact TR-2 will remain significant and unavoidable.

Therefore, no feasible mitigation measures were found to reduce the proposed Project's significant impact at the intersections of 17th Street and Mississippi Street, Mariposa Street and Pennsylvania Street, and Mariposa Street and Mississippi Street to less-than-significant levels, rendering Impact TR-2 significant and unavoidable.

**Transportation and Circulation Impacts Associated with Cumulative Level of Service at Four Study Intersections (Impact C-TR-2) (FEIR, IV.A.66-68)**

The proposed Project, combined with past, present, and reasonably foreseeable future projects, would contribute considerably to significant cumulative traffic impacts at four of the 14 study intersections: Mariposa Street and Mississippi Street, Mariposa Street and Pennsylvania Street, 17th Street and Mississippi Street, and 7th/16th/Mississippi Street. No feasible mitigation measures were identified that would reduce this impact to a less than significant level after consideration of several potential mitigation



measures. The Project Sponsor has agreed to implement two of the mitigation measures discussed above for Impact TR-2, however as noted the feasibility of each is not assured or assumed. Additionally, SFMTA has determined no improvements would be feasible at the already signalized 7th/16th/Mississippi Street intersection as additional or reconfigured lanes would conflict with goals for pedestrian and transit usage of this intersection. Therefore, no feasible mitigation measures were found to reduce the proposed Project's contribution to significant cumulative impacts at the intersections of Mariposa Street and Mississippi Street, Mariposa Street and Pennsylvania Street, 17th Street and Mississippi Street, and 7th/16th/Mississippi Street to less-than-significant levels, rendering Impact C-TR-2 significant and unavoidable.

**Land Use and Land Use Planning Impacts Associated with Loss of PDR (FEIR, S.3-4; Appendix A, CPE Checklist, pp. 25-26)**

The proposed Project would also contribute to a significant and unavoidable impact identified in the *Eastern Neighborhoods PEIR*. The *Eastern Neighborhoods PEIR* determined that adoption of the Eastern Neighborhoods Area Plans would result in an unavoidable significant impact on land use due to the cumulative loss of PDR (Production, Distribution, and Repair). While land use controls in Western SoMa were identified as possible mitigation, this was determined not to be feasible and would not be applicable to the proposed project in any case, as the proposed project is not located in that area. A Statement of Overriding Considerations was adopted by the City accepting this significant impact because retention of the PDR uses would conflict with planned growth of the area. The Project's proposed loss of 109,500 square feet of existing PDR uses represents a considerable contribution to the loss of the PDR space analyzed in the *Eastern Neighborhoods PEIR*, but would not result in significant impacts that were not identified or more severe impacts than were analyzed in the *Eastern Neighborhoods PEIR*. The findings and analysis of the *Eastern Neighborhoods PEIR* with respect to loss of PDR is hereby incorporated by reference.

## **V. EVALUATION OF PROJECT ALTERNATIVES**

### **A. Alternatives Analyzed in the FEIR**

This section describes the alternatives analyzed in the Project FEIR and the reasons for rejecting the alternatives as infeasible. CEQA mandates that an EIR evaluate a reasonable range of alternatives to the Project or the Project location that generally reduce or avoid potentially significant impacts of the Project. CEQA requires that every EIR also evaluate a "No Project" alternative. Alternatives provide a basis of comparison to the Project in terms of their significant impacts and their ability to meet project objectives. This comparative analysis is used to consider reasonable, potentially feasible options for minimizing environmental consequences of the Project.

The Planning Department considered a range of alternatives in Chapter 6 of the FEIR. The FEIR analyzed the No Project Alternative, the Reduced Density Alternative, and the Metal Shed Reuse Alternative. Each alternative is discussed and analyzed in these findings, in addition to being analyzed in Chapter 6 of the FEIR. The Planning Commission certifies that it has independently reviewed and considered the information on the alternatives provided in the FEIR and in the record. The FEIR reflects the Planning Commission's and the City's independent judgment as to the alternatives. The Planning Commission finds that the Project provides the best balance between satisfaction of Project objectives and mitigation of environmental impacts to the extent feasible, as described and analyzed in the FEIR.



## B. Reasons for Approving the Project

- ▶ To redevelop a large underutilized site into a development with a mix of ground floor retail uses along 16th Street and 17th Street, residential dwelling units, and substantial open space amenities.
- ▶ To create a mixed-use project consistent with the Urban Mixed Use (UMU) zoning and the Showplace Square/Potrero Area Plan's policies that encourage a mix of land uses by providing both residential uses and community-serving retail uses on the site.
- ▶ To build a substantial number of residential dwelling units on the site to contribute to the City's General Plan Housing Element goals and ABAG's Regional Housing Needs Allocation for the City and County of San Francisco.
- ▶ To create a project that is consistent with the site's 48-X and 68-X height and bulk districts and is compatible with existing and contemplated development in the immediate vicinity.
- ▶ To incorporate open space for the use of project residents in an amount equal to or greater than required by the UMU zoning.
- ▶ To preserve and integrate the historic brick office building into the development, while removing the obsolete metal shed warehouses.

## C. Evaluation of Project Alternatives

CEQA provides that alternatives analyzed in an EIR may be rejected if "specific economic, legal, social, technological, or other considerations, including provision of employment opportunities for highly trained workers, make infeasible . . . the project alternatives identified in the EIR." (CEQA Guidelines § 15091(a)(3).) The Commission has reviewed each of the alternatives to the Project as described in the FEIR that would reduce or avoid the impacts of the Project and finds that there is substantial evidence of specific economic, legal, social, technological and other considerations that make these Alternatives infeasible, for the reasons set forth below.

In making these determinations, the Planning Commission is aware that CEQA defines "feasibility" to mean "capable of being accomplished in a successful manner within a reasonable period of time, taking into account economic, environmental, social, legal, and technological factors." The Commission is also aware that under CEQA case law the concept of "feasibility" encompasses (i) the question of whether a particular alternative promotes the underlying goals and objectives of a project, and (ii) the question of whether an alternative is "desirable" from a policy standpoint to the extent that desirability is based on a reasonable balancing of the relevant economic, environmental, social, legal, and technological factors.

Three alternatives were considered as part of the FEIR's overall alternatives analysis, but ultimately rejected from detailed analysis. Those alternatives are as follows:

- **Off-site Alternative.** This alternative was rejected because the Project sponsor does not have control of another site that would be of sufficient size to develop a mixed-use project with the intensities and mix of uses that would be necessary to achieve most of the basic Project objectives.



- **Open Space Alternative.** An alternative which considers the development of exclusive open space on the site was not considered for further analysis as it would not meet most of the basic project objectives, the proposed Project exceeds the Planning Code open space requirements for the proposed development, the City does not own the Project site, and acquisition of the site for City open space is not within the City's open space acquisition priority list.
- **Medical Office and Residential Alternative.** The Project was originally proposed in 2011 with a medical office building along 16th Street and a mixed use residential building along 17th Street. The medical group has since moved forward with the medical office project at a different location and is no longer interested in this type of development at this site. An alternative with a medical office building would not substantially reduce Project impacts and was therefore rejected as an alternative.

The following alternatives were fully considered and compared in the FEIR:

### **1. No Project Alternative**

Under the No Project Alternative, the Project Site would remain in its existing condition and would not be redeveloped with a mix of residential, commercial, and open space uses. No open space would be developed within the site and no changes to surrounding loading or curb space would occur. The existing warehouse and office uses totaling approximately 109,500 square feet would continue operating at the site. The existing buildings would likely continue to remain in their current condition for the foreseeable future. Baseline conditions described in detail for each environmental topic in Chapter 4, Environmental Setting, Impacts and Mitigation Measures, would remain and none of the impacts associated with the Project would occur.

The Planning Commission rejects the No Project Alternative as infeasible because it would fail to meet the Project Objectives and the City's policy objectives for the following reasons:

- 1) The No Project Alternative would not meet any of the Project Sponsor's objectives;
- 2) The No Project Alternative would be inconsistent with key goals of the Eastern Neighborhood Plan with respect to housing production. With no new housing created here and no construction, the No Project Alternative would not increase the City's housing stock of both market rate and affordable housing, would not create new job opportunities for construction workers, and would not expand the City's property tax base.
- 3) The No Project Alternative would leave the Project Site physically unchanged, and thus would not achieve any of the objectives regarding the redevelopment of a large underutilized site (primarily consisting of obsolete warehouses and a surface parking lot), creation of a mixed-use project within the UMU District, contribution to regional housing needs, provision of affordable dwelling units, provision of publicly-accessible open space, and provision of new neighborhood services.

For the foregoing reasons, the Planning Commission rejects the No Project Alternative as infeasible.



## **2. Reduced Density Alternative**

The FEIR identified the Reduced Density Alternative as an environmentally superior alternative under the LOS analysis because it would reduce the project's significant impacts at four study intersections.

Under the Reduced Density Alternative, all existing on-site buildings (with the exception of the historic brick building) and surface pavements on the Project Site would be demolished and the site would be redeveloped with a mix of residential and commercial uses within two buildings. The configuration of the buildings would be similar to the configuration of the proposed Project, although compared to the proposed Project: 1) the courtyards would be expanded, reducing the footprint of the buildings; 2) there would be less commercial frontage, with locations toward the western end of the buildings, including the existing brick building, becoming residential amenities or lobby areas instead of commercial areas. A total of 273 residential units and 16,880 square feet of commercial uses would be developed, for a total building area of 561,625 gsf. This alternative would include 122 fewer residential units and 7,588 fewer square feet of commercial space compared to the proposed Project. Similar to the proposed Project, this alternative would feature a public pedestrian alley along the west side of the development with residences opening onto a mews and residential courtyards.

The Reduced Density Alternative would include underground residential parking garages in both the 16th Street Building and 17th Street Building with access via two driveways with 20-foot curb cuts from Mississippi Street. Class 1 bicycle parking would be included in the underground garages. Retail parking as well as additional bicycle parking would be provided at ground level in the 16th Street Building. Two off-street loading spaces would be provided with one being adjacent to the retail parking area and the other off of 17th Street (a 12-foot curb cut).

This alternative would eliminate some (but not all) of the Project-specific and cumulative traffic-related significant and unavoidable impacts of the proposed Project. Specifically, the Reduced Density Alternative would, under Existing Plus Project conditions, reduce the number of significantly-impacted intersections from three to one (at Mariposa Street and Pennsylvania Street), and under Cumulative Conditions, reduce the number of significantly-impacted intersections from four to two (7th/16th/Mississippi Street, and Mariposa Street and Pennsylvania Street).

Based on substantial evidence in the record, the Planning Commission rejects the Reduced Density Alternative as infeasible because it would fail to meet the Project Objectives and City policy objections for reasons including, but not limited to, the following:

- 1) The Reduced Density Alternative would limit the Project to 273 dwelling units; whereas the proposed Project would provide 395 units to the City's housing stock and maximize the creation of new residential units. The City's important policy objective as expressed in Policy 1.1 of the Housing Element of the General Plan and Policy 1.2.1 of the Showplace Square/Potrero Area Plan of the General Plan is to increase the housing stock whenever possible to address a shortage of housing in the City.
- 2) The Reduced Density Alternative would create a project that would not fully utilize this site for housing production, thereby not fully satisfying General Plan policies such as Housing Element Policies 1.1 and 1.4, among others. While the Reduced Density Alternative would ameliorate certain of the significant unavoidable impacts of the proposed Project, the alternative would



not create a project that is consistent with and enhances the existing scale and urban design character of the area or furthers the City's housing policies to create more housing, particularly affordable housing opportunities, and would not remove all significant unavailable impacts.

- 3) The Reduced Density Alternative would create a project with fewer housing units in an area well-served by transit, services and shopping and adjacent to employment opportunities which would then push demand for residential development to other sites in the City or the Bay Area. This would result in the Reduced Density Alternative not meeting, to the same degree as the Project, the City's *Strategies to Address Greenhouse Gas Emissions* or CEQA and the Bay Area Air Quality Management District's ("BAAQMD") requirements for GHG reductions, by not maximizing housing development in an area with abundant local and region-serving transit options.
- 4) The Reduced Density Alternative is also economically infeasible. Large development projects are capital-intensive and depend on obtaining financing from equity investors to cover a significant portion of the project's costs, obtain a construction loan for the bulk of construction costs, and provide significant costs out-of-pocket. Equity investors require a certain profit margin to finance development projects and must achieve established targets for their internal rate of return and return multiple on the investment. Because the Reduced Density Alternative would result in a project that is significantly smaller than the Project, and contains 122 fewer residential units, the total potential for generating revenue is lower while the construction cost per square foot is higher due to lower economies of scale and the impact of fixed project costs associated with development. The reduced unit count would not generate a sufficient economic return to obtain financing and allow development of the proposed Project and therefore would not be built.

The Project Sponsor provided the City a memorandum entitled "Financial Feasibility Analysis of 901 16th Street and 1200 17th Street Project" prepared by Seifel Consulting, Inc., which is included in the record. The memorandum concludes that the Reduced Density Alternative is not financially feasible because the development costs for the Reduced Density Alternative significantly exceed potential revenues, resulting in a negative developer margin or return. Specifically, implementation of the Reduced Density Alternative will result in total development costs of \$258,440,000 million and result in a total value of \$254,123,000 million, resulting in negative \$4,317,000 million net developer margin or return. In addition, the Reduced Density Alternative does not meet either of the return thresholds as measured by Yield On Cost or Return on Cost. Given the significant fixed development costs (such as property acquisition and site improvement costs), the lower number of units in the Reduced Density Alternative negatively impacts its financial viability, as there are fewer units over which these fixed development costs can be spread in comparison to the Project.

- 5) On March 3, 2016, the Planning Commission adopted Motion No. 19579 to no longer consider intersection level of service impacts as significant impacts under CEQA. Under this new policy, the Reduced Density Alternative would not avoid or reduce any significant impacts of the Proposed Project.

For the foregoing reasons, the Planning Commission rejects the Reduced Density Alternative as infeasible.



### **3. Metal Shed Reuse Alternative**

The FEIR identified the Metal Shed Reuse Alternative as an environmentally superior alternative because it would reduce the Proposed Project's contribution to the cumulative loss of PDR space identified in the Eastern Neighborhoods PEIR.

The Metal Shed Reuse Alternative would repurpose the existing warehouse buildings on the site and redevelop the existing parking lot and modular office building as follows. The existing metal shed warehouse building at 1200/1100 17th Street would be reused to include 46,957 square feet of artists' workspaces on two floors, 13,200 square feet of restaurant and retail space, and 8,366 square feet of public arts activity space. The warehouse building at 1210 17th Street/975 16th Street would also be retained but modified with windows and cutouts for light and air access and with new construction added above to four stories encompassing a total of 95 residential units and residential lobby and amenity areas. The Cor-O-Van modular office building and parking lot at 901 16th Street (the northeast corner of the site) would be developed with underground parking and a new five story mixed-use building and courtyard above encompassing 82 residential units and related lobby and amenity areas as well as 7,000 gsf of ground-level commercial space along 16th Street, for a total of 177 dwelling units.

This alternative would include 36,291 square feet of open space. A publically-accessible pedestrian alley would be provided cutting through the warehouse turned artist workshops at 1200 17th Street and continuing between the 975 16th Street warehouse turned residential building and the new mixed-use building at 901 16th Street. Additionally, off-street parking would be provided in a single basement-level garage with 123 residential parking spaces accessed via a driveway off of Mississippi Street. The size of the parking area would be limited by areas with existing structures to remain above. Three off-street loading spaces would be provided, including one adjacent to the basement garage ramp, utilizing the same curb cut. The other two loading spaces would be accessed via two 12-foot curb cuts off of 17th Street.

Under the Metal Shed Reuse Alternative, none of the Proposed Project's significant transportation and circulation impacts would be avoided, but the Project's impacts regarding the loss of PDR would be avoided.

The Planning Commission rejects the Metal Shed Reuse Alternative as infeasible because it would not reduce any of the significant unavoidable individual impacts of the proposed Project and it would not meet the Project Objectives or City policy objectives as well as the proposed Project, for reasons including, but not limited to, the following:

- 1) The Metal Shed Reuse Alternative would limit the Project to 177 dwelling units; whereas the proposed Project would provide 395 units to the City's housing stock. The City's important policy objective as expressed in Policy 1.1 of the Housing Element of the General Plan and Policy 1.2.1 of the Showplace Square/Potrero Area Plan of the General Plan is to increase the housing stock whenever possible to address a shortage of housing in the City.
- 2) The Metal Shed Reuse Alternative would not successfully address any of the significant and unavoidable traffic-related project- and cumulative-level impacts of the proposed Project, which are the only "significant and unavoidable" individual impacts of the Project.



- 3) In adopting the Showplace Square/Potrero Plan Area, the City rezoned formerly M-1 and M-2 zoned areas to either PDR zoning districts, designed to protect and accommodate new PDR uses, or to the UMU zoning district, designed to encourage housing development and mixed use structures. In adopting the Showplace Square/Potrero Plan Area, the City adopted overriding findings that the loss of PDR space and uses within the UMU district was an unavoidable but acceptable cumulative land use impact, and the Project's contribution to that cumulative impact is within the projections of the Showplace Square/Potrero Plan Area, without the need to reuse one of the metal sheds for PDR uses.
- 4) The Metal Shed Reuse Alternative would create a project with fewer housing units and significantly less neighborhood serving retail space than the proposed Project in an area well-served by transit, services and shopping and adjacent to employment opportunities which would then push demand for residential development to other sites in the City or the Bay Area. This would result in the Metal Shed Reuse Alternative not meeting, to the same degree as the Project, the City's *Strategies to Address Greenhouse Gas Emissions* or CEQA and the Bay Area Air Quality Management District's ("BAAQMD") requirements for a GHG reductions, by not maximizing mixed use housing development in an area with abundant local and region-serving transit options. This would result in the Metal Shed Reuse Alternative not meeting, to the same degree as the Project, the City's *Strategies to Address Greenhouse Gas Emissions* or CEQA and the Bay Area Air Quality Management District's ("BAAQMD") requirements for GHG reductions, by not maximizing housing development in an area with abundant local and region-serving transit options.
- 5) The Metal Shed Alternative retains much of the blank metal façades of the existing warehouses along 17<sup>th</sup> Street and Mississippi Street and does not incorporate many of the positive urban design features of the proposed Project, including a mid-block pedestrian alley along the western property line, walk-up stoop residential units along 17<sup>th</sup> Street and the pedestrian alley, and removal of incompatible elements surrounding the historic brick office building. In addition, approximately 48 residential units in the Metal Shed Alternative would have light and air exposure only onto small courtyards along the western property line, which would provide inferior unit exposure compared to the light and air exposure provided to courtyard units in the proposed Project by the proposed Project's much larger courtyards.
- 6) The Metal Shed Reuse Alternative is also economically infeasible. Large development projects are capital-intensive and depend on obtaining financing from equity investors to cover a significant portion of the Project's costs, obtain a construction loan for the bulk of construction costs, and provide significant costs out-of-pocket. Equity investors require a certain profit margin to finance development projects and must achieve established targets for their internal rate of return and return multiple on the investment. Because the Metal Shed Reuse Alternative would result in a project that is significantly smaller than the Project, and contains 218 fewer residential units, the total potential for generating revenue is lower while the construction cost per square foot is higher due to lower economies of scale and the impact of fixed project costs associated with development. The reduced unit count would not generate a sufficient economic return to obtain financing and allow development of the proposed Project and therefore would not be built.



The Project Sponsor provided the City a memorandum entitled "Financial Feasibility Analysis of 901 16th Street and 1200 17th Street Project" prepared by Seifel Consulting, Inc., which is included in the record. The memorandum concludes that the Metal Shed Reuse Alternative is not financially feasible because the development costs for the Metal Shed Reuse Alternative significantly exceed potential revenues, resulting in a negative developer margin or return. Specifically, implementation of the Metal Shed Reuse Alternative will result in total development costs of \$185,790,000 and result in a total value of \$190,090,000, resulting in only \$4,300,000 net developer margin or return. In addition, the Metal Shed Reuse Alternative does not meet either of the return thresholds as measured by Yield On Cost or Return on Cost. Given the significant fixed development costs (such as property acquisition and site improvement costs), the lower number of units and the high cost to rehabilitate the metal sheds in the Metal Shed Reuse Alternative negatively impacts its financial viability, as there are fewer units over which these fixed development costs can be spread in comparison to the Project and estimated PDR rent levels are relatively low compared to the rehabilitation costs.

For the foregoing reasons, the Planning Commission rejects the Metal Shed Reuse Alternative as infeasible.

## **VI. STATEMENT OF OVERRIDING CONSIDERATIONS**

The Planning Commission finds that, notwithstanding the imposition of all feasible mitigation measures, impacts related to Transportation and Circulation, when analyzed according to vehicle delay, and loss of PDR, will remain significant and unavoidable. Pursuant to CEQA section 21081 and CEQA Guidelines Section 15093, the Planning Commission hereby finds, after consideration of the Final EIR and the evidence in the record, and incorporating by reference the findings in the *Eastern Neighborhoods PEIR* regarding loss of PDR, that each of the specific overriding economic, legal, social, technological and other benefits of the Project as set forth below independently and collectively outweighs these significant and unavoidable impacts and is an overriding consideration warranting approval of the Project. Any one of the reasons for approval cited below is sufficient to justify approval of the Project. Thus, even if a court were to conclude that not every reason is supported by substantial evidence, the Commission will stand by its determination that each individual reason is sufficient. The substantial evidence supporting the various benefits can be found in the preceding findings, which are incorporated by reference into this Section, and in the documents found in the record, as defined in Section I.

On the basis of the above findings and the substantial evidence in the whole record of this proceeding, the Planning Commission specifically finds that there are significant benefits of the Project to support approval of the Project in spite of the unavoidable significant impacts, and therefore makes this Statement of Overriding Considerations. The Commission further finds that, as part of the process of obtaining Project approval, significant effects on the environment from implementation of the Project have been eliminated or substantially lessened where feasible. All mitigation measures proposed in the FEIR and MMRP are adopted as part of the Approval Actions described in Section I, above.

Additionally, the Planning Commission finds that, under a VMT analysis for transportation and circulation impacts as required by Planning Commission Resolution 19579, there would be no significant and unavoidable impacts from the Project, and all impacts would be either less-than-significant or less-than-significant with mitigation. The Commission further finds that, while the FEIR characterized the transportation and circulation impacts as significant and unavoidable, and properly analyzed the impacts



as such, the implementation of Planning Commission Resolution 19579 is one of the overriding considerations for the Commission's approval of the Project.

Furthermore, the Commission has determined that any remaining significant effects on the environment found to be unavoidable are acceptable due to the following specific overriding economic, technological, legal, social and other considerations.

The Project will have the following benefits:

1. The Project would add up to 395 dwelling units to the City's housing stock. The City's important policy objective as expressed in Policy 1.1 of the Housing Element of the General Plan and Policy 1.2.1 of the Showplace Square/Potrero Area Plan of the General Plan is to increase the housing stock whenever possible to address a shortage of housing in the City.
2. The Project would increase the stock of permanently affordable housing by creating approximately 42 units affordable to low-income households on-site and by contributing significant funds to the City's Affordable Housing Fund, as, required by the City's Affordable Inclusionary Housing Ordinance.
3. The Project Site is currently underused and the construction of up to 395 new housing units at this underutilized site will directly help to alleviate the City's housing shortage and lead to more affordable housing. A primary objective of the Eastern Neighborhood Area Plan is to increase housing locally through the build out of the plan area. The Project develops the Project Site in a manner envisioned by the Plan in its density and design.
4. The Project promotes a number of General Plan Objectives and Policies, including Housing Element Policy 1.1, which provides that "Future housing policy and planning efforts must take into account the diverse needs for housing;" and Policies 11.1, 11.3 and 11.6, which "Support and respect the diverse and distinct character of San Francisco's Neighborhoods." San Francisco's housing policies and programs should provide strategies that promote housing at each income level, and furthermore identify sub-groups, such as middle income and extremely low income households that require specific housing policy. In addition to planning for affordability, the City should plan for housing that serves a variety of household types and sizes." The Project will provide a mix of housing types at this location, including 53 studio units, 182 one-bedroom units, 146 two-bedroom units, and 14 three-bedroom units, increasing the diversity of housing types in this area of the City.
5. The Project adds nearly 25,000 gross square feet of neighborhood serving retail sales and service space in an area with a growing residential and workplace population, consistent with the site's Urban Mixed Use zoning.
6. The Project provides both publicly accessible and private open space in excess of the amounts required by the Planning Code.
7. The Project provides 455 Class 1 secure indoor bicycle parking spaces, significantly more than the minimum required by the Planning Code, and 52 Class 2 sidewalk bike racks, encouraging residents and visitors to access the site by bicycle.



8. The Project reduces the number of vehicular curb cuts around the site from eight to three, thereby minimizing conflicts between vehicles and pedestrians and bicyclists. No vehicular curb cuts are proposed along 16<sup>th</sup> Street, in conformance with the City's approved plans for a bus rapid transit line with a dedicated transit lane on 16<sup>th</sup> Street, or along 17<sup>th</sup> Street, where the City proposes to relocate Bicycle Route No. 40 with a dedicated Class II bike lane adjacent to the Project.
9. The Project will implement a Transportation Demand Management program to reduce trips by single occupant vehicles.
10. The Project meets the City's *Strategies to Address Greenhouse Gas Emissions* and the BAAQMD requirements for a GHG reductions by maximizing development on an infill site that is well-served by transit, services and shopping and is suited for dense residential development, where residents can commute and satisfy convenience needs without frequent use of a private automobile and is adjacent to employment opportunities, in an area with abundant local and region-serving transit options. The Project would leverage the site's location and proximity to transit by building a dense mixed use project that allows people to live and work close to transit sources.
11. The Project's innovative design furthers Housing Element Policy 11.1, which provides that "The City should continue to improve design review to ensure that the review process results in good design that complements existing character."
12. The Project promotes a number of Showplace Square/Potrero Area Plan Objectives and Policies, including Policies 1.2.1 and 1.2.2, which "In areas of Showplace/Potrero where housing and mixed use is encouraged, maximize development potential in keeping with neighborhood character;" Policies 2.1.1 and 2.1.3, which "Ensure that a significant percentage of new housing created in the Showplace/Potrero is affordable to people with a wide range of incomes;" and Policies 2.3.1 and 2.3.3, which "Require that a significant number of units in new developments have two or more bedrooms." As discussed in Paragraphs 2 and 4 above, the Project includes a mix of housing types, a substantial number of two-plus bedroom units, and complies with the Inclusionary Affordable Housing Program.
13. The Project would construct a development that is in keeping with the scale, massing and density of other structures in the immediate vicinity, with minimal effects on public views from uphill locations on Potrero Hill.
14. The Project rehabilitates the historic brick office building on 17<sup>th</sup> Street in a manner consistent with the Secretary of the Interior's Standards for Rehabilitation and removes and replaces the site's unsightly and obsolete non-historic metal shed warehouses.
15. The Conditions of Approval for the Project include all the mitigation and improvement measures that would mitigate the Project's potentially significant impact to insignificant levels, except for its impact on Transportation and Circulation.
16. The Project will create temporary construction jobs and permanent jobs in the retail sector. These jobs will provide employment opportunities for San Francisco residents, promote the City's role



as a commercial center, and provide additional payroll tax revenue to the City, providing direct and indirect economic benefits to the City.

17. The Project will substantially increase the assessed value of the Project Site, resulting in corresponding increases in tax revenue to the City.

Having considered the above, the Planning Commission finds that the benefits of the Project outweigh the unavoidable adverse environmental effects identified in the FEIR, and that those adverse environmental effects are therefore acceptable.



## Exhibit 2: Mitigation Monitoring and Reporting Program

| Adopted Mitigation Measures  | MONITORING AND REPORTING PROGRAM |   |  |                                     |                              |
|--|----------------------------------|---|--|-------------------------------------|------------------------------|
|  | Responsibility for Implementing  | Mitigation Schedule                             | Mitigation Action                              | Monitoring/Reporting Responsibility | Monitoring Schedule          |
| <b>MITIGATION MEASURES AGREED TO BY THE PROJECT SPONSOR (EIR)</b>  |                                  |   |  |                                     |                              |
| <i>TRANSPORTATION AND CIRCULATION</i>  |                                  |   |  |                                     |                              |
| <p><b>Mitigation Measure M-TR-2c: Implement a Transportation Demand Management Plan.</b></p> <p>The project applicant and subsequent property owners shall prepare and implement a TDM Plan with a goal of reducing estimated one-way vehicle trips by 10 (ten) percent compared to the projections within the project's Transportation Impact Study. Prior to final certificate of occupancy for any new building associated with the project, the project applicant shall submit a TDM Plan to the Planning Department staff.</p> <p>The project applicant is responsible for identifying the components of the TDM Plan that could reasonably be expected to achieve the reduction goal for the project, and for making good faith efforts to implement them. Components of the TDM Plan beyond Planning Code requirements could include, but are not limited to, education and marketing of transportation options; on-site safety strategies; subsidies for transportation options other than the single occupancy vehicle; providing additional car-share or bicycle parking; reducing the amount or restricting access to vehicular parking; unbundling vehicular parking from commercial tenants occupancy; and increasing the cost of vehicular parking.</p> <p>The TDM Plan shall include monitoring of person and vehicle trips traveling to and from the project site to determine the TDM Plan's effectiveness, as outlined below. The TDM Plan shall be adjusted based on the monitoring results if three consecutive monitoring results show that existing measures are not creating a trend toward meeting the reduction goal.</p> <p><i>TDM Plan Monitoring:</i> The project sponsor shall collect data and</p> | Project sponsor                  | Prior to issuance of a certificate of occupancy | Implement TDM measures as specified in M-TR-2c | Planning Department                 | Throughout project operation |



MONITORING AND REPORTING PROGRAM

| Adopted Mitigation Measures   | Responsibility<br>for<br>Implementing | Mitigation<br>Schedule | Mitigation<br>Action | Monitoring/<br>Reporting<br>Responsibility | Monitoring<br>Schedule |
|---|---------------------------------------|------------------------|----------------------|--|------------------------|
| <p>make monitoring reports available for review and approval by the Planning Department staff.</p> <p><u>Timing:</u> Monitoring data and reports shall be required to be submitted to Planning Department staff every two years for a period of eight years and every four years thereafter (referred to as reporting periods), until two consecutive reporting periods display the project has met the reduction goal. The first monitoring report is required one year after initial occupancy of either building. The timing may be modified by the Planning Department as needed to consolidate this requirement with other annual monitoring and/or reporting requirements for the project. Each trip count and survey (see below for definitions) shall be completed within 90 days following the end of the applicable reporting period. Each monitoring report shall be completed within 180 days following the applicable reporting period.</p> <p><u>Components:</u> The monitoring report, including trip counts and surveys, shall include the following components OR comparable alternative methodology and components as approved or provided by Planning Department staff:</p> <ul style="list-style-type: none"> <li>• Trip Count and Intercept Survey: Trip count and intercept survey of persons and vehicles arriving and leaving the building for no less than two days of the reporting period between 6:00 a.m. and 8:00 p.m. One day shall be a Tuesday, Wednesday, or Thursday, and another day shall be a Saturday.</li> <li>• Property Manager/Coordinator Survey: The project sponsor shall request in writing from Planning Department Staff a survey (online or paper) that shall be completed by property manager/coordinator to document which TDM Plan was implemented during the reporting period and obtain basic building information (e.g., percent unit occupancy, off-site parking utilization by occupants of the building, loading frequency, etc.). This survey shall be included in the monitoring report submitted to Planning</li> </ul> |                                       |                        |                      |  |                        |



| MONITORING AND REPORTING PROGRAM   |                                       |                        |                      |  |                        |
|--|---------------------------------------|------------------------|----------------------|--|------------------------|
| Adopted Mitigation Measures  | Responsibility<br>for<br>Implementing | Mitigation<br>Schedule | Mitigation<br>Action | Monitoring/<br>Reporting<br>Responsibility | Monitoring<br>Schedule |
| <p>Department staff.</p> <ul style="list-style-type: none"> <li>Travel Demand Information: The above trip count and survey information shall be able to provide travel demand analysis characteristics as outlined in the SF Guidelines in effect at the time of the survey.</li> <li>Assistance and Confidentiality: Planning Department staff will assist the TDM Coordinator on questions regarding the components of the monitoring report and shall ensure that the identity of individual survey responders is protected.</li> </ul> |                                       |                        |                      |  |                        |

#### IMPROVEMENT MEASURES AGREED TO BY THE PROJECT SPONSOR (EIR)

##### TRANSPORTATION AND CIRCULATION

|  |                 |   |  |                            |  |
|--|-----------------|---|--|----------------------------|--|
| <p><b>Improvement Measure I-TR-5a: On-site Bicycle Safety Strategies.</b></p> <p>To reduce potential conflicts with cyclists, the project sponsor should implement all of the following safety measures:</p> <ul style="list-style-type: none"> <li>Restrict commercial loading at the off-street loading dock to hours outside of the weekday AM and PM peak periods.</li> <li>Provide on-site signage (stop sign; sign indicating to drivers to be aware of pedestrians and bicyclists; and a no left turn sign, if warranted by SFMTA after further study as identified in Improvement I-TR-5b) at the exit point for the new parking garages and off-street loading dock. Deploy staff at the loading dock while commercial vehicles are being received in order to minimize the disruption to other modes of transportation.</li> </ul> | Project sponsor | Prior to issuance of a certificate of occupancy | Implement on-site bicycle measures as specified in I-TR-5a | Planning Department, SFMTA | Signage considered complete once installed, staff deployment for receiving commercial vehicles ongoing throughout operations |
|--|-----------------|---|--|----------------------------|--|



| Adopted Mitigation Measures  | MONITORING AND REPORTING PROGRAM      |  |   |  |  |
|--|---------------------------------------|--|---|--|--|
|  | Responsibility<br>for<br>Implementing | Mitigation<br>Schedule   | Mitigation<br>Action  | Monitoring/<br>Reporting<br>Responsibility | Monitoring<br>Schedule   |
| <p><b>Improvement Measure I-TR-5b: On-street Bicycle Safety Strategies.</b></p> <p>To reduce potential conflicts with cyclists and turning vehicles accessing and leaving the project site, the project sponsor should coordinate with the San Francisco Municipal Transportation Agency (SFMTA) to determine whether the following would be appropriate:</p> <ul style="list-style-type: none"> <li>• Provide bicycle lane visibility improvements for drivers of vehicles exiting the new parking garages by designating the first 20 feet of curb space to the north of the off-street loading curb cut for the 16th Street Building as well as the first 20 feet of curb space to the north of the new parking garage curb cut for the 17th Street Building as red zones or for motorcycle parking or Class 2 bicycle space parking.</li> <li>• Provide bicycle lane visibility and transition improvements by providing colored pavement markings along Mississippi Street and dashed line markings at entrance points to the new parking garages, such as those described in the NACTO Urban Bikeway Design Guide.</li> <li>• If determined to be necessary by the SFMTA after a one-year observation period following initial occupancy of the proposed project, restrict northbound and southbound traffic from turning left along Mississippi Street mid-block between 16th and 17th Street by restriping it with double-yellow lines.</li> <li>• If determined to be necessary by the SFMTA after a one-year observation period following initial occupancy of the proposed project, restrict on-street commercial loading during the weekday AM and PM peak periods.</li> </ul> | Project sponsor                       | Prior to issuance of a certificate of occupancy, following a one-year observational period | Implement on-street bicycle measures as specified in I-TR-5b and coordinated with SFMTA | Planning Department, SFMTA                 | Considered complete once improvements are installed or determined by SFMTA not to be appropriate/necessary |



| Adopted Mitigation Measures   | MONITORING AND REPORTING PROGRAM |  |  |                                      |  |
|---|----------------------------------|--|--|--------------------------------------|--|
|   | Responsibility for Implementing  | Mitigation Schedule  | Mitigation Action  | Monitoring/ Reporting Responsibility | Monitoring Schedule  |
| <p><b>Improvement Measure I-TR-6: Off-street Loading Management.</b></p> <p>To minimize the potential for double parking due to potential shortage of available off-street or on-street commercial and passenger loading spaces, the project sponsor, property owner, or official designee of the development should implement all of the following measures:</p> <ul style="list-style-type: none"> <li>Identify a Loading Coordinator(s) for each new building. The Loading Coordinator is responsible for the implementation and ongoing operation of all other loading measures identified below, as well as those identified in Improvement Measures I-TR-5a and I-TR-5b: <ul style="list-style-type: none"> <li>Require residential move-in and move-out activities to be scheduled and coordinated.</li> <li>Require large vehicle commercial loading delivery (i.e., those lasting longer than 30 minutes and/or 45-foot-long vehicles) to be scheduled and coordinated.</li> <li>Discourage commercial vehicles and large residential move-in and move-out vehicles from double parking by advising the operators to return at a time when the off-street and on-street spaces are available for use.</li> </ul> </li> </ul> | Project sponsor                  | Prior to issuance of a certificate of occupancy            | Implement Loading Management as specified in I-TR-6      | Planning Department                  | Throughout project operation   |
| <p><b>Improvement Measure I-TR-8: Construction Management.</b></p> <p>The project sponsor should develop and, upon review and approval by the San Francisco Municipal Transportation Agency (SFMTA) and San Francisco Public Works, implement a Construction Management Plan (CMP), addressing transportation-related circulation, access, staging, and hours for deliveries.</p> <p>The CMP would disseminate appropriate information to contractors and affected agencies with respect to coordinating construction activities to minimize overall disruptions and ensure that overall circulation in the project area is maintained</p>  | Project sponsor                  | Prior to issuance of a demolition and construction permits | Implement construction management as specified in I-TR-8 | SFMTA, Department of Public Works    | Considered complete once improvements are installed or determined by SFMTA not to be appropriate/necessary |



MONITORING AND REPORTING PROGRAM

| Adopted Mitigation Measures   | Responsibility<br>for<br>Implementing | Mitigation<br>Schedule | Mitigation<br>Action | Monitoring/<br>Reporting<br>Responsibility | Monitoring<br>Schedule |
|---|---------------------------------------|------------------------|----------------------|--|------------------------|
| <p>to the extent possible, with particular focus on ensuring transit, pedestrian, and bicycle connectivity. The CMP would supplement and expand, rather than modify or supersede, any manual, regulations, or provisions set forth by the SFMTA, Public Works, or other City departments and agencies, and the California Department of Transportation. The CMP should include, but not be limited to, the following:</p> <ul style="list-style-type: none"> <li>• Management practices that include, but are not limited to, the following: <ul style="list-style-type: none"> <li>○ Identifying ways to reduce construction worker vehicle-trips through transportation demand management programs and methods to manage construction worker parking demands (e.g., recommending that construction companies encourage their workers to walk, cycle, rideshare or take transit to and from the construction site).</li> <li>○ Identifying best practices for accommodating pedestrians, such as temporary pedestrian wayfinding signage or temporary walkways.</li> <li>○ Identifying best practices for accommodating bicyclists and bicycle facilities such as bicycle wayfinding signage or temporary detours.</li> <li>○ Identify a route for construction-related trucks to utilize during construction. This route should follow 16th Street, 3rd Street, and Owens Street.</li> <li>○ Minimizing deliveries and trucks trips to the project site during peak hours (generally 7 AM to 9 AM and 4 PM to 6 PM, but may include other times during nearby event days) where feasible, and having the construction manager endeavor to efficiently schedule deliveries and truck trips to the project site when necessary during peak hours to minimize secondary effects to the surrounding transportation infrastructure.</li> </ul> </li> </ul> |                                       |                        |                      |  |                        |



| Adopted Mitigation Measures  | MONITORING AND REPORTING PROGRAM      |   |  |  |                              |
|--|---------------------------------------|---|--|--|------------------------------|
|  | Responsibility<br>for<br>Implementing | Mitigation<br>Schedule  | Mitigation<br>Action   | Monitoring/<br>Reporting<br>Responsibility | Monitoring<br>Schedule       |
| <ul style="list-style-type: none"> <li>Develop a public information plan to provide adjacent residents and businesses with regularly-updated information regarding project construction activities, peak construction vehicle activities, (e.g. concrete pours), travel lane closures, and other lane closures.</li> <li>As part of the CMP review, the project sponsor should consult with SFMTA to assist coordination of construction traffic management strategies as they relate to transit operations and the needs of other users adjacent to the project site. Construction traffic management strategies include having a construction management contact person, advertisement of the construction schedule to local businesses and schools, and encouragement of construction workers to carpool or use alternative modes of travel.</li> </ul>   |                                       |   |  |  |                              |
| <p><b>Improvement Measure I-TR-9: Queue Abatement.</b></p> <p>It should be the responsibility of the owner(s)/operator(s) of the 16th Street Building and the 17th Street Building off-street parking facility to ensure that recurring vehicle queues do not occur on the Mississippi Street public right-of-way fronting the subject property. A vehicle queue is defined as one or more vehicles (destined to the off-street parking facility) blocking any portion of the Mississippi Street public right-of-way fronting the subject property for a consecutive period of three minutes or longer on a daily or weekly basis.</p> <p>If a recurring queue occurs, the owner/operator of the parking facility should employ abatement methods as needed to abate the queue. Appropriate abatement methods will vary depending on the characteristics and causes of the recurring queue, as well as the characteristics of the parking facility, the street(s) to which the facility connects, and the associated land uses (if applicable).</p> <p>Suggested abatement methods include but are not limited to the following: redesign of facility to improve vehicle circulation</p> | Project sponsor                       | In the event that recurring queues occur at project driveways | Conduct queue monitoring and/or implement queue abatement methods as specified in I-TR-9 | Planning Department                        | Throughout project operation |



| Adopted Mitigation Measures  | MONITORING AND REPORTING PROGRAM      |                        |                      |  |                        |
|--|---------------------------------------|------------------------|----------------------|--|------------------------|
|  | Responsibility<br>for<br>Implementing | Mitigation<br>Schedule | Mitigation<br>Action | Monitoring/<br>Reporting<br>Responsibility | Monitoring<br>Schedule |
| <p>and/or on-site queue capacity; employment of parking attendants; installation of LOT FULL signs with active management by parking attendants; use of valet parking or other space-efficient parking techniques; use of off-site parking facilities or shared parking with nearby uses; use of parking occupancy sensors and signage directing drivers to available spaces; travel demand management strategies such as additional bicycle parking, customer shuttles, delivery services; and/or parking demand management strategies such as parking time limits, paid parking, time-of-day parking surcharge, or validated parking.</p> <p>If the Planning Director, or his or her designee, suspects that a recurring queue is present, the Department should notify the property owner in writing. Upon request, the owner/operator should hire a qualified transportation consultant to evaluate the conditions at the site for no less than seven days. The consultant should prepare a monitoring report to be submitted to the Department for review. If the Department determines that a recurring queue does exist, the facility owner/operator should have 90 days from the date of the written determination to abate the queue.</p> |                                       |                        |                      |  |                        |

#### MITIGATION MEASURES AGREED TO BY THE PROJECT SPONSOR (CPE)

##### CULTURAL RESOURCES

##### **Project Mitigation Measure M-CP-1: Archeological Resources Testing (Implementing *Eastern Neighborhoods PEIR* Mitigation Measure J-2)**

Based on a reasonable presumption that archeological resources may be present within the project site, the following measures shall be undertaken to avoid any potentially significant adverse effect from the proposed project on buried or submerged historical resources. The project sponsor shall retain the services of an archaeological consultant from the rotational Department Qualified Archaeological Consultants

|   |  |   |  |   |
|---|--|---|--|---|
| Project sponsor's qualified archaeological consultant and construction contractor | Prior to issuance of construction permits and throughout the construction period | Prepare and submit an archeological testing plan (ATP) in compliance with the requirements of M-CP-1 and implement additional measures if warranted | Planning Department (Environmental Review Officer) | Considered complete after construction activities are completed |
|---|--|---|--|---|



| MONITORING AND REPORTING PROGRAM  |                                       |                        |                      |  |                        |
|---|---------------------------------------|------------------------|----------------------|--|------------------------|
| Adopted Mitigation Measures   | Responsibility<br>for<br>Implementing | Mitigation<br>Schedule | Mitigation<br>Action | Monitoring/<br>Reporting<br>Responsibility | Monitoring<br>Schedule |
| <p>List (QACL) maintained by the Planning Department archaeologist. The project sponsor shall contact the Department archaeologist to obtain the names and contact information for the next three archeological consultants on the QACL. The archeological consultant shall undertake an archeological testing program as specified herein. In addition, the consultant shall be available to conduct an archeological monitoring and/or data recovery program if required pursuant to this measure. The archeological consultant's work shall be conducted in accordance with this measure at the direction of the Environmental Review Officer (ERO). All plans and reports prepared by the consultant as specified herein shall be submitted first and directly to the ERO for review and comment, and shall be considered draft reports subject to revision until final approval by the ERO. Archeological monitoring and/or data recovery programs required by this measure could suspend construction of the project for up to a maximum of four weeks. At the direction of the ERO, the suspension of construction can be extended beyond four weeks only if such a suspension is the only feasible means to reduce to a less-than-significant level potential effects on a significant archeological resource as defined in CEQA Guidelines Sect. 15064.5 (a)(c).</p> <p>Consultation with Descendant Communities: On discovery of an archeological site associated with descendant Native Americans, the Overseas Chinese, or other descendant group an appropriate representative of the descendant group and the ERO shall be contacted. The representative of the descendant group shall be given the opportunity to monitor archeological field investigations of the site and to consult with ERO regarding appropriate archeological treatment of the site, of recovered data from the site, and, if applicable, any interpretative treatment of the associated archeological site. A copy of the Final Archaeological Resources Report shall be provided to the representative of the descendant group.</p> |                                       |                        |                      |  |                        |



MONITORING AND REPORTING PROGRAM

| Adopted Mitigation Measures   | Responsibility<br>for<br>Implementing | Mitigation<br>Schedule | Mitigation<br>Action | Monitoring/<br>Reporting<br>Responsibility | Monitoring<br>Schedule |
|---|---------------------------------------|------------------------|----------------------|--|------------------------|
| <p>Archeological Testing Program. The archeological consultant shall prepare and submit to the ERO for review and approval an archeological testing plan (ATP). The archeological testing program shall be conducted in accordance with the approved ATP. The ATP shall identify the property types of the expected archeological resource(s) that potentially could be adversely affected by the proposed project, the testing method to be used, and the locations recommended for testing. The purpose of the archeological testing program will be to determine to the extent possible the presence or absence of archeological resources and to identify and to evaluate whether any archeological resource encountered on the site constitutes an historical resource under CEQA.</p> <p>At the completion of the archeological testing program, the archeological consultant shall submit a written report of the findings to the ERO. If based on the archeological testing program the archeological consultant finds that significant archeological resources may be present, the ERO in consultation with the archeological consultant shall determine if additional measures are warranted. Additional measures that may be undertaken include additional archeological testing, archeological monitoring, and/or an archeological data recovery program. No archeological data recovery shall be undertaken without the prior approval of the ERO or the Planning Department archeologist. If the ERO determines that a significant archeological resource is present and that the resource could be adversely affected by the proposed project, at the discretion of the project sponsor either:</p> <p>A) The proposed project shall be re-designed so as to avoid any adverse effect on the significant archeological resource; or</p> <p>B) A data recovery program shall be implemented, unless the ERO determines that the archeological resource is of greater interpretive than research significance and that interpretive use of the resource is feasible.</p> <p>Archeological Monitoring Program. If the ERO in consultation</p> |                                       |                        |                      |  |                        |



| MONITORING AND REPORTING PROGRAM  |                                       |                        |                      |  |                        |
|---|---------------------------------------|------------------------|----------------------|--|------------------------|
| Adopted Mitigation Measures   | Responsibility<br>for<br>Implementing | Mitigation<br>Schedule | Mitigation<br>Action | Monitoring/<br>Reporting<br>Responsibility | Monitoring<br>Schedule |
| <p>with the archeological consultant determines that an archeological monitoring program shall be implemented the archeological monitoring program shall minimally include the following provisions:</p> <ul style="list-style-type: none"> <li>• The archeological consultant, project sponsor, and ERO shall meet and consult on the scope of the AMP reasonably prior to any project-related soils disturbing activities commencing. The ERO in consultation with the archeological consultant shall determine what project activities shall be archeologically monitored. In most cases, any soils- disturbing activities, such as demolition, foundation removal, excavation, grading, utilities installation, foundation work, driving of piles (foundation, shoring, etc.), site remediation, etc., shall require archeological monitoring because of the risk these activities pose to potential archaeological resources and to their depositional context;</li> <li>• The archeological consultant shall advise all project contractors to be on the alert for evidence of the presence of the expected resource(s), of how to identify the evidence of the expected resource(s), and of the appropriate protocol in the event of apparent discovery of an archeological resource;</li> <li>• The archeological monitor(s) shall be present on the project site according to a schedule agreed upon by the archeological consultant and the ERO until the ERO has, in consultation with project archeological consultant, determined that project construction activities could have no effects on significant archeological deposits;</li> <li>• The archeological monitor shall record and be authorized to collect soil samples and artifactual/ecofactual material as warranted for analysis;</li> <li>• If an intact archeological deposit is encountered, all soils-disturbing activities in the vicinity of the deposit shall</li> </ul> |                                       |                        |                      |  |                        |



MONITORING AND REPORTING PROGRAM

| Adopted Mitigation Measures   | Responsibility<br>for<br>Implementing | Mitigation<br>Schedule | Mitigation<br>Action | Monitoring/<br>Reporting<br>Responsibility | Monitoring<br>Schedule |
|---|---------------------------------------|------------------------|----------------------|--|------------------------|
| <p>cease. The archeological monitor shall be empowered to temporarily redirect demolition/excavation/pile driving/construction activities and equipment until the deposit is evaluated. If in the case of pile driving activity (foundation, shoring, etc.), the archeological monitor has cause to believe that the pile driving activity may affect an archeological resource, the pile driving activity shall be terminated until an appropriate evaluation of the resource has been made in consultation with the ERO. The archeological consultant shall immediately notify the ERO of the encountered archeological deposit. The archeological consultant shall make a reasonable effort to assess the identity, integrity, and significance of the encountered archeological deposit, and present the findings of this assessment to the ERO.</p> <p>Whether or not significant archeological resources are encountered, the archeological consultant shall submit a written report of the findings of the monitoring program to the ERO.</p> <p>Archeological Data Recovery Program. The archeological data recovery program shall be conducted in accord with an archeological data recovery plan (ADRP). The archeological consultant, project sponsor, and ERO shall meet and consult on the scope of the ADRP prior to preparation of a draft ADRP. The archeological consultant shall submit a draft ADRP to the ERO. The ADRP shall identify how the proposed data recovery program will preserve the significant information the archeological resource is expected to contain. That is, the ADRP will identify what scientific/historical research questions are applicable to the expected resource, what data classes the resource is expected to possess, and how the expected data classes would address the applicable research questions. Data recovery, in general, should be limited to the portions of the historical property that could be adversely affected by the proposed project. Destructive data recovery methods shall not</p> |                                       |                        |                      |  |                        |



| MONITORING AND REPORTING PROGRAM  |                                       |                        |                      |  |                        |
|---|---------------------------------------|------------------------|----------------------|--|------------------------|
| Adopted Mitigation Measures   | Responsibility<br>for<br>Implementing | Mitigation<br>Schedule | Mitigation<br>Action | Monitoring/<br>Reporting<br>Responsibility | Monitoring<br>Schedule |
| <p>be applied to portions of the archeological resources if nondestructive methods are practical.</p> <p>The scope of the ADRP shall include the following elements:</p> <ul style="list-style-type: none"> <li>• Field Methods and Procedures. Descriptions of proposed field strategies, procedures, and operations.</li> <li>• Cataloguing and Laboratory Analysis. Description of selected cataloguing system and artifact analysis procedures.</li> <li>• Discard and Deaccession Policy. Description of and rationale for field and post-field discard and deaccession policies.</li> <li>• Interpretive Program. Consideration of an on-site/off-site public interpretive program during the course of the archeological data recovery program.</li> <li>• Security Measures. Recommended security measures to protect the archeological resource from vandalism, looting, and non-intentionally damaging activities.</li> <li>• Final Report. Description of proposed report format and distribution of results.</li> <li>• Curation. Description of the procedures and recommendations for the curation of any recovered data having potential research value, identification of appropriate curation facilities, and a summary of the accession policies of the curation facilities.</li> </ul> <p>Human Remains and Associated or Unassociated Funerary Objects. The treatment of human remains and of associated or unassociated funerary objects discovered during any soils disturbing activity shall comply with applicable State and Federal laws. This shall include immediate notification of the Coroner of the City and County of San Francisco and in the event of the Coroner's determination that the human remains are Native American remains, notification of the California State Native American Heritage Commission (NAHC) who</p> |                                       |                        |                      |  |                        |



| Adopted Mitigation Measures   | MONITORING AND REPORTING PROGRAM      |                        |                      |  |                        |
|---|---------------------------------------|------------------------|----------------------|--|------------------------|
|   | Responsibility<br>for<br>Implementing | Mitigation<br>Schedule | Mitigation<br>Action | Monitoring/<br>Reporting<br>Responsibility | Monitoring<br>Schedule |
| <p>shall appoint a Most Likely Descendant (MLD) (Pub. Res. Code Sec. 5097.98). The archeological consultant, project sponsor, ERO, and MLD shall make all reasonable efforts to develop an agreement for the treatment of, with appropriate dignity, human remains and associated or unassociated funerary objects (CEQA Guidelines. Sec. 15064.5(d)). The agreement should take into consideration the appropriate excavation, removal, recordation, analysis, custodianship, curation, and final disposition of the human remains and associated or unassociated funerary objects.</p> <p>Final Archeological Resources Report. The archeological consultant shall submit a Draft Final Archeological Resources Report (FARR) to the ERO that evaluates the historical significance of any discovered archeological resource and describes the archeological and historical research methods employed in the archeological testing/monitoring/data recovery program(s) undertaken. Information that may put at risk any archeological resource shall be provided in a separate removable insert within the final report.</p> <p>Once approved by the ERO, copies of the FARR shall be distributed as follows: California Archaeological Site Survey Northwest Information Center (NWIC) shall receive one (1) copy and the ERO shall receive a copy of the transmittal of the FARR to the NWIC. The Environmental Planning division of the Planning Department shall receive one bound, one unbound and one unlocked, searchable PDF copy on CD of the FARR along with copies of any formal site recordation forms (CA DPR 523 series) and/or documentation for nomination to the National Register of Historic Places/California Register of Historical Resources. In instances of high public interest in or the high interpretive value of the resource, the ERO may require a different final report content, format, and distribution than that presented above.</p> |                                       |                        |                      |  |                        |



| Adopted Mitigation Measures  | MONITORING AND REPORTING PROGRAM  |   |   |   |   |
|--|---|---|---|---|---|
|  | Responsibility<br>for<br>Implementing   | Mitigation<br>Schedule  | Mitigation<br>Action  | Monitoring/<br>Reporting<br>Responsibility                | Monitoring<br>Schedule  |
| <b>NOISE</b>   |   |   |   |   |   |
| <p><b>Project Mitigation Measure M-NO-1: Construction Noise, Pile-Driving (<i>Eastern Neighborhoods PEIR</i> Mitigation Measure F-1)</b></p> <p>The project sponsor shall ensure that piles be pre-drilled wherever feasible to reduce construction-related noise and vibration. No impact pile drivers shall be used unless absolutely necessary. Contractors shall be required to use pile-driving equipment with state-of-the-art noise shielding and muffling devices. To reduce noise and vibration impacts, sonic or vibratory sheetpile drivers, rather than impact drivers, shall be used wherever sheetpiles are needed. The project sponsor shall also require that contractors schedule pile-driving activity during times of the day that would minimize disturbance to neighbors.</p>   | Project sponsor's construction contractor                                     | Prior to issuance of building and construction permits and throughout the construction period | Specify appropriate pile techniques in construction contracts as detailed in M-NO-1           | Planning Department and Department of Building Inspection | Considered complete after construction activities are completed |
| <p><b>Project Mitigation Measure M-NO-2: Construction Noise (Implementing <i>Eastern Neighborhoods PEIR</i> Mitigation Measure F-2)</b></p> <p>Prior to commencing construction, the project sponsor shall submit a plan for noise attenuation measures to the Department of Building Inspection to ensure that maximum feasible noise attenuation will be achieved. These attenuation measures shall include as many of the following control strategies as feasible:</p> <ol style="list-style-type: none"> <li>1. Conduct noise monitoring at the beginning of major construction phases (e.g., demolition, excavation) to determine the need and the effectiveness of noise-attenuation measures.</li> <li>2. Erect temporary plywood noise barriers around the construction site where the site adjoins noise-sensitive receivers, including the existing residences at 999 16th Street and 49 Missouri Street and any other known adjacent noise-sensitive receivers.</li> </ol> | Project sponsor's qualified acoustical consultant and construction contractor | Prior to issuance of building and construction permits and throughout the construction period | Prepare and implement a plan for noise attenuation measures that meets the criteria of M-NO-2 | Planning Department and Department of Building Inspection | Considered complete after construction activities are completed |



MONITORING AND REPORTING PROGRAM

| Adopted Mitigation Measures   | Responsibility<br>for<br>Implementing | Mitigation<br>Schedule | Mitigation<br>Action | Monitoring/<br>Reporting<br>Responsibility | Monitoring<br>Schedule |
|---|---------------------------------------|------------------------|----------------------|--|------------------------|
| <p>3. Utilize noise control blankets on the building structure adjacent to noise-sensitive receivers as the building is erected to reduce noise emission from the site.</p> <p>4. Post signs on-site pertaining to permitted construction days and hours and complaint procedures and who to notify in the event of a problem, with telephone numbers listed.</p> <p>5. Notify the Department of Building Inspection and neighbors in advance of the schedule for each major phase of construction (i.e., building demolition, site preparation, grading, excavation, and building construction) and expected loud activities.</p> <p>6. Limit construction to the hours of 7:00 a.m. to 8:00 p.m. per San Francisco Police Code Article 29. Construction outside of these hours may be approved through a development permit based on a site-specific construction noise mitigation plan and a finding by the Director of Building Inspection that the construction noise mitigation plan is adequate to prevent noise disturbance of affected residential uses.</p> <p>7. When feasible, select “quiet” construction methods and equipment (e.g., improved mufflers, equipment redesign, use of intake silencers, ducts, engine enclosures, and acoustically attenuating shields or shrouds) wherever feasible.</p> <p>8. Locate noisy station equipment (e.g., generators and compressors) and material unloading and staging away from the most sensitive adjacent uses and to areas with the most ambient noise (e.g., the corner of 16th Street and Mississippi Street).</p> <p>9. Require that all construction equipment be in good working order and that mufflers are inspected to be functioning properly. Avoid unnecessary idling of equipment and engines.</p> <p>The on-site noise monitoring shall be conducted throughout the site and at nearby noise sensitive receivers at the beginning of major construction phases (e.g., demolition, excavation). The purpose would be to help determine the loudest activities and</p> |                                       |                        |                      |  |                        |



| MONITORING AND REPORTING PROGRAM  |                                       |  |  |   |   |
|---|---------------------------------------|--|--|---|---|
| Adopted Mitigation Measures   | Responsibility<br>for<br>Implementing | Mitigation<br>Schedule                         | Mitigation<br>Action   | Monitoring/<br>Reporting<br>Responsibility                | Monitoring<br>Schedule  |
| <p>what additional measures can be provided as needed to reduce the potential for noise impacts. Continuous noise monitoring shall occur for the first two weeks of each phase and a summary report shall be provided to the Planning Department at the conclusion of each major phase of construction documenting noise levels and additional measures to reduce project impacts as needed.</p> <p><b>Project Mitigation Measure M-NO-3: Siting of Noise-Sensitive Uses (<i>Eastern Neighborhoods PEIR</i> Mitigation Measure F-4)</b></p> <p>To reduce potential conflicts between existing noise-generating uses and new sensitive receptors, for new development including noise-sensitive uses, the Planning Department shall require the preparation of an analysis that includes, at a minimum, a site survey to identify potential noise-generating uses within 900 feet of, and that have a direct line-of-sight to, the project site, and including at least one 24-hour noise measurement (with maximum noise level readings taken at least every 15 minutes), prior to the first project approval action. The analysis shall be prepared by persons qualified in acoustical analysis and/or engineering and shall demonstrate with reasonable certainty that Title 24 standards, where applicable, can be met, and that there are no particular circumstances about the proposed project site that appear to warrant heightened concern about noise levels in the vicinity. Should such concerns be present, the Department may require the completion of a detailed noise assessment by person(s) qualified in acoustical analysis and/or engineering prior to the first project approval action, in order to demonstrate that acceptable interior noise levels consistent with those in the Title 24 standards can be attained.</p> | Project sponsor                       | Prior to issuance of certificates of occupancy | (Acoustical analysis has been completed)<br>Install building materials with higher noise ratings and appropriate HVAC systems per recommendations of acoustical analysis to Planning Department's satisfaction | Planning Department and Department of Building Inspection | Considered complete after construction activities are completed |



| Adopted Mitigation Measures   | MONITORING AND REPORTING PROGRAM                  |  |   |   |   |
|---|---|--|---|---|---|
|   | Responsibility<br>for<br>Implementing             | Mitigation<br>Schedule                                     | Mitigation<br>Action  | Monitoring/<br>Reporting<br>Responsibility                | Monitoring<br>Schedule  |
| <p><b>Project Mitigation Measure M-NO-4: Siting of Noise-Generating Uses (<i>Eastern Neighborhoods PEIR Mitigation Measure F-5</i>)</b></p> <p>To reduce potential conflicts between existing sensitive receptors and new noise-generating uses, for new development including commercial, industrial or other uses that would be expected to generate noise levels in excess of ambient noise, either short-term, at nighttime, or as a 24-hour average, in the proposed project site vicinity, the Planning Department shall require the preparation of an analysis that includes, at a minimum, a site survey to identify potential noise-sensitive uses within 900 feet of, and that have a direct line-of-sight to, the project site, and including at least one 24-hour noise measurement (with maximum noise level readings taken at least every 15 minutes), prior to the first project approval action. The analysis shall be prepared by persons qualified in acoustical analysis and/or engineering and shall demonstrate with reasonable certainty that the proposed use would comply with the use compatibility requirements in the General Plan and in Police Code Section 29091, would not adversely affect nearby noise-sensitive uses, and that there are no particular circumstances about the proposed project site that appear to warrant heightened concern about noise levels that would be generated by the proposed use. Should such concerns be present, the Department may require the completion of a detailed noise assessment by person(s) qualified in acoustical analysis and/or engineering prior to the first project approval action.</p> | Project sponsor's qualified acoustical consultant | Prior to issuance of certificates of occupancy             | (Acoustical analysis has been completed) Install diesel backup generator and shielding per recommendations of acoustical analysis to Planning Department's satisfaction | Planning Department and Department of Building Inspection | Considered complete after construction activities are completed |
| <i>AIR QUALITY</i>  |   |  |   |   |   |
| <p><b>Project Mitigation Measure M-AQ-1: Construction Air Quality (Implementing <i>Eastern Neighborhoods PEIR Mitigation Measure G-1</i>)</b></p>   | Project sponsor's construction contractor         | Prior to issuance of building and construction permits and | Prepare and Implement a Construction Emissions Minimization Plan  | Planning Department (Environmental Review Officer)        | Considered complete after construction activities are completed |



| Adopted Mitigation Measures   | MONITORING AND REPORTING PROGRAM      |                                    |                                   |  |                        |
|---|---------------------------------------|------------------------------------|-----------------------------------|--|------------------------|
|   | Responsibility<br>for<br>Implementing | Mitigation<br>Schedule             | Mitigation<br>Action              | Monitoring/<br>Reporting<br>Responsibility | Monitoring<br>Schedule |
| The project sponsor or the project sponsor's Contractor shall comply with the following   |                                       | throughout the construction period | that meets the criteria of M-AQ-1 |  |                        |
| A. Engine Requirements.   |                                       |                                    |                                   |  |                        |
| 1. All off-road equipment greater than 25 hp and operating for more than 20 total hours over the entire duration of construction activities shall have engines that meet or exceed either U.S. Environmental Protection Agency (USEPA) or California Air Resources Board (ARB) Tier 3 off-road emission standards, and have been retrofitted with an ARB Level 3 Verified Diesel Emissions Control Strategy. Equipment with engines meeting Tier 4 Interim or Tier 4 Final off-road emission standards automatically meet this requirement. |                                       |                                    |                                   |  |                        |
| 2. Where access to alternative sources of power are available, portable diesel engines shall be prohibited.   |                                       |                                    |                                   |  |                        |
| 3. Diesel engines, whether for off-road or on-road equipment, shall not be left idling for more than two minutes, at any location, except as provided in exceptions to the applicable state regulations regarding idling for off-road and on-road equipment (e.g., traffic conditions, safe operating conditions). The Contractor shall post legible and visible signs in English, Spanish, and Chinese, in designated queuing areas and at the construction site to remind operators of the two minute idling limit.                       |                                       |                                    |                                   |  |                        |
| 4. The Contractor shall instruct construction workers and equipment operators on the maintenance and tuning of construction equipment, and require that such workers and operators properly maintain and tune equipment in accordance with manufacturer specifications.   |                                       |                                    |                                   |  |                        |
| B. Waivers.   |                                       |                                    |                                   |  |                        |
| 1. The Planning Department's Environmental Review Officer or designee (ERO) may waive the alternative source of power requirement of Subsection (A)(2) if an alternative source of power is limited or infeasible at the  |                                       |                                    |                                   |  |                        |



MONITORING AND REPORTING PROGRAM

| Adopted Mitigation Measures  | Responsibility<br>for<br>Implementing | Mitigation<br>Schedule | Mitigation<br>Action | Monitoring/<br>Reporting<br>Responsibility | Monitoring<br>Schedule |
|--|---------------------------------------|------------------------|----------------------|--|------------------------|
| <p>project site. If the ERO grants the waiver, the Contractor must submit documentation that the equipment used for onsite power generation meets the requirements of Subsection (A)(1).</p> <p>2. The ERO may waive the equipment requirements of Subsection (A)(1) if: a particular piece of off-road equipment with an ARB Level 3 VDECS is technically not feasible; the equipment would not produce desired emissions reduction due to expected operating modes; installation of the equipment would create a safety hazard or impaired visibility for the operator; or, there is a compelling emergency need to use off-road equipment that is not retrofitted with an ARB Level 3 VDECS. If the ERO grants the waiver, the Contractor must use the next cleanest piece of off-road equipment, according to Table below.</p> |                                       |                        |                      |  |                        |

Table – Off-Road Equipment Compliance Step-down Schedule

| Compliance Alternative     | Engine Emission   |
|----------------------------|-------------------|
| Standard Emissions Control |                   |
| 1 Tier 3                   | ARB Level 2 VDECS |
| 2 Tier 3                   | ARB Level 1 VDECS |
| 3 Tier 3                   | Alternative Fuel* |

How to use the table: If the ERO determines that the equipment requirements cannot be met, then the project sponsor would need to meet Compliance Alternative 1. If the ERO determines that the Contractor cannot supply off-road equipment meeting Compliance Alternative 1, then the Contractor must meet Compliance Alternative 2. If the ERO determines that the Contractor cannot supply off-road equipment meeting Compliance Alternative 2, then the Contractor must meet Compliance Alternative 3.



MONITORING AND REPORTING PROGRAM

| Adopted Mitigation Measures   | Responsibility<br>for<br>Implementing | Mitigation<br>Schedule | Mitigation<br>Action | Monitoring/<br>Reporting<br>Responsibility | Monitoring<br>Schedule |
|---|---------------------------------------|------------------------|----------------------|--|------------------------|
| <p>* Alternative fuels are not a VDECS.</p> <p>C. Construction Emissions Minimization Plan. Before starting on-site construction activities, the Contractor shall submit a Construction Emissions Minimization Plan (Plan) to the ERO for review and approval. The Plan shall state, in reasonable detail, how the Contractor will meet the requirements of Section A.</p> <p>1. The Plan shall include estimates of the construction timeline by phase, with a description of each piece of off-road equipment required for every construction phase. The description may include, but is not limited to: equipment type, equipment manufacturer, equipment identification number, engine model year, engine certification (Tier rating), horsepower, engine serial number, and expected fuel usage and hours of operation. For VDECS installed, the description may include: technology type, serial number, make, model, manufacturer, ARB verification number level, and installation date and hour meter reading on installation date. For off-road equipment using alternative fuels, the description shall also specify the type of alternative fuel being used.</p> <p>2. The ERO shall ensure that all applicable requirements of the Plan have been incorporated into the contract specifications. The Plan shall include a certification statement that the Contractor agrees to comply fully with the Plan.</p> <p>3. The Contractor shall make the Plan available to the public for review on-site during working hours. The Contractor shall post at the construction site a legible and visible sign summarizing the Plan. The sign shall also state that the public may ask to inspect the Plan for the project at any time during working hours and shall explain how to request to inspect the Plan. The ERO shall review and approve The Contractor shall post at least one copy of the sign in a visible location on each side of the construction site facing a public right-of-way.</p> <p>D. Monitoring. After start of Construction Activities, the</p> |                                       |                        |                      |  |                        |



| Adopted Mitigation Measures   | MONITORING AND REPORTING PROGRAM          |   |  |  |   |
|---|---|---|--|--|---|
|   | Responsibility<br>for<br>Implementing     | Mitigation<br>Schedule                                      | Mitigation<br>Action   | Monitoring/<br>Reporting<br>Responsibility | Monitoring<br>Schedule  |
| Contractor shall submit quarterly reports to the ERO documenting compliance with the Plan. After completion of construction activities and prior to receiving a final certificate of occupancy, the project sponsor shall submit to the ERO a final report summarizing construction activities, including the start and end dates and duration of each construction phase, and the specific information required in the Plan.   |   |   |  |  |   |
| <p><b>Project Mitigation Measure M-AQ-2: Best Available Control Technology for Diesel Generators (Implementing <i>Eastern Neighborhoods PEIR</i> Mitigation Measure G-4)</b></p> <p>The project sponsor shall ensure that the backup diesel generator meets or exceeds one of the following emission standards for particulate matter: Tier 4 certified engine (interim or final, whichever is in effect), or (2) use of a current EPA Tier 2 or Tier 3 certified engine that is equipped with a California Air Resources Board (ARB) Level 3 Verified Diesel Emissions Control Strategy (VDECS). A non-verified diesel emission control strategy may be used if the filter is identical to the ARB verified model and if the Bay Area Air Quality Management District (BAAQMD) approves of its use. The project sponsor shall submit documentation of compliance with the BAAQMD New Source Review permitting process (Regulation 2, Rule 2, and Regulation 2, Rule 5) and the emission standard requirement of this mitigation measure to the Planning Department for review and approval prior to issuance of a permit for a backup diesel generator from any City agency.</p> | Project sponsor's construction contractor | Prior to issuance of a permit for a backup diesel generator | Submit documentation of compliance with BAAQMD regulations and emission standards requirements of M-AQ-2 | Planning Department                        | Considered complete after construction activities are completed |
| <b>HAZARDOUS MATERIALS</b>  |   |   |  |  |   |
| <p><b>Project Mitigation Measure M-HZ-1: Hazardous Building Materials (<i>Eastern Neighborhoods PEIR</i> Mitigation Measure L-1)</b></p> <p>The project sponsor shall ensure that any equipment containing PCBs or DEPH, such as fluorescent light ballasts, are removed</p>  | Project sponsor's construction contractor | Prior to and throughout demolition                          | Ensure that hazardous building materials are properly disposed of  | Department of Building Inspection          | Considered complete after demolition activities are completed   |



|   | MONITORING AND REPORTING PROGRAM      |   |   |  |   |
|---|---------------------------------------|---|---|--|---|
|   | Responsibility<br>for<br>Implementing | Mitigation<br>Schedule                          | Mitigation<br>Action                    | Monitoring/<br>Reporting<br>Responsibility | Monitoring<br>Schedule                      |
| Adopted Mitigation Measures   |                                       |   |   |  |   |
| and properly disposed of according to applicable federal, state, and local laws prior to the start of renovation, and that any fluorescent light tubes, which could contain mercury, are similarly removed and properly disposed of. Any other hazardous materials identified, either before or during work, shall be abated according to applicable federal, state, and local laws.  |                                       |   |   |  |   |
| <b>MITIGATION MEASURES AGREED TO BY THE PROJECT SPONSOR BUT WITH UNCERTAIN FEASABILITY OF IMPLEMENTATION (EIR)</b>  |                                       |   |   |  |   |
| <i>TRANSPORTATION AND CIRCULATION</i>   |                                       |   |   |  |   |
| <b>Mitigation Measure M-TR-2a: 17<sup>th</sup> Street and Mississippi Street Signalization.</b>   | Project sponsor                       | Prior to issuance of a certificate of occupancy | Ensure fair-share payment has been made | Planning Department, SFMTA                 | Considered complete upon receipt of payment |
| To mitigate poor operating conditions at the intersection of 17th Street and Mississippi Street, the project sponsor shall pay their fair share for the cost of design and of signalization or other similar mitigation to improve automobile delay at this intersection, as determined by the SFMTA. [Full funding of this measure has not been identified, so feasibility of implementation is not assured or assumed.]                     |                                       |   |   |  |   |
| <b>Mitigation Measure M-TR-2b: Mariposa Street and Pennsylvania Street Signalization.</b>   | Project sponsor                       | Prior to issuance of a certificate of occupancy | Ensure fair-share payment has been made | Planning Department, SFMTA                 | Considered complete upon receipt of payment |
| To mitigate poor operating conditions at the intersection of Mariposa Street and Pennsylvania Street, the project sponsor shall pay their fair share for the cost of design and implementation of signalization or other similar mitigation to improve automobile delay at this intersection, as determined by the SFMTA. [Full funding of this measure has not been identified, so feasibility of implementation is not assured or assumed.] |                                       |   |   |  |   |





# SAN FRANCISCO PLANNING DEPARTMENT

---

*Subject to: (Select only if applicable)*

☒ Affordable Housing (Sec. 415)

☐ Jobs Housing Linkage Program (Sec. 413)

☒ First Source Hiring (Admin. Code)

☒ Child Care Requirement (Sec. 414A)

☒ Other (TSF, Sec. 411A & EN Impact Fees, Sec. 423)

## Planning Commission Draft Motion

HEARING DATE: MAY 12, 2016

*Case No.:* 2011.1300X  
*Project Address:* 901 16<sup>TH</sup> STREET & 1200 17<sup>TH</sup> STREET  
*Zoning:* UMU (Urban Mixed-Use) Zoning District  
48-X Height and Bulk District (1200 17<sup>th</sup> Street Site)  
68-X Height and Bulk District (901 16<sup>th</sup> Street Site)  
*Block/Lot:* 3949/002; 3950/001 (1200 17<sup>th</sup> Street Site)  
3949/001A, 001, 002; 3950/001 (901 16<sup>th</sup> Street Site)  
*Project Sponsor:* Potrero Partners, LLC (Attn: Josh Smith)  
445 Virginia Avenue  
San Mateo, CA 94402  
*Staff Contact:* Chris Townes – (415) 575-9195  
[chris.townes@sfgov.org](mailto:chris.townes@sfgov.org)

ADOPTING FINDINGS RELATING TO A LARGE PROJECT AUTHORIZATION PURSUANT TO PLANNING CODE SECTION 329, TO ALLOW EXCEPTIONS TO 1) REAR YARD PURSUANT TO PLANNING CODE SECTION 134, 2) PERMITTED OBSTRUCTIONS OVER THE STREET PURSUANT TO PLANNING CODE SECTION 136, 3) PARKING/LOADING ENTRANCE WIDTH PURSUANT TO PLANNING CODE SECTION 145.1, 4) OFF-STREET LOADING MINIMUM PURSUANT TO PLANNING CODE SECTION 152.1, 5) HORIZONTAL MASS REDUCTION PURSUANT TO PLANNING CODE SECTION 270.1, AND 6) FLEXIBLE UNITS-MODIFICATION OF THE ACCESSORY USE PROVISIONS FOR DWELLING UNITS PURSUANT TO PLANNING CODE SECTIONS 329(D)(10) AND 803.3(B)(1)(C), TO ALLOW CONSTRUCTION OF TWO, FOUR-TO-SIX-STORY MIXED-USE BUILDINGS WITH A TOTAL OF 396 DWELLING UNITS, 24,486 SQUARE FEET OF RETAIL, AND 388 OFF-STREET PARKING SPACES, LOCATED AT 901 16<sup>TH</sup> STREET AND 1200 17<sup>TH</sup> STREET, LOTS 001A, 001, 002 ON ASSESSOR'S BLOCK 3949 AND LOTS 001 ON ASSESSOR'S BLOCK 3950, WITHIN THE UMU (URBAN MIXED-USE) ZONING DISTRICT AND A 48-X AND 68-X HEIGHT AND BULK DISTRICT.

THE TWO BUILDINGS INCLUDE A NORTH BUILDING ("16TH STREET BUILDING") AND A SOUTH BUILDING ("17TH STREET BUILDING"). THE 16TH STREET BUILDING IS A SIX-STORY, 68-FOOT TALL, MIXED-USE BUILDING (APPROXIMATELY 402,943 GSF) WITH 260 DWELLING UNITS (CONSISTING OF (53) STUDIOS, (103) 1-BEDROOM UNITS, (95) 2-BEDROOM UNITS, AND (9) 3-BEDROOM UNITS), 20,318 SQUARE FEET OF COMMERCIAL SPACE, AND 263 OFF-STREET PARKING SPACES LOCATED AT 901 16TH STREET, LOT 001A, 001 AND 002 IN



ASSESSOR'S BLOCK 3949 AND LOT 001 IN ASSESSOR'S BLOCK 3950, WITHIN THE UMU (URBAN MIXED USE) ZONING DISTRICT AND A 68-X HEIGHT AND BULK DISTRICT. THE 17TH STREET BUILDING IS A FOUR-STORY, 48-FOOT TALL, MIXED-USE BUILDING (APPROXIMATELY 213,009 GSF) WITH 135 DWELLING UNITS (CONSISTING OF (79) 1-BEDROOM UNITS, (51) 2-BEDROOM UNITS, AND (5) 3-BEDROOM UNITS), 4,150 SQUARE FEET OF COMMERCIAL SPACE, AND 125 OFF-STREET PARKING SPACES LOCATED AT 1200 17TH STREET, LOT 002 IN ASSESSOR'S BLOCK 3949 AND LOT 001 IN ASSESSOR'S BLOCK 3950, WITHIN THE UMU (URBAN MIXED USE) ZONING DISTRICT AND A 48-X HEIGHT AND BULK DISTRICT.

## **PREAMBLE**

On June 19, 2014, Josh Smith of Potrero Partners, LLC (hereinafter "Project Sponsor") filed Application No. 2011.1300EX (hereinafter "Application") with the Planning Department (hereinafter "Department") for a Large Project Authorization to demolish a surface parking lot and approximately 105,000 square feet of existing warehouse (PDR) use (Cor-O-Van Moving and Storage Company), and construct two four-to-six-story mixed-use buildings at 901 16<sup>th</sup> Street and 1200 17<sup>th</sup> Street on Lots 001,001A and 002 in Assessor's Block 3949 and on Lot 001 in Assessor's Block 3950 in San Francisco, California. The two buildings consist of: a North Building ("16th Street Building")--a 6-story, 68-foot tall, 402,943 gross square foot (gsf) mixed-use building with 260 dwelling unit, 20,318 square feet of retail, and 263 off-street parking spaces; and, a South Building ("17th Street Building")--a 4-story, 48-foot tall, 213,009 gsf mixed-use building with 135 dwelling unit, 4,650 square feet of retail and 125 off-street parking spaces.

On August 12, 2015, the Department published a Draft Environmental Impact Report ("DEIR") for the Project for public review (Case No. 2011.1300E). The DEIR was available for public comment until October 5, 2015. On October 1, 2015, the Commission conducted a duly noticed public hearing at a regularly scheduled meeting to solicit comments regarding the DEIR. On April 28, 2016, the Department published a Response to Comments document, responding to comments made regarding the DEIR for the Project.

On May 12, 2016, the Commission certified the FEIR for the Project as adequate, accurate and complete. On May 12, 2016, the Commission adopted the CEQA Findings for the FEIR, prior to the approval of the Project (See Case No. 2011.1300E).

On May 12, 2016, the Planning Commission ("Commission") conducted a duly noticed public hearing at a regularly scheduled meeting on Large Project Authorization Application No. 2011.1300EX. The Commission has heard and considered the testimony presented to it at the public hearing and has further considered written materials and oral testimony presented on behalf of the applicant, Department staff, and other interested parties.

On May 12, 2016, the Planning Commission ("Commission") conducted a duly noticed public hearing at a regularly scheduled meeting on Large Project Authorization Application No. 2011.1300X.

The Planning Department, Jonas P. Ionin, is the custodian of records, located in the File for Case No. 2011.1300X at 1650 Mission Street, Fourth Floor, San Francisco, California.



The Commission has heard and considered the testimony presented to it at the public hearing and has further considered written materials and oral testimony presented on behalf of the applicant, Department staff, and other interested parties.

**MOVED**, that the Commission hereby authorizes the Large Project Authorization requested in Application No. 2011.1300X, subject to the conditions contained in “EXHIBIT A” of this motion, based on the following findings:

## **FINDINGS**

Having reviewed the materials identified in the preamble above, and having heard all testimony and arguments, this Commission finds, concludes, and determines as follows:

1. The above recitals are accurate and constitute findings of this Commission.
2. **Site Description and Present Use.** The Project Site consists of four adjacent lots in the lower Potrero Hill neighborhood (Assessor’s block/lot: 3949/001, 001A, 002, and 3950/001). The approximately 3.5-acre Project Site is bounded by 16th Street to the north, Mississippi Street to the east, 17th Street to the south, and residential and industrial buildings to the west. The Project Site currently contains four existing buildings: two metal shed industrial warehouse buildings, a vacant brick office building, and a modular office structure. The vacant brick building was originally constructed by the Pacific Rolling Mill Co. in 1926 to house the office functions of the company’s steel fabricating operation at the site, while the modular office structure was occupied by Cor-O-Van Moving and Storage Company until early 2016. In total, the four existing buildings on the Project Site amount to approximately 109,500 gross square feet of building space. Surrounding the modular office structure is an open surface parking lot which is also used for access to the University of California, San Francisco (UCSF) storage and for fleet storage of the Cor-O-Van trucks and moving vans. The Project Site is within the Urban Mixed-Use (UMU) Zoning District. Per the San Francisco General Plan (General Plan), UMU is a land use designation intended to promote a vibrant mix of uses while maintaining the characteristics of this formerly industrial-zoned area. The site is located within the Showplace Square/Potrero Subarea of the Eastern Neighborhoods Rezoning and Area Plan (Eastern Neighborhoods Plan).
3. **Surrounding Properties and Neighborhood.** The project site is located in the UMU Zoning District along a transitioning industrial corridor connecting the Mission neighborhood to Mission Bay within the Showplace Square/Potrero Hill Plan Area. Directly across 16<sup>th</sup> Street to the north, is a new 453-unit, mixed-use development project (referred to as “Potrero 1010”) currently under construction consisting of two, 6-story buildings with residential units above flex and retail spaces located on a triangular site. Potrero 1010 includes a new 1-acre park and a public mid-block pedestrian mews lined with active uses. The adjacent properties to the west include two 3-story live/work building, as well as, a 1-story auto body shop. The properties to the south, across 17<sup>th</sup> Street include a 1-story industrial building, a vacant lot and a 2-story mixed-use building with residential above a place of entertainment (“Bottom of the Hill”). Properties to the east, across Mississippi Street include 1- to 2-story industrial buildings and a 3-story, 19-unit live/work



building. The elevated Interstate 280 Freeway borders the site along the eastern edge. The surrounding neighborhood includes several other one- and two-story commercial and warehouse buildings. The broader neighborhood includes a variety of light industrial uses, Whole Foods grocery store, Live Oak School, California College of the Arts (CCA), residential, live/work, and mixed-use developments, restaurants, places of entertainment and Jackson Playground Park. The project site is located approximately one mile from the 4th and Townsend Street, and 22nd Street Caltrain stations, is located along the No. 55 bus route, and is blocks from the No. 8, 10, 19, and 22 bus lines and within a half-mile of the T-Third Street Muni line. Surrounding properties to the north, west, east, and south are all zoned UMU (Urban Mixed Use). Properties further northwest are zoned PDR-1-D (Production, Distribution, Repair- 1- Design) while properties further south are zoned RH-2 (Residential-House, Two Family).

4. **Project Description.** The project proposes to merge four lots into two lots totaling approximately 3.5 acres, to demolish approximately 105,000 square feet of existing PDR warehouse use (Cor-O-Van Moving and Storage Company) with surface parking and to construct: a North Building ("16th Street Building")- A 6-story, 68-foot tall, 260 dwelling unit, 402,943 gross square foot mixed-use building with 20,318 square feet of retail and 263 parking spaces; and, a South Building ("17th Street Building")- A 4-story, 48-foot tall, 135 dwelling unit, 213,009 gross square foot mixed-use building with 4,650 square feet of retail and 125 parking spaces. A publicly accessible pedestrian alley connecting 16<sup>th</sup> Street to 17<sup>th</sup> Street would be constructed along the western property line. Combined, the two new buildings would contain a total of 395 dwelling units and 24,468 gross square feet of retail space, in addition to a total of 388 off-street parking spaces, 455 Class 1 and 52 Class 2 bicycle parking spaces. The project would retain an existing two-story brick building determined to be an eligible historic resource. The Project would include 12,219 square feet of public open space, 27,268 square feet of common open space shared by Project occupants, and 4,950 square feet of open space private to dwelling units.
5. **Public Comment.** To date, the Department has received (15) letters of support for the Project from individuals, as well as from the Dogpatch Merchants, Potrero Hill Neighborhood House, the Bookkeeper who cite support for the project's design and program, proposed density, and relationship to the 16th Street transit corridor. In opposition, the Department has received (25) community letters, 330 community signers of a petition, and 135 comments by community signers to the petition. Issues cited include concern for the project's height, scale, and massing, lack of adequate public open space, neighborhood compatibility, lack of area and diversity of commercial uses, traffic impacts, and inadequacy of EIR.
6. **Planning Code Compliance:** The Commission finds that the Project is consistent with the relevant provisions of the Planning Code in the following manner:
  - A. **Permitted Uses in UMU Zoning Districts.** Planning Code Section 843.20 and 843.45 state that residential and retail uses are principally permitted uses within the UMU Zoning District.



*The Project would construct two new mixed-use buildings with residential and retail uses within the UMU Zoning District; therefore, the Project complies with Planning Code Sections 843.20 and 843.45.*

- B. Floor Area Ratio (FAR).** Planning Code Section 124 establishes a maximum FAR within the UMU Zoning District of 3.0 to 1 for non-residential floor area within the 48-X Height and Bulk District and a 5.0 to 1 for non-residential floor area within the 68-X Height and Bulk District.

*The Project site has a total lot area of 152,000, with 90,060 square feet within the 68-X Height and Bulk District and 61,940 square feet within the 48-X Height and Bulk District. Therefore, the maximum permitted FAR for non-residential uses for the 1200 17<sup>th</sup> Street site is 185,820 square feet and the maximum permitted FAR for non-residential uses for the 901 16<sup>th</sup> Street site is 450,300 square feet. The project proposes 20,318 square feet of retail with an FAR of .22 for all non-residential floor area within the 68-X Height and Bulk portion of the project site and 4,150 square feet of retail with an FAR of .07 for all non-residential floor area within the 48-X Height and Bulk District portion of the project site; therefore, the project complies with Planning Code Section 124.*

- C. Rear Yard.** Planning Code Section 134 requires a minimum rear yard equal to 25% of the total lot depth of the lot to be provided at every residential level. Therefore, the Project would have to provide a rear yard, which measures approximately 38,000 square feet, located along the rear property line.

*The rectangular-shaped, 152,000 square feet subject property is a double-corner lot with frontage along 16<sup>th</sup>, Mississippi, and 17<sup>th</sup> Streets. Since the surrounding area is predominantly light industrial in nature, the subject block does not possess a mid-block open space pattern. The Project provides a total of 27,268 square feet of common open space, 12,219 square feet of publicly-accessible open space, in addition to 4,950 square feet of private open space. The common open space is provided within a mews between the two buildings, four interior residential courtyards and two roof decks. The publicly-accessible open space is provided within a north-south, 30-foot wide, pedestrian promenade mid-block alley, as well as, at two pockets at the corner of 16<sup>th</sup> Street and Mississippi Street and at the eastern entrance of the east-west residential mews. The private open space is provided within private patios and decks/balconies. As a whole, the Project exceeds the total amount of open space which would have been provided through a qualifying rear yard by 6,437 square feet. The open spaces provided at ground and podium level within the Project that are open to the sky and provide massing breaks represent approximately 25.5% of the total lot area. The north-south pedestrian promenade intersects with the east-west residential mews in a manner that serves to provide appropriately-located mid-block pedestrian alleys through the block while providing massing breaks between the North and South Buildings. The four interior, comparably-sized residential courtyards are appropriately sized and exceed the Planning Code requirements for dwelling unit exposure for dwellings units within the interior court. The Project is seeking a modification of the rear yard requirement as part of the Large Project Authorization since the Project's building footprint does not meet the minimum required setback.*

- D. Useable Open Space.** Planning Code Section 135 requires a minimum of 80 square feet of open space per dwelling unit, if not publically accessible, or 54 square feet of open space



per dwelling unit, if publically accessible. Private useable open space shall have a minimum horizontal dimension of six feet and a minimum area of 36 square feet is located on a deck, balcony, porch or roof, and shall have a minimum horizontal dimension of 10 feet and a minimum area of 100 square feet if located on open ground, a terrace or the surface of an inner or outer court. Common useable open space shall be at least 15 feet in every horizontal dimension and shall be a minimum of 300 square feet. Inner courts may be credited as common usable open space if the enclosed space is not less than 20 feet in every horizontal dimension and 400 square feet in area, and if the height of the walls and projections above the court on at least three sides is such that no point on any such wall or projection is higher than one foot for each foot that such point is horizontally distant from the opposite side of the clear space in the court.

*The required residential usable open space requirement will be satisfied by non-publicly accessible, common open spaces, as well as, publicly-accessible open spaces. The non-publicly accessible, common open spaces include interior residential courtyards (4 total), a 39-foot wide residential mews, and roof decks totaling 27,268 square feet. The publicly-accessible open spaces include a 30-foot wide, north-south, pedestrian promenade mid-block alley, and pockets of open spaces at the corner of 16<sup>th</sup> Street and Mississippi Street and at the entrance into the east-west residential mews totaling 12,219 square feet. The landscaped courtyards and pedestrian promenade mid-block alley meet the dimensional requirements of Planning Code Section 135, 135(h)(1)(C), and 270.2. Additional private open space are provided in the form of patios, decks/balconies.*

*In total, the Project exceeds the amount of open space required by constructing a total of 27,268 square feet of usable Code-complying non-publicly accessible common open space (via a residential interior courtyards, a residential mews, and roof decks, as well as, 12,219 square feet of usable Code-complying publicly-accessible open space (via a north-south pedestrian promenade mid-block alley and pockets of open spaces). Overall, the Project complies with Planning Code Section 135.*

- E. **Streetscape and Pedestrian Improvements.** Planning Code Section 138.1 requires a streetscape plan, which includes elements from the Better Streets Plan for a project proposing new construction on a lot that is greater than one-half acre with frontage encompassing the entire block face between the nearest two intersections with any other publicly-accessible rights-of-way.

*The Project includes the new construction of a north building ("16<sup>th</sup> Street Building"- a 6-story, 260-unit, mixed-use building) and a south building ("17<sup>th</sup> Street Building"- a 4-story, 135-unit, mixed-use building) on a site with 380 feet of frontage along 16<sup>th</sup> Street, 399.75 feet of frontage along Mississippi Street and 380 feet of frontage along 17<sup>th</sup> Street. The Project's streetscape has been designed in accordance with the San Francisco Better Streets Plan and provides widened sidewalks measuring 15 feet on 16th Street, 15 feet on Mississippi Street, and 10 feet on 17<sup>th</sup> Street. The Project also provides streetscape elements, including new paving, landscaping, street trees, bicycle racks, and street furniture. Therefore, the Project complies with Planning Code Section 138.1.*

- F. **Bird Safety.** Planning Code Section 139 outlines the standards for bird-safe buildings, including the requirements for location-related and feature-related hazards.



*The subject lot is not located within an Urban Bird Refuge. The Project meets the requirements of feature-related standards and does not include any unbroken glazed segments 24 square feet and larger in size; therefore, the Project complies with Planning Code Section 139.*

- G. Dwelling Unit Exposure.** Planning Code Section 140 requires that at least one room of all dwelling units face onto a public street, public alley at least 20 feet in width, side yard at least 25 feet in width, a rear yard meeting the requirements of this Code or other open area that meets minimum requirements for area and horizontal dimensions.

*The Project organizes the dwelling units to have exposure either on 16<sup>th</sup> Street, Mississippi Street, 17<sup>th</sup> Street or along the inner courtyards or mid-block promenades. Since 16<sup>th</sup> Street, Mississippi Street and 17<sup>th</sup> Street are all greater than 25 feet in width, all street-facing units comply with the dwelling unit exposure requirements. Since the inner courtyards within the 6-story tall 16<sup>th</sup> Street Building are at least 40 feet wide by 40 feet in length, and since the inner courtyards within the 4-story tall 17<sup>th</sup> Street Building are at least 35 feet wide by 35 feet in length all courtyard-facing units meet the dimensional requirements of the Planning Code. Lastly, the 30-foot wide north-south mid-block and 39-foot east-west residential mews meet the minimum clearance dimensional requirement for those units facing onto these spaces.*

- H. Street Frontage in Mixed Use Districts.** Planning Code Section 145.1 requires off-street parking at street grade on a development lot to be set back at least 25 feet on the ground floor; that no more than one-third of the width or 20 feet, whichever is less, of any given street frontage of a new structure parallel to and facing a street shall be devoted to parking and loading ingress or egress; that space for active uses be provided within the first 25 feet of building depth on the ground floor; that non-residential uses have a minimum floor-to-floor height of 14 feet; that the floors of street-fronting interior spaces housing non-residential active uses and lobbies be as close as possible to the level of the adjacent sidewalk at the principal entrance to these spaces; and that frontages with active uses that are not residential or PDR be fenestrated with transparent windows and doorways for no less than 60% of the street frontage at the ground level.

*The Project meets all the requirements of Planning Code Section 145.1, with the exception of the maximum parking and loading entrance width requirement. The off-street parking is located on the ground level and within a single subterranean level but is located in the interior of the buildings either below grade or setback at least 30 feet from the street frontages. At-grade parking is setback approximately, 50 feet from the 16<sup>th</sup> Street frontage, 30 feet from the 16<sup>th</sup> Street frontage, and 40 feet from the north-south pedestrian promenade mid-block alley. At grade, within the first 25 feet from the street-fronting property lines, the ground floor plan consists of active uses including residential walk-up units with direct, individual pedestrian access to a public sidewalk along 17<sup>th</sup> Street, residential lobbies, residential fitness room and lobby, and retail tenant spaces. All non-residential ground floor uses (retail) have a minimum floor to floor height of 17 feet with floors of street-fronting interior spaces housing non-residential active uses and lobbies level with the adjacent sidewalk at the principal entrances into these spaces. In addition, the Project features floor-to-floor aluminum storefront systems with clear glazing that meets the ground-level visual transparency and fenestration requirements of Planning Code Section 145.1.*



*The 263-space at-grade and subterranean garage for the 16<sup>th</sup> Street Building is accessed from a 20-foot wide curb cut and driveway entrance along Mississippi Street. An adjacent 12-foot wide curb cut and driveway entrance provide vehicular access to a retail loading and trash area. The 125-space subterranean garage for the 17<sup>th</sup> Street Building is accessed from a single 20-foot wide curb cut and driveway entrance along Mississippi Street. Since collectively, the proposed curb cuts for parking and loading entrances exceed 20 feet along the Mississippi Street frontage, the Project does not meet Planning Code Section 145.1, the Project is seeking a modification of this requirement as part of the Large Project Authorization (see below).*

- I. **Off-Street Parking.** Planning Section 151.1 of the Planning Code allows off-street parking at a maximum ratio of .75 per dwelling unit. For the 395 dwelling units proposed, the Project is allowed a maximum of 296 off-street parking spaces. Planning Code Section 151.1 allows dwelling units within the UMU District with at least 2 bedrooms and at least 1,000 square feet of occupied floor area a maximum parking ratio of 1.0 subject to the space-efficient criteria of Planning Code Section 151.1(g). Planning Code Section 151.1(g) establishes the following criteria:

*The Project provides a total of 336 residential parking spaces including 221 spaces within the 16<sup>th</sup> Street Building and 115 spaces within the 17<sup>th</sup> Street Building. Since the Project seeks one residential parking space per 2-bedroom and 3-bedroom unit, the Project is seeking a modification of this requirement as part of the Large Project Authorization (see below).*

With regard to the non-residential uses, Planning Code Section 151.1 of the Planning Code allows 1 space per 500 square feet of retail space. With a total of 24,468 square feet of retail space, the Project is allowed a maximum of 49 retail parking spaces.

*The Project provides 47 off-street parking spaces for retail use; therefore, the Project complies with Planning Code Section 151.1 maximum parking requirements for retail use.*

- J. **Off-Street Freight Loading.** Planning Code Section 152.1 requires three off-street loading spaces for residential uses above 500,000 gross square feet.

*The Project includes approximately 591,484 gross square feet of residential area and 24,968 gross square feet of retail use; therefore, at least three residential off-street loading spaces and one retail off-street loading space are required. The Project does not possess any off-street residential loading spaces; however, the Project is proposing one compliant off-street retail loading zone within the building accessed from Mississippi Street, and two 80-foot long on-street loading zones on Mississippi Street. One of these 80-foot loading zones would be located direct in front of the Project's east-west residential mews and the other is nearer the 17<sup>th</sup> Street corner. Therefore, the Project is seeking a modification from the residential loading requirement as part of the Large Project Authorization (see below).*

- K. **Bicycle Parking.** Planning Section 155.2 of the Planning Code requires for buildings containing more than 100 dwelling units, Class 1 bicycle parking spaces plus one Class 1 space for every four dwelling units over 100 and one Class 2 bicycle parking spaces for every 20 dwelling units. With regard to retail space, Planning Code Section 155.2 requires



one Class 1 bicycle parking space per 7500 square feet and one Class 2 bicycle parking space per 2500 square feet with a minimum of two spaces.

*The Project includes 395 dwelling units with 24,468 square feet of retail; therefore, the Project is required to provide a total of 177 Class 1 bicycle parking spaces and 30 Class 2 bicycle parking spaces. The Project will provide 455 Class 1 bicycle parking spaces and 52 Class 2 bicycle parking spaces, thus significantly exceeding the minimum Code requirement for bicycle parking. Therefore, the Project complies with Planning Code Section 155.2.*

- L. **Car Share Requirements.** For a project with 201 dwelling units or more, Planning Code Section 166 requires two car-share parking spaces plus one for every 200 dwelling units over 200.

*Since the Project includes 395 dwelling units, it is required to provide a minimum of two car-share parking space. The Project provides five off-street car share parking space within the garage; therefore, the Project complies with Planning Code Section 166.*

- M. **Unbundled Parking.** Planning Code Section 167 requires that all off-street parking spaces accessory to residential uses in new structures of 10 dwelling units or more be leased or sold separately from the rental or purchase fees for dwelling units for the life of the dwelling units.

*The Project is providing off-street parking that is accessory to the dwelling units. These spaces will be unbundled and sold and/or leased separately from the dwelling units; therefore, the Project complies with Planning Code Section 167.*

- N. **Dwelling Unit Mix.** Planning Code Section 207.6 requires that no less than 40% of the total number of proposed dwelling units contain at least two bedrooms, or no less than 30% of the total number of proposed dwelling units contain at least three bedrooms.

*For the 395 dwelling units, the Project is required to provide at least (158) two-bedroom or larger units or (119) 3-bedroom or larger units. The Project provides (14) 3-bedrooms, (146) 2-bedrooms, (182) 1- bedrooms, and (53) studios; thus, 40.5% of the total number of proposed dwelling units as two-bedroom or larger. Therefore, the Project meets the dwelling unit mix requirements of Planning Code Section 207.6.*

- O. **Horizontal Mass Reduction.** Planning Code Section 270.1 outlines the requirements for horizontal mass reduction on large lots within the Eastern Neighborhoods Mixed-Use Districts. For buildings with street frontage greater than 200-feet in length, one or more mass reduction breaks must be incorporated to reduce the horizontal scale of the building into discrete sections not more than 200-feet in length. Specifically, the mass reduction must 1) be not less than 30-feet in width; 2) be not less than 60-feet in depth from the street-facing building façade; 3) extend up to the sky from a level not higher than 25-feet above grade or the third story, whichever is lower; and 4) result in discrete building sections with a maximum plan length along the street frontage not greater than 200-feet.



*Given the 350 linear feet of building frontage along 16<sup>th</sup> Street, the Project is required to provide one or more mass breaks along this frontage which is not less than 30-feet wide by 60-feet deep starting at the third story and open to the sky. Along the 16<sup>th</sup> Street frontage, the Project provides a mass break 57.75-feet wide and 30.3-feet deep that begins at the third story and is open to the sky. This mass break, divides the 16<sup>th</sup> Street elevation in two distinct building segments which measure 179'-10" and 62'-0".*

*Given the 350 linear feet of building frontage along 17<sup>th</sup> Street, the Project is required to provide one or more mass breaks along this frontage which is not less than 30-feet wide by 60-feet deep starting at the third story and open to the sky. Along the 17<sup>th</sup> Street frontage, the Project provides a mass break 61.33 feet wide and more than 30-feet deep that begins at the third story and is open to the sky. This mass break divides the 17<sup>th</sup> Street elevation into two distinct building segments which measure 173'-0" and 79'-3".*

*Since the horizontal mass breaks described above for 16<sup>th</sup> Street does not meet the dimensions required by Code Section 270.1, the Project is seeking a modification of this requirement as part of the Large Project Authorization (see below). The horizontal mass break described above for 17<sup>th</sup> Street complies with Planning Code Section 270.1.*

- P. **Mid-Block Alley.** Planning Code Section 270.2 outlines the requirements for mid-block alleys on large lots within the Eastern Neighborhoods Mixed-Use Districts. This requirement applies to all new construction on parcels that have one or more street frontage of over 200 linear feet on a block face longer than 400 feet between intersections. On lots with frontage of over 200 linear feet on a block face longer than 400 feet between intersections. On lots with frontage greater than 300 feet, the project shall provide a publicly-accessible mid-block alley for the entire depth of the property, generally located toward the middle of the subject block face, perpendicular to the subject frontage and connecting to any existing streets and alleys.

*The property frontage along 16<sup>th</sup> and 17<sup>th</sup> Street of 380 feet per street frontage exceeds 200 linear feet on a block face (480 feet) that exceeds 400 linear feet; therefore, a publicly-accessible north-south mid-block alley in accordance with Planning Code Section 270.2 is required.*

*The Project provides a 30-foot wide, 10,800 sf, publicly-accessible north-south mid-block alley designed in accordance with Planning Code Section 270.2; therefore, the Project complies with Planning Code Section 270.2.*

- Q. **Shadow.** Planning Code Section 295 restricts net new shadow, cast by structures exceeding a height of 40 feet, upon property under the jurisdiction of the Recreation and Park Commission. Any project in excess of 40 feet in height and found to cast net new shadow must be found by the Planning Commission, with comment from the General Manager of the Recreation and Parks Department, in consultation with the Recreation and Park Commission, to have no adverse impact upon the property under the jurisdiction of the Recreation and Park Commission.



*The Department has conducted a preliminary shadow fan which indicates that the project does not cast any net new shadows upon property under the jurisdiction of the Recreation and Parks Commission.*

- R. **Inclusionary Affordable Housing Program.** The Project contains two buildings that have two different Inclusionary Affordable Housing requirements. The portion of the Project located in the 48-X Height and Bulk District has a Tier A UMU District Affordable Housing Requirement and the portion of the Project located in the 68-X Height and Bulk District has a Tier B UMU District Affordable Housing Requirement. Planning Code Section 415 and 419 sets forth the requirements and procedures for the Inclusionary Affordable Housing Program. Under Planning Code Section 415.3 and 419.3, these requirements would apply to projects that consist of 10 or more units, where the first application (EE or BPA) was applied for on or after July 18, 2006. Pursuant to Planning Code Section 415.5, 415.6, and 419.5 the current Inclusionary Affordable Housing Program requirement for the On-site Affordable Housing Alternative is to provide 16% of the proposed dwelling units as affordable for that portion of the Project within the 68-X Height and Bulk District and 14.4% of the proposed dwelling units as affordable for that portion of the Project within the 48-X Height and Bulk District. Pursuant to Planning Code Section 415.5, the Project must pay the Affordable Housing Fee ("Fee"). This Fee is made payable to the Department of Building Inspection ("DBI") for use by the Mayor's Office of Housing and Community Development for the purpose of increasing affordable housing citywide. This requirement is subject to change under a proposed Charter amendment and pending legislation if the voters approve the Charter Amendment at the June 7, 2016 election. This requirement is subject to change under a proposed Charter amendment and pending legislation if the voters approve the Charter Amendment at the June 7, 2016 election.

*The Project Sponsor has demonstrated that it is eligible for the On-Site Affordable Housing Alternative under Planning Code Section 415.5, 415.6 and 419.3, and has submitted a 'Affidavit of Compliance with the Inclusionary Affordable Housing Program: Planning Code Section 415,' to satisfy the requirements of the Inclusionary Affordable Housing Program by providing the affordable housing on-site instead of through payment of the Affordable Housing Fee for the 16th Street Building and by payment of the Affordable Housing Fee for the 17th Street Building. In order for the Project Sponsor to be eligible for the On-Site Affordable Housing Alternative, the Project Sponsor must submit an 'Affidavit of Compliance with the Inclusionary Affordable Housing Program: Planning Code Section 415,' to the Planning Department stating that any affordable units designated as on-site units shall be sold as ownership units and will remain as ownership units for the life of the project or submit to the Department a contract demonstrating that the project's on- or off-site units are not subject to the Costa Hawkins Rental Housing Act, California Civil Code Section 1954.50 because, under Section 1954.52(b), the Project Sponsor has entered into an agreement with a public entity in consideration for a direct financial contribution or any other form of assistance specified in California Government Code Sections 65915 et seq. and submits an Affidavit of such to the Department. All such contracts entered into with the City and County of San Francisco must be reviewed and approved by the Mayor's Office Housing and the City Attorney's Office. The Project Sponsor has indicated the intention to enter into an agreement with the City to qualify for a waiver from the Costa-Hawkins Rental Housing Act based upon the proposed density bonus and concessions provided by the City and approved herein. The Project Sponsor submitted such Affidavit on April 28, 2016 and a draft of the Costa Hawkins agreement on April 27, 2015. The EE application was submitted on April*



4, 2012. Pursuant to Planning Code Section 415.3, 415.6 and 419.3 the current on-site requirement is 16% for 16th Street building. 42 units (9 studios, 17 one-bedroom, 15 two-bedroom, and 1 three-bedroom) of the 260 units provided within the 16th Street Building will be affordable rental units. The Project Sponsor has indicated the intention to comply with the Inclusionary Affordable Housing Program by payment of the Affordable Housing Fee prior to the first site or building permit issuance for the 17th Street building portion of the Project at a current rate equivalent to an off-site requirement of 23%. If the Project becomes ineligible to meet its Inclusionary Affordable Housing Program obligation through the On-site Affordable Housing Alternative, it must pay the Affordable Housing Fee with interest, if applicable. The Project must execute the Costa Hawkins agreement prior to Planning Commission approval or must revert to payment of the Affordable Housing Fee.

- S. **Transit Impact Development Fee (TIDF) & Transportation Sustainability Fee (TSF).** Planning Code Sections 411 and 411A are applicable to new development over 800 square feet.

*The Project includes 24,468 square feet of non-residential use; however, the existing site contains approximately 105,000 square feet of PDR use. The Project would also be subject to a fee for the new residential gross square feet of approximately 591,484 gross square feet (subject to the residential TSF at one half the cost, based on adopted grandfathering). Additionally, the Project will receive a prior use credit, as outlined in Planning Code Section 411.4(b). These fees must be paid prior to the issuance of the building permit application.*

- T. **Residential Child Care Impact Fee.** Pursuant to Section 414A, the Project Sponsor shall pay the in-lieu fee as required. The net addition of gross floor area subject to the fee shall be determined based on drawings submitted with the Building Permit Application.

*The Project proposes a 395-unit mixed-use development with approximately 591,484 gross square feet of residential floor area. Therefore, the Project would be subject to the Childcare Fee which must be paid prior to the issuance of the building permit application.*

- U. **Eastern Neighborhood Infrastructure Impact Fees.** Planning Code Section 423 is applicable to any development project within the UMU (Urban Mixed-Use) Zoning District that results in the addition of at least one net new residential unit.

*The Project proposes the replacement of an existing 105,000 square foot industrial building with new construction of a North Building (16<sup>th</sup> Street Building): a 6-story, 68-foot tall, 402,943 square foot, mixed-use building with 260 dwelling units, and 20,318 square feet of retail, as well as, a South Building (17<sup>th</sup> Street Building): a 4-story, 48-foot tall, 213,009 square foot, mixed-use building with 135 dwelling units, and 4,150 square feet of retail. Therefore, the Project is subject to Eastern Neighborhood Infrastructure Impact Fees, as outlined in Planning Code Section 423. This fee must be paid prior to the issuance of the building permit application.*

7. **Large Project Authorization in Eastern Neighborhoods Mixed Use District.** Planning Code Section 329(c) lists nine aspects of design review in which a project must comply; the Planning Commission finds that the project is compliant with these nine aspects as follows:



A. Overall building mass and scale.

*The Project's mass and scale are appropriate for a large corner lot along the 16<sup>th</sup> Street corridor which includes a number of new and recently approved 6-story mixed-use developments, including the project directly across 16<sup>th</sup> Street, to the north, which is a new comparably-sized 453-unit, mixed-use development project (referred to as "Potrero 1010") currently under construction. This neighboring project consists of two, 6-story buildings with residential units above flex and retail spaces located on a triangular site. The comparable mass and scale of these two projects, in particular, will provide a well-defined termination of the 16<sup>th</sup> Street corridor as it meets the I-280 elevated freeway. The broader context includes a variety of 1-2 story, large rectangular-shaped industrial buildings that typically occupy their entire lots, 2-3 story mixed-use buildings and 3-story live/work buildings. The Project massing is also compatible with the adjacent elevated Interstate 280 Freeway bordering the site along the eastern edge. The Project is compatible with the mass and scale of other industrial properties and the larger scale four- and five-story mixed-use properties located within the broader neighborhood on 17<sup>th</sup> Street and along the 16<sup>th</sup> Street corridor, including the recently approved 6-story, 172 dwelling unit mixed-use development located at 1301 16<sup>th</sup> Street. The Project composes the massing in a manner that maintains a well-defined street wall along the three street frontages while providing distinct massing breaks that allow for mid-block passageways (including a north-south mid-block that intersects with an east-west residential mews) and upper level horizontal mass breaks along the 16<sup>th</sup> Street and 17<sup>th</sup> Street frontages. Thus, the Project is appropriate and consistent with the mass and scale of the surrounding neighborhood.*

B. Architectural treatments, facade design and building materials:

*The Project's architectural treatments, façade design and building materials for the 16<sup>th</sup> Street Building incorporate a palate of quality materials and finishes, including cement plaster, corrugated metal panels, smooth metal panels, corten steel, extruded aluminum-framed windows, brick, cement tile, aluminum storefront system with transparent glass, glass and cable-rail guardrails, and board-formed concrete. The 17<sup>th</sup> Street Building materials include corrugated metal siding and cast concrete, restored brick, board-formed concrete, cement and fiber cement panels, and metal railings. The Project's façade design in terms of both materiality and composition successfully references the light industrial character of the surrounding neighborhood in its ordered, regular fenestration pattern composed of large rectangular windows, rhythm of bays, and large rectangular openings at the ground level commercial spaces that are similar in scale to many of the warehouse garage door dimensions found on a typical industrial building in the neighborhood. The building materials are textural in nature and the extruded aluminum-framed windows on the 16<sup>th</sup> Street Building's north elevation emphasizes the structural framework of the building which relates well to the industrial character of the existing neighborhood. Along the 17<sup>th</sup> Street south elevation, the Project responds well to the existing historic brick building being retained and restored, by using its height as a datum line for the 2-story expression along the entire 17<sup>th</sup> Street frontage. The ceiling height of the visually transparent aluminum storefront system applied consistently throughout reinforces an active ground floor along the street, which draws from the industrial language of the surrounding neighborhood.*



- C. The design of lower floors, including building setback areas, commercial space, townhouses, entries, utilities, and the design and siting of rear yards, parking and loading access;

*At the ground floor along 16<sup>th</sup> Street, the Project provides a well-defined, centrally-located residential lobby flanked by abundant retail tenant spaces (6 total) totaling 20,318 square feet. At the ground floor along Mississippi Street, the Project provides another well-defined, centrally-located residential lobby immediately adjacent to the east-west residential mews entrance. At the corners, retail tenant spaces wrap around to 16<sup>th</sup> and 17<sup>th</sup> Streets. At the ground floor along 17<sup>th</sup> Street, the Project provides walk-up flexible use dwelling units with individual pedestrian access flanked by two retail spaces. These flex dwelling units and retail spaces provide for activity along the street and serve to further connect the building with the neighborhood. The height of the non-residential uses along the ground floor have a ceiling height of at least 17 feet which is well-proportioned with the façade as a whole. The Project features a well-defined yet architecturally varied ground floor utilizing a quality and textural palate of materials including aluminum storefront system with clear glazing, corrugated concrete and metal siding, board-formed concrete, large rectangular-shaped lumber-framed commercial openings, cement panels, restored brick and metal railings. The ground floor conforms to the active street frontage requirements of Planning Code Section 145.1.*

- D. The provision of required open space, both on- and off-site. In the case of off-site publicly accessible open space, the design, location, access, size, and equivalence in quality with that otherwise required on-site;

*The Project provides the required open space for its 395 dwelling units through 27,268 square feet of usable Code-complying non-publicly accessible common open space (via residential interior courtyards, a residential mews, and roof decks, as well as, 12,219 square feet of usable Code-complying publicly-accessible open space (via a north-south pedestrian promenade mid-block alley and pockets of open spaces). In addition, private open spaces in the form of patios, decks/balconies are provided for dwelling units on upper floors.*

- E. The provision of mid-block alleys and pathways on frontages between 200 and 300 linear feet per the criteria of Section 270, and the design of mid-block alleys and pathways as required by and pursuant to the criteria set forth in Section 270.2;

*The Project is subject to the mid-block alley requirements of Planning Code Section 270.2 since the 16<sup>th</sup> and 17<sup>th</sup> Street frontages are over 200 linear feet on a block face longer than 400 linear feet between intersections and is located within the Eastern Neighborhoods Mixed-Use District. In accordance with this provision, the Project provides a 30-foot wide, north-south, publicly-accessible pedestrian mid-block alley connecting 16<sup>th</sup> Street to 17<sup>th</sup> Street along the site's western property line. The mid-block is designed in accordance with the design and performance standards of Planning Code Section 270.2(e).*

- F. Streetscape and other public improvements, including tree planting, street furniture, and lighting.



*In accordance with Planning Code Section 138.1, the Project provides a total of 51 new, regularly-spaced street trees along the 16<sup>th</sup> Street, Mississippi Street, and 17<sup>th</sup> Street frontages. The Project Sponsor would pay an in-lieu fee for any required street trees not provided due to proximity of underground utilities, etc., as specified by the Department of Public Works. In addition, the Project includes comprehensive streetscape elements, including sidewalk landscaping and paving, sidewalk bicycle racks, and site furnishings. The sidewalk dimensions (from property line to curb) along each of the three frontages are proposed as 15 feet along 16<sup>th</sup> Street, 15 feet along Mississippi Street, and 10 feet along 17<sup>th</sup> Street in accordance with the San Francisco Better Streets Plan. The Department finds that these improvements would significantly improve the public realm.*

G. Circulation, including streets, alleys and mid-block pedestrian pathways;

*The Project provides ample circulation in and around the Project site through comprehensive sidewalk improvements, well defined walk-up entrances along the street, prominent residential lobby entrances and vehicular garage entrances accessed from Mississippi Street to an at-grade and subterranean level garage. Vehicular access is limited to two 20-foot wide curb cuts and driveway openings, and a single 12-foot wide curb cut and driveway along Mississippi so as not to interfere with anticipated MTA transit improvements along the 16<sup>th</sup> Street corridor and the approved bicycle lane on 17<sup>th</sup> Street. Vehicular access taken from Mississippi, a secondary street, is also intended to mitigate negative circulation impacts to 16<sup>th</sup> Street which is the primary transit corridor.*

H. Bulk limits;

*The Project is within an 'X' Bulk District, which does not restrict bulk.*

I. Other changes necessary to bring a project into conformance with any relevant design guidelines, Area Plan or Element of the General Plan;

*The Project, on balance, meets the Objectives and Policies of the General Plan. See Below.*

**8. Additional Design Standards for Large Project Authorizations (LPA) in Showplace Square/Potrero Hill Area Plan.** Per Board of Supervisor's File No. 151281, effective February 19, 2016, Planning Commission shall consider additional design standards for projects in the Showplace Square, Potrero Hill, and Central Waterfront Area Plans receiving a Large Project Authorization. Approvals for an LPA under Planning Code Section 329 within the applicable Plan Area must conform to the existing provisions of the Planning Code and must also demonstrate the following:

A. An awareness of urban patterns, and harmonizes visual and physical relationships between existing buildings, streets, open spaces, natural features, and view corridors;

*The mass and scale of the Project are appropriate for the large corner lot and the surrounding context. The surrounding context is light industrial and mixed-use in character. The industrial buildings tend to be broad and rectangular in form, whereas, the newer mixed-use residential/commercial developments tend to be 4-6 stories in height with a height and bulk consistent with the underlying*



*height and bulk district while maintaining a defined street wall. The proposed Project is compatible with the mass and scale of the nearby industrial properties, as well as, the larger-scale four- and five-story mixed-use properties located nearby. Along the 17<sup>th</sup> Street south elevation, the Project responds well to the existing historic brick building being retained and restored, by using its height as a datum line for the 2-story massing expression along the entire 17<sup>th</sup> Street frontage. Given the lack of any mid-block open space on this predominantly industrial block, the Project composes the massing around two distinct mid-block alleys/passages including a 30-foot wide north-south mid-block alley along the western property line that intersects with a 39-foot wide east-west residential mews that bisects the site laterally with a centrally-located entrance along the Mississippi Street frontage. The Project will add up to 51 new street trees and will activate the three street frontages at the Project site by replacing an industrial building with a mixed-use building that includes 24,468 square feet of ground floor commercial spaces and dwelling units with sidewalk-facing stoops that will activate the adjacent street frontages and greatly enhance the pedestrian environment surrounding the Project.*

- B. An awareness of neighborhood scale and materials, and renders building facades with texture, detail, and depth; and

*The surrounding context is light industrial and mixed-use in character. Many of the industrial buildings in the surrounding neighborhood are broad and rectangular in form with large uniformly-composed metal-framed windows. The materiality of surrounding older buildings include textured and smooth stucco, brick, metal-framed windows while many of the newer buildings include cement board, metal paneling, clear storefront glazing, metal-framed windows, smooth stucco and natural wood. The Project articulates the massing by providing upper level setbacks on the 17<sup>th</sup> Street elevation while maintaining a well-defined street wall along the 16<sup>th</sup> Street corridor. On the 17<sup>th</sup> Street elevation, ground level dwelling units feature walk-up entrances from the street to provide a pedestrian scale at the building's ground floor. The Project relates to both the older industrial structures and newer residential buildings in the neighborhood by using a mix of building materials that for the 16<sup>th</sup> Street Building incorporate a palate of quality materials and finishes, including cement plaster, corrugated metal panels, smooth metal panels, corten steel, extruded aluminum-framed windows, brick, cement tile, aluminum storefront system with transparent glass, glass and cable-rail guardrails, and board-formed concrete. The 17<sup>th</sup> Street Building materials include corrugated metal siding and cast concrete, restored brick, board-formed concrete, cement and fiber cement panels, and metal railings. The building materials are textural in nature and the extruded aluminum-framed windows on the 16<sup>th</sup> Street Building's north elevation emphasizes the structural framework of the building which relates well to the industrial character of the existing neighborhood.*

- C. A modulation of buildings vertically and horizontally, with rooftops and façade designed to be seen from multiple vantage points.

*The Project design modulates both vertically and horizontally by providing an ordered and regular fenestration pattern composed of large rectangular windows within vertically-rectangular bays across each elevation. Along the 17<sup>th</sup> Street south elevation, the Project uses the existing historic brick building's height as a datum line to inform the 2-story massing expression along the entire 17<sup>th</sup> Street frontage above which the building is setback 7 feet to further emphasize the 2-story scale that relates well to the lower scale surrounding building. Along the Mississippi Street elevation, a series of five distinct vertically-rectangular, equally spaced, angled bays which lead into a large rectangular, horizontally oriented bay projection balances the overall façade. Horizontal building modulation is*



*successfully expressed through the use of an extruded aluminum-framework that aligns with the fenestration pattern across the entire 16<sup>th</sup> Street north elevation. The saw-tooth roof forms for the 17<sup>th</sup> Street building are reminiscent of older industrial buildings and will provide visual interest when viewing the site from uphill.*

9. **Large Project Authorization Exceptions.** Proposed Planning Code Section 329 allows exceptions for Large Projects in the Eastern Neighborhoods Mixed Use Districts:

- A. Rear Yard: Exception for rear yards, pursuant to the requirements of Section 134(f);

The rear yard requirement in Eastern Neighborhoods Mixed Use Districts may be modified or waived by the Planning Commission pursuant to Section 329. The rear yard requirement in Eastern Neighborhoods Mixed Use Districts may be modified by the Zoning Administrator pursuant to the procedures set forth in Section 307(h) for other projects, provided that:

- (1) A comparable, but not necessarily equal amount of square footage as would be created in a code conforming rear yard is provided elsewhere within the development;

*The Project provides a comparable amount of open space, in lieu of the required rear yard. Overall, the Project site is 152,000 square feet in area, and would be required to provide a rear yard measuring 38,000 square feet. The Project provides a total of 27,268 square feet of usable Code-complying non-publicly accessible common open space (via a residential interior courtyards, a residential mews, and roof decks, as well as, 12,219 square feet of usable Code-complying publicly-accessible open space (via a north-south pedestrian promenade mid-block alley and pockets of open spaces), thus exceeding the amount of open space which would have been provided in Code-compliant rear yard. As a whole, the Project exceeds the total amount of open space which would have been provided through a qualifying rear yard by 6,437 square feet. The open spaces provided at ground and podium level within the Project that are open to the sky and provide massing breaks represent approximately 25.5% of the total lot area.*

- (2) The proposed new or expanding structure will not significantly impede the access to light and air from adjacent properties or adversely affect the interior block open space formed by the rear yards of adjacent properties; and

*The existing block is predominantly industrial in nature and there is no mid-block open space pattern present. The adjacent and surrounding buildings are predominantly industrial, large rectangular-shaped buildings with few windows. The Project's massing is organized around two distinct mid-block passages, including a 30-foot wide north-south mid-block alley adjacent to the rear yards of the two live-work buildings on Missouri Street and a 39-foot wide pedestrian mews. The Project does not impede access to light and air for the adjacent properties, in that the mid-block alley provides light and air to the adjacent live-work buildings and other adjacent properties are not residential in nature and the subject block does not have a pattern of rear yard open space.*



(3) The modification request is not combined with any other residential open space modification or exposure variance for the project, except exposure modifications in designated landmark buildings under Section 307(h)(1).

*The Project is not seeking a modification to the open space requirements.*

- B. Permitted Obstructions Over the Street. Where not specified elsewhere in subsection (d) of Planning Code Section 329, modification of other Code requirements which could otherwise be modified as a Planned Unit Development (as set forth in Section 304), irrespective of the zoning district in which the property is located. Since Planning Code Section 304 allows for modification of permitted obstructions over the street requirements, the Project is eligible to seek obstructions over the street modification (for bay windows) as part of the Large Project Authorization request.

*The Project seeks a modification of the dimensional requirements associated with permitted obstructions over the street, as defined in Planning Code Section 136. Specifically, the Project Sponsor seeks a modification for the bay window projection at the southeast corner of the 16<sup>th</sup> Street Building (where the residential mews intersects Mississippi Street) at levels four and five which do not conform to Planning Code Section 136. The maximum length of each bay window or balcony shall be 15 feet at the line establishing the required open area, and shall be reduced in proportion to the distance from such line by means of 45 degree angles drawn inward from the ends of such 15-foot dimension, reaching a maximum of nine feet along a line parallel to and at a distance of three feet from the line establishing the required open area.*

*The Commission finds the modification to be appropriate and justified, and will improve the overall Project design. Given the Project's overall massing composition, scale and height of 68 feet, the bay window element is well scaled within the Mississippi Street elevation and the rectangular shape is compatible within the broader rectangular architectural form of the building and within the light industrial and mixed-use neighborhood context. The grid-like fenestration pattern with floor-to-floor glazing within the broader extruded bay frame, relates well to the vertically-rectangular series of five bays further north on the Mississippi Street elevation and serves to balance the elevation as a whole. The shape, size and placement of the proposed bay fits well within the façade composition and the manner in which it wraps around onto the interior residential mews elevation successfully signals this important mid-block passage. Lastly, since the total area occupied by the proposed bay windows adjacent to balconies approximates what would otherwise be occupied by a Code-compliant bay window configuration along the same frontage, such a modification can be supported.*

- C. Parking and Loading Entrance Width. Where not specified elsewhere in subsection (d) of Planning Code Section 329, modification of other Code requirements which could otherwise be modified as a Planned Unit Development (as set forth in Section 304), irrespective of the zoning district in which the property is located. Since Planning Code Section 304 allows for modification of parking and loading entrance width requirements, the Project is eligible to seek a modification for parking and loading entrance width as part of the Large Project Authorization request.



For projects in the Eastern Neighborhoods Mixed Use Districts that are subject to Section 329, the Planning Commission may waive these requirements per the procedures of Section 329 if it finds that the design of the project, particularly ground floor frontages, would be improved and that such loading could be sufficiently accommodated on adjacent streets and alleys.

*The Project's requested modification to the parking and loading entrance width is based on the anticipated MTA Transit Priority Project improvements along the 16<sup>th</sup> Street corridor and along 17<sup>th</sup> Street as well. MTA's plan is intended to address critical safety needs for people walking and bicycling, average transit speed and aging infrastructure. So as not to obstruct anticipated street improvements along the 16<sup>th</sup> Street primary corridor, the Project seeks to consolidate all vehicular access points along the secondary Mississippi Street. It is anticipated that no curb cuts will be allowed on 16<sup>th</sup> Street and Mississippi Street can accommodate such access in that it is 80 feet wide. All frontages, including the Mississippi Street frontage are designed in accordance with the active street frontage requirements of the Planning Code.*

- D. Off-Street Parking: Exceeding the principally permitted accessory residential parking ratio described in Section [151.1](#) and pursuant to the criteria therein;

(1) In granting such Conditional Use or exception per 329 for parking in excess of that principally permitted in Table 151.1, the Planning Commission shall make the following affirmative findings according to the uses to which the proposed parking is accessory:

(A) Parking for All Uses.

- (i) Vehicle movement on or around the project does not unduly impact pedestrian spaces or movement, transit service, bicycle movement, or the overall traffic movement in the district;
- (ii) Accommodating excess accessory parking does not degrade the overall urban design quality of the project proposal;
- (iii) All above-grade parking is architecturally screened and lined with active uses according to the standards of Section 145.1, and the project sponsor is not requesting any exceptions or variances requiring such treatments elsewhere in this Code; and
- (iv) Excess accessory parking does not diminish the quality and viability of existing or planned streetscape enhancements.

(B) Parking for Residential Uses.

- (i) For projects with 50 dwelling units or more, all residential accessory parking in excess of 0.5 spaces per unit shall be stored and accessed by mechanical stackers or lifts, valet, or other space-efficient means that reduces space used for parking and maneuvering, and maximizes other uses.



*The Project provides 336 residential off-street parking spaces without providing the space efficient means for those units that are 2-bedroom or larger and at least 1000 square feet in area as required by Code for parking for residential uses pursuant to Planning Code Section 151.1(g)(1)(B)(i); therefore, the Project's proposed amount of off-street residential parking exceeds the maximum 296 spaces allowed. Of the 336 residential off-street parking spaces allowed. Therefore, the Project does not comply with Planning Code Section 151.1 and exceeds the principally permitted maximum amount of off-street residential parking. Staff has evaluated the LPA modification sought to exceed the off-street parking maximum by 40 spaces; however, Staff is unable to support such a modification in that granting such modification would be contrary to the principles of the City's Transit First Policy established in Article 8A of the San Francisco Charter. Given the anticipated MTA transit improvements along the 16<sup>th</sup> Street corridor in addition to the comprehensive streetscape improvements proposed in accordance with the San Francisco Better Streets Plan, occupants will have increasingly greater transit, bicycle, and walking alternatives to travel than by private automobile.*

- E. Off-Street Loading Minimum. Exception from satisfaction of loading requirements per Section 152.1 pursuant to the criteria contained therein.

*The Project includes approximately 591,484 gross square feet of residential area; therefore, at least three off-street residential loading spaces are required. The Project does not possess any off-street loading parking spaces; however, the Project is proposing one compliant off-street retail loading zone within the building accessed from Mississippi Street, and two 80-foot long on-street loading zones on Mississippi Street; therefore, an exception is required and being sought. One of these 80-foot loading zones would be located direct in front of the Project's east-west residential mews and the other is nearer the 17<sup>th</sup> Street corner.*

*Given the existing and proposed character of the related street frontages, the Project can accommodate the two loading parking spaces on the street being developed in accordance with the San Francisco Better Streets Plan design standards. Furthermore, by providing for on-street loading, the Project has reduced the overall size and scale of the garage openings.*

- F. Horizontal Mass Reduction: Modification of the horizontal massing breaks required by Section 270.1 in light of any equivalent reduction of horizontal scale, equivalent volume of reduction, and unique and superior architectural design, pursuant to the criteria of Section 270.1(d).

The Planning Commission may modify or waive this requirement through the process set forth in Section 329. When considering any such application, the Commission shall consider the following criteria:

- 1) no more than 50% of the required mass is reduced unless special circumstances are evident;



*With regard to the requested modification along the 16<sup>th</sup> Street elevation, while the proportions of the mass break have been modified, the area open to the sky remains approximately the same.*

- 2) the depth of any mass reduction breaks provided is not less than 15 feet from the front facade, unless special circumstances are evident;

*The Project incorporates a mass break from the front façade that is 30'-4" deep, which is more than 15-ft deep from the front facade.*

- 3) the proposed building envelope can be demonstrated to achieve a distinctly superior effect of reducing the apparent horizontal dimension of the building; and

*The placement and width of the proposed mass break provide a clear reduction in horizontal length across the 16<sup>th</sup> Street elevation.*

- 4) the proposed building achieves unique and superior architectural design.

*The proposed mass break placement relates well to the Texas Street axis and allow for a narrower street fronting mass adjacent to the north-south mid-block.*

- G. Flexible Units: Modification of the accessory use provisions of Section [803.3\(b\)\(1\)\(c\)](#) for Dwelling Units. Dwelling Units modified under this Subsection shall continue to be considered Dwelling Units for the purposes of this Code and shall be subject to all such applicable controls and fees. Additionally, any building that receives a modification pursuant to this Subsection shall (i) have appropriately designed street frontages to accommodate both residential and modified accessory uses and (ii) obtain comment on the proposed modification from other relevant agencies prior to the Planning Commission hearing, including the Fire Department and Department of Building Inspection. Modifications are subject to the following:

- (i) A modification may only be granted for the ground floor portion of Dwelling Units that front on a street with a width equal to or greater than 40 feet.

*The Project seeks modification for the six dwelling units (four one-bedroom and two two-bedroom) on the ground floor along Mississippi Street, which are both more than 40 feet wide.*

- (ii) The accessory use may only include those uses permitted as of right at the subject property. However, uses permitted in any unit obtaining an accessory use modification may be further limited by the Planning Commission.

*The Project will only include accessory uses that are principally-permitted uses in the UMU Zoning District, as defined in Planning Code Section 843. The anticipated accessory uses will either be retail or home office.*



(iii) The Planning Commission may grant exceptions to the size of the accessory use, type and number of employees, and signage restrictions of the applicable accessory use controls.

*The Project is seeking modification to the accessory use provisions for dwelling units to allow for greater flexibility in the size of an accessory use on the ground floor level only, to provide for a limited number of employees, and to allow for public access.*

8. **General Plan Compliance.** The Project is, on balance, consistent with the following Objectives and Policies of the General Plan:

## **HOUSING**

### **Objectives and Policies**

#### **OBJECTIVE 1**

**IDENTIFY AND MAKE AVAILABLE FOR DEVELOPMENT ADEQUATE SITES TO MEET THE CITY'S HOUSING NEEDS, ESPECIALLY PERMANENTLY AFFORDABLE HOUSING.**

##### **Policy 1.1**

Plan for the full range of housing needs in the City and County of San Francisco, especially affordable housing.

*The Project is a medium-density residential development on an underutilized site in a transitioning industrial and residential area. The Project site is an ideal infill site that currently contains a surface parking lot and an approximately 105,000 square foot warehouse use, a vacant brick office building and a small modular office. The project site was rezoned to UMU as part of a long range planning goal to create a cohesive, higher density residential and mixed-use neighborhood while recognizing the importance of industrial areas. Although predominantly light industrial in character, the surrounding neighborhood also includes a variety of residential, residential/commercial mixed-use, grocery, restaurant, school and park uses. The Project's mixed-use composition and industrially-inspired form and design is compatible with the uses and character of the broader neighborhood context. The Project will provide on-site affordable housing for rent, which will provide opportunities for affordable housing across the City.*

#### **OBJECTIVE 4**

**FOSTER A HOUSING STOCK THAT MEETS THE NEEDS OF ALL RESIDENTS ACROSS LIFECYCLES**

##### **Policy 4.4**

Encourage sufficient and suitable rental housing opportunities, emphasizing permanently affordable rental units wherever possible.

*The Project meets the affordable housing requirements for the UMU Zoning District by providing on-site affordable housing units for rent within the 16<sup>th</sup> Street Building located within the 68-X Height and Bulk District and has elected to satisfy the affordable housing requirements for the 17<sup>th</sup> Street Building through payment of the applicable Affordable Housing Fee. The 16<sup>th</sup> Street Building will provide 42 affordable dwelling units into the City's housing stock.*



## **OBJECTIVE 11**

### **SUPPORT AND RESPECT THE DIVERSE AND DISTINCT CHARACTER OF SAN FRANCISCO'S NEIGHBORHOODS.**

#### **Policy 11.1**

Promote the construction and rehabilitation of well-designed housing that emphasizes beauty, flexibility, and innovative design, and respects existing neighborhood character.

#### **Policy 11.2**

Ensure implementation of accepted design standards in project approvals.

#### **Policy 11.4**

Continue to utilize zoning districts which conform to a generalized residential land use and density plan and the General Plan.

#### **Policy 11.6**

Foster a sense of community through architectural design, using features that promote community interaction.

*The architecture of this Project responds to the site's location as a transition between industrial zones and the contemporary and traditional architecture of residential zones. The Project's facades provide a unique expression not commonly found within the surrounding area, while providing for a varied material palette and appropriate massing given the character of the surrounding street. The Project relates to both the older industrial structures and newer residential buildings in the neighborhood by using a mix of building materials that for the 16<sup>th</sup> Street Building incorporate a palate of quality materials and finishes, including cement plaster, corrugated metal panels, smooth metal panels, corten steel, extruded aluminum-framed windows, brick, cement tile, aluminum storefront system with transparent glass, glass and cable-rail guardrails, and board-formed concrete. The 17<sup>th</sup> Street Building materials include corrugated metal siding and cast concrete, restored brick, board-formed concrete, cement and fiber cement panels, and metal railings. The building materials are textural in nature and the extruded aluminum-framed windows on the 16<sup>th</sup> Street Building's north elevation emphasizes the structural framework of the building which relates well to the industrial character of the existing neighborhood.*

## **TRANSPORTATION ELEMENT**

### **Objectives and Policies**

#### **OBJECTIVE 24:**

#### **IMPROVE THE AMBIENCE OF THE PEDESTRIAN ENVIRONMENT.**

##### **Policy 24.2:**

Maintain and expand the planting of street trees and the infrastructure to support them.

##### **Policy 24.3:**

Install pedestrian-serving street furniture where appropriate.



**Policy 24.4:**

Preserve pedestrian-oriented building frontages.

**OBJECTIVE 28:**

**PROVIDE SECURE AND CONVENIENT PARKING FACILITIES FOR BICYCLES.**

**Policy 28.1:**

Provide secure bicycle parking in new governmental, commercial, and residential developments.

**Policy 28.3:**

Provide parking facilities which are safe, secure, and convenient.

*The Project includes comprehensive streetscape improvements across all frontages (including 16<sup>th</sup> Street, Mississippi Street and 17<sup>th</sup> Street) including new street trees, sidewalk improvements, landscaping, street furniture, and Class 2 bicycle parking. These amenities significantly improve the pedestrian environment surrounding the site.*

**OBJECTIVE 34:**

**RELATE THE AMOUNT OF PARKING IN RESIDENTIAL AREAS AND NEIGHBORHOOD COMMERCIAL DISTRICTS TO THE CAPACITY OF THE CITY'S STREET SYSTEM AND LAND USE PATTERNS.**

**Policy 34.1:**

Regulate off-street parking in new housing so as to guarantee needed spaces without requiring excesses and to encourage low auto ownership in neighborhoods that are well served by transit and are convenient to neighborhood shopping.

**Policy 34.3:**

Permit minimal or reduced off-street parking supply for new buildings in residential and commercial areas adjacent to transit centers and along transit preferential streets.

*The Project provides a total of 388 at-grade and subterranean, off-street parking spaces; however, since the proposed residential parking amount (336 spaces) exceeds the maximum permitted .75 ratio allowed by Code, a modification is being sought as part of the Large Project Authorization. Staff has evaluated the LPA modification sought to exceed the off-street residential parking maximum by 40 spaces; however, Staff is unable to support such a modification in that granting such a modification would be contrary to the principles of the City's Transit First Policy. As such, the Project has been conditioned to comply with the maximum off-street residential parking requirement to better relate the amount of parking to the capacity of the City's street system and land use patterns.*

**URBAN DESIGN ELEMENT**

**Objectives and Policies**



**OBJECTIVE 1:**

**EMPHASIS OF THE CHARACTERISTIC PATTERN WHICH GIVES TO THE CITY AND ITS NEIGHBORHOODS AN IMAGE, A SENSE OF PURPOSE, AND A MEANS OF ORIENTATION.**

**Policy 1.7:**

Recognize the natural boundaries of districts, and promote connections between districts.

**OBJECTIVE 2:**

**CONSERVATION OF RESOURCES WHICH PROVIDE A SENSE OF NATURE, CONTINUITY WITH THE PAST, AND FREEDOM FROM OVERCROWDING.**

**Policy 2.6:**

Respect the character of older development nearby in the design of new buildings.

*The Project is located within the Potrero Hill neighborhood in an area that includes a mix of residential, commercial and industrial uses. As such, the Project provides expressive street façades, which respond to form, scale and material palette of the existing neighborhood, while also providing a new contemporary architectural vocabulary.*

**SHOWPLACE SQUARE/POTRERO AREA PLAN**

**Objectives and Policies**

**Land Use**

**OBJECTIVE 1.1**

ENCOURAGE THE TRANSITION OF PORTIONS OF SHOWPLACE/POTRERO TO A MORE

MIXED USE AND NEIGHBORHOOD-SERVING CHARACTER, WHILE PROTECTING THE CORE OF DESIGN-RELATED PDR USES.

**Policy 1.1.4**

Permit and encourage greater retail use on the ground floor on parcels that front 16<sup>th</sup> Street to take advantage of transit service and encourage more mixed uses, while protecting against the wholesale displacement of PDR uses.

*The proposed 395-dwelling unit mixed-use project provides for a total of eight separate retail tenant spaces distributed across the ground floor totaling 24,468 square feet. These retail tenant spaces have frontage along 16<sup>th</sup> Street, Mississippi Street and 17<sup>th</sup> Street and include retail spaces that wrap the corners of 16<sup>th</sup> and 17<sup>th</sup> Streets to better activate the corners. These ground floor uses are strategically grouped to take advantage of the transit service along the 16<sup>th</sup> Street corridor and have frontages that are designed in accordance with the active street frontage requirements of the Planning Code.*

**OBJECTIVE 1.2**



IN AREAS OF SHOWPLACE/POTERO WHERE HOUSING AND MIXED USE IS ENCOURAGED, MAXIMIZE DEVELOPMENT POTENTIAL IN KEEPING WITH NEIGHBORHOOD CHARACTER.

**Policy 1.2.1**

Ensure that in-fill housing development is compatible with its surroundings.

*The surrounding context is light industrial and mixed-use in character. Many of the industrial buildings in the surrounding neighborhood are broad and rectangular in form with large uniformly-composed metal-framed windows. The materiality of surrounding older buildings include textured and smooth stucco, brick, metal-framed windows while many of the newer buildings include cement board, metal paneling, clear storefront glazing, metal-framed windows, smooth stucco and natural wood. The Project articulates the massing by providing upper level setbacks on the 17<sup>th</sup> Street elevation while maintaining a well-defined street wall along the 16<sup>th</sup> Street corridor. On the 17<sup>th</sup> Street elevation, ground level dwelling units feature walk-up entrances from the street to provide a pedestrian scale at the building's ground floor. The Project relates to both the older industrial structures and newer residential buildings in the neighborhood by using a mix of building materials that for the 16<sup>th</sup> Street Building incorporate a palate of quality materials and finishes, including cement plaster, corrugated metal panels, smooth metal panels, corten steel, extruded aluminum-framed windows, brick, cement tile, aluminum storefront system with transparent glass, glass and cable-rail guardrails, and board-formed concrete. The 17<sup>th</sup> Street Building materials include corrugated metal siding and cast concrete, restored brick, board-formed concrete, cement and fiber cement panels, and metal railings. The building materials are textural in nature and the extruded aluminum-framed windows on the 16<sup>th</sup> Street Building's north elevation emphasizes the structural framework of the building which relates well to the industrial character of the existing neighborhood.*

**Housing**

**OBJECTIVE 2.1**

ENSURE THAT A SIGNIFICANT PERCENTAGE OF NEW HOUSING CREATED IN SHOWPLACE/POTRERO IS AFFORDABLE TO PEOPLE WITH A WIDE RANGE OF INCOMES.

**Policy 2.1.1**

Require developers in some formally industrial areas to contribute towards the City's very low, low, moderate and middle income needs as identified in the Housing Element of the General Plan.

**Policy 2.1.3**

Provide units that are affordable to households at moderate and "middle incomes"- working households earning above traditional below-market rate thresholds but still well below what is needed to buy a market rate priced home, with restrictions to ensure affordability continues.

*The Project provides a range of unit types and sizes that will be affordable to a range of income groups. The studio units will be relatively affordable as compared to the other units due to their smaller size, and the 16<sup>th</sup> Street Building provides 42 affordable units that range in size from studios to three-bedrooms.*



**OBJECTIVE 2.3**

REQUIRE THAT A SIGNIFICANT NUMBER OF UNITS IN NEW DEVELOPMENT HAVE TWO OR MORE BEDROOMS EXCEPT SENIOR HOUSING AND SRO DEVELOPMENTS UNLESS ALL BELOW MARKET RATE UNITS ARE TWO OR MORE BEDROOM UNITS.

**Policy 2.3.1**

Target the provision of affordable units for families.

**Policy 2.3.2**

Prioritize the development of affordable family housing, both rental and ownership, particularly along transit corridors and adjacent to community amenities.

*The 16<sup>th</sup> Street Building provides a total of 260 dwelling units of which 104 (or 40%) are two or three bedrooms, contributing to the City's stock of housing suitable for families. The 17<sup>th</sup> Street Building provides a total of 135 dwelling units of which 56 (or 41.4%) are two or three bedrooms. Also, 16 of the 42 affordable units provided by the Project have at least two bedrooms. Because the Project is located along a major transit corridor and within proximity of two Caltrain Stations, the larger units are well suited for families who rely on public transit.*

**Built Form****OBJECTIVE 3.2**

PROMOTE AN URBAN FORM AND ARCHITECTURAL CHARACTER THAT SUPPORTS WALKING AND SUSTAINS A DIVERSE, ACTIVE AND SAFE PUBLIC REALM.

**Policy 3.2.3**

Minimize the visual impact of parking.

*The Project minimizes the visual impact of parking by locating the garage within the interior of the buildings so that it cannot be seen from the street. The two ingress/egress points for the parking garages are each 20 feet wide and the single ingress/egress point for the retail loading and trash area is 12 feet wide across the Mississippi Street frontage which has a length of 237 feet. Garage doors shall be opaque in order to further diminish the visual impact of parking.*

**Policy 3.2.4**

Strengthen the relationship between a building and its fronting sidewalk.

*Along the 16<sup>th</sup> Street frontage, the Project's centrally-located residential lobby flanked by retail tenant spaces that wrap the 16<sup>th</sup> Street corner provides spaces that provide direct connection to the street. On the Mississippi Street frontage, the retail tenant spaces wrap both corners and there is small plaza-like public space approximately 39 feet wide by 29 feet deep that in front of the residential mews entry that will encourage public gathering and interactions. Along 17<sup>th</sup> Street, there is diversity of uses and spaces that line the ground floor frontage, including the entry point into the north-south mid-block, residential lobby, centrally-located retail, a series of six flexible residential units with raised entry stoops, and retail that*



wraps the corner onto Mississippi Street. In addition, all ground floor frontages are designed in accordance with active street frontage requirements of the Planning Code.

**Policy 3.2.6**

Sidewalks abutting new developments should be constructed in accordance with locally appropriate guidelines based on established best practices in streetscape design.

*In accordance with Planning Code Section 138.1, the Project provides a total of 51 new, regularly-spaced street trees along the 16<sup>th</sup> Street, Mississippi Street, and 17<sup>th</sup> Street frontages. The Project Sponsor would pay an in-lieu fee for any required street trees not provided due to proximity of underground utilities, etc., as specified by the Department of Public Works. In addition, the Project includes comprehensive streetscape elements, including sidewalk landscaping and paving, sidewalk bicycle racks, and site furnishings. The sidewalk dimensions (from property line to curb) along each of the three frontages are proposed as 15 feet along 16<sup>th</sup> Street, 15 feet along Mississippi Street, and 10 feet along 17<sup>th</sup> Street in accordance with the San Francisco Better Streets Plan. The Department finds that these improvements would significantly improve the public realm.*

9. **Planning Code Section 101.1(b)** establishes eight priority-planning policies and requires review of permits for consistency with said policies. On balance, the project does comply with said policies in that:

- A. That existing neighborhood-serving retail uses be preserved and enhanced and future opportunities for resident employment in and ownership of such businesses be enhanced.

*The project does not displace any neighborhood-serving retail uses.*

- B. That existing housing and neighborhood character be conserved and protected in order to preserve the cultural and economic diversity of our neighborhoods.

*The Project does not displace any existing housing, nor would the existing units in the surrounding neighborhood be adversely affected. The Project will enhance the neighborhood character in that the proposed mass, scale and architectural design are compatible with the light-industrial and mixed-use neighborhood context.*

- C. That the City's supply of affordable housing be preserved and enhanced.

*The Project will not displace any affordable housing because there is currently no housing on the site. The Project will comply with the City's Inclusionary Housing Program by providing 42 affordable units for rent within the 16<sup>th</sup> Street Building and pay the applicable Affordable Housing Fee for the 17<sup>th</sup> Street Building.*

- D. That commuter traffic not impede MUNI transit service or overburden our streets or neighborhood parking.



*The site is located on the south side of 16<sup>th</sup> Street between Missouri Street and Mississippi Street, within a mile from both the 4<sup>th</sup> and Townsend and 22<sup>nd</sup> Street Caltrain stations. The site is also located near the No. 22 MUNI line and is blocks from the No. 8, 10, 19, and 22 MUNI lines. It is presumable that residents would commute by transit thereby mitigating possible effects on street parking.*

- E. That a diverse economic base be maintained by protecting our industrial and service sectors from displacement due to commercial office development, and that future opportunities for resident employment and ownership in these sectors be enhanced.

*The Project is consistent with the Potrero Area Plan, which provides for a balance between industrial and residential development. The Project does not include commercial office development, and provides new housing, which is a priority for the City.*

- F. That the City achieve the greatest possible preparedness to protect against injury and loss of life in an earthquake.

*The Project will be designed and will be constructed to conform to the structural and seismic safety requirements of the Building Code. This proposal will not impact the property's ability to withstand an earthquake.*

- G. That landmarks and historic buildings be preserved.

*There are no landmarks on the project site. The existing historic 2-story brick building located along the 17<sup>th</sup> Street frontage will be preserved and occupied by a retail use. The Project successfully incorporates this brick building into the overall design by using its roof height as a datum line for the two-story expression along the entire 17<sup>th</sup> Street frontage. Further deference to this historic brick building is provided in the form of setbacks on either side by the new construction, aligning a massing break along the 17<sup>th</sup> Street elevation across its entire frontage and aligning a pedestrian entry into the site along its western edge.*

- H. That our parks and open space and their access to sunlight and vistas be protected from development.

*The Project will not affect the City's parks or open space or their access to sunlight and vistas. A shadow study was completed and concluded that the Project will not cast shadows on any property under the jurisdiction of, or designated for acquisition by, the Recreation and Park Commission.*

- 10. **First Source Hiring.** The Project is subject to the requirements of the First Source Hiring Program as they apply to permits for residential development (Section 83.4(m) of the Administrative Code), and the Project Sponsor shall comply with the requirements of this Program as to all construction work and on-going employment required for the Project. Prior to the issuance of any building permit to construct or a First Addendum to the Site Permit, the Project Sponsor shall have a First Source Hiring Construction and Employment Program approved by the First Source Hiring Administrator, and evidenced in writing. In the event that both the Director of Planning



and the First Source Hiring Administrator agree, the approval of the Employment Program may be delayed as needed.

*The Project Sponsor submitted a First Source Hiring Affidavit and prior to issuance of a building permit will execute a First Source Hiring Memorandum of Understanding and a First Source Hiring Agreement with the City's First Source Hiring Administration.*

11. The Project is consistent with and would promote the general and specific purposes of the Code provided under Section 101.1(b) in that, as designed, the Project would contribute to the character and stability of the neighborhood and would constitute a beneficial development.
12. The Commission hereby finds that approval of the Large Project Authorization would promote the health, safety and welfare of the City.



## DECISION

That based upon the Record, the submissions by the Applicant, the staff of the Department and other interested parties, the oral testimony presented to this Commission at the public hearings, and all other written materials submitted by all parties, the Commission hereby **APPROVES Large Project Authorization Application No. 2011.1300X** under Planning Code Section 329 to allow the new construction of two four-to-six-story mixed-use buildings with a total of 395 dwelling units, 24,468 gross square feet of retail space, and 388 off-street parking spaces, and to allow exceptions to the requirements for rear yard (Planning Code Section 134), permitted obstructions over the street (Planning Code Section 136), parking/loading entrance width (Planning Code Section 145.1), off-street loading (Planning Code Section 152.1), horizontal mass reduction (Planning Code Section 270.1), and flexible units-modification of the accessory use provisions for dwelling units (Planning Code Section 329(D)(10) and 803.3(B)(1)(C)), within UMU (Urban Mixed-Use) Zoning District and a 48-X & 68-X Height and Bulk District. The project is subject to the following conditions attached hereto as "EXHIBIT A" in general conformance with plans on file, dated May XX, 2016, and stamped "EXHIBIT B", which is incorporated herein by reference as though fully set forth.

The Planning Commission hereby adopts the MMRP attached hereto as Exhibit C and incorporated herein as part of this Motion by this reference thereto. All required mitigation measures identified in the Eastern Neighborhoods Plan EIR and contained in the MMRP are included as conditions of approval.

**APPEAL AND EFFECTIVE DATE OF MOTION:** Any aggrieved person may appeal this Section 329 Large Project Authorization to the Board of Appeals within fifteen (15) days after the date of this Motion. The effective date of this Motion shall be the date of adoption of this Motion if not appealed (after the 15-day period has expired) OR the date of the decision of the Board of Appeals if appealed to the Board of Appeals. For further information, please contact the Board of Appeals at (415) 575-6880, 1660 Mission, Room 3036, San Francisco, CA 94103.

**Protest of Fee or Exaction:** You may protest any fee or exaction subject to Government Code Section 66000 that is imposed as a condition of approval by following the procedures set forth in Government Code Section 66020. The protest must satisfy the requirements of Government Code Section 66020(a) and must be filed within 90 days of the date of the first approval or conditional approval of the development referencing the challenged fee or exaction. For purposes of Government Code Section 66020, the date of imposition of the fee shall be the date of the earliest discretionary approval by the City of the subject development.

If the City has not previously given Notice of an earlier discretionary approval of the project, the Planning Commission's adoption of this Motion, Resolution, Discretionary Review Action or the Zoning Administrator's Variance Decision Letter constitutes the approval or conditional approval of the development and the City hereby gives **NOTICE** that the 90-day protest period under Government Code Section 66020 has begun. If the City has already given Notice that the 90-day approval period has begun for the subject development, then this document does not re-commence the 90-day approval period.



I hereby certify that the Planning Commission ADOPTED the foregoing Motion on May 12, 2016.

Jonas P. Ionin  
Commission Secretary

AYES:

NAYS:

ABSENT:

ADOPTED:     May 12, 2016



# EXHIBIT A

## AUTHORIZATION

This authorization is for a Large Project Authorization to allow the new construction of two four-to-six-story mixed-use buildings with a total of 395 dwelling units, 24,468 gross square feet of retail space, and 388 off-street parking spaces, and to allow exceptions to the requirements for rear yard (Planning Code Section 134), permitted obstructions over the street (Planning Code Section 136), parking/loading entrance width (Planning Code Section 145.1), off-street loading (Planning Code Section 152.1), horizontal mass reduction (Planning Code Section 270.1), and flexible units-modification of the accessory use provisions for dwelling units (Planning Code Section 329(D)(10) and 803.3(B)(1)(C)); in general conformance with plans, dated May XX, 2016, and stamped "EXHIBIT B" included in the docket for Case No. 2011.1300X and subject to conditions of approval reviewed and approved by the Commission on May 12, 2016 under Motion No. XXXXX. This authorization and the conditions contained herein run with the property and not with a particular Project Sponsor, business, or operator.

## RECORDATION OF CONDITIONS OF APPROVAL

Prior to the issuance of the building permit or commencement of use for the Project the Zoning Administrator shall approve and order the recordation of a Notice in the Official Records of the Recorder of the City and County of San Francisco for the subject property. This Notice shall state that the project is subject to the conditions of approval contained herein and reviewed and approved by the Planning Commission on May 12, 2016 under Motion No. XXXXX.

## PRINTING OF CONDITIONS OF APPROVAL ON PLANS

The conditions of approval under the 'Exhibit A' of this Planning Commission Motion No. XXXXX shall be reproduced on the Index Sheet of construction plans submitted with the Site or Building permit application for the Project. The Index Sheet of the construction plans shall reference to the Office Development Authorization and any subsequent amendments or modifications.

## SEVERABILITY

The Project shall comply with all applicable City codes and requirements. If any clause, sentence, section or any part of these conditions of approval is for any reason held to be invalid, such invalidity shall not affect or impair other remaining clauses, sentences, or sections of these conditions. This decision conveys no right to construct, or to receive a building permit. "Project Sponsor" shall include any subsequent responsible party.

## CHANGES AND MODIFICATIONS

Changes to the approved plans may be approved administratively by the Zoning Administrator. Significant changes and modifications of conditions shall require Planning Commission approval of a new authorization.



## Conditions of Approval, Compliance, Monitoring, and Reporting

### PERFORMANCE

**Validity.** The authorization and right vested by virtue of this action is valid for three (3) years from the effective date of the Motion. The Department of Building Inspection shall have issued a Building Permit or Site Permit to construct the project and/or commence the approved use within this three-year period.

*For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, [www.sf-planning.org](http://www.sf-planning.org)*

**Expiration and Renewal.** Should a Building or Site Permit be sought after the three (3) year period has lapsed, the project sponsor must seek a renewal of this Authorization by filing an application for an amendment to the original Authorization or a new application for Authorization. Should the project sponsor decline to so file, and decline to withdraw the permit application, the Commission shall conduct a public hearing in order to consider the revocation of the Authorization. Should the Commission not revoke the Authorization following the closure of the public hearing, the Commission shall determine the extension of time for the continued validity of the Authorization.

*For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, [www.sf-planning.org](http://www.sf-planning.org)*

**Diligent Pursuit.** Once a site or Building Permit has been issued, construction must commence within the timeframe required by the Department of Building Inspection and be continued diligently to completion. Failure to do so shall be grounds for the Commission to consider revoking the approval if more than three (3) years have passed since this Authorization was approved.

*For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, [www.sf-planning.org](http://www.sf-planning.org)*

**Extension.** All time limits in the preceding three paragraphs may be extended at the discretion of the Zoning Administrator where implementation of the project is delayed by a public agency, an appeal or a legal challenge and only by the length of time for which such public agency, appeal or challenge has caused delay.

*For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, [www.sf-planning.org](http://www.sf-planning.org)*

**Conformity with Current Law.** No application for Building Permit, Site Permit, or other entitlement shall be approved unless it complies with all applicable provisions of City Codes in effect at the time of such approval.

*For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, [www.sf-planning.org](http://www.sf-planning.org)*

**Improvement and Mitigation Measures.** Improvement and Mitigation Measures described in the MMRP for the Project EIR (Case No. 2011.1300E) attached as Exhibit C are necessary to avoid potential significant effects of the proposed project and have been agreed to by the project sponsor.



*For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, [www.sf-planning.org](http://www.sf-planning.org)*

## **DESIGN – COMPLIANCE AT PLAN STAGE**

**Entertainment Commission Noise Attenuation Conditions for Chapter 116 Residential Projects.** The Project shall comply with the following conditions of approval:

- Project sponsor shall upgrade their sound study from May 2013 to consider the impact of dbC on the development that might be emitted during performances at The Bottom of the Hill, a permitted place of entertainment. Sponsor shall contact Bottom of the Hill to insure that readings are taken on nights that have potential impact.
- Project sponsor shall mitigate any impact to the Bottom of the Hill, including parking for large vehicles associated with performances at the venue. Project shall not at any time block entrances or exits to the venue.
- Community Outreach: Project sponsor shall include in its community outreach process any businesses located within 300 feet of the proposed project that operate between the hours of 9PM-3AM. Notice shall be made in person, written or electronic form.
- Sound Study: Project sponsor shall conduct an acoustical sound study, which shall include sound readings taken when performances are taking place at the proximate Places of Entertainment, as well as when patrons arrive and leave these locations at closing time. Readings should be taken at locations that most accurately capture sound from the Place of Entertainment to best of their ability. Any recommendation(s) in the sound study regarding window glaze ratings and soundproofing materials including but not limited to walls, doors, roofing, etc. shall be given highest consideration by the project sponsor when designing and building the project.
- Design Considerations:
  - (1) During design phase, project sponsor shall consider the entrance and egress location and paths of travel at the Place(s) of Entertainment in designing the location of (a) any entrance/egress for the residential building and (b) any parking garage in the building.
  - (2) In designing doors, windows, and other openings for the residential building, project sponsor should consider the POE's operations and noise during all hours of the day and night.
- Construction Impacts: Project sponsor shall communicate with adjacent or nearby Place(s) of Entertainment as to the construction schedule, daytime and nighttime, and consider how this schedule and any storage of construction materials may impact the POE operations.
- Communication: Project Sponsor shall make a cell phone number available to Place(s) of Entertainment management during all phases of development through construction. In addition, a line of communication should be created to ongoing building management throughout the occupation phase and beyond.

**Final Materials.** The Project Sponsor shall continue to work with Planning Department on the building design. Final materials, glazing, color, texture, landscaping, and detailing shall be subject to Department staff review and approval. The architectural addenda shall be reviewed and approved by the Planning Department prior to issuance.



*For information about compliance, contact the Case Planner, Planning Department at 415-558-6378, [www.sf-planning.org](http://www.sf-planning.org)*

**Garbage, Composting and Recycling Storage.** Space for the collection and storage of garbage, composting, and recycling shall be provided within enclosed areas on the property and clearly labeled and illustrated on the architectural addenda. Space for the collection and storage of recyclable and compostable materials that meets the size, location, accessibility and other standards specified by the San Francisco Recycling Program shall be provided at the ground level of the buildings.

*For information about compliance, contact the Case Planner, Planning Department at 415-558-6378, [www.sf-planning.org](http://www.sf-planning.org)*

**Rooftop Mechanical Equipment.** Pursuant to Planning Code 141, the Project Sponsor shall submit a roof plan to the Planning Department prior to Planning approval of the building permit application for each building. Rooftop mechanical equipment, if any is proposed as part of the Project, is required to be screened so as not to be visible from any point at or below the roof level of the subject building.

*For information about compliance, contact the Case Planner, Planning Department at 415-558-6378, [www.sf-planning.org](http://www.sf-planning.org)*

**Streetscape Plan.** Pursuant to Planning Code Section 138.1, the Project Sponsor shall continue to work with Planning Department staff, in consultation with other City agencies, to refine the design and programming of the Streetscape Plan so that the plan generally meets the standards of the Better Streets Plan and all applicable City standards. The Project Sponsor shall complete final design of all required street improvements, including procurement of relevant City permits, prior to issuance of first architectural addenda, and shall complete construction of all required street improvements prior to issuance of first temporary certificate of occupancy.

*For information about compliance, contact the Case Planner, Planning Department at 415-558-6378, [www.sf-planning.org](http://www.sf-planning.org)*

## **PARKING AND TRAFFIC**

**Unbundled Parking.** All off-street parking spaces shall be made available to Project residents only as a separate “add-on” option for purchase or rent and shall not be bundled with any Project dwelling unit for the life of the dwelling units. The required parking spaces may be made available to residents within a quarter mile of the project. All affordable dwelling units pursuant to Planning Code Section 415 shall have equal access to use of the parking as the market rate units, with parking spaces priced commensurate with the affordability of the dwelling unit. Each unit within the Project shall have the first right of refusal to rent or purchase a parking space until the number of residential parking spaces are no longer available. No conditions may be placed on the purchase or rental of dwelling units, nor may homeowner’s rules be established, which prevent or preclude the separation of parking spaces from dwelling units.

*For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, [www.sf-planning.org](http://www.sf-planning.org)*

**Parking Maximum.** Pursuant to Planning Code Section 151.1, the Project shall provide no more than 296 off-street parking spaces for the 395 dwelling units contained therein.



*For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, [www.sf-planning.org](http://www.sf-planning.org)*

**Car Share.** Pursuant to Planning Code Section 166, no fewer than three car share spaces shall be made available, at no cost, to a certified car share organization for the purposes of providing car share services for its service subscribers.

*For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, [www.sf-planning.org](http://www.sf-planning.org)*

**Bicycle Parking.** Pursuant to Planning Code Sections 155.1, 155.4, and 155.5, the Project shall provide no fewer than 177 Class 1 bicycle parking spaces and 30 Class 2 bicycle parking spaces for the 199 dwelling units, 7,007 square feet of ground floor retail use, and 3,938 square feet of PDR use.

*For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, [www.sf-planning.org](http://www.sf-planning.org)*

**Managing Traffic During Construction.** The Project Sponsor and construction contractor(s) shall coordinate with the Traffic Engineering and Transit Divisions of the San Francisco Municipal Transportation Agency (SFMTA), the Police Department, the Fire Department, the Planning Department, and other construction contractor(s) for any concurrent nearby Projects to manage traffic congestion and pedestrian circulation effects during construction of the Project.

*For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, [www.sf-planning.org](http://www.sf-planning.org)*

## **PROVISIONS**

**First Source Hiring.** The Project shall adhere to the requirements of the First Source Hiring Construction and End-Use Employment Program approved by the First Source Hiring Administrator, pursuant to Section 83.4(m) of the Administrative Code. The Project Sponsor shall comply with the requirements of this Program regarding construction work and on-going employment required for the Project.

*For information about compliance, contact the First Source Hiring Manager at 415-581-2335, [www.onestopSF.org](http://www.onestopSF.org)*

**Anti-Discriminatory Housing.** The Project shall adhere to the requirements of the Anti-Discriminatory Housing policy, pursuant to Administrative Code Section 1.61.

*For information about compliance, contact the Case Planner, Planning Department at 415-558-6378, [www.sf-planning.org](http://www.sf-planning.org)*

**Eastern Neighborhoods Infrastructure Impact Fee.** Pursuant to Planning Code Section 423 (formerly 327), the Project Sponsor shall comply with the Eastern Neighborhoods Public Benefit Fund provisions through payment of an Impact Fee pursuant to Article 4.

*For information about compliance, contact the Case Planner, Planning Department at 415-558-6378, [www.sf-planning.org](http://www.sf-planning.org)*

**Transportation Sustainability Fee.** Pursuant to Planning Code Section 411A, the Project Sponsor shall pay the Transportation Sustainability Fee (TSF) as required by and based on drawings submitted with the



Building Permit Application. The Project Sponsor shall pay the fee at the time of and in no event later than the issuance of the First Construction Document.

*For information about compliance, contact the Case Planner, Planning Department at 415-558-6378, [www.sf-planning.org](http://www.sf-planning.org)*

**Residential Child Care Impact Fee.** Pursuant to Planning Code Section 414A, the Project Sponsor shall pay the Residential Child Care Impact Fee, as required by and based on drawings submitted with the Building Permit Application. The Project Sponsor shall pay the fee at the time of and in no event later than the issuance of the First Construction Document.

*For information about compliance, contact the Case Planner, Planning Department at 415-558-6378, [www.sf-planning.org](http://www.sf-planning.org)*

## **MONITORING**

**Enforcement.** Violation of any of the Planning Department conditions of approval contained in this Motion or of any other provisions of Planning Code applicable to this Project shall be subject to the enforcement procedures and administrative penalties set forth under Planning Code Section 176 or Section 176.1. The Planning Department may also refer the violation complaints to other city departments and agencies for appropriate enforcement action under their jurisdiction.

*For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, [www.sf-planning.org](http://www.sf-planning.org)*

**Revocation Due to Violation of Conditions.** Should implementation of this Project result in complaints from interested property owners, residents, or commercial lessees which are not resolved by the Project Sponsor and found to be in violation of the Planning Code and/or the specific conditions of approval for the Project as set forth in Exhibit A of this Motion, the Zoning Administrator shall refer such complaints to the Commission, after which it may hold a public hearing on the matter to consider revocation of this authorization.

*For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, [www.sf-planning.org](http://www.sf-planning.org)*

## **OPERATION**

**Garbage, Recycling, and Composting Receptacles.** Garbage, recycling, and compost containers shall be kept within the premises and hidden from public view, and placed outside only when being serviced by the disposal company. Trash shall be contained and disposed of pursuant to garbage and recycling receptacles guidelines set forth by the Department of Public Works.

*For information about compliance, contact Bureau of Street Use and Mapping, Department of Public Works at 415-554-.5810, <http://sfdpw.org>*

**Sidewalk Maintenance.** The Project Sponsor shall maintain the main entrance to the building and all sidewalks abutting the subject property in a clean and sanitary condition in compliance with the Department of Public Works Streets and Sidewalk Maintenance Standards.

*For information about compliance, contact Bureau of Street Use and Mapping, Department of Public Works, 415-695-2017, <http://sfdpw.org>*



**Noise Control.** The premises shall be adequately soundproofed or insulated for noise and operated so that incidental noise shall not be audible beyond the premises or in other sections of the building and fixed-source equipment noise shall not exceed the decibel levels specified in the San Francisco Noise Control Ordinance.

*For information about compliance with the fixed mechanical objects such as rooftop air conditioning, restaurant ventilation systems, and motors and compressors with acceptable noise levels, contact the Environmental Health Section, Department of Public Health at (415) 252-3800, [www.sfdph.org](http://www.sfdph.org).*

*For information about compliance with the construction noise, contact the Department of Building Inspection, 415-558-6570, [www.sfdbi.org](http://www.sfdbi.org).*

*For information about compliance with the amplified sound including music and television contact the Police Department at 415-553-0123, [www.sf-police.org](http://www.sf-police.org)*

**Community Liaison.** Prior to issuance of a building permit to construct the project and implement the approved use, the Project Sponsor shall appoint a community liaison officer to deal with the issues of concern to owners and occupants of nearby properties. The Project Sponsor shall provide the Zoning Administrator with written notice of the name, business address, and telephone number of the community liaison. Should the contact information change, the Zoning Administrator shall be made aware of such change. The community liaison shall report to the Zoning Administrator what issues, if any, are of concern to the community and what issues have not been resolved by the Project Sponsor.

*For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, [www.sf-planning.org](http://www.sf-planning.org)*

**Lighting.** All Project lighting shall be directed onto the Project site and immediately surrounding sidewalk area only, and designed and managed so as not to be a nuisance to adjacent residents. Nighttime lighting shall be the minimum necessary to ensure safety, but shall in no case be directed so as to constitute a nuisance to any surrounding property.

*For information about compliance, contact Code Enforcement, Planning Department at 415-575-6863, [www.sf-planning.org](http://www.sf-planning.org)*

## **INCLUSIONARY HOUSING**

**Eastern Neighborhoods Affordable Housing Requirements for UMU.** Pursuant to Planning Code Section 419.3, Project Sponsor shall meet the requirements set forth in Planning Code Section 419.3 in addition to the requirements set forth in the Affordable Housing Program, per Planning Code Section 415. Prior to issuance of first construction document, the Project Sponsor shall select one of the options described in Section 419.3 or the alternatives described in Planning Code Section 419.5 to fulfill the affordable housing requirements and notify the Department of their choice. Any fee required by Section 419.1 et seq. shall be paid to the Development Fee Collection Unit at DBI prior to issuance of the first construction document an option for the Project Sponsor to defer payment to prior to issuance of the first certificate of occupancy upon agreeing to pay a deferral surcharge in accordance with Section 107A.13.3 of the San Francisco Building Code.

*For information about compliance, contact the Case Planner, Planning Department at 415-575-9195, [www.sf-planning.org](http://www.sf-planning.org)*



## Affordable Units

1. **Number of Required Units.** The Project contains two buildings: the 16<sup>th</sup> Street Building will provide on-site rental inclusionary units, while the 17<sup>th</sup> Street Building will satisfy the requirements of the Inclusionary Affordable Housing Program through payment of the Affordable Housing Fee. Pursuant to Planning Code Section 415.6 and 419.5, the 16<sup>th</sup> Street Building is currently required to provide 16% of the proposed dwelling units as affordable to qualifying households, but is subject to change under a proposed Charter amendment and pending legislation if the voters approve the Charter Amendment at the June 7, 2016 election. The Project Sponsor must pay an Affordable Housing Fee at a rate equivalent to the applicable percentage of the number of units in an off-site project needed to satisfy the Inclusionary Affordable Housing Program Requirement for the principal project. The applicable percentage for this portion of the projects project is twenty three percent (23%), but it is subject to change under a proposed Charter amendment and pending legislation if the voters approve the Charter Amendment at the June 7, 2016 election. The 16<sup>th</sup> Street Building contains 260 units; therefore, 42 affordable units are currently required. The Project Sponsor will fulfill this requirement by providing the 42 affordable units on-site. If the Project is subject to a different requirement if the Charter Amendment is approved and new legislative requirements take effect, the Project will comply with the applicable requirements at the time of compliance. If the number of market-rate units change, the number of required affordable units shall be modified accordingly with written approval from Planning Department staff in consultation with the Mayor's Office of Housing and Community Development ("MOHCD").

*For information about compliance, contact the Case Planner, Planning Department at 415-558-6378, [www.sf-planning.org](http://www.sf-planning.org) or the Mayor's Office of Housing and Community Development at 415-701-5500, [www.sf-moh.org](http://www.sf-moh.org).*

2. **Unit Mix.** The 16<sup>th</sup> Street Building contains 53 studios, 103 one-bedroom, 95 two-bedroom, and 9 three-bedroom units; therefore, the required affordable unit mix is 9 studios, 17 one-bedroom, 15 two-bedroom, and 1 three-bedroom units, or the unit mix that may be required if the inclusionary housing requirements change as discussed above. If the market-rate unit mix changes, the affordable unit mix will be modified accordingly with written approval from Planning Department staff in consultation with MOHCD.

*For information about compliance, contact the Case Planner, Planning Department at 415-558-6378, [www.sf-planning.org](http://www.sf-planning.org) or the Mayor's Office of Housing and Community Development at 415-701-5500, [www.sf-moh.org](http://www.sf-moh.org).*

3. **Unit Location.** The affordable units shall be designated on a reduced set of plans recorded as a Notice of Special Restrictions on the 16th Street Building property prior to the issuance of the first construction permit.

*For information about compliance, contact the Case Planner, Planning Department at 415-575-9195, [www.sf-planning.org](http://www.sf-planning.org) or the Mayor's Office of Housing and Community Development at 415-701-5500, [www.sf-moh.org](http://www.sf-moh.org).*



4. **Phasing.** If any building permit is issued for partial phasing of the Project, the Project Sponsor shall have designated not less than sixteen percent (16%), or the applicable percentage as discussed above, of the each phase's total number of dwelling units as on-site affordable units.

*For information about compliance, contact the Case Planner, Planning Department at 415-558-6378, [www.sf-planning.org](http://www.sf-planning.org) or the Mayor's Office of Housing and Community Development at 415-701-5500, [www.sf-moh.org](http://www.sf-moh.org).*

5. **Duration.** Under Planning Code Section 415.8, all units constructed pursuant to Section 415.6, must remain affordable to qualifying households for the life of the project.

*For information about compliance, contact the Case Planner, Planning Department at 415-558-6378, [www.sf-planning.org](http://www.sf-planning.org) or the Mayor's Office of Housing and Community Development at 415-701-5500, [www.sf-moh.org](http://www.sf-moh.org).*

6. **Other Conditions.** The Project is subject to the requirements of the Inclusionary Affordable Housing Program under Section 415 et seq. of the Planning Code and City and County of San Francisco Inclusionary Affordable Housing Program Monitoring and Procedures Manual ("Procedures Manual"). The Procedures Manual, as amended from time to time, is incorporated herein by reference, as published and adopted by the Planning Commission, and as required by Planning Code Section 415. Terms used in these conditions of approval and not otherwise defined shall have the meanings set forth in the Procedures Manual. A copy of the Procedures Manual can be obtained at the MOHCD at 1 South Van Ness Avenue or on the Planning Department or Mayor's Office of Housing's websites, including on the internet at:

<http://sf-planning.org/Modules/ShowDocument.aspx?documentid=4451>.

As provided in the Inclusionary Affordable Housing Program, the applicable Procedures Manual is the manual in effect at the time the subject units are made available for sale.

*For information about compliance, contact the Case Planner, Planning Department at 415-558-6378, [www.sf-planning.org](http://www.sf-planning.org) or the Mayor's Office of Housing and Community Development at 415-701-5500, [www.sf-moh.org](http://www.sf-moh.org).*

- a. The affordable unit(s) shall be designated on the building plans prior to the issuance of the first construction permit by the Department of Building Inspection ("DBI"). The affordable unit(s) shall (1) reflect the unit size mix in number of bedrooms of the market rate units, (2) be constructed, completed, ready for occupancy and marketed no later than the market rate units, and (3) be evenly distributed throughout the building; and (4) be of comparable overall quality, construction and exterior appearance as the market rate units in the principal project. The interior features in affordable units should be generally the same as those of the market units in the principal project, but need not be the same make, model or type of such item as long they are of good and new quality and are consistent with then-current standards for new housing. Other specific standards for on-site units are outlined in the Procedures Manual.
- b. If the units in the building are offered for rent, the affordable unit(s) shall be rented to qualifying households, as defined in the Procedures Manual, whose gross annual income, adjusted for household size, does not exceed an average fifty-five (55) percent of Area



Median Income under the income table called “Maximum Income by Household Size derived from the Unadjusted Area Median Income for HUD Metro Fair Market Rent Area that contains San Francisco,” but these income levels are subject to change under a proposed Charter amendment and pending legislation if the voters approve the Charter Amendment at the June 7, 2016 election. If the Project is subject to a different income level requirement if the Charter Amendment is approved and new legislative requirements take effect, the Project will comply with the applicable requirements. The initial and subsequent rent level of such units shall be calculated according to the Procedures Manual. Limitations on (i) occupancy; (ii) lease changes; (iii) subleasing, and; are set forth in the Inclusionary Affordable Housing Program and the Procedures Manual.

- c. The Project Sponsor is responsible for following the marketing, reporting, and monitoring requirements and procedures as set forth in the Procedures Manual. MOHCD shall be responsible for overseeing and monitoring the marketing of affordable units. The Project Sponsor must contact MOHCD at least six months prior to the beginning of marketing for any unit in the building.
- d. Required parking spaces shall be made available to renters of affordable units according to the Procedures Manual.
- e. Prior to the issuance of the first construction permit by DBI for the Project, the Project Sponsor shall record a Notice of Special Restriction on the property that contains these conditions of approval and a reduced set of plans that identify the affordable units satisfying the requirements of this approval. The Project Sponsor shall promptly provide a copy of the recorded Notice of Special Restriction to the Department and to MOHCD or its successor.
- f. The Project Sponsor has demonstrated that it is eligible for the On-site Affordable Housing Alternative under Planning Code Section 415.6 instead of payment of the Affordable Housing Fee, and has submitted the *Affidavit of Compliance with the Inclusionary Affordable Housing Program: Planning Code Section 415* to the Planning Department stating the intention to enter into an agreement with the City to qualify for a waiver from the Costa-Hawkins Rental Housing Act based upon the proposed density bonus and concessions (as defined in California Government Code Section 65915 et seq.) provided herein. The Project Sponsor has executed the Costa Hawkins agreement and will record a Memorandum of Agreement prior to issuance of the first construction document or must revert payment of the Affordable Housing Fee.
- g. If the Project Sponsor fails to comply with the Inclusionary Affordable Housing Program requirement, the Director of DBI shall deny any and all site or building permits or certificates of occupancy for the development project until the Planning Department notifies the Director of compliance. A Project Sponsor’s failure to comply with the requirements of Planning Code Section 415 et seq. shall constitute cause for the City to record a lien against the development project and to pursue any and all available remedies at law.



- h. If the Project becomes ineligible at any time for the On-site Affordable Housing Alternative, the Project Sponsor or its successor shall pay the Affordable Housing Fee prior to issuance of the first construction permit. If the Project becomes ineligible after issuance of its first construction permit, the Project Sponsor shall notify the Department and MOHCD and pay interest on the Affordable Housing Fee and penalties, if applicable.
- i. The Project Sponsor must pay the Fee in full sum to the Development Fee Collection Unit at the DBI for use by MOHCD prior to the issuance of the first construction document.
- j. Prior to the issuance of the first construction permit by the DBI for the Project, the Project Sponsor shall record a Notice of Special Restriction on the property that records a copy of this approval. The Project Sponsor shall promptly provide a copy of the recorded Notice of Special Restriction to the Department and to MOHCD or its successor.
- k. If project applicant fails to comply with the Inclusionary Affordable Housing Program requirement, the Director of DBI shall deny any and all site or building permits or certificates of occupancy for the development project until the Planning Department notifies the Director of compliance. A Project Sponsor's failure to comply with the requirements of Planning Code Sections 415 et seq. shall constitute cause for the City to record a lien against the development project and to pursue any and all other remedies at law.



Free Recording Requested Pursuant to  
Government Code Section 27383

When recorded, mail to:

San Francisco Planning Department  
1650 Mission Street, Room 400  
San Francisco, California 94103  
Attn: Director

Block 3949, Lots 001, 001A and 002 and Block 3950, Lot 001

**AGREEMENT TO PROVIDE ON-SITE AFFORDABLE HOUSING UNITS BETWEEN  
THE CITY AND COUNTY OF SAN FRANCISCO AND  
POTRERO PARTNERS LLC, RELATIVE TO THE  
DEVELOPMENT KNOWN AS 901 16<sup>TH</sup> STREET**

THIS AGREEMENT TO PROVIDE ON-SITE AFFORDABLE HOUSING UNITS ("Agreement") dated for reference purposes only as of this \_\_\_\_ day of \_\_\_\_\_, 2016, is by and between the CITY AND COUNTY OF SAN FRANCISCO, a political subdivision of the State of California (the "City"), acting by and through its Planning Department, and POTRERO PARTNERS LLC, a California limited liability company ("Developer") with respect to the project approved for 901 16<sup>th</sup> Street (the "Project"). City and Developer are also sometimes referred to individually as a "Party" and together as the "Parties."

**RECITALS**

This Agreement is made with reference to the following facts:

A. Code Authorization. Chapter 4.3 of the California Government Code directs public agencies to grant concessions and incentives to private developers for the production of housing for lower income households. The Costa-Hawkins Rental Housing Act (California Civil Code Sections 1954.50 et seq., hereafter the "Costa-Hawkins Act") imposes limitations on the establishment of the initial and all subsequent rental rates for a dwelling unit with a certificate of occupancy issued after February 1, 1995, with exceptions, including an exception for dwelling units constructed pursuant to a contract with a public entity in consideration for a direct financial contribution or any other form of assistance specified in Chapter 4.3 of the California Government Code (Section 1954.52(b)). Pursuant to Civil Code Section 1954.52(b), the City's Board of Supervisors has enacted as part of the Inclusionary Affordable Housing Program, Planning Code Section 415 et seq, procedures and requirements for entering into an agreement with a private developer to memorialize the concessions and incentives granted to the developer and to provide an exception to the Costa-Hawkins Act for the inclusionary units included in Developer's project.

B. Property Subject to this Agreement. The property that is the subject of this Agreement consists of the real property in the City and County of San Francisco, California, at 901 16<sup>th</sup> Street , a portion of Lots 001, 001A and 002 in Assessor's Block 3949 and Lot 001 in



Assessor's Block 3950 (the "Property"). The Property is more particularly described in Exhibit A attached hereto. The Property is owned in fee by Developer.

C. Development Proposal; Intent of the Parties. The Developer proposes to remove the existing buildings and construct a new 6-story, 68-foot tall, 260-unit mixed use residential building with 20,318 square feet of retail space and 263 off-street parking spaces (the "16<sup>th</sup> Street Building") and a publicly accessible pedestrian alley connecting 16<sup>th</sup> Street and 17<sup>th</sup> Street along the western property line on the Property (the "Project"). Some or all of the dwelling units would be offered as rental units and the inclusionary affordable housing would be provided on-site. The Project would fulfill its inclusionary affordable housing requirement by providing 16% of the dwelling units in the Project, or 42 below-market rate (BMR) units on-site, assuming that 260 residential units are constructed.

On \_\_\_\_\_, 2016, pursuant to Motion No. \_\_\_\_\_ the Planning Commission approved a Large Project Authorization pursuant to Planning Code Section 327 (the "Project Approval") with exceptions from Planning Code requirements related to rear yard, horizontal mass reduction, off-street parking, off-street freight loading, parking/loading entrance width, and permitted obstructions over the street. The Project Approval also approved construction of a new 4-story, 48-foot tall, 135-unit mixed use residential building with 4,650 square feet of retail space and 125 parking spaces at 1200 17<sup>th</sup> Street Building on another portion of the Property. This Agreement applies only to the Project at 901 16<sup>th</sup> Street, and does not apply to the 1200 17<sup>th</sup> Street building, for which the Developer has elected to pay the Affordable Housing Fee to fulfill the inclusionary affordable housing obligation for the 1200 17<sup>th</sup> Street building.

The dwelling units that are the subject of this Agreement are the Project's on-site inclusionary units representing sixteen percent (16%) of the Project's dwelling units, which assuming that 260 dwelling units are constructed, would total 42 inclusionary units (the "Inclusionary Units"). The dwelling units in the Project that are not Inclusionary Units, representing eighty-four percent (84%) of the Project's dwelling units, which, assuming that 260 units are constructed, would total 218 units, are referred to herein as the "Market Rate Units". This Agreement is not intended to impose restrictions on the Market Rate Units or any portions of the Project other than the Inclusionary Units. The Parties acknowledge that this Agreement is entered into in consideration of the respective burdens and benefits of the Parties contained in this Agreement and in reliance on their agreements, representations and warranties.

D. Inclusionary Affordable Housing Program. The Inclusionary Affordable Housing Program, San Francisco Planning Code Section 415 et seq. (the "Affordable Housing Program") provides that developers of any housing project consisting of ten or more units to pay an Affordable Housing Fee, as defined therein. The Affordable Housing Program provides that developers may be eligible to meet the requirements of the program through the alternative means of entering into an agreement with the City and County of San Francisco pursuant to Chapter 4.3 of the California Government Code for concessions and incentives, pursuant to which the developer covenants to provide affordable on-site units as an alternative to payment of the Affordable Housing Fee to satisfy the requirements of the Affordable Housing Program and in consideration of the City's concessions and incentives.



E. Developer's Election to Provide On-Site Units. Developer has elected to enter into this Agreement to provide the Inclusionary Units on-site in lieu of payment of the Affordable Housing Fee in satisfaction of its obligation under the Affordable Housing Program, and to provide for an exception to the rent restrictions of the Costa-Hawkins Act for the Inclusionary Units only.

F. Compliance with All Legal Requirements. It is the intent of the Parties that all acts referred to in this Agreement shall be accomplished in such a way as to fully comply with the California Environmental Quality Act (Public Resources Code Section 21000 et seq., "CEQA"), Chapter 4.3 of the California Government Code, the Costa-Hawkins Act, the San Francisco Planning Code, and all other applicable laws and regulations.

G. Project's Compliance with CEQA. Pursuant to CEQA, the CEQA Guidelines, and Chapter 31 of the San Francisco Administrative Code, the environmental impacts associated with the Project were described and analyzed, and alternatives and mitigation measures that could avoid or reduce those impacts were discussed in the 901 16<sup>th</sup> Street and 1200 17<sup>th</sup> Street Project Final Environmental Impact Report certified by the Planning Commission on May 12, 2016 (Motion No. \_\_\_\_\_) (the "FEIR"). The information in the FEIR was considered by all entities with review and approval authority over the Project prior to the approval of the Project.

H. General Plan Findings. This Agreement is consistent with the objectives, policies, general land uses and programs specified in the General Plan and any applicable area or specific plan, and the Priority Policies enumerated in Planning Code Section 101.1, as set forth in Planning Commission Motion No. \_\_\_\_\_.

## **AGREEMENT**

The Parties acknowledge the receipt and sufficiency of good and valuable consideration and agree as follows:

### **1. GENERAL PROVISIONS**

1.1 Incorporation of Recitals and Exhibits. The preamble paragraph, Recitals, and Exhibits, and all defined terms contained therein, are hereby incorporated into this Agreement as if set forth in full.

### **2. CITY'S DENSITY BONUS AND CONCESSIONS AND INCENTIVES FOR THE INCLUSIONARY UNITS.**

2.1 Exceptions, Concessions and Incentives. The Developer has received the following concessions and incentives for the production of the Inclusionary Units on-site.

2.1.1 Rear Yard. The Project Approval provided an exception to the rear yard requirements set forth in Planning Code Section 134. Prior to adoption of the Project Approval, Section 134(a)(1) would have required an open rear yard with a minimum depth equal to 25 percent of the total depth of the lot. The Project Approval allowed the Project to meet the rear yard requirement through a series of publicly-accessible pedestrian passageways, a residential mews and several courtyards that provide a comparable amount of open space as mandated by the rear yard



requirement. As a result of the Project Approval, the Project was approved with a superior site plan that would not have otherwise been possible under the Planning Code.

2.1.2 Horizontal Mass Reduction. The Project Approval provided for an exception to the horizontal mass reduction requirement set forth in Planning Code Section 270.1 . Prior to adoption of the Project Approval, Section 270.1 would have required a mass reduction break along 16<sup>th</sup> Street not less than 30 feet in width, not less than 60 feet in depth from the street-facing building façade, and extending up to the sky from a level not higher than 25 feet above grade or the third story, whichever is lower. The Project Approval allowed the Project to meet the horizontal mass reduction requirement with a shallower mass reduction. As a result of the Project Approval, the Project was approved with a superior building massing and was able to provide a more efficient plan resulting in a greater number of building square feet that would not have otherwise been possible under the Planning Code.

2.1.3 Off-Street Parking. The Project Approval provided for an exception to the maximum off-street parking requirement set forth in Planning Code Section 151.1. Prior to the adoption of the Project Approval, Section 151.1 would have permitted the Project to provide 195 off-street residential spaces. As a result of the Project Approval, the Project is providing 216 off-street residential parking spaces.

2.1.4 Off-Street Loading. The Project Approval provided for an exception to the Off-Street Loading requirements set forth in Planning Code Section 152.1. Prior to the adoption of the Project Approval, Section 152.1 would have required the Project to provide three off-street loading spaces. As a result of the Project Approval, the Project is providing one off-street loading space and was able to provide a more efficient plan resulting in a greater number of building square feet than would have otherwise been possible under the Planning Code.

2.1.5 Parking/Loading Entrance Width. The Project Approval provided for an exception to the Parking/Loading Entrance Width requirements set forth in Planning Code Section 145.1. Prior to the adoption of the Project Approval, Section 145.1 would have limited the street frontage on Mississippi Street devoted to parking and loading ingress or egress to 20 feet in width. As a result of the Project Approval, the Project is providing parking and loading ingress and egress at three points for a total of 32 feet of street frontage width, such that the Project was able to provide a more efficient plan resulting in a greater number of building square feet that would not have otherwise been possible under the Planning Code.

2.1.6 Obstruction over Streets and Alleys. The Project Approval provided for an exception to the Obstruction over Streets and Alleys requirements set forth in Planning Code Section 136(c)(2). Prior to the adoption of the Project Approval, Section 136(c)(2) would have limited the size of overhanging obstructions to a depth of three feet and a length of nine feet. As a result of the Project Approval, the Project has obstructions over the street that exceed this maximum obstruction and was able to provide a more efficient plan resulting in a greater number of building square feet than would have otherwise been possible under the Planning Code.

2.2 Waiver of Affordable Housing Fee. City hereby determines that the Developer has satisfied the requirements of the Affordable Housing Program by covenanting to provide the Inclusionary Units on-site, as provided in Section 3.1, and accordingly hereby waives the obligation



of the Developer to pay the Affordable Housing Fee. City would not be willing to enter into this Agreement and waive the Affordable Housing Fee without the understanding and agreement that Costa-Hawkins Act provisions set forth in California Civil Code section 1954.52(a) do not apply to the Inclusionary Units as a result of the exemption set forth in California Civil Code section 1954.52(b). Upon completion of the Project and identification of the Inclusionary Units, Developer agrees to record a notice of restriction against the Inclusionary Units in the form required by the Affordable Housing Program.

### 2.3 Costa-Hawkins Act Inapplicable to Inclusionary Units Only.

2.3.1 Inclusionary Units. The parties acknowledge that, under Section 1954.52(b) of the Costa-Hawkins Act, the Inclusionary Units are not subject to the Costa-Hawkins Act. Through this Agreement, Developer hereby enters into an agreement with a public entity in consideration for forms of concessions and incentives specified in California Government Code Sections 65915 et seq. The concessions and incentives are comprised of, but not limited to, the concessions and incentives set forth in Section 2.1.

2.2.2 Market Rate Units. The Parties hereby agree and acknowledge that this Agreement does not alter in any manner the way that the Costa-Hawkins Act or any other law, including the City's Rent Stabilization and Arbitration Ordinance (Chapter 37 of the San Francisco Administrative Code) apply to the Market Rate Units.

## 3. **COVENANTS OF DEVELOPER**

3.1 On-Site Inclusionary Affordable Units. In consideration of the concessions and incentives set forth in Section 2.1 and in accordance with the terms and conditions set forth in the Affordable Housing Program and the Project Approvals, upon Developer obtaining its first certificate of occupancy for the Project, Developer shall provide sixteen percent (16%) of the dwelling units in the Project as on-site Inclusionary Units in lieu of payment of the Affordable Housing Fee. For example, based on the contemplated total of 260 units comprising the Project, a total of 42 Inclusionary Units would be required in lieu of payment of the Affordable Housing Fee.

3.2 Developer's Waiver of Rights Under the Costa-Hawkins Act Only as to the Inclusionary Units. The Parties acknowledge that under the Costa-Hawkins Act, the owner of newly constructed residential real property may establish the initial and all subsequent rental rates for dwelling units in the property without regard to the City's Residential Rent Stabilization and Arbitration Ordinance (Chapter 37 of the San Francisco Administrative Code). The Parties also understand and agree that the Costa-Hawkins Act does not and in no way shall limit or otherwise affect the restriction of rental charges for the Inclusionary Units because this Agreement falls within an express exception to the Costa-Hawkins Act as a contract with a public entity in consideration for a direct financial contribution or other forms of assistance specified in Chapter 4.3 (commencing with section 65915) of Division 1 of Title 7 of the California Government Code including but not limited to the density bonus, concessions and incentives specified in Section 2. Developer acknowledges that the density bonus and concessions and incentives result in identifiable and actual cost reductions to the Project. Should the Inclusionary Units be deemed subject to the Costa-Hawkins Act, as a material part of the consideration for entering into this Agreement, Developer, on behalf of itself and all its successors and assigns to this Agreement, hereby expressly waives, now



and forever, any and all rights it may have under the Costa-Hawkins Act with respect only to the Inclusionary Units (but only the Inclusionary Units and not as to the Market Rate Units) consistent with Section 3.1 of this Agreement. Without limiting the foregoing, Developer, on behalf of itself and all successors and assigns to this Agreement, agrees not to bring any legal or other action against City seeking application of the Costa-Hawkins Act to the Inclusionary Units for so long as the Inclusionary Units are subject to the restriction on rental rates pursuant to the Affordable Housing Program. The Parties understand and agree that the City would not be willing to enter into this Agreement without the waivers and agreements set forth in this Section 3.2.

3.3 Developer's Waiver of Right to Seek Waiver of Affordable Housing Program. Developer specifically agrees to be bound by all of the provisions of the Affordable Housing Program applicable to on-site inclusionary units with respect to the Inclusionary Units. Developer covenants and agrees that it will not seek a waiver of the provisions of the Affordable Housing Program applicable to the Inclusionary Units.

#### **4. MUTUAL OBLIGATIONS**

4.1 Good Faith and Fair Dealing. The Parties shall cooperate with each other and act in good faith in complying with the provisions of this Agreement and implementing the Project Approvals.

4.2 Other Necessary Acts. Each Party shall execute and deliver to the other all further instruments and documents as may be reasonably necessary to carry out this Agreement, the Project Approvals, the Affordable Housing Program (as applied to the Inclusionary Units) and applicable law in order to provide and secure to each Party the full and complete enjoyment of its rights and privileges hereunder.

4.3 Effect of Future Changes to Affordable Housing Program. The City hereby acknowledges and agrees that, in the event that the City adopts changes to the Affordable Housing Program after the date this Agreement is executed by both Parties, nothing in this Agreement shall be construed to limit or prohibit any rights Developer may have to modify Project requirements with respect to the Inclusionary Units to the extent permitted by such changes to the Affordable Housing Program.

#### **5. DEVELOPER REPRESENTATIONS, WARRANTIES AND COVENANTS.**

5.1 Interest of Developer. Developer represents that it is or will be the legal and equitable fee owner of the Property, that it has the power and authority to bind all other persons with legal or equitable interest in the Inclusionary Units to the terms of this Agreement, and that all other persons holding legal or equitable interest in the Inclusionary Units are to be bound by this Agreement. Developer is a limited liability company, duly organized and validly existing and in good standing under the laws of the State of California. Developer has all requisite power and authority to own property and conduct business as presently conducted. Developer has made all filings and is in good standing in the State of California.

5.2 No Conflict With Other Agreements; No Further Approvals; No Suits. Developer warrants and represents that it is not a party to any other agreement that would conflict with the Developer's obligations under this Agreement. Neither Developer's articles of organization, bylaws,



or operating agreement, as applicable, nor any other agreement or law in any way prohibits, limits or otherwise affects the right or power of Developer to enter into and perform all of the terms and covenants of this Agreement. No consent, authorization or approval of, or other action by, and no notice to or filing with, any governmental authority, regulatory body or any other person is required for the due execution, delivery and performance by Developer of this Agreement or any of the terms and covenants contained in this Agreement. To Developer's knowledge, there are no pending or threatened suits or proceedings or undischarged judgments affecting Developer or any of its members before any court, governmental agency, or arbitrator which might materially adversely affect Developer's business, operations, or assets or Developer's ability to perform under this Agreement.

5.3 No Inability to Perform; Valid Execution. Developer warrants and represents that it has no knowledge of any inability to perform its obligations under this Agreement. The execution and delivery of this Agreement and the agreements contemplated hereby by Developer have been duly and validly authorized by all necessary action. This Agreement will be a legal, valid and binding obligation of Developer, enforceable against Developer in accordance with its terms.

5.4 Conflict of Interest. Through its execution of this Agreement, the Developer acknowledges that it is familiar with the provisions of Section 15.103 of the City's Charter, Article III, Chapter 2 of the City's Campaign and Governmental Conduct Code, and Section 87100 et seq. and Section 1090 et seq. of the California Government Code, and certifies that it does not know of any facts which constitute a violation of said provisions and agrees that it will immediately notify the City if it becomes aware of any such fact during the term of this Agreement.

5.5 Notification of Limitations on Contributions. Through execution of this Agreement, the Developer acknowledges that it is familiar with Section 1.126 of City's Campaign and Governmental Conduct Code, which prohibits any person who contracts with the City, whenever such transaction would require approval by a City elective officer or the board on which that City elective officer serves, from making any campaign contribution to the officer at any time from the commencement of negotiations for the contract until three (3) months after the date the contract is approved by the City elective officer or the board on which that City elective officer serves. San Francisco Ethics Commission Regulation 1.126-1 provides that negotiations are commenced when a prospective contractor first communicates with a City officer or employee about the possibility of obtaining a specific contract. This communication may occur in person, by telephone or in writing, and may be initiated by the prospective contractor or a City officer or employee. Negotiations are completed when a contract is finalized and signed by the City and the contractor. Negotiations are terminated when the City and/or the prospective contractor end the negotiation process before a final decision is made to award the contract.

5.6 Nondiscrimination. In the performance of this Agreement, Developer agrees not to discriminate on the basis of the fact or perception of a person's, race, color, creed, religion, national origin, ancestry, age, height, weight, sex, sexual orientation, gender identity, domestic partner status, marital status, disability or Acquired Immune Deficiency Syndrome or HIV status (AIDS/HIV status), or association with members of such protected classes, or in retaliation for opposition to discrimination against such classes, against any City employee, employee of or applicant for employment with the Developer, or against any bidder or contractor for public works or improvements, or for a franchise, concession or lease of property, or for goods or services or



supplies to be purchased by the Developer. A similar provision shall be included in all subordinate agreements let, awarded, negotiated or entered into by the Developer for the purpose of implementing this Agreement.

## **6. AMENDMENT; TERMINATION**

6.1 Amendment or Termination. Except as provided in Sections 6.2 (Automatic Termination) and 8.3 (Remedies for Default), this Agreement may only be amended or terminated with the mutual written consent of the Parties.

6.1.1 Amendment Exemptions. No amendment of a Project Approval shall require an amendment to this Agreement. Upon approval, any such matter shall be deemed to be incorporated automatically into the Project and this Agreement (subject to any conditions set forth in the amendment). Notwithstanding the foregoing, in the event of any direct conflict between the terms of this Agreement and any amendment to a Project Approval, then the terms of this Agreement shall prevail and any amendment to this Agreement shall be accomplished as set forth in Section 6.1 above.

6.2 Automatic Termination. This Agreement shall automatically terminate in the event that the Inclusionary Units are no longer subject to regulation as to the rental rates of the Inclusionary Units and/or the income level of households eligible to rent the Inclusionary Units under the Affordable Housing Program, or successor program.

## **7. TRANSFER OR ASSIGNMENT; RELEASE; RIGHTS OF MORTGAGEES; CONSTRUCTIVE NOTICE**

7.1 Agreement Runs With The Land. Developer may assign or transfer its duties and obligations under this Agreement to another entity, provided such entity is the legal and equitable fee owner of the Property ("Transferee"). As provided in Section 9.2, this Agreement runs with the land and any Transferee will be bound by all of the terms and conditions of this Agreement.

7.2 Rights of Developer. The provisions in this Section 7 shall not be deemed to prohibit or otherwise restrict Developer from (i) granting easements or licenses to facilitate development of the Property, (ii) encumbering the Property or any portion of the improvements thereon by any mortgage, deed of trust, or other device securing financing with respect to the Property or Project, (iii) granting a leasehold interest in all or any portion of the Property, or (iv) transferring all or a portion of the Property pursuant to a sale, transfer pursuant to foreclosure, conveyance in lieu of foreclosure, or other remedial action in connection with a mortgage. None of the terms, covenants, conditions, or restrictions of this Agreement or the other Project Approvals shall be deemed waived by City by reason of the rights given to the Developer pursuant to this Section 7.2. Furthermore, although the Developer initially intends to operate the Project on a rental basis, nothing in this Agreement shall prevent Developer from later selling all or part of the Project on a condominium basis, provided that such sale is permitted by, and complies with, all applicable City and State laws including, but not limited to that, with respect to any inclusionary units, those shall only be sold pursuant to the City Procedures for sale of inclusionary units under the Affordable Housing Program.

7.3 Developer's Responsibility for Performance. If Developer transfers or assigns all or any portion of the Property or any interest therein to any other person or entity, Developer shall



continue to be responsible for performing the obligations under this Agreement as to the transferred property interest until such time as there is delivered to the City a legally binding agreement pursuant to which the Transferee assumes and agrees to perform Developer's obligations under this Agreement from and after the date of transfer of the Property (or an interest therein) to the Transferee (an "Assignment and Assumption Agreement"). The City is entitled to enforce each and every such obligation assumed by the Transferee directly against the Transferee as if the Transferee were an original signatory to this Agreement with respect to such obligation. Accordingly, in any action by the City against a Transferee to enforce an obligation assumed by the Transferee, the Transferee shall not assert any defense against the City's enforcement of performance of such obligation that is attributable to Developer's breach of any duty or obligation to the Transferee arising out of the transfer or assignment, the Assignment and Assumption Agreement, the purchase and sale agreement, or any other agreement or transaction between the Developer and the Transferee. The transferor Developer shall remain responsible for the performance of all of its obligations under the Agreement prior to the date of transfer, and shall remain liable to the City for any failure to perform such obligations prior to the date of the transfer.

7.4 Release Upon Transfer or Assignment. Upon the Developer's transfer or assignment of all or a portion of the Property or any interest therein, including the Developer's rights and interests under this Agreement, the Developer shall be released from any obligations required to be performed from and after the date of transfer under this Agreement with respect to the portion of the Property so transferred; provided, however, that (i) the Developer is not then in default under this Agreement and (ii) the Transferee executes and delivers to the City the legally binding Assignment and Assumption Agreement. Following any transfer, in accordance with the terms of this Section 7, a default under this Agreement by the Transferee shall not constitute a default by the Developer under this Agreement and shall have no effect upon the Developer's rights under this Agreement as to the remaining portions of the Property owned by the Developer. Further, a default under this Agreement by the Developer as to any portion of the Property not transferred or a default under this agreement by the Developer prior to the date of transfer shall not constitute a default by the Transferee and shall not affect any of Transferee's rights under this Agreement.

7.5 Rights of Mortgagees; Not Obligated to Construct; Right to Cure Default.

7.5.1 Notwithstanding anything to the contrary contained in this Agreement (including without limitation those provisions that are or are intended to be covenants running with the land), a mortgagee or beneficiary under a deed of trust, including any mortgagee or beneficiary who obtains title to the Property or any portion thereof as a result of foreclosure proceedings or conveyance or other action in lieu thereof, or other remedial action, ("Mortgagee") shall not be obligated under this Agreement to construct or complete the Inclusionary Units required by this Agreement or to guarantee their construction or completion solely because the Mortgagee holds a mortgage or other interest in the Property or this Agreement. The foregoing provisions shall not be applicable to any other party who, after such foreclosure, conveyance, or other action in lieu thereof, or other remedial action, obtains title to the Property or a portion thereof from or through the Mortgagee or any other purchaser at a foreclosure sale other than the Mortgagee itself. A breach of any obligation secured by any mortgage or other lien against the mortgaged interest or a foreclosure under any mortgage or other lien shall not by itself defeat, diminish, render invalid or unenforceable, or otherwise impair the obligations or rights of the Developer under this Agreement.



7.5.2 Subject to the provisions of the first sentence of Section 7.5.1, any person, including a Mortgagee, who acquires title to all or any portion of the mortgaged property by foreclosure, trustee's sale, deed in lieu of foreclosure, or otherwise shall succeed to all of the rights and obligations of the Developer under this Agreement and shall take title subject to all of the terms and conditions of this Agreement. Nothing in this Agreement shall be deemed or construed to permit or authorize any such holder to devote any portion of the Property to any uses, or to construct any improvements, other than the uses and improvements provided for or authorized by the Project Approvals and this Agreement.

7.5.3 If City receives a written notice from a Mortgagee or from Developer requesting a copy of any Notice of Default delivered to Developer and specifying the address for service thereof, then City shall deliver to such Mortgagee, concurrently with service thereon to Developer, any Notice of Default delivered to Developer under this Agreement. In accordance with Section 2924 of the California Civil Code, City hereby requests that a copy of any notice of default and a copy of any notice of sale under any mortgage or deed of trust be mailed to City at the address shown on the first page of this Agreement for recording, provided that no Mortgagee or trustee under a deed of trust shall incur any liability to the City for any failure to give any such notice of default or notice of sale except to the extent the City records a request for notice of default and notice of sale in compliance with Section 2924b of the California Civil Code (a "Request for Special Notice") with respect to a specific mortgage or deed of trust and the Mortgagee or trustee fails to give any notice required under Section 2924b of the California Civil Code as a result of the recordation of a Request for Special Notice.

7.5.4 A Mortgagee shall have the right, at its option, to cure any default or breach by the Developer under this Agreement within the same time period as Developer has to remedy or cause to be remedied any default or breach, plus an additional period of (i) thirty (30) calendar days to cure a default or breach by the Developer to pay any sum of money required to be paid hereunder and (ii) ninety (90) days to cure or commence to cure a non-monetary default or breach and thereafter to pursue such cure diligently to completion; provided that if the Mortgagee cannot cure a non-monetary default or breach without acquiring title to the Property, then so long as Mortgagee is diligently pursuing foreclosure of its mortgage or deed of trust, Mortgagee shall have until ninety (90) days after completion of such foreclosure to cure such non-monetary default or breach. Mortgagee may add the cost of such cure to the indebtedness or other obligation evidenced by its mortgage, provided that if the breach or default is with respect to the construction of the improvements on the Property, nothing contained in this Section or elsewhere in this Agreement shall be deemed to permit or authorize such Mortgagee, either before or after foreclosure or action in lieu thereof or other remedial measure, to undertake or continue the construction or completion of the improvements (beyond the extent necessary to conserve or protect improvements or construction already made) without first having expressly assumed the obligation to the City, by written agreement reasonably satisfactory to the City, to complete in the manner provided in this Agreement the improvements on the Property or the part thereof to which the lien or title of such Mortgagee relates. Notwithstanding a Mortgagee's agreement to assume the obligation to complete in the manner provided in this Agreement the improvements on the Property or the part thereof acquired by such Mortgagee, the Mortgagee shall have the right to abandon completion of the improvement at any time thereafter.



7.5.5 If at any time there is more than one mortgage constituting a lien on any portion of the Property, the lien of the Mortgagee prior in lien to all others on that portion of the mortgaged property shall be vested with the rights under this Section 7.5 to the exclusion of the holder of any junior mortgage; provided that if the holder of the senior mortgage notifies the City that it elects not to exercise the rights sets forth in this Section 7.5, then each holder of a mortgage junior in lien in the order of priority of their respective liens shall have the right to exercise those rights to the exclusion of junior lien holders. Neither any failure by the senior Mortgagee to exercise its rights under this Agreement nor any delay in the response of a Mortgagee to any notice by the City shall extend Developer's or any Mortgagee's rights under this Section 7.5. For purposes of this Section 7.5, in the absence of an order of a court of competent jurisdiction that is served on the City, a then current title report of a title company licensed to do business in the State of California and having an office in the City setting forth the order of priority of lien of the mortgages shall be reasonably relied upon by the City as evidence of priority.

7.6 Constructive Notice. Every person or entity who now or hereafter owns or acquires any right, title or interest in or to any portion of the Project or the Property is and shall be constructively deemed to have consented and agreed to every provision contained herein, whether or not any reference to this Agreement is contained in the instrument by which such person acquired an interest in the Project or the Property.

## **8. ENFORCEMENT OF AGREEMENT; REMEDIES FOR DEFAULT; DISPUTE RESOLUTION**

8.1 Enforcement. The only parties to this Agreement are the City and the Developer. This Agreement is not intended, and shall not be construed, to benefit or be enforceable by any other person or entity whatsoever.

8.2 Default. For purposes of this Agreement, the following shall constitute a default under this Agreement: the failure to perform or fulfill any material term, provision, obligation, or covenant hereunder and the continuation of such failure for a period of thirty (30) calendar days following a written notice of default and demand for compliance; provided, however, if a cure cannot reasonably be completed within thirty (30) days, then it shall not be considered a default if a cure is commenced within said 30-day period and diligently prosecuted to completion thereafter, but in no event later than one hundred twenty (120) days.

8.3 Remedies for Default. In the event of an uncured default under this Agreement, the remedies available to a Party shall include specific performance of the Agreement in addition to any other remedy available at law or in equity. In addition, the non-defaulting Party may terminate this Agreement subject to the provisions of this Section 8 by sending a Notice of Intent to Terminate to the other Party setting forth the basis for the termination. The Agreement will be considered terminated effective upon receipt of a Notice of Termination. The Party receiving the Notice of Termination may take legal action available at law or in equity if it believes the other Party's decision to terminate was not legally supportable.

8.4 No Waiver. Failure or delay in giving notice of default shall not constitute a waiver of default, nor shall it change the time of default. Except as otherwise expressly provided in this Agreement, any failure or delay by a Party in asserting any of its rights or remedies as to any default



shall not operate as a waiver of any default or of any such rights or remedies; nor shall it deprive any such Party of its right to institute and maintain any actions or proceedings that it may deem necessary to protect, assert, or enforce any such rights or remedies.

## **9. MISCELLANEOUS PROVISIONS**

9.1 Entire Agreement. This Agreement, including the preamble paragraph, Recitals and Exhibits, constitute the entire understanding and agreement between the Parties with respect to the subject matter contained herein.

9.2 Binding Covenants; Run With the Land. From and after recordation of this Agreement, all of the provisions, agreements, rights, powers, standards, terms, covenants and obligations contained in this Agreement shall be binding upon the Parties, and their respective heirs, successors (by merger, consolidation, or otherwise) and assigns, and all persons or entities acquiring the Property, any lot, parcel or any portion thereof, or any interest therein, whether by sale, operation of law, or in any manner whatsoever, and shall inure to the benefit of the Parties and their respective heirs, successors (by merger, consolidation or otherwise) and assigns. Regardless of whether the procedures in Section 7 are followed, all provisions of this Agreement shall be enforceable during the term hereof as equitable servitudes and constitute covenants and benefits running with the land pursuant to applicable law, including but not limited to California Civil Code Section 1468.

9.3 Applicable Law and Venue. This Agreement has been executed and delivered in and shall be interpreted, construed, and enforced in accordance with the laws of the State of California. All rights and obligations of the Parties under this Agreement are to be performed in the City and County of San Francisco, and such City and County shall be the venue for any legal action or proceeding that may be brought, or arise out of, in connection with or by reason of this Agreement.

9.4 Construction of Agreement. The Parties have mutually negotiated the terms and conditions of this Agreement and its terms and provisions have been reviewed and revised by legal counsel for both City and Developer. Accordingly, no presumption or rule that ambiguities shall be construed against the drafting Party shall apply to the interpretation or enforcement of this Agreement. Language in this Agreement shall be construed as a whole and in accordance with its true meaning. The captions of the paragraphs and subparagraphs of this Agreement are for convenience only and shall not be considered or referred to in resolving questions of construction. Each reference in this Agreement to this Agreement or any of the Project Approvals shall be deemed to refer to the Agreement or the Project Approval as it may be amended from time to time pursuant to the provisions of the Agreement, whether or not the particular reference refers to such possible amendment.

### **9.5 Project Is a Private Undertaking; No Joint Venture or Partnership.**

9.5.1 The development proposed to be undertaken by Developer on the Property is a private development. The City has no interest in, responsibility for, or duty to third persons concerning any of said improvements. The Developer shall exercise full dominion and control over the Property, subject only to the limitations and obligations of the Developer contained in this Agreement or in the Project Approvals.



9.5.2 Nothing contained in this Agreement, or in any document executed in connection with this Agreement, shall be construed as creating a joint venture or partnership between the City and the Developer. Neither Party is acting as the agent of the other Party in any respect hereunder. The Developer is not a state or governmental actor with respect to any activity conducted by the Developer hereunder.

9.6 Signature in Counterparts. This Agreement may be executed in duplicate counterpart originals, each of which is deemed to be an original, and all of which when taken together shall constitute one and the same instrument.

9.7 Time of the Essence. Time is of the essence in the performance of each and every covenant and obligation to be performed by the Parties under this Agreement.

9.8 Notices. Any notice or communication required or authorized by this Agreement shall be in writing and may be delivered personally or by registered mail, return receipt requested. Notice, whether given by personal delivery or registered mail, shall be deemed to have been given and received upon the actual receipt by any of the addressees designated below as the person to whom notices are to be sent. Either Party to this Agreement may at any time, upon written notice to the other Party, designate any other person or address in substitution of the person and address to which such notice or communication shall be given. Such notices or communications shall be given to the Parties at their addresses set forth below:

**To City:**

John Rahaim  
Director of Planning  
San Francisco Planning Department  
1650 Mission Street  
San Francisco, California 94102

with a copy to:

Dennis J. Herrera, Esq.  
City Attorney  
City Hall, Room 234  
1 Dr. Carlton B. Goodlett Place  
San Francisco, CA 94102

**To Developer:**

Potrero Partners LLC  
445 Virginia Avenue  
San Mateo, CA 94402  
Attn: Josh Smith

and a copy to:



Steven L. Vettel  
Farella Braun + Martel LLP  
235 Montgomery Street  
San Francisco, CA 94104

9.9 Severability. If any term, provision, covenant, or condition of this Agreement is held by a court of competent jurisdiction to be invalid, void, or unenforceable, the remaining provisions of this Agreement shall continue in full force and effect unless enforcement of the remaining portions of the Agreement would be unreasonable or grossly inequitable under all the circumstances or would frustrate the purposes of this Agreement.

9.10 MacBride Principles. The City urges companies doing business in Northern Ireland to move toward resolving employment inequities and encourages them to abide by the MacBride Principles as expressed in San Francisco Administrative Code Section 12F.1 et seq. The City also urges San Francisco companies to do business with corporations that abide by the MacBride Principles. Developer acknowledges that it has read and understands the above statement of the City concerning doing business in Northern Ireland.

9.11 Tropical Hardwood and Virgin Redwood. The City urges companies not to import, purchase, obtain or use for any purpose, any tropical hardwood, tropical hardwood wood product, virgin redwood, or virgin redwood wood product.

9.12 Sunshine. The Developer understands and agrees that under the City's Sunshine Ordinance (San Francisco Administrative Code, Chapter 67) and the State Public Records Law (Gov't Code Section 6250 et seq.), this Agreement and any and all records, information, and materials submitted to the City hereunder are public records subject to public disclosure.

9.13 Effective Date. This Agreement will become effective on the date that the last Party duly executes and delivers this Agreement.



IN WITNESS WHEREOF, the Parties hereto have executed this Agreement as of the day and year first above written.

**CITY**

CITY AND COUNTY OF SAN  
FRANCISCO,  
a municipal corporation

Approved as to form:  
Dennis J. Herrera, City Attorney

By: \_\_\_\_\_  
John Rahaim  
Director of Planning

By:  \_\_\_\_\_  
Deputy City Attorney

**DEVELOPER**

POTRERO PARTNERS LLC,  
a California limited liability company

By:  \_\_\_\_\_  
Name: Josh Smith  
Title: Manager



A notary public or other officer completing this certificate verifies only the identity of the individual who signed the document to which this certificate is attached, and not the truthfulness, accuracy, or validity of that document.

STATE OF CALIFORNIA )  
COUNTY OF San Francisco )

Subscribed and sworn to (or affirmed) before me on this May 3, 2016, by  
John Smith, who proved to me on the basis of satisfactory evidence to be  
the person(s) who appeared before me.

Signature Mary Staats (seal)





A notary public or other officer completing this certificate verifies only the identity of the individual who signed the document to which this certificate is attached, and not the truthfulness, accuracy, or validity of that document.

STATE OF CALIFORNIA )  
COUNTY OF \_\_\_\_\_ )

Subscribed and sworn to (or affirmed) before me on this \_\_\_\_\_, by \_\_\_\_\_, who proved to me on the basis of satisfactory evidence to be the person(s) who appeared before me.

Signature: \_\_\_\_\_ (seal)



## **EXHIBIT A**

### **Legal Description of Property**



May 2, 2016

Hon. Rodney Fong, President  
San Francisco Planning Commission  
1650 Mission Street, 4<sup>th</sup> Floor  
San Francisco, CA 94103

Re: 901 16<sup>th</sup> Street/1200 17<sup>th</sup> Street  
Case No. 2011.1300EX (Large Project Authorization)  
Hearing Date: May 12, 2016

Dear President Fong and Commissioners:

I am writing on behalf of Potrero Partners and its principal Josh Smith, the project sponsor of the 901 16<sup>th</sup> Street/1200 17<sup>th</sup> Street project proposed at the property commonly referred to as the "Cor-O-Van site" (although Cor-O-Van no longer occupies any portion of the site). The project has been in the entitlement pipeline since 2012 and includes the rehabilitation of an historic brick structure on 17<sup>th</sup> Street, demolition of several interconnected non-historic metal shed warehouses and a modular office building, and construction of a mixed use project containing 395 dwelling units and nearly 25,000 square feet of ground floor neighborhood serving retail space fronting both 16<sup>th</sup> Street and 17<sup>th</sup> Street. The project comprises two separate buildings -- the "16<sup>th</sup> Street building" designed by BAR Architects and the "17<sup>th</sup> Street building" designed by Christiani Johnson Architects. The project will be before you on May 12 for certification of the Final EIR, adoption of CEQA findings and consideration of a Large Project Authorization.

Previously, Kaiser Permanente had proposed a medical office building use for the 16<sup>th</sup> Street building with Potrero Partners proposing housing on 17<sup>th</sup> Street. When Kaiser decided to build in Mission Bay instead, the project evolved into both buildings being primarily residential (with a major redesign of both buildings in response to extensive community input) but remains mixed use, with a substantial neighborhood retail component. Throughout this process, the sponsor has been in frequent contact with Potrero Hill community organizations and neighbors and has incorporated the input of those neighbors, groups and the Planning Department staff into the project program and design. Attached as Exhibit A is a summary of the several years of outreach conducted by Josh.

Project Description. The project site is large (~3.5 acres) and comprises the eastern 2/3 of the large block bound by 16<sup>th</sup> Street, Mississippi Street, 17<sup>th</sup> Street and Missouri Street in lower Potrero Hill within the Showplace Square/Potrero Area Plan. To encourage mixed-use development with heightened inclusionary housing requirements, the 2008 Eastern Neighborhoods Plan rezoned the site from an M-1 district to the UMU (Urban Mixed Use) district, with height limits of 68 feet along 16<sup>th</sup> Street and 48 feet along 17<sup>th</sup> Street.



The project architects are Sherry Scott of Christiani Johnson Architects for the 17<sup>th</sup> Street building and David Israel of BAR Architects for the 16<sup>th</sup> Street building. The landscape architect is Cathy Garrett of PGA Design. Olle Lundberg of Lundberg Design is also consulting on the project and is focused on incorporating the few interesting elements of the metal warehouses, such as some roof trusses and a water tank, into the landscape design. Your Commission packet includes drawing sets from all four firms.

For the four-story 17<sup>th</sup> Street building, the design centers around the historic brick office building that will be retained and repurposed as a restaurant/retail space. The new construction is pulled back from the brick building so that it retains its historic integrity, and the materials for the new work pay homage to the site's industrial past, including board formed concrete, metal siding, and sawtooth roofs. On November 4, 2015, the Architectural Review Committee of the Historic Preservation Commission enthusiastically endorsed the design of the 17<sup>th</sup> Street building and concurred that the treatment of the brick building conforms to the Secretary of the Interior's Standards for Rehabilitation.

For the six-story 16<sup>th</sup> Street building, BAR took design cues from several industrial buildings in Potrero Hill, Showplace Square, and Dogpatch, and designed each of the four facades in a distinctive manner. The 17-foot tall retail base is clearly demarcated; a large horizontal mass reduction marks the residential lobby fronting 16<sup>th</sup> Street; and ground floor stoop units open onto the mid-block pedestrian alley and the private mews running between the two buildings.

The project open spaces are varied and generous. A 30-foot wide heavily landscaped pedestrian alley runs along the western property line, separating the project from the rear yards of the two live/work buildings adjacent to the site that front Missouri Street. Another public plaza is located along Mississippi Street adjacent to the 17<sup>th</sup> Street building's "bike lobby." A gated mews incorporating a secure family and children's play area runs between the Mississippi Street plaza and the pedestrian alley. In addition, each of the two buildings has two residential courtyards and one or more common roof decks. In all, the project includes 44,437 square feet of public and private open space, far in excess of the 28,172 square feet required by the Planning Code.

Environmental Review. The Final EIR determined that the project will have some intersection level of service impacts (using the prior LOS significance criteria that this Commission discontinued in March 2016), but no Vehicle Miles Travelled (VMT) impacts. The project would also contribute to the cumulative loss of PDR space expected and analyzed in the Eastern Neighborhoods EIR. All other environmental impacts, including to historic resources, are avoided or reduced to less than significant with the incorporation of mitigation measures agreed to by the sponsor. (*Had VMT been in place when the environmental review started, no EIR would have been necessary and the project would have qualified for a Community Plan Exemption.*) The EIR also contains photomontages demonstrating that the project will have no significant effects on public (or most private) views from Potrero Hill, especially since completion of the 68-foot high 1010 Potrero project directly across 16<sup>th</sup> Street, which is the same height as the 16<sup>th</sup> Street building.



Community Benefits. In addition to its compliance with the policies of the Eastern Neighborhoods Plan, the project proposes the following community benefits, which have been developed in consultation with community members and the district supervisor:

- Pursuant to Planning Code Section 419.5, the inclusionary housing requirements are high in the UMU district. The project will satisfy those requirements by providing 42 on-site BMR units in the 16<sup>th</sup> Street building (16% of that building's dwelling units) and the payment of approximately \$9,709,727 to the Affordable Housing Fund for the 17<sup>th</sup> Street building, representing a 23% in lieu/off-site contribution.
- The project's total of 395 units will make a significant contribution to addressing San Francisco's housing shortage, including housing for families. During design development, the sponsor has agreed with community members to increase the number of three-bedroom units from the 14 units shown in the current plans to 22 three-bedroom units. The current plans also include 146 two-bedroom/2 full bath units, and the mews between the two buildings has a gated area set aside for a children's play area.
- The project provides a landscaped 30-foot wide mid-block pedestrian alley connecting 16<sup>th</sup> and 17<sup>th</sup> Street along the western property line. The alley will be ungated and publicly accessible 24/7, and be aligned with and directly across 16<sup>th</sup> Street from the currently under-construction Daggett Park that is scheduled to be completed by this July.
- Potrero Partners will make a \$1,000,000 contribution to Friends of Jackson Playground to support the renovation of this large neighborhood park, situated within 2 blocks of the project site.
- Potrero Partners will pay approximately \$2,889,776 to the Eastern Neighborhoods Infrastructure Fund to support other neighborhood open space, streetscape and transit improvements and is not seeking any reduction in that payment through an in-kind agreement. The Final EIR describes how improvements in the Eastern Neighborhoods, including Daggett Park and the 16<sup>th</sup> Street bus rapid transit project to be completed in 2018, are utilizing these fees to support the infrastructure required to support new development. The project is also subject to and will pay the Transportation Sustainability Fee (approximately \$2,087,273) and the Child Care Fee (approximately \$802,786). Potrero Partners (along the 1601 Mariposa Street and the 1010 Potrero project) has also voluntarily agreed to fund a portion of the cost to signalize the intersection of Mariposa Street and Pennsylvania Street, which is currently difficult for pedestrians to cross.
- The project is truly mixed-use by providing nearly 25,000 square feet of ground floor neighborhood serving retail space along both 16<sup>th</sup> and 17<sup>th</sup> Streets, including a large space at the corner of 16<sup>th</sup> and Mississippi Streets that is of sufficient size to accommodate a neighborhood market or pharmacy. Thirty-nine (39) retail parking spaces are provided immediately adjacent to the neighborhood-serving retail space fronting 16<sup>th</sup> Street.
- The ground floor of the 17<sup>th</sup> Street building that is located diagonally across the street from Bottom of the Hill nightclub has been repurposed from dwelling units to a fitness



room and resident lobby and lounge in order to avoid conflicts with the club, and the units above will be heavily soundproofed.

- Six ground floor units further to the east along 17<sup>th</sup> Street have been redesigned as “flex units” to allow residents to operate a business, PDR or arts activity from their home.

Current Uses, Loss of PDR and Mixed Use. The non-historic metal warehouses on the site that were formerly occupied by Cor-O-Van Moving and Storage and are currently occupied by a few tenants for storage and assembly and by UCSF for construction storage. Only about 35 people are employed at the 3.5 acre site, and all current businesses are planning to relocate when their leases expire later this year or in early 2017.

The project’s EIR evaluated a “Metal Shed Reuse Alternative,” and the project sponsor retained Seifel Associates to evaluate its feasibility (a copy of Seifel’s feasibility study is attached hereto as Exhibit B). The EIR determined the metal sheds are not historic resources, such that this alternative would not reduce historic resource impacts, and that the loss of PDR represented by the demolition of the sheds was already analyzed in the Eastern Neighborhoods EIR’s cumulative land use analysis. When the City rezoned the site from M-1 to UMU, it made findings that the loss of PDR space that would be associated with the rezoning was overridden by the benefits associated with the Eastern Neighborhoods Plan, primarily in the creation of a significant amount of new housing. Seifel’s report concludes that reuse of the sheds is financially infeasible due to the high cost to rehabilitate them for modern PDR or residential occupancy and the significant reduction in dwelling units (from 395 units to 177 units) in the reuse alternative. Seifel determined that the EIR’s 273-unit “Reduced Density Alternative” was also financially infeasible. The Planning Department is requesting that the Commission reject both alternatives in your CEQA Findings.

Rather than incorporate PDR space in this mixed use project, the focus is on creating a neighborhood-serving retail hub along 16<sup>th</sup> Street, with 24,968 square feet of retail space in this project along with the 10,000 square feet of retail in the Potrero 1010 project that is located directly across 16<sup>th</sup> Street. This area of Potrero Hill and Showplace Square is in need of neighborhood serving retail uses, and this location is ideal for that purpose. Along 17<sup>th</sup> Street, the project incorporates two small ground floor retail spaces and six “flex units” from which residents can operate a small business, including small PDR and arts uses.

Large Project Authorization. Dense housing is principally permitted in the UMU district, provided 40% of the units are 2-bedroom or larger and the district’s heightened inclusionary housing requirements are met. The project meets that standard and the sponsor is seeking LPA approval from the Commission for the project design, with seven permitted minor exceptions. The exceptions are for the pedestrian alley, mews and courtyards in lieu of a single rear yard; for the dimensions of the horizontal mass reduction on 16<sup>th</sup> Street; for the location of three curb cuts on Mississippi Street (because curb cuts are not permitted on 16<sup>th</sup> Street and a bike lane will be located on the north side of 17<sup>th</sup> Street); for obstruction over the street for the design of a bay window system facing Mississippi Street; for the flex units on 17<sup>th</sup> Street; to utilize on-street residential loading spaces; and to permit 1 parking space per unit for the 2-and 3-bedroom units (the smaller units are parked at a 0.75:1 ratio).



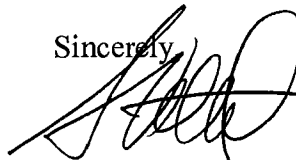
The Planning Department staff is supportive of the project design and the minor exceptions sought, except for the 40 additional parking spaces which would serve the family-friendly 2 bedroom/2 full bath units as well as the 3 bedroom/2 full bath units, and which under a strict reading of the Code should be "space efficient." However, given the dimensions of the below grade single level garage in each building, it is not feasible, nor would it advance any design or public policy purpose, for these parking spaces to be in stackers. Even with the requested exception, the parking ratio is low: 0.85 space per unit. The project complies with every other applicable provision of the Planning Code, including its uses, height, car share parking, bike parking, active street frontages, open space dimensions and unit exposure. We request that the Commission grant these minor exceptions.

Residential projects consistent with the Eastern Neighborhoods Plan warrant approval. There is a small but dedicated group that has opposed this project since it was initially proposed, first because they opposed a medical office building on Potrero Hill and now, after Kaiser withdrew from the project, for various reasons focused on views, an insistence that the metal sheds are historic resources, disputing the Eastern Neighborhood's fully anticipated loss of PDR space in UMU districts, or a desire to revisit the cumulative effects of the Eastern Neighborhoods Plan. Many others in the neighborhood are in support, and your Commission packet contains many letters and emails expressing that support.

In response to the State of California's chronic housing shortage, the legislature has enacted the Housing Accountability Act (Cal. Govt. Code Section 65589.5). It directs cities not to disapprove housing projects generally consistent with objective general plan policies and zoning unless the project will cause significant adverse impacts to public health or safety. The Board of Appeals has recognized the applicability of the Act in San Francisco in its recent 2015 decision on the 1050 Valencia project. Here, no one has identified any public health or safety impacts associated with this project, and, in fact, the Final EIR establishes otherwise. A desire by some to halt implementation of the Eastern Neighborhoods Plan does not provide valid grounds for disapproving this development, which has been in the Planning pipeline for over four years. Accordingly, consistent with the Showplace Square/Potrero Area Plan, the UMU zoning, the City's Housing Element and other General Plan policies, and with the mandate of the Housing Accountability Act, we request that the Commission approve the 901 16<sup>th</sup> Street/1200 17<sup>th</sup> Street project as proposed, including the significant package of community benefits the sponsor is offering.

Please contact me prior to the May 12 hearing if we can provide any additional information.

Sincerely,



Steven L. Vettel

cc: Chris Townes, Planner  
Josh Smith, Potrero Partners



# EXHIBIT A



April 28, 2016

Hon. Rodney Fong, President  
San Francisco Planning Commission  
1650 Mission Street, 4<sup>th</sup> Floor  
San Francisco, CA 94103

Re: **Summary of Community Outreach Re: Proposed Project at  
901 16<sup>th</sup> Street/1200 17<sup>th</sup> Street, San Francisco  
Case No. 2011.1300X (Large Project Authorization)  
Hearing Date: May 12, 2016**

Dear President Fong and Commissioners:

For the past 11 years, we have been consistently and frequently involved in and engaged with the Potrero Hill community.

Since 2005, we have attended nearly every meeting of the Potrero Boosters Neighborhood Association (the "Boosters"), the Potrero Dogpatch Merchant's Association (the "PDMA"), and the Dogpatch Neighborhood Association (the "DNA").

A few years ago, the Boosters gave us an award for "Outstanding Community Outreach" at their annual dinner.

We have also attended almost every meeting of the Eastern Neighborhoods Citizens Advisory Committee ("ENCAC") since it was formed in 2009.

As Keith Goldstein, who is President of the Potrero Dogpatch Merchant's Association and a member of the Executive Committee of the Potrero Boosters Neighborhood Association stated in his letter of support for the proposed project:

*I have seen the developer, Walden Development, engage with our community like no other over the past 10 years (or more). He may have even attended more community meetings than any one of my neighbors. He has been quick to respond to concerns raised by the community and has offered his support to numerous local groups. I believe his heart is truly set on the improvement and quality of life of our neighborhood.*



Over the past 11 years, we have had hundreds of conversations, meetings, open houses, and presentations re: the various iterations of the proposed project. This extensive level of consultation with the community has led to us incorporate into the project numerous suggestions we received from our neighbors, including the following:

- In response to the request that the project's architecture acknowledge and celebrate Potrero Hill's industrial past, both buildings (901 16<sup>th</sup> Street by BAR Architecture as well as 1200 17<sup>th</sup> Street by Christiani Johnson Architects) incorporate an industrial architectural vocabulary. Authentic materials (such as brick, steel, board formed concrete, and corrugated metal) are used extensively;
- In addition to the above, a new metal shed retail building at the corner of 17<sup>th</sup> Street & Mississippi Street is designed to be reminiscent of the corrugated metal shed that currently exists at that corner. The retail use that will be located at that corner will activate 17<sup>th</sup> Street and create a vibrant street scape where members of the community can meet and socialize;
- In response to the request that the brick building at the corner of 17<sup>th</sup> Street & Texas Street be prominently featured and "celebrated", the brick building will be restored and repurposed as a retail use (such as a restaurant or bar) and as such, will be open to the public during normal business hours. Prior iterations of the project contemplated the brick building being used as a lobby, which would not have been open to the public;
- In response to the request that the project contain a mix of uses:
  - The proposed project includes approximately 25,000 square feet of ground floor, neighborhood serving retail space that will activate both 16<sup>th</sup> Street and 17<sup>th</sup> Street; and
  - Six (6) units that are located on the ground floor facing 17<sup>th</sup> Street between the brick building and the metal shed retail building (both mentioned above) are designated as "flex" units, and as such, may be used for commercial uses, including PDR activities and artist work spaces;
- In response to the request that the project be "family friendly":
  - the unit mix has been modified to be very "family friendly": there will be twenty two (22) 3 bedroom/2 full bath units as well as one hundred forty six (146) 2 bedroom/2 full bath units;
  - the specific transportation needs of families have been taken into account as all 168 of the "family friendly" units referenced above are parked at a ratio of 1 parking space per family friendly unit (vs. the normal ratio of only .75 parking space per unit); and
  - the area between the two buildings will be programmed to be a kid friendly play area where residents will be able to watch their kids play and socialize in a gated family friendly recreation area.
- In response to the request that the Pedestrian Promenade connecting 17<sup>th</sup> Street to 16<sup>th</sup> Street be inviting as well as pedestrian and family friendly, the



width of the Pedestrian Promenade at grade has been voluntarily increased from the required width of 20' to a width that varies between 26' to 32'. Additionally the hardscape within the pedestrian promenade gently slopes between 16<sup>th</sup> Street and 17<sup>th</sup> Street without any stairs so that it is "baby stroller and bicycle friendly".

Our extensive and frequent outreach efforts with the local community include:

**Potrero Hill Boosters Neighborhood Association ("The Boosters"):**

- We have made several presentations re: the proposed project to the Boosters at their general meeting;
- We have made several presentations re: the proposed project to the members of the Executive Committee of the Boosters;
- We made a presentation to the recently formed Potrero Boosters Neighborhood Association Development Committee;
- We have had several hundred in person meetings and telephone conversations with individual members of the Boosters about virtually every aspect of the proposed project;

**Potrero Dogpatch Merchant's Association ("PDMA"):**

- We made several presentations re: the proposed project to the PDMA their general meeting;
- We have had dozens of in person meetings and phone conversations with individual members of the PDMA about virtually every aspect of the proposed project;

**Dogpatch Neighborhood Association ("DNA"):**

- We have made several presentations re: the proposed project to the DNA at their general meeting;
- We have two presentations to the members of the Executive Committee of the DNA;
- We have had well over one hundred in person meetings and telephone conversations with individual members of the DNA about virtually every aspect of the proposed project;

**Friends of Jackson Park :**

- We have met with Jude Deckenback and have also had several subsequent conversations (both in person and via phone) with Jude about the potential improvements to Jackson Park that are currently being contemplated; and

**The Loop:**

- We have met with Jean Bogiages several times about the potential improvements and modifications to the area underneath and adjacent to U.S. Highway 101 near 16<sup>th</sup> Street and 17<sup>th</sup> Street on Potrero Hill.



### **Neighboring Property Owners:**

- 999 16<sup>th</sup> Street: We have made several presentations to the HOA of this 20-unit live-work building that is located just west of the proposed project and we have also had several follow up telephone conversations with representatives of the president of 999 16<sup>th</sup> Street's HOA;
- 49 Missouri Street: We have made several presentations to the HOA of this 12-unit live work building that is located just west of the proposed project and we have also had several follow up telephone conversations with some of the residents of the building;
- 1240-1250 17<sup>th</sup> Street: We have met with the owners of this property that is located just west of the proposed project and we have also had dozens of telephone conversations with them about the proposed project;
- 100 Texas Street (Vacant Lot at SWC of 17<sup>th</sup> Street & Texas Street): We have met with the owners of this property several times and have had dozens of telephone conversations with them;
- Bottom of the Hill Nightclub (1233 17<sup>th</sup> Street): We have met with all four (4) owners of The Bottom of the Hill and as a result of their concerns about the proposed project, the ground floor of the 17<sup>th</sup> Street building that is located diagonally across the street from Bottom of the Hill nightclub has been repurposed from dwelling units to a fitness room and resident lobby & lounge in order to avoid conflicts with the club, and the units above will be heavily soundproofed;
- 1111 17<sup>th</sup> Street: We have met with the owners of this property several times and have and dozens of the telephone conversations with them.

### **Numerous Individuals (partial list):**

Additionally, we have had dozens of meetings and phone calls with many individuals who live and/or work on or near Potrero Hill including (partial list):

- |                     |                    |                         |
|---------------------|--------------------|-------------------------|
| • Mayor Art Agnos   | • J.R. Eppler      | • Tony Kelly            |
| • Stacey Bartlett   | • Susan Eslick     | • Ron Miguel            |
| • Joe Boss          | • Frank Gilson     | • Dick Millet           |
| • Janet Carpinelli  | • Keith Goldstein  | • Steve Moss            |
| • Philip De Andrade | • Kayren Hudiburgh | • Lisa Schiller-Tehrani |
| • John DeCastro     | • Bruce Huie       | • Lester Zeidman        |
|                     | • Edward Hatter    |                         |



**Save the Hill:**

We have had several meetings with “Save The Hill”, which is a group that is opposed the proposed project and has opposed all of the previous iterations of the proposed project.

At their request, we have met with representatives of Save the Hill on:

- September 5, 2012 at a private residence on Mariposa Street
- October 18, 2012 at a private residence on Texas Street
- September 25, 2013 at a private residence on Texas Street
- October 18, 2013 at a private residence on Missouri Street
- December 11, 2013 at Farley’s on 18<sup>th</sup> Street
- February 15, 2014 at Live Oak School
- January 29, 2016 at California College of the Arts
- March 18, 2016 at California College of the Arts

In addition to the eight (8) meetings listed above that we have had with representatives from Save the Hill, we have also had numerous telephone conversations with several of their members over the past five (5) years.

Respectfully,

A handwritten signature in blue ink, appearing to read "Josh Smith", with a stylized, flowing script.

Josh Smith  
Potrero Partners/Walden Development



# EXHIBIT B





# Memorandum

**Date:** April 12, 2016  
**To:** Josh Smith of Potrero Partners LLC, Steven Vettel of Farella Braun + Martel LLP  
**From:** Seifel Consulting Inc.  
**Subject:** Financial Feasibility Analysis of 901 16<sup>th</sup> Street and 1200 17<sup>th</sup> Street Project

Potrero Partners LLC (Project Sponsor) retained Seifel Consulting Inc. (Seifel) to provide real estate advisory services in connection with the environmental review process for the Project Sponsor's proposed mixed-use development located on 16<sup>th</sup> and 17<sup>th</sup> Streets in the Potrero Hill neighborhood of San Francisco. Seifel performed an independent review of development assumptions and the financial feasibility for the proposed 395-unit project (Proposed Project) and both of the Draft Environmental Impact Report's lower density alternatives that would result in fewer residential units being developed on the site.

This memorandum summarizes the findings of our financial analysis, and is organized as follows:

- A. Description of Proposed Project (901 16<sup>th</sup> Street and 1200 17<sup>th</sup> Street Project)
- B. Description of EIR Alternatives
- C. Review of Pro Forma Assumptions and Methodology
- D. Financial Feasibility Findings
- E. Conclusion
- F. Appendix - EIR Alternatives Modeled as Condominiums

The financial analysis presented in this memorandum clearly indicates that neither of the EIR Alternatives is financially feasible because many of the development costs are fixed, and neither of the Alternatives would generate sufficient revenues to cover the development costs and provide a sufficient developer margin in order for the Alternatives to be financially feasible.



## **A. Description of Proposed Project (901 16<sup>th</sup> Street and 1200 17<sup>th</sup> Street Project)**

Potrero Partners proposes to develop residential and ground-floor commercial uses on an approximately 3.5-acre site located at 901 16<sup>th</sup> Street and 1200 17<sup>th</sup> Street in the lower Potrero Hill neighborhood.<sup>1</sup> The site is located within the Showplace Square/Potrero Subarea of the Eastern Neighborhoods Rezoning and Area Plan (Eastern Neighborhoods Plan).<sup>2</sup>

The project sponsor proposes to construct two new buildings on the site:

- The 16<sup>th</sup> Street Building would consist of a new, 6-story, 68-foot tall building (excluding rooftop projections of up to 82 feet), with 260 dwelling units and about 20,000 Gross Square Feet (GSF) of retail on the northern lot.
- The 17<sup>th</sup> Street Building would consist of a new, 4-story, 48-foot tall building (excluding rooftop projections of up to 52 feet), with 135 dwelling units and about 5,000 GSF of retail on the southern lot.

The proposed development would contain the following components:

- 395 dwelling units
- About 24,000 square feet of leasable retail space
- 389 vehicular parking spaces
- 455 off-street bicycle parking spaces
- About 51,000 square feet of open space, including a courtyard terrace of about 25,000 square feet and about 15,000 square feet of public open space.
- A new, publicly accessible pedestrian promenade (i.e. alley) along the entirety of the Proposed Project's western property line.

Approximately 16% of the residential units in the 16<sup>th</sup> Street building (or 42 units) and 14.4% of the residential units in the 17<sup>th</sup> Street building (or 19 units) would be designated as affordable, below-market rate (BMR) rental units<sup>3</sup> that would provide homes for households earning 55% of Areawide Median Income (AMI), or \$44,850 per year for a two-person household.

---

<sup>1</sup> The approximately 3.5-acre project site is bounded by 16th Street to the north, Mississippi Street to the east, 17th Street to the south and residential and industrial buildings to the west. The project site currently contains two metal shed industrial warehouse buildings, a brick office building, a modular office structure and surface parking lots. The proposed project would merge the four lots into two, demolish the two warehouses and the modular office structure, preserve the brick office building and retain some materials from the steel sheds for reuse within the proposed project.

<sup>2</sup> Proposed and EIR Alternative project descriptions are taken from both the August 2015 Draft Environmental Impact Report ([http://sfmea.sfplanning.org/2011.1300E\\_DEIR\\_Part1.pdf](http://sfmea.sfplanning.org/2011.1300E_DEIR_Part1.pdf)) and information provided by the Project Sponsor.

<sup>3</sup> Position of the properties within the Eastern Neighborhoods Rezoning and Area Plan places different levels of BMR unit requirements on different portions of the property, with a higher BMR unit requirement on the 16<sup>th</sup> Street property location.



## **B. Description of EIR Alternatives**

The California Environmental Quality Act (CEQA) Guidelines require that an EIR describe a reasonable range of potentially feasible alternatives to the proposed project that could attain most of the basic project objectives while reducing the adverse impacts caused by the proposed project. The project EIR considered three alternatives:

- No Project Alternative
- Reduced Density Alternative
- Metal Shed Reuse Alternative

### **1. Reduced Density Alternative**

The EIR concluded that the Reduced Density Alternative would be an environmentally superior alternative to the proposed Project under a level of service (LOS) analysis for traffic-related impacts because it would meet the project sponsor's basic objectives to some extent, while avoiding some of the significant, unavoidable with mitigation traffic-related impacts of the proposed Project.<sup>4</sup> The Reduced Density Alternative would alternatively develop the site with the following uses:

- 273 residential units, which are larger than the units in the Proposed Project
- About 17,000 square feet of leasable retail space
- About 57,000 square feet of open space, including a courtyard terrace of about 34,000 square feet.
- 275 vehicular parking spaces within a below-grade garage and associated improvements.

The building heights would be 6 stories (68 feet, excluding rooftop projections of up to 82 feet) along 16<sup>th</sup> Street and 4 stories (48 feet, excluding rooftop projections of up to 52 feet) along 17<sup>th</sup> Street. As with the proposed project, the Reduced Density Alternative is proposed to provide rental housing, including 42 units made available at BMR<sup>5</sup> rents affordable to households at 55% of AMI.<sup>6</sup>

### **2. Metal Shed Reuse Alternative**

The EIR concluded that the Metal Shed Reuse Alternative would be an environmentally superior alternative to the proposed Project because it would meet the project sponsor's basic objectives to some extent, while reducing impacts related to the cumulative loss of PDR (production, distribution and repair) space in the Eastern Neighborhood. The Metal Shed Reuse Alternative would build a new, 5-story building (58 feet, excluding rooftop projections of up to 72 feet) along 16<sup>th</sup> Street and would retain and reuse the warehouse buildings along 17<sup>th</sup> Street with building heights up to 4 stories (48 feet, excluding rooftop projections of up to 62 feet) along 17<sup>th</sup> Street. This alternative would consist of a mix of residential units, commercial space, and artist workspace and exhibition space as follows:

- 177 residential units, which are larger than the units in the Proposed Project
- About 20,000 square feet of leasable retail space

---

<sup>4</sup> This impact reduction would be achieved because this alternative would have fewer residential units and commercial space at the site compared to the proposed project, and therefore have associated reductions in vehicle traffic compared to the proposed project.

<sup>5</sup> As with the Proposed Project, the BMR requirements differ by building. The 16<sup>th</sup> Street building requires 16% (or 28 units), and the 17<sup>th</sup> Street building requires 14.4% (or 14 units), for a total of 42 BMR units.

<sup>6</sup> The Reduced Density Alternative was also modeled as a for-sale condominium project, with 42 BMR units affordable at 90% AMI. That analysis is contained in the Appendix.



- About 55,000 square feet of artist workspace and exhibition space
- About 36,000 square feet of open space, including a courtyard terrace of about 25,000 square feet.
- 126 vehicular parking spaces within a below-grade garage and associated improvements.

As with the Proposed Project and the Reduced Density Alternative, the Metal Shed Reuse Alternative is proposed to provide rental housing, including 27 units made available at BMR<sup>7</sup> rents affordable to households at 55% of AMI.<sup>8</sup>

## C. Review of Pro Forma Assumptions and Methodology

The financial analysis is based on information from the Project Sponsor, and builds upon Seifel's recent work for San Francisco's Planning Department, Mayor's Office of Housing and Community Development and Municipal Transportation Agency to analyze the financial feasibility of similar mixed-use developments. During the performance of these assignments for the City of San Francisco, Seifel interviewed City staff and members of the real estate community (including developers, contractors, residential and commercial market specialists and architects) to obtain current development revenue, cost and financial performance data and assumptions. Seifel also reviewed a broad range of development pro formas for projects recently constructed or in the development pipeline in San Francisco.

This section describes the project assumptions, development costs, revenues, expenses and return metrics used in the financial analysis. Except where noted, the same assumptions apply to both EIR Alternatives and the Proposed Project for this financial evaluation.

### 1. Development Program Assumptions

The residential units in the Proposed Project and EIR Alternatives include a mix of studios to three-bedroom apartments, ranging from 480 to 1,350 square feet. For this financial analysis, Seifel assumes an average size of 865-square-foot residences for the Proposed Project, which is consistent with the average unit mix of comparable new apartment buildings in San Francisco. The Reduced Density Alternative analysis assumes larger, 989-square-foot residences, and includes no studios, to reflect the fact that a developer would likely increase unit sizes to maximize the revenue from the project. The Metal Shed Reuse Alternative assumes 958-square-foot residences.<sup>9</sup>

As described above, the Project Sponsor intends to provide required affordable BMR rental units on site, and the analysis assumes that 14.4% to 16% of units, depending on the building requirements, are BMR units.

As described above, the Proposed Project includes 395 units, the Reduced Density Alternative 273 units, and the Metal Shed Reuse Alternative 177 units. See Table 1 for a summary of the development programs for each, including Retail/Restaurant and other space.

<sup>7</sup> As with the Proposed Project, the BMR requirements differ by building. The Metal Shed Reuse Alternative requires different positioning of the residential buildings on the site, such that Building 1 spans the BMR zoning requirement between 14.4% and 16%, so an average 15.2% (or 14 units) was modeled for Building 1. Building 2 requires 16% BMR (or 13 units), for a total of 27 BMR units.

<sup>8</sup> The Metal Shed Reuse Alternative was also modeled as a for-sale condominium project, with 27 BMR units affordable at 90% AMI. That analysis is contained in the Appendix.

<sup>9</sup> The Reduced Density and Metal Shed Reuse Alternatives are less efficient than the proposed project, as a higher proportion of the buildings must be dedicated to common area to accommodate the greater number of dead-end corridors and larger proportion of stairs. Due to the deep warehouse footprint for the Metal Shed Reuse alternative, a large amount of common area is dedicated to bicycles and other non-revenue producing uses because of the lack of exterior frontage.



**Table 1**  
**Summary of Development Programs**  
**901 16th Street and 1200 17th Street Project**

|   | Proposed Project     |                      |                | Reduced Density Alternative |                      |                | Metal Shed Reuse Alternative     |                       |            |                |
|---|----------------------|----------------------|----------------|-----------------------------|----------------------|----------------|----------------------------------|-----------------------|------------|----------------|
|   | 16th Street Building | 17th Street Building | Project Total  | 16th Street Building        | 17th Street Building | Project Total  | Metal Shed Commercial Public Use | Metal Shed Building 1 | Building 2 | Project Total  |
| <b>Total Building Area( GSF)</b>        |                      |                      | <b>616,452</b> |                             |                      | <b>561,625</b> |                                  |                       |            | <b>369,907</b> |
| <b>Residential</b>                      |                      |                      |                |                             |                      |                |                                  |                       |            |                |
| Total Residential Units                 | 260                  | 135                  | 395            | 177                         | 96                   | 273            | 0                                | 95                    | 82         | 177            |
| Below Market Rate Units                 | 42                   | 19                   | 61             | 28                          | 14                   | 42             | 0                                | 14                    | 13         | 27             |
| Very Low (55% AMI)                      | 42                   | 19                   | 61             | 0                           | 0                    | 0              | 0                                | 0                     | 0          | 0              |
| Moderate (90% AMI)                      | 0                    | 0                    | 0              | 28                          | 14                   | 42             | 0                                | 14                    | 13         | 27             |
| Total Residential Gross SF (GSF)        | 278,150              | 160,531              | 438,681        | 252,192                     | 148,436              | 400,628        | 0                                | 139,963               | 105,555    | 245,518        |
| Total Residential Net SF (NSF)          | 222,410              | 119,267              | 341,677        | 174,655                     | 95,430               | 270,085        | 0                                | 79,850                | 89,695     | 169,545        |
| Average Unit Size (NSF)                 | 855                  | 883                  | 865            | 987                         | 994                  | 989            | 0                                | 944                   | 974        | 958            |
| <b>PDR (GSF)</b>                        | 0                    | 0                    | 0              | 0                           | 0                    | 0              | 47,728                           | 0                     | 0          | 47,728         |
| PDR (LSF)                               | 0                    | 0                    | 0              | 0                           | 0                    | 0              | 46,957                           | 0                     | 0          | 46,957         |
| <b>Public Arts Activity Space (GSF)</b> | 0                    | 0                    | 0              | 0                           | 0                    | 0              | 9,524                            | 0                     | 0          | 9,524          |
| Public Arts Activity Space (LSF)        | 0                    | 0                    | 0              | 0                           | 0                    | 0              | 8,366                            | 0                     | 0          | 8,366          |
| <b>Retail (GSF)</b>                     | 20,318               | 4,650                | 24,968         | 18,379                      | 2,200                | 20,579         | 0                                | 14,484                | 7,265      | 21,749         |
| Retail (LSF)                            | 19,302               | 4,418                | 23,720         | 15,300                      | 1,580                | 16,880         | 0                                | 13,200                | 7,000      | 20,200         |
| <b>Parking Structure (GSF)</b>          | 104,475              | 48,328               | 152,803        | 95,103                      | 45,315               | 140,418        | 0                                | 0                     | 45,388     | 45,388         |
| Residential Parking Spaces              |                      |                      | 338            |                             |                      | 233            |                                  |                       |            | 121            |
| Car Share Spaces                        |                      |                      | 5              |                             |                      | 3              |                                  |                       |            | 2              |
| Commercial/ Loading Spaces              |                      |                      | 46             |                             |                      | 39             |                                  |                       |            | 3              |
| <b>Total Parking</b>                    |                      |                      | <b>389</b>     |                             |                      | <b>275</b>     |                                  |                       |            | <b>126</b>     |
| <b>Courtyard/Terrace Area (GSF)</b>     |                      |                      | <b>25,220</b>  |                             |                      | <b>33,900</b>  |                                  |                       |            | <b>25,212</b>  |

Source: City of San Francisco, Potrero Partners, Christiani Johnson Architects.

## 2. Development Costs

Development costs consist of the following key categories: land, hard construction costs, site improvements, government fees, financing and other soft costs (such as development costs from project design). Some of these development costs are driven primarily by the size of the development (such as hard construction costs) while others have a significant fixed-cost component (such as land costs).

Total apartment development costs are approximately \$280 million for the Proposed Project and \$260 million and \$190 million for the Reduced Density and Metal Shed Reuse Alternatives, respectively.

### a. Land Cost

Total land costs are estimated at \$38 million, or approximately \$250 per square foot, and are the same for the Proposed Project and both Alternatives.<sup>10</sup>

### b. Hard Construction Costs

Hard construction costs include direct construction costs related to building, parking and site work (including general contractor overhead, profit and general conditions). The hard construction costs for new construction are based on typical building construction costs for wood frame construction over below-grade parking for projects with residential heights of 48-68 feet, and are calculated based on the gross square feet (GSF) of building area for the applicable use: \$295/GSF for Residential, \$190/GSF for Retail, PDR and Arts space, and \$200/GSF for below-grade parking. Given the additional complexity associated with constructing a new building within existing structures and the need to retrofit the metal shed buildings to comply with current structural, life safety and energy efficiency requirements, the hard

<sup>10</sup> This is based on historical sales for similar properties in the Potrero and South of Market neighborhoods, adjusted to reflect the specific characteristics of the property.



construction costs for the metal shed buildings in the Metal Shed Reuse alternative are estimated to be 12% higher than new construction.<sup>11</sup>

### c. Demolition and Site Improvements

Site improvement costs consist of all of the costs needed to ready the site for development, including the demolition of existing structures, completion of the environmental remediation work and the provision of public and private pathways and landscaped areas of the project. As indicated in the project descriptions, the Proposed Project and Reduced Density Alternative include demolition of existing buildings and preparation of the site for development, estimated at \$4.7 million. The Metal Shed Reuse Alternative cost is lower (\$2.9 million) because the metal sheds would not be demolished. Courtyard/terrace area development and improvements are estimated at \$114/square foot.<sup>12</sup>

### d. Permits and Development Fees

The Project Sponsor will be required to pay City permits and development impact fees, including the Eastern Neighborhoods Infrastructure Impact Fee, Transportation Sustainability Fee, Child Care fees, water and wastewater capacity fees, school fees, building permit fees, large project authorization permit, and planning fees. The Project Sponsor and Seifel prepared a current cost estimate for permits and development fees based on the City's published fee schedules. Table 2 summarizes the estimates of the fees for each alternative.

**Table 2**  
**Supporting Calculations for Permit and Development Fees**  
**901 16th Street and 1200 17th Street Project**

|                             | <b>Proposed Project</b> | <b>Reduced Project</b> | <b>Reuse Alternative</b> |
|-----------------------------|-------------------------|------------------------|--------------------------|
| <b>Residential SF</b>       | 438,681                 | 400,628                | 245,518                  |
| Total Units                 | 395                     | 273                    | 177                      |
| <b>Non-Residential SF</b>   | 24,968                  | 20,579                 | 79,001                   |
| <b>Development Fees</b>     | \$0                     | \$0                    | \$0                      |
| Jobs Housing Linkage        | \$149,059               | \$122,857              | \$129,842                |
| Eastern Neighborhood        | \$2,889,776             | \$2,630,612            | \$1,647,031              |
| Childcare                   | \$802,786               | \$733,149              | \$449,298                |
| TSP                         | \$2,087,273             | \$1,863,726            | \$1,200,428              |
| Water                       | \$482,470               | \$482,470              | \$536,078                |
| Wastewater                  | \$1,641,643             | \$1,641,643            | \$1,824,047              |
| School fee                  | \$1,482,607             | \$1,353,230            | \$832,466                |
| Subtotal                    | <b>\$9,535,614</b>      | <b>\$8,827,687</b>     | <b>\$6,619,189</b>       |
| <b>Planning/Permit Fees</b> | \$1,265,115             | \$1,105,115            | \$1,265,115              |
| <b>Total Fees (2016)</b>    | <b>\$10,800,729</b>     | <b>\$9,932,802</b>     | <b>\$7,884,304</b>       |

<sup>11</sup> These hard construction cost estimates were developed in consultation with Webcor and the development team and are consistent with cost assumptions used by Seifel in recent work for the City of San Francisco for new construction projects that have significant excavation and foundation costs. The costs for new construction represent a blend of Type III and Type V construction based on the varying building heights. Webcor performed a site inspection of the metal shed buildings and indicated that hard construction costs for the buildings' reuse and redevelopment would be 12% to 15% higher per gross building square foot than new construction, given the additional construction complexity.

<sup>12</sup> The site improvement cost estimates were developed in consultation with Webcor.



### **e. Construction Financing**

Construction financing typically represents the major source of capital that pays for development costs during construction. The construction financing assumptions used in this analysis are based on discussions with construction lenders, and are intended to be generally representative of construction financing terms for similar projects over the near term.

The construction interest rate is assumed at 5.0% with a loan fee of 1% for all projects. The loan amount is based on a 60% loan to total development value at an average outstanding balance of 60% of development cost.<sup>13</sup>

The term of the construction loan is directly related to project timing, as the construction loan is the primary source of capital during the construction of the project. The Proposed Project is anticipated to have a 24-month construction period until the permanent equity financing will be in place. The same construction duration is also assumed for the Reduced Density and Metal Shed Reuse Alternatives. The absorption periods for all scenarios are based on an average absorption rate of 20 apartment units per month until the units are occupied, with a 6-month overlap with construction, as units could begin leasing ahead of construction completion.<sup>14</sup> The absorption time periods were used to estimate operating costs that must be capitalized until rental revenues begin.

### **f. Other Soft Costs**

Other soft costs include predevelopment land carry and other project costs, as well as all other indirect construction costs, such as architectural design, engineering, legal fees, marketing and other professional fees paid by the developer (excluding sales expenses for the City's transfer tax and brokerage fees for buyer representation and other transaction-related expenses, which are considered separately).

These other soft costs are calculated as a percentage of hard construction costs based on a review of pro formas and interviews with developers and real estate professionals. Other soft costs are assumed at 18% of hard construction costs.<sup>15</sup>

## **3. Revenues**

Revenues for the project come primarily from rental of the residential units, parking spaces and the commercial spaces, all of which vary between the Proposed Project and EIR Alternatives.

- Residential market rate units are assumed at an average rent of approximately \$4,760 per month for an 865-square-foot unit, inclusive of all other revenues that a landlord might receive, such as income from the use of laundry facilities and other apartment services.<sup>16</sup>
- Between 14.4% and 16% of the residential units are designated for rental at below-market rates as described above. Rents for BMR units are assumed at approximately \$1,200 per month for 55% AMI

<sup>13</sup> These terms are typical for a construction loan for development of this scale.

<sup>14</sup> The Proposed Project and Alternatives assume an absorption rate of 20 units/month for rental units and 15 units/month for condominiums, which is typical for rental or condominium projects of this size. The overlap period allows rentals or sales to begin, although move-in would not take place until completion of construction. A 6-month overlap period shortens the net absorption period for Alternatives, respectively.

<sup>15</sup> Based on recent work for the City of San Francisco, other soft costs for rental developments are typically 18% of hard construction costs, inclusive of predevelopment expenses.

<sup>16</sup> Based on \$66 per net square foot annually or about \$5.50 per net square foot per month. The monthly rental rate for the Alternatives is calculated based on their respective average unit sizes multiplied by this rental rate per square foot, which likely overstates rental income in the EIR Alternatives because developments with larger average unit sizes typically generate lower rents on a per square foot basis given renters' sensitivity to overall monthly rents, particularly for two- and three-bedroom units.



affordable units, which represent the average rents affordable to households at these income levels according to the City of San Francisco.<sup>17</sup>

- Monthly residential parking rates for market rate units are assumed to be \$350 per space, and parking rates for BMR units and commercial spaces are assumed at \$175 per space, consistent with comparable developments in the Eastern Neighborhoods. No revenue is assumed from car sharing spaces.
- The monthly rental rate for storage units is \$75.
- Retail space rental is assumed at a rate of \$5.25 per square foot per month (or \$63 per square foot annually) reflecting its visible location and design, and PDR and Public Arts spaces at \$2.50 per square foot per month (or \$30 per square foot annually), both on a triple net basis.

Revenues from residential units assume a 5% vacancy rate, while an 8% vacancy rate is assumed for commercial and PDR space rental given the project's location near UCSF and public transit.

#### **4. Sales Value and Expenses**

The potential value of the property is determined by applying a capitalization rate to the net annual income from the property using a 4.5% capitalization rate for residential, 5.5% capitalization for retail (reflecting its location and design) and 6% for other non-residential uses.<sup>18</sup>

Sales expenses include brokerage fees and City transfer taxes that are in addition to the marketing and sales costs included within soft costs. These expenses are deducted from the rental revenue proceeds in order to generate net development revenues for the financial analysis.

The analysis assumes sales expenses equal to 3.0% of sales price, representing an allowance of 0.5% for brokerage fees (reflecting the large size and value of the development) and 2.5% for San Francisco's transfer tax. Transfer taxes are based on the City's transfer tax schedule, which is calculated according to building value, and are assumed to be paid by the developer.

#### **5. Return Metrics**

Developers, lenders and investors evaluate and measure returns in several ways. Based on input from real estate developers, equity investors and lenders, development returns are based on two key measures typically used by the real estate community.

##### **a. Developer Margin and Margin on Cost**

Developer margin is equal to the difference between net development value and total development costs (before consideration of developer return or profit).<sup>19</sup> A developer will not proceed to build a project unless the project generates sufficient developer margin to warrant the risk and private investment needed to undertake the project.

Developers and investors use different target margin on cost thresholds depending on the level of complexity of the project, construction types, construction schedule, sales/rental absorption timeline and

---

<sup>17</sup> The BMR rents are based on the affordable rents, including utilities, published by the 2016 City of San Francisco Mayor's Office of Housing and Community Development for households at these income levels and assuming an average of BMR rents for one- and two-bedroom units.

<sup>18</sup> Capitalization rate assumptions are based on discussions with real estate professionals regarding current valuation assumptions for similar projects in San Francisco and on the capitalization rate survey by Integra Realty Resources (Viewpoint, 2015 Real Estate Value Trends).

<sup>19</sup> Net development value equals gross development value less transaction expenses.



potential equity sources. Projects with a greater number of units and longer timelines have higher risk and as a result require a higher margin on cost. This type and size of apartment development (wood frame construction averaging around 40 to 70 feet) would likely have a margin on cost threshold that ranges between 18 and 25%, as measured by developer margin divided by development cost.<sup>20</sup>

## **b. Yield on Cost**

Yield on cost (YOC) is used to evaluate development feasibility for apartment buildings.<sup>21</sup> YOC is measured based on Net Operating Income (NOI) divided by development costs.<sup>22</sup> NOI is equal to projected rental revenues less vacancy allowance less operating expenses.

The target yield on cost for apartments in San Francisco over the past decade has ranged from 5% to 7% based on a review of project pro formas and discussions with developers and equity investors. Currently, developers and investors are using a 5.5% to 6% threshold in San Francisco, which is considered to be a very desirable rental market, with 5.5% considered a minimum threshold.<sup>23</sup>

## **D. Financial Feasibility Findings**

The financial analysis compares the anticipated development costs with the potential revenues that could be generated by the Proposed Project and EIR Alternatives in order to test the overall financial feasibility using typical return measures of developer margin, margin on cost and yield on cost (YOC).

The summary financial pro forma analysis shown in Table 3A is based on the development assumptions previously described in Section C, and it compares the financial feasibility of the Proposed Project and EIR Alternatives. (Please refer to Appendix Table 1A for the development pro forma that was used to prepare the summary financial comparison.)

---

<sup>20</sup> This is equivalent to a return on cost threshold range of 15% to 20% when measured by return on net revenues.

<sup>21</sup> Also referred to as Return on Cost by real estate developers, lenders and investors.

<sup>22</sup> These return metrics are considered the typical “back of the envelope” way of determining real estate feasibility and are typically based on current rent and cost assumptions (not trended upward to reflect potential future increases).

<sup>23</sup> These YOC thresholds are consistent with the return thresholds used in the financial analysis on housing development performed for the San Francisco Mayor’s Office of Housing and Community Development. These are based on input from a Technical Advisory Committee and City staff, interviews with developers and real estate professionals and key assumptions from more than 40 development pro formas for projects constructed or in the development pipeline over the past decade.



**Table 3A**  
**Summary Financial Comparison**  
**901 16th Street and 1200 17th Street Project**

|  | <b>Proposed Project</b> | <b>Reduced Density Alternative</b> | <b>Metal Shed Reuse Alternative</b> |
|--|-------------------------|------------------------------------|-------------------------------------|
| <i>Proposed Project Type</i>                   | <i>Rental</i>           | <i>Rental</i>                      | <i>Rental</i>                       |
| <b>Mixed Use Development Program</b>           |                         |                                    |                                     |
| <b>Residential Units</b>                       | 395                     | 273                                | 177                                 |
| <i>Below Market Rate Units</i>                 | 61                      | 42                                 | 27                                  |
| <b>PDR (LSF)</b>                               | 0                       | 0                                  | 46,957                              |
| <b>Retail (LSF)</b>                            | 23,720                  | 16,880                             | 20,200                              |
| <b>Public Arts (LSF)</b>                       | 0                       | 0                                  | 8,366                               |
| <b>Parking Spaces</b>                          | 389                     | 275                                | 126                                 |
| <b>Total Development Value</b>                 | <b>\$327,870,000</b>    | <b>\$254,123,000</b>               | <b>\$190,090,000</b>                |
| Less: Development Cost                         | <b>\$278,687,000</b>    | <b>\$257,532,000</b>               | <b>\$190,553,000</b>                |
| <b>Return (Net Developer Margin)</b>           | <b>\$49,183,000</b>     | <b>(\$3,409,000)</b>               | <b>(\$463,000)</b>                  |
| <i>As Percent of Total Development Cost</i>    | <b>18%</b>              | <b>-1%</b>                         | <b>0%</b>                           |
| <b>Target Return on Total Development Cost</b> | <b>18% to 25%</b>       |                                    |                                     |
| <b>Return (Yield on Cost)</b>                  | <b>5.5%</b>             | <b>4.6%</b>                        | <b>4.9%</b>                         |
| <b>Target Return (Yield on Cost)</b>           | <b>5.5% to 6%</b>       |                                    |                                     |

Note: Refer to supporting tables for assumptions and calculations. Dollar values rounded to nearest \$1,000.

Source: City of San Francisco, Potrero Partners, Seifel Consulting Inc.

Based on the projected development revenues and costs described earlier, the apartment development financial analysis indicates that the Proposed Project is financially feasible while neither of the EIR Alternatives is financially feasible:

- The Proposed Project yields an 18% return (net developer margin) and a Yield on Cost of 5.5%, which is within the range of feasibility.
- The Reduced Density Alternative does not generate sufficient return or Yield on Cost, as total development costs exceed development value, resulting in a negative return, and the Yield on Cost is 4.6%—well below the target threshold.
- The Metal Shed Reuse Alternative also does not generate sufficient return or Yield on Cost, as total development costs exceed development value, resulting in a negative return and a Yield on Cost of 4.9%—well below the target threshold.<sup>24</sup>

As an additional check on the feasibility of the EIR Alternatives, both alternatives were modeled as for-sale, condominium developments with the same number of onsite affordable housing requirements. The condominium alternative analysis did not achieve a sufficient developer margin, well below the 18% to 25% target for margin on development costs.

<sup>24</sup> In reviewing the differences in costs and revenues on the Proposed Project vs. EIR Alternatives, Appendix Table 1A illustrates how revenues drop in direct proportion to the size of the development, while costs on a per unit basis increase in the EIR Alternatives.



A summary of the results of this financial comparison for potential condominium development of the EIR Alternatives is shown in Table 3B. (Please refer to the Appendix for a description of the financial analysis performed on these condominium alternatives. The Appendix summarizes the financial model assumptions that differ from those described in Section C above and presents the financial results.)

In comparison, the Proposed Project is feasible as it yields a positive developer margin, and its returns (as measured by margin on cost and YOC) are within the target return thresholds for development feasibility.

**Table 3B**  
**Summary Financial Comparison**  
**901 16th Street and 1200 17th Street Project**

|  | <b>Proposed Project</b> | <b>Reduced Density Alternative</b> | <b>Metal Shed Reuse Alternative</b> |
|--|-------------------------|------------------------------------|-------------------------------------|
| <i>Proposed Project Type</i>                   | <i>Rental</i>           | <i>Condominium</i>                 | <i>Condominium</i>                  |
| <b>Mixed Use Development Program</b>           |                         |                                    |                                     |
| <b>Residential Units</b>                       | 395                     | 273                                | 177                                 |
| <i>Below Market Rate Units</i>                 | 61                      | 42                                 | 27                                  |
| <b>PDR (LSF)</b>                               | 0                       | 0                                  | 46,957                              |
| <b>Retail (LSF)</b>                            | 23,720                  | 16,880                             | 20,200                              |
| <b>Public Arts (LSF)</b>                       | 0                       | 0                                  | 8,366                               |
| <b>Parking Spaces</b>                          | 389                     | 275                                | 126                                 |
| <b>Total Development Value</b>                 | <b>\$327,870,000</b>    | <b>\$294,794,000</b>               | <b>\$217,676,000</b>                |
| Less: Development Cost                         | <b>\$278,687,000</b>    | <b>\$278,186,000</b>               | <b>\$204,009,000</b>                |
| <b>Return (Net Developer Margin)</b>           | <b>\$49,183,000</b>     | <b>\$16,608,000</b>                | <b>\$13,667,000</b>                 |
| <i>As Percent of Total Development Cost</i>    | <b>18%</b>              | <b>6%</b>                          | <b>7%</b>                           |
| <b>Target Return on Total Development Cost</b> | <b>18% to 25%</b>       |                                    |                                     |
| <b>Return (Yield on Cost)</b>                  | <b>5.5%</b>             | <b>N/A</b>                         | <b>N/A</b>                          |
| <b>Target Return (Yield on Cost)</b>           | <b>5.5% to 6%</b>       |                                    |                                     |

Note: Refer to supporting tables for assumptions and calculations. Dollar values rounded to nearest \$1,000.

Source: City of San Francisco, Potrero Partners, Seifel Consulting Inc.



## E. Conclusion

This memorandum summarizes the results of an independent review of development assumptions and financial feasibility for both the Reduced Density Alternative and the Metal Shed Reuse Alternative as well as the Proposed Project, based on data provided by the Project Sponsor and on Seifel's recent work in San Francisco.

As this analysis clearly demonstrates, neither of the EIR Alternatives is a financially feasible alternative to the Proposed Project.<sup>25</sup> The development costs for the EIR Alternatives significantly exceed potential revenues as apartments, resulting in a negative developer margin or return. In addition, the EIR Alternatives do not meet either of the return thresholds as measured by either Yield on Cost or Margin on Cost. Furthermore, even if the EIR Alternatives were developed as condominiums, they still do not meet sufficient return thresholds in order to be financially feasible.

---

<sup>25</sup> Even when modeled as condominium projects, the EIR Alternatives do not achieve financial feasibility.



**Appendix Table 1A**  
**Proforma - Rental**  
**Financial Feasibility Analysis**  
**901 16th Street and 1200 17th Street Project**

|   | <b>Proposed Project</b> |                      | <b>Reduced Density Alternative</b> |                      | <b>Metal Shed Reuse Alternative</b> |                      |
|---|-------------------------|----------------------|------------------------------------|----------------------|-------------------------------------|----------------------|
| <i>Proposed Project Type</i>                        | <i>Rental</i>           |                      | <i>Rental</i>                      |                      | <i>Rental</i>                       |                      |
| <b>Development Value</b>                            | <b>Total</b>            | <b>Per Res. Unit</b> | <b>Total</b>                       | <b>Per Res. Unit</b> | <b>Total</b>                        | <b>Per Res. Unit</b> |
| <b>Residential - Rental</b>                         |                         |                      |                                    |                      |                                     |                      |
| Annual MR Rent Revenue                              | \$19,068,000            |                      | \$15,083,000                       |                      | \$9,483,000                         |                      |
| Annual BMR Rent Revenue                             | \$872,000               |                      | \$600,000                          |                      | \$386,000                           |                      |
| Other Revenue (Parking, Storage, etc.)              | \$1,713,000             |                      | \$1,195,000                        |                      | \$631,000                           |                      |
| Less Vacancy  | (\$1,083,000)           |                      | (\$844,000)                        |                      | (\$525,000)                         |                      |
| <u>Less Operating Expenses</u>                      | <u>(\$6,496,000)</u>    |                      | <u>(\$5,063,000)</u>               |                      | <u>(\$3,150,000)</u>                |                      |
| Net Revenues (NOI)                                  | \$14,074,000            |                      | \$10,971,000                       |                      | \$6,825,000                         |                      |
| Sales Value   | \$312,756,000           |                      | \$243,800,000                      |                      | \$151,667,000                       |                      |
| <u>Less Marketing Expense</u>                       | <u>(\$9,383,000)</u>    |                      | <u>(\$7,314,000)</u>               |                      | <u>(\$4,550,000)</u>                |                      |
| Net Proceeds  | <b>\$303,373,000</b>    | <b>\$768,000</b>     | <b>\$236,486,000</b>               | <b>\$866,000</b>     | <b>\$147,117,000</b>                | <b>\$831,000</b>     |
| <b>PDR</b>  |                         |                      |                                    |                      |                                     |                      |
| Annual Rent Revenue                                 | \$0                     |                      | \$0                                |                      | \$1,409,000                         |                      |
| Less Vacancy  | \$0                     |                      | \$0                                |                      | (\$113,000)                         |                      |
| <u>Less Operating Expenses</u>                      | <u>\$0</u>              |                      | <u>\$0</u>                         |                      | <u>(\$70,000)</u>                   |                      |
| Net Revenues (NOI)                                  | \$0                     |                      | \$0                                |                      | \$1,226,000                         |                      |
| Sales Value   | \$0                     |                      | \$0                                |                      | \$20,433,000                        |                      |
| <u>Less Marketing Expense</u>                       | <u>\$0</u>              |                      | <u>\$0</u>                         |                      | <u>(\$613,000)</u>                  |                      |
| Net Proceeds  | <b>\$0</b>              |                      | <b>\$0</b>                         |                      | <b>\$19,820,000</b>                 |                      |
| <b>Retail</b>                                       |                         |                      |                                    |                      |                                     |                      |
| Annual Rent Revenue                                 | \$1,494,000             |                      | \$1,063,000                        |                      | \$1,273,000                         |                      |
| Annual Commercial Parking Revenue                   | \$97,000                |                      | \$82,000                           |                      | \$6,000                             |                      |
| Less Vacancy  | (\$127,000)             |                      | (\$92,000)                         |                      | (\$102,000)                         |                      |
| <u>Less Operating Expenses</u>                      | <u>(\$75,000)</u>       |                      | <u>(\$53,000)</u>                  |                      | <u>(\$64,000)</u>                   |                      |
| Net Revenues (NOI)                                  | \$1,389,000             |                      | \$1,000,000                        |                      | \$1,113,000                         |                      |
| Sales Value   | \$25,255,000            |                      | \$18,182,000                       |                      | \$20,236,000                        |                      |
| <u>Less Marketing Expense</u>                       | <u>(\$758,000)</u>      |                      | <u>(\$545,000)</u>                 |                      | <u>(\$607,000)</u>                  |                      |
| Net Proceeds  | <b>\$24,497,000</b>     |                      | <b>\$17,637,000</b>                |                      | <b>\$19,629,000</b>                 |                      |
| <b>Arts Space</b>                                   |                         |                      |                                    |                      |                                     |                      |
| Annual Rent Revenue                                 | \$0                     |                      | \$0                                |                      | \$251,000                           |                      |
| Less Vacancy  | \$0                     |                      | \$0                                |                      | (\$20,000)                          |                      |
| <u>Less Operating Expenses</u>                      | <u>\$0</u>              |                      | <u>\$0</u>                         |                      | <u>(\$13,000)</u>                   |                      |
| Net Revenues (NOI)                                  | \$0                     |                      | \$0                                |                      | \$218,000                           |                      |
| Sales Value   | \$0                     |                      | \$0                                |                      | \$3,633,000                         |                      |
| <u>Less Marketing Expense</u>                       | <u>\$0</u>              |                      | <u>\$0</u>                         |                      | <u>(\$109,000)</u>                  |                      |
| Net Proceeds  | <b>\$0</b>              |                      | <b>\$0</b>                         |                      | <b>\$3,524,000</b>                  |                      |
| <b>Total Value</b>                                  | <b>\$327,870,000</b>    | <b>\$830,000</b>     | <b>\$254,123,000</b>               | <b>\$931,000</b>     | <b>\$190,090,000</b>                | <b>\$1,074,000</b>   |
| <b>Development Cost</b>                             |                         |                      |                                    |                      |                                     |                      |
| <b>Land Value</b>                                   | <b>\$38,000,000</b>     | <b>\$96,000</b>      | <b>\$38,000,000</b>                | <b>\$139,000</b>     | <b>\$38,000,000</b>                 | <b>\$215,000</b>     |
| <b>Hard Construction Costs</b>                      |                         |                      |                                    |                      |                                     |                      |
| Site Improvements <sup>a</sup>                      | \$7,587,000             |                      | \$8,577,000                        |                      | \$4,428,000                         |                      |
| Residential   | \$129,411,000           |                      | \$118,185,000                      |                      | \$77,327,000                        |                      |
| PDR   | \$0                     |                      | \$0                                |                      | \$10,166,000                        |                      |
| Retail  | \$4,744,000             |                      | \$3,910,000                        |                      | \$4,459,000                         |                      |
| Arts Space  | \$0                     |                      | \$0                                |                      | \$2,029,000                         |                      |
| Structured Parking                                  | \$30,561,000            |                      | \$28,084,000                       |                      | \$9,078,000                         |                      |
| Hard Cost Contingency                               | \$8,615,000             |                      | \$7,938,000                        |                      | \$5,374,000                         |                      |
| <b>Subtotal: Hard Construction Costs</b>            | <b>\$180,918,000</b>    | <b>\$458,000</b>     | <b>\$166,694,000</b>               | <b>\$611,000</b>     | <b>\$112,861,000</b>                | <b>\$638,000</b>     |
| <b>Tenant Improvements</b>                          |                         |                      |                                    |                      |                                     |                      |
| PDR   | \$0                     |                      | \$0                                |                      | \$2,348,000                         |                      |
| Retail  | \$1,898,000             |                      | \$1,350,000                        |                      | \$1,616,000                         |                      |
| <b>Subtotal: Tenant Improvement Costs</b>           | <b>\$1,898,000</b>      | <b>\$5,000</b>       | <b>\$1,350,000</b>                 | <b>\$5,000</b>       | <b>\$3,964,000</b>                  | <b>\$22,000</b>      |
| <b>Subtotal: Land and Direct Construction Costs</b> | <b>\$220,816,000</b>    | <b>\$559,000</b>     | <b>\$206,044,000</b>               | <b>\$755,000</b>     | <b>\$154,825,000</b>                | <b>\$875,000</b>     |
| <b>Development Soft Costs</b>                       |                         |                      |                                    |                      |                                     |                      |
| Permit and Development Fees                         | \$10,801,000            |                      | \$9,933,000                        |                      | \$7,884,000                         |                      |
| Construction Financing Costs                        | \$14,505,000            |                      | \$11,550,000                       |                      | \$7,529,000                         |                      |
| Other Soft Costs                                    | \$32,565,000            |                      | \$30,005,000                       |                      | \$20,315,000                        |                      |
| <b>Subtotal: Soft Costs</b>                         | <b>\$57,871,000</b>     | <b>\$147,000</b>     | <b>\$51,488,000</b>                | <b>\$189,000</b>     | <b>\$35,728,000</b>                 | <b>\$202,000</b>     |
| <b>Total Development Cost / Per Res Unit</b>        | <b>\$278,687,000</b>    | <b>\$706,000</b>     | <b>\$257,532,000</b>               | <b>\$943,000</b>     | <b>\$190,553,000</b>                | <b>\$1,077,000</b>   |
| <b>Developer Margin / Per Res Unit</b>              | <b>\$49,183,000</b>     | <b>\$125,000</b>     | <b>(\$3,409,000)</b>               | <b>(\$12,000)</b>    | <b>(\$463,000)</b>                  | <b>(\$3,000)</b>     |
| <b>As Percent of Total Development Cost</b>         | <b>18%</b>              |                      | <b>-1%</b>                         |                      | <b>0%</b>                           |                      |
| <b>Target Return on Total Development Cost</b>      | <b>18% to 25%</b>       |                      | <b>18% to 25%</b>                  |                      |                                     |                      |

Note: Values rounded to nearest 1,000

a. Includes costs of site work, demolition of existing buildings (if applicable), environmental remediation, pathways and landscaping of open spaces.

Source: City of San Francisco, Potrero Partners, Seifel Consulting Inc.



## Appendix: EIR Alternatives Modeled as Condominiums

As an additional check for financial feasibility of the lower-density EIR Alternatives, both were modeled as for-sale condominium developments, with BMR units modeled as for-sale units affordable to households at 90% AMI. These Condominium Alternatives do achieve sufficient developer margins and the margin on cost is well below the 18% to 25% target for return on development cost. The following describes the model assumptions that differ from those described in Section C above, and compares the results to the Proposed Project and EIR Alternatives.

### a. Development Program Assumptions

This scenario assumes the baseline required on-site affordable housing percentage of BMR units (42 units for Reduced Density and 27 units for Metal Shed Reuse) according to the requirements for this Eastern Neighborhoods Plan subarea. As a condominium for-sale project, these BMR units would be required to be affordable to households at 90% of AMI, and this is what is modeled. The gross square feet of both commercial and parking uses are unchanged from those shown in Table 1.

### b. Development Costs

#### Hard Construction Costs

Hard construction cost assumptions remain the same for the Condominium Alternative, except that \$20,000 per unit was added to reflect the cost of enhanced finish work that is generally needed on condominium properties in this size range.<sup>26</sup>

#### Construction Financing

The construction financing assumptions do not change from those described in Section C above. The same construction periods are also assumed, despite the potential additional time needed to complete condominium finishes. As condominiums take much longer to sell than apartments take to lease, a slower absorption rate (15 units per month) is assumed (typical of comparable new condominium buildings), overlapping construction for 6 months, for a net period (post-construction) of 18 months for the Reduced Density Alternative and 12 months for the Metal Shed Reuse Alternative to be fully sold.

#### Other Soft Costs

Other Soft Costs were increased for the Condominium Alternative by 7% (to a total of 25%) of Hard Construction Costs to account for the additional soft costs related to condominium insurance and for the substantial marketing and sales costs associated with condominiums.<sup>27</sup>

### c. Revenues

Revenues for the project come primarily from sale of the residential units, along with rental income from parking and the retail space. A price of \$1,200 per net square foot was assumed for the sale of market rate units, based on recent sales rates in the Potrero/Dogpatch neighborhood.<sup>28</sup> A price of approximately \$285,000 was used for BMR units sold.<sup>29</sup>

---

<sup>26</sup> Based on interviews with contractors, condominium finish costs are at least \$15,000 to \$20,000 higher per unit as compared to rental units.

<sup>27</sup> Based on recent work for the City of San Francisco, other soft costs for condominium developments are typically 25% of hard construction costs, inclusive of predevelopment expenses.

<sup>28</sup> Polaris Pacific Report, San Francisco, January 2016 San Francisco Report Download

<sup>29</sup> Per City of San Francisco MOH, affordable purchase price for 3-person household at 90% AMI (\$82,550), with 33% of income spent on housing expenses. Assumes an average of one- and two-bedroom units.



All other rental income remains the same as in the rental alternatives.

#### **d. Sales Value and Expenses**

For the sales value of the Condominium Alternative, the Sales/Marketing Expense rate was increased to 5.5% to reflect the additional cost of sales related to the broker costs for individual condominiums and for the property transfer tax, consistent with standard assumptions for San Francisco projects of this type.<sup>30</sup>

#### **e. Return Metrics**

##### **Developer Margin and Margin on Cost**

As described in Section C.5, developers and investors use different target margin on cost thresholds depending on the level of complexity of the project, construction types, construction schedule, sales/rental absorption timeline and potential equity sources. Projects with longer timelines have higher risk and as a result require a higher margin on cost. This type of condominium development (wood frame construction averaging 55 feet) would likely have a margin on cost threshold between 18% and 25% on development costs (developer margin/development cost) or 15% and 20% on net sales revenues (developer margin/net sales revenues).

##### **Yield on Cost**

Yield on cost (YOC) is used to evaluate development feasibility for apartment buildings, and not applicable to condominium projects.

## **2. Financial Feasibility Findings**

Appendix Table 1B presents financial pro forma of the EIR Alternatives modeled as for-sale condominiums. Despite the higher development value of the EIR Alternatives as condominiums, both Alternatives still do not yield sufficient Developer Margin, and Margin on Cost (as measured by developer margin on total development cost) is well below the target return threshold of 18% to 25% required to be financially feasible.<sup>31</sup>

In conclusion, this analysis demonstrates that neither the Reduced Density nor Metal Shed Reuse EIR Alternative is feasible as either an apartment or condominium development.

---

<sup>30</sup> Based on information gathered by Seifel Consulting from developers and real estate professionals during Seifel's work for the City of San Francisco, as described in Section C.

<sup>31</sup> In addition, the Margin on Net Sales Revenue is also significantly below a developer threshold of 15% to 20%. It is also likely that for a relatively complex condominium project, with a long timeframe from predevelopment through to occupancy, the threshold could be 20% or higher.



**Appendix Table 1B**  
**Proforma - Condominium**  
**Financial Feasibility Analysis**  
**901 16th Street and 1200 17th Street Project**

|   | Reduced Density Alternative |                    | Metal Shed Reuse Alternative |                    |
|---|-----------------------------|--------------------|------------------------------|--------------------|
| Proposed Project Type                               | Condominium                 |                    | Condominium                  |                    |
| Development Value                                   | Total                       | Per Res. Unit      | Total                        | Per Res. Unit      |
| <b>Residential - For Sale</b>                       |                             |                    |                              |                    |
| MR Sales Proceeds                                   | \$274,240,000               |                    | \$172,419,000                |                    |
| AH Sales Proceeds                                   | \$11,958,000                |                    | \$7,687,000                  |                    |
| Total Sales Proceeds                                | \$286,198,000               |                    | \$180,106,000                |                    |
| Less Sales Expense                                  | (\$8,586,000)               |                    | (\$5,403,000)                |                    |
| Net Proceeds  | \$277,612,000               | \$1,017,000        | \$174,703,000                | \$987,000          |
| <b>PDR</b>  |                             |                    |                              |                    |
| Annual Rent Revenue                                 | \$0                         |                    | \$1,409,000                  |                    |
| Less Vacancy  | \$0                         |                    | (\$113,000)                  |                    |
| Less Operating Expenses                             | \$0                         |                    | (\$70,000)                   |                    |
| Net Revenues (NOI)                                  | \$0                         |                    | \$1,226,000                  |                    |
| Sales Value   | \$0                         |                    | \$20,433,000                 |                    |
| Less Marketing Expense                              | \$0                         |                    | (\$613,000)                  |                    |
| Net Proceeds  | \$0                         |                    | \$19,820,000                 |                    |
| <b>Retail</b>                                       |                             |                    |                              |                    |
| Annual Rent Revenue                                 | \$1,063,000                 |                    | \$1,273,000                  |                    |
| Annual Commercial Parking Revenue                   | \$82,000                    |                    | \$6,000                      |                    |
| Less Vacancy  | (\$92,000)                  |                    | (\$102,000)                  |                    |
| Less Operating Expenses                             | (\$53,000)                  |                    | (\$64,000)                   |                    |
| Net Revenues (NOI)                                  | \$1,000,000                 |                    | \$1,113,000                  |                    |
| Sales Value   | \$18,182,000                |                    | \$20,236,000                 |                    |
| Less Marketing Expense                              | (\$1,000,000)               |                    | (\$607,000)                  |                    |
| Net Proceeds  | \$17,182,000                |                    | \$19,629,000                 |                    |
| <b>Arts Space</b>                                   |                             |                    |                              |                    |
| Annual Rent Revenue                                 | \$0                         |                    | \$251,000                    |                    |
| Less Vacancy  | \$0                         |                    | (\$20,000)                   |                    |
| Less Operating Expenses                             | \$0                         |                    | (\$13,000)                   |                    |
| Net Revenues (NOI)                                  | \$0                         |                    | \$218,000                    |                    |
| Sales Value   | \$0                         |                    | \$3,633,000                  |                    |
| Less Marketing Expense                              | \$0                         |                    | (\$109,000)                  |                    |
| Net Proceeds  | \$0                         |                    | \$3,524,000                  |                    |
| <b>Total Value</b>                                  | <b>\$294,794,000</b>        | <b>\$1,080,000</b> | <b>\$217,676,000</b>         | <b>\$1,230,000</b> |
| <b>Development Cost</b>                             |                             |                    |                              |                    |
| <b>Land Value</b>                                   | <b>\$38,000,000</b>         | <b>\$139,000</b>   | <b>\$38,000,000</b>          | <b>\$215,000</b>   |
| <b>Hard Construction Costs</b>                      |                             |                    |                              |                    |
| Site Improvements <sup>a</sup>                      | \$8,577,000                 |                    | \$4,428,000                  |                    |
| Residential   | \$123,645,000               |                    | \$80,867,000                 |                    |
| PDR   | \$0                         |                    | \$10,166,000                 |                    |
| Retail  | \$3,910,000                 |                    | \$4,459,000                  |                    |
| Arts Space  | \$0                         |                    | \$2,029,000                  |                    |
| Structured Parking                                  | \$28,084,000                |                    | \$9,078,000                  |                    |
| Hard Cost Contingency                               | \$8,211,000                 |                    | \$5,551,000                  |                    |
| <b>Subtotal: Hard Construction Costs</b>            | <b>\$172,427,000</b>        | <b>\$632,000</b>   | <b>\$116,578,000</b>         | <b>\$659,000</b>   |
| <b>Tenant Improvements</b>                          |                             |                    |                              |                    |
| PDR   | \$0                         |                    | \$2,348,000                  |                    |
| Retail  | \$1,350,000                 |                    | \$1,616,000                  |                    |
| <b>Subtotal: Tenant Improvement Costs</b>           | <b>\$1,350,000</b>          | <b>\$5,000</b>     | <b>\$3,964,000</b>           | <b>\$22,000</b>    |
| <b>Subtotal: Land and Direct Construction Costs</b> | <b>\$211,777,000</b>        | <b>\$776,000</b>   | <b>\$158,542,000</b>         | <b>\$896,000</b>   |
| <b>Development Soft Costs</b>                       |                             |                    |                              |                    |
| Permit and Development Fees                         | \$9,933,000                 |                    | \$7,884,000                  |                    |
| Construction Financing Costs                        | \$13,369,000                |                    | \$8,438,000                  |                    |
| Other Soft Costs                                    | \$43,107,000                |                    | \$29,145,000                 |                    |
| <b>Subtotal: Soft Costs</b>                         | <b>\$66,409,000</b>         | <b>\$243,000</b>   | <b>\$45,467,000</b>          | <b>\$257,000</b>   |
| <b>Total Development Cost / Per Res Unit</b>        | <b>\$278,186,000</b>        | <b>\$1,019,000</b> | <b>\$204,009,000</b>         | <b>\$1,153,000</b> |
| <b>Developer Margin / Per Res Unit</b>              | <b>\$16,608,000</b>         | <b>\$61,000</b>    | <b>\$13,667,000</b>          | <b>\$77,000</b>    |
| <b>As Percent of Total Development Cost</b>         | <b>6%</b>                   |                    | <b>7%</b>                    |                    |
| <b>Target Return on Total Development Cost</b>      | <b>18% to 25%</b>           |                    |                              |                    |

Note: Values rounded to nearest 1,000

a. Includes costs of site work, demolition of existing buildings (if applicable), environmental remediation, pathways and landscaping of open spaces.

Source: City of San Francisco, Potrero Partners, Seifel Consulting Inc.



**Townes, Chris (CPC)**

---

**From:** Josh Smith <jsmith@waldendevlopment.com>  
**Sent:** Thursday, May 05, 2016 11:40 AM  
**To:** Townes, Chris (CPC)  
**Cc:** Steve Vettel  
**Subject:** Quick Note Re: Increase in Number of 3BR units

Chris--

In response to requests from some members of the community that we modify the unit mix of the proposed project to make it even more "family-friendly" than it currently is, we have recently agreed to increase the number of 3-bedroom/2 full bath units in the project to 22 (vs. 14 that are shown on the current plan).

The total number of units would remain unchanged.

Can you include in the Commissioner's packet a note informing them of this recent change (or simply include a copy of this e-mail in the packet)?

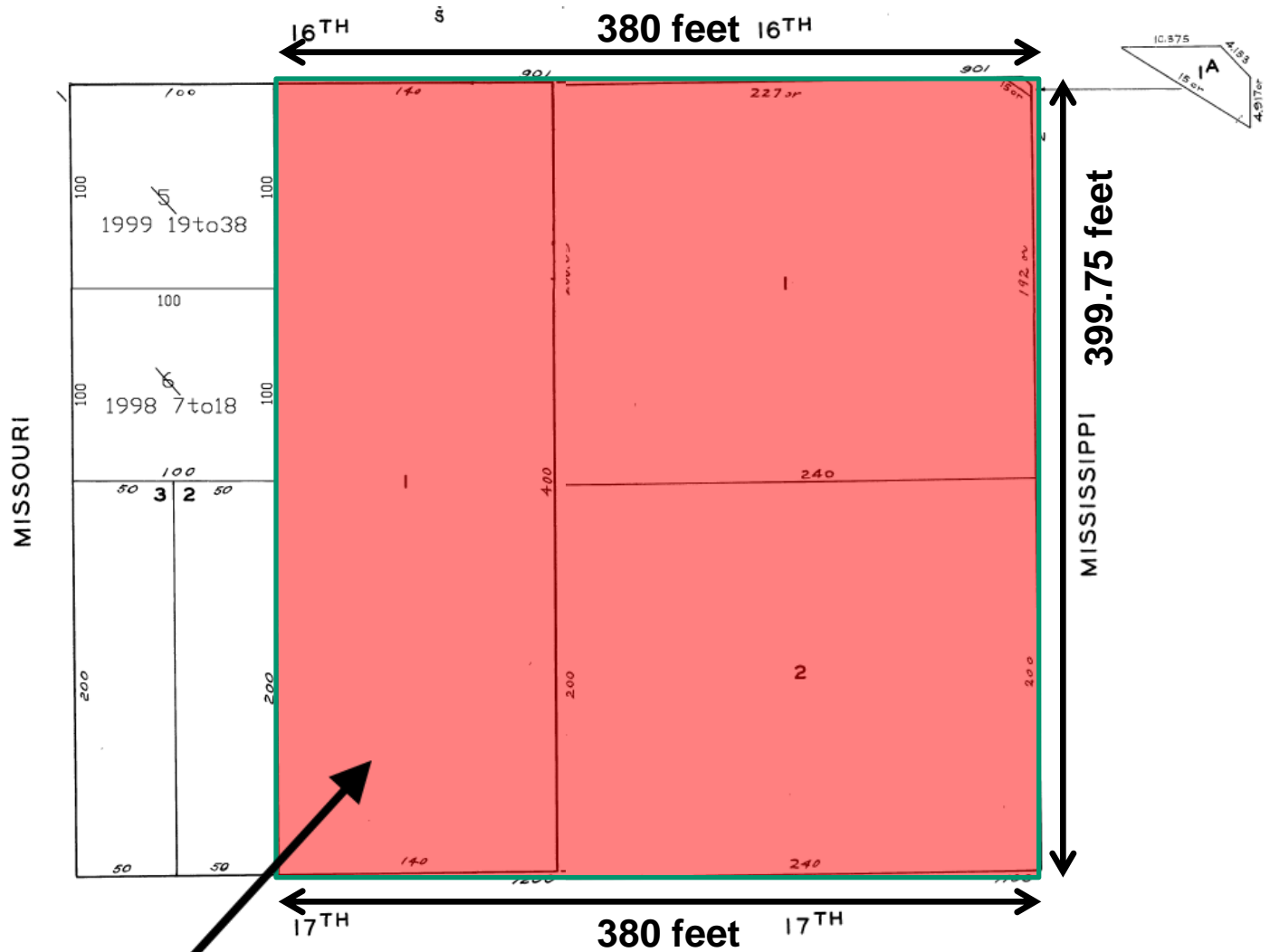
Thanks very much,

Josh

Josh Smith  
Potrero Partners/Walden Development  
415-516-5153



# Parcel Map

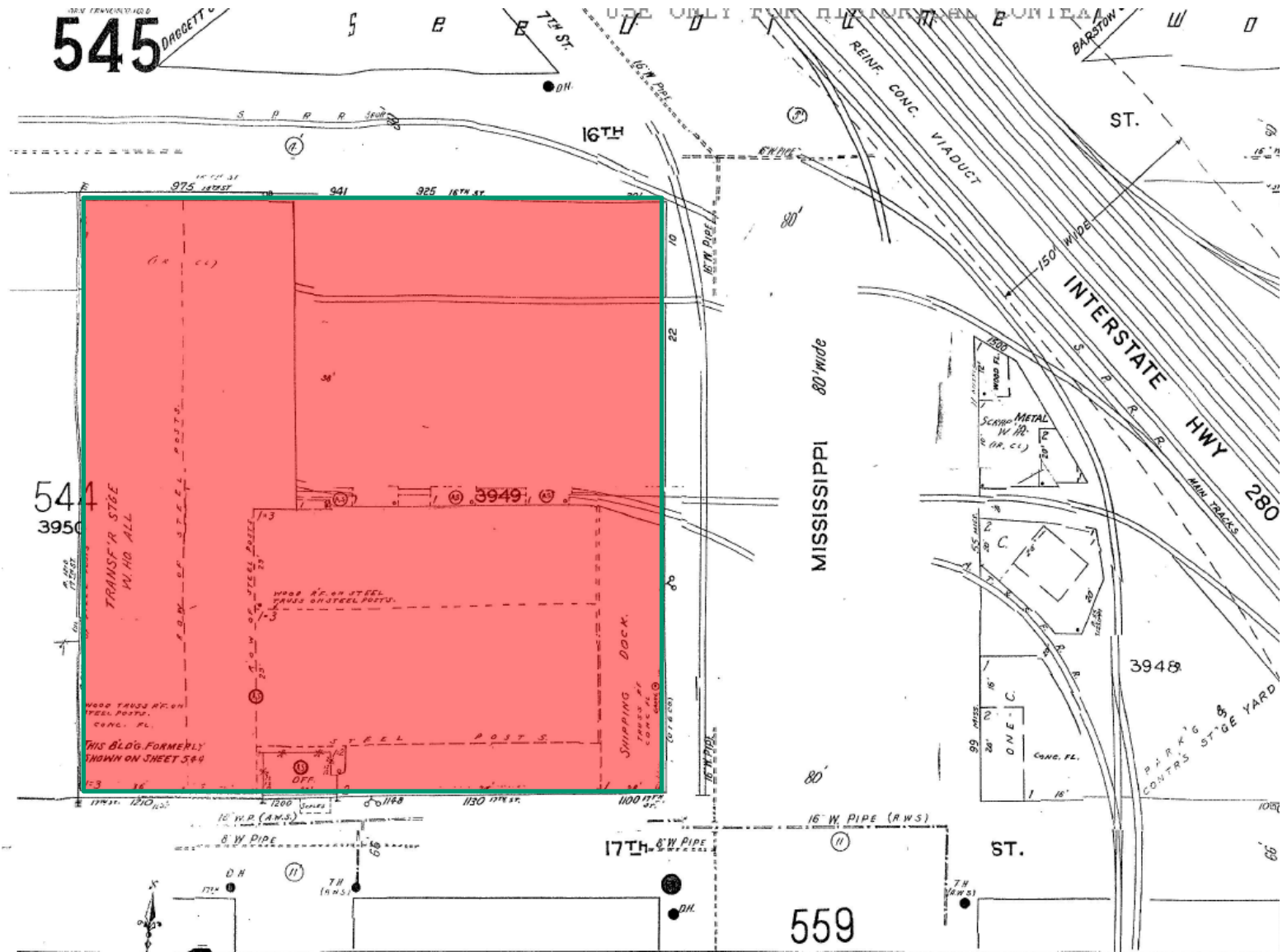


**SUBJECT PROPERTY**





# Sanborn Map\*

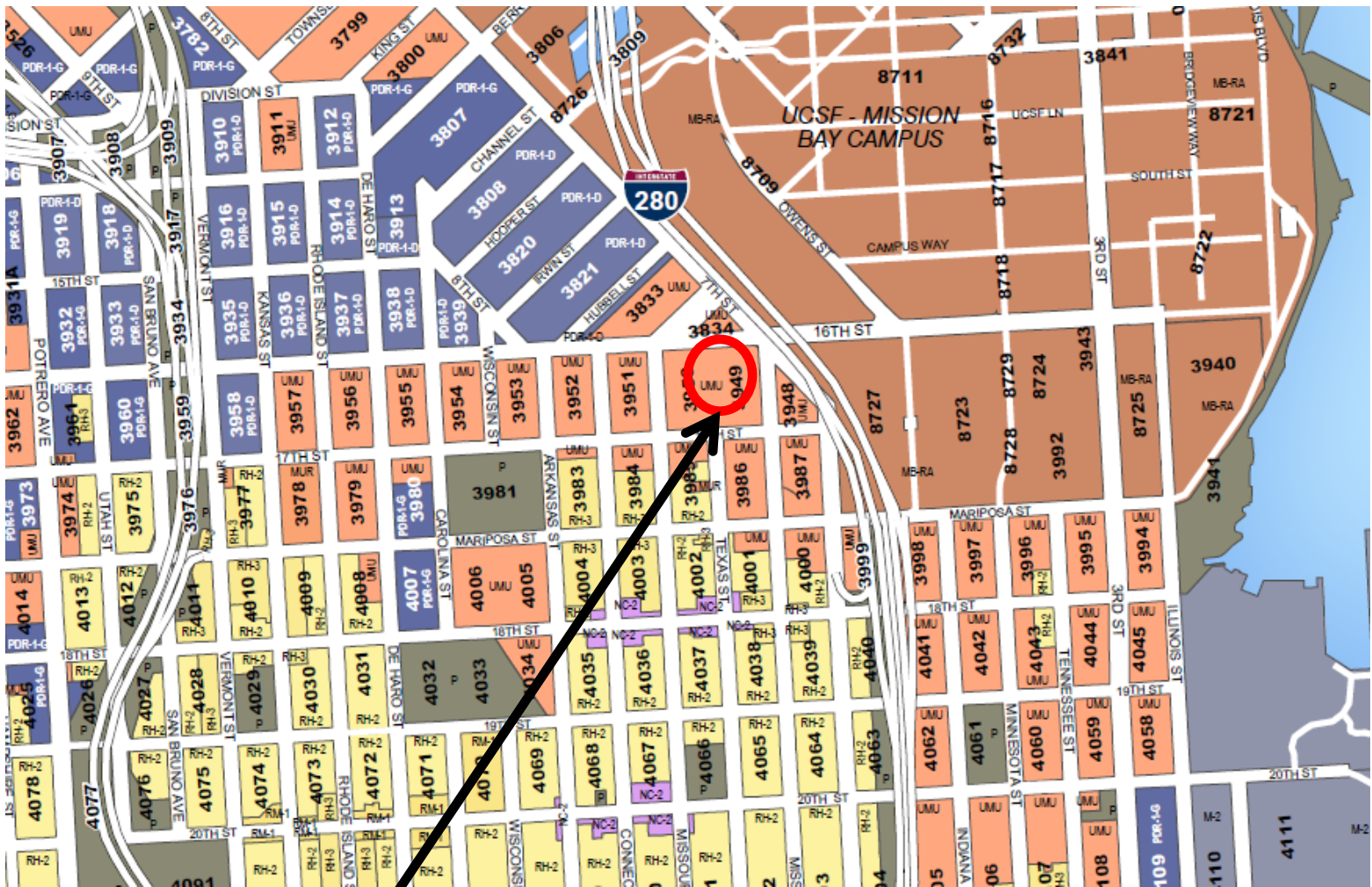


\*The Sanborn Maps in San Francisco have not been updated since 1998, and this map may not accurately reflect existing conditions.





# Zoning Map

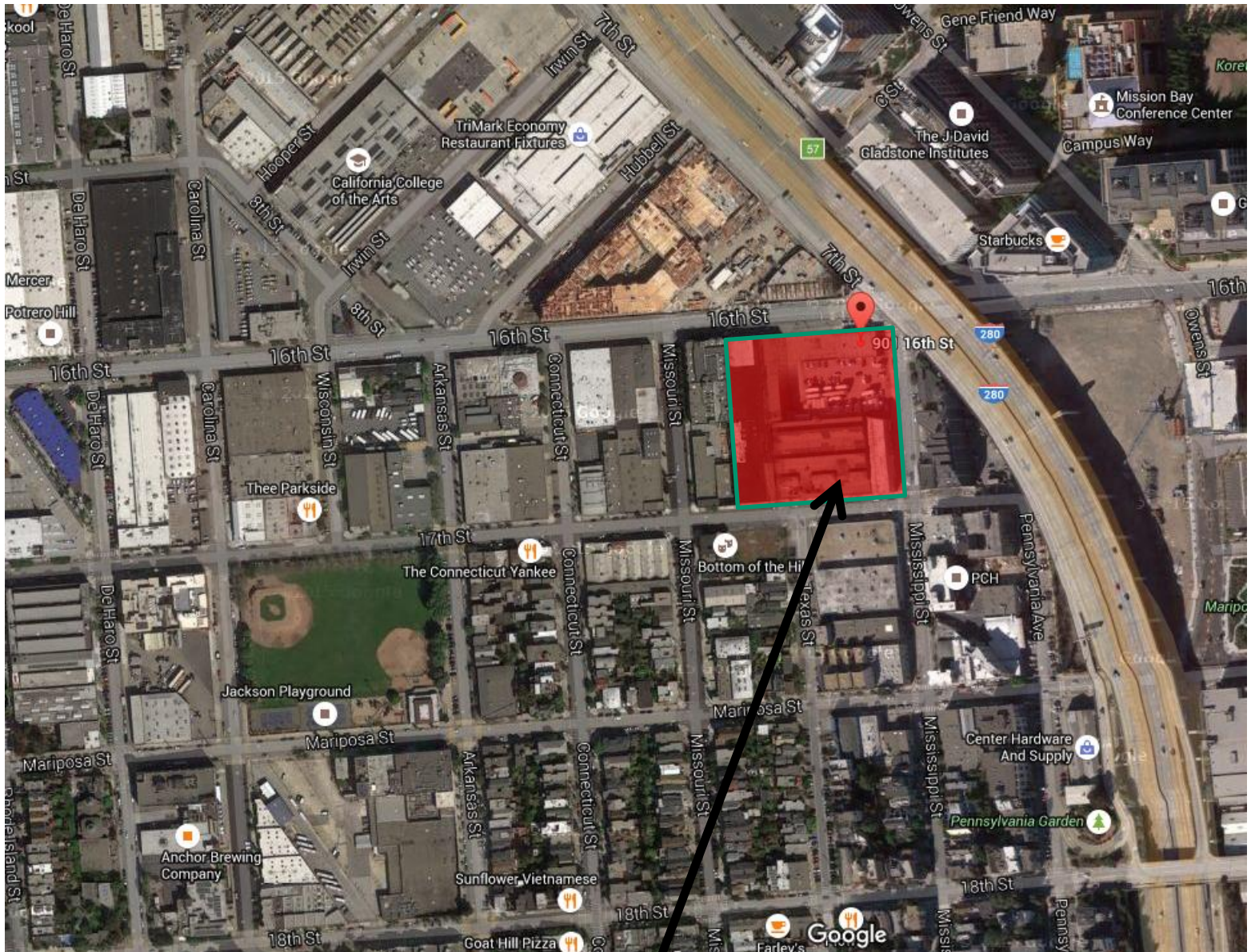


SUBJECT PROPERTY





# Aerial Photo



**PROJECT SITE**





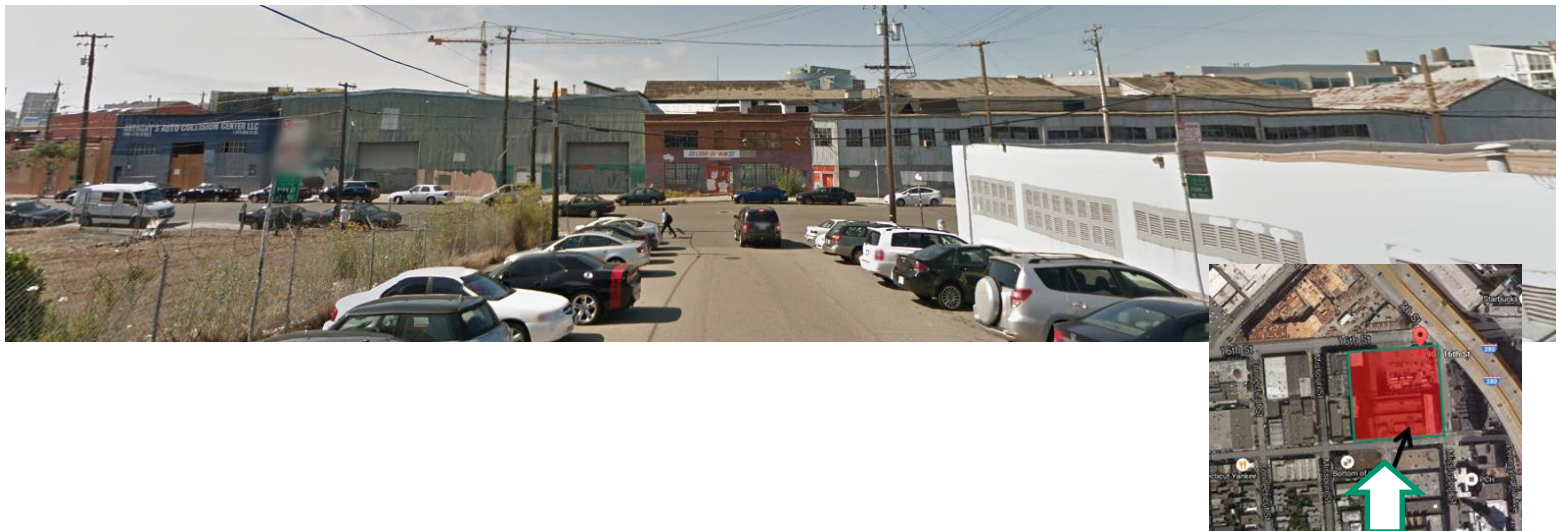
# Site Photo



Large Project Authorization  
Case Number 2011.1300X  
901 16<sup>th</sup> Street & 1200 17<sup>th</sup> Street



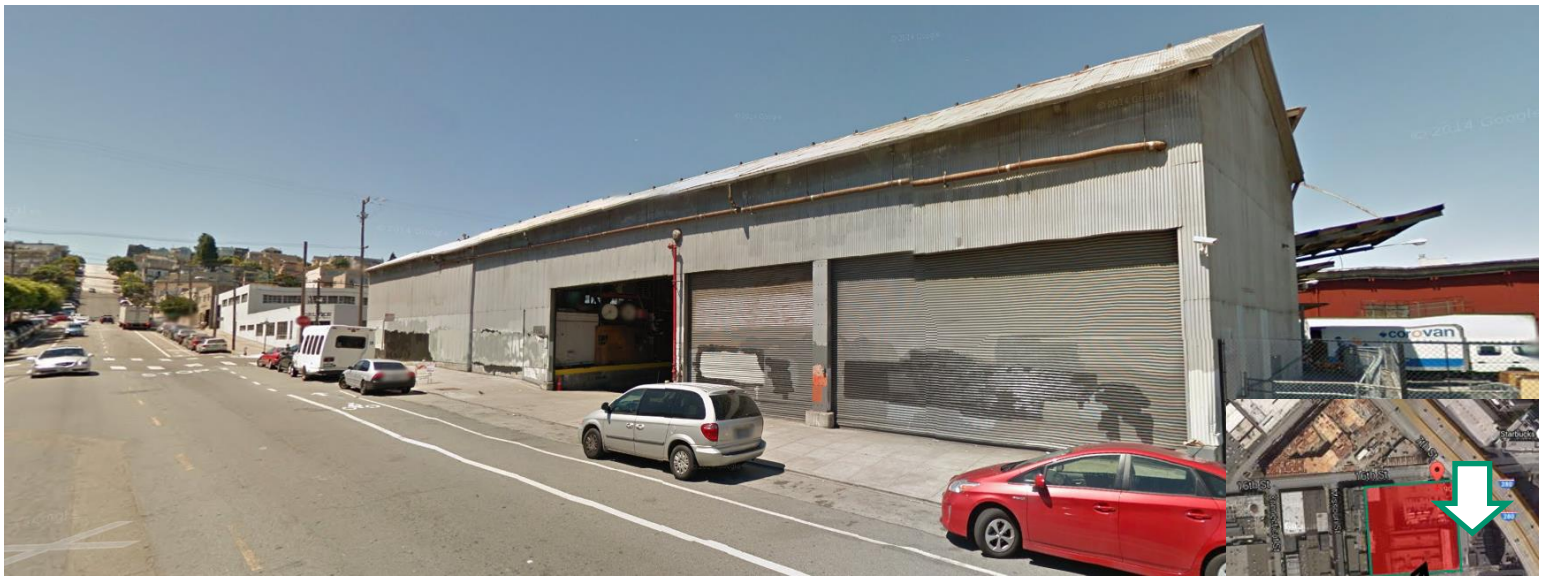
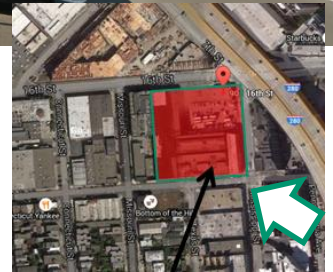
# Site Photo



Large Project Authorization  
Case Number 2011.1300X  
901 16<sup>th</sup> Street & 1200 17<sup>th</sup> Street



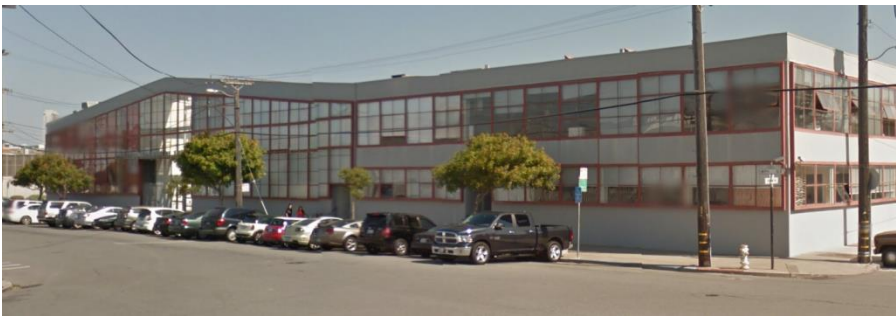
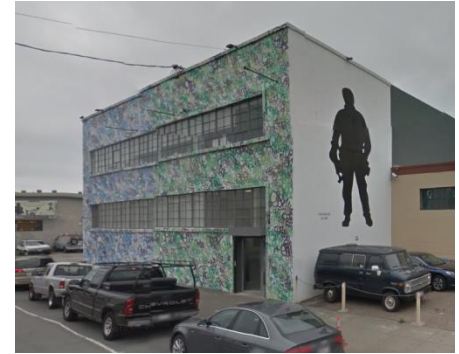
# Site Photo



Large Project Authorization  
Case Number 2011.1300X  
901 16<sup>th</sup> Street & 1200 17<sup>th</sup> Street



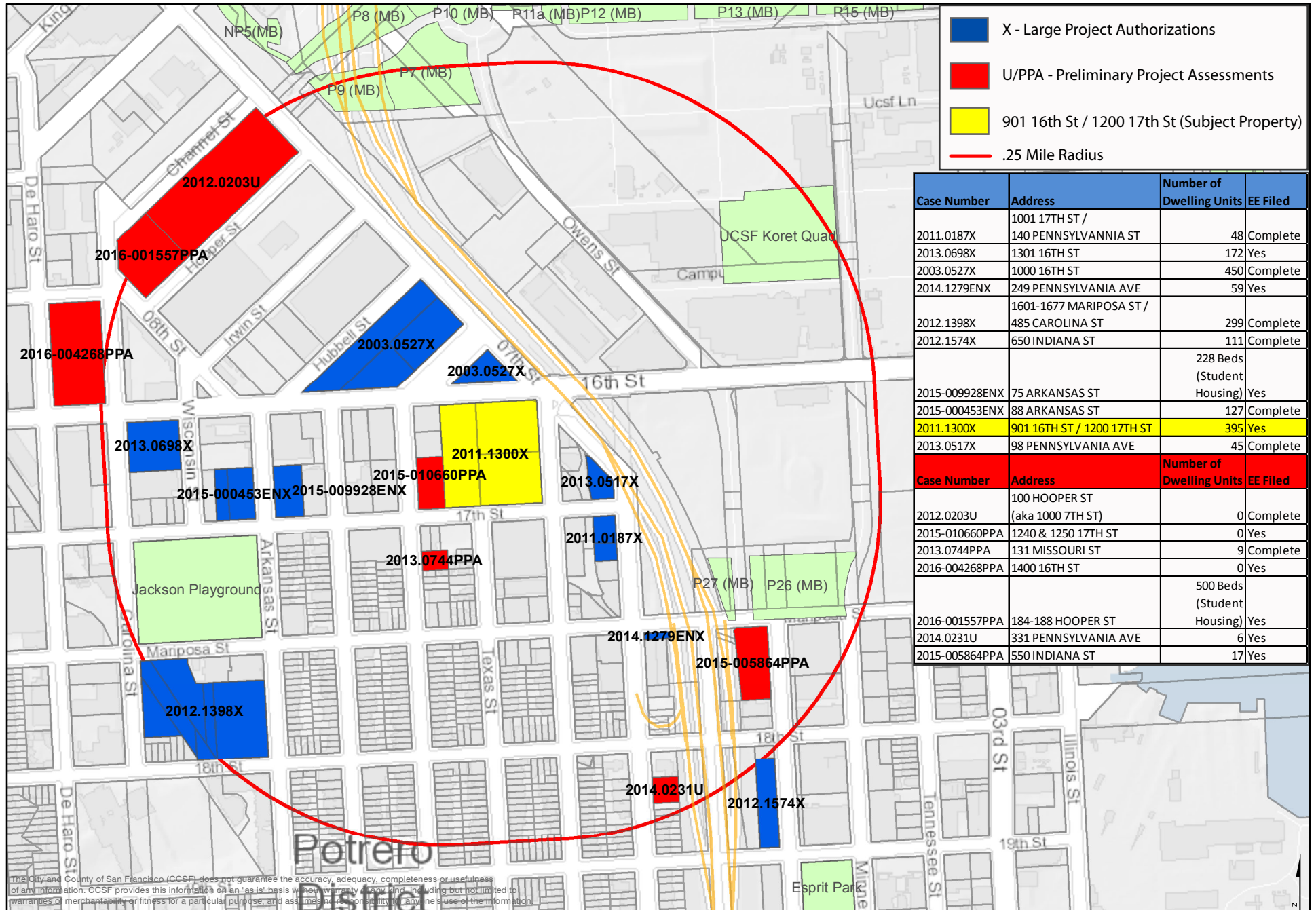
# 16<sup>th</sup> Street Corridor Buildings (Existing and Proposed)



Large Project Authorization  
Case Number 2011.1300X  
901 16<sup>th</sup> Street & 1200 17<sup>th</sup> Street



# Major Projects within .25 Mile Radius of 901 16th Street / 1200 17th Street



The City and County of San Francisco (CCSF) does not guarantee the accuracy, adequacy, completeness or usefulness of any information. CCSF provides this information on an "as is" basis without warranty of any kind, including but not limited to warranties of merchantability or fitness for a particular purpose, and assumes no responsibility for anyone's use of the information.

0 215 430 860 Feet



## Townes, Chris (CPC)

---

**From:** Kane, Jocelyn (ADM)  
**Sent:** Wednesday, March 02, 2016 11:53 AM  
**To:** Townes, Chris (CPC); Josh Smith (jsmith@waldendevlopment.com); Steven Vettel; Eric B. Mori  
**Cc:** Thomas, Christopher (CPC); VanHouten, Ben (ECN); lynn@bottomofthehill.com  
**Subject:** 901 16th St & 1200 17th St  
**Attachments:** RECOMMENDED NOISE ATTENUATION CONDITIONS FOR CHAPTER 116 RESIDENTIAL PROJECTS.pdf

The Entertainment Commission received notification of the proposed residential project at **901 16<sup>th</sup> St. & 1200 17<sup>th</sup> St.** The project is located within 300 radial feet of a Place of Entertainment ("POE") and is subject to Chapter 116 of the Administrative Code.

In accordance with the Entertainment Commission's approved "*Guidelines for Entertainment Commission Review of Residential Development Proposals Under Administrative Code Chapter 116*," Entertainment Commission staff determined that a hearing on this project was required under Section 116.7(c) of the Administrative Code.

This hearing was held on February 2, 2016, and the Entertainment Commission made a motion to recommend the standard "*Recommended Noise Attenuation Conditions for Chapter 116 Projects*," (see attached.) The Commission recommends that the Planning Department and/or Department of Building Inspection adopt these standard recommendations into the development permit(s) for this project.

In addition, recommendations also included the following:

- 1) Project sponsor shall upgrade their sound study from May 2013 to consider the impact of dbC on the development that might be emitted during performances at The Bottom of the Hill, a permitted place of entertainment. Sponsor shall contact Bottom of the Hill to insure that readings are taken on nights that have potential impact.
- 2) Project sponsor shall mitigate any impact to the Bottom of the Hill, including parking for large vehicles associated with performances at the venue. Project shall not at any time block entrances or exits to the venue.

Thanks very much.

Jocelyn Kane, Executive Director  
San Francisco Entertainment Commission  
City Hall, Room 453  
415 554-5793 (voice)  
415 554-7934 (fax)  
[jocelyn.kane@sfgov.org](mailto:jocelyn.kane@sfgov.org)  
[facebook](#) [blog](#)





## **RECOMMENDED NOISE ATTENUATION CONDITIONS FOR CHAPTER 116 RESIDENTIAL PROJECTS:**

- Community Outreach: Project sponsor shall include in its community outreach process any businesses located within 300 feet of the proposed project that operate between the hours of 9PM-5AM. Notice shall be made in person, written or electronic form.
- Sound Study: Project sponsor shall conduct an acoustical sound study, which shall include sound readings taken when performances are taking place at the proximate Places of Entertainment, as well as when patrons arrive and leave these locations at closing time. Readings should be taken at locations that most accurately capture sound from the Place of Entertainment to best of their ability. Any recommendation(s) in the sound study regarding window glaze ratings and soundproofing materials including but not limited to walls, doors, roofing, etc. shall be given highest consideration by the project sponsor when designing and building the project.
- Design Considerations:
  - (1) During design phase, project sponsor shall consider the entrance and egress location and paths of travel at the Place(s) of Entertainment in designing the location of (a) any entrance/egress for the residential building and (b) any parking garage in the building.
  - (2) In designing doors, windows, and other openings for the residential building, project sponsor should consider the POE's operations and noise during all hours of the day and night.
- Construction Impacts: Project sponsor shall communicate with adjacent or nearby Place(s) of Entertainment as to the construction schedule, daytime and nighttime, and consider how this schedule and any storage of construction materials may impact the POE operations.
- Communication: Project Sponsor shall make a cell phone number available to Place(s) of Entertainment management during all phases of development through construction. In addition, a line of communication should be created to ongoing building management throughout the occupation phase and beyond.





SAN FRANCISCO  
PLANNING  
DEPARTMENT

Planning Department  
1650 Mission Street  
Suite 400  
San Francisco, CA  
94103-9425

T: 415.558.6378  
F: 415.558.6409

## AFFIDAVIT FOR Compliance with the Inclusionary Affordable Housing Program

Date: January 11, 2013

To: Applicants subject to Planning Code Section 415: Inclusionary  
Affordable Housing Program

From: San Francisco Planning Department

Re: Compliance with the Inclusionary Affordable Housing Program

All projects that involve ten or more new dwelling units must participate in the *Inclusionary Affordable Housing Program* contained in Section 415 of the Planning Code. Every project subject to Section 415 must pay an Affordable Housing Fee that is equivalent to the applicable percentage of the number of units in the principal project, which is 20% of the total number of units proposed (or the applicable percentage if subject to different area plan controls or requirements).

A project may be eligible for an Alternative to the Affordable Housing Fee if the developer chooses to commit to sell the new on- or off-residential units rather than offer them as rental units. Second, the project may be eligible for an Alternative to the Affordable Housing Fee if it has demonstrated to the Planning Department that the affordable units are not subject to the Costa Hawkins Rental Housing Act. All projects that can demonstrate that they are eligible for an alternative to the Affordable Housing Fee must provide the necessary documentation to the Planning Department and the Mayor's Office of Housing. Additional material may be required to determine if a project is eligible to fulfill the Program's requirements through an alternative.

Before the Planning Department and/or Planning Commission can act on the project, this *Affidavit for Compliance with the Inclusionary Affordable Housing Program* must be completed.

1 California Civil Code Section 1954.50 et al.



Affidavit for Compliance with the Inclusionary Affordable Housing Program: Planning Code Section 415

Date April 28, 2016

I, Josh Smith, do hereby declare as follows:

a. The subject property is located at (address and block/lot):

901 16<sup>th</sup> Street and 1200 17<sup>th</sup> Street  
Address

3949/001, 001A, 002  
3950/001  
Block / Lot

b. The proposed project at the above address is subject to the Inclusionary Affordable Housing Program, Planning Code Section 415 et seq.

The Planning Case Number and/or Building Permit Number is:

2011.1300X  
Planning Case Number

Building Permit Number

This project requires the following approval:

- ☒ Planning Commission approval (e.g. Conditional Use Authorization, Large Project Authorization)  
☐ This project is principally permitted.

The Current Planner assigned to my project within the Planning Department is:

Chris Townes  
Planner Name

Is this project within the Eastern Neighborhoods Plan Area?

- ☒ Yes (if yes, please indicate Tier) Showplace Square/Potrero  
☐ No

This project is exempt from the Inclusionary Affordable Housing Program because:

- ☐ This project is 100% affordable.

c. This project will comply with the Inclusionary Affordable Housing Program by:

- ☒ Payment of the Affordable Housing Fee prior to the first site or building permit issuance (Planning Code Section 415.5).  
☒ On-site or Off-site Affordable Housing Alternative (Planning Code Sections 415.6 and 416.7).



Affidavit for Compliance with the Inclusionary Affordable Housing Program

d. If the project will comply with the Inclusionary Affordable Housing Program through an On-site or Off-site Affordable Housing Alternative, please fill out the following regarding how the project is eligible for an alternative and the accompanying unit mix tables on page 4.

☐ Ownership. All affordable housing units will be sold as ownership units and will remain as ownership units for the life of the project.

☒ Rental. Exemption from Costa Hawkins Rental Housing Act.<sup>2</sup> The Project Sponsor has demonstrated to the Department that the affordable units are not subject to the Costa Hawkins Rental Housing Act, under the exception provided in Civil Code Sections 1954.50 through one of the following:

☐ Direct financial contribution from a public entity.

☒ Development or density bonus or other public form of assistance.

☐ Development Agreement with the City. The Project Sponsor has entered into or has applied to enter into a Development Agreement with the City and County of San Francisco pursuant to Chapter 56 of the San Francisco Administrative Code and, as part of that Agreement, is receiving a direct financial contribution, development or density bonus, or other form of public assistance.

e. The Project Sponsor acknowledges that failure to sell the affordable units as ownership units or to eliminate the on-site or off-site affordable ownership-only units at any time will require the Project Sponsor to:

- (1) Inform the Planning Department and the Mayor's Office of Housing and, if applicable, fill out a new affidavit;
- (2) Record a new Notice of Special Restrictions; and
- (3) Pay the Affordable Housing Fee plus applicable interest (using the fee schedule in place at the time that the units are converted from ownership to rental units) and any applicable penalties by law.

f. The Project Sponsor must pay the Affordable Housing Fee in full sum to the Development Fee Collection Unit at the Department of Building Inspection for use by the Mayor's Office of Housing prior to the issuance of the first construction document, with an option for the Project Sponsor to defer a portion of the payment to prior to issuance of the first certificate of occupancy upon agreeing to pay a deferral surcharge that would be deposited into the Citywide Affordable Housing Fund in accordance with Section 107A.13.3 of the San Francisco Building Code.

g. I am a duly authorized officer or owner of the subject property.

I declare under penalty of perjury under the laws of the State of California that the foregoing is true and correct.  
Executed on this day in:

San Francisco, CA  
Location

4/28/16  
Date

Signature

Josh Smith  
Name (Print), Title

(650) 348-3232  
Contact Phone Number

cc: Mayor's Office of Housing  
Planning Department Case Docket  
Historic File, if applicable  
Assessor's Office, if applicable



## Unit Mix Tables

| NUMBER OF ALL UNITS IN PRINCIPAL PROJECT |     |         |                   |                   |                     |
|--|-----|---------|-------------------|-------------------|---------------------|
| Total Number of Units                    | SRO | Studios | One-Bedroom Units | Two-Bedroom Units | Three-Bedroom Units |
| 395                                      |     | 53      | 182               | 146               | 14                  |

If you selected an On-site or Off-Site Alternative, please fill out the applicable section below:

- ☐ On-site Affordable Housing Alternative (Charter Section 16.110 (g) and Planning Code Section 415.6): calculated at 12% of the unit total.

| NUMBER OF AFFORDABLE UNITS TO BE LOCATED ON-SITE |     |         |                   |                   |                     |
|--|-----|---------|-------------------|-------------------|---------------------|
| Total Affordable Units                           | SRO | Studios | One-Bedroom Units | Two-Bedroom Units | Three-Bedroom Units |
|  |     |         |                   |                   |                     |

- ☐ Off-site Affordable Housing Alternative (Planning Code Section 415.7): calculated at 20% of the unit total.

| NUMBER OF AFFORDABLE UNITS TO BE LOCATED OFF-SITE    |                            |         |                   |   |                     |
|--|----------------------------|---------|-------------------|---|---------------------|
| Total Affordable Units                               | SRO                        | Studios | One-Bedroom Units | Two-Bedroom Units                                   | Three-Bedroom Units |
|  |                            |         |                   |   |                     |
| Area of Dwellings in Principal Project (in sq. feet) | Off-Site Project Address   |         |                   |   |                     |
| Area of Dwellings in Off-Site Project (in sq. feet)  |                            |         |                   |   |                     |
| Off-Site Block/Lot(s)                                | Molton No. (if applicable) |         |                   | Number of Market-Rate Units in the Off-site Project |                     |
|  |                            |         |                   |   |                     |

- ☒ Combination of payment of a fee, on-site affordable units, or off-site affordable units with the following distribution:

Indicate what percent of each option would be implemented (from 0% to 99%) and the number of on-site and/or off-site below market rate units for rent and/or for sale.

1. Fee 23 % of affordable housing requirement. 1200 - 17th St. building  
 2. On-Site 16 % of affordable housing requirement. 901 - 16th St. building


| NUMBER OF AFFORDABLE UNITS TO BE LOCATED ON-SITE |     |         |                   |                   |                     |
|--|-----|---------|-------------------|-------------------|---------------------|
| Total Affordable Units                           | SRO | Studios | One-Bedroom Units | Two-Bedroom Units | Three-Bedroom Units |
| 42   |     | 9       | 17                | 15                | 1                   |

3. Off-Site \_\_\_\_\_ % of affordable housing requirement.

| NUMBER OF AFFORDABLE UNITS TO BE LOCATED OFF-SITE    |                            |         |                   |   |                     |
|--|----------------------------|---------|-------------------|---|---------------------|
| Total Affordable Units                               | SRO                        | Studios | One-Bedroom Units | Two-Bedroom Units                                   | Three-Bedroom Units |
|  |                            |         |                   |   |                     |
| Area of Dwellings in Principal Project (in sq. feet) | Off-Site Project Address   |         |                   |   |                     |
| Area of Dwellings in Off-Site Project (in sq. feet)  |                            |         |                   |   |                     |
| Off-Site Block/Lot(s)                                | Molton No. (if applicable) |         |                   | Number of Market-Rate Units in the Off-site Project |                     |
|  |                            |         |                   |   |                     |



Affidavit for Compliance with the Inclusionary Affordable Housing Program

| CONTACT INFORMATION AND DECLARATION OF SPONSOR OF PRINCIPAL PROJECT   | CONTACT INFORMATION AND DECLARATION OF SPONSOR OF OFF-SITE PROJECT (IF DIFFERENT)   |
|---|---|
| Company Name<br><b>Potrero Partners</b>   | Company Name  |
| Print Name of Contact Person<br><b>Josh Smith</b>   | Print Name of Contact Person  |
| Address<br><b>445 Virginia Ave.</b>   | Address   |
| City, State, Zip<br><b>San Mateo, CA 94402</b>  | City, State, Zip  |
| Phone, Fax<br><b>(415) (650) 348-3232</b>   | Phone, Fax  |
| Email<br><b>j.smith@waldendevelopment.com</b>   | Email   |
| I hereby declare that the information herein is accurate to the best of my knowledge and that I intend to satisfy the requirements of Planning Code Section 415 as indicated above. | I hereby declare that the information herein is accurate to the best of my knowledge and that I intend to satisfy the requirements of Planning Code Section 415 as indicated above. |
| Signature<br>  | Signature   |
| Name (Print), Title<br><b>Manager</b>   | Name (Print), Title   |





Planning Department  
1650 Mission Street  
Suite 400  
San Francisco, CA  
94103-9425

T: 415.558.6378  
F: 415.558.6409

## SUPPLEMENTAL INFORMATION PACKET FOR

# Anti-Discriminatory Housing Policy

Pursuant to Administrative Code Section 1.61, certain housing projects must complete and submit a completed Anti-Discriminatory Housing Policy form as part of any entitlement or building permit application that proposes an increase of ten (10) dwelling units or more.

Planning Department staff is available to advise you in the preparation of this application. Call (415)558-6377 for further information.

### WHEN IS THE SUPPLEMENTAL INFORMATION FORM NECESSARY?

Administrative Code Section 1.61 requires the Planning Department to collect an application/form with information about an applicant's internal anti-discriminatory policies for projects proposing an increase of ten (10) dwelling units or more.

### WHAT IF THE PROJECT SPONSOR OR PERMITTEE CHANGE PRIOR TO THE FIRST ISSUANCE OF CERTIFICATE OF OCCUPANCY?

If the permittee and/or sponsor should change, they shall notify the Planning Department and file a new supplemental information form with the updated information.

### HOW IS THIS INFORMATION USED?

The Planning Department is not to review the responses other than to confirm that all questions have been answered. Upon confirmation, the information is routed to the Human Rights Commission.

For questions about the Human Rights Commission (HRC) and/or the Anti-Discriminatory Housing Policy, please contact Mullane Ahern at (415) 252-2514 or [mullane.ahern@sfgov.org](mailto:mullane.ahern@sfgov.org).

All building permit applications and/or entitlements related to a project proposing 10 dwelling units or more will not be considered complete until all responses are provided.

### WHAT PART OF THE POLICY IS BEING REVIEWED?

The Human Rights Commission will review the policy to verify whether it addresses discrimination based on sexual orientation and gender identity. The policy will be considered incomplete if it lacks such protections.

### WILL THE ANSWERS TO THE QUESTIONS EFFECT THE REVIEW OF MY PROJECT?

The Planning Department's and Planning Commission's processing of and recommendations or determinations regarding an application shall be unaffected by the applicant's answers to the questions.

### INSTRUCTIONS

The attached supplemental information form is to be submitted as part of the required entitlement application and/or Building Permit Application. This application does not require an additional fee.

Answer all questions fully and type or print in ink. Attach additional pages if necessary.

Please see the primary entitlement application or Building Permit Application instructions for a list of necessary materials required.



THIS PAGE INTENTIONALLY LEFT BLANK.

FOR MORE INFORMATION

Call or visit the San Francisco Planning Department



**Central Reception**

1650 Mission Street, Suite 400  
San Francisco, CA 94103-2479

TEL: 415.558.6378

FAX: 415.558-6409

WEB: <http://www.sfplanning.org>

**Planning Information Center (PIC)**

1660 Mission Street, First Floor  
San Francisco, CA 94103-2479

TEL: 415.558.6377

Planning staff are available by phone and at the PIC counter for assistance as necessary.



# SUPPLEMENTAL INFORMATION FOR Anti-Discriminatory Housing Policy

## 1. Owner/Applicant Information

PROPERTY OWNER'S NAME

Potrero Partners LLC

PROPERTY OWNER'S ADDRESS

445 Virginia Avenue  
San Mateo, CA 94402

TELEPHONE

(650) 348-3232

EMAIL

Jsmith@waldendev.com

APPLICANT'S NAME

Same as Above ☒

APPLICANT'S ADDRESS

TELEPHONE

( )

EMAIL

CONTACT FOR PROJECT INFORMATION

Josh Smith of Potrero Partners LLC

ADDRESS

445 Virginia Avenue  
San Mateo, CA 94402

TELEPHONE

(650) 348-3232

EMAIL

Jsmith@waldendev.com

Same as Above ☐

COMMUNITY LIAISON FOR PROJECT (PLEASE REPORT CHANGES TO THE ZONING ADMINISTRATOR)

ADDRESS

Same as Above ☒

TELEPHONE

( )

EMAIL

## 2. Location and Project Description

STREET ADDRESS OF PROJECT

901 16<sup>th</sup> Street and 1200 17<sup>th</sup> Street

ZIP CODE

94107

CROSS STREETS

16<sup>th</sup> Street, Mississippi Street, and 17<sup>th</sup> Street

ASSESSORS BLOCK/LOT

3449/001, 002

ZONING DISTRICT

UMA

HEIGHT BULK DISTRICT

68-X, 48-X

PROJECT TYPE (Please check all that apply)

☒ New Construction

☒ Demolition

☐ Alteration

☐ Other: \_\_\_\_\_

EXISTING DWELLING UNITS

PROPOSED DWELLING UNITS

NET INCREASE

0

395

395



## Compliance with the Anti-Discriminatory Housing Policy

1. Does the applicant or sponsor, including the applicant or sponsor's parent company, subsidiary, or any other business or entity with an ownership share of at least 30% of the applicant's company, engage in the business of developing real estate, owning properties, or leasing or selling individual dwelling units in States or jurisdictions outside of California?

☐ YES ☒ NO

1a. If yes, in which States? \_\_\_\_\_

- 1b. If yes, does the applicant or sponsor, as defined above, have policies in individual States that prohibit discrimination based on sexual orientation and gender identity in the sale, lease, or financing of any dwelling units enforced on every property in the State or States where the applicant or sponsor has an ownership or financial interest?

☐ YES ☐ NO

- 1c. If yes, does the applicant or sponsor, as defined above, have a national policy that prohibits discrimination based on sexual orientation and gender identity in the sale, lease, or financing of any dwelling units enforced on every property in the United States where the applicant or sponsor has an ownership or financial interest in property?

☐ YES ☐ NO

*If the answer to 1b and/or 1c is yes, please provide a copy of that policy or policies as part of the supplemental information packet to the Planning Department.*

Human Rights Commission contact information  
**Mullane Ahern at (415)252-2514 or [mullane.ahern@sfgov.org](mailto:mullane.ahern@sfgov.org)**

## Applicant's Affidavit

Under penalty of perjury the following declarations are made:

- a. The undersigned is the owner or authorized agent of the owner of this property.
- b. The information presented is true and correct to the best of my knowledge.
- c. Other information or applications may be required.

Signature: \_\_\_\_\_

Date: \_\_\_\_\_

Print name, and indicate whether owner, or authorized agent.

☒ Owner ☐ Authorized Agent (circle one)

Josh Smith



## PLANNING DEPARTMENT USE ONLY

### PLANNING DEPARTMENT VERIFICATION:

- ☒ Anti-Discriminatory Housing Policy Form is **Complete**  
☐ Anti-Discriminatory Housing Policy Form is **Incomplete**

Notification of Incomplete Information made:

To: \_\_\_\_\_ Date: \_\_\_\_\_

BUILDING PERMIT NUMBER(S):

N/A

DATE FILED:

RECORD NUMBER:

2011.1300 X

DATE FILED:

6/19/2014

VERIFIED BY PLANNER

Signature:



Date:

5/2/2016

Printed Name

CHRISTOPHER TOWNES

Phone:

(415) 575-9195

ROUTED TO HRC:

DATE:

☐ Emailed to: \_\_\_\_\_





SAN FRANCISCO  
PLANNING  
DEPARTMENT

# AFFIDAVIT FOR FIRST SOURCE HIRING PROGRAM

## Administrative Code

### Chapter 83

1650 Mission Street, Suite 400 • San Francisco CA 94103-2479 • 415.558.6378 • <http://www.sfplanning.org>

#### Section 1: Project Information

|  |   |   |
|--|---|---|
| PROJECT ADDRESS<br><i>901 16<sup>th</sup> Street and 1200 17<sup>th</sup> Street</i> |   | BLOCK/LOT(S)<br><i>3949/001, 001A, 002 3950/001</i>         |
| BUILDING PERMIT APPLICATION NO.  | CASE NO. (IF APPLICABLE)<br><i>2011.1300EX</i>    | MOTION NO. (IF APPLICABLE)                                  |
| PROJECT SPONSOR<br><i>Podrero Partners LLC</i>                                       | MAIN CONTACT<br><i>Josh Smith</i>                 | PHONE<br><i>650-348-3232</i>                                |
| ADDRESS<br><i>445 Virginia Avenue</i>  |   |   |
| CITY, STATE, ZIP<br><i>San Mateo, CA</i>   |   | EMAIL<br><i>Jsmith@Waldendevelopment.com</i>                |
| ESTIMATED RESIDENTIAL UNITS<br><i>395</i>  | ESTIMATED SQ FT COMMERCIAL SPACE<br><i>24,960</i> | ESTIMATED HEIGHT/FLOORS<br><i>68' 6 Floors 48' 4 Floors</i> |
| ANTICIPATED START DATE<br><i>TBD</i>   |   | ESTIMATED CONSTRUCTION COST<br><i>\$160 million</i>         |

#### Section 2: First Source Hiring Program Verification

|   |  |
|---|--|
| CHECK ALL BOXES APPLICABLE TO THIS PROJECT  |  |
| <input type="checkbox"/>  | Project is wholly Residential  |
| <input type="checkbox"/>  | Project is wholly Commercial   |
| <input checked="" type="checkbox"/>   | Project is Mixed Use   |
| <input checked="" type="checkbox"/>   | A: The project consists of ten (10) or more residential units;                     |
| <input type="checkbox"/>  | B: The project consists of 25,000 square feet or more gross commercial floor area. |
| <input type="checkbox"/>  | C: Neither 1A nor 1B apply.  |
| <p>NOTES:</p> <ul style="list-style-type: none"> <li>If you checked <b>C</b>, this project is <u>NOT</u> subject to the First Source Hiring Program. Sign Section 4: Declaration of Sponsor of Project and submit to the Planning Department.</li> <li>If you checked <b>A</b> or <b>B</b>, your project is <u>SUBJECT</u> to the First Source Hiring Program. Please complete the reverse of this document, sign, and submit to the Planning Department prior to any Planning Commission hearing. If principally permitted, Planning Department approval of the Site Permit is required for all projects subject to Administrative Code Chapter 83.</li> <li>For questions, please contact OEWD's CityBuild program at <a href="mailto:CityBuild@sfgov.org">CityBuild@sfgov.org</a> or (415) 701-4848. For more information about the First Source Hiring Program visit <a href="http://www.workforcedevelopmentstf.org">www.workforcedevelopmentstf.org</a></li> <li>If the project is subject to the First Source Hiring Program, you are required to execute a Memorandum of Understanding (MOU) with OEWD's CityBuild program prior to receiving construction permits from Department of Building Inspection.</li> </ul> |  |

Continued...



### Section 3 First Source Hiring Program – Workforce Projection

Per Section 83.11 of Administrative Code Chapter 83, it is the developer's responsibility to complete the following information to the best of their knowledge.

Provide the estimated number of employees from each construction trade to be used on the project, indicating how many are entry and/or apprentice level as well as the anticipated wage for these positions.

Check the anticipated trade(s) and provide accompanying information (Select all that apply):

| TRADE/CRAFT            | ANTICIPATED JOURNEYMAN WAGE | # APPRENTICE POSITIONS | # TOTAL POSITIONS | TRADE/CRAFT            | ANTICIPATED JOURNEYMAN WAGE | # APPRENTICE POSITIONS | # TOTAL POSITIONS |
|------------------------|-----------------------------|------------------------|-------------------|------------------------|-----------------------------|------------------------|-------------------|
| Abatement Laborer      | 75.00                       | 1                      | 5                 | Laborer                | 55.00                       | 5                      | 25                |
| Boilermaker            | 60.00                       | 1                      | 2                 | Operating Engineer     | 75.00                       | 2                      | 10                |
| Bricklayer             | 60.00                       | 1                      | 5                 | Painter                | 65.00                       | 5                      | 25                |
| Carpenter              | 75.00                       | 4                      | 25                | Pile Driver            | 78.00                       | 1                      | 5                 |
| Cement Mason           | 60.00                       | 5                      | 25                | Plasterer              | 75.00                       | 2                      | 15                |
| Drywall/Latherer       | 75.00                       | 7                      | 30                | Plumber and Pipefitter | 85.00                       | 5                      | 20                |
| Electrician            | 95.00                       | 5                      | 25                | Roofer/Water proofer   | 80.00                       | 1                      | 5                 |
| Elevator Constructor   | 105.00                      | 1                      | 5                 | Sheet Metal Worker     | 85.00                       | 1                      | 5                 |
| Floor Coverer          | 78.00                       | 5                      | 20                | Sprinkler Fitter       | 85.00                       | 2                      | 10                |
| Glazier                | 80.00                       | 2                      | 15                | Taper                  | 70.00                       | 3                      | 15                |
| Heat & Frost Insulator | 75.00                       | 0                      | 2                 | Tile Layer/Finisher    | 78.00                       | 4                      | 20                |
| Ironworker             | 78.00                       | 2                      | 10                | Other:                 |                             |                        |                   |
| <b>TOTAL:</b>          |                             |                        | <b>169</b>        | <b>TOTAL:</b>          |                             |                        | <b>155</b>        |

- Will the anticipated employee compensation by trade be consistent with area Prevailing Wage? ☒ YES ☐ NO
- Will the awarded contractor(s) participate in an apprenticeship program approved by the State of California's Department of Industrial Relations? ☒ YES ☐ NO
- Will hiring and retention goals for apprentices be established? ☒ YES ☐ NO
- What is the estimated number of local residents to be hired? 32

### Section 4 Declaration of Sponsor of Principal Project

PRINT NAME AND TITLE OF AUTHORIZED REPRESENTATIVE

EMAIL

PHONE NUMBER

*Josh Smith, Manager*

*Jsmith@Wubanddevelopment.com*

*650-348-3232*

I HEREBY DECLARE THAT THE INFORMATION PROVIDED HEREIN IS ACCURATE TO THE BEST OF MY KNOWLEDGE AND THAT I COORDINATED WITH OEWD'S CITYBUILD PROGRAM TO SATISFY THE REQUIREMENTS OF ADMINISTRATIVE CODE CHAPTER 83.

(SIGNATURE OF AUTHORIZED REPRESENTATIVE)

(DATE)

FOR PLANNING DEPARTMENT STAFF ONLY: PLEASE EMAIL AN ELECTRONIC COPY OF THE COMPLETED AFFIDAVIT FOR FIRST SOURCE HIRING PROGRAM TO OEWD'S CITYBUILD PROGRAM AT CITYBUILD@SFGOV.ORG

Cc: Office of Economic and Workforce Development, CityBuild  
Address: 1 South Van Ness 5th Floor San Francisco, CA 94103 Phone: 415-701-4848  
Website: www.workforcedevelopment.org Email: CityBuild@sfgov.org



**From:** Art Agnos artagnos@yahoo.com  
**Subject:** Draft EIR: 901 16th St Case No. 2011.1300 E  
**Date:** September 9, 2015 at 10:36 AM  
**To:** Commissions.Secretary@sfgov.org

Mr. Ionin...this email letter was sent to your Commissioners and this is your copy for the file. Thanks, Art Agnos

Mr. Rodney Fong, President  
San Francisco Planning Commission

September 9, 2015

**Re: Draft Environmental Impact Report  
901 16<sup>th</sup> Street and 1200 17<sup>th</sup> Street Project in San Francisco, CA  
San Francisco Planning Department Case No. 2011.1300E**

Dear Rod,

As you know, I don't do this very often, but as a long time resident of Potrero Hill and a former mayor, I am writing to you regarding the Draft Environmental Impact Report for the proposed 901 16<sup>th</sup> Street and 1200 17<sup>th</sup> Street Project. It is time to do something about this blighted corner of Potrero Hill.

The Draft Environmental Impact Report appears to be comprehensive and accurate and concludes that, other than contributing traffic to a few intersections, the project has no significant environmental impacts.

That is why I am writing to express my strong support of the proposed project. In addition to creating badly needed new housing that will help to alleviate the San Francisco's housing shortage, including family sized 2- and 3-bedroom units, the project includes approximately 25,000 square feet of ground floor neighborhood-serving retail space. This will be a vast improvement over the blank walls of rusting corrugated metal that now deface the street and diminish the neighborhood.

I respectfully encourage you to certify this EIR and support and approve the proposed project. The developer has worked diligently and successfully with the neighborhood to offer this progressive design.

Sincerely,  
Art Agnos



# **RON MIGUEL**

600 De Haro St., San Francisco, CA 94107  
T-415.285.0808 F-415.641.8621 E-rm@well.com C-415.601.0708

25 April 2016

San Francisco Planning Commission  
1650 Mission St., 4<sup>th</sup> Floor  
San Francisco, CA 94103

RE: 901-16<sup>th</sup> St. / 1200-17<sup>th</sup> St.

Commissioners:

I fully and enthusiastically support this project. It exemplifies the concepts imbedded in the Eastern Neighborhoods Plan as presented to me when I served on the Commission.

I have viewed plans for this important site starting well over five years ago. The current program of two residential/mixed use structures is by far the best I have seen. In fact, it may be the best designed project in this area of our city where I've lived for 40 years. As you know, I have worked with architects and developers in an effort to enhance the architecture and viability of projects in the area – Walden Development gets it. The two designs work both individually and back-to-back.

These proposed buildings take full advantage of the most important corridor linking Mission Bay/ UCSF/Kaiser and other developments at the eastern end of the major transportation corridor – 16<sup>th</sup> St. – with the vitality and transit-rich areas centering on the 16<sup>th</sup> St. BART station.

These projects employ obvious enhancements which have been missing from many of the Dogpatch/Potrero Hill buildings you have seen: it is family-ready with a larger number of 3-bedroom units; a unique and innovative use of material from the replaced industrial structures of the site; restoration of the historic brick building at 17<sup>th</sup> & Texas; the addition of true retail spaces which can be configured for a variety of tenants; and six flex units to enhance the PDR concept embedded in the UMU zoning. In addition there is a public Pedestrian Promenade which aligns with the new Daggett Park as well as the more private family-friendly mews between the two buildings.

I have attended many meetings of both the Dogpatch Neighborhood Association and the Potrero Boosters where the evolving plans were presented by Walden Development. I have also met personally with the developer and architect as well as toured the site. The projects have improved by Walden's careful attention to detail and taking into consideration the neighborhoods' concerns and suggestions.

Please move this project forward.

Thanks,

Ron Miguel



April 12, 2016

San Francisco Planning Department  
Re: 901 16th Street and 1200 17th Street

To Whom It May Concern,

I wish to express my support for the pending project at 16th Street and 1200 17th Street. Having been involved in the community meetings to develop the Eastern Neighborhoods Plan and as a neighborhood resident and business owner who has attended numerous developer presentations over the past 10-12 years, I can state that this project is probably the most sensitive to the neighborhood in many respects:

It will provide family friendly housing with a generous mix of 2 bedroom and 3 bedroom units; this is at a time of great need for such housing.

This is easily the best-designed multi-unit building that has been presented to the neighborhood since the EN Plan was instituted. The design fits the industrial past of the area; it has thoughtful set-backs and it is varied and interesting to view.

I make my living as a Restoration contractor, having worked on numerous historic buildings in the City, including many "listed" buildings. For me, the existing structure has very little value - it is rusted-out, ugly, with panels of uninteresting, corrugated metal. Should we maintain such a structure because it was built with rivets?? It provides no value to the neighborhood.

The proposed development will activate ground floor spaces on 16th and 17th Streets with retail spaces and a pedestrian promenade.

I have seen the developer, Walden Development, engage with our community like no other over the past 10 years (or more). He may have even attended more community meetings than any one of my neighbors. He has been quick to respond to concerns raised by the community and has offered his support to numerous local groups. I believe his heart is truly set on the improvement and quality of life of our neighborhood.

I urge you to approve this development.

Sincerely,



Keith Goldstein  
President, Potrero Dogpatch Merchants Association  
Member EN CAC  
Board Member, Potrero Boosters  
Co Chair -Potrero Hill Festival  
Everest Waterproofing & Restoration, Inc, President



May 2, 2016

To whom it may concern:

I am pleased, that after many years of planning and development in the Potrero Hill, Showplace square area there has emerged a developer who has genuinely taken the community's needs and concerns into consideration.

Over the past 5 years Walden Development has painstakingly held design and historical reviews meetings, where they gather and implemented community recommend design changes. Walden Development has also taken existing commercial operations into consideration as they planned pedestrian and traffic access to their project, while including passive community amenity.

Walden Development has been, and I am sure will continue to be an excellent neighbor in the Potrero Hill/ Showplace Square communities.

Sincerely,

Edward Hatter,  
Executive Director  
Potrero Hill Neighborhood House  
Co-Chair of the Potrero Festival



# the bookkeeper

April 25, 2016

San Francisco Planning Department  
1650 Mission Street, Suite 400  
San Francisco, CA 94103

RE: 901 16<sup>th</sup> Street & 1200 17<sup>th</sup> Street

To whom it May Concern,

Please use this letter as my enthusiastic support for the proposed project at 16<sup>th</sup> Street & 1200 17<sup>th</sup> Street from Walden Development.

I have attended each and every presentation from the project sponsor over the past 10 years. The developer has listened to community feedback and concerns throughout the design process like no other developer I have worked with. This final design utilizes two San Francisco based architects that have designed buildings that are unique, architecturally extremely attractive, and incorporate several design references that acknowledge and pay homage to Potrero Hill's industrial past. Additionally, the project preserves, restores and repurposes the historic brick office building at the corner of 17<sup>th</sup> Street & Texas Street into a very engaging and sophisticated retail space, probably for a restaurant and or bar.

One of the things I really appreciate about this project is that it creates a retail space at the corner of 17<sup>th</sup> Street and Mississippi Street that will activate this important corner which is currently a blank wall of rusting corrugated metal into a vibrant pedestrian friendly space where neighbors and co-workers can meet and socialize. In an effort to acknowledge the industrial past of this site, the retail space at this corner is designed to resemble the existing corrugated metal shed.

This project also provides a new 30 foot wide Pedestrian Promenade along the westerly property line that is aligned with the new Daggett Park which should be completed by this July. This will provide the needed connectivity between 16<sup>th</sup> Street and 17<sup>th</sup> Street instead of the blank walls of the rusting corrugated metal that currently exists.

The current structure provides absolutely no benefit or value to the neighborhood. It is a rusted, patched together corrugated metal mess.

Please support this project as designed and give us the architectural design, housing, and retail services we greatly need at this location.

Sincerely



Susan Eslick  
Board Member and Treasurer, The Dogpatch & NW Potrero Hill Green Benefit District (GBD)  
Formation Member, Dogpatch Arts Plaza (DAP)  
the bookkeeper

CC: Josh Smith, Walden Development  
Supervisor Malia Cohen

susan eslick

1121 Tennessee Street  
San Francisco, CA 94107  
eslickdesigns@gmail.com  
415.257.1101



From: Bruce K Huie brucehuie@sbcglobal.net  
Subject: Walden Development - Letter of support 901 16th Street & 1200 17th Street  
Date: April 26, 2016 at 7:29 PM  
To: Malia Cohen malia.cohen@sfgov.org  
Cc: Andrea Bruss andrea.bruss@sfgov.org, Josh Smith jsmith@waldendevlopment.com, Susan Eslick susan.thebookkeeper@gmail.com

Supervisor Cohen -

I support this project.

Josh Smith of Walden Development represents the "platinum" standard every developer should achieve when working with neighbors.

Project helps San Francisco by ...

1. Utilizing two San Francisco-based architects (BAR Architects and Christiani Johnson Architects) that are unique, architecturally attractive, and incorporate design references that acknowledge and pay homage to Potrero's Hill's industrial past
2. Offering "family-friendly" attributes
  - a. twenty two (22) 3-bedroom/2 full bath units as well as one hundred forty six (146) 2-bedroom/2 full bath units,
  - b. gated, family friendly recreation area in the mews area that is located between the two buildings; and
  - c. on-site parking, which many families value.
3. Preserves, restores and reuses the historic brick office building at the corner of 17th Street & Texas Street.

For your consideration, please join me in supporting this project as it sets the bar high for what we need to expect from all projects in and around Dogpatch and Potrero Hill neighborhoods and the city at large.

Thanks in advance for you consideration.

Bruce Kin Huie  
Dogpatch Resident  
President - DNA  
EN CAC - Vice Chair  
UCSF CAG member



# POTRERO CHIROPRACTORS & ACUPUNCTURE

NECK, BACK, & HEADACHE PAIN RELIEF CENTER

FRANK GILSON, D.C. | BRENDA HATLEY, L.Ac.

---

October 1, 2015

Planning Department Case No. 2011.1300E

RE: Walden Development Project at Coravan Building

Dear Planning Use Committee,

My name is Dr. Frank Gilson, Vice President of the Potrero Dogpatch Merchant Association. As an active member of my local business community, I fully endorse this project.

The sponsor has done a tremendous effort of reaching out to our community for years. He has made his priority to listen to and hear the neighbors' and businesses' concerns, and he has been above-duty in addressing them.

Here are the facts:

- Draft EIR is accurate and adequate
- Project is well designed and addresses the community's concerns
- It will activate what is currently a blank streetscape
- The sponsor is very community-oriented and has donated to numerous non-profit causes like Daniel Webster Public School and the Potrero Hill Festival
- This project will help small businesses all over the southeastern neighborhoods like South Beach, Dogpatch, and Potrero Hill
- This project is exactly what the city wants; housing, which is what we all know we desperately need in San Francisco, and that which is close to mass transit. The light rail and numerous Muni lines are within a short walking distance.

It is because of these reasons that I urge you to endorse this project.

Thank you for your time.

Sincerely,

Frank Gilson, D.C.



April 25, 2016

To: San Francisco Planning artment

Re: 901 16<sup>th</sup> Street and 1200 17<sup>th</sup> St. Proposed development project

To whom it may concern:

I support the above referenced proposed project because it will:

1. Provide family-friendly housing in a neighborhood that really needs it (twenty two (22) 3-bedroom/2 bath units and one hundred forty six (146) 2-bedroom/2 bath units with an enclosed family recreation area on-site.
2. Provide on-site parking which this neighborhood needs as our public transportation is abysmal.
3. Preserves and repurposes, with public access, the small historic brick office building at the corner of 17th and Texas streets.
4. Provides a new 30' wide landscaped pedestrian alley along the western property line, which will be aligned with the soon to be completed Daggett Park, thus providing a pedestrian friendly connection between 16th and 17th streets in an area with a dearth of public open space.
5. Unlike many new large projects in the UMU district in Potrero and Dogpatch, this project creates approximately 25,000 sf of ground floor neighborhood-serving retail space along 16th and 17th streets and a retail space at the corner of 17th and Mississippi streets that will activate this neighborhood corner.
6. The project is designed by two local architectural firms, resulting in two distinct designs.

Thank you,



Janet Carpinelli  
Member Potrero Boosters Neighboerhood Assoc.  
Member, Dogpatch Neighborhood Assoc.



April 26, 2016

San Francisco Planning Department via email to: [chris.townes@sfgov.org](mailto:chris.townes@sfgov.org)  
Walden Development via email to: [jsmith@waldendevlopment.com](mailto:jsmith@waldendevlopment.com),

Re: Case No. 2011.1300EX  
901 16th Street and 1200 17th Street

To Whom It May Concern,

I wish to express my support for approval of the Large Project Authorization and Adoption of CEQA findings for the pending project at 16th Street and 1200 17<sup>th</sup> Street.

This project reflects extensive community engagement. It incorporates several design references that acknowledge Potrero Hill's industrial past, and fits with the current mixed use development in the area. It has thoughtful set-backs, particularly the plaza at 16<sup>th</sup> and Mississippi Street, and it is varied and interesting to view.

It will provide much needed housing with a generous mix of 2 bedroom and 3 bedroom units. It preserves, restores and repurposes the historic brick office building at the corner of 17th Street & Texas Street into a retail space.

The proposed development will activate the pedestrian environment with approximately 25,000 s.f. of neighborhood-serving ground floor retail along both 16th Street and 17th Street. This will be a vast improvement over the rusting corrugated metal sheds that now form blank walls along both 16th Street & 17th Street. The project provides a new 30' wide pedestrian promenade along the westerly property line that is aligned with the new Daggett Park. The pedestrian promenade will provide connectivity between 16th Street and 17th Street. My one concern is that the pedestrian connections across 16<sup>th</sup> Street between this project and Daggett Park and across Mississippi Street be coordinated with SFMTA for pedestrian and traffic safety.

I have followed the evolution of this project for many years, and worked with the developer on several issues of mutual concern. Walden Development has actively engaged with the community, and has been quick to respond to concerns, for the benefit of all our neighborhoods.

I urge you to approve this development.

Sincerely,

Corinne Woods  
300 Channel Street, Box 10  
San Francisco, CA 94158-1520



April 24, 2016

To: The San Francisco Planning Commission and  
The San Francisco Planning Department

Re: Support for the Proposed Project at 901 16<sup>th</sup> Street and 1200 17<sup>th</sup> Street,  
San Francisco, CA 94017

To Whom It May Concern:

I am the owner of property that is located at 1240-50 17<sup>th</sup> Street, which is immediately adjacent to the proposed project referenced above, and I am writing to offer my enthusiastic and wholehearted support for the proposed project.

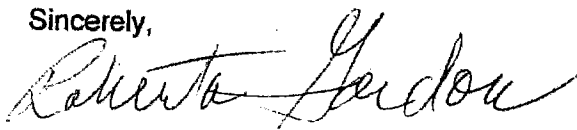
My family has owned the property at the northeast corner of 17<sup>th</sup> Street and Missouri Street for several decades, and we believe that the proposed project will greatly improve and enhance this portion of Potrero Hill.

The existing rusting corrugated metal sheds are a source of blight and present nothing but blank walls to the street. The proposed project will replace those ugly rusting corrugated metal sheds with an attractive and well designed mixed use project that will activate 17<sup>th</sup> Street and 16<sup>th</sup> Street as well as provide needed housing.

Josh Smith, the project sponsor, has reached out to me dozens of times to keep me updated about the project. The design of this project is new and innovative and again an enhancement to the neighborhood.

I respectfully encourage you to certify the EIR and support and approve the proposed project.

Sincerely,

A handwritten signature in cursive script, appearing to read "Roberta Gordon".

Roberta Gordon



**From:** Natalie natalieinsf@yahoo.com  
**Subject:** Residential project between 16th Street and 17th Street at 7th Street  
**Date:** April 25, 2016 at 11:52 PM  
**To:** Josh Smith jsmith@waldenddevelopment.com

Hi Josh,

Thank you for contacting me the other day. I am glad to hear that things are once again moving forward for the residential project you are proposing next door. As a neighbor of the proposed project, I appreciate that you have reached out to me and the other occupants of my condominium building. I am very excited at the prospect of having a new building with street-level retail space and a pedestrian promenade within a stone's throw of my home. I also appreciate that you have been cooperative in addressing our concerns regarding noise, landscaping, lighting and security.

I would like to express my support of your project and I hope to make it to the hearing on May 12, but if I am unable to, please share my email with the Planning Commission.

Sincerely,  
Natalie Young  
999 16th Street, Unit #21



April 25, 2016

To: The San Francisco Planning Commission and  
The San Francisco Planning Department

Re: Support for the Proposed Project at 901 16<sup>th</sup> Street and 1200 17<sup>th</sup> Street,  
San Francisco, CA 94017

Dear Members of the Planning Commission:

We own the property at 1111 17<sup>th</sup> Street, which is directly across 17<sup>th</sup> Street from the proposed project referenced above.

I am writing to offer my enthusiastic support for the proposed project.

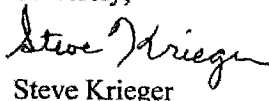
My family has owned the property at the southwest corner of 17<sup>th</sup> Street and Mississippi Street for many years, and we believe that the proposed project referenced above will significantly improve and enhance this portion of Potrero Hill.

The existing rusting corrugated metal sheds are a source of blight and present nothing but blank walls to the street. The proposed project will replace those ugly rusting corrugated metal sheds with an attractive and well designed mixed use project that will activate 17<sup>th</sup> Street and 16<sup>th</sup> Street as well as provide badly needed housing that will help to alleviate our City's housing crises.

Josh Smith, the project sponsor, has reached out to me many times in order to keep me updated about the project and answer any questions I may have about the project.

I respectfully encourage you to certify the EIR and support and approve the proposed project.

Sincerely,

  
Steve Krieger





333 Third St, Suite 210  
San Francisco, CA 94107

EquityApartments.com

April 25, 2016

Hon. Rodney Fong, President  
San Francisco Planning Commission  
1650 Mission Street, 4<sup>th</sup> Floor  
San Francisco, CA 94103

Re: Letter of Support for the Proposed Project at  
901 16<sup>th</sup> Street and 1200 17<sup>th</sup> Street  
San Francisco, CA 94017  
Case No. 2011.1300X

Dear President Fong and Commissioners:

We own the apartment community at 1010 16<sup>th</sup> Street, which is directly across 16<sup>th</sup> Street from the proposed project referenced above.

I am writing to offer our enthusiastic support for the proposed project.

We believe that the proposed project referenced above will significantly improve and enhance this portion of Potrero Hill and will help to create a complete neighborhood, where residents can live, shop and socialize with their neighbors.

The existing rusting corrugated metal sheds that are located at the site of the proposed project are a source of blight and present nothing but blank walls to the street. The proposed project will replace those ugly rusting corrugated metal sheds with an attractive and well-designed mixed use project that will activate 16<sup>th</sup> Street and 17<sup>th</sup> Street and provide additional housing.

Josh Smith, the project sponsor, has reached out to me many times in order to keep me updated about the project and answer any questions I may have about the project.

I respectfully encourage you to certify the EIR and support and approve the proposed project.

Sincerely,



Jim Kelly

First Vice President – Development  
Equity Residential



April 12, 2016

San Francisco Planning Commission

Proposed Development to Redevelop Corovan Site @ 16<sup>th</sup> and 17<sup>th</sup> Street

To Whom It May Concern,

I wish to express my support for this proposed project, having been very involved in development and the Eastern Neighborhoods Plan and as a neighborhood resident and real estate consultant for the past 30 years. In my opinion, this project is the most responsive to the Potrero neighborhood as well as the City. It will provide family friendly housing with a generous mix of 2 bedroom and 3 bedroom units, as well as open space for the residents and neighbors.

I truly believe that by utilizing the work of two of the most sensitive architectural firms in San Francisco the proposed project respects the history of the original uses and replaces an obsolete structure and helps to transition the past industrial area to a residential and retail neighborhood.

This is easily one of the best multi-unit building that has been presented to the neighborhood since the EN Plan, as well as the design guidelines were adopted. The design fits the industrial past of the area and has considerate set-backs.

The developer, Walden Development, has connected with the community selflessly over the past 15 years, attending most community meetings.

I urge your support for this project.

Sincerely,

Joe Boss

Board Member, Potrero Boosters, ENCAC, Dogpatch Neighborhood Association  
(for identification only)



**From:** Thomas Rogers  
**To:** Townes, Chris (CPC)  
**Subject:** 901 16th Street and 1200 17th Street - SUPPORT  
**Date:** Tuesday, May 03, 2016 4:56:12 PM

---

Chris,

Please include this email with the 5/12 PC staff report for the 901 16th Street and 1200 17th Street proposal. I would like to relay my SUPPORT of the project, which would be located just a block down the hill from where I live. It would actually affect the views from my apartment, but that would be a selfish reason to oppose a new project that conforms to all the key development standards. In particular, I'd like to note:

- **The 395 new housing units at this underutilized site would help to address the City's housing shortage.** A primary objective of the Eastern Neighborhoods Plan is to increase housing locally through the build out of the plan area. The project would develop the project site in a manner envisioned by the Plan in its density and design.
- **This, like other parts of Potrero Hill/Showplace Square, is a great location for car-free/car-light living.** It seems like every day brings more bad news about climate change (today's entry is about a Louisiana island town that needs to relocate inland: <http://nyti.ms/23kzyiz>), but the good news is that projects like this enable people to get around via transit/bicycling/walking, helping to reduce per-capita greenhouse gases. *If anything, I'd encourage you to make sure the parking at this project is as limited as it can be. I'm not sure if the applicant is still asking for an exception to exceed the off-street parking maximum- if they are, please note that you do not have to grant that!*
- **The design is varied and context-sensitive, and would improve the character of the area.** For such a large site, the buildings feel modulated and non-monolithic, especially on the 17th Street side. The incorporation of the brick office building and the corner shed form that evokes the non-historic shed structures really help break up the design. All of the structures have features relating to existing neighboring buildings and land uses, and I think you won't have a problem saying the Potrero Interim Controls are addressed. Also, although I think the pedestrian promenade between 16th and 17th Streets is a requirement of the Eastern Neighborhoods Plan, that's still a great feature.
- **The environmental review has been complete and careful.** In particular, I think the EIR has conclusively addressed the historic resources topic, establishing that the brick building is the only structure with historic integrity. The traffic analysis used the very conservative Level of Service (LOS) analysis, which is in the process of being replaced by the more progressive Vehicle Miles Traveled (VMT) analysis. If the latter had been in effect for this project, I suspect there wouldn't even be any transportation impacts found.



Thanks for your consideration,  
Thomas Rogers  
Mariposa Street



May 4, 2016

Rodney Fong, Commission President  
Dennis Richards, Commission Vice President  
Cindy Wu, Commissioner  
Michael J. Antonini, Commissioner  
Rich Hillis, Commissioner  
Christine D. Johnson, Commissioner  
Kathrin Moore, Commissioner

Re: 901 16<sup>th</sup> / 1200 17<sup>th</sup> Streets  
Case No. 2011.1300

Dear Commissioners,

I am writing on behalf of Save The Hill, a grassroots neighborhood group dedicated to the health, culture, heritage and scenic beauty of Potrero Hill.

As of May 4, 2016, 330 people from the Potrero Hill community have signed our petition calling on the developers of 901 16<sup>th</sup> / 1200 17<sup>th</sup> to make a number of reasonable modifications to their proposed project. Each one of you has received a copy of this petition along with more than 135 comments by signers. Many of these community signers have left very thoughtful comments regarding the importance of this project to the future of the neighborhood. Additionally, a number of neighbors have submitted letters to you in support of Save The Hill's proposed project modifications. To date, the developers have offered no concessions or meaningful modifications on any of the points of community concern – concerns that are highlighted below.

We worry that a development much larger in scale and impact than nearby Daggett Place (1010 16<sup>th</sup> Street at 7<sup>th</sup>) will soon rise, despite wide support from the community favoring reasonable modifications. The 3.5-acre development site is a "Gateway" location to the neighborhood but the developer's current proposal fails to treat it as such.

Numerous community meetings and extensive outreach organized by Save The Hill over the past few years have informed the following list of community priorities:

**\* Reduce Height, Scale, Massing On 16<sup>th</sup> Street:**

- Reduce the project's height and massing on 16<sup>th</sup> Street to respect the topography of the hill and to reduce shadowing of the project's proposed pedestrian alleyway and the new Daggett Park. Attendees at multiple Save The Hill meetings over the past few years have called for breaking up the mass of the 16<sup>th</sup> Street buildings. ("Make 16<sup>th</sup> Street look more like 17<sup>th</sup> Street".) The currently proposed 16<sup>th</sup> Street structures have the effect of creating a canyon on 16th and walling off Potrero Hill. Moreover, the Potrero Hill Area Plan specifically calls for following topography and reducing height starting on the south side of 16th Street.

- Reduce or eliminate eyesore rooftop mechanical/elevator/stair penthouses. Cap all heights (including penthouses) on the 16<sup>th</sup> Street side of the project to between 58 feet to 68 feet. The developer's current plan proposes heights of between 72 feet and 83 feet due to sizable clusters



of rooftop mechanical/stair/elevator penthouses. Especially egregious is the Northwest corner building that is adjacent to the pedestrian alleyway and utilizes oversized rooftop elevator and stairway penthouses.

**\* Increase Public Open Space:** Increase publicly accessible open space by widening the proposed North/South pedestrian promenade to at least 40 feet throughout the development site. The proposed public open space in the project remains inadequate; the planned private open space is double the size offered for public open space. The so-called public pedestrian North-South alleyway should be widened from the planned 22 feet to at least 40 feet throughout the passageway. There should be more green soft-scape and less hard-scape for the pedestrian alleyway. A 40-foot wide promenade would significantly improve the pedestrian experience, enlarge publicly accessible open space, encourage public gathering, and mitigate shadowing. The developers of 1601 Mariposa agreed to dramatically widen a similar pedestrian passageway for that project — a model of goodwill that the developers of 901 16<sup>th</sup> / 1200 17<sup>th</sup> have so far refused to emulate.

**\* Amplify Authenticity / Adaptive Reuse:** Reuse distinctive materials and features of the current metal warehouses in order to create visual and historic continuity between old and new uses at the site. The Final Environmental Impact Report (EIR) has now identified the Metal Shed Reuse Alternative as the environmentally superior project. We support this alternative as the preferred choice. The developer should do more to include elements of adaptive reuse in the project that go beyond tokenism. This would entail retaining and incorporating into the proposed project more signature features and materials of the existing metal warehouses. Save The Hill submitted to the developers specific examples of what could be retained and repurposed. As one example, the steel framing of the western metal warehouse (currently the green/red warehouse running between 17<sup>th</sup> and 16<sup>th</sup> Streets) could be retained and utilized as a “canopy” for the pedestrian promenade. The overall effect of incorporating original features and materials would be more respectful of existing neighborhood character.

**\* Increase Commercial / Retail On 17<sup>th</sup> Street, More Diverse Uses:** Replace housing on 17<sup>th</sup> Street with more diverse commercial uses such as space for artists, makers (light PDR), non-profit groups, and neighborhood serving retail, which will help reduce conflicts between the Bottom of the Hill nightclub and new residents, as well as providing neighborhood services and amenities.

**\* Traffic:** Reduce parking and traffic congestion by shrinking the project and limiting the amount of stalls in the off-street parking garage.

**\* Formula Retail:** Prohibit formula retail within the development

A number of other items related to project impacts remain points of controversy and concern. The Environmental Impact Report and Response to Comments for 901 16<sup>th</sup> / 1200 17<sup>th</sup> Street inadequately or inaccurately addressed the following:

#### **Metal Shed Reuse Alternative – Economic Feasibility**

The DEIR and Response to Comments inadequately address or consider economic feasibility of the Metal Shed Reuse Alternative thus impairing informed decision-making. Per California



Superior Court case Preservation Action Council v. City of San Jose, 2006, 141 Cal. App.4th 1336, the City of San Francisco needs to independently review and confirm with qualified experts any information provided by the developer regarding economic feasibility or infeasibility. The Planning Department continues to duck this issue. Various drafts of the draft EIR and Response to Comments contain verbal acrobatics in addressing and considering economic feasibility. The following timeline of excerpts from various drafts addressing the Metal Shed Reuse Alternative serves as an example (**bold font my emphasis**):

May 2015 / DEIR: *It is **unknown** if this alternative would meet the objective to develop a financially feasible project.*

August 2015 / DEIR: *The project sponsors contend that this alternative **would fail** to meet the objective to develop a financially feasible project.*

April 2016 / DEIR RTC: *As noted in the Draft EIR, the financial feasibility of the alternatives **has not been proven or disproven** through substantial evidence (Draft EIR pages S.24, VI.5, VI.13, and VI.34), though it was noted that the project sponsor contended the Metal Shed Reuse Alternative **would not be financially feasible** (VI.27). Page IV.27 of the Draft EIR is revised as follows to clarify that the alternative is considered to be **potentially feasible**: ....While the City considers this alternative to be **potentially feasible**, **The project sponsors contend that this alternative ~~would~~ could fail to meet the objective to develop a financially feasible project.***

As seen in the above editing revisions, the Planning Department (under pressure by the developers who were allowed to revise and edit various EIR/RTC drafts) is simply dodging an answer to the economic feasibility issue that would be resolved by further study and analysis. To date, no substantial evidence finding infeasibility of the Metal Shed Reuse Alternative has been provided/submitted even though the developer continues to assert the Metal Shed Reuse Alternative “would” or “could” be infeasible.

### **Inadequate & Inaccurate Study of Land Use (And Planning Policies Ignored)**

The DEIR and Response to Comments as well as City Planning’s previous environmental studies and projections for Potrero Hill fail to take into account a project of this scope at this site – including its impacts. Official environmental analysis currently on record in the Eastern Neighborhoods Plan considered heights of between 45 feet - 50 feet at the property, not 72 feet to 83 feet. There is deficient evidence that the density and height have been adequately or properly evaluated in prior environmental review by the City during the Eastern Neighborhoods EIR process (including in Comments and Responses to both the 901 16<sup>th</sup> Street /1200 17<sup>th</sup> Street and final Eastern Neighborhoods EIRs).

The proposed project remains inconsistent with many policies and principles of the Potrero Hill Area Plan. In accordance with the Showplace Square/Potrero Hill Area Plan policy calling for lowered heights on the south side of 16th Street, the underlying final Eastern Neighborhood EIR (which the 901 16<sup>th</sup> / 1200 17<sup>th</sup> Street EIR tiers off from) does address heights rising 65 feet to 68 feet – but only on the north side of 16<sup>th</sup> Street (not the south side of 16<sup>th</sup> where the proposed project is located). As cited in the Eastern Neighborhood EIR: “Height limits in the established residential areas of Potrero Hill would remain unchanged at 40 feet. The Preferred Project establishes heights of 65-68 feet within the core of Showplace Square between U.S. 101 and I-



280, north of 16<sup>th</sup> and south of Bryant Streets.”

Moreover, this 45 to 50 feet height and density were affirmed, codified and called for in the final Showplace Square/Potrero Hill. Objective 3.1/Policies 3.1.1 & 3.1.2 state: Adopt heights that respect, “the residential character of Potrero Hill.” “Respect the natural topography of Potrero Hill .... Lowering heights from the north to the south side of 16th Street would help accentuate Potrero Hill.”

### **Area Plan, City Policy Objectives & Principles Ignored**

The DEIR and Response to Comments remain inadequate and inaccurate because they fail to adequately consider that the proposed project conflicts with the Showplace Square / Potrero Area Plan, and the Urban Design and Housing Elements of the City’s General Plan, by disregarding policies of preserving neighborhood scale and character, providing adequate infrastructure, and preserving PDR uses. Both the Corovan development project and the DEIR/Response to Comments fail to adequately address the following consistency issues:

#### **A. Objective 3 of the San Francisco General Plan’s Urban Design Element:**

“Moderation of major new development to complement the city pattern, the resources to be conserved, and the neighborhood environment.”

The scale and density of the Prado/Walden project are substantially greater than existing surrounding Potrero Hill land uses and the project would be inconsistent with the established land use character of the neighborhood. The DEIR and Response to Comments fail to acknowledge and consider that the Daggett Triangle development at 1010 16<sup>th</sup> Street in Showplace Square, as well as other large developments in nearby Mission Bay, are in separate and distinct neighborhoods that are not part of the Corovan site in Potrero Hill.

#### **B. Objectives of the Showplace Square / Potrero Area Plan:**

The Prado/Walden project conflicts with a number of Area Plan objectives including Objective 1.2, which promotes development in keeping with neighborhood character. This project is inconsistent with the established neighborhood character of Potrero Hill. The Showplace Square/Potrero Hill Area Plan, in Policy 3.1.6, states that, “new buildings should epitomize the best in contemporary architecture, but should do so with a full awareness of, and respect for, the height, mass, articulation and materials of the best of the older buildings that surrounds them.” As proposed, the project’s 16<sup>th</sup> Street building fails to match the height, mass, and articulation of existing buildings in the Potrero Hill vicinity and provides little awareness of surrounding neighborhood structures.

#### **C. Policy 2 of the City’s General Plan: “That existing housing and neighborhood character be conserved and protected in order to preserve the cultural and economic diversity of our neighborhoods.”**

The Prado/Walden project is not consistent with this policy because scale, mass, bulk and height are inconsistent with and will negatively impact established neighborhood development pattern and character. The proposed development is dramatically out of scale with nearby residences and



small businesses.

### **Aesthetics – Not Adequately Addressed or Analyzed**

As noted above, the scale, height, and density of the proposed project (72 feet to 83 feet and 395 residential units) remain inconsistent with numerous terms set out in the Showplace / Potrero Hill Area Plan. This was not adequately addressed in the DEIR and Response to Comments. Prior study contained in the Eastern Neighborhoods Environmental Impact Report, produced and relied upon by City Planning for all new development, is now eight years old and did not properly and adequately evaluate, analyze, consider or anticipate a project of the size, height, or density proposed by the developer at the Corovan location. In fact, all of the analyses completed for the Eastern Neighborhoods EIR anticipated a height on the Corovan parcel of 45 feet to 50 feet – not 72 feet to 83 feet as proposed by the developer.

The developer's drawings indicate 72' to 83' high mechanical/stair/elevator penthouses that push the building heights well above the 68 feet height limit. These penthouses only serve to enable private views via access to amenity rooftop decks; they should not be credited as legitimate open space. The developer's proposed project and penthouses will also contribute to obscuring a cherished landmark of Potrero Hill – scenic public views of downtown San Francisco. This conflicts with long-standing city and state policies regarding protection of public scenic vistas. Even though the general public and decision-makers rely on an EIR for primary source information to make informed decisions about a project, the Planning Department has failed to provide a robust analysis of aesthetic impacts. City Planning ignored calls to provide accurate and adequate computer generated 3-D modeling visual simulations on the impacts of the project (including stair, elevator, mechanical penthouses) to public scenic views of downtown. The visual simulations offered in the DEIR remain inadequate and highly misleading. The DEIR and Response to Comments do not adequately address the above issues.

### **Inconsistent with Showplace / Potrero Hill Area Plan On Respecting Public View Corridors**

"Respect Public View Corridors", Policy 3.1.5 of the Showplace Square/Potrero Hill Area Plan states: "San Francisco's natural topography provides important way finding cues for residents and visitors alike, and views towards the hills or the bay enable all users to orient themselves vis-à-vis natural landmarks. Further, the city's striking location between the ocean and the bay, and on either side of the ridgeline running down the peninsula, remains one of its defining characteristics and should be celebrated by the city's built form."

By proposing a single massive structure at the base of Potrero Hill the developers completely ignore the natural environment surrounding the site. The height, bulk, and mass, of their project will undermine (and in some cases destroy) Potrero Hill's visual integration with downtown. The significant impacts on aesthetics including public views have not been adequately or properly evaluated in the DEIR and Response to Comments, and have been ignored by the developers.

### **Inadequacy of Addressing Cumulative Impacts**

Recent analysis shows the 395 units proposed for 901 16<sup>th</sup> / 1200 17<sup>th</sup> project will result in the Potrero Hill / Showplace Square area exceeding the number of housing units the City planned and projected for 2025. Under its Preferred Project Option approved in 2008, the Planning



Department planned for up to 3,180 housing units built by 2025 in the Potrero Hill / Showplace Square area. But as of February 2016, recent City Planning analysis shows 3,315 units already in the pipeline or built. The City failed to anticipate the dramatic pace of development and has not delivered on its promise to provide necessary public improvements (parks, transit, roads, etc.) to support thousands of new residents. City Planning analysis understates the “cumulative impacts” of large developments throughout Potrero Hill/Showplace Square by continuing to rely on outdated data from the 2008 Eastern Neighborhoods Environmental Impact Report to inform analysis in the EIRs of large projects, including the proposed 901 16<sup>th</sup> / 1200 17<sup>th</sup> Street development. Assumptions and mitigation measures provided in that document are simply no longer valid. The DEIR and Response to Comments do not adequately address cumulative impacts.

### **Loss of PDR**

The project would eliminate 109,500 square feet of PDR space. The DEIR and Response to Comments inadequately address the pace of PDR loss and the need for greater diversity of uses (other than residential) in the proposed project. The project should be revised to include light PDR / trade spaces.

### **Historic Resource**

Collectively, the Potrero Hill industrial complex at 901 16<sup>th</sup> / 1200 17<sup>th</sup> Streets contains the last remaining structures of the Pacific Rolling Mill, which began operating in the Central Waterfront in 1868 before reorganizing and relocating to Potrero Hill in the early 1900s. The buildings are also the last remaining extant structures of the merged companies, Judson-Pacific Company (1928), and Judson-Pacific-Murphy Company (1945) in San Francisco.

Recently, members of the San Francisco Historic Preservation Commission applauded the DEIR’s “Metal Shed Reuse Alternative” and recommended the project sponsor incorporate more of it into a final design – a recommendation the developer has continued to ignore (see 9/21/15, HPC letter to Sarah B. Jones, SF Planning Department).

The DEIR, Response to Comments, and developers fail to address and propose mitigations that would avoid significant and negative impacts due to mass and scale upon the *entire* building site (which includes the metal warehouses) and environment in accordance with the Secretary of Interiors standards.

Among many other reasons noted by architectural historian Katherine Petrin, the metal steel buildings should be added as historic resources because the 1,200 square foot red-brick office building alone insufficiently conveys the historic significance of the Pacific Rolling Mill site. Moreover, Petrin made other determinations of historic integrity and merit that have been inadequately addressed or ignored by the DEIR and Response to Comments including the following: 1) a continuity of heritage existed through various mergers of the Potrero Hill company 2) association with important or significant persons other than the company’s original founder, Patrick Noble 3) expanded Period of Significance (POS) to 1946/1947. For these and the other above reasons, the DEIR and Response to Comments remain inadequate.



### **Transportation / Circulation – Inadequacies and Inaccuracies:**

1. The change in traffic study impact methodology from LOS to VMT distorts and minimizes real and significant traffic congestion impacts. Thus these impacts are inadequately addressed and mitigated in the DEIR and Response to Comments. Moreover, the change in study methodology to VMT occurred after the DEIR comment period for 901 16<sup>th</sup> Street closed and thus without adequate opportunity for public comment.

2. Under Response TR-8, page RTC.69 in the Response to Comments:

*... neither 17th Street nor Mississippi Street are identified as routes having significant truck traffic and San Francisco does not otherwise designate "Truck Routes."*

This is simply inaccurate. Mississippi to 17<sup>th</sup> is a designated truck route that is heavily used by trucks – especially trucks exiting off or entering I-280. Mariposa Street between Connecticut and Mississippi Street is a restricted truck route (no vehicles over three tons). There is signage on Mariposa at Mississippi Streets stating: "Truck Route" directing large trucks to turn North onto Mississippi. The EIR does not adequately address significant impacts of the proposed project regarding truck traffic.

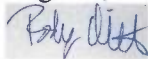
3. The EIR and Response to Comments do not address impacts of the project related to SFMTA's proposal to place a commuter Shuttle stop at the 17<sup>th</sup> and Mississippi Street intersection.

### **Exceptions Should Be Rejected**

The Project Sponsor seeks waivers or exceptions for the following: 1) Rear Yard 2) Parking 3) Horizontal Massing. These exception requests are unnecessary and improper, and, if granted, would contribute to an inferior and poorly designed project.

For all of the above reasons, we respectfully urge you to support and insist on the reasonable modifications called for by the community. I would be happy to discuss this matter with you at your convenience ahead of the May 12<sup>th</sup> hearing – my phone and email are listed below.

Regards,



Rodney Minott  
On behalf of Save The Hill  
[rodneyminnott@outlook.com](mailto:rodneyminnott@outlook.com)  
(415) 407-7115



# BUILD A GATEWAY, NOT A WALL!

901 16th Street @ Mississippi



Before Proposed Development



After Proposed Development



Recipient: City of San Francisco - Walden Development - and The Prado Group, John Rahaim, Christopher Thomas, Malia Cohen, Wade Wietgreffe, Chris Townes, Andrea Bruss, Rodney Fong, Dennis Richards, Michael Antonini, Rich Hillis, Christine Johnson, Kathrin Moore, Cindy Wu, Eric Mar, Mark Farrell, Aaron Peskin, Katy Tang, London Breed, Jane Kim, Norman Yee, Scott Wiener, David Campos, John Avalos, Sarah Jones, and Devyani Jain

Letter: Greetings,

Build A Gateway, Not A Wall!

The City of San Francisco must protect Potrero Hill from the unacceptable impacts of the proposed 901 16th Street project, a 616,000 square foot development spanning Mississippi and Missouri Streets east-west and 16th and 17th Streets north-south. The developer's current plan fails to treat this site as a special "gateway" into Potrero Hill. Instead, the developer's current plan will:

\*Worsen Traffic. The City's own studies forecast crippling impacts from traffic and parking congestion created by this project, including more than 12,000 daily trips by people (thousands by cars) and spillover demand for more than 458 parking spots on surrounding neighborhood streets.

\*Degrade Neighborhood Character and Livability. The developer's project is completely out of scale with the neighborhood and violates multiple Area Plan principles including provisions to "respect the natural topography of Potrero Hill", to lower building "heights from the north to south side of 16th Street", to "promote preservation of other buildings and features that provide continuity with past development," and to "ensure that infill development is compatible with its surroundings." The project will dramatically and detrimentally change the character of the Hill and intensify the Mission Bay-type development already encroaching upon our neighborhood.

\*Worsen Density Without Adding More Infrastructure. Research shows our community has already far exceeded its projected share of new housing development while the City has failed to deliver badly needed public infrastructure such as adequate public open space, schools and community centers, transit, and traffic control.



\*Threaten Famed Neighborhood Nightclub Bottom of The Hill. Adding multiple housing units on 17th Street will threaten the survival of famed nightclub Bottom of the Hill due to frivolous noise complaints.

\*Destroy Historically Significant Buildings. Signature features and materials of the existing metal warehouses, an industrial complex that at one time housed the largest steel-producing factory on the West Coast, would be lost forever due to demolition.

We entreat the developers and City leaders to revise 901 16th Street to:

- \* Reduce height, scale, and massing; reduce or eliminate eyesore rooftop mechanical/elevator/stair penthouses.

- \* Increase publicly accessible open space by widening the proposed pedestrian promenade to at least 40 feet.

- \* Reuse distinctive materials and features of the current metal warehouses in order to create visual and historic continuity between old and new uses at the site.

- \* Reduce parking and traffic congestion by shrinking the project.

- \* Replace housing on 17th Street with more diverse commercial uses such as space for artists, makers, non-profit groups, and neighborhood serving retail (all of which would help shield the adjacent Bottom of The Hill nightclub from unnecessary noise complaints).

- \* Prohibit formula retail within the development.



# Comments

| Name                | Location          | Date       | Comment  |
|---------------------|-------------------|------------|--|
| Rodney Minott       | San Francisco, CA | 2016-03-30 | The scale and height of this proposed project remain far too big – particularly the buildings on 16th Street. If approved, the 16th Street buildings will in effect create a giant wall at the base of Potrero Hill. In addition, the project developers and City planners are ignoring widespread community concerns about severe impacts on traffic and neighborhood character. Publicly accessible open space should also be increased as part of this development. Keep Potrero Hill livable and redesign this proposed project!   |
| David Goldenberg    | San Francisco, CA | 2016-03-31 | Is there nothing better we can do with the space in Potrero than continue to build monolithic residential buildings?   |
| Cathryn Blum        | San Francisco, CA | 2016-03-31 | The rampant growth in the southeast quadrant of the City is overwhelming, and needs to be curtailed or modified to maintain the quality of life in the area. Additional infrastructure in the form of better transportation for the entire area needs to exist before any more exceedingly large projects are given the green light. Yes, we need more housing in SF. But not at the expense of those who are already calling it home, please! We appreciate your taking into consideration the whole impact of such a large project, and sincerely hope it will be built to honor the history of the neighborhood in scope and scale. |
| Patricia Gleeson    | San Francisco, CA | 2016-03-31 | As a concerned San Franciscan and Hill resident, responding to the overdevelopment in our area that creates traffic congestion and will increase the parking problems blocks from the proposed project The proposed project is ugly. Look for comparison to the project built at corner of 7th and 16th.   |
| Gayle Keck          | San Francisco, CA | 2016-03-31 | I'm not against progress - but it should make the neighborhood MORE livable, not less. Let's have a project the entire neighborhood can be proud of, one that's an example of progressive development respectful of design, open space and environment!  |
| yvonne gahre        | San Francisco, CA | 2016-03-31 | The proposed construction project is grossly inappropriate for our neighborhood due to its huge mass. The proposal would create long shadows, wind tunnels & increased density. It doesn't add anything positive but would have many negative consequences including traffic congestion, pollution & general chaos.<br>This project must be downsized! Otherwise it is an insult to creative innovation & an embarrassment to the field of architecture.   |
| Jim Wilkins         | san francisco, CA | 2016-03-31 | It is truly astonishing to see the city overrun by the greed of developers and their enablers in city hall. These developments are being rammed down the throats of the citizens with little regard to their quality of life; i.e. no parks, no traffic abatement, no provision for adequate public transit. The proposed development at the corner of 16th and 7th streets represents yet another example of the above. This area is already choked with traffic; the city has no answer to this problem. The Board of Supervisors needs to step in to control the Planning Department's unchecked authority in these matters.        |
| Kathy Pagan Quadros | Fremont, CA       | 2016-03-31 | I believe we need careful and thoughtful development on Potrero Hill, where long term residents and developers come together to work on a plan that all will support. We do not need massive development in our neighborhood - please retain the feel and character of our neighborhood.   |



| Name              | Location          | Date       | Comment   |
|-------------------|-------------------|------------|---|
| tommy cinquegrano | san francisco, CA | 2016-03-31 | The city and neighborhood need additional housing...there's no denying that...but this development squanders a rare and amazing chance to utilize such a unique and historical structure in an interesting way. I'm asking the developer to please take advantage of the existing architectural language and the fact that this is essentially the gateway to potrero hill to create something memorable and respectful.  |
| lucy farey-jones  | San Francisco, CA | 2016-03-31 | This project is way too large for the neighborhood that is already overrun with massive developments  |
| Denise Hill       | SF, CA            | 2016-03-31 | this is my home and I want to preserve this neighborhood.   |
| Nancy Anding      | San Francisco, CA | 2016-03-31 | ease rethink the Corocan project-such an opportunity missed if the proposed density goes through. Good design and Planning does make a difference! Thank you  |
| Ron Kurash        | San Francisco, CA | 2016-03-31 | This proposed project for the Corovan site is just another example of a project that is out of scale and ignores wishes of the people that actually live in Potrero Hill. There is an opportunity to re use an existing historically important building, one that will add to the uniqueness of the area yet the efforts of the developers seem to ignore this and are aiming for yet another chance to maximize profit devoid of any sensibility or concern for the character of the area. |
| Gennie Fermin     | San Francisco, CA | 2016-03-31 | There is so much traffic and congestion already! Just imagine how much worse it will be during and after construction. Nightmare!!!!  |
| Peter Delacorte   | San Francisco, CA | 2016-03-31 | This is an extremely important issue to me and my family. There's little about this project to like. Especially ominous is the increased congestion it will bring to this soon-to-be overstuffed neighborhood.  |
| Susan Zuckman     | San Francisco, CA | 2016-03-31 | Too much building without considering the neighborhood or infrastructure.   |
| Raymond Kerr      | San Francisco, CA | 2016-04-01 | Potrero/South of Market are becoming a soulless wall of condos. The qualities that make this city special are being rapidly eroded by rampant development that strains resources, alienates neighbors, and creates monstrous traffic impacts.   |
| marjorie Hill     | sf, CA            | 2016-04-01 | Out of control building that is destroying our whole wonderful City   |
| Diana Bunimovitz  | San Francisco, CA | 2016-04-01 | Potrero hill has seen huge growth in the past few years, with huge condo developments , enough already, streets and traffic are already unable to sustain   |
| Kitty Quinn-Friel | San Francisco, CA | 2016-04-01 | I am a long-term resident of Potrero Hill and I am very much concerned about the city over-developing this area, especially in terms of traffic, inadequate and outdated Mariposa Street freeway entrance and exit, not to mention the increased parking congestion.  |
| Mary Renner       | San Francisco, CA | 2016-04-01 | The proposed building is too high and massive.  |
| Christina Quiroz  | SAN FRANCISCO, CA | 2016-04-01 | As a longtime resident, I'm concerned about the unbridled growth on PH. We need to be thoughtful and visionary in our plans.  |
| d b               | sf, CA            | 2016-04-01 | It's too big for humans. We're not supposed to live and work like this.   |
| David Dawson      | San Francisco, CA | 2016-04-01 | I work in Potrero Hill, I am here everyday and do not want the neighborhood compromised because of some developers greed.   |
| noelle maylander  | SAN FRANCISCO, CA | 2016-04-01 | I'm signing because this was my home for 7 years and the neighborhood in which I loved and lived in would be changed forever if this wall went of cement went up...for the worst.   |
| Carrie Kahn       | San Francisco, CA | 2016-04-01 | Potrero Hill is a true neighborhood and not a part of the high density city. Local services do not support high density residences and we see no future plans to do so.   |



| Name               | Location          | Date       | Comment  |
|--------------------|-------------------|------------|--|
| Sean Angles        | San Francisco, CA | 2016-04-01 | This ugly oversized proposal will ruin Potrero Hill historical character and negatively impact our quality of life. I support the neighborhood's exciting reuse alternative "Community Gateway" Proposal proposed by Save The Hill. Renderings can be found at <a href="http://www.savethehill.com" rel="nofollow">http://www.savethehill.com</a> . Urge the Planning Commission to order a 'time out' halt to this poor proposal until cumulative negative impacts by overdevelopment are assessed and mitigated.   |
| Susan Backman      | San Francisco, CA | 2016-04-01 | I'm disheartened by the destruction of so many San Francisco neighborhoods. There is no thoughtful planning that considers the people and structures that currently populate the city's small enclaves. Wanton development destroys the quality of life for those who cannot afford the "new" city and drives out any semblance of neighborhood and small businesses that already thrive there.  |
| Arcadia Smails     | San Francisco, CA | 2016-04-01 | This project is one of many huge developments being shoe-horned into Potrero Hill and its immediate vicinity without adequate consideration to the severe CUMULATIVE impacts these projects - taken as a whole - will have on the livability, character, and infrastructure of the neighborhood. The draft Environmental Impact Report for this project predicts this development will have a severe impact on traffic along already clogged streets leading to and from the Mariposa Street I-280 exit, and the DEIR was published in isolation from the snowballing traffic impacts of the upcoming Warriors development, and other large projects planned for the area. I implore City planners and supervisors to take a close look at the *reality* of the current development pipeline in the Potrero Hill neighborhood! Please balance the need for additional housing with the need for additional infrastructure, open space, and community benefits to go along with it. |
| Kate Kelly         | San Francisco, CA | 2016-04-01 | I want to keep Potrero Hill livable!   |
| Kepa Askenasy      | San Francisco, CA | 2016-04-01 | This project should not only benefit the developers' pocketbook but the community at large with arts and other community amenities.  |
| Cornelis van Aalst | San Francisco, CA | 2016-04-01 | It is too large and totally out of character with the surrounding neighborhood.  |
| John Martin        | San Francisco, CA | 2016-04-02 | I live blocks from this proposed structure. The neighborhood is losing its character and too dense population.   |
| Sebra Leaves       | San Francisco, CA | 2016-04-02 | No Walls on Potrero Hill.  |
| David Wurtman      | San Francisco, CA | 2016-04-02 | Developers should not be allowed to harm neighborhoods just because they have money or own a piece of property. Walden Development is an ironic name because Thoreau, who put Walden Pond on the map, would roll over in his grave if he knew of the insensitive, oversized, light-blocking, hideous, out of place development Josh and his cronies are trying to build. These same people got it wrong with Kaiser and they still have it wrong. This city belongs to all of us.  |
| Katharine Minott   | Aptos, CA         | 2016-04-02 | Be courageous City Planners! Do more than dully follow the lead of a developer's team and forsake an opportunity to respect the character and scale of the neighborhood and to sensitively treat this unique location as a meaningful "gateway" into Potrero Hill.   |
| Rennie Lum         | Sf, CA            | 2016-04-02 | This will congest that area so badly   |
| Racine Christensen | San Francisco, CA | 2016-04-02 | This is a terrible idea and ruins a beautiful neighborhood with a lot of character!  |



| Name              | Location          | Date       | Comment   |
|-------------------|-------------------|------------|---|
| Alison Heath      | SF, CA            | 2016-04-02 | This project will be the largest we've seen on Potrero Hill, with nearly 400 residential units and a footprint larger than 1010 Potrero (aka Dagger). There are nearly 4000 new units expected on Potrero Hill in the next few several years, well beyond what was anticipated in the Eastern Neighborhoods Plan. Yet we have none of the community benefits or the promised infrastructure improvements. As these so-called Urban Mixed Use (but primarily residential) developments fill our landscape we lose the opportunity for "complete" neighborhoods with amenities, jobs and services. What we are seeing instead is becoming less a neighborhood and more of a vertical suburb. Sad... |
| Mark Smallcombe   | San Francisco, CA | 2016-04-02 | I want to keep Potrero Hill a true neighborhood - not a Walmart style bedroom community / vertical suburb   |
| Leslie Myrick     | Berkeley, CA      | 2016-04-02 | I'm signing this as a concerned grandmother of a toddler who is growing up on the Hill.   |
| Richard Rockwell  | San Francisco, CA | 2016-04-03 | I'm interested in the future of Potrero Hill  |
| kathleen owen     | oakland, CA       | 2016-04-03 | We would love more retail on 17th St.   |
| Michael Gemignani | San Francisco, CA | 2016-04-03 | The current proposal is not in keeping with preserving, or adding space for medium to small manufacturing shops. Although it proposes preserving the small Corovan warehouse and raised galvanize tank. The size and number of apartments will embarrassingly shadow these historic icons and show how little the planning department cares about preserving the neighborhood's character. It will cut off the traffic passageway between my business and downtown with the increased congestion it will cause. There are already numerous developments in the works which should suffice the need for housing. What is needed are shop spaces for small businesses.                              |
| David Boyd        | San Francisco, CA | 2016-04-03 | This gargantuan project is a pending disaster for our Potrero Hill neighborhood. Simply TOO BIG TO ALLOW!   |
| Marti McKee       | San Francisco, CA | 2016-04-03 | I don't want to lose the character of Potrero Hill. The project is simply too big.  |
| wendy macnaughton | san francisco, CA | 2016-04-03 | i'm all for building new housing, but considering the artists/small arts businesses who were evicted (me included) there needs to be some arts/non-profits/something remotely cultural incorporated into the development, that considers and contributes something more than bodies and traffic to the neighborhood and to the city.  |
| Zoe Becker        | San Francisco, CA | 2016-04-03 | The character of our city really matters !!   |
| Rebecca Muzquiz   | San Francisco, CA | 2016-04-03 | I am a lifetime resident of Potrero Hill since 1966. We do not need or want anymore complexes added to block our beautiful views and worsen up the neighborhood traffic.  |
| Alilda Ferraro    | San Francisco, CA | 2016-04-03 | I am a Potrero Hill resident and am concerned about the mega developments such as this one by developers who only care about money and zero regard for building to suit the character of this neighbor and the lack of infrastructure that will lead to horrible traffic and congestion.  |
| Carol Sundell     | San Francisco, CA | 2016-04-03 | The design is not character with the neighborhood. The start design has no connection with the neighborhood it is not conducive to families, it not conducive to extending the Potrero Hill community. The out of town developer has turned a deaf ear to the concerns of the neighborhood and the city of San Francisco is not supporting its own neighborhood.  |



| Name            | Location          | Date       | Comment   |
|-----------------|-------------------|------------|---|
| Sandra Cortez   | San Francisco, CA | 2016-04-03 | I'm signing because I am tired of all of these major developers building these outrageously huge complexes not only in the Potrero Hill area but all over the City. Hard working people who were able to afford a house with a view and who have lived on the hill for many years are quickly losing the beautiful views of the bay and City. You are destroying the character of SF's neighborhoods. And to say you are creating more "affordable " housing is a joke. Quit building walls around the neighborhoods! |
| David Smith     | San Francisco, CA | 2016-04-03 | There is already too much density in the neighborhood. The neighborhood needs services for the new apartments and condos already being constructed, not more housing.   |
| Lynka Adams     | San Francisco, CA | 2016-04-03 | We are not Manhattan. Skyscrapers do not belong on Potrero Hill.  |
| Barbara Bradley | Pacifica, CA      | 2016-04-03 | This project is a monster with its 7 stories high. My neighbors are really not NIMBYs with regards to housing. We ask for scaled down versions.   |
| Olga Kist       | San Francisco, CA | 2016-04-04 | More housing is not affordable housing. The planning department is rubber stamping anything with all exceptions using the need for housing to enrich developers and destroy the character of San Francisco. It is a monstrous wall that boxes in our topography and the hills.  |
| Doug Bartlett   | San Francisco, CA | 2016-04-04 | This is a ridiculous scale for any building near Potrero Hill.  |
| Richard Shapiro | San Francisco, CA | 2016-04-04 | Livability and infrastructure are essential to city life.   |
| Henry Jackson   | San Francisco, CA | 2016-04-04 | I'm signing because I've been a 20 year tenant at the Noonan and a major supporter in my local community.   |
| a ma            | san francisco, CA | 2016-04-04 | we need character, not a wall. Awful design too large scale is massive, square and imposing. Need stepped back design and more articulation.  |
| Claudia DeCicco | Vienna, VA        | 2016-04-04 | I grew up in the Bay Area. Let's save the neighborhoods!  |
| Robin Evans     | San Francisco, CA | 2016-04-04 | This amounts to runaway development that will essentially create a new neighborhood at the bottom of Potrero Hill that overwhelms everything already there.   |
| Cameron Tuttle  | Oakland, CA       | 2016-04-04 | SF is losing all of its heart, soul, and character due, in part, to monster developments like this one.   |
| Marcy Fraser    | San Francisco, CA | 2016-04-04 | This is ugly and unnecessary.   |
| Richard Hutson  | San Francisco, CA | 2016-04-05 | I'm signing the petition because the 16th Street elevation is too massive and uninteresting; the project has too many units and inadequate off street parking; it will severely exacerbate traffic congestion at an intersection that is already almost impassible at various times during the day.   |
| matthwe rogers  | San Francisco, CA | 2016-04-05 | <p>Dear Planning Commission,</p> <p>Why is this kind of thing allowed?</p> <p>What about:</p> <ul style="list-style-type: none"> <li>• infrastructure</li> <li>• Open Space</li> <li>• Livability</li> </ul> <p>Is San Francisco only to be a whore for outside capital?</p> <p>Say No to Ed Lee and his cronies.</p>   |



| Name                    | Location           | Date       | Comment  |
|-------------------------|--------------------|------------|--|
| Christine Ponelle       | San Francisco, CA  | 2016-04-05 | This proposed project is a WALL not a thought-out project that fits into the existing neighborhood.<br>It deeply saddens me to see what San Francisco is allowed to become.<br>City Planning should be sensitive and visionary with respect and understanding for the surroundings and the people who live there now and in the future.<br>Stop this insanity! The infrastructure is already buckling. |
| Patricia Reischl Crahan | San Francisco, CA  | 2016-04-05 | We need no more monstrosities like this proposal...respect the Potrero Hill neighborhood.  |
| Rachel Leibman          | Montclair, NJ      | 2016-04-05 | There are too many huge, ugly developments!  |
| Gina Orofino            | San Francisco, CA  | 2016-04-05 | I am a homeowner within several blocks of the Corovan site who feels this project will negatively impact the QOL in our community by degrading the livability of this area of the city and will impact the beauty of the skyline for all residents and visitors alike.   |
| Robert Poyas            | san francisco, CA  | 2016-04-05 | There is already too much development in the area and it is changing the character of the neighborhood.  |
| Laura Crahan            | San Francisco, CA  | 2016-04-06 | I believe in height and size limits on buildings, especially in residential areas so that they may continue to be liveable spaces.   |
| Rebecca Ronsaville      | San Francisco, CA  | 2016-04-06 | This project doesn't sustain the character of the hill and instead seeks to profit at residents expense. Please reconsider and build smaller units such as 2 story townhouses which are prolific on the hill.  |
| Renee Tan               | San Franciscoc, CA | 2016-04-06 | This project does not align with the goals of a livable city. It is too big, will impact traffic and quality of life for all.  |
| Stephen Berke           | Palo Alto, CA      | 2016-04-07 | We live in Potrero Hill half the time.   |
| Hajdeja Ehline          | San Francisco, CA  | 2016-04-07 | This is just rediculously over built and again what's the infrastructure regarding the bloating of this neighborhood. There is no thought and consideration regarding this.  |
| Laura Sydell            | SAN FRANCISCO, CA  | 2016-04-07 | This building will destroy the character of Potrero Hill. I'm not opposed to new housing, but I am opposed to unattractive housing that harms the charm and historical character of a neighborhood.  |
| Ivan Uranga             | San Francisco, CA  | 2016-04-07 | Keep the neighborhood special. No sky monsters.  |
| Marta Camer             | San Francisco, CA  | 2016-04-07 | I am a native San Franciscan and I am starting to not recognize the city I have lived in all my life.... Which is over 40 years.   |
| Linda Hoxit-Almeida     | Sacramento, CA     | 2016-04-07 | 2220 23RD AVE  |
| hiroschi yoshida        | San Francisco, CA  | 2016-04-07 | No more building!!! What is this city doing to accommodate the already horrific traffic??!! Housing is being built everywhere in this neighborhood! Do I nee to show you on Google Maps to see what is begin built within within our neighborhood?   |
| Michael Thede           | San Francisco, CA  | 2016-04-07 | This city is too crowded<br>This city is losing its charm<br>Developers are greedy bastards<br>I could go on forever   |
| susan miller            | san francisco, CA  | 2016-04-07 | enough is enough   |
| peter linenthal         | san francisco, CA  | 2016-04-07 | This building is just much too big for that lot making it out of character with the neighborhood. I live two blocks from the project and do not want the boxy Mission Bay style advancing into Potrero Hill.   |
| Sean Regent             | San Francisco, CA  | 2016-04-07 | Too DENSE!   |



| Name               | Location          | Date       | Comment  |
|--------------------|-------------------|------------|--|
| Rick Hall          | San Francisco, CA | 2016-04-07 | This project is terrible. Potrero is seeing a constant march of these projects invading from the north. Soon the whole East side of San Francisco will be one big ugly homogeneous extension of Mission Bay. Developer and political greed is destroying San Francisco. We do not need the explosive growth being forced on us and sold to us as "this is what millennials want". These are temporary bedroom communities for newcomers until they grow up and leave. We need housing for families and long term residents. Stop this project and stop destroying San Francisco's character. Too Much, Too Fast, and all for Greed   |
| Connie Yannacone   | San Francisco, CA | 2016-04-07 | Because I am a long term resident of Potrero Hill and I don't want to see the neighborhood ruined with many massive structures and congested traffic.  |
| Jude Smith         | Dallas, TX        | 2016-04-07 | I grew up here, and after returning home to visit family, was appalled at what developers are being allowed to do to our beloved city. Shame on the mayor and the city planning commission.  |
| Sergio Raya        | San Francisco, CA | 2016-04-07 | Too much condensed housing is sprouting up and no ample road ways to accomodate any new residents with vehicles therefore creating more smog while sitting in traffic. Not to mention the new Warriors arena is by far gonna exceed traffic levels I.  |
| Henrik Kam         | San Francisco, CA | 2016-04-07 | Too blocky, too tall and too ugly!   |
| Daria Iaconi       | San Francisco, CA | 2016-04-07 | Walls are not the answer.  |
| Lael Robertson     | SF, CA            | 2016-04-07 | I love the hill.   |
| Xander Walbridge   | San Francisco, CA | 2016-04-07 | Watching the city I was born and raised in and still live crumble in front of my eyes is making me sick  |
| Karen Larsen       | San Francisco, CA | 2016-04-08 | Potrero Hill is a wonderful place. It has already changed and we want to stop more development which would ruin this lovely neighborhood.  |
| Richard Drechsler  | San Francisco, CA | 2016-04-08 | Preserving quality of life in our neighborhoods is the only thing that will keep them intact and the city solvent. S.F. is now constructing the conditions that promote urban flight.  |
| Andrea Devries     | San Francisco, CA | 2016-04-08 | Potrero hill is a beautiful neighborhood, it's a shame to change so much so fast. Please Consider a compromise keeping the character Potrero hill.   |
| Julie Trachtenberg | San Francisco, CA | 2016-04-08 | Mission bay is already a monolithic massing of buildings that do not create human scale spaces.  |
| Pierce Gould       | San Francisco, CA | 2016-04-08 | The 16th Street side of this building looks megalithic and monotonous just like the building (Potrero 1010) being completed on the opposite side of the street (which is a design disaster). The height and width of the development is wrong for Potrero Hill, and the building facade looks like a wall from the 16th Street side. It creates a canyon affect on 16th Street. The 16th Street side of this project should be brought into line with what's planned for the 17th Street side - which has more visual interest and is a slightly better scale. Why can't a large building facade give the appearance of multiple smaller buildings to make this seem more personable and less like like an office? Bring some character to these buildings, and make them fit with the neighborhood. |
| Glenn DeSandre     | Menlo Park, CA    | 2016-04-08 | I am not opposed to development but it needs to be sensitive to and preserve the existing character of its surroundings. One of the things that makes SF great is the character/feel of its neighborhoods. This proposal also cuts us further off from the rest of the city by erecting a massive wall between the Hill and the rest of the City.  |



| Name            | Location          | Date       | Comment   |
|-----------------|-------------------|------------|---|
| inga larsen     | San Francisco, CA | 2016-04-08 | I have lived on Potrero Hill for 43 years and I want to help to keep it a beautiful neighborhood for my children and theirs.  |
| Lori Tenny      | San Francisco, CA | 2016-04-08 | This behemoth is completely out of sync with the character of the neighborhood.   |
| gavin murphy    | san francisco, CA | 2016-04-08 | This project is just wrong for our neighborhood. It's too huge, makes traffic in this mostly residential neighborhood worse, will make parking much much more difficult.  |
| Tina Lindinger  | San Francisco, CA | 2016-04-09 | Neighborhood context and scale is extremely important. This development is unacceptable and I'm usually someone who is all for progress.  |
| Donna Madrid    | Fremont, CA       | 2016-04-09 | I'm 4th generation Fremont and it's disgusting to see all the new high density housing going up in San Francisco. Politicians and developers honestly don't give proper consideration to the huge additional traffic and the effect extra cars and additional population have on the carbon footprint they leave in a beautiful historical city like San Francisco.   |
| judith courtney | Guttenberg, NJ    | 2016-04-09 | I lived in SF for 25 years and remember potrero hill as a marvelous people-sized community. Please keep it that and keep mass/crass urbanization out!   |
| John O'Rourke   | Mountain view, CA | 2016-04-09 | We own adjacent property  |
| Walker Bass     | San Francisco, CA | 2016-04-11 | I believe in new housing, but think that the Prado/Walden team could create something iconic and architecturally sensational, instead of simple and massive.  |
| Robin Roth      | San Francisco, CA | 2016-04-12 | We cannot in any way handle the additional traffic. Draw the line at this monstrous behemoth.   |
| Cynthia Navarro | San Francisco, CA | 2016-04-14 | Potrero hill is a wonderful neighborhood and this proposed development will dramatically change the flavor of the neighborhood and make it crowded, noisy and polluted.   |
| Thomas Kennett  | Carmel Valley, CA | 2016-04-15 | I've lived in SF off and on for almost 50 years. This project, as presented, is obscene and defamatory of the Potrero Hills neighborhood. Any SF official who signs off on it should be summarily dismissed. As for the developers, go back to the drawing board and come up with a plan that adds beauty and balance. If you can't, piss off.  |
| Philip Schwartz | San Francisco, CA | 2016-04-17 | It's way past time to make quality of life and neighborhood character real priorities when assessing proposed development, especially massive projects like this.   |
| Mica Ringel     | san francisco, CA | 2016-04-17 | Large Scale development in this location is inappropriate.  |
| Marjorie Hill   | San Francisco, CA | 2016-04-17 | Keep our neighborhood the way it's meant to be with homes of working class people.  |
| Gustavo Leao    | San Francisco, CA | 2016-04-17 | Please don't allow a monster wall to be build in our neighborhood!  |
| Bill Reque      | San Francisco, CA | 2016-04-17 | 619 Connecticut Street  |
| Mark Smallcombe | San Francisco, CA | 2016-04-17 | History will deeply regret the decisions by our city officials to turn Potrero Hill into an ugly bedroom community (without real people community) by the monstrous and out of scale developments they are allowing to change the true authentic SF neighborhood uniqueness and attractiveness of our city and especially Potrero Hill. The Potrero Hill neighborhood is about homes - not walmart style structures that forever change the landscape and tenure of our SF treasure. Traffic, parking, noise, eventual crime and all that goes with overcrowding and lack of infrastructure makes these plans a no deal |



| Name                | Location          | Date       | Comment  |
|---------------------|-------------------|------------|--|
| Richard Hutson      | San Francisco, CA | 2016-04-18 | I agree with the comments and statements that have already been made. And I want to point out that the Draft EIR for this project "admits" that it will create spill-over demand for 458 parking spaces and proposes absolutely nothing to mitigate this nightmare. The City will be adding insult to injury if this project is approved without solving this and the other deficiencies that have been noted.   |
| Maria Cristini      | San Francisco, CA | 2016-04-18 | Potrero Hill was a village where everyone knew each other. Now with all the density, we are simply cranky about loss of visibility, crazy traffic. Our SF neighborhood has been hit harder than any other and it's all for greed and money in the pockets of the city and developers. STOP over populating Potrero Hill - it's a small hill and not meant to be a down town metropolis. Why isn't this happening in the Marina? Why are their voices heard and not ours. On Potrero Hill we get to love the homeless and the developers simultaneously. The greed and homeless situation shows what the City leaders value but for those of us paying property taxes living on Potrero Hill over 25 years -- that is NOT our values or why we live here. That is not the value of most living here. Stop the dense buildings, they do not belong in this small neighborhood. |
| Rachel Strickland   | San Francisco, CA | 2016-04-18 | I live here already and observe that plenty of what's valuable and has made the neighborhood vital for long is getting gutted for the interests of development. It's obvious that even in a capitalist regime, some specific infrastructure planning (such as services, industry, artists and artisans, mobility, parking, public transit, urban forest) need precede the entire relocation and condensation of Silicon Valley to San Francisco, and unchecked proliferation of new dwelling units.  |
| Sebra Leaves        | San Francisco, CA | 2016-04-18 | No more removal of PDR space. We need to adhere to the 1:1 replacement standard.   |
| Richard Berkowitz   | San Francisco, CA | 2016-04-18 | It would obliterate the topography of the Potrero Hill neighborhood and destroy the unique character of Potrero Hill's terrain.  |
| Jane Nicholson      | San Francisco, CA | 2016-04-18 | The size and height of this development are too large and too tall for the neighborhood. It does not respect either the scale or character of Potrero Hill. It has ignored the intentions of the neighborhood groups that have tried to work with the developer. Having the monstrosity that they have proposed sends a poor message about Potrero Hill.   |
| Franklin Cariffe    | San Francisco, CA | 2016-04-21 | Out of scale for our hill.   |
| Sandra Cook         | San Francisco, CA | 2016-04-21 | I agree, it's too massive!   |
| Rina WEISMAN        | San Francisco, CA | 2016-04-21 | I live on the hill. I don't want to be TRAPPED on the hill because I can't drive out, or take a bus downtown, or be unable to shop or eat locally on the hill. It's horrible enough already that what was a lovely view of the downtown and Bay is now mostly buildings at the Mission Bay development. For heaven's sake, rein in the greed and lack of forethought of these developers already!!!  |
| mike smith          | San Francisco, CA | 2016-04-21 | because i care   |
| Molly Lazarus       | San Francisco, CA | 2016-04-21 | This building will adversely effect the character of Potrero Hill as it is too tall and too massively big.   |
| Frank Joseph Butler | San Francisco, CA | 2016-04-22 | The metal sheds are a significant and important structure to the City, Region and State.<br>The Pacific Rolling Mills fabricated steel here that built San Francisco and California. The sheds should be reused, not demolished.   |
| Bill Reque          | San Francisco, CA | 2016-04-22 | This entire area should be zoned for single family housing only. No large apartment construction!  |



| Name            | Location               | Date       | Comment  |
|-----------------|------------------------|------------|--|
| Diane Merlino   | San Francisco, CA      | 2016-04-22 | Please stop allowing so much massive development on Potrero Hill. We understand there is a housing crisis and more building needs to be done, but what is being allowed, including this new project at 901 16th Street, is way out of character with the scope of development on the hill, especially given the 5 or 6 other projects already underway or recently completed. Please respect the neighborhood and the people who live here at least as much as the developers. |
| Helen Adam      | Côte Saint-Luc, Canada | 2016-04-23 | Potrero Hill has been my home for about 30 years. I love this neighborhood. I want to help preserve its unique character.  |
| sabrina Brennan | Moss Beach, CA         | 2016-04-23 | I love Potrero Hill.   |
| Emiley Ginn     | San Francisco, CA      | 2016-04-26 | This city is losing its soul.  |
| Jim Billings    | San Francisco, CA      | 2016-04-30 | The SF Planning Commission is giving too many exemptions, ignoring the character the neighborhoods, and not taking into account current and future traffic conditions when it is approving these giant housing projects. The Corovan project needs to be significantly downsized. We don't need another high-priced eyesore at the base of Potrero Hill.<br>Thank you, Jim Billings  |
| Matthew Boden   | San Francisco, CA      | 2016-05-01 | I own a home here and I'm passionate about maintaining the integrity and character of Potrero Hill.  |
| Cindy Michael   | San Francisco, CA      | 2016-05-03 | I don't want any more big developments in our neighborhood   |



# Signatures

| <b>Name</b>            | <b>Location</b>                  | <b>Date</b> |
|------------------------|----------------------------------|-------------|
| Rodney Minott          | San Francisco, CA, United States | 2016-03-29  |
| David Goldenberg       | San Francisco, CA, United States | 2016-03-31  |
| Cathryn Blum           | San Francisco, CA, United States | 2016-03-31  |
| Catherine Wayland      | San Francisco, CA, United States | 2016-03-31  |
| Susan McDevitt         | San Francisco, CA, United States | 2016-03-31  |
| Anna Stern             | San Francisco, CA, United States | 2016-03-31  |
| Ann Brinkley           | San Francisco, CA, United States | 2016-03-31  |
| patricia gleeson       | San Francisco, CA, United States | 2016-03-31  |
| Gayle Keck             | San Francisco, CA, United States | 2016-03-31  |
| Yvonne Gavre           | San Francisco, CA, United States | 2016-03-31  |
| jim wilkins            | San Francisco, CA, United States | 2016-03-31  |
| Kathy Pagan Quadros    | San Francisco, CA, United States | 2016-03-31  |
| Tommy Cinquegrano      | San Francisco, CA, United States | 2016-03-31  |
| Ergin Guney            | San Francisco, CA, United States | 2016-03-31  |
| Damien Shulock         | San Francisco, CA, United States | 2016-03-31  |
| Susan West             | San Francisco, CA, United States | 2016-03-31  |
| Stephanie Murri        | Fremont, CA, United States       | 2016-03-31  |
| lucy farey-jones       | San Francisco, CA, United States | 2016-03-31  |
| Joyce Lavey            | San Francisco, CA, United States | 2016-03-31  |
| Maureen Oddone         | San Francisco, CA, United States | 2016-03-31  |
| Denise Hill            | San Francisco, CA, United States | 2016-03-31  |
| Shunya Anding          | San Francisco, CA, United States | 2016-03-31  |
| Rabea Amin             | San Francisco, CA, United States | 2016-03-31  |
| Christina Marcet       | San Francisco, CA, United States | 2016-03-31  |
| Kasha Maznio           | San Francisco, CA, United States | 2016-03-31  |
| Will Scoggin           | San Francisco, CA, United States | 2016-03-31  |
| Ramona Downey          | San Francisco, CA, United States | 2016-03-31  |
| Audra Morse            | San Francisco, CA, United States | 2016-03-31  |
| Ron Kurash             | San Francisco, CA, United States | 2016-03-31  |
| Gennie Fermin-Leerkamp | San Francisco, CA, United States | 2016-03-31  |



| <b>Name</b>       | <b>Location</b>                         | <b>Date</b> |
|-------------------|---|-------------|
| Peter Delacorte   | San Francisco, CA, United States        | 2016-03-31  |
| nicole jacobson   | san francisco, CA, United States        | 2016-03-31  |
| Barbara Angeli    | San Francisco, CA, United States        | 2016-03-31  |
| Ruth Miller       | San Francisco, CA, United States        | 2016-03-31  |
| Randall Whitehead | San Francisco, CA, United States        | 2016-03-31  |
| Susan Zuckman     | San Francisco, CA, United States        | 2016-03-31  |
| Waiyde Palmer     | San Francisco, CA, United States        | 2016-03-31  |
| Polly Kennedy     | San Francisco, CA, United States        | 2016-03-31  |
| Raymond Kerr      | San Francisco, CA, United States        | 2016-04-01  |
| Richard Eusey     | San Francisco, CA, United States        | 2016-04-01  |
| Roberto Bonilla   | San Francisco, CA, United States        | 2016-04-01  |
| Will Sloan        | Longboat Key, FL, United States         | 2016-04-01  |
| sharon beals      | san Franciisco, CA, United States       | 2016-04-01  |
| marjorie Hill     | San Francisco, CA, United States        | 2016-04-01  |
| talmadge Talmadge | San Francisco, CA, United States        | 2016-04-01  |
| Victoria Hall     | San Francisco, CA, United States        | 2016-04-01  |
| Frixos Michael    | San Francisco, CA, United States        | 2016-04-01  |
| Kaitlin Kushner   | San Francisco, CA, United States        | 2016-04-01  |
| Dan Lyon          | San Francisco, CA, United States        | 2016-04-01  |
| Brenda Beebe      | San Francisco, CA, United States        | 2016-04-01  |
| fred rinne        | san francisco, CA, United States        | 2016-04-01  |
| ashley wessinger  | san francisco, CA, United States        | 2016-04-01  |
| Diana Bunimovitz  | San Francisco, CA, United States        | 2016-04-01  |
| rick salazar      | san francisco, ca, United Arab Emirates | 2016-04-01  |
| Adam Teitelbaum   | San Francisco, CA, United States        | 2016-04-01  |
| Concerned Citizen | New City, NY, United States             | 2016-04-01  |
| Patricia Atlas    | San Francisco, CA, United States        | 2016-04-01  |
| Kitty Quinn-Friel | San Francisco, CA, United States        | 2016-04-01  |
| Mary Renner       | San Francisco, CA, United States        | 2016-04-01  |
| christina quiroz  | San Francisco, CA, United States        | 2016-04-01  |
| Dean Bellerby     | San Francisco, CA, United States        | 2016-04-01  |
| David Dawson      | San Francisco, CA, United States        | 2016-04-01  |



| <b>Name</b>        | <b>Location</b>                  | <b>Date</b> |
|--------------------|----------------------------------|-------------|
| noelle maylander   | Seattle, WA, United States       | 2016-04-01  |
| Carrie Kahn        | San Francisco, CA, United States | 2016-04-01  |
| Yoram Meroz        | San Francisco, CA, United States | 2016-04-01  |
| JEAN MAKANNA       | SAN FRANCISCO, CA, United States | 2016-04-01  |
| Jacob Weisman      | San Francisco, CA, United States | 2016-04-01  |
| Wendy Beyer        | Atlanta, GA, United States       | 2016-04-01  |
| David Rittenhouse  | San Francisco, CA, United States | 2016-04-01  |
| Shannon Stevenson  | San Francisco, CA, United States | 2016-04-01  |
| Michael Agor       | San Francisco, CA, United States | 2016-04-01  |
| paul magnuson      | San Francisco, CA, United States | 2016-04-01  |
| Sean Angles        | San Francisco, CA, United States | 2016-04-01  |
| Susan Backman      | San Francisco, CA, United States | 2016-04-01  |
| Arcadia Smails     | San Francisco, CA, United States | 2016-04-01  |
| christine kristen  | san francisco, CA, United States | 2016-04-01  |
| Tiffany Hill       | San Francisco, CA, United States | 2016-04-01  |
| Katherine Kelly    | San Francisco, CA, United States | 2016-04-01  |
| Jani Mussetter     | San Francisco, CA, United States | 2016-04-01  |
| kepa askenasy      | San Francisco, CA, United States | 2016-04-01  |
| Cornelis van Aalst | San Francisco, CA, United States | 2016-04-01  |
| Roberts Kent       | Brisbane, CA, United States      | 2016-04-01  |
| Caldin Seides      | San Francisco, CA, United States | 2016-04-01  |
| John Martin        | San Francisco, CA, United States | 2016-04-02  |
| Richard Dwyer      | San Francisco, CA, United States | 2016-04-02  |
| Sebra Leaves       | San Francisco, CA, United States | 2016-04-02  |
| Dave Nelson        | San Francisco, CA, United States | 2016-04-02  |
| David Wurtman      | San Francisco, CA, United States | 2016-04-02  |
| Cambria Minott     | San Francisco, CA, United States | 2016-04-02  |
| KATHARINE MINOTT   | San Francisco, CA, United States | 2016-04-02  |
| Rennie Lum         | San Francisco, CA, United States | 2016-04-02  |
| Jamie Putnam       | San Francisco, CA, United States | 2016-04-02  |
| Susan Marsh        | San Francisco, CA, United States | 2016-04-02  |
| Sue Nuccio         | Syracuse, NY, United States      | 2016-04-02  |



| <b>Name</b>                               | <b>Location</b>                  | <b>Date</b> |
|---|----------------------------------|-------------|
| Leslie Pitts                              | Chandler, AZ, United States      | 2016-04-02  |
| Nikki Clancy                              | San Francisco, CA, United States | 2016-04-02  |
| Joseph O'Connell III                      | San Francisco, CA, United States | 2016-04-02  |
| holly friedman<br>holly@hollyfriedman.com | San Francisco, CA, United States | 2016-04-02  |
| john Alper                                | Bridgewater, CT, United States   | 2016-04-02  |
| Racine Christensen                        | San Francisco, CA, United States | 2016-04-02  |
| erika delacorte                           | San Francisco, CA, United States | 2016-04-02  |
| Cynthia Wigginton                         | San Francisco, CA, United States | 2016-04-02  |
| Alison Heath                              | San Francisco, CA, United States | 2016-04-02  |
| Mark Smallcombe                           | San Francisco, CA, United States | 2016-04-02  |
| John Clancy                               | San Francisco, CA, United States | 2016-04-02  |
| Leslie Myrick                             | Berkeley, CA, United States      | 2016-04-02  |
| Richard Rockwell                          | San Francisco, CA, United States | 2016-04-03  |
| Kathleen Owen                             | San Francisco, CA, United States | 2016-04-03  |
| matthew steen                             | San Francisco, CA, United States | 2016-04-03  |
| Colleen Kelly                             | Redwood City, CA, United States  | 2016-04-03  |
| Joyce Book                                | San Francisco, CA, United States | 2016-04-03  |
| Michael Gemignani                         | San Francisco, CA, United States | 2016-04-03  |
| Mara Iaconi                               | San Francisco, CA, United States | 2016-04-03  |
| DAVID BOYD                                | San Francisco, CA, United States | 2016-04-03  |
| Marti McKee                               | San Francisco, CA, United States | 2016-04-03  |
| wendy macnaughton                         | san francisco, CA, United States | 2016-04-03  |
| Zoe Becker                                | San Francisco, CA, United States | 2016-04-03  |
| Lori Nunokawa                             | San Francisco, CA, United States | 2016-04-03  |
| David Page                                | San Francisco, CA, United States | 2016-04-03  |
| philip cravens                            | san francisco, CA, United States | 2016-04-03  |
| Rebecca Muzquiz                           | San Francisco, CA, United States | 2016-04-03  |
| Bill Carpenter                            | San Francisco, CA, United States | 2016-04-03  |
| Alilda Ferraro                            | San Francisco, CA, United States | 2016-04-03  |
| Carol Sundell                             | San Francisco, CA, United States | 2016-04-03  |
| Sandra Cortez                             | San Francisco, CA, United States | 2016-04-03  |



| <b>Name</b>             | <b>Location</b>                  | <b>Date</b> |
|-------------------------|----------------------------------|-------------|
| Vicente Agor            | San Francisco, CA, United States | 2016-04-03  |
| David Smith             | San Francisco, CA, United States | 2016-04-03  |
| Lynka Adams             | San Francisco, CA, United States | 2016-04-03  |
| Barbara A Bradley       | San Francisco, CA, United States | 2016-04-03  |
| John O'Boyle            | San Francisco, CA, United States | 2016-04-04  |
| Olga Kist               | San Francisco, CA, United States | 2016-04-04  |
| Doug Bartlett           | San Francisco, CA, United States | 2016-04-04  |
| Richard Shapiro         | San Francisco, CA, United States | 2016-04-04  |
| David Magnusson         | San Francisco, CA, United States | 2016-04-04  |
| Henry Jackson           | San Francisco, CA, United States | 2016-04-04  |
| Christine McCutcheon    | Fremont, CA, United States       | 2016-04-04  |
| Nancy Spittler          | Lafayette,, CA, United States    | 2016-04-04  |
| Andrea Alfonso          | San Francisco, CA, United States | 2016-04-04  |
| Marilyn Moore           | Moraga, CA, United States        | 2016-04-04  |
| Claudia Decicco         | Vienna, VA, United States        | 2016-04-04  |
| Robin Evans             | San Francisco, CA, United States | 2016-04-04  |
| Cameron Tuttle          | San Francisco, CA, United States | 2016-04-04  |
| Sherrie Groshong        | San Francisco, CA, United States | 2016-04-04  |
| Philip Anasovich        | San Francisco, CA, United States | 2016-04-04  |
| Marcy Fraser            | San Francisco, CA, United States | 2016-04-04  |
| Richard Hutson          | San Francisco, CA, United States | 2016-04-05  |
| Matthew Rogers          | San Francisco, CA, United States | 2016-04-05  |
| Andrea Davidson         | San Francisco, CA, United States | 2016-04-05  |
| Christine Ponelle       | San Francisco, CA, United States | 2016-04-05  |
| Patricia Reischl Crahan | San Francisco, CA, United States | 2016-04-05  |
| Jacqueline Thompson     | San Francisco, CA, United States | 2016-04-05  |
| Paul Garfinkel          | San Francisco, CA, United States | 2016-04-05  |
| Tamara Radler           | San Francisco, CA, United States | 2016-04-05  |
| Rachelvenu Leibman      | San Francisco, CA, United States | 2016-04-05  |
| Martha Rust             | Berkeley, CA, United States      | 2016-04-05  |
| Gina Orofino            | San Francisco, CA, United States | 2016-04-05  |
| Robert Poyas            | san francisco, CA, United States | 2016-04-05  |



| <b>Name</b>         | <b>Location</b>                  | <b>Date</b> |
|---------------------|----------------------------------|-------------|
| Bindu damani        | San Francisco, CA, United States | 2016-04-05  |
| Leslie Castellanos  | San Francisco, CA, United States | 2016-04-06  |
| jay schumann        | san francisco, CA, United States | 2016-04-06  |
| Laura Crahan        | San Francisco, CA, United States | 2016-04-06  |
| Rebecca Ronsaville  | San Francisco, CA, United States | 2016-04-06  |
| Karyn Campbell      | San Francisco, CA, United States | 2016-04-06  |
| Victoria Lau        | San Francisco, CA, United States | 2016-04-06  |
| Renee Tan           | San Francisco, CA, United States | 2016-04-06  |
| Mike Crahan         | San Francisco, CA, United States | 2016-04-06  |
| Meg Hilgartner      | San Francisco, CA, United States | 2016-04-06  |
| Stephen Berke       | Palo Alto, CA, United States     | 2016-04-07  |
| Hajdeja Ehline      | San Francisco, CA, United States | 2016-04-07  |
| Kathe Hashimoto     | San Francisco, CA, United States | 2016-04-07  |
| Laura Sydell        | San Francisco, CA, United States | 2016-04-07  |
| Ken nagahara        | Oakland, CA, United States       | 2016-04-07  |
| Michelle Homme      | San Francisco, CA, United States | 2016-04-07  |
| Ivan Uranga         | San Francisco, CA, United States | 2016-04-07  |
| Marta Camer         | San Francisco, CA, United States | 2016-04-07  |
| karen mussette      | beverly hills, CA, United States | 2016-04-07  |
| Linda Hoxit-Almeida | Sacramento, CA, United States    | 2016-04-07  |
| Hans Kwiotek        | San Francisco, CA, United States | 2016-04-07  |
| Spencer Mussetter   | San Diego, CA, United States     | 2016-04-07  |
| hiroshi yoshida     | San Francisco, CA, United States | 2016-04-07  |
| Michael Thede       | San Francisco, CA, United States | 2016-04-07  |
| Dr Seumas Coutts    | San Francisco, CA, United States | 2016-04-07  |
| susan miller        | san francisco, CA, United States | 2016-04-07  |
| Kadidja Sallak      | San Francisco, CA, United States | 2016-04-07  |
| Carrie Leeb         | San Francisco, CA, United States | 2016-04-07  |
| peter linenthal     | san francisco, CA, United States | 2016-04-07  |
| Jim Mackey          | San Francisco, CA, United States | 2016-04-07  |
| Sean Regent         | San Francisco, CA, United States | 2016-04-07  |
| Heidi Ehrich        | Burlingame, CA, United States    | 2016-04-07  |



| <b>Name</b>         | <b>Location</b>                  | <b>Date</b> |
|---------------------|----------------------------------|-------------|
| Michael Rowell      | San Francisco, CA, United States | 2016-04-07  |
| Rebekah Engel       | San Francisco, CA, United States | 2016-04-07  |
| Denise Klarquist    | San Francisco, CA, United States | 2016-04-07  |
| Rick Hall           | San Francisco, CA, United States | 2016-04-07  |
| Noel Harmonson      | San Francisco, CA, United States | 2016-04-07  |
| Shannon Assad       | San Francisco, CA, United States | 2016-04-07  |
| Dianne Catechi      | Sonoma, CA, United States        | 2016-04-07  |
| Kelly Lamb          | San Francisco, CA, United States | 2016-04-07  |
| Kalman Varga        | San Francisco, CA, United States | 2016-04-07  |
| Connie Yannacone    | San Francisco, CA, United States | 2016-04-07  |
| Dan Feldman         | San Francisco, CA, United States | 2016-04-07  |
| Ana Fonseca         | San Francisco, CA, United States | 2016-04-07  |
| Linda McKay         | San Francisco, CA, United States | 2016-04-07  |
| Jude Smith          | Dallas, TX, United States        | 2016-04-07  |
| Michael Cipresso    | San Francisco, CA, United States | 2016-04-07  |
| Philip Schwartz     | San Francisco, CA, United States | 2016-04-07  |
| Sergio Raya         | San Francisco, CA, United States | 2016-04-07  |
| Matt Litchfield     | San Francisco, CA, United States | 2016-04-07  |
| Matthew Fountain    | San Francisco, CA, United States | 2016-04-07  |
| Otter Kohl          | San Francisco, CA, United States | 2016-04-07  |
| Henrik Kam          | San Francisco, CA, United States | 2016-04-07  |
| Emily Goldenberg    | Sacramento, CA, United States    | 2016-04-07  |
| Daria Iaconi        | San Francisco, CA, United States | 2016-04-07  |
| Lael Robertson      | SF, CA, United States            | 2016-04-07  |
| Xander Walbridge    | San Francisco, CA, United States | 2016-04-07  |
| Luca Iaconi-Stewart | San Francisco, CA, United States | 2016-04-07  |
| karen capraro       | San Francisco, CA, United States | 2016-04-07  |
| shaina lerner       | Oakland, CA, United States       | 2016-04-07  |
| Lisa Chanoff        | San Francisco, CA, United States | 2016-04-07  |
| Dominic Martello    | San Francisco, CA, United States | 2016-04-07  |
| David Stewart       | San Francisco, CA, United States | 2016-04-07  |
| August Fischer      | San Francisco, CA, United States | 2016-04-07  |



| <b>Name</b>        | <b>Location</b>                  | <b>Date</b> |
|--------------------|----------------------------------|-------------|
| Carolee Wheeler    | San Francisco, CA, United States | 2016-04-07  |
| cynthia hobbs      | Oakland, CA, United States       | 2016-04-07  |
| Greg Tietz         | San Francisco, CA, United States | 2016-04-07  |
| Karen Larsen       | San Francisco, CA, United States | 2016-04-08  |
| Lanie Abano        | San Francisco, CA, United States | 2016-04-08  |
| Reid Spice         | San Francisco, CA, United States | 2016-04-08  |
| Richard Drechsler  | San Francisco, CA, United States | 2016-04-08  |
| Andrea De Vries    | San Francisco, CA, United States | 2016-04-08  |
| Barbara Wyeth      | San Francisco, CA, United States | 2016-04-08  |
| Dale Scott         | San Francisco, CA, United States | 2016-04-08  |
| Julie Trachtenberg | San Francisco, CA, United States | 2016-04-08  |
| nicolle rodriguez  | los angeles, CA, United States   | 2016-04-08  |
| David Rees         | Woodford, VT, United States      | 2016-04-08  |
| Avery McGinn       | San Francisco, CA, United States | 2016-04-08  |
| Tee Minit          | San Francisco, CA, United States | 2016-04-08  |
| Petrin Petrin      | San Francisco, CA, United States | 2016-04-08  |
| Pierce Gould       | San Francisco, CA, United States | 2016-04-08  |
| Maryann Faricy     | San Francisco, CA, United States | 2016-04-08  |
| Glenn DeSandre     | San Francisco, CA, United States | 2016-04-08  |
| inga larsen        | San Francisco, CA, United States | 2016-04-08  |
| Maya Owings        | Oakland, CA, United States       | 2016-04-08  |
| Iris Feldman       | San Francisco, CA, United States | 2016-04-08  |
| James Salazar      | San Francisco, CA, United States | 2016-04-08  |
| Marilyn Curry      | San Francisco, CA, United States | 2016-04-08  |
| gavin murphy       | San Francisco, CA, United States | 2016-04-08  |
| Sheldon Trimble    | San Francisco, CA, United States | 2016-04-08  |
| Carter Murphy      | San Francisco, CA, United States | 2016-04-08  |
| Erik Bower         | San Francisco, CA, United States | 2016-04-08  |
| Mauro Iaconi       | San Francisco, CA, United States | 2016-04-09  |
| Tina Lindinger     | San Francisco, CA, United States | 2016-04-09  |
| Jan Lenci          | Fremont, CA, United States       | 2016-04-09  |
| Donna Madrid       | Fremont, CA, United States       | 2016-04-09  |



| <b>Name</b>             | <b>Location</b>                  | <b>Date</b> |
|-------------------------|----------------------------------|-------------|
| Keith Jackson           | Fremont, CA, United States       | 2016-04-09  |
| judith courtney         | West New York, NJ, United States | 2016-04-09  |
| O'Rourke John           | Mountain View, CA, United States | 2016-04-09  |
| Katherine McCarthy      | San Francisco, CA, United States | 2016-04-10  |
| Andrea Lucich           | Pacifica, CA, United States      | 2016-04-10  |
| Teresa (Cruz) CARNS     | Ruidoso, NM, United States       | 2016-04-11  |
| Joan Lyons              | San Francisco, CA, United States | 2016-04-11  |
| Travers Flynn           | San Francisco, CA, United States | 2016-04-11  |
| Walker Bass             | San Francisco, CA, United States | 2016-04-11  |
| marty mcdonough         | san francisco, CA, United States | 2016-04-11  |
| Robin Roth              | San Francisco, CA, United States | 2016-04-12  |
| Betty Maddux            | San Francisco, CA, United States | 2016-04-12  |
| Jeffrey Dauber          | San Francisco, CA, United States | 2016-04-13  |
| Cynthia Navarro         | San Francisco, CA, United States | 2016-04-14  |
| Paul Herman             | San Francisco, CA, United States | 2016-04-14  |
| Thomas Kennett          | Carmel Valley, CA, United States | 2016-04-15  |
| Marlayne Morgan         | San Francisco, CA, United States | 2016-04-15  |
| Gina Silverman          | San Francisco, CA, United States | 2016-04-15  |
| Faith Sedlin            | San Francisco, CA, United States | 2016-04-17  |
| Sarah Glicken           | San Francisco, CA, United States | 2016-04-17  |
| Elizabeth Nager         | San Francisco, CA, United States | 2016-04-17  |
| Mica Ringel             | San Francisco, CA, United States | 2016-04-17  |
| Pascual & Carmen Castro | San Francisco, CA, United States | 2016-04-17  |
| russell zeidner         | San Francisco, CA, United States | 2016-04-17  |
| david edwards           | San Francisco, CA, United States | 2016-04-17  |
| Bill Reque              | San Francisco, CA, United States | 2016-04-17  |
| Lon Shapiro             | San Francisco, CA, United States | 2016-04-17  |
| Maria Cristini          | San Francisco, CA, United States | 2016-04-18  |
| dean dinelli            | San Francisco, CA, United States | 2016-04-18  |
| Erika Coplon            | San Francisco, CA, United States | 2016-04-18  |
| Rachel Strickland       | San Francisco, CA, United States | 2016-04-18  |
| Richard Berkowitz       | San Francisco, CA, United States | 2016-04-18  |




| <b>Name</b>       | <b>Location</b>                  | <b>Date</b> |
|-------------------|----------------------------------|-------------|
| Jane Nicholson    | San Francisco, CA, United States | 2016-04-18  |
| Joseph Nolan      | San Francisco, CA, United States | 2016-04-18  |
| Janine Firpo      | San Francisco, CA, United States | 2016-04-19  |
| Chris Tachiki     | San Francisco, CA, United States | 2016-04-20  |
| Gallagher Nancy   | San Francisco, CA, United States | 2016-04-21  |
| COLLEEN BYRNE     | San Francisco, CA, United States | 2016-04-21  |
| Marina Luderer    | San Francisco, CA, United States | 2016-04-21  |
| Franklin Cariffe  | San Francisco, CA, United States | 2016-04-21  |
| Christine Chao    | SACRAMENTO, CA, United States    | 2016-04-21  |
| Connie Lee        | San Francisco, CA, United States | 2016-04-21  |
| Robin Scholle     | san francisco, CA, United States | 2016-04-21  |
| Tara Mark         | San Francisco, CA, United States | 2016-04-21  |
| Sandra Cook       | San Francisco, CA, United States | 2016-04-21  |
| Samantha Luks     | San Francisco, CA, United States | 2016-04-21  |
| Kathryn Hannay    | Moss Landing, CA, United States  | 2016-04-21  |
| Dan Redmond       | San Francisco, CA, United States | 2016-04-21  |
| Rina WEISMAN      | San Francisco, CA, United States | 2016-04-21  |
| mike smith        | San Francisco, CA, United States | 2016-04-21  |
| Eric Poulson      | San Francisco, CA, United States | 2016-04-21  |
| Suryanshu Prakash | San Francisco, CA, United States | 2016-04-21  |
| Molly Lazarus     | San Francisco, CA, United States | 2016-04-21  |
| Madelyn Stewart   | Daly City, CA, United States     | 2016-04-21  |
| stephanie chao    | Placentia, CA, United States     | 2016-04-21  |
| John Rehling      | San Francisco, CA, United States | 2016-04-22  |
| f. joseph butler  | San Francisco, CA, United States | 2016-04-22  |
| Hugh Harsh        | San Francisco, CA, United States | 2016-04-22  |
| Pauline Bourbon   | San Francisco, CA, United States | 2016-04-22  |
| Diane Merlino     | San Francisco, CA, United States | 2016-04-22  |
| Elena Myers       | San Francisco, CA, United States | 2016-04-23  |
| Helen Adam        | San Francisco, CA, United States | 2016-04-23  |
| Sabrina Brennan   | Moss Beach, CA, United States    | 2016-04-23  |
| Sasha Gala        | San Francisco, CA, United States | 2016-04-23  |



| <b>Name</b>        | <b>Location</b>                  | <b>Date</b> |
|--------------------|----------------------------------|-------------|
| Joanna Curlee      | San Francisco, CA, United States | 2016-04-23  |
| Michael Econmy     | San Francisco, CA, United States | 2016-04-23  |
| Erin Lampe         | San Francisco, CA, United States | 2016-04-24  |
| Colleen McCarthy   | San Francisco, CA, United States | 2016-04-24  |
| Marie Nola         | San Francisco, CA, United States | 2016-04-25  |
| Kimetha Vanderveen | San Francisco, CA, United States | 2016-04-25  |
| Emiley Ginn        | San Francisco, CA, United States | 2016-04-26  |
| wendy hough        | San Francisco, CA, United States | 2016-04-28  |
| Kim Urbain         | San Francisco, CA, United States | 2016-04-28  |
| Jim Billings       | San Francisco, CA, United States | 2016-04-30  |
| Scott Simons       | San Francisco, CA, United States | 2016-05-01  |
| Matthew Boden      | San Francisco, CA, United States | 2016-05-01  |
| Cindy Michael      | San Francisco, CA, United States | 2016-05-03  |



From: **Allison Heath** [alisonheath@sbcglobal.net](mailto:alisonheath@sbcglobal.net)   
Subject: **901-16th Street - Grow Potrero Responsibly**  
Date: **May 1, 2016 at 7:48 PM**  
To: [planning@rodneymong.com](mailto:planning@rodneymong.com), **Dennis Richards** [dennis.richards@sfgov.org](mailto:dennis.richards@sfgov.org), [cwu.planning@gmail.com](mailto:cwu.planning@gmail.com), **Michael Antonini** [wordweaver21@aol.com](mailto:wordweaver21@aol.com), **Rich Hillis** [richhillisf@yahoo.com](mailto:richhillisf@yahoo.com), **Christine D.(CPC) Johnson** [christine.d.johnson@sfgov.org](mailto:christine.d.johnson@sfgov.org), [mooreurban@aol.com](mailto:mooreurban@aol.com)  
Cc: **Susan Brandt-Hawley** [susanbh@preservationlawyers.com](mailto:susanbh@preservationlawyers.com), **Rachel Mansfield-Howlett** [Rhowlett1aw@gmail.com](mailto:Rhowlett1aw@gmail.com), **Malia (BOS) Cohen** [Malia.Cohen@sfgov.org](mailto:Malia.Cohen@sfgov.org), **Andrea Bruss** [Andrea.Bruss@sfgov.org](mailto:Andrea.Bruss@sfgov.org), **John (CPC) Rahaim** [john.rahaim@sfgov.org](mailto:john.rahaim@sfgov.org), **Chris Townes** [Chris.Townes@sfgov.org](mailto:Chris.Townes@sfgov.org), **Thomas, Christopher (CPC)** [christopher.thomas@sfgov.org](mailto:christopher.thomas@sfgov.org), **Josh Smith** [jsmith@waldenddevelopment.com](mailto:jsmith@waldenddevelopment.com)

Dear Commissioners,

As you prepare for the 901-16th Street and 1200-17th Street hearing, I ask that you please consider that the residential component of this project will bring us beyond what was anticipated in the Showplace/Potrero Area Plan. In the context of unmitigated impacts, the potential to abandon all other objectives, for the sake of housing alone, should be taken very seriously.

This development also offers a tremendous opportunity and benefit if it is designed with the goal of a livable and complete neighborhood as the primary objective.

I've attached a letter which details some of our concerns.

As always, I am more than happy to discuss this with you via phone or email.

Best,  
Allison Heath  
415-412-2723



<http://www.alisonheath.com>  
[alisonheath@sbcglobal.net](mailto:alisonheath@sbcglobal.net)



May 1, 2016

Rodney Fong, Commission President  
Dennis Richards, Commission Vice President  
Cindy Wu, Commissioner  
Michael J. Antonini, Commissioner  
Rich Hillis, Commissioner  
Christine D. Johnson, Commissioner  
Kathrin Moore, Commissioner

Re: 901-16<sup>th</sup> Street / 1200-17<sup>th</sup> Street

Dear Commissioners:

On May 12 will be the hearing for the largest, and one of the most controversial, projects to be proposed for Potrero Hill. Covering 3.5 acres and serving as a gateway to the neighborhood, this development will define the area for decades to come.

The Eastern Neighborhoods Plan sought to balance the twin goals of providing housing, while preserving and growing a diverse economy:

*People and Neighborhoods:*

- 1) Encourage new housing at appropriate locations and make it as affordable as possible to a range of city residents*
- 2) Plan for transportation, open space, community facilities and other critical elements of complete neighborhoods*

*The Economy and Jobs:*

- 3) Reserve sufficient space for production, distribution and repair activities, in order to support the city's economy and provide good jobs for residents*
- 4) Take steps to provide space for new industries that bring innovation and flexibility to the city's economy (Showplace Square/Potrero Area Plan, p.viii)*

As project after project is approved, all the evidence shows that the Eastern Neighborhoods Plan, though clear in its objectives, never had the means to enforce its goals at a project-specific level. Furthermore, the City has failed to hold up its end of the bargain to plan for and ensure that large swaths of developable land were to be complete neighborhoods.

**Failure to Mitigate Cumulative and Project-Specific Impacts**

The Eastern Neighborhoods Plan promised, "...a full array of public benefits, to ensure the development of complete neighborhoods, including open space, improved public transit, transportation, streetscape improvements, community facilities, and affordable housing." Unfortunately the City has never provided most of the necessary infrastructure to support anticipated development, particularly in the context of cumulative growth.



Similarly, the San Francisco Housing Element requires that infrastructure needs be planned and coordinated to accommodate new development. Objective 12 specifically states that the City must *"balance housing growth with adequate infrastructure that serves the city's growing population"*.

The environmental analysis for this project relies on outdated analysis and fails to identify adequate mitigations of the impacts of this project, and the cumulative impacts of overbuilding, throughout the Showplace Square/Potrero Hill Area. The Eastern Neighborhoods PEIR "Preferred Project" that was approved by the Planning Commission and Board of Supervisors in 2008 allowed for 3180 residential units in the Showplace Square / Potrero Hill Area. Recent Planning Department analysis indicates that as of February 23, 2016 projects containing 3315 units in the Area have completed or are proposed to complete environmental review. This project, with 395 residential units, is the one that brings us over the top. As we have repeatedly said, the impacts of this project and others in the area are not being addressed. Impact fees do not come close to covering the costs, while the City has never identified the funding sources to provide the necessary infrastructure and community benefits promised to us in the Eastern Neighborhoods Plan.

### **Loss of PDR and the Need for Balance**

Potrero Hill and Showplace Square have already lost 60% of the PDR that was anticipated would be lost over 25 years. This project will displace 109,500 square feet of PDR. UMU zoning lacks the requirements for actual mixed uses, and as a result there is a stark overemphasis on residential development. We ask that the balance be shifted drastically and the project be redesigned to accommodate a large proportion of small businesses, neighborhood services, arts space, and a PDR and maker component.

### **Onsite Open Space and Pedestrian Promenade**

The Showplace Square/Potrero Hill Plan specifically stated that the area has *"comparatively little access to open space compared with the rest of the city and that the addition of new residents makes it imperative to provide more open space to serve both existing and new residents, workers and visitors."*

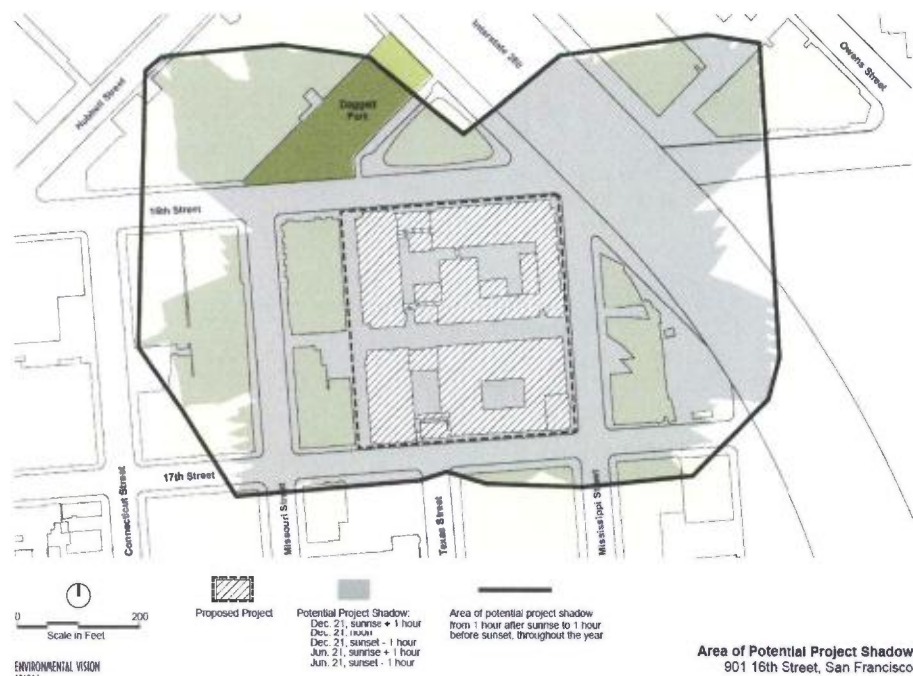
Given the size and significant impacts of the project, specific onsite mitigation measures to include more onsite open space should be included. Additional public and private areas with setbacks, plazas and expansion of courtyards, should be included to meet this need. In the proposed design, the public promenade along the western side of the building is 30 feet in places, but then narrows to as little as 22 feet where residential stoops are located. In order to activate the passageway and increase onsite open space and opportunities for gathering, this should be increased to at least 40 feet.

### **Shadowing of Daggett Park**

As noted above, the Showplace Square / Potrero Hill Area is already underserved in terms of open space. Any additional shadowing will compromise the neighborhoods limited recreational opportunities. Daggett Park is now a POPOS (Privately Owned Public



Open Space). As such, it falls under Section 147 of the Planning Code: *"New buildings and additions to existing buildings in C-3, South of Market Mixed Use, and Eastern Neighborhoods Mixed Use Districts where the building height exceeds 50 feet shall be shaped, consistent with the dictates of good design and without unduly restricting the development potential of the site in question, to reduce substantial shadow impacts on public plazas and other publicly accessible spaces other than those protected under Section 295. In determining the impact of shadows, the following factors shall be taken into account: The amount of area shadowed, the duration of the shadow, and the importance of sunlight to the type of open space being shadowed. Determinations under this Section with respect to C-3 Districts shall be made in accordance with the provisions of Section 309 of this Code. Determinations under this Section with respect to South of Market Mixed Use and Eastern Neighborhoods Mixed Use Districts shall be made in accordance with the provisions of Section 307 of this Code."*



Because the proposed project is over 50 feet tall, and adds net and cumulative shadow to Daggett Park, particularly from mid-fall to mid-winter, the design of north side of the building should be refined to reduce shadowing, through a reduction in height along 16<sup>th</sup> Street and setbacks. Additionally the rooftop mechanical structures should be designed to minimize shadow and reduce overall height.

### Support for the Metal Shed Alternative

The "Metal Shed Alternative" identified in the FEIR as the environmentally superior alternative, addresses all of the issues identified here. It provides a more balanced mix of uses, including additional open space, at a density that is notably less impactful than



the proposed project. Additionally, through adaptive reuse of buildings and materials, it honors the neighborhood's industrial past while providing a unique sense of place.

### **Additional Design Considerations**

While the Interim Design Controls provide broad guidance, we urge Planning to carefully review and revise the final design, whatever it may be, following the Urban Design Guidelines, currently in draft form. They provide excellent guidance in terms of things like rooftop design, scale and massing, and street level experience.

### **Hazardous Materials**

We ask that the same process for engagement with the neighbors on construction activities and hazardous materials that were a condition of approval for 1301-16<sup>th</sup> Street be followed for this project. This adds a layer of public notification and accountability that has been sorely lacking in the Maher Program.

### **Limit Residential Parking**

The requested exception for a .85 ratio on residential parking will bring more cars to the neighborhood and contribute to already intolerable congestion at key intersections. We need to plan for the 21<sup>st</sup> century and dramatically reduce the amount of onsite residential parking places, while maintaining the level of parking necessary for businesses to thrive in the area.

### **A Critical Juncture**

Potrero Hill is at the breaking point, facing a doubling of population, with minimal investment in community amenities and necessary infrastructure. Your decision is to continue to the point of no return and approve this project as proposed, or to take a deep breath and insist on a responsible development that will be a legacy and benefit to the neighborhood for years to come.

Thank you for your consideration.

Sincerely,

A handwritten signature in cursive script, appearing to read "Alison Heath".

Alison Heath  
For Grow Potrero Responsibly  
alisonheath@sbcglobal.net



## **Case 2011.1300E 901 16th Street & 1200 17th Street Corovan - Responses to Comments**

From: **Judith** (jci.cpuc@gmail.com)

Sent: Wed 5/04/16 12:06 AM

Re: Case 2011.1300E 901 16th Street & 1200 17th Street Corovan - Responses to Comments

May 4, 2016

To Planning Commission and Chris Thomas, Planning Commission Staff,

I continue to have concerns about the project as initially proposed by the project sponsor, Potrero Partners, LLC in the revised application of June 17, 2014.

1) It is against the character of the neighborhood. The Metal Shed Reuse Alternative would have fewer impacts and would be much more consistent with the underlying PEIR including the slope from 16th street and support of PDR. In addition, residential use next to a heavy rail corridor and entertainment venue may create problems in achieving adequate quiet to sleep and other essential life functions for residents of the actual project. No formula retail should be allowed and additional commercial or PDR should be added on 17th St.

2) It is situated near an existing entertainment venue "Bottom of the Hill" that provides frequent all ages shows and thus serves the community including youth. It also serves to provide space for young performers who are locked out of many venues due to age restrictions or cost.

3) Current construction in the neighborhood is inadequately monitored to safeguard residents and members of the community.

Wind Conditions create unhealthy levels of dust and construction activities are not properly monitored and controlled. This creates the potential for hazardous PM 10/PM 2.5 dust to be ingested or breathed in by passerby and residents. The soils in this area have long time industrial uses and contaminants in addition to natural hazards like serpentine minerals and fill with hazardous materials.

For example last week April 24-30, 2016 it was windy in Potrero Hill and SF.

Numerous measurements were above 20, 25 and it was even 30 miles per hour.

However the construction activities in Daggett "Open Space" created considerable dust and were not shut



down to control the dust. I observed some minimal watering the following week which seems to be related to breaking up macadam with a back hoe and likely was to prevent sparking with the metal shovel on hard tarmac and not dust control.

Per Website with Historic Weather for

SF: <http://www.timeanddate.com/weather/usa/san-francisco/historic>

Wind measurements above 15 MPH Week of April 24-30 2016 Measurements provided 4 times a day at Midnight, 6 AM, Noon and 6 PM.

4/24

Sunday 12 AM 17 MPH; 12 PM 35 MPH; 6 PM 34 MPH

4/25

Monday 12 AM 17 MPH; 12 PM 27 MPH; 6 PM 24 MPH

4/26

Tuesday 12 PM 25 MPH; 6 PM 25 MPH

4/27

Wednesday 12 PM 32 MPH; 6 PM 25 MPH

4/28

Thursday 12 PM 26 MPH; 6 PM 21 MPH

4/29

Friday 12 PM 30 MPH; 6 PM 28 MPH

4/30

Saturday 12 AM 16 MPH; 12 PM 21 MPH; 6 PM 16 MPH



In addition although environmental documents, indicate construction trucks should not block traffic after 7 AM, the trucks at the Daggett project are routinely doing so.

Although this is not about the adjacent project the lack of adequate oversight and enforcement of existing mitigation measures in the programmatic EIR and project documents, diminishes the communities confidence that the Mitigation Measures are adequate or will be vigorously enforced in order to protect the community members nearby.

It would be useful to know if any construction was shut down due to the high winds the week of April 24-30 apart from large cranes either in Potrero Hill or citywide in order to minimize exposure to harmful dust. If not, why not?

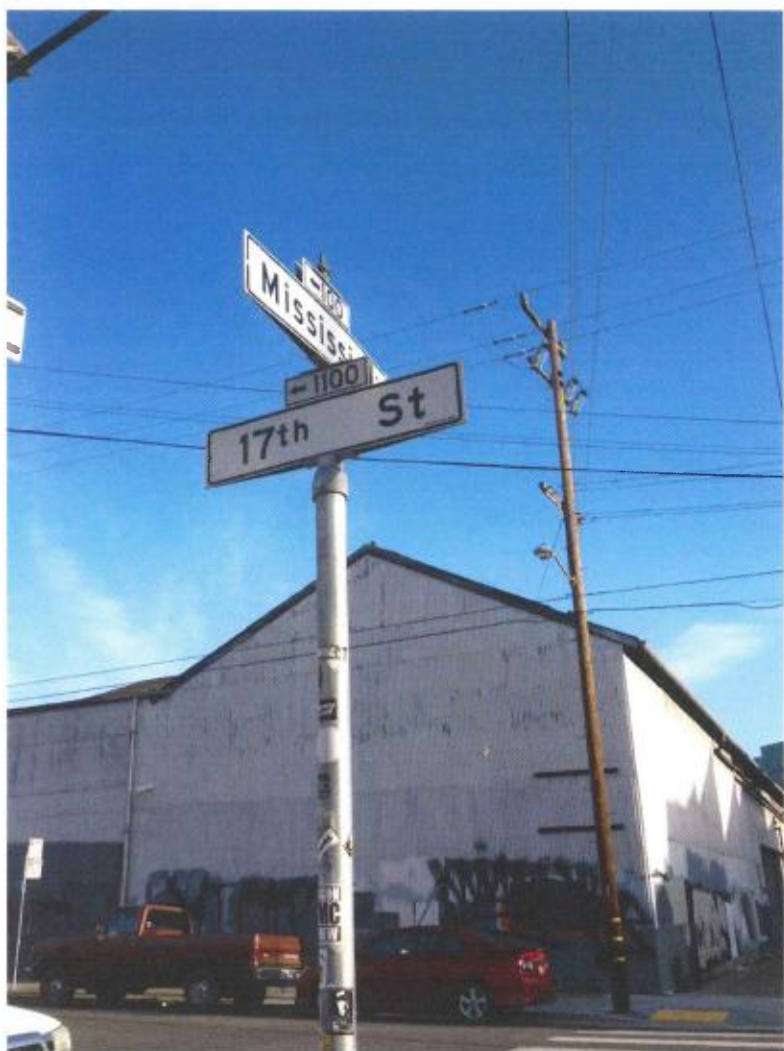
4) Existing and future transit, bike, truck and Cal Train Routes must be protected. The Response to Comments still does not correct factual matters related to transportation. Cumulative Impacts on Trucks, Trains, Bus and Bike routes were not adequately addressed in the DEIR or RTC. With a VMT lense impacts on other transportation modes and transit becomes more important and need adequate examination.

Truck Routes: The Proposed Project is surrounded by designated truck route that are clearly marked and used each day by large trucks. On RTC-69 Under TR-8 the document references the general plan designation of significant truck routes. The document states this is not a truck route per general plan.













However as these pictures show from 17th and Mississippi both 17th and Mississippi adjacent to the project are designated and used truck routes. Perhaps this is a reflection of a poor level of study of this and other areas of the city. For example, although the criteria for an urban wildlife refuge is 2 acres and nearby Jackson Park is 4 acres, it has not been classified as an urban wildlife refuge yet by the city - perhaps another oversight of this once neglected industrial area.





Caltrain: The Planning Department is already conducting out reach on electrification of CalTrain and bringing High Speed Rail to SF.

<http://sf-planning.org/railyard-alternatives-and-i-280-boulevard-feasibility-study-rab> Per SF Planning "The RAB will also study the possibility of new transit-oriented development, neighborhood connectivity, open space and public amenities to accommodate growth in this area of the City."

This proposed project is within 100 Feet of CalTrain. Any additional entitlement in the NE corner of the proposed project might impend public use for mandatory electrification of Caltrain for Air Quality and Black Carbon Management objectives. CalTrain is a vital part of the Commuting system in the Bay Area. With major construction and possibly underground tunneling the need for open space and public amenities in this area of the City is paramount. Not over-building the NE corner of the project would serve the public interest long term as we accommodate additional heavy rail in the area.

Caltrain has an at-grade crossing right next to the project. The RTC did not study the interaction of traffic and the rail crossing in the morning commute time. In addition the document made some assumptions which underestimated the time the crossing is blocked due to train passage - by speculating concurrent (North South) passage of trains. In addition, electrification of CalTrain will allow at least one additional train and of course greater track and crossing use.

Buses: Public and Private Large Buses use 16th, Mississippi and 17th Streets. The impact on bus transit was not adequately studied.



Widening of sidewalks into the public right of way is a bad idea as proposed on 16th Street. The Project should be stepped back from the property line to allow pedestrian and considerable scooter use along the sidewalk per the Better Street Plan while preserving 16th street for future public uses such as bike lanes, additional turn lanes, etc. The proposed project would take about 5 feet of public roadway ROW to use to comply with the need for additional pedestrian space - this should be rejected and the project should move the equivalent space from the road and create pedestrian space that way. I disagree with the RTC on this issue.







Bike: 16th Street is a current bike route as is Mississippi adjacent to the project. In addition 17th St is designated to become a bike path for the Blue Green way. The Bike paths should not be impacted during construction and must be preserved and developed with the project if it is approved. Bike Share should be located on the project site and all tenants should be provided with a membership to encourage use of City Bike Share for short trips.

In closing, I would like to urge that the Commission adopt the alternative Metal Shed Reuse Alternative.

I agree with the suggestions of Save the Hill to improve the project and appreciate the dedication and involvement of the community in developing a viable alternative that supports neighborhood values, housing as well as PDR.

Thank you for your consideration,  
Judith



PTo: SF Planning Commission

May 3, 2016

Dear Commissioners,

Please reject current plans for the proposed Corovan development. They are out of scale with the neighborhood, do not preserve enough of the historic quality of the existing building, and would make traffic problems worse in what is already a congested area. A Metal Shed Reuse Alternative was proposed; it is so much better than the current plans. Everyone I heard respond to it has basically said "I LIKE that!" The Metal Shed Reuse Alternative is scaled to the neighborhood and does not have the height and massing along 16th St. which recalls the bland big box style of Mission Bay. Reducing the height of the proposed development along 16th St as well as reducing the height or eliminating the rooftop structures (mechanical/elevator/stair penthouse) would be a big improvement. The Metal Shed Reuse Alternative showed clearly that preserving old elements enhances new elements. The Pacific Rolling Mills was once the largest steel producing factory on the West Coast. Early industries like this defined the neighborhood; losing another historic structure when creative possibilities show clearly that development and historic preservation can be beautifully combined would be a real shame. A wall-like building along 16th St. would dramatically change the character of a distinctive San Francisco neighborhood. Please recommend a rethinking of the current proposal.

Sincerely, Peter Linenthal  
298 Missouri St., SF, CA, 94107  
[ppotrero@pacbell.net](mailto:ppotrero@pacbell.net)  
415-863-0784

Director, Potrero Hill Archives Project  
Potrero Hill resident since 1975



## PROPOSED DEVELOPMENT @ 901 16<sup>th</sup> St. / 17<sup>th</sup> & MISSISSIPPI STREET

May 3, 2016

Submitted by: Philip Anasovich, Architect, AIA  
Address: 298 Missouri St. San Francisco, CA 94107  
Residency on Potrero Hill: 30+ years

### Comments:

I still object to the size of the proposed project, its height and bulk, the traffic problems that it will aggrandize, the lack of PDR, and the fact that this project ignores the significant historical buildings that now occupy the site. The developer in my opinion has done little or nothing to address these issues.

Density at the site is a major concern to me as an architect. It has been pointed out that we have **already** achieved the housing goals set by the Planning Department for **2025**. We need better transportation systems in place, not promises that we have had from City hall for the last 25 years. We need parks and open spaces. Increased housing density for the Potrero at this time seems foolhardy without laying the proper groundwork of services.

In massing and general appearance the proposed development is out of character with our neighborhood; its size is so daunting that it relates to no other existing building in the neighborhood. It has a facade that rises to plus 80 feet without setbacks. This new structure goes against all the rules and objectives that are a part of the Potrero Hill Plan and the City General Plan. We must find a way to scale the current proposal back.

In the Planning Department's "Response to Comments" released recently, reusing the historic structures in the Metal Shed Reuse Alternative was highlighted as "environmentally superior" to the current proposal by the developer. I still believe that this alternative represents the best design approach for this difficult and busy site, an approach which could yield a superior project that is beneficial to the community and the environment.

Philip Anasovich  
A.I.A.



May 4, 2016

Re: Case # 2011.1300 / 901 16th / 1200 17th Streets  
From: Pat Curtis / Owner, San Francisco Scrap Metal  
99 Mississippi Street

Dear Planning Commissioners,

The above referenced proposal has been a continuing battle for the residents and business owners of Potrero Hill for more than three years. Our community (citizens) continue to be overlooked and unheard during this battle. There are important issues that must be addressed by the Planning Commission. Valuable information has been researched, gathered and presented to the Planning Commission, number one being hazardous materials and toxins at the Corovan site — which has not been addressed.

We urge the Planning Commission to do more extensive research on the affects of coal tar toxins and the long term effects and exposure these toxins will have on our citizens in the future. Our children, parks, and schools.

Toxin

A - Coal tar and heavy metals - Coal tar ten feet thick and contains up to 8900 mg/kg of hazardous volatile petroleum hydrocarbons which are contaminating the soil and water (OSHA documentation attached):

- Coal tar has extremely high viscosity
- Coal tar can be injurious to health
- 5% of crude coal tar are Group I carcinogens
- Levels of 80 mg/m3 coal tar pitch are immediately dangerous to life and health.

Several states have banned the use of coal tar. It is imperative that California join these states and protect the citizens.

B - Pollution - Additional vehicles will create more airborne pollution. The Planning Commission needs to inspect existing conditions and provide in depth study.

C - Small Business / PDR - Small businesses are being forced out, the backbone of San Francisco. These businesses provide a valuable service to the residents.

D - Traffic - Vehicle traffic is gridlocked at the intersections of 16th and 17th Streets. More cars will create more pollution imposing a danger to pedestrians and cyclists. The City of San Francisco has to provide a safe environment by conducting an extensive study and research alternatives.

E - Parking - There is not enough parking for current residents and businesses. Adding more cars is not feasible — there is no space. Garages are not the answer. Builders are not providing enough space for tenants — where will they park?

Sincerely,

Pat Curtis  
Owner / San Francisco Scrap Metal  
99 Mississippi Street



# Coal tar

From Wikipedia, the free encyclopedia

**Coal tar** is a brown or black liquid of extremely high viscosity. Coal tar is among the by-products when coal is carbonized to make coke or gasified to make coal gas. Coal tars are complex and variable mixtures of phenols, polycyclic aromatic hydrocarbons (PAHs), and heterocyclic compounds.<sup>[1]</sup>

It is on the WHO Model List of Essential Medicines, the most important medications needed in a basic health system.<sup>[2]</sup>

## Contents

- 1 Uses
  - 1.1 Pavement sealcoat
  - 1.2 Industrial
  - 1.3 Medical
- 2 Safety
- 3 Coal tar distillers
- 4 See also
- 5 Notes
- 6 References
- 7 External links

## Uses

### Pavement sealcoat

Coal tar is incorporated into some parking-lot sealcoat products, which are used to protect and beautify the underlying pavement.<sup>[3]</sup> Sealcoat products that are coal-tar based typically contain 20 to 35 percent coal-tar pitch.<sup>[3]</sup> Research <sup>[4]</sup> shows it is used in United States states from Alaska to Florida and several areas have banned its use in sealcoat products <sup>[5][6][7]</sup> including: The District of Columbia; the City of Austin, Texas; Dane County, Wisconsin; Washington State; and several municipalities in Minnesota and others.<sup>[8][9]</sup>

### Industrial

Being flammable, coal tar is sometimes used for heating or to fire boilers. Like most heavy oils, it must be heated before it will flow easily.

Coal tar was a component of the first sealed roads. In its original development by Edgar Purnell Hooley, tarmac was tar covered with granite chips. Later the filler used was industrial slag. Today, petroleum derived binders and sealers are more commonly used. These sealers are used to extend the life and reduce maintenance cost associated with asphalt pavements, primarily in asphalt road paving, car parks and walkways.

A large part of the binders used in the graphite industry for making "green blocks" are coke oven volatiles



(COV). A considerable portion of these COV used as binders is coal tar. During the baking process of the green blocks as a part of commercial graphite production, most of the coal tar binders are vaporised and are generally burned in an incinerator to prevent release into the atmosphere, as COV and coal tar can be injurious to health.

Coal tar is also used to manufacture paints, synthetic dyes, and photographic materials.

## Medical

Also known as **liquor carbonis detergens** (LCD),<sup>[10]</sup> and *liquor picis carbonis* <sup>[a]</sup> (LPC) BP<sup>[11]</sup> it can be used in medicated shampoo, soap and ointment, as a treatment for dandruff and psoriasis, as well as being used to kill and repel head lice. When used as a medication in the U.S., coal tar preparations are considered over-the-counter drug pharmaceuticals and are subject to regulation by the USFDA. Named brands include Denorex, Balnetar, Psoriasin, Tegrin, T/Gel, and Neutar. When used in the extemporaneous preparation of topical medications, it is supplied in the form of coal tar topical solution USP, which consists of a 20% w/v solution of coal tar in alcohol, with an additional 5% w/v of polysorbate 80 USP; this must then be diluted in an ointment base such as petrolatum.

(Pine tar has historically also been used for this purpose, but has been banned as a medical product by the FDA since no evidence was submitted proving it is effective.<sup>[12]</sup>)

Various phenolic coal tar derivatives have analgesic (pain-killer) properties. These included acetanilide, phenacetin, and paracetamol (acetaminophen).<sup>[13]</sup> Paracetamol is the only coal-tar derived analgesic still in use today, but industrial phenol is now usually synthesized from crude oil rather than coal tar.

## Safety

\* According to the International Agency for Research on Cancer, preparations that include more than five percent of crude coal tar are Group 1 carcinogens.

\* The residue from the distillation of high-temperature coal tar, primarily a complex mixture of three or more membered condensed ring aromatic hydrocarbons, was listed on 28 October 2008 as a substance of very high concern by the European Chemicals Agency.

\* People can be exposed to coal tar pitch volatiles in the workplace by breathing them in, skin contact, or eye contact. The Occupational Safety and Health Administration (OSHA) has set the legal limit (permissible exposure limit) for coal tar pitch volatiles exposure in the workplace as 0.2 mg/m<sup>3</sup> benzene-soluble fraction over an 8-hour workday. The National Institute for Occupational Safety and Health (NIOSH) has set a recommended exposure limit (REL) of 0.1 mg/m<sup>3</sup> cyclohexane-extractable fraction over an 8-hour workday. At



levels of 80 mg/m<sup>3</sup>, coal tar pitch volatiles are immediately dangerous to life and health.<sup>[17]</sup>

## Coal tar distillers

In the coal gas era, there were many companies in Britain whose business was to distill coal tar to separate the higher-value fractions, such as naphtha, creosote and pitch. These companies included:<sup>[18]</sup>

- British Tar Products
- Lancashire Tar Distillers
- Midland Tar Distillers
- Newton, Chambers & Company (owners of IZAL brand disinfectant)
- Sadlers Chemicals

## See also

- Creosote
- Cresolene
- Preservative
- Red 40
- Tar
- Sealcoat

## Notes

- a. Latin: coal tar solution

## References

1. "Toxicological profile for wood creosote, coal tar creosote, coal tar, coal tar pitch, and coal tar pitch volatiles" (PDF). U.S. Department of Health and Human Services. September 2002. Retrieved 8 March 2013.
2. "19th WHO Model List of Essential Medicines (April 2015)" (PDF). WHO. April 2015. Retrieved May 10, 2015.
3. Mahler BJ; Van Metre PC (2 February 2011). "Coal-Tar-Based Pavement Sealcoat, Polycyclic Aromatic Hydrocarbons (PAHs), and Environmental Health". U.S. Geological Survey Fact Sheet. Retrieved 8 March 2013.
4. Van Metre PC; Mahler BJ (15 December 2010). "Contribution of PAHs from coal-tar pavement sealcoat and other sources to 40 U.S. lakes". U.S. Geological Survey. doi:10.1016/j.scitotenv.2010.08.014. PMID 21112613. Retrieved 8 March 2013.
5. "City of Austin Ordinance 20051117-070" (PDF). 17 November 2005. Retrieved 8 March 2013.
6. "District Bans Coal-Tar Pavement Products". 26 June 2009. Retrieved 8 March 2013.
7. "Ordinance 80 : Establishing Regulations on Coal Tar Sealcoat Products Application and Sale" (PDF). Dane County Office of Lakes and Watersheds. 1 July 2007. Retrieved 8 March 2013.
8. "Coal Tar Free America – Bans". Retrieved 8 March 2013.
9. Barbara J Mahler (14 April 2011). *Causes of Increasing Concentrations of Polycyclic Aromatic Hydrocarbons (PAHs) in U.S. Lakes* (PDF). PAHs Increasing in Urban U.S. Lakes. Environmental and Energy Study Institute. Retrieved 8 March 2013.
10. Paghdal KV; Schwartz RA (31 January 2009). "Topical tar: back to the future". PMID 19185953. Retrieved 8 March 2013.
11. Berenblum I (25 September 1948). "Liquor Picis Carbonis". *British Medical Journal* 2 (4577): 601. doi:10.1136/bmj.2.4577.601. PMC 2091540. PMID 18882998.



May 4, 2016

Re: 901 16th / 1200 17th Streets

Commissioners,

I urge you not to certify the EIR for the current proposal for 901 16th St./1200 17th St., a.k.a. the Corovan site, nor to sign off on the Large Project Authorization for same.

**TRAFFIC.** Without any mitigation, the current project, by its scale and location, will add considerable passenger traffic at the very worst point of traffic congestion in the area. Barely moving traffic which now stretches for several blocks over several hours will get longer. That is not only inconvenient for drivers, but also impedes public transit, generates localized pollution, and endangers pedestrians. Current plans attempt mitigation by signaling certain intersections. While that may help traffic move more smoothly, it will do so at the cost of spreading slow traffic over longer distances, as I pointed out in comment TR-21 to the EIR. Increased traffic slowdowns already channel drivers to 17th and to Mariposa streets, along Jackson Playground and Live Oak elementary school. The response to the comment skirts the issue, and this likely future impact, on top of the very real and noticeable current impact of spillover traffic, remains unresolved. This spillover effect, already noticeable from the street, has not been addressed in the project DEIR, nor was it studied as a cumulative impact in the Eastern Neighborhoods EIR.

This traffic is not just localized congestion. It represents thousands of drivers daily getting onto the 280 freeway for a long ride down the peninsula. The proposed project, which contains hundreds of parking spots, only two blocks from the freeway access, is a magnet for private car commuters, and would achieve the opposite of transit-friendly planning, while adding thousands of daily Vehicle Miles Traveled to area traffic.

**LAND USE.** The planned project project has a mix of 96% housing to 4% retail; this in a neighborhood which, due to its topography, is already low on commercial areas. At the same time it takes away PDR space which provides ever-rarer local jobs for non-office employment. The planned project squanders one of the few opportunities left to us to add a neighborhood walking destination of more than a few retail stores. Whether or not it meets the technical definition of a mixed use building, in practice it adds great demand for local services and jobs, and almost nothing to satisfy even current demand.

With the enormous Daggett Triangle housing monolith across the street, it is especially important to diversify structures and land use in this neighborhood, rather than extend it to across 16th St. into a another, similar monolith.

**ARCHITECTURAL AND HISTORICAL VALUE.** The Showplace Area Plan policy 3.1.9 states, "Important historic buildings cannot be replaced if destroyed. Their rich palette of materials and architectural styles imparts a unique identity to a neighborhood and provides valuable additions to the public realm." If any single architectural idiom represents the neighborhood's industrial history, it is the metal-clad building. Newer area



buildings have been imitating that idiom for decades. And yet actual buildings using the style are becoming rarer and rarer. The Corovan buildings are the oldest and most historically significant exemplars of that style remaining in the area, and the few remaining others are facing demolition. The current plan calls for 3% of the original metal shed footprint to be retained, either with salvaged materials or with new imitation. For practical purposes, the plan calls for complete removal of the metal buildings, while retaining a token acknowledgment of their significance. In contrast, adaptive reuse of the Corovan structures would benefit local residents and create a profitable commercial space, while showcasing the most noticeable historical structure of the immediate neighborhood, and maintaining one authentic, functional example of its iconic style.

IN SUM. The developers of this site were lucky enough to purchase 3.5 acres of desirable land in San Francisco for a mere \$14 million, during the last recession. With such remarkable profit margins they have far more flexibility than most developers, who are severely constrained by land costs, and they have considerable room to consider community desires and needs in their design. And yet, for as long as this project has been on the table, the project sponsors have steadfastly refused to budge to accommodate any community suggestions. Neighborhood residents don't have deep pockets or influence. All we have to give us a fair representation is city government. With that in mind, I again ask the Planning Commission to integrate these issues, which we face daily and will continue to face, into the approval process for this project. Please reject the current proposal, and give the developer of the Corovan property an opportunity to create a fresh design for a project which would benefit all of us.

Yoram Meroz  
Potrero Hill



As a long time homeowner in Potrero Hill, I wanted to add my voice to the discussion of the proposed project at the Corovan Site. After attending many neighborhood meetings with the architects and developers, I agree with suggestions below as ways to make the project a better fit for our area.

- \* Reduce the project's height and massing on 16th Street to respect the topography of the hill and reduce shadowing of the new Daggett Park; reduce or eliminate eyesore rooftop mechanical/elevator/stair penthouses.

- \* Increase publicly accessible open space by widening the proposed pedestrian promenade to at least 40 feet.

- \* Reuse distinctive materials and features of the current metal warehouses in order to create visual and historic continuity between old and new uses at the site. The Final Environmental Impact Report (EIR) has now identified the Metal Shed Reuse Alternative as the environmentally superior project.

- \* Reduce parking and traffic congestion by shrinking the project.

- \* Replace housing on 17th Street with more diverse commercial uses such as space for artists, makers, non-profit groups, and neighborhood serving retail, that will help reduce conflicts between Bottom of the Hill and new residents, as well as providing neighborhood services and amenities.

- \* Prohibit formula retail within the development.

- \* Include space for Production / Distribution / Repair (PDR).

Currently the scale of the project, the height of the project, the disregard for traffic congestion, and parking limitations, has not really been realistically addressed. While the concessions offered are more a token gesture to calm the residents while not actually changing much in the original plan.

I feel there is an opportunity to retain some of the history of our neighborhood by adopting the adaptive reuse plan that has been proposed as an alternative.

In other cities where historic buildings are used as the framework for new projects the results are much more architecturally interesting and unique than buildings that are new. The conversion of the old Greyhound bus terminal into the California College of Art is a great example. In my opinion, one only must look at the development at Mission Bay to see many examples of what should not be continued in our section of town.

In 2002 after searching all over the city for a home to buy, my wife and I were thrilled to find one in Potrero Hill. The mix of architectural styles and a blend of commercial and residential buildings attracted us. This has been eroded over our time here



by developments that are designed with more interest in profit than design. While understanding the need to maximize the developer's financial gain, I feel that this proposal needlessly degrades the unique character of our neighborhood.

I hold hope that the planners will recognize the need to retain some of what makes this city so unique and take this opportunity to mandate a development plan for the site that will allow that.

In a big way San Francisco depends on this to continue to attract business, tourist dollars, and to maintain it's reputation of one of the most beautiful and interesting cities in the world. History and style are important parts of any city, especially ours.

Ron Kurash  
Lynka Adams  
348 Texas Street  
San Francisco, CA.



**From:** Joe Butler fjoseph1butler@gmail.com  
**Subject:** 16th and Mississippi, Hearing May 12, 2016  
**Date:** May 3, 2016 at 4:55 PM  
**To:** Dennis Richards drichards20@outlook.com, rodney fong planning@rodneymfong.com, cynthia wu cwu.planning@gmail.com, kathrin moore mooreurban@aol.com, michael antonini wordweaver21@aol.com  
**Cc:** Rodney Minott rodneyminott@outlook.com

---

Dear Presidnet Fong:

My name is F. Joseph Butler AIA, and I am an architect here in San Francisco.

I would like to make comments about the evaluation that was done for this site. The Pacific Rolling Mills Company has a long history in San Francisco. They began on Potrero Point in 1868, and one Patrick Noble was employed at that time.

Thirty years later, he took the company from Potrero Point to 16th and Mississippi. In fact, he was up against Texas on 17th with his first foundry, and eventually moved across the street. As you see on this map (indicating) it was on the edge of the water. So basically, the site was an array of metal sheds, as they grew as a company on the land that was dry -- high and dry, as they say. So 20 years as its president, he left the company to his son. Edward Noble took the company from 1920 into World War II, and came out the other end. Finally, the operations on Potrero Hill slowed down, and eventually other uses filled the space. It's uniquely flexible. It's the most quality PDR space that you have in this whole Showplace Square district. It was the innovative building. Those people who fabricated steel in this building produced the Fairmont and St. Francis Hotel structural frames, the Crocker Building, the Flood Building, the State Capitol in Sacramento, the Standard Oil building downtown, the Spreckles Building, the BelAir Apartments on Russian Hill, the PG&E outlet in a variety of different places, the School of Education at Stanford, the University of Berkeley Gymnasium in 1931, post offices, hospitals, libraries and bridges. They built these sheds the entire Richmond/San Rafael Bridge. And you're going to tell me at the end of saving a red brick building that that's adequate to convey the significance of what occurred on this site? Not even close.

The reuse sheds alternative should be adopted as the preferred alternative because it's environmentally superior. It saves the most high quality, the most innovative and creative early structures of this type in the neighborhood. This is so important to this community, because this is where people on Potrero Hill were trained in trade jobs. They had an association with Lick Lormity school. They trained people to come and draw and fabricate steel with them. It is a trade school and it still exists to this day.

There is no finer PDR space in the Showplace Square area, if your Commission lets the Sheds be demolished, then the mixed use designation is just words on paper to be ignored with impunity.

Sincerely:

Joe  
F. Joseph Butler, AIA  
324 Chestnut Street  
San Francisco, CA  
94133  
(415) 990 6021



May 2, 2016

San Francisco City Planning Commission  
Re: Proposed Project at Corovan Site  
901 16<sup>th</sup> Street/1200 17<sup>th</sup> Street

Commissioners,

I ask that you reject the project as currently proposed for the Corovan site. Instead, please endorse the use of the Metal Shed Reuse Alternative which was highlighted in the Planning Department's "Response to Comments" released just last week.

The proposed project will significantly exacerbate existing traffic and parking problems in the immediate vicinity of the project and nearby blocks. At certain times of the day traffic is backed up for many blocks to the extent that it takes several minutes to drive just one or two blocks. Adding several thousand new car trips a day is unacceptable. Please reduce parking and traffic congestion by shrinking this project. The City's own studies forecast crippling impacts from traffic and parking congestion created by this project, including more than 12,000 daily trips by people and spillover demand for more than 458 parking spots on surrounding neighborhood streets.

The massive scale of the proposed project will create another giant monolithic wall like the project completing construction on the other side of 16<sup>th</sup> Street, Daggett Triangle. The lack of respect for the height, mass, articulation, and materials of existing buildings on Potrero Hill is very apparent. Please require that this project's height and massing on 16<sup>th</sup> Street is reduced and require or eliminate the eyesore rooftop mechanical/elevator/stair penthouses.

The project as currently proposed degrades our neighborhood character and livability. The developer's project is completely out of scale with the neighborhood within which I have lived for 37 years and violates multiple Area Plan principles including provisions to "respect the natural topography of Potrero Hill", to lower building "heights from the north to south side of 16<sup>th</sup> Street", to "promote preservation of other buildings and features that provide continuity with past development," and to "ensure that infill development is compatible with its surroundings."

The developer's project will dramatically and detrimentally change the character of Potrero Hill and intensify the Mission Bay-type development already encroaching upon our neighborhood. Please reject this project, or at least send it back to the drawing board with a requirement for reduced scale, height, and density that is more consistent with the existing built environment of Potrero Hill.

Sincerely,  
Jane Nicholson

240 Mississippi Street  
San Francisco, CA 94107



Dear Planning Commissioners,

I'm a resident of Potrero Hill for over eight years and I'm writing to you about the development project planned for 901 16th Street and 1200 17th Street about which I had contacted you before. With the draft environmental impact report nearing the stage of finalization, I was pleased (along with plenty of other Potrero Hill residents, I'm sure) to learn that the adaptive reuse option being considered for the site was found to be the most environmentally friendly option. It is my understanding, however, that this is not going to be stopping the developers from pursuing their original project plans for the site in almost unchanged form. I'm writing now to voice my wishes, which I believe are shared by many of my fellow Potrero Hill residents, that even if the developers still pursue their original plans, that they at least adapt these plans to the context, character, and requirements of the site and to the expectations of the local residents to the greatest extent possible.

One thing residents of this area would love to see in this project is more prominent use of the materials and components of the current structures on the site, if an as-is adaptive reuse approach is not feasible. I've heard news that the developers have shown some signs of willingness in this direction and I think that this is a vital factor for the project to "make sense" in its location, rather than looking like a building that could have been built by anyone to be plopped down just anywhere. I would like to see prominent design features on the building that clearly and unambiguously recall what sat on that site before it.

Meanwhile, the size of the proposed structures is still one of the biggest unaddressed concerns about the current project. The scale of the project has not been dialed back by any significant amount since the first time the developers showed any plans. The proposed buildings are of a size that will be even more smothering to the area than originally feared, given the massively monolithic Daggett Place project that is currently near completion straight across 16th Street from this site. Starting with traffic density and parking, a lot of the adverse impacts of the project arises from its out-of-proportion scale in relation to the surrounding area. One thing I wish to see is a meaningful reduction made by the developers to the excessive number of residential units. Another change that would really help along the same lines is a solution to the proposed inclusion of an abusive amount of "mechanical penthouse" volume, which creates an eyesore and makes a de facto mockery of the height restrictions for this site.

My hope is that these concerns are seriously taken into account and reach the attention of the developers. Many thanks in advance for your efforts to see to it that it happens.

Respectfully,

Ergin Guney



May 3, 2016

Rodney Fong, Commission President  
Dennis Richards, Commission Vice President  
Cindy Wu, Commissioner  
Michael J. Antonini, Commissioner  
Rich Hillis, Commissioner  
Christine D. Johnson, Commissioner  
Kathrin Moore, Commissioner

Re: 901-16th Street / 1200-17th Street

Dear Commissioners:

One of the impetuses for the formation of Friends of Jackson Park (FoJP) was the immediate need to help create and save the negligible green, open space we have in our neighborhood. As you know, with all the approved and planned development in Potrero Hill, any open space is even more precious.

We were hopeful that we'd see the "full array of public benefits, to ensure the development of complete neighborhoods, **including open space**, improved public transit, transportation, **streetscape improvements**, community facilities, and affordable housing." set forth in the Eastern Neighborhoods Plan.

Especially promising is the Showplace Square/Potrero Hill Plan which specifically stated that the area has "**comparatively little access to open space compared with the rest of the city and that the addition of new residents makes it imperative to provide more open space to serve both existing and new residents, workers and visitors.**"

Unfortunately, we haven't seen the City make good on these promises. But it's not too late. FoJP would like to see more public open space in the 901-16th Street / 1200-17th Street project. We are proponents of the public promenade being 40 feet wide – this would make it a more attractive place to hang out with friends and family while increasing the likelihood of activating the ground floor.

Another concern is the cumulative shadow to Daggett Park. We finally get some new green open space and it's going to be in shadows for several months a year? That shouldn't happen.

On a positive note, the Developer has promised to financially support our efforts towards the Jackson Park Renovation Project. Friends of Jackson Park requests that you consider our neighborhood's need for open space and make your determination on this project accordingly.

Thank you,

Jude Deckenbach  
Friends of Jackson Park





# SEAN ANGLES

382 ARKANSAS STREET SAN FRANCISCO CA 94107

May 3, 2016

Planning Commission Members  
1650 Mission Street  
San Francisco, CA 94103

## OPPOSED to 901 16<sup>th</sup> Street and 1200 17<sup>th</sup> Street Case No. 2011.1300E

### Negative Feedback Public Comment

Dear Planning Commission Members,

I am OPPOSED to the current proposal for housing at 901 16<sup>th</sup> Street and 1200 17<sup>th</sup> Street.

While opposing this housing proposal, I am encouraged to support the **Metal Shed Reuse Alternative**, under which all the warehouse buildings on the site (1210 17<sup>th</sup> Street/975 16<sup>th</sup> Street and 1200 17<sup>th</sup> Street) would be retained and reused.

I urge the Planning Commission to order a 'time out' halt to this current proposal and all future projects in the Eastern Neighborhoods Potrero/Showplace Square areas until the cumulative negative impacts that are already underway and deteriorating our neighborhood's quality of life caused by current projects and construction-in-progress are assessed and mitigated.

The Environmental Impact Report appears incomplete, insufficient and inadequate due to obsolete studies and false facts.

My primary concerns are:

TRAFFIC GRIDLOCK. Recently with the opening of new UCSF hospitals and launch of new residential buildings, an explosion in vehicle traffic has deteriorated circulation on both 16<sup>th</sup> Street and 17<sup>th</sup> Street to unacceptable levels. The nearby existing Caltrain rail crossing at 16<sup>th</sup> Street presents a huge impact with gridlock when the crossing gates come down for trains passing. The proposed project will significantly worsen traffic and parking along streets and intersections already plagued by congestion. The already opening of the new 453-unit apartment and mixed use Potrero 1010 Apartments at Daggett Place complex on 16<sup>th</sup> Street at Seventh Street—which is



directly across from this new proposal— was not accurately evaluated in the obsolete traffic studies for the further negative traffic circulation and gridlock before this new proposal is properly evaluated by Planning Department.

DENSITY. The proposed project is too big for the project site and surrounding Potrero Hill neighborhood. With the onslaught of new projects already under construction, the cumulative effects of too much housing development without delivery of a single neighborhood benefit promised the Eastern Neighborhoods Plan adopted 2008 is unacceptable to our community. Erroneous study assumptions and violations of the Showplace / Potrero Hill Area Plan must be corrected before any application to the proposed project.

OPEN SPACE. If any new use should be proposed for 901 16<sup>th</sup> Street and 1200 17<sup>th</sup> Street, the parcel should be proposed for city acquisition as new public open space and recreational facilities for the benefit of forecasted expanding numbers of new residents on Potrero Hill and schoolchildren at Live Oak School and International Studies Academy. Our community is desperately needed recreation and open space in our increasingly congested neighborhood.

SF BOMB SQUAD VEHICLE YARD – Public safety issue. Emergency Vehicle circulation delayed by increased traffic. As this is a potential target for terrorism, high density residential Conflicts with Area Plans and General Plan: Conclusions in the Draft EIR conflict with the Showplace Square/Potrero Area Plan and General Plan by disregarding policies of preserving neighborhood character and protecting parks and open space from shadowing.

OBSOLETE STUDIES: The Planning Department is relying on the Eastern Neighborhoods Final EIR that is eight years old and is now stale for the environmental review of the current proposal for housing. Some of the studies and research rely on data that is as old as the 2000 census.

CUMULATIVE IMPACTS IGNORED: The issue of cumulative impact has been dismissed entirely and misrepresents the projections made in the Eastern Neighborhoods Final EIR. The fact is that the City already has more units in the pipeline for Showplace Square/Potrero area than were anticipated to be built in the area by 2025. Furthermore, the City has failed to fund the promised infrastructure improvements to support growth. The assumption that cumulative impacts will be limited is no longer true.

RECREATION NEGATIVE IMPACTS: Recreation impacts are not fully addressed. The addition of new residential households would result in an increased demand on Jackson Playground. The Draft EIR relies on outdated projections and doesn't account



for cumulative impacts. It points to onsite public and private open space to fulfill recreation needs for residents.

**LAND USE OBJECTIVES IGNORED:** The UMU (Urban Mixed Use) zoning for this project does not honor the Showplace Square/Potrero Hill Plan's Objective 6.1 to "support the economic well being of a variety of businesses". This new proposal for housing will result in the displacement of current tenant Corovan jobs, many of them viable PDR and blue-collar jobs. Most of the development's square footage will be devoted solely to residential use. The extent of the cumulative loss of PDR space was not fully anticipated in the Eastern Neighborhoods' FEIR (Final EIR) and merits further study.

**HAZARDOUS MATERIALS NEED FURTHER DETAILED STUDY:** The EIR should be revised to include more specific information about hazardous materials and measures to protect children and neighbors from exposure during demolition, remediation and construction. Based on the lack of detail in the Draft EIR, we are not confident that our children and neighbors will be safe.

**AIR QUALITY:** Studies are obsolete and fail to consider the current conditions of cumulative construction under way right now. Recently, all existing Potrero Hill neighbors have witnessed major increases in black dust invading our homes and outdoor surfaces. We have witnessed failures at nearby construction sites to properly control and monitor dust. Watering down is not an adequate mitigation, particularly under windy conditions. Alternate measures should be provided.

## **Specific Problems With The Draft Environmental Impact Report For The Proposed Project**

### **1) Inadequate & Inaccurate Analysis of Traffic, Parking and Transit Impacts**

#### **Failing grade "F" Traffic Circulation Intersections**

The DEIR acknowledges that the proposed Corovan project will significantly and unavoidably worsen traffic congestion. It identifies at least four intersections that will be severely impacted. These include:

- 17th & Mississippi Streets
- Mississippi & Mariposa Streets
- Mariposa & Pennsylvania Streets
- 7th/16th & Mississippi Streets.

The DEIR indicates there's currently no way of feasibly mitigating the increased traffic congestion at the above intersections, either due to lack of funding or practicality.

**DEIR Flaws:** Traffic data used in the DEIR was collected on a single day in 2012 during the peak evening commute. The DEIR does not consider data collected over a period



of time, or that includes the morning peak commute or a Giants game day. The DEIR also fails to consider cumulative impacts on traffic and parking from recent, present, imminent and long-range development projects.

The DEIR fails to consider the major imminent impacts of new first time residents who will begin to occupy the new 453-unit apartment and mixed use Daggett Place and Archstone Potrero complex at Seventh and 16th streets which is directly across 16<sup>th</sup> Street from this new proposal. Off-street parking is 'unbundled' from this new Daggett Place complex which means new residents will seek on-street parking to avoid expensive off-street garage parking lot fees.

The DEIR fails to adequately mitigate or address expected queuing in and out of the proposed project's Mississippi Street parking garage. With close to 400 units of housing and more than 24,000 square feet of commercial retail space, the project will generate an estimated 4,233 new car trips daily — with up to 12,361 trips daily by people entering and exiting the project. In spite of this compelling data, the report claims lines won't be significant and it defers responsibility for further studies or mitigation proposals until after the project is built. Additionally, the DEIR fails to sufficiently address expected pedestrian and vehicular hazards posed by the proposed development's single vehicle entry and exit point along Mississippi Street.

While the DEIR acknowledges the traffic-generating problems posed by the dense development, it does not adequately address the gravity of the situation nor does it satisfactorily assess proposed solutions. It ignores consideration of traffic calming measures proposed in previous years by SFMTA and community members for the Mariposa & Mississippi Street intersection as well as other intersections along Mariposa, and it relies on outdated data and a limited study of traffic conditions. In this way, the DEIR fails to identify solutions to predictable problems and neglects an invaluable opportunity to work with the community to mitigate those problems.

### **Parking Spillover**

The DEIR concludes the planned development will create spillover demand of between 358 - 458 parking spots — cars that will clog surrounding streets.

**DEIR Flaws:** The DEIR shirks responsibility for parking problems posed by the dense development by claiming no legal obligation, but it should acknowledge the degree to which an alternate proposal and further requirements of the developers would prevent unnecessarily negative impacts.

### **Larger Traffic Impacts**

What the DEIR Says:

**Golden State Warriors Event Center:** *Due to the relative timing of the proposals, the Warriors' event center project was not included in the cumulative analysis of the*



*proposed project .... (T)he Event Center project would not cause any significant change to the results given in this report and may potentially reduce the percent contribution to the impacted intersection from the proposed project. (page 124, Part 2, Draft EIR, 901 16th St & 1200 17th Street, August 2015)*

**DEIR Flaws:** The notable and inexplicable passage above is another example of how the DEIR's analysis relies on outdated and inadequate traffic data from 2012 and 1998. The DEIR not only fails to adequately consider and analyze the traffic and parking impacts of the Warriors Arena proposed for 3rd & 16th Streets, it claims that the Warriors Arena might actually help by shrinking the Corovan development's proportional contribution to traffic congestion. This absurd and unsubstantiated argument minimizes one of the most troubling aspects of the Developer's proposal.

Finally, the DEIR references only one large development in the area, 1000 16th Street (Daggett), while ignoring many other impactful projects in the pipeline including 1301 16th Street, 1601 Mariposa Street, 88 Arkansas Street, 249 Pennsylvania, 98 Pennsylvania, 1001 17th / 140 Pennsylvania, 790 Pennsylvania & 22nd Street, 580 De Haro Street, 540 – 522 De Haro, 131 Missouri Street, 1150 16th Street, 801 Brannan Street, 975 Bryant Street, 645 Texas Street, and 1717 17th Street.

### **Interstate 280 Ramps at Mariposa**

**DEIR Flaws:** The DEIR perpetuates the false claim that traffic impacts caused by the Corovan project to the I-280 on and off ramps at Mariposa Street will be significantly lessened through various mitigations – for example, new traffic signals and the expansion of Owens Street to connect Mariposa and 16th Street. These so-called mitigation measures were identified in the Mission Bay Environmental Impact Report – a study that is now 17 years old and outdated. Both the Mission Bay EIR and the recent Warriors Arena transportation report fail to offer adequate mitigations and analysis to reduce increased volume of traffic to and from Potrero Hill from past, present, and reasonably foreseeable future development.

### **Inadequate Public Transit**

**DEIR Flaws:** Adding thousands of residents with little investment in public transit will be a disaster for the neighborhood, resulting in further dependence on cars while traffic congestion grows and degrades our quality of life. For example, the 10 Townsend bus is already at 95% capacity yet the Corovan DEIR claims no mitigation measures are needed. Public transportation to the site is limited to a single future bus line that is already overburdened, underfunded, and suffering maintenance and scheduling difficulties. The San Francisco Municipal Transportation Agency's (SFMTA) projections state that the future 22-Fillmore line serving an extended 16th Street transit corridor will be overburdened from the start -- constrained by funding challenges, inadequate bus capacity and service, rising amounts of automobile and truck congestion, and uncertainty about the future of the Caltrain tracks, High Speed Rail, and the I-280 freeway. SFMTA's own forecast through 2035 projects that the intersection at 7th, 16th,



and Mississippi Streets will “degrade” to a service level of “F” – among the worst in the city. Yet the DEIR fails to adequately address and mitigate these significant impacts.

**What Existing Potrero Hill Neighbors Want:** A “Transit First” policy should put transit first and ensure that viable options be in place before we experience significant population growth. New studies of existing and cumulative conditions, inadequately addressed in the Corovan DEIR and not anticipated in the Eastern Neighborhoods Environmental Impact Report, must now be priority and undertaken.

City Planning should conduct additional traffic studies that are more current and robust. Traffic calming measures (such as bulb-outs and pedestrian islands) should be approved and implemented. Save the Hill agrees with SFMTA on certain traffic calming measures, and these and other options should be carefully considered. Furthermore, the Developers should be expected to invest in more traffic reducing strategies and should collaborate with both the community and City Planning on an alternate proposal to achieve this outcome. The Developers propose a very ambitious, large-scale development for a very sensitive site, and it is reasonable that they shoulder more of the responsibility for traffic reducing measures in the surrounding area. A reduction in the density of the project is only one way they could positively impact traffic problems posed by their proposal.

## **2) Inadequate & Inaccurate Study of Land Use (And Planning Policies Ignored)**

### **Largest & Densest**

**DEIR Flaws:** As proposed, the developer's project (72-82 ft. / 395 housing units) would be one of the largest, densest building developments in Potrero Hill history. Yet City Planning's previous environmental studies and projections for Potrero Hill fail to take into account a project of this scope at this site – including its impacts. Official analysis currently on record in the Eastern Neighborhoods Plan considered heights of between 45 feet - 50 feet at the property, not 72 feet to 82 feet. The DEIR fails to address this discrepancy.

### **Topography of Potrero Hill**

**DEIR Flaws:** The developer's project violates multiple Area Plan principles including provisions to “respect the natural topography of Potrero Hill”, to lower building “heights from the north to south side of 16th Street”, and to “promote preservation of other buildings and features that provide continuity with past development.” The DEIR fails to adequately address these issues.

### **Loss of Production, Distribution, Repair (PDR)**

**DEIR Flaws:** The proposed project would eliminate rather than retain 109,000 square feet of valuable Production, Distribution, and Repair space. The DEIR acknowledges this loss as a significant impact but nonetheless defends it as consistent with planning



goals. However, the potential loss of the Corovan site's PDR space must be understood in the context of recent and unanticipated PDR losses in both our neighborhood and across the Eastern Neighborhoods. This is yet another example of how the DEIR fails to incorporate new and accurate data.

### **Area Plan & City Policy Objectives And Principles Ignored**

**DEIR Flaws:** The proposed project conflicts with the Showplace Square / Potrero Area Plan, and the Urban Design and Housing Elements of the City's General Plan by disregarding policies of preserving neighborhood scale and character, providing adequate infrastructure, and preserving PDR uses. Both the Corovan development project and the DEIR fail to address the following consistency issues:

- A *Objective 3 of the San Francisco General Plan's Urban Design Element:*  
"Moderation of major new development to complement the city pattern, the resources to be conserved, and the neighborhood environment."

The scale and density of the Prado/Walden project are substantially greater than existing surrounding land uses and the project would be inconsistent with the established land use character of the neighborhood. The DEIR fails to acknowledge and consider that the Daggett Triangle development at 1000 16<sup>th</sup> Street in Showplace Square, as well as other large developments in nearby Mission Bay, are in separate and distinct neighborhoods that are not part of the Corovan site in Potrero Hill.

- A *Objectives of the Showplace Square / Potrero Area Plan*

The Prado/Walden project conflicts with a number of Area Plan objectives including Objective 1.2, which promotes development in keeping with neighborhood character. This project is inconsistent with the established neighborhood character of Potrero Hill. The Showplace Square/Potrero Hill Area Plan, in Policy 3.1.6, states that, "new buildings should epitomize the best in contemporary architecture, but should do so with a full awareness of, and respect for, the height, mass, articulation and materials of the best of the older buildings that surrounds them." As proposed, the project fails to match the height, mass, and articulation of existing buildings in the Potrero Hill vicinity and provides little awareness of surrounding structures.

- A *Policy 2 of the City's General Plan:* "That existing housing and neighborhood character be conserved and protected in order to preserve the cultural and economic diversity of our neighborhoods."

The Prado/Walden project is not consistent with this policy because scale, mass, bulk and height are inconsistent with and will negatively impact established neighborhood development pattern and character. The proposed development is dramatically out of scale with nearby residences and small businesses.

**What Existing Potrero Hill Neighbors Want:** For the DEIR to fulfill its purpose, it must include updated data reflecting neighborhood growth and it must acknowledge Area Plan principles developed and accepted by the community. Among other things, the cumulative loss of PDR to the City has not been accurately addressed and evaluated in the DEIR, and we ask that this study be conducted. Since the extent of the cumulative loss of PDR space was not fully anticipated in the 2008 Eastern Neighborhoods' Environmental Impact Report and no mitigations were identified,



these impacts require further study in the EIR for this project. Analysis should include a full exploration of feasible mitigations such as the inclusion of significant amount of new PDR space onsite.

We ask that the DEIR adequately address, analyze, and mitigate the aforementioned growth and planning policies and consider an alternate to the developer's proposal so as to honor these considerations. Moreover, the DEIR fails to acknowledge and consider that the south side of 16th Street in this area remains part of Potrero Hill and not Showplace Square or Mission Bay, which are separate and distinct neighborhoods. Showplace Square's 1000 16th Street (Daggett Triangle) project is neither appropriate for nor consistent with the character of Potrero Hill. This fact is clearly established in City planning policy and principle and should be respected and complied with. City Planning in a final EIR should address this issue.

#### **4) Inadequate & Inaccurate Analysis of Recreation & Open Space**

##### **Inadequate Parks**

**DEIR Flaws:** The DEIR fails to adequately consider the impact of the developer's proposal on our open and recreational space. Potrero Hill currently suffers from inadequate parks, open space, and recreational facilities. The addition of thousands of residents from this and other new large developments will put significant additional strain on nearby parks including Jackson Playground – already heavily used and lacking in maintenance upgrades. Moreover, the vast majority of so-called open space provided in the developer's currently proposed project would remain private and off limits to the public.

**What Existing Potrero Hill Neighbors Want:** The DEIR should include data and projections accounting for the dearth of recreation and open space and the degree to which developments already in the pipeline will further tax these inadequate resources. Planning should offer mitigations, including the addition of new parks that achieve the four acres promised by the City in prior planning reports. The Developers proposal should be revised to provide more open space accessible to the general public. For example, Planning should require the east-west "pedestrian mews" remain open to the public and not privately closed off space. The north-south pedestrian alley of the developer's project should also be widened by 20 feet and include more green soft-scape.

#### **5) Inadequate Study and Mitigation of Soil Hazards**

##### **Contamination Risks**

**DEIR Flaws:** The DEIR fails to adequately address the hazardous materials that will be exposed during construction. Furthermore, the DEIR does not acknowledge the fact that a kindergarten operated by the ALT School plans to open its doors in an adjacent building (99 Missouri Street) in the Fall of 2016.



**What Existing Potrero Hill Neighbors Want:** The DEIR should be revised to include more specific information about hazardous soils and measures to protect children (who are more vulnerable) and neighbors from exposure during demolition, excavation, and remediation. To date the City has not treated this issue seriously enough. Planning should address and analyze the potential risks of a new children's school (AltSchool) locating next to the Corovan site and detail mitigation measures that go well beyond what is currently planned. The California Department of Toxic Substances should also be involved in monitoring and coordinating this effort to ensure the safety of both children and neighbors.

## **6) Inadequate and Inaccurate Study of Population / Housing**

### **Excessive Density & Outdated Data**

**DEIR Flaws:** Recent analysis shows the Potrero Hill / Showplace Square area has already exceeded the number of housing units and population growth the City planned and projected for 2025! The Planning Department assumed up to 3,891 housing units would be built by 2025 in the Potrero Hill / Showplace Square area. But as of 2015, 3,953 units were already in the pipeline or built. The City failed to anticipate the dramatic pace of development and has not delivered on its promise to provide necessary public improvements (parks, transit, roads, etc.) to support thousands of new residents. City Planning analysis understates the "cumulative impacts" of large developments on our community by continuing to rely on outdated data from the 2008 Eastern Neighborhoods Environmental Impact Report to inform analysis in the EIRs of large projects, including the proposed Corovan development. Assumptions and mitigation measures provided in that document are simply no longer valid.

**What Existing Potrero Hill Neighbors Want:** The City failed to anticipate the dramatic pace of development and has not delivered on its promise to provide necessary public improvements (parks, transit, roads, etc.) to support thousands of new residents. In this as in other areas, we ask for more recent and relevant data to account for the extraordinary changes in this area's density. In order for the DEIR to be constructive for the neighborhood and for the city, it must incorporate new and accurate population data, and it must acknowledge the degree to which public improvements lag behind the neighborhood's growth. City Planning needs to acknowledge that Potrero Hill has already exceeded development targets projected for 2025. Environmental study and mitigations should reflect this fact to help inform current and future planning.

## **7) Inadequate and Inaccurate Study of Historic Buildings**

I urge the Planning Department to strongly consider the recent outcome of the San Francisco Historic Preservation Commission meeting on September 16, 2015 -- summarized in a letter from the Historic Preservation Commission to the Planning Commission—which found major violations posed by the current proposal for housing. The Historic Preservation Commission positively agreed that Metal Shed



**DEIR Flaws:** The DEIR does not adequately or accurately address issues related to the historic merit and integrity of the existing metal warehouses. The draft rejects arguments supporting historic integrity for the metal buildings. Evidence, including the research and opinion of a highly respected architectural historian, demonstrates these buildings remain historic despite alterations and company mergers over the years.

**What Existing Potrero Hill Neighbors Want:** The alternate plan proposed by City Planning incorporates the historic metal structures with new construction. We ask that this reasonable compromise be modified to achieve an environmentally superior status and be adopted. Planning is simply wrong in rejecting the historic significance of the metal warehouse buildings. The City should revise the DEIR to reflect the historic significance of these buildings based on architectural historian Katherine Petrin's report.

## **8) Inaccurate and Inadequate Study of Alternative Project Proposal**

### **Metal Shed Reuse Alternative Plan**

**DEIR Flaws:** The DEIR includes a plan that is City Planning's modified version of an alternate project plan submitted by Save the Hill (see "Metal Shed Reuse Alternative", Chapter VI). While some aspects of this alternate, lower-density "adaptive reuse" proposal are commendable, other aspects are inadequate and some of the data from which this proposal is driven is simply flawed. City Planning took Save The Hill's original suggestive renderings and manipulated numbers to suggest that space set aside for artists would generate volumes of vehicle traffic equal to the developer's vastly bigger project proposal. The effect was to deny awarding the adaptive reuse alternate plan the designation of "environmentally superior".

**What Existing Potrero Hill Neighbors Want:** We ask that City Planning acknowledge the "suggestive" nature of the proposed renderings and understand that the PDR segments of the proposal were intended for light and low impact purposes. We ask that their version of an adaptive reuse plan be revised to achieve environmental superiority. We suggest several changes that might help achieve the aforementioned goal including: reducing PDR workspaces for artists and replacing them with added residential units, and increasing the amount of underground parking by adding a second level. Currently the adaptive reuse alternative project allows for 177 units of housing while retaining the existing metal buildings. While Save The Hill is willing to support this plan with height limits at 58 feet along 16th Street and 48 feet along 17th Street, we ask that any added height for mechanical/stair penthouses capped at 68-feet, instead of 74.5 feet. Save The Hill is more than willing to work with both the developer and City Planning to improve this alternative as a workable solution.

### **"Reduced Density" Alternative Plan**



**DEIR Flaws:** As noted, the DEIR includes analysis of a "Reduced Density" alternate plan that is identified as "environmentally superior" (see "Reduced Density Alternative", Chapter VI). While Save the Hill supports reduced density, this plan does not nearly go far enough. Under this alternate plan, the height, scale and massing of the developer's current project proposal would remain essentially unchanged. The "Reduced Density" plan would contain 122 fewer residential units. However, the subtraction of space from these units is used to expand an interior private "pedestrian mews" for residents of the project. Thus, the private space is replaced with a different type of private space rather than the provision of open space. Moreover, commercial space that would benefit the community is dramatically reduced in this plan.

**What Existing Potrero Hill Neighbors Want:** The DEIR should include a meaningful reduced density alternative – one that eliminates at least one story of the residential building complex along 16th Street, widens the pedestrian "alley" along the western side of the development by at least 20 feet, and includes commercial space along 17th Street. The east-west "pedestrian mews" should be open and accessible to the general public. Heights (including mechanical penthouses) should be capped at 68 feet along 16th, and capped at 48 feet along 17th Street.

In conclusion, I strongly urge the Planning Department to reject this draft Environmental Impact Report and to not approve this proposal for housing.

I encourage the Planning Department to support the **Metal Shed Reuse Alternative Plan** as the only viable potential development for this property.

Sincerely,

Sean D Angles  
seanangles@hotmail.com



**From:** Jerry Doyle [jerrygjd@gmail.com](mailto:jerrygjd@gmail.com)  
**Subject:** Corovan  
**Date:** May 3, 2016 at 8:14 PM  
**To:** [contact@savethehill.com](mailto:contact@savethehill.com)



May 3, 2016

SAVE THE HILL  
[1206 Mariposa Street](#)  
[San Francisco CA 94107](#)

Dear Planning Commissioners,

As a long-time resident of Potrero Hill, who lives about half a block from the site of the Corovan project, I'm writing in opposition to the Corovan project as it is currently envisioned by the developers.

If built as proposed, the project's negative impacts would outweigh the good. The massive scale of the Corovan project would disfigure the aesthetics of the area's topography and overshadow the new Daggett Park, in addition to obstructing views for existing housing. Furthermore, plans for Corovan fail to address the lack of infrastructure in the area—infrastructure necessary for preventing the inevitable gridlocking of traffic on Mississippi and Sixteenth Streets that a parking garage exit at that intersection would entail. Development of the area as proposed would worsen density without adding infrastructure and must be reevaluated.

Development should not proceed until these issues have been addressed. I would also like to be assured that the Corovan project's developers will preserve the distinctive character of Potrero Hill by (1) prohibiting formula retail, (2) limiting the residential zoning in the area in order to preserve adequate public open spaces and community centers, and (3) incorporating the signature (and historically significant) warehouses and other industrial buildings of Potrero Hill into the development rather than demolishing them.

Thank you for your consideration and your time.

Sincerely,

Jerry Doyle  
[126 Texas Street](#)

---

[Contact@savethehill.com](mailto:Contact@savethehill.com)



**From:** Richard Hutson rchutson@comcast.net  
**Subject:** Corovan Project  
**Date:** May 3, 2016 at 6:15 PM  
**To:** Rod Minott roDMINOTT@hotmail.com  
**Cc:** Alison Heath alisonheath@sbcglobal.net



Dear Commissioners,

I oppose the proposed project on the Corovan site for a multitude of reasons but most importantly:

The addition of almost 400 new units to the several hundred that have just been built across the street will turn the bottom of the Hill into a ghetto with too many people jammed into too small of a space.

The project does not provide any meaningful community benefits or open public space.

The design of the 16th street building facade presents as an uninteresting massive wall. Although it will look somewhat better than the ugly blight on the neighborhood across the street - that you approved while the neighbors were asleep - it is still a massive wall.

The acknowledged - and unmitigated - overflow parking shortfall will will have a significant adverse impact the neighborhood.

The placement of the entrance to the limited off street parking on Mississippi Street will significantly exacerbate the daily traffic jams at one of - if not the worst - intersections in the entire city.

Approval of this project as currently proposed will be a slap in the face of the good citizens of Potrero Hill and a bow to ill advised development with little consideration of the existing scale and density of the neighborhood.

Respectfully,  
Richard Hutson  
347 Mississippi Street  
San Francisco, California

Sent from my iPad



May 2, 2016

Dear SF Planning Commissioners:

As a resident of Potrero Hill for over 18 years, I am writing you with regard to the Corovan property in Potrero Hill - 2011.1300E Notice of Preparation / 901 16th Street & 1200 17th Street.

Now that the Final Environmental Impact Report (EIR) has identified the Metal Shed Reuse Alternative as the environmentally superior project, I encourage you to replace housing on 17th Street with more space for artists, makers, non-profit groups, and neighborhood serving retail. I believe that will help reduce conflicts between Bottom of the Hill and new residents, as well as provide neighborhood services and amenities.

The developer's project is completely out of scale with the current neighborhood and violates several Area Plan principles. The project, as it is currently proposed, will dramatically change the character of the Hill and continue with the Mission Bay-type development already encroaching on our neighborhood.

Thank you.

Sincerely,

Vicente Agor  
225 Mississippi Street  
San Francisco, CA 94107



May 2, 2016

Dear SF Planning Commissioners:

As a resident of Potrero Hill for over 10 years, I am writing you with regard to the Corovan property in Potrero Hill - 2011.1300E Notice of Preparation / 1901 16th Street & 1200 17th Street.

I ask that City Planning please consider the use of distinctive materials and features of the current metal warehouses in order to create a historic continuity between old and new uses at the site. The Final Environmental Impact Report (EIR) has now identified the Metal Shed Reuse Alternative as the environmentally superior project.

In addition, I ask that you reduce parking and traffic congestion by shrinking the current project. As a pedestrian in this area during the morning and evening hours, the increased traffic from the current project will be a load too large for this neighborhood and streets to bear.

Thank you.

Sincerely,  
Mike Pfeffer  
225 Mississippi Street  
San Francisco, CA 94107



**From:** David Boyd [djboyd42@me.com](mailto:djboyd42@me.com)  
**Subject:** STATEMENT OF (CONTINUED) OPPOSITION TO COROVAN PROJECT  
**Date:** May 2, 2016 at 12:10 PM  
**To:** Minott Rodney [rodneyminott@outlook.com](mailto:rodneyminott@outlook.com)



Planning Commission  
City/County of San Francisco

Dear Commissioners:

With regard to the coming May 12th meeting of the Planning Commission, I would like, once again, to state my opposition to the proposed project for the Corovan site — officially designated the 901 16th St & 1200 17th St Project.

As you are very well aware, this Project has generated enormous opposition from residents of Potrero Hill for reasons that have been rehearsed ad nauseum. Since all the time and effort that has been expended to encourage the developers and the Commission to alter the project has resulted, to date, in only small incremental changes, you may wonder why I even bother writing yet another letter to the Commission.

There are basically two reasons:

- 1) The Project is simply way too huge for the site. It, together with the Daggett monstrosity already built across 16th St., simply bury the entrance to our neighborhood. As is plain to anyone who has traveled south on 7th St towards 16th St — or has seen the depiction of the 7th St/Mississippi St & 16th St intersection when both Daggett and the Corovan site projects are completed — Potrero Hill barely is visible behind these enormous human warehouses. Frankly, it is heartbreaking to see how our community will be altered. When we point out the impending disasters of increasing traffic congestion, parking shortages, lack of public transit, to name only a few, the DEIR even notes that they will get worse, but declares them "significant, but unavoidable" impacts. Basically, residents are told to "suck it up!" That is totally unacceptable.
- 2) It doesn't have to be this way! There is an alternative in play — the METAL SHED REUSE option. Although still quite large, it does lower the overall project height, reduces the number of residential units a bit, and at least attempts to retain some semblance of the historic site by repurposing rather than destroying the existing metal structures. As many others have noted, it is a very desirable compromise.

I strongly urge you, the Commissioners, to give thoughtful consideration to the METAL SHED REUSE option. It is a progressive option ... and we residents will have to "suck it up" just a bit less.

David J. Boyd  
1208 Mariposa St., SF



**From:** Yvonne Gavre [ygavre@me.com](mailto:ygavre@me.com)  
**Subject:** Letter Re Proposed Project for 16/17th Streets at Texas and Mississippi  
**Date:** May 1, 2016 at 5:44 PM  
**To:** Save The Hill [contact@savethehill.com](mailto:contact@savethehill.com)



May 1, 2016

To whom it may concern:

I am a native San Franciscan who was born on Rhode Island Street in 1942. I remember riding my bike down the hill with my friends to hang out in the steelyard where the proposed construction is set to take place (near the corner of 17th and Texas Sts.) It was a friendly, active place where we searched for scraps of metal and small materials. Often some of the workers fixed our bikes for us and we would bring them homemade jam and other foods that our families made.

I have been a resident of the Lower Potrero District since 1989 when I moved back to the area. At that time we were still a cozy little community comprised of residential and small industrial businesses. Some of the properties were being renovated in a manner complementary to the neighborhood. Times have changed. I am not against development. I am against thoughtless construction that pays little attention to the artistry and history of the area.

The proposed project is so large and out of character that it is shocking to the senses. Where is the open space? The shadows will be long and the sunlight will be short. A wind tunnel will emerge and residents will not feel uplifted by such a structure. Traffic and parking is already a problem and the additional noise and pollution is apparent right now. I used to have faith in the Planning Commission when Sue Bierman was a member. Now I just don't understand why this unbridled development continues.

I strongly protest this project. .

We, as a city, need to slow down and really look at the streets and neighborhoods in order to create good spaces for people to live and work in.

Sincerely,

Yvonne Gavre  
1208 Mariposa Street  
San Francisco, CA 94107

[ygavre@me.com](mailto:ygavre@me.com)



**From:** Anna Stern annastern1200@gmail.com  
**Subject:** Revise Corovan Project  
**Date:** May 2, 2016 at 5:39 PM  
**To:** rodneyminnott@outlook.com



Dear Planning Commission

I am writing to ask you to consider some points and to please modify the proposed Corovan Project.

Potrero Hill has already been hugely negatively impacted by the rapid growth of Mission Bay. Please reduce the mass and height of Corovan on 16th Street to reduce **eyesore rooftop mechanical/elevator/stair penthouses**. Reducing the size of the project will also reduce the impact of increased traffic, which has already become congested.

Also consider the character of the neighborhood. Replace housing on 17th Street with more diverse commercial uses such as space for artists, makers, non-profit groups, and neighborhood serving retail, that will help reduce conflicts between Bottom of the Hill and new residents, as well as providing neighborhood services and amenities.

Do not allow formula retail within the development. Potrero Hill is a real community we love and want to keep it as a community.

Best regards,

Anna Stern  
1200 18th Street



From: **Jim Wilkins** <[wilkins.jim@gmail.com](mailto:wilkins.jim@gmail.com)>

Date: Tues, May 3, 2016

Subject: Problems with the Draft Environmental Impact Report (DEIR) for the Corovan Site (901 16th/1200 17th Street; San Francisco) and suggestions for its improvement

Dear Commissioners,

Below please find my comments on the proposed development project at 901 16th / 1200 17th Streets. I am a resident of Potrero Hill and am appalled by the unbridled exploitation of our city that is currently taking place with your department in the lead. Many of the concerns raised above could be applied to other locations in the city. There seems to be a total lack of coordination between Planning and Transportation and the current project represents a particularly egregious example of this phenomenon.

### ***Problems With The Proposed Project***

#### **1) Inadequate & Inaccurate Analysis of Traffic, Parking and Transit Impacts**

##### **Jammed Intersections**

The DEIR acknowledges that the proposed Corovan project will significantly and unavoidably worsen traffic congestion. It identifies at least four intersections that will be severely impacted. These include:

- 17th & Mississippi Streets
- Mississippi & Mariposa Streets
- Mariposa & Pennsylvania Streets
- 7th/16th & Mississippi Streets.

The DEIR indicates there's currently no way of feasibly mitigating the increased traffic congestion at the above intersections, either due to lack of funding or practicality.

**DEIR Flaws:** Traffic data used in the DEIR was collected on a single day in 2012 during the peak evening commute. The DEIR does not



consider data collected over a period of time, or that includes the morning peak commute or a Giants game day. The DEIR also fails to consider cumulative impacts on traffic and parking from recent, present, imminent and long-range development projects.

The DEIR fails to adequately mitigate or address expected queuing in and out of the proposed project's Mississippi Street parking garage. With close to 400 units of housing and more than 24,000 square feet of commercial retail space, the project will generate an estimated 4,233 new car trips daily — with up to 12,361 trips daily by people entering and exiting the project. In spite of this compelling data, the report claims lines won't be significant and it defers responsibility for further studies or mitigation proposals until after the project is built. Additionally, the DEIR fails to sufficiently address expected pedestrian and vehicular hazards posed by the proposed development's single vehicle entry and exit point along Mississippi Street.

While the DEIR acknowledges the traffic-generating problems posed by the dense development, it does not adequately address the gravity of the situation nor does it satisfactorily assess proposed solutions. It ignores consideration of traffic calming measures proposed in previous years by SFMTA and community members for the Mariposa & Mississippi Street intersection as well as other intersections along Mariposa, and it relies on outdated data and a limited study of traffic conditions. In this way, the DEIR fails to identify solutions to predictable problems and neglects an invaluable opportunity to work with the community to mitigate those problems.

### **Parking Spillover**

The DEIR concludes the planned development will create spillover demand of between 358 - 458 parking spots — cars that will clog surrounding streets.

**DEIR Flaws:** The DEIR shirks responsibility for parking problems posed by the dense development by claiming no legal obligation, but it



should acknowledge the degree to which an alternate proposal and further requirements of the developers would prevent unnecessarily negative impacts.

### **Larger Traffic Impacts**

What the DEIR Says:

***Golden State Warriors Event Center:*** *Due to the relative timing of the proposals, the Warriors' event center project was not included in the cumulative analysis of the proposed project .... (T)he Event Center project would not cause any significant change to the results given in this report and may potentially reduce the percent contribution to the impacted intersection from the proposed project. (page 124, Part 2, Draft EIR, 901 16th St & 1200 17th Street, August 2015)*

**DEIR Flaws:** The notable and inexplicable passage above is another example of how the DEIR's analysis relies on outdated and inadequate traffic data from 2012 and 1998. The DEIR not only fails to adequately consider and analyze the traffic and parking impacts of the Warriors Arena proposed for 3rd & 16th Streets, it claims that the Warriors Arena might actually help by shrinking the Corovan development's proportional contribution to traffic congestion. This absurd and unsubstantiated argument minimizes one of the most troubling aspects of the Developer's proposal.

Finally, the DEIR references only one large development in the area, 1000 16th Street (Daggett), while ignoring many other impactful projects in the pipeline including 1301 16th Street, 1601 Mariposa Street, 88 Arkansas Street, 249 Pennsylvania, 98 Pennsylvania, 1001 17th / 140 Pennsylvania, 790 Pennsylvania & 22nd Street, 580 De Haro Street, 540 – 522 De Haro, 131 Missouri Street, 1150 16th Street, 801 Brannan Street, 975 Bryant Street, 645 Texas Street, and 1717 17th Street.

### **Interstate 280 Ramps at Mariposa**



**DEIR Flaws:** The DEIR perpetuates the false claim that traffic impacts caused by the Corovan project to the I-280 on and off ramps at Mariposa Street will be significantly lessened through various mitigations – for example, new traffic signals and the expansion of Owens Street to connect Mariposa and 16th Street. These so-called mitigation measures were identified in the Mission Bay Environmental Impact Report – a study that is now 17 years old and outdated. Both the Mission Bay EIR and the recent Warriors Arena transportation report fail to offer adequate mitigations and analysis to reduce increased volume of traffic to and from Potrero Hill from past, present, and reasonably foreseeable future development.

### **Inadequate Public Transit**

**DEIR Flaws:** Adding thousands of residents with little investment in public transit will be a disaster for the neighborhood, resulting in further dependence on cars while traffic congestion grows and degrades our quality of life. For example, the 10 Townsend bus is already at 95% capacity yet the Corovan DEIR claims no mitigation measures are needed. This is indeed a ridiculous statement with no basis in fact. Public transportation to the site is limited to a single future bus line that is already overburdened, underfunded, and suffering maintenance and scheduling difficulties. The San Francisco Municipal Transportation Agency's (SFMTA) projections state that the future 22-Fillmore line serving an extended 16th Street transit corridor will be overburdened from the start -- constrained by funding challenges, inadequate bus capacity and service, rising amounts of automobile and truck congestion, and uncertainty about the future of the Caltrain tracks, High Speed Rail, and the I-280 freeway. SFMTA's own forecast through 2035 projects that the intersection at 7th, 16th, and Mississippi Streets will "degrade" to a service level of "F" – among the worst in the city. Yet the DEIR fails to adequately address and mitigate these significant impacts.



**What IS NEEDED?:** A “Transit First” policy should put transit first and ensure that viable options be in place before we experience significant population growth. New studies of existing and cumulative conditions, inadequately addressed in the Corovan DEIR and not anticipated in the Eastern Neighborhoods Environmental Impact Report, must now be priority and undertaken.

City Planning should conduct additional traffic studies that are current and robust. Traffic calming measures (such as bulb-outs and pedestrian islands) should be approved and implemented. Furthermore, the Developers should be expected to invest in more traffic reducing strategies and should collaborate with both the community and City Planning on an alternate proposal to achieve this outcome. The Developers propose a very ambitious, large-scale development for a very sensitive site, and it is reasonable that they shoulder more of the responsibility for traffic reducing measures in the surrounding area. A reduction in the density of the project is only one way they could positively impact traffic problems posed by their proposal.

## **2) Inadequate & Inaccurate Study of Land Use (And Planning Policies Ignored)**

### **Largest & Densest**

**DEIR Flaws:** As proposed, the developer’s project (72-82 ft. / 395 housing units) would be one of the largest, densest building developments in Potrero Hill history. Yet City Planning’s previous environmental studies and projections for Potrero Hill fail to take into account a project of this scope at this site – including its impacts. Official analysis currently on record in the Eastern Neighborhoods Plan considered heights of between 45 feet - 50 feet at the property, not 72 feet to 82 feet. The DEIR fails to address this discrepancy.

### **Topography of Potrero Hill**



**DEIR Flaws:** The developer's project violates multiple Area Plan principles including provisions to "respect the natural topography of Potrero Hill", to lower building "heights from the north to south side of 16th Street", and to "promote preservation of other buildings and features that provide continuity with past development." The DEIR fails to adequately address these issues.

### **Loss of Production, Distribution, Repair (PDR)**

**DEIR Flaws:** The proposed project would eliminate rather than retain 109,000 square feet of valuable Production, Distribution, and Repair space. The DEIR acknowledges this loss as a significant impact but nonetheless defends it as consistent with planning goals. The DEIR does not consider this proposed development in the context of broader, unanticipated, PDR losses both in our neighborhood and across the Eastern Neighborhoods – and this is yet another example of how the DEIR fails to incorporate new and accurate data. A clear remedy at this site would be to retain some portion of the project for light PDR, or "Trade Shop", uses.

### **Area Plan & City Policy Objectives And Principles Ignored**

**DEIR Flaws:** The proposed project conflicts with the Showplace Square / Potrero Area Plan, and the Urban Design and Housing Elements of the City's General Plan by disregarding policies of preserving neighborhood scale and character, providing adequate infrastructure, and preserving PDR uses. Both the Corovan development project and the DEIR fail to address the following consistency issues:

#### ***A. Objective 3 of the San Francisco General Plan's Urban Design Element:***

"Moderation of major new development to complement the city pattern, the resources to be conserved, and the neighborhood environment."



The scale and density of the Prado/Walden project are substantially greater than existing surrounding land uses and the project would be inconsistent with the established land use character of the neighborhood. The DEIR fails to acknowledge and consider that the Daggett Triangle development at 1000 16<sup>th</sup> Street in Showplace Square, as well as other large developments in nearby Mission Bay, are in separate and distinct neighborhoods that are not part of the Corovan site in Potrero Hill.

*B. Objectives of the Showplace Square / Potrero Area Plan*

The Prado/Walden project conflicts with a number of Area Plan objectives including Objective 1.2, which promotes development in keeping with neighborhood character. This project is inconsistent with the established neighborhood character of Potrero Hill. The Showplace Square/Potrero Hill Area Plan, in Policy 3.1.6, states that, "new buildings should epitomize the best in contemporary architecture, but should do so with a full awareness of, and respect for, the height, mass, articulation and materials of the best of the older buildings that surrounds them." As proposed, the project fails to match the height, mass, and articulation of existing buildings in the Potrero Hill vicinity and provides little awareness of surrounding structures.

*C. Policy 2 of the City's General Plan:* "That existing housing and neighborhood character be conserved and protected in order to preserve the cultural and economic diversity of our neighborhoods."

The Prado/Walden project is not consistent with this policy because scale, mass, bulk and height are inconsistent with and will negatively impact established neighborhood development pattern and character. The proposed development is dramatically out of scale with nearby residences and small businesses.

**What IS NEEDED?:** For the DEIR to fulfill its purpose, it must include updated data reflecting neighborhood growth and it must acknowledge



Area Plan principles developed and accepted by the community. Among other things, the cumulative loss of PDR to the City has not been accurately addressed and evaluated in the DEIR, and we ask that this study be conducted. Since the extent of the cumulative loss of PDR space was not fully anticipated in the 2008 Eastern Neighborhoods' Environmental Impact Report and no mitigations were identified, these impacts require further study in the EIR for this project. Analysis should include a full exploration of feasible mitigations such as the inclusion of significant amount of new PDR space onsite.

DEIR needs to adequately address, analyze, and mitigate the aforementioned growth and planning policies and consider an alternate to the developer's proposal so as to honor these considerations. Moreover, the DEIR fails to acknowledge and consider that the south side of 16th Street in this area remains part of Potrero Hill and not Showplace Square or Mission Bay, which are separate and distinct neighborhoods. Showplace Square's 1000 16th Street (Daggett Triangle) project is neither appropriate for or consistent with the character of Potrero Hill. This fact is clearly established in City planning policy and principle and should be respected and complied with. This issue should be addressed by City Planning in a final EIR.

### **3) Inadequate & Inaccurate Analysis of Recreation & Open Space**

#### **Inadequate Parks**

**DEIR Flaws:** The DEIR fails to adequately consider the impact of the developer's proposal on our open and recreational space. Potrero Hill currently suffers from inadequate parks, open space, and recreational facilities. The addition of thousands of residents from this and other new large developments will put significant additional strain on nearby parks including Jackson Playground – already heavily used and lacking in maintenance upgrades. Moreover, the vast majority of so-called open space provided in the developer's currently proposed project would remain private and off limits to the public. It is absurd for



this type of space to be allowed to be called "open space" when there is no public access.

**What IS NEEDED?:** The DEIR should include data and projections accounting for the dearth of recreation and open space and the degree to which developments already in the pipeline will further tax these inadequate resources. Planning should offer mitigations, including the addition of new parks that achieve the four acres promised by the City in prior planning reports. The Developers proposal should be revised to provide more open space accessible to the general public. For example, Planning should require the east-west "pedestrian mews" remain open to the public and not privately closed off space. The north-south pedestrian alley of the developer's project should also be widened by 20 feet and include more green soft-scape.

#### **4) Inadequate Study and Mitigation of Soil Hazards**

##### **Contamination Risks**

**DEIR Flaws:** The DEIR fails to adequately address the hazardous materials that will be exposed during construction. Furthermore, the DEIR does not acknowledge the fact that a kindergarten operated by the ALT School plans to open its doors in an adjacent building (99 Missouri Street) in the Fall of 2016.

**What We Want:** The DEIR should be revised to include more specific information about hazardous soils and measures to protect children (who are more vulnerable) and neighbors from exposure during demolition, excavation, and remediation. To date the City has not treated this issue seriously enough. Planning should address and analyze the potential risks of a new children's school (AltSchool) locating next to the Corovan site and detail mitigation measures that go well beyond what is currently planned. The California Department of Toxic Substances should also be involved in monitoring and



coordinating this effort to ensure the safety of both children and neighbors.

## **5) Inadequate and Inaccurate Study of Population / Housing**

### **Excessive Density & Outdated Data**

**DEIR Flaws:** Recent analysis shows the Potrero Hill / Showplace Square area has already exceeded the number of housing units and population growth the City planned and projected for 2025! The Planning Department assumed up to 3,891 housing units would be built by 2025 in the Potrero Hill / Showplace Square area. But as of 2015, 3,953 units were already in the pipeline or built. The City failed to anticipate the dramatic pace of development and has not delivered on its promise to provide necessary public improvements (parks, transit, roads, etc.) to support thousands of new residents. CityPlanning analysis understates the “cumulative impacts” of large developments on our community by continuing to rely on outdated data from the 2008 Eastern Neighborhoods Environmental Impact Report to inform analysis in the EIRs of large projects, including the proposed Corovan development. Assumptions and mitigation measures provided in that document are simply no longer valid.

**What IS NEEDED?:** The City failed to anticipate the dramatic pace of development and has not delivered on its promise to provide necessary public improvements (parks, transit, roads, etc.) to support thousands of new residents. In this as in other areas, we ask for more recent and relevant data to account for the extraordinary changes in this area’s density. In order for the DEIR to be constructive for the neighborhood and for the city, it must incorporate new and accurate population data, and it must acknowledge the degree to which public improvements lag behind the neighborhood’s growth. City Planning needs to acknowledge that Potrero Hill has already exceeded development targets projected for 2025. Environmental study and



mitigations should reflect this fact to help inform current and future planning.

## **6) Inadequate and Inaccurate Study of Historic Buildings**

**DEIR Flaws:** The DEIR does not adequately or accurately address issues related to the historic merit and integrity of the existing metal warehouses. The draft rejects arguments supporting historic integrity for the metal buildings. Evidence, including the research and opinion of a highly respected architectural historian, Katherine Petrin, demonstrates these buildings remain historic despite alterations and company mergers over the years.

We believe Petrin wrote up a very solid report that documented a strong case for historic integrity. The period of significance was longer than City Planning's claim of 1906 – 1928. And while the steel warehouses may have been altered to some degree over the years (they were built between 1908 and 1926), modifications in industrial spaces are to be expected given the utilitarian purpose of these buildings and the need for flexible space.

Collectively, the Potrero Hill industrial complex contains the last remaining structures of the Pacific Rolling Mill, which began operating in the Central Waterfront in 1868 before reorganizing and relocating to Potrero Hill in the early 1900s. The buildings are also the last remaining extant structures of the merged companies, Judson-Pacific Company (1928), and Judson-Pacific-Murphy Company (1945) in San Francisco.

Photos of the buildings at 17th & Mississippi Streets from 1941 verify the intimately linked heritage and history of the Pacific Rolling Mill and its successor companies. Two SF News Call Bulletin photos show the following sign on the red brick office building: "Judson-Pacific Co. Successor To Pacific Rolling Mill Co. Established 1868."



A photograph from 1941 demonstrates that the corrugated steel building at 1200 17th Street was not simply an unenclosed shed with open side walls up until December 1947. A partial photograph of the structure clearly shows an enclosed building that matches its present day aesthetic (see Petrin, Evaluation of Integrity pg. 8).

Moreover, Edward Noble (the son of Patrick Noble who founded the reorganized Pacific Rolling Mill) headed the company as President after his father's death in 1920 and continued running the the firm long after the first of two mergers. He remained at the helm until 1945 and was aided along the way by employees who had been hired at the original Pacific Rolling Mill at both the Potrero Hill and Central Waterfront sites.

**What IS NEEDED:** The alternate plan proposed by City Planning incorporates the historic metal structures with new construction. This reasonable compromise should be modified to achieve an environmentally superior status and be adopted. The City should revise the DEIR to reflect the historic significance and integrity of these buildings based on Petrin's report.

## **7) Inaccurate and Inadequate Study of Alternative Project Proposal**

### **"Metal Shed" Reuse Alternative Plan**

**DEIR Flaws:** The DEIR includes City Planning's modified version of an alternate project plan submitted by Save the Hill (see "Metal Shed Reuse Alternative", Chapter VI). While some aspects of this alternate, lower-density "adaptive reuse" proposal are commendable, other aspects are inadequate and some of the data from which this proposal is driven is simply flawed. City Planning took Save The Hill's original suggestive renderings and manipulated numbers to suggest that PDR space set aside for artists would generate volumes of vehicle traffic equal to the developer's vastly bigger project proposal. The effect was



to deny awarding the adaptive reuse alternate plan the designation of "environmentally superior". I question the adequacy and accuracy of City Planning's methodology to analyze the traffic impact of light or low impact PDR Trade Shop / artist workspaces.

**What IS NEEDED?** : City Planning needs to acknowledge the "suggestive" nature of the proposed renderings submitted by Save The Hill in its proposed adaptive reuse project, and more specifically that the PDR / Trade Shop component of the proposal was intended for light and low impact purposes. We ask that City Planning's version of an adaptive reuse plan be revised such that inclusion of light or low impact PDR / Trade Shop workspaces achieve environmental superiority.

Currently the adaptive reuse alternative project allows for 177 units of housing while retaining the existing metal buildings. While Save The Hill is willing to support this plan with height limits at 58 feet along 16th Street and 48 feet along 17th Street, we ask that any added height for mechanical/stair penthouses be capped at 68-feet, instead of 74.5 feet. Save The Hill is more than willing to work with both the developer and City Planning to improve this alternative as a workable solution.

### **"Reduced Density" Alternative Plan**

**DEIR Flaws:** As noted, the DEIR includes analysis of a "Reduced Density" alternate plan that is identified as "environmentally superior" (see "Reduced Density Alternative", Chapter VI). While Save the Hill supports reduced density, this plan does not nearly go far enough. Under this alternate plan, the height, scale and massing of the developer's current project proposal would remain essentially unchanged. The "Reduced Density" plan would contain 122 fewer residential units. However, the subtraction of space from these units is used to expand an interior private "pedestrian mews" for residents of the project. Thus, the private space is replaced with a different type of private space rather than the provision of open space. Moreover,



commercial space that would benefit the community is dramatically reduced in this plan.

**What IS NEEDED?:** The DEIR should include a meaningful reduced density alternative – one that eliminates at least one story of the residential building complex along 16th Street, widens the pedestrian “alley” along the western side of the development by at least 20 feet, and includes commercial space along 17th Street. The east-west “pedestrian mews” should be open and accessible to the general public. Heights (including mechanical penthouses) should be capped at 68 feet along 16th, and capped at 48 feet along 17th Street.

Regards,

James A. Wilkins, Ph.D.  
254 Pennsylvania Av.  
San Francisco, CA 94107



**From:** Ray Kerr raykerrsf@hotmail.com  
**Subject:** Corovan  
**Date:** May 1, 2016 at 9:42 PM  
**To:** rodneyminnott@outlook.com  
**Cc:** Raymond Kerr raykerrsf@hotmail.com




Dear Planning Commission,

Good architecture enhances and supports the integrity of the place on which it is built. This project doesn't compliment the neighborhood, it obliterates it. Every last tract of land seems destined to become a high rise condo along this corridor, with little or no thought for traffic impacts and strain on municipal resources. (Does Hetch Hetchy provide us with an unlimited supply of water?) The impact of enormous projects on Brannan between 7th and 8th, a whole block of Harrison at 8th, the traffic circle end of Rhode Island, and on 16th at 7th, not to mention the two gigantic projects in Dogpatch by 280, all these have yet to be felt, so it seems rash to hastily rubber stamp another sprawling behemoth without carefully considering the repercussions. Might want to wait for those traffic reports to come in. I guarantee you they won't look good. The neighborhood has already become an industrial ghost town and you can be sure there will be plenty of condo-plexes for you to consider as the owners of those lots come before you, hat in hand. Thriving local businesses like San Francisco Gravel and California Casters are now shuttered; even Center Hardware, the best hardware store in Northern California, is slated to become the next row of condos, blotting out the sun. These vast monolithic developments have become the new urban McMansions, completely out of scale with and showing no regard for the neighborhood they occupy.

We can do better. I hope you do.

Raymond Kerr, Potrero resident



**From:** Jani Mussetter [janimusse@gmail.com](mailto:janimusse@gmail.com)   
**Subject:** Old Pacific Mill/Corovan Project  
**Date:** May 3, 2016 at 8:33 PM  
**To:** Rodney Minott [rdminott@hotmail.com](mailto:rdminott@hotmail.com)

Dear Commissioners,

It breaks my heart that every time I turn around another high density monstrosity is being built in this once fair city of ours. These buildings are stripping the character and style away from San Francisco. Mission bay looks like San Jose! **PLEASE, I BEG YOU** to vote for an adaptive reuse for the Cordovan/Old Pacific Rolling Mill buildings!!! **PLEASE** think of how cool the Ferry Building is and why it "fits in". I understand in this capitalist society that people who buy a building should do what they want, but **NOT** when it **NEGATIVELY IMPACTS** an entire neighborhood!! I'm still trying to cope with that horrendous Daggett project on 16th St. The massiveness of that building is overwhelming!! The traffic on that corner and that corridor is unforgiving!

**How you maintain charm and character in a neighborhood is by using the existing buildings!!! Also it's more sustainable for the environment!!!!!! And you will make the people that actually live there....HAPPY!!!!**

**Please reduce the size and height!!! It's too massive....again!!!**

I own my home, but it does not have a garage, so I rely on street parking. I'm **REALLY** concerned about parking over here!! What is going to happen when that hellacious Daggett is filled? If it ever does gets filled.

From the Chronicle:

"San Francisco residents know that when you build luxury housing, rents and evictions in the surrounding neighborhood go up," she said. "They don't want whole neighborhoods being bulldozed so you can build housing for the wealthy."

<http://www.sfgate.com/bayarea/article/Cost-of-living-traffic-have-a-third-of-Bay-Area-7386717.php?cmpid=twitter-tablet#photo-2705807>

Thank you,  
Jani Mussetter





**From:** Rita Meakin <rcmeakin@me.com>

**Date:** May 1, 2016 at 1:13:58 PM PDT

**To:** "contact@savethehill.com" <contact@savethehill.com>

**Subject:** Corovan project and it's impact on the already overwhelmed Potrero Hill.

As a resident of Potrero Hill for the last 20 years I am dismayed at the city of San Francisco and in particular the planning commission. I actually watch city TV and the planning commission in particular so have watched in real time the projects they have approved without any regard for this wonderful city. Oh yes we need housing but not for the people who developers target but rather the poorly paid and the newly evicted.

Everyone is not as lucky as the people Tony Robbins (spelling ????) helped when he read about their plight. The congestion everywhere in this city is beyond measure. The city applauds Über and Lyft because it promotes less cars and by extension fewer garage places in new buildings. I fume when these drivers double park wherever they choose, put on their blinkers and never look up. There is no reason to believe that this new idea is solving any traffic issue but to the point of housing and Corovan in particular this overbuilt project that we have fought for so long continues to be moving in the wrong direction. The developers in all these projects do not live in the city do not care about the city and are arrogant when confronted about issues with the usual comment, "we will look into it". Today, Sunday May 1, I watched a rerun of a Muni plan for the third st line, possible removal of 280 and more development of the train yard.....these people are madder than hatters. I watched the same man the other night at the planning commission reject the proposed building on 1st and Harrison because of parking issues as millions of cars descend on the Bay Bridge in the evening. There should be a comment by every person who speaks telling all of us where they live. I know this is a scattered letter but it is sincere and I hope all of our letters are read. I was born in San Francisco, my children were raised here, and prior to Potrero Hill I lived on Willard street near U C medical center. I do not feel I'm a complainer just someone who realizes the city and it's streets have already reached their max. I just returned from London where their congestion zone for cars is 49 square miles the exact size of San Francisco. This is not a big city so don't try to make it London or New York.

Thank you, Most sincerely, Rita Meakin.

PS by the way if anyone has gotten this far FIX THE POTHOLEs, OBSERVE the TRAFFIC on LAGUNA and FELL or OAK in the AM and hire a person to direct people who block the box. Solve the current issues and problems and then say yes to a reduced version of the Corovan Project.



Commissioners:

I am writing concerning the planned development of the property at 901 16<sup>th</sup> Street/1201 17<sup>th</sup> Street/Mississippi Street.

Although I support the development of this derelict site, what is proposed appears to be a virtual whale in a rather tight pool. The issues are building-bulk and traffic flow, not just of the building itself, but rather as a system, as part of cumulative effect of too many buildings being dropped into tight confines with little realistic understanding of ACTUAL traffic on the ground.

Already this neighborhood is suffering from development indigestion, especially when large building after large building is approved, adding burden to streets that were never designed for the traffic capacity we are currently experiencing, and certainly not capacious enough for what is proposed.

The traffic flow and constriction in this neighborhood and specifically surround this lot is already quite over-burdened. Adding nearly 400 new dwellings, and resulting cars will truly be a crushing load that will bring traffic to a standstill. This will bring additional frustration to residents, both established and prospective, that we are already experiencing with too much development, too fast. It would seem to make sense to delay or scale back this project to allow the neighborhood to digest the properties that are already in development.

The commissioners need to adopt a more systems-oriented approach to approving these massive developments. On their own, one or two of these projects might be digestible, but coming one on top of the other.

In terms of specifics, development for this project needs to address Mississippi Street. For purely practical reasons, there should be no entrances or exits on Mississippi Street, which is already strained as the connection between the Mariposa Exit of 280 and 7<sup>th</sup> street to SOMA.

I would hope that the commissioners be more prospective about what they are creating, remembering that they are creating a cumulative effect that magnifies the impact of each project. The impact of a project this massive can best be addressed with a reduction of scale.

Jason Benlevi  
325 Mississippi Street



**From:** [Rodney Minott](#)  
**To:** [Townes, Chris \(CPC\)](#)  
**Cc:** [Minott Rodney](#)  
**Subject:** 901 16th / 1200 17th community feedback  
**Date:** Tuesday, February 02, 2016 9:39:26 AM

---

Hi Chris,

I'm writing on behalf of Save The Hill in regards to the proposed project at 901 16th / 1200 17th Streets, also known as "The Corovan Site", Case no. 2011.1300.

As you may know Save The Hill is a grassroots neighborhood group in Potrero Hill. A week ago we held a community meeting to gather input on the latest renderings of the proposed Corovan development. The meeting was held at Live Oak School and was the latest of at least two dozen community meetings Save The Hill has organized over this project in the past four years. Our outreach has been very extensive and thorough and our meetings have been very well attended. Our large base of followers remain engaged and committed.

I wanted to share with you concerns expressed most passionately and unanimously at our recent meeting. These are concerns that, frankly, have remained consistent and unchanged since the current project was unveiled in its latest iteration two years ago. Last week we shared these concerns (as we've done previously) with the Corovan site developers — Josh Smith and Dan Safier.

**1) Height and Massing:** The proposed buildings for the 16th Street component remain too tall. The developer's current plan proposes heights of between 72 feet and 83 feet. Sizable clusters of rooftop mechanical/stair/elevator penthouses will average between 78 feet and 83 feet. These heights remain unacceptable to us. Especially egregious is the Northwest corner building that is adjacent to the pedestrian promenade and utilizes oversized rooftop elevator and stairway penthouses. Common refrains voiced at our meeting about the 16th Street buildings included the following: "It's too big"; "We don't want another Daggett" [the project at 1001 16th Street]; "Reduce the height and scale." Attendees called for breaking up the mass of the 16th Street buildings ("make 16th Street look more like 17th Street"). The 16th Street structures have the effect of creating a cavern on 16th and walling off Potrero Hill. Moreover, the Potrero Hill Area Plan specifically calls for following topography and reducing height starting on the south side of 16th Street. Consensus formed around reducing height on 16th Street to 58 feet with penthouse mechanical/elevators/stairways capped at 68 feet. Supervisor Malia Cohen has also indicated to us that she is not supportive of the developer's current proposed heights, nor the scale of the 16th Street buildings. This is a "Gateway" location to the neighborhood and the developer's current proposal fails to treat it as such. Other feedback: widen the sidewalks for 16th and 17th Streets by trimming back building facades and including more green landscaping.

**2) Open Space:** Proposed public open space in the project remains inadequate. The public pedestrian North-South promenade should be widened to 40 feet throughout the passageway. There should be more green softscape and less hardscape. A 40-foot wide promenade would significantly improve the pedestrian experience, enlarge publicly accessible open space, encourage public gathering, and mitigate shadowing. As a side note, as you know the developers of 1601 Mariposa agreed to dramatically widen a similar pedestrian passageway for their project. Unfortunately, the developers for the Corovan project are proposing mostly inauthentic open spaces that will remain inaccessible to the general public. These include large rooftop decks (particularly on the Northwest corner of the project) that are merely amenities and are unnecessary.

**3) Authenticity / Adaptive Reuse:** The developer should do more to include elements of adaptive reuse in the project. This would entail retaining and incorporating into the proposed project more signature features and materials of the existing metal warehouses. Save The Hill



submitted to the developers specific examples of what could be retained and repurposed. As one example, the steel framing of the western metal warehouse (currently the green/red warehouse running between 17th and 16th Streets) could be retained and utilized as a “canopy” for the pedestrian promenade. The overall effect of incorporating original features and materials would be more respectful of existing neighborhood character.

**4) Commercial / Retail On 17th Street, More Diverse Uses:** 17th Street buildings should be less residential and more commercial / retail. The goal would be to truly diversify the mix of uses to include spaces for makers, artists, non profits, and neighborhood serving retail. This would also help shield the Bottom of the Hill nightclub (across the street) from potential noise complaints filed by new residential tenants. Formula retail should not be allowed at either 17th Street or 16th Street.

**5) Traffic:** Reducing the scale of the project would substantially mitigate significant traffic and parking congestion impacts. A more robust array of traffic calming measures should be implemented – measures that would entail pedestrian bulb-outs and marked crosswalks for surrounding streets — including at 17th and Texas, Mariposa and Texas, and Mississippi and Mariposa (western edge). Currently planned traffic mitigations remain inadequate. Other comments from attendees: turn Mississippi into a one-way street; the City needs to take a more holistic/comprehensive view of traffic impacts for this area of Potrero Hill; Mariposa remains backed up and in gridlock both mornings and afternoons; traffic signals would not be appropriate for the Mississippi & Mariposa Street intersection; eliminate or dramatically reduce all parking spots for the planned development.

I'd be happy to discuss the above with you at your convenience.

Best,

Rodney  
on behalf of Save The Hill  
cc: Susan Brandt-Hawley, attorney  
Supervisor Malia Cohen



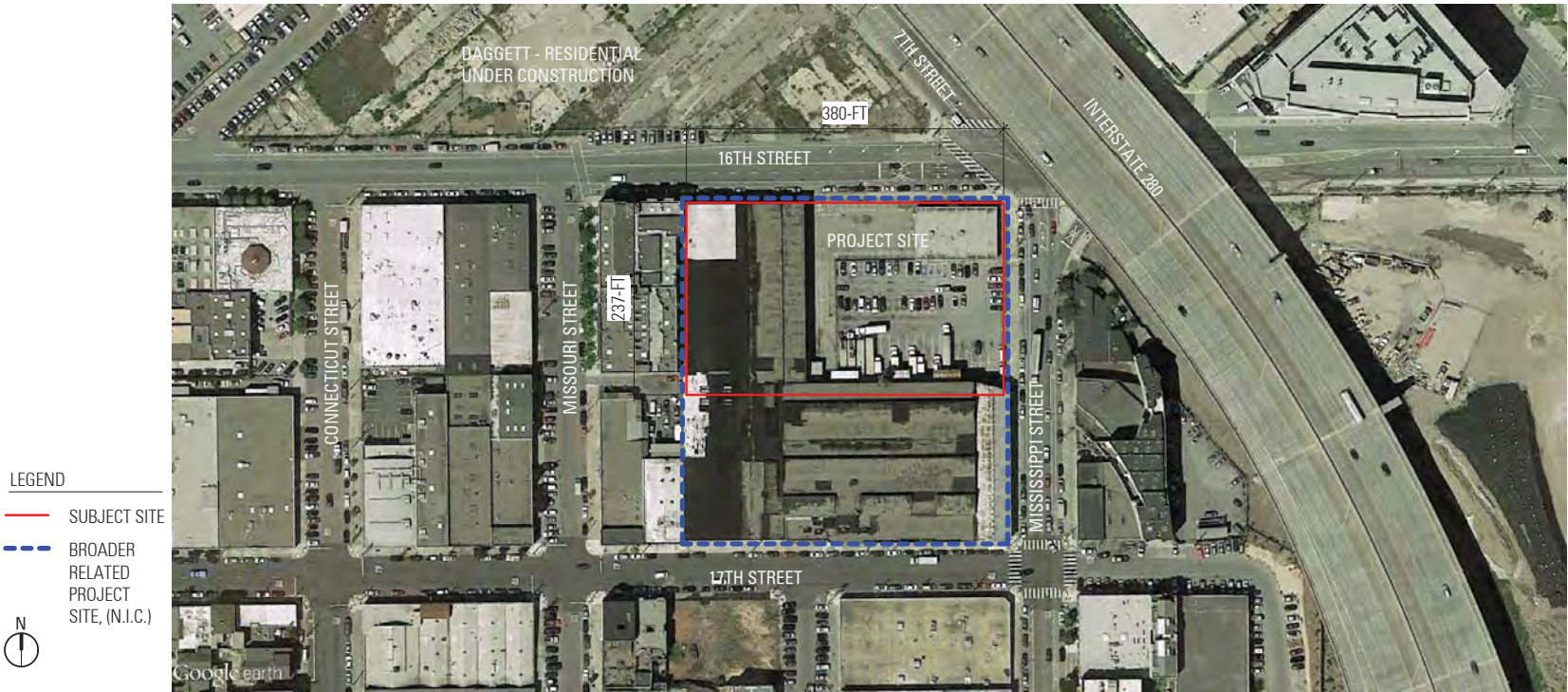
# 901 16th STREET

PLANNING COMMISSION HEARING -- MAY 12, 2016

**SCOPE OF WORK:** REQUEST FOR LARGE PROJECT AUTHORIZATION (LPA) PER PLANNING CODE SEC. 329 AND ADOPTION OF CEQA FINDING TO DEMOLISH 105,000 SF OF EXISTING PDR WAREHOUSE USE AND TO CONSTRUCT:  
A NORTH BUILDING (901 16TH STREET) - A 6-STORY, 68-FOOT TALL, 260 DWELLING UNIT, MIXED USE BUILDING WITH 20,318 SF OF RETAIL AND 263 PARKING SPACES; AND,  
A SOUTH BUILDING (1200 17TH STREET) - A 4-STORY, 48-FOOT TALL, 135 DWELLING UNIT, MIXED USE BUILDING WITH 4,650 SF OF RETAIL AND 125 PARKING SPACES.  
A PUBLICLY ACCESSIBLE PEDESTRIAN ALLEY CONNECTING 16TH STREET AND 17TH STREET WOULD BE CONSTRUCTED ALONG THE WESTERN PROPERTY LINE.  
UNDER THE LPA, THE PROJECT SEEKS MODIFICATION TO THE FOLLOWING PLANNING CODE REQUIREMENTS:

1. REAR YARD (P.C. Sec. 134)
2. PERMITTED OBSTRUCTIONS OVER THE STREET (P.C. Sec. 136)
3. PARKING/LOADING ENTRANCE WIDTH (P.C. Sec. 145.1)
4. OFF-STREET PARKING MAXIMUM (P.C. Sec. 151.1)
5. OFF-STREET LOADING MINIMUM (P.C. Table 152.1)
6. HORIZONTAL MASS REDUCTION (P.C. Sec 270.1)
7. ACCESSORY UNITS PROVISIONS FOR DWELLING UNITS (P.C. Sec 329(d)(10))

## VICINITY MAP



## 901 16TH STREET, 1200 17TH STREET SUMMARY DATA

|  | Gross     | Gross     |             | Total |     |       |       |       | Res     | Retail   | Car   | Total      | Publicly        | Common | Private      |
|--|-----------|-----------|-------------|-------|-----|-------|-------|-------|---------|----------|-------|------------|-----------------|--------|--------------|
| Parcel   | Site Area | Bldg Area | Retail Area | Units | St. | 1 Bds | 2 bds | 3 bds | Parking | Parking* | Share | Open Space | Accessible O.S. | O.S.   | O.S.         |
| Existing Condition                                 | 152,000   | 105,000   | -           | -     |     |       |       |       | -       | -        | -     | -          |                 | -      | -            |
| 901 16th Street                                    | 90,060    | 402,943   | 20,318      | 260   | 53  | 103   | 95    | 9     | 221     | 39       | 3     | 26,180     | 7,514           | 18,666 | Not Included |
| 1200 17th Street                                   | 61,940    | 213,009   | 4,150       | 135   | 0   | 79    | 51    | 5     | 115     | 8        | 2     | 18,257     | 4,705           | 8,602  | 4,950        |
| Total  | 152,000   | 615,952   | 24,468      | 395   | 53  | 182   | 146   | 14    | 336     | 47       | 5     | 44,437     | 12,219          | 27,268 | 4,950        |
|  |           |           |             |       | 13% | 46%   | 37%   | 4%    |         |          |       |            |                 |        |              |
| * Based on 1200 17th Street alternate parking plan |           |           |             |       |     |       |       |       |         |          |       |            |                 |        |              |

## PROJECT TEAM

|                      |  |                                |  |                   |  |                           |  |
|----------------------|--|--------------------------------|--|-------------------|--|---------------------------|--|
| OWNER:               |  | ARCHITECT:                     |  | LANDSCAPE         |  | CIVIL                     |  |
| Potrero Partners LLC |  | BAR Architects                 |  | PGAdesign Inc.    |  | BKF ENGINEERS             |  |
| Josh Smith           |  | David Israel, Chris Haeggglund |  | Cathy Garrett     |  | Eric Girod                |  |
| 445 Virginia Street  |  | 543 Howard Street              |  | 444 17th Street   |  | 4670 Willow Road, STE 250 |  |
| San Mateo, CA 94402  |  | San Francisco, CA 94105        |  | Oakland, CA 94612 |  | Pleasanton, CA 94588      |  |
| 650-348-3232         |  | 415-293-5700                   |  | 510-550-8852      |  | 925-396-7751              |  |

901 16th Street

COVER SHEET

## SHEET INDEX

| 05/12/16 SUBMITTAL 2 | 06/19/14 SUBMITTAL | SHEET NAME                                | 05/12/16 SUBMITTAL 2 | 06/19/14 SUBMITTAL | SHEET NAME   |
|----------------------|--------------------|---|----------------------|--------------------|--|
|                      |                    | LARGE PROJECT AUTHORIZATION               |                      |                    | PLANNING CODE DIAGRAM ADDENDUM                                 |
| X                    | X                  | CS COVER SHEET                            |                      |                    |  |
| X                    | X                  | G0.1 PROJECT DATA                         | X                    | -                  | 16-A MODIFICATION REQUEST 01: REAR YARD - Sec. 134             |
| X                    | X                  | G0.2 PLANNING CODE DIAGRAMS               | X                    | -                  | 16-B MODIFICATION REQUEST 02: OBSTRUCTIONS - Sec. 136          |
| X                    | X                  | G0.3 MODIFICATION REQUEST                 |                      |                    |  |
| X                    | X                  | G0.4 CONTEXT PHOTOS                       | X                    | -                  | 16-C MODIFICATION REQUEST 03: ACTIVE FRONTAGE - Sec. 145       |
| X                    | -                  | G1.1 PROJECT VIEW                         | X                    | -                  | 16-D MODIFICATION REQUEST 04: OFF STREET LOADING - Table 152.1 |
| X                    | -                  | G1.2 PROJECT VIEW                         |                      |                    |  |
| X                    | -                  | G1.3 PROJECT VIEW                         | X                    | -                  | 16-E MODIFICATION REQUEST 05: BULK LIMITATIONS - Sec. 270.1    |
| X                    | -                  | G1.4 PROJECT VIEW                         |                      |                    |  |
| X                    | -                  | G1.5 PROJECT VIEW                         | X                    | -                  | 16-F COMPLIANCE - OPEN SPACE - Sec 135                         |
| X                    | -                  | G1.6 PROJECT VIEW                         |                      |                    |  |
| X                    | -                  | G1.7 PROJECT VIEW                         | X                    | -                  | 16-G COMPLIANCE - UNIT EXPOSURE - Sec 140                      |
| X                    | -                  | G1.8 PROJECT VIEW                         |                      |                    |  |
| X                    | -                  | G1.9 PROJECT VIEW                         | X                    | -                  | 16-H COMPLIANCE - 16TH ST. ACTIVE FRONTAGE - Sec 145.1         |
| X                    | -                  | G2.1 PROJECT MATERIALS                    |                      |                    |  |
| X                    | -                  | G2.2 PROJECT MATERIALS                    | X                    | -                  | 16-J COMPLIANCE - RESIDENTIAL STOOPS - Sec 145.1               |
| X                    | X                  | R1 SURVEY                                 |                      |                    |  |
|                      |                    |   | X                    | -                  | 16-K COMPLIANCE - BUILDING HEIGHT DIAGRAM - Sec 260            |
| X                    | X                  | L-0.0 OVERALL SITE PLAN                   |                      |                    |  |
| X                    | X                  | L-0.1 PUBLIC / SHARED SPACES              | X                    | -                  | 16-L COMPLIANCE - MID BLOCK PASSAGE - Sec 270.2                |
| X                    | X                  | L-0.2 MISSISSIPPI MEWS                    |                      |                    |  |
| X                    | X                  | L-0.3 PUBLIC / SHARED IMAGES              |                      |                    |  |
| X                    | X                  | L-16.1 16th ST COURTYARDS                 | X                    | -                  | L1 CONTEXT PLAN  |
| X                    | X                  | L-16.2 16th ST AMENITY ROOF DECKS         | X                    | -                  | L2 PROPOSED STREETSCAPE PLAN                                   |
| X                    | X                  | L-16.3 16th ST EXTENSIVE ROOF DECKS       | X                    | -                  | L3 PROPOSED SITE FURNISHINGS                                   |
| X                    | X                  | L-16.4 16th ST SECTIONS                   | X                    | -                  | L4 EXISTING CONDITIONS   |
| X                    | X                  | L-16.5 16th ST IMAGES                     |                      |                    |  |
| -                    | X                  | A100 (REMOVED FROM SET - SEE G .1 SERIES) | X                    | -                  | STUDY 0 ELEMENT REUSE CONCEPT - COVER SHT.                     |
| -                    | X                  | A101 (REMOVED FROM SET - SEE G .1 SERIES) | X                    | -                  | STUDY 1 ELEMENT REUSE CONCEPT - SITE PLAN                      |
| X                    | X                  | A110 SITE / ROOF PLAN                     |                      |                    |  |
| X                    | X                  | A2B1 BASEMENT LEVEL PLAN                  | X                    | -                  | STUDY 2 ELEMENT REUSE CONCEPT - PEDESTRIAN PROMENADE           |
| X                    | X                  | A201 GROUND FLOOR PLAN                    |                      |                    |  |
| X                    | X                  | A202 PLAN LEVEL 2 (PODIUM)                | X                    | -                  | STUDY 3 ELEMENT REUSE CONCEPT - PEDESTRIAN PROMENADE AT 17TH   |
| X                    | X                  | A203 PLAN LEVELS 3 / 4                    |                      |                    |  |
| -                    | -                  | A204 NOT INCLUDED (SIM A203)              | X                    | -                  | STUDY 4 ELEMENT REUSE CONCEPT - PEDESTRIAN PROMENADE AT 16TH   |
| X                    | X                  | A205 PLAN LEVEL 5                         |                      |                    |  |
| X                    | X                  | A206 PLAN LEVEL 6                         | X                    | -                  | STUDY 5 ELEMENT REUSE CONCEPT - RETAIL CORNER                  |
| X                    | X                  | A211 ROOF PLAN                            |                      |                    |  |
| X                    | X                  | A311 EXTERIOR ELEVATIONS                  | X                    | -                  | STUDY 6 ELEMENT REUSE CONCEPT - RETAIL CORNER                  |
| X                    | X                  | A312 EXTERIOR ELEVATIONS                  |                      |                    |  |
| X                    | X                  | A321 BUILDING SECTIONS                    | X                    | -                  | STUDY 7 ELEMENT REUSE CONCEPT - RETAIL CORNER                  |
| X                    | X                  | A322 BUILDING SECTIONS                    |                      |                    |  |
| X                    | X                  | A401 TYPICAL UNIT PLANS                   |                      |                    |  |
|                      |                    |   |                      |                    | STUDY 8 ELEMENT REUSE CONCEPT - MISSISSIPPI MEWS ENTRY         |



AREA TABULATIONS

|       | Residential  | Residential |         | Residential | Retail | Garage       | Grand Total |
|-------|--------------|-------------|---------|-------------|--------|--------------|-------------|
| Level | Rentable GSF | Amenity*    | Core SF | Total GSF   | GSF    | Total GSF ** | GSF         |
| 6     | 38,000       | 0           | 7,900   | 45,900      | 0      | 0            | 45,900      |
| 5     | 43,600       | 0           | 7,900   | 51,500      | 0      | 0            | 51,500      |
| 4     | 45,400       | 0           | 7,900   | 53,300      | 0      | 0            | 53,300      |
| 3     | 44,200       | 600         | 7,900   | 52,700      | 0      | 0            | 52,700      |
| 2     | 43,900       | 2,750       | 7,650   | 54,300      | 0      | 0            | 54,300      |
| 1     | 10,900       | 4,540       | 6,750   | 20,450      | 20,318 | 23,975       | 64,743      |
| B1    |              |             |         |             |        | 80,500       | 80,500      |
| Total | 226,000      | 7,890       | 46,000  | 278,150     | 20,318 | 104,475      | 402,943     |

OFF-STREET PARKING

| PERMITTED (no parking required)   |             |            |               |                 |        | PROVIDED     |        |
|---|-------------|------------|---------------|-----------------|--------|--------------|--------|
| Use   | Counts      | Req'd      |               | Total Permitted |        | Level        | Spaces |
| Retail  | 20,318 SF   | 1 / 500    | Spaces /SF    | 41              | Spaces | Ground Level | 39     |
|   |             |            |               |                 |        | Basement     | 0      |
| Total Retail  |             |            |               | 41              |        |              | 39     |
| Residential   | 156 S/1bd   | 0.75 / 1   | Spaces / Unit | 117             | Spaces | Ground Level | 0      |
|   | 104 2's/3's | 1.00 / 1 * | Spaces / Unit | 104             | Spaces | Basement     | 221    |
| Total Residential   |             |            |               | 221             |        |              | 221 ** |
| * Note: Project's 2 and 3 bedrooms are at least 1,000 sf; parking above 0.75:1.0 is subject to the conditions of 151.1.(g), otherwise max allowed is 195 residential spaces (0.75 x 260 dwelling units) |             |            |               |                 |        |              |        |
| ** See Modification Request 6, sheet G0.3   |             |            |               |                 |        |              |        |

OPEN SPACE TABULATIONS

| REQUIRED  |           |         |                |                | PROVIDED ON-SITE** |                       |        |        |
|---|-----------|---------|----------------|----------------|--------------------|-----------------------|--------|--------|
| Use   | Counts    | Req'd   |                | Total Required | Level              | Publically Accessible | Common |        |
| Retail*   | 14,000 SF | 1 / 250 | SF/SF Usable   | 56 SF          | Podium Level       |                       | 7,500  | 7,500  |
| Residential   | 126 Units | x 80    | SF/Unit        | 10,080 SF      | Ground Floor       | 7,514                 | 11,166 | 18,680 |
|   | 134 Units | x 54    | SF/Unit Public | 7,236 SF       |                    |                       |        |        |
| Total   |           |         |                | 17,372         |                    | 7,514                 | 18,666 | 26,180 |
| * Retail SF shown as usable floor area and excludes back of house areas   |           |         |                |                |                    |                       |        |        |
| **Additional private and common open space provided at upper levels, but not shown as not required to meet code |           |         |                |                |                    |                       |        |        |

BIKE PARKING

| REQUIRED  |               |                 |               |                 |               | PROVIDED             |                |                |
|---|---------------|-----------------|---------------|-----------------|---------------|----------------------|----------------|----------------|
| Use   | Counts        | Class 1         | Class 1 Req'd | Type 2          | Class 2 Req'd | Use                  | Class 1 Spaces | Class 2 Spaces |
| Retail Sales  | 10,000        | 1:7500sf usable | 1             | 1:2500sf usable | 4             |                      |                |                |
| Services/Restaurant                                 | 4,000         | 1:7500sf usable | 1             | 1:750sf usable  | 8             | Retail Provided      | 4              | 25             |
| Total Retail  |               |                 | 2             |                 | 12            |                      |                |                |
| Residential   | Units 1-100   | 1:01            | 100           | 1:20 units      | 5             | Residential Provided | 260            | 15             |
|   | Units 101-260 | 1:04            | 40            | 1:20:units      | 8             | Total Provided       | 264            | 40             |
| Total Residential                                   |               |                 | 140           |                 | 13            |                      |                |                |
| Total Required                                      |               |                 | 142           |                 | 25            |                      |                |                |
| 2/3 of required to be provided in horizontal racks: |               |                 | 95            |                 |               |                      |                |                |

UNIT COUNT

|                        |         |         |            |          | Residential |
|------------------------|---------|---------|------------|----------|-------------|
|                        | Studio  | 1 bd    | 2bd        | 3bd      | # Units     |
|                        | 9       | 13      | 17         | 3        | 42          |
|                        | 11      | 21      | 17         | 2        | 51          |
|                        | 11      | 22      | 19         | 1        | 53          |
|                        | 10      | 21      | 19         | 1        | 51          |
|                        | 10      | 21      | 19         | 1        | 51          |
|                        | 2       | 5       | 4          | 1        | 12          |
| Total                  | 53      | 103     | 95         | 9        | 260         |
|                        | 20.4%   | 39.6%   | 36.5%      | 3.5%     | 100.0%      |
| 2-bdrm or larger = 40% |         |         |            |          |             |
| Typ. Size              | 488 qsf | 732 qsf | 1067.5 qsf | 1300 qsf |             |

OFF-STREET CAR SHARE

| REQUIRED    |     |     |                 | PROVIDED |        |
|-------------|-----|-----|-----------------|----------|--------|
| Use         | Min | Max | Total Permitted |          | Spaces |
| Retail      | 1   | 5   | 6               |          | 1      |
| Residential | 2   | 5   | 7               |          | 2      |
| Total       | 3   | 10  | 13              |          | 3      |

PLANNING CODE SUMMARY

|                  |   |
|------------------|---|
| PROJECT ADDRESS: | 901 16th Street<br>San Francisco, CA 94107  |
| BLOCK / LOT:     | 3949 / 001a, 001, 002 and 3950 / 001  |
| LOT AREA         | Proposed lot - 350' x 237' = 82,950 SF  |
| ZONING DISTRICT  | UMU: Urban Mixed Use  |
| OVERLAY DISTRICT | Eastern Neighborhoods<br>Subject to Section 329   |
| PERMITTED USES   | Mutli-Family Residential Permitted - Min. 40% 2-bedrooms or larger and at least 1,000 SF<br>Retail Permitted - Up to 25,000 GSF |
| HEIGHT / BULK    | 68-X - See sheet G2 for compliance  |

BULK LIMITATIONS

Section 270 applies - A mid-block passage is required. Passage shall be 20-ft wide with a 10-ft pedestrian walking surface. Must be ungated and publicly accessible 24hrs. A passage connecting 16th and 17th Streets is provided and meets the requirements of this section.  
Section 261.1 #3 - Mid block passages shall have a setback of 10-ft above 25-ft datum. Our passage is required to be 20-ft, so clear width above 25 feet must be 30-ft. Clear width provided.  
Section 270.1 applies - see Modification Request 3

PARKING

Section 151.1 and 151.1(g) applies - No parking required. For allowable parking see tabulations this sheet

CAR SHARE

Table 166 applies - see tabulations this sheet. Car share parking shall not count against maximum allowable parking provided.

OPEN SPACE

Table 843.11 applies. - see tabulations this sheet.

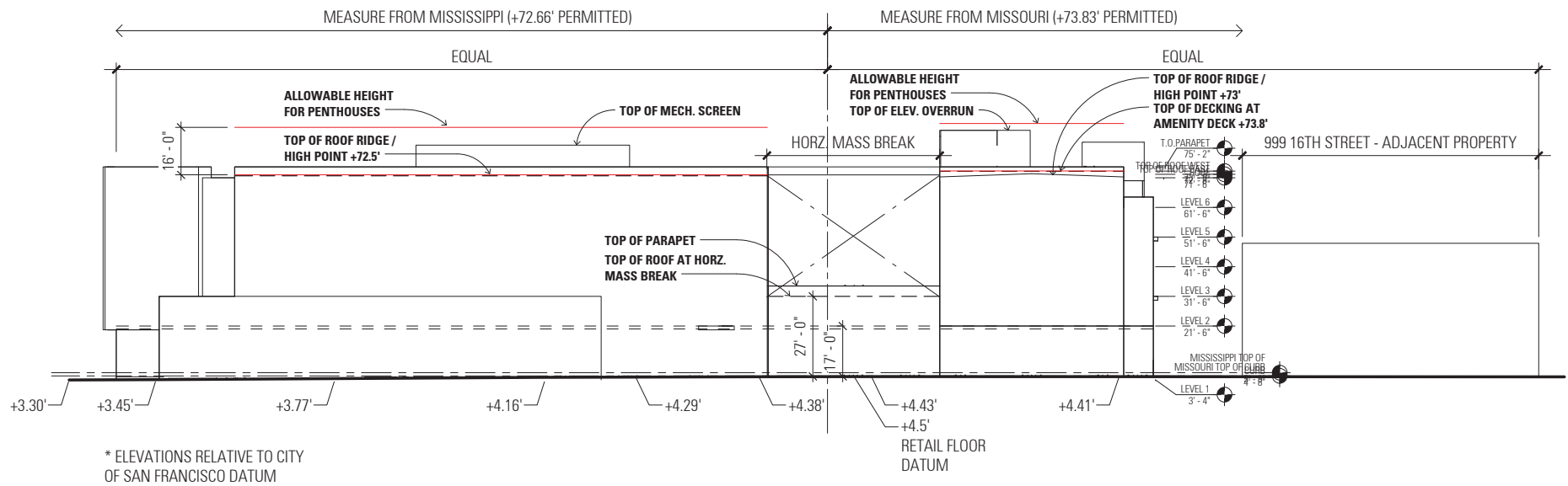
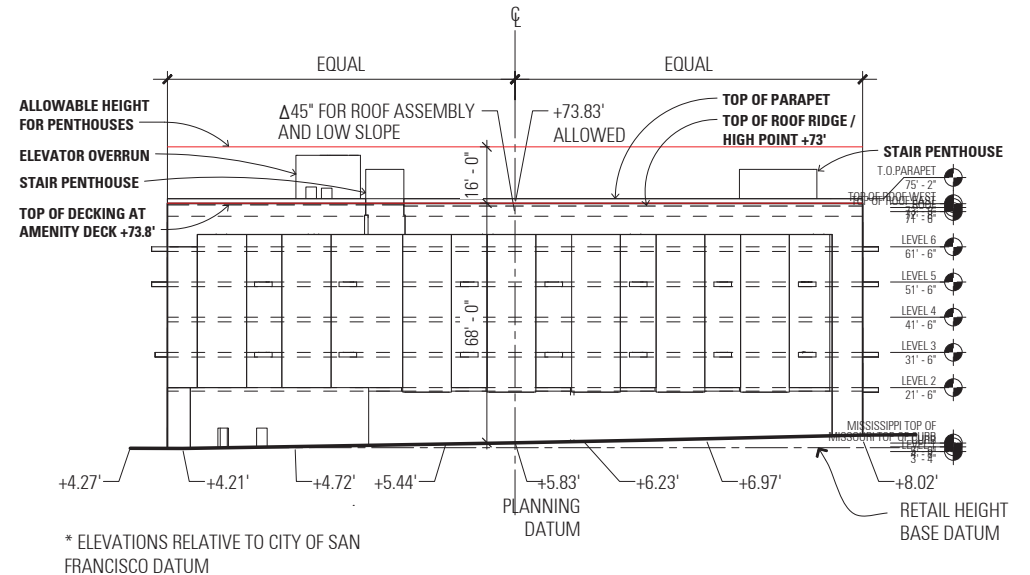
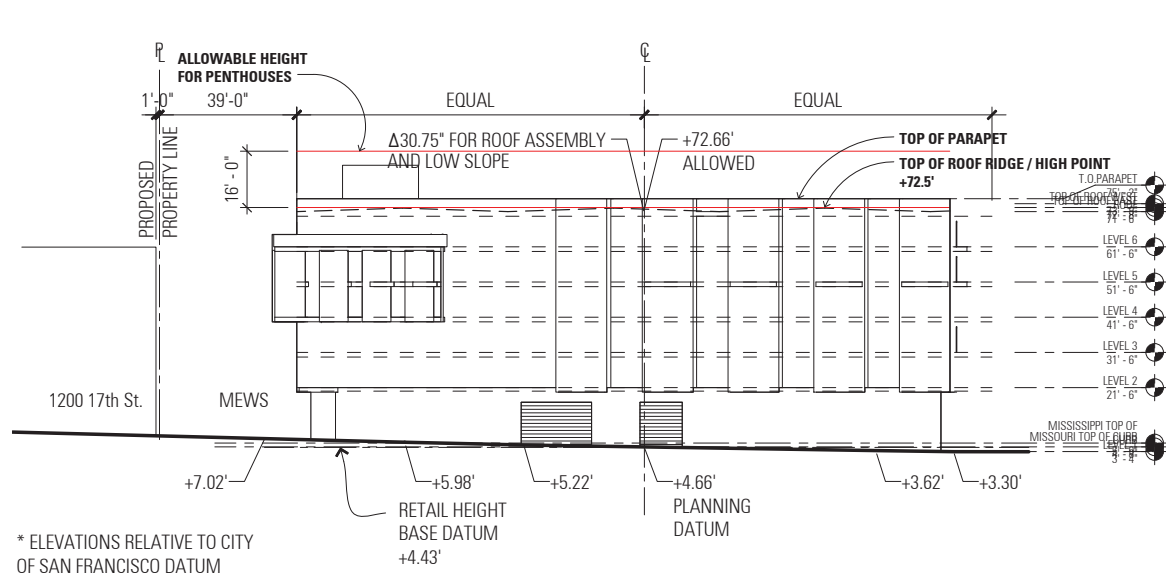
BUILDING CODE SUMMARY

|                         |  |
|-------------------------|--|
| OCCUPANCY               | A-3 - RESIDENTIAL AMENITY SPACES<br>B, M - RETAIL<br>S-2 - PARKING GARAGE<br>R-2 - RESIDENTIAL |
| CONSTRUCTION TYPES      | TYPES 1A AND IIIA  |
| MAX. ALLOWABLE BLDG HT. | 85'  |
| MAX. NUMBER OF STORIES  | 6 ABOVE GRADE PLANE<br>(5 TYPE IIIA OVER 1 TYPE 1A)  |

PROJECT DATA

901 16th Street





### BUILDING HEIGHT COMPLIANCE - SECTION 102.12

#### Required:

68-X

Measured from the top of curb elevation at the midpoint of the building frontage, to the top of the low-slope roof. Where the lot has frontage on two or more streets, either street may be used for measurement. The height is to be measured to a line equidistant between such street and the street on the opposite side of the block.

- Parapets are permitted to extend 4-ft above height limit
- Elevator, stair penthouses, mechanical screens and penthouses, fire towers, and dormer windows may extend 16-ft above the allowable height provided that the height increase is limited to 20% of the roof area.

#### Provided:

The project is measured at the midpoints of the Mississippi Street and Missouri Street frontages.

- Mississippi is measured from a datum of 4.66'
- Missouri is measured from a datum of 5.83'

901 16th Street

PLANNING CODE DIAGRAMS

**BAR**architects

901 Battery Street, Suite 300 | San Francisco, CA 94111 | 415 293 5700 | [www.bararch.com](http://www.bararch.com)

POTRERO PARTNERS LLC

PGA design

13051

5/12/2016

SCALE: 1" = 20'-0"

G0.2



MODIFICATION REQUEST 01 - REAR YARD

SEE SHEET 16-A

Required:

Per Section 134 (a)(1) UMU Districts are required to provide a rear yard depth of 25% total lot depth, but no less than 15-ft.

Comparable Rear Yard = .25 x 82,950 sf lot area = 20,737.5 sf

Provided:

39' provided to property line. Parcel 3949/002 provides an additional1'-0" to face of building, allowing for a total clear dimension of 40'.

|   |                         |
|---|-------------------------|
| Comparable Rear Yard Areas Provided:              |                         |
| Inner Courtyard 1                                 | 3,150 sf                |
| Inner Courtyard 2                                 | 4350 sf                 |
| Pedestrian Promenade                              | 6,095 sf                |
| Private Residential Mews                          | 11,166 sf               |
| Public Use Areas                                  | 1,419 sf                |
| Total   | 26,180 sf (Open to sky) |
| ** Exceeds 25% rear yard equivalent by 5,442.5 sf |                         |

Justification:

Section 134(f) allows for a modification of the requirement through approval of the Planning Commission pursuant to Section 329. Project proposes providing a comparable amount of area elsewhere in the project. Project is providing an excess of the required Open Space, including 7,514 SF of publically accessible open space.

MODIFICATION REQUEST 02 - OBSTRUCTIONS OVER STREETS

SEE SHEET 16-B

Required:

Per Section 136 (c)(2) Bay (projecting) windows are governed by length and depth proportions established by this subsection.

Provided:

Bay windows throughout the project meet the requirements of Section 136 unless otherwise denoted.

The bay window projection at the Southeast corner (Mississippi and Mews) of the project at Levels 4 and 5 requires a modification (see A205 for dimensions) due to the projection depth and length.

Justification:

The intent of the projection is to architecturally bridge the transition between the differing design languages of the Mississippi and Mews elevations. In order to provide superior architectural design it is important that this element does not have the proportions of a standard bay outlined in Section 136. Rather it is an element with its own proportions different from that of a bay. Within the overall expression each bay component is expressed with a width which is Planning Code compliant. Due to the projection's height above grade, and it's location at the buildings corner, the projection does not negatively impact the streetscape experience.

Compliant: 144 SF/FLR x 5 allowable floors = 720 SF total  
Proposed: 144 SF/FLR x 2 designed floors = 288 SF total

MODIFICATION REQUEST 03 - ACTIVE FRONTAGE AT MISSISSIPPI STREET

Required:

Per Section 145.1  
Parking and loading entrances are limited to 20-ft per frontage

Provided:

Project proposes two openings on Mississippi. One combined, retail and residential, parking entry with a 20-ft wide opening. One, 12-ft wide opening for retail loading.

Justification:

No curb cuts are permitted along 16th Street, thus the project is limited to providing curb cuts along Mississippi, since no other street frontages exist. The project proposes limiting number of curb cuts by combing retail and residential parking to one entry. The retail loading and parking entries are being provided at minimum clear dimensions required for safety.

MODIFICATION REQUEST 04 - OFF STREET PARKING

Required:

Per Section 151.1  
For residential uses the following off-street parking ratio's are permitted  
0.75 :1 du permitted  
1.00 : 1 du for 2 and 3 bedroom units over 1,000 sf. Stalls in excess of 0.75:1 are subject to the provisions of Sec. 151.1(g).

At 0.75:1 project is permitted 195 spaces  
Including increased ratio for two and three bedrooms project is permitted 221 spaces  
(0.75 x 156 studio and one beds) + (1 x 104 two/three beds over 1,000 sf) = 117 + 104 = 221  
Therefore, 26 spaces subject to 151.1(g)

Provided:

221 non-encumbered residential spaces parked below grade

Justification:

The project requests a modification to the off street parking requirements in section 151.1. The project proposes to provide .75 spaces for units with 1 bedroom or smaller and 1 standard space for untis with 2 bedrooms and larger and at least 1,000 sf in the underground garage. Several factors contribute to the desire 1 space for these units: Street parking will be eliminated on 16th Street to accommodate the BRT and parking on 17th street will be eliminated to accommodate a bicycle lane. In addition, the 2 and 3 bedroom units are designed with families in mind, and as such may require 1 car per unit. Parking stackers would require additional excavation depth and are not desired in this site with hazardous soil conditions.

1. Street parking will be eliminated on 16th Street to accommodate the BRT.
2. Street parking on 17th street will be eliminated to accommodate abicycle lane.
3. The 2 and 3 bedroom units are designed with families in mind, andas such require 1 car per unit.
4. The Bottom of the Hill music venue at 17th street and Missouri depends on street parking for their patrons and has expressed concern that the demand for street parking for the new residential development will impact their business.
5. The configuration of the below-ground parking garage is within the building footprint and does not require additional excavation into adjacent landscaped areas to achieve the modified parking request. Parking stackers would require additional excavation depth and are not desired in this site with hazardous soil conditions.

MODIFICATION REQUEST 05 - OFF STREET LOADING

Required:

Per Table 152.1  
Retail: 1 loading spaces required for retail uses 10,001 - 30,000 GSF  
Residential: 2 loading space required for residential uses between 200,001-500,000 GSF

Provided:

One retail off-street loading zone provided along Mississippi - complies.  
A 80-ft long painted loading zone on Mississippi near 16th is proposed

Justification:

No curb cuts are permitted along 16th Street, thus the project is limited to providing curb cuts along Mississippi, since no other street frontages exist. The project proposes two curb cuts on Mississippi for garage entry and retail loading. It is therefore proposed to provide residential loading on the street.

MODIFICATION REQUEST 06 - HORIZONTAL MASS REDUCTION

Required:

Per Section 270.1  
Horizontal Mass Reduction break > 30' wide, >60' deep, from top of third story, and open to the sky, for length > 200 feet.

Provided:

The 16th Street frontage is provided with a 57.75-ft wide by 30.3-ft deep void open to the sky. The reduction begins a 27-ft, providing a bulk reduction of 70,069 cubic feet. The reduction results in discrete frontages less than 200-ft.

Justification:

Per Section 270.1(d) the Planning Commission may modify the requirement through the process set forth in Section 329. The following criteria for this modification has been met:  
- Section 270.1(d) Item 1: no more than 50% of the mass shall be reduced - while the proportions of the void have been modified, the area open to the sky remain the same as required.  
- Section 270.1(d) Item 2: the depth of any mass reduction breaks shall no be less than 15 feet in depth; 30.3-ft provided.  
- Section 270.1(d) Item 3: the proposed building envelope can be demonstrated to achieve the distinctly superior effect of reducing the apparent horizontal dimension of the building; the placement and width of the proposed break provide a clear reduction in horizontal length.  
-Section 270.1(d) Item 4: the proposed building achieves unique and superior architectural design; the proposed mass break occurs in a logical place in plan as it relates to the Texas Street axis and allows for a narrower street fronting mass adjacent to the Pedestrian Promenade.

901 16th Street





1 - MISSISSIPPI ELEVATION (SUBJECT FRONTAGE)



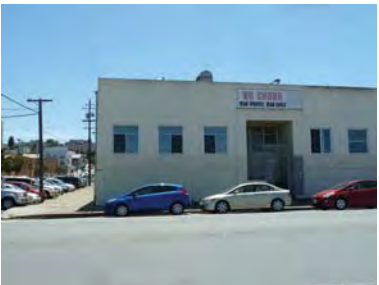
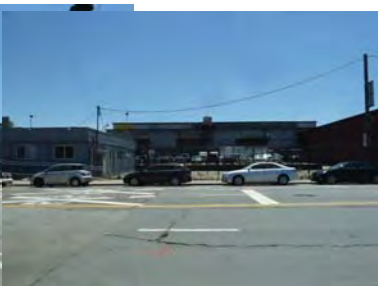
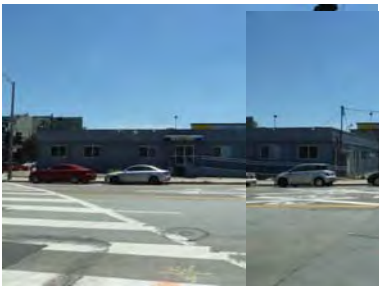
2 - MISSISSIPPI ELEVATION (ACROSS STREET)



3 - 16TH STREET ELEVATION (ACROSS STREET)



4 - 16TH STREET ELEVATION (SUBJECT FRONTAGE)



5 - 1001 16TH STREET



6 - 99 MISSOURI STREET



7 - EQR POTRERO PROJECT (DAGGETT)

# 901 16th Street

## CONTEXT VIEWS





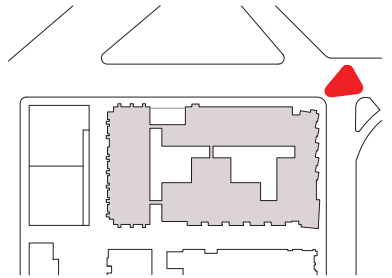
VIEW FROM MISSISSIPPI AND 16TH STREETS

\* LANDSCAPE SHOWN FOR ILLUSTRATIVE PURPOSES ONLY - S.L.D. FOR LANDSCAPE DESIGN INTENT

# 901 16th Street

# PROJECT VIEW





VIEW OF RETAIL CORNER - AT MISSISSIPPI AND 16TH STREETS

\* LANDSCAPE SHOWN FOR ILLUSTRATIVE PURPOSES ONLY - S.L.D. FOR LANDSCAPE DESIGN INTENT

901 16th Street

PROJECT VIEW





VIEW FROM DAGGETT PARK OF 16TH AND PEDESTRIAN PROMENADE

901 16th Street

PROJECT VIEW

**BAR**architects

901 Battery Street, Suite 300 | San Francisco, CA 94111 | 415 293 5700 | www.bararch.com

POTRERO PARTNERS LLC



13051

05/12/16

SCALE: 1" = 160'-0"

**G1.3**





VIEW THROUGH PEDESTRIAN PROMENADE - LOOKING SOUTH FROM 16TH STREET

901 16th Street

PROJECT VIEW

**BAR**architects

901 Battery Street, Suite 300 | San Francisco, CA 94111 | 415 293 5700 | www.bararch.com

POTRERO PARTNERS LLC



13051

05/12/16

SCALE: 1" = 160'-0"

G1.4





VIEW OF 16TH STREET LOOKING SOUTH AT RESIDENTIAL ENTRY

901 16th Street

PROJECT VIEW



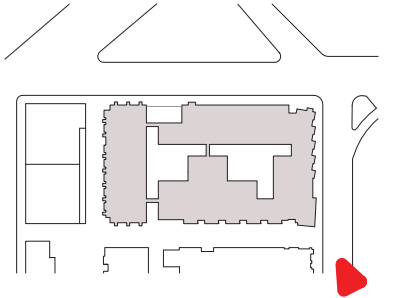


VIEW FROM 16TH AND MISSISSIPPI OF MISSISSIPPI STREET

901 16th Street

PROJECT VIEW





VIEW FROM MISSISSIPPI STREET AND MEWS

901 16th Street

PROJECT VIEW





VIEW THROUGH MEWS

901 16th Street

PROJECT VIEW

**BAR**architects

901 Battery Street, Suite 300 | San Francisco, CA 94111 | 415 293 5700 | www.bararch.com

POTRERO PARTNERS LLC



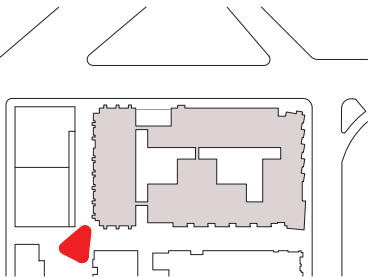
13051

05/12/16

SCALE: 1" = 160'-0"

G1.8





VIEW FROM PEDESTRIAN PROMENADE AND MEWS LOOKING NORTH TOWARDS 16TH STREET

901 16th Street

PROJECT VIEW

**BAR**architects

901 Battery Street, Suite 300 | San Francisco, CA 94111 | 415 293 5700 | www.bararch.com

POTRERO PARTNERS LLC



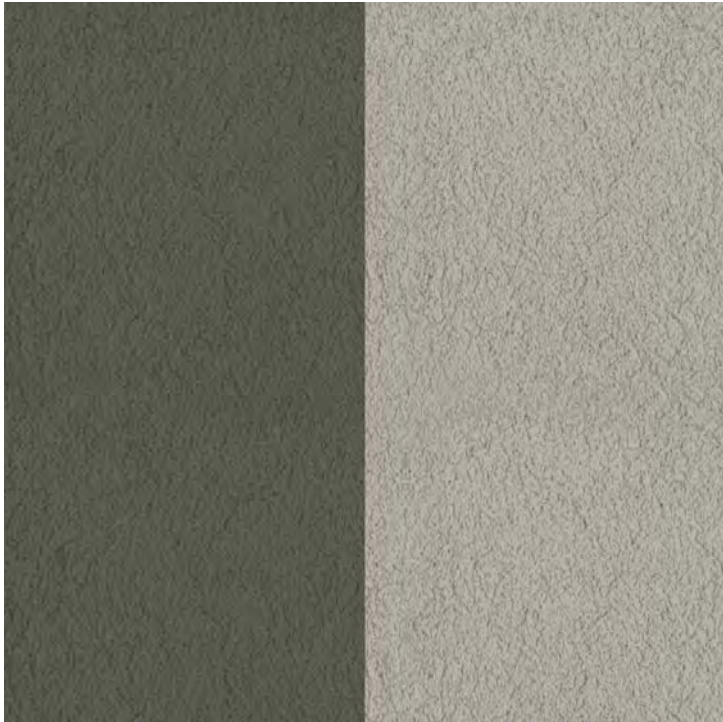
13051

05/12/16

SCALE: 1" = 160'-0"

**G1.9**





1. CEMENT PLASTER



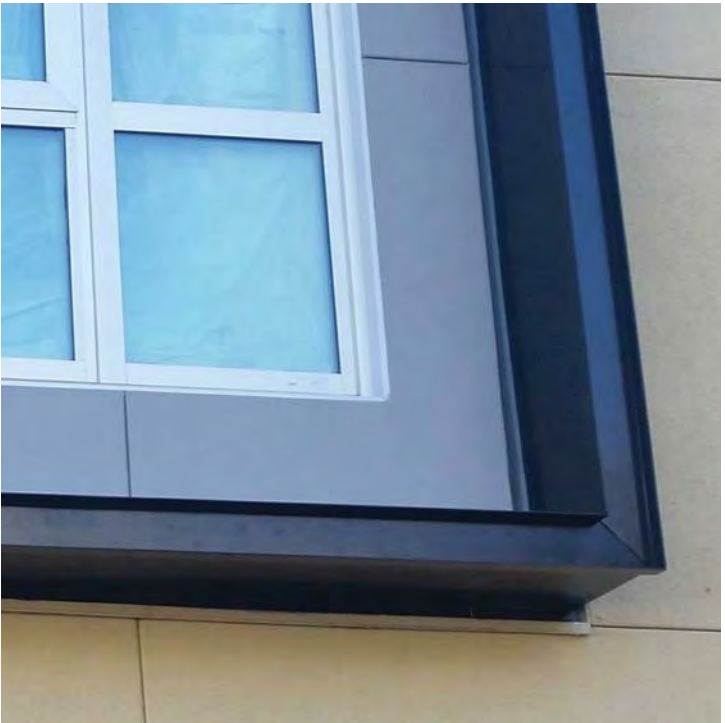
2a. METAL PANEL - CORRUGATED



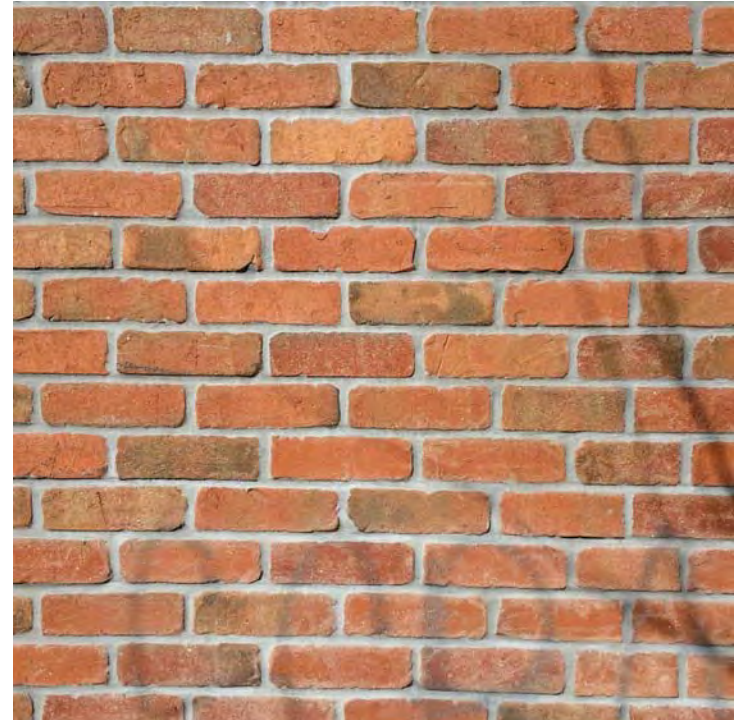
2b. METAL PANEL



2c. CORTEN STEEL - UNIT STOOPS



3. EXTRUDED ALUMINUM FRAME

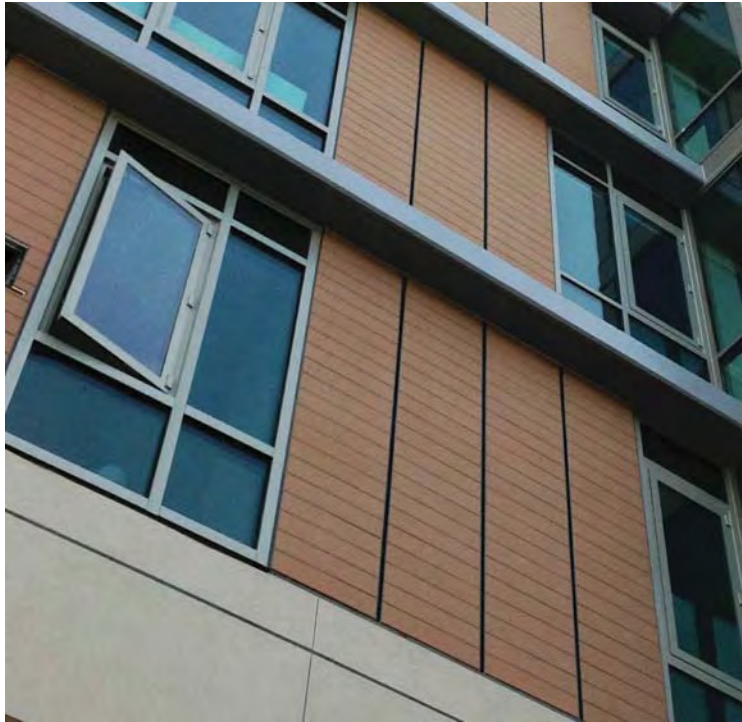


4. BRICK

# 901 16th Street

## PROJECT MATERIALS





5. CERAMIC TILE  
6. ALUMINUM WINDOW



7. STOREFRONT WINDOW SYSTEM



8. GLASS GUARDRAIL



9. CABLE-RAIL GUARDRAIL



10. ALUMINUM SCREEN

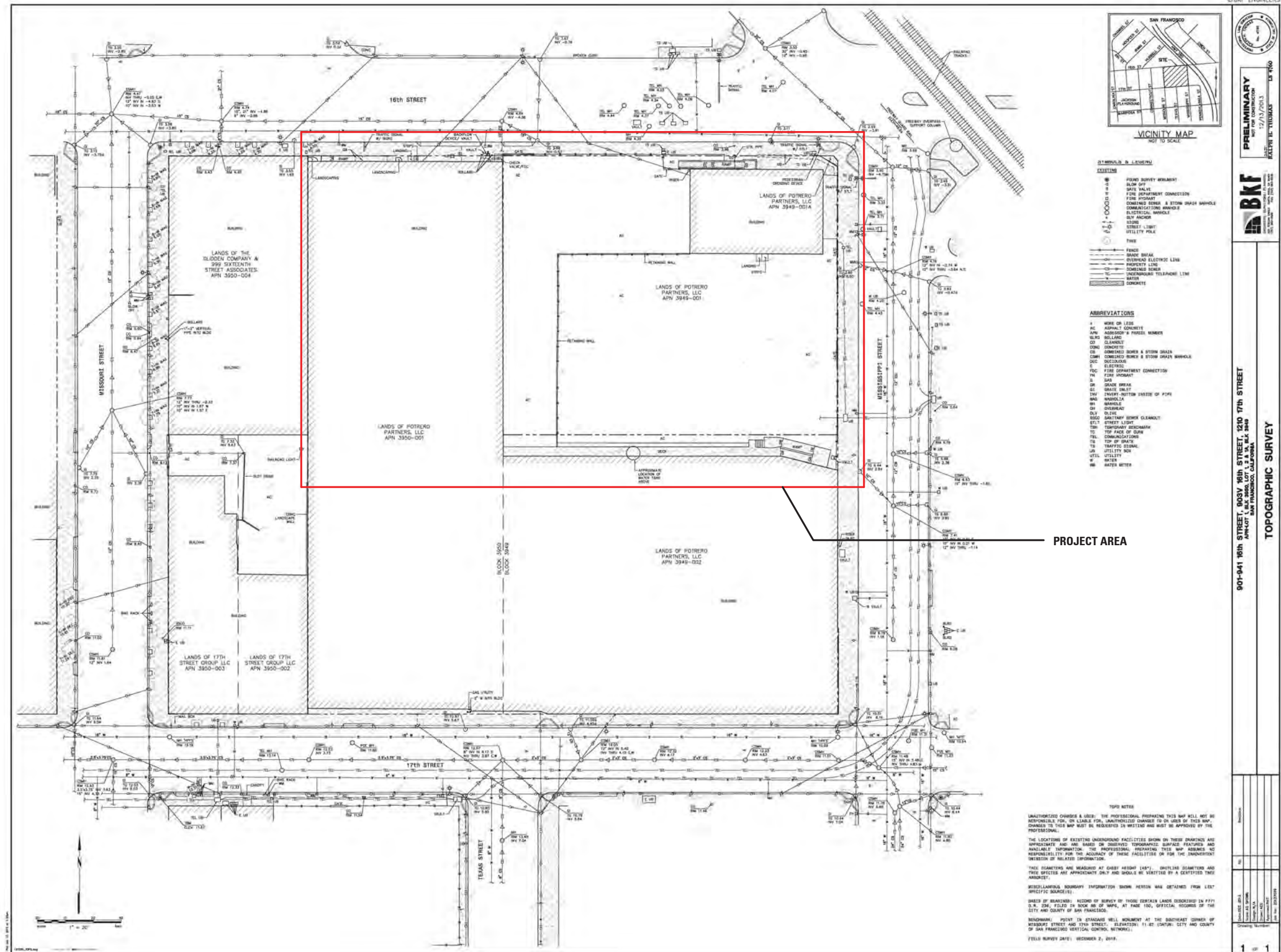


11. ARCHITECTURAL CONCRETE

901 16th Street

PROJECT MATERIALS

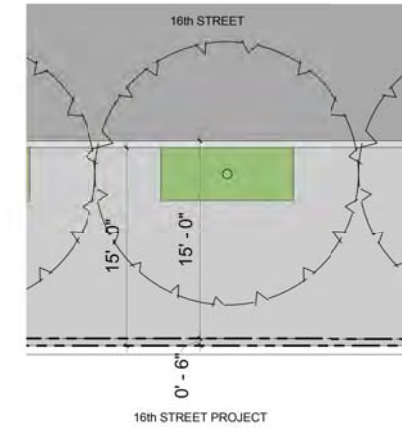




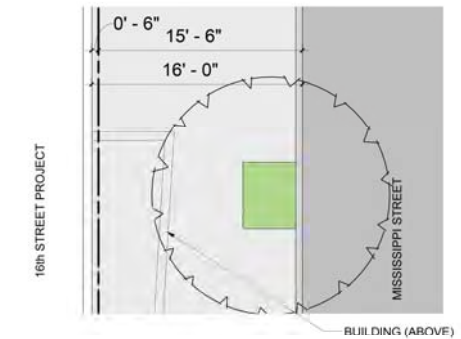
901 16th Street

# SURVEY

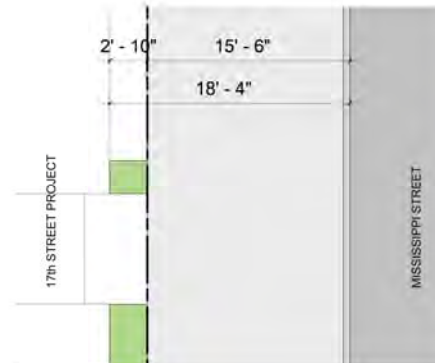




1 16th STREET SIDEWALK  
3/16" = 1'-0"



2 MISSISSIPPI SIDEWALK AT 16th ST PROJECT  
3/16" = 1'-0"



3 MISSISSIPPI STREET AT 17th ST PROJECT  
3/16" = 1'-0"



4 17th STREET SIDEWALK  
3/16" = 1'-0"

#### INDEX OF LANDSCAPE SHEETS

|        |                                |
|--------|--------------------------------|
| L-0.0  | OVERALL SITE PLAN              |
| L-0.1  | Public / Shared Spaces         |
| L-0.2  | Mississippi Mews               |
| L-0.3  | Public / Shared Images         |
| L-16.1 | 16th St Courtyards             |
| L-16.2 | 16th St Amenity Roof Decks     |
| L-16.3 | 16th St Extensive Roof Decks   |
| L-16.4 | 16th St Sections               |
| L-16.5 | 16th St Images                 |
| L-17.1 | 17th St Courtyards & Roof Deck |
| L-17.2 | 17th St Sections and Images    |



## OVERALL SITE PLAN L-0.0

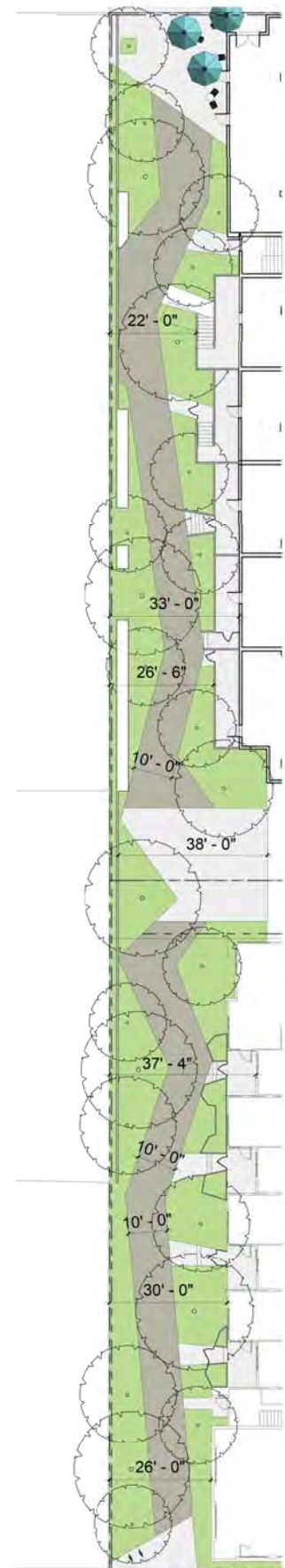
PGA design



05/12/16

1200 17th St. / 901 16th St.  
San Francisco, California





1 Pedestrian Promenade  
1/16" = 1'-0"



## PUBLIC / SHARED SPACES NOTES

### 16TH STREET FRONTAGE:

- 1 PLANTING:  
-STREET TREES: ROBINIA 'PURPLE ROBE'  
-DROUGHT TOLERANT SUCCULENTS, GROUNDCOVER, AND PERENNIALS
- 2 HARDSCAPE:  
-SIDEWALK: CIP CONCRETE WITH LAMP BLACK AND GRID SCORING  
AT PEDESTRIAN PROMENADE: CIP CONCRETE WITH LAMP BLACK, CUSTOM SCORING, AND SURFACE FINISH
- 3 FURNISHINGS:  
-RECLAIMED STEEL CABLE BIKE RACKS, RESISTANT TO CUTTING  
-BENCHES PLANTERS CAST CONCRETE AND GFRG
- 4 LIGHTING:  
-STREET LIGHTING  
-ARCHITECTURAL LIGHTING, BUILDING MOUNTED

### MISSISSIPPI STREET FRONTAGE:

- 5 PLANTING:  
-STREET TREES:  
CORYMBIA CITRIODORA (AKA EUCALYPTUS CITRIODORA)  
PODOCARPUS GRACILIOR  
PRUNUS 'KWANZAN'  
-UNDERSTORY:  
DROUGHT TOLERANT SUCCULENTS, GROUNDCOVER, AND PERENNIALS
- 6 HARDSCAPE:  
-SIDEWALK: SIDEWALK: CIP CONCRETE WITH LAMP BLACK AND GRID SCORING
- 7 FURNISHINGS:  
-BIKE RACKS MADE FROM RECLAIMED STEEL CABLE, RESISTANT TO CUTTING  
-BENCHES
- 8 LIGHTING:  
-STREET LIGHTING  
-ARCHITECTURAL LIGHTING, BUILDING MOUNTED

### 17TH STREET FRONTAGE:

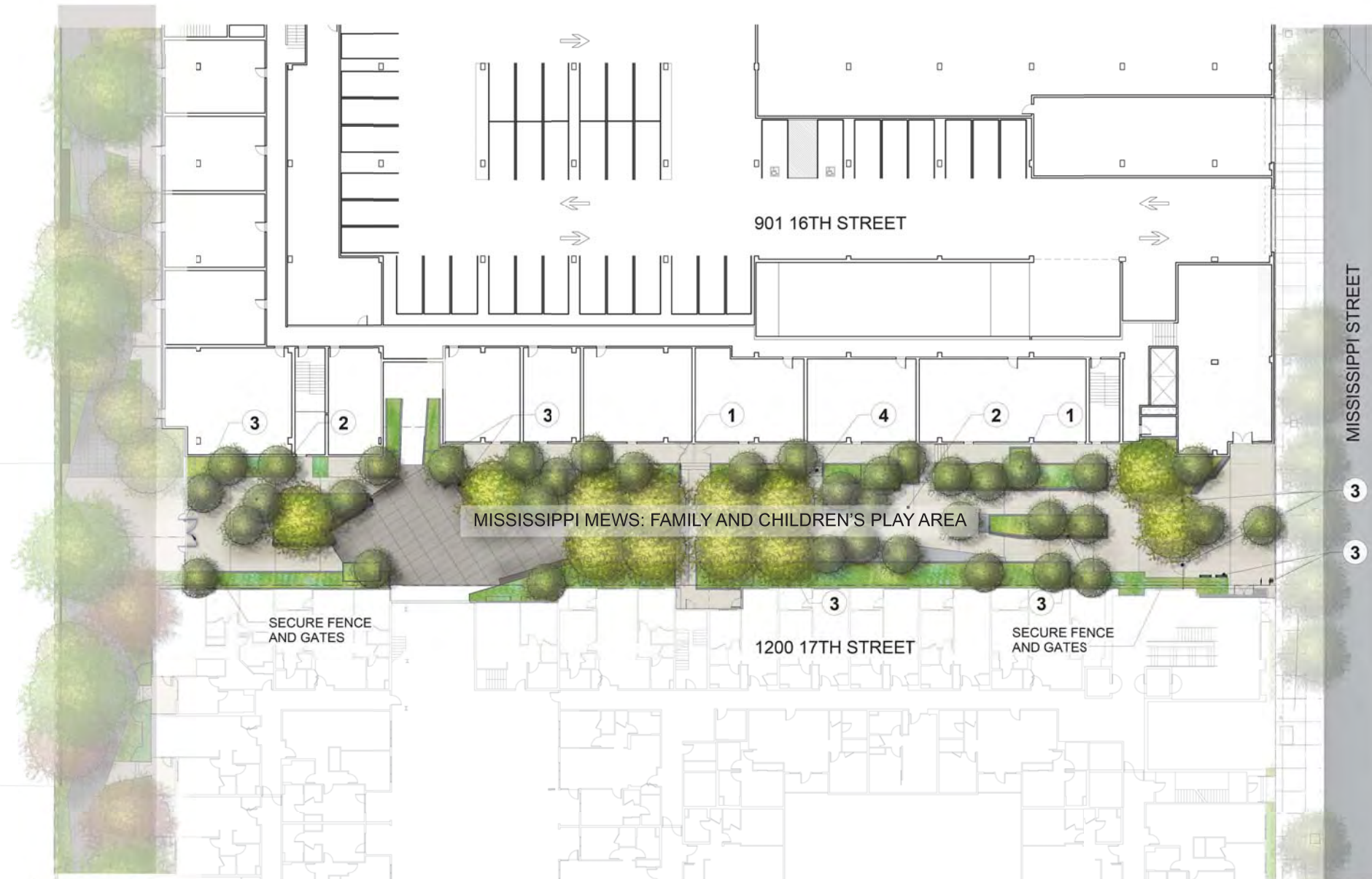
- 9 PLANTING:  
-STREET TREES:  
ROBINIA 'PURPEL ROBE'  
CORYMBIA CITRIODORA  
PRUNUS SERRULATA  
-UNDERSTORY:  
DROUGHT TOLERANT SHRUBS, SUCCULENTS, GROUNDCOVER, AND PERENNIALS
- 10 HARDSCAPE:  
-SIDEWALK: CIP CONCRETE WITH LAMP BLACK AND GRID SCORING  
AT PEDESTRIAN PROMENADE: CIP CONCRETE WITH LAMP BLACK, CUSTOM SCORING, AND SURFACE FINISH
- 11 FURNISHINGS:  
-BIKE RACKS MADE FROM RECLAIMED STEEL CABLE, RESISTANT TO CUTTING  
-BENCHES  
-PLANTERS: RAISED PLANTERS AND POTS
- 12 LIGHTING:  
-STREET LIGHTING  
-STEP LIGHTING AT STOOPS  
-ARCHITECTURAL LIGHTING, BUILDING MOUNTED

### PEDESTRIAN PROMENADE:

- 13 PLANTING:  
-TREES:  
GINKGO BILOBA  
GINKGO BILOBA 'PRINCETON SENTRY'  
ULMUS PARVIFOLIA  
DIOSPYROS KAKI  
-UNDERSTORY:  
DROUGHT TOLERANT SHRUBS, SUCCULENTS, GROUNDCOVER, AND PERENNIALS
- 14 HARDSCAPE:  
-AT SIDEWALKS: CIP CONCRETE WITH LAMP BLACK, CUSTOM SCORING, AND SURFACE FINISH  
-AT MEWS NODE: CIP CONCRETE WITH LAMP BLACK, CUSTOM SCORING, AND SURFACE FINISH  
-BETWEEN: ASPHALTIC PAVERS IN BLACK AND GREY
- 15 SCREENING:  
-SCREENING: TALL METAL FRAME WITH GREEN SCREEN AND VINES  
-SCREENING: SHORT SCREENING
- 16 FURNISHINGS:  
-RECLAIMED STEEL CABLE BIKE RACKS AT 17TH STREET NODE  
-CAFE SEATING AND TABLES AT 16TH ST CAFE  
-RAISED PLANTERS AT STOOPS WITH INDUSTRIAL CHARACTER
- 17 LIGHTING:  
-SCULPTURAL PATH LIGHTING (SAFETY LIGHTING)  
-STEP LIGHTING AT STOOPS

### 18 STORMWATER: CISTERN IN GARAGE



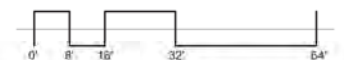


#### MEWS NOTES

##### MEWS MATERIALS:

- 1 PLANTING:  
TREES:  
GINKGO BILOBA  
GINKGO BILOBA 'PRINCETON SENTRY'  
MELALEUCA QUINQUENERVIA  
  
UNDERSTORY: DROUGHT TOLERANT SHRUBS, SUCCULENTS, GROUNDCOVER, AND PERENNIALS
- 2 HARDSCAPE:  
CONCRETE WITH SAWCUT SCORING  
SPECIAL CONCRETE WITH SAWCUT SCORING AND COLOR  
WALKABLE PEBBLE SURFACE
- 3 FURNISHINGS:  
BUILT-IN BENCHES  
BUILT-IN PLANTERS: BOARD-FORMED CONCRETE AND METAL STORAGE  
CONTAINER COMPONENTS  
PERCHING/SEATING RAILS  
RECLAIMED CABLE BIKE RACKS  
CUSTOM ORNAMENTAL SECURITY FENCING
- 4 LIGHTING:  
PATH LIGHTING  
STEP LIGHTING AT STOOPS  
ARCHITECTURAL LIGHTING, BUILDING MOUNTED

1 16th St level 1 Mews  
1/16" = 1'-0"







NOTES:  
1. COMMON NAME: LEMON SCENTED GUM  
2. EVERGREEN  
*Corymbia citriodora*



NOTES:  
1. COMMON NAME: FUYU JAPANESE PERSIMMON  
2. DECIDUOUS  
*Diospyros kaki 'Fuyu'*



NOTES:  
1. COMMON NAME: JACARANDA  
2. DECIDUOUS  
*Jacaranda mimosifolia*



NOTES:  
1. COMMON NAME: GINKGO  
2. DECIDUOUS  
*Ginkgo biloba*



NOTES:  
1. COMMON NAME: PRINCETON SENTRY GINKGO  
2. DECIDUOUS  
*Ginkgo biloba 'Princeton Sentry'*



NOTES:  
1. COMMON NAME: PAPERBARK MELALEUCA  
2. EVERGREEN  
*Melaleuca quinquenervia*



NOTES:  
1. COMMON NAME: PODOCARPUS  
2. EVERGREEN  
*Podocarpus gracilior*



NOTES:  
1. COMMON NAME: KWANZAN CHERRY  
2. DECIDUOUS  
*Prunus 'Kwanzan'*



NOTES:  
1. COMMON NAME: ARISTOCRAT FLOWERING PEAR  
2. DECIDUOUS  
*Pyrus calleryana 'Aristocrat'*



NOTES:  
1. COMMON NAME: PURPLE ROBE ROBINIA  
2. DECIDUOUS  
*Robinia 'Purple Robe'*

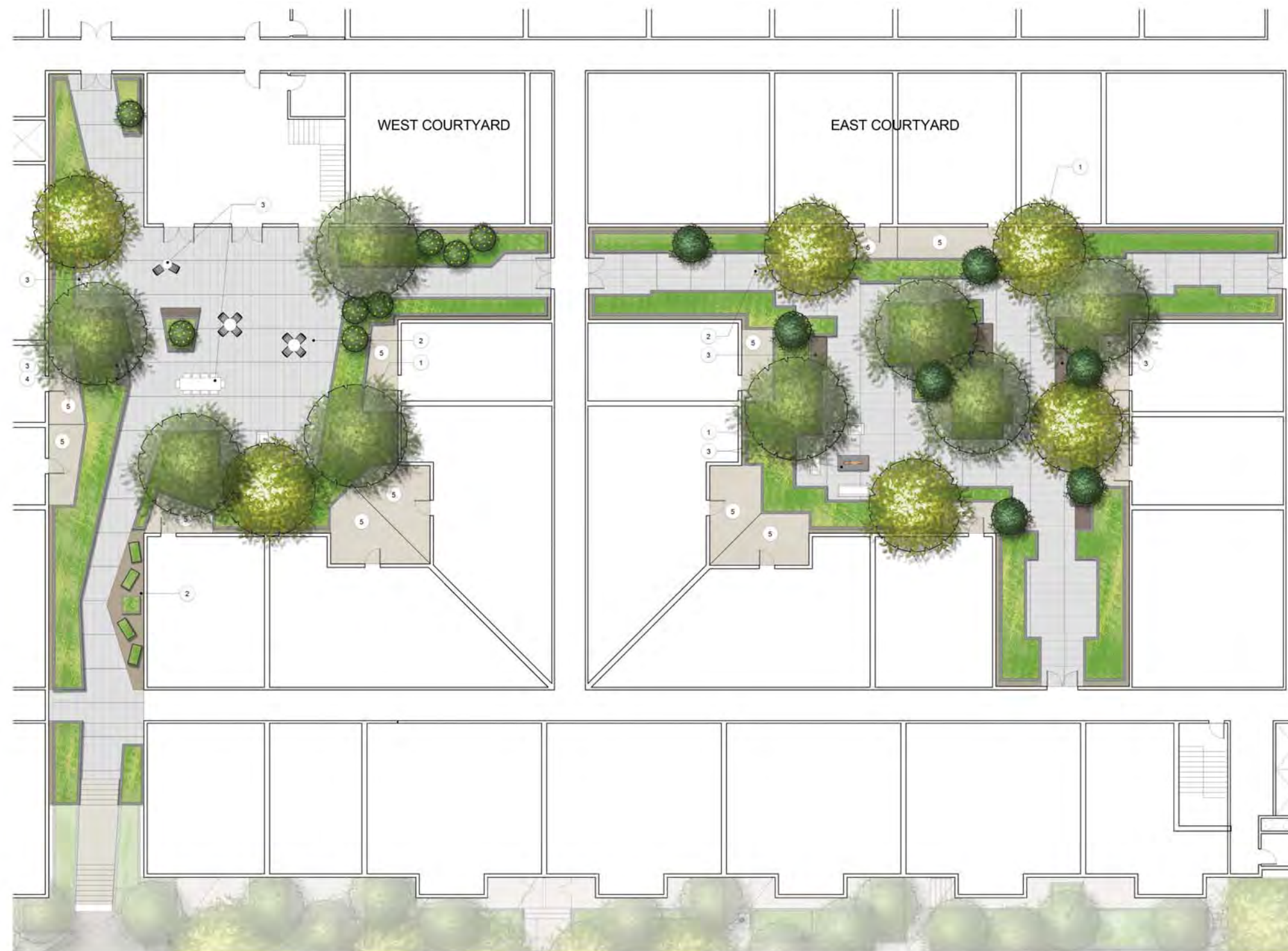


NOTES:  
1. COMMON NAME: LACEBARK ELM  
2. DECIDUOUS  
*Ulmus parvifolia*



NOTES:  
1. ECOFLEX BIKE RACK  
2. LOCALLY SOURCED AND MANUFACTURED  
3. MADE FROM RECLAIMED, COATED STEEL CABLE: RESISTANT TO CUTTING  
*BIKE RACK*

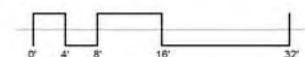




#### 16TH COURTYARD NOTES

##### BOTH COURTYARD SPACES:

- 1 PLANTING:  
TREES:  
LARGE SHADE TREES:  
MAGNOLIA X 'ELIZABETH'  
  
SMALL ORNAMENTAL TREES:  
CERCIS CANADENSIS 'ALBA'  
CITRUS 'MEYER IMPROVED'  
CUPRESSUS SEMPERVIRENS 'STRICTA'  
  
UNDERSTORY:  
DROUGHT TOLERANT PERENNIALS, GROUNDCOVERS, AND GRASSES.
- 2 HARDSCAPE:  
PAVING: PERMEABLE CONCRETE PAVERS  
PAVING UNDER PLANTERS: DECORATIVE PEBBLES  
WALLS: CAST IN PLACE BOARD-FORMED CONCRETE
- 3 SITE FURNISHINGS:  
LOUNGE AND CAFE SEATING AND TABLES  
BUILT-IN PLATFORM BENCHES  
FIREPIT: CIP BOARD-FORMED CONCRETE  
SUMMER KITCHEN
- 4 LIGHTING:  
PATH LIGHTING  
ARCHITECTURAL LIGHTING BY ARCHITECTS  
TASK LIGHTING AT SUMMER KITCHEN
- 5 PRIVATE PATIOS
- 6 PATH AND STEPS TO MISSISSIPPI MEWS







1 16th St Roof Level Amenity Deck  
1/8" = 1'-0"

#### ROOF LEVEL AMENITY DECK NOTES

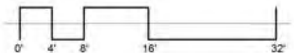
- 1 PLANTING:  
SMALL ORNAMENTAL TREES  
CASUARINA STRICTA  
  
DROUGHT TOLERANT SUCCULENTS, PERENNIALS, AND GRASSES
- 2 HARDSCAPE:  
PAVING: IPE WOOD DECKING WITH STEEL DIVIDERS  
GLASS PERIMETER GUARDRAIL BY ARCHITECT  
ORNAMENTAL SCREENING FENCE BETWEEN STAIR AND ELEVATOR OVERRUNS
- 3 SITE FURNISHINGS:  
LOUNGE SEATS AND TABLES, OUTDOOR FLOOR "POUF" SEATING  
RAISED CUSTOM PLANTERS  
FIREPIT, CAST CONCRETE WITH INTEGRAL COLOR  
BAR
- 4 LIGHTING:  
TASK LIGHTING AT COUNTER  
ARCHITECTURAL LIGHTING, BUILDING MOUNTED
- 5 MAINTENANCE GATE



2 16th St Level 3 Amenity Roof Deck  
1/8" = 1'-0"

#### LEVEL 3 AMENITY ROOF DECK NOTES

- 1 PLANTING:  
SMALL ORNAMENTAL TREES:  
CITRUS MEYER IMPROVED  
  
DROUGHT TOLERANT SUCCULENTS, PERENNIALS, AND GRASSES
- 2 HARDSCAPE:  
PAVING: IPE WOOD DECKING  
PERIMETER GUARDRAIL: BY ARCHITECT
- 3 SITE FURNISHINGS:  
BENCHES  
FIREPITS  
GFRP PLANTERS IN DECORATIVE PEBBLE AREA
- 4 LIGHTING:  
STRING LIGHTS (NOT SHOWN)

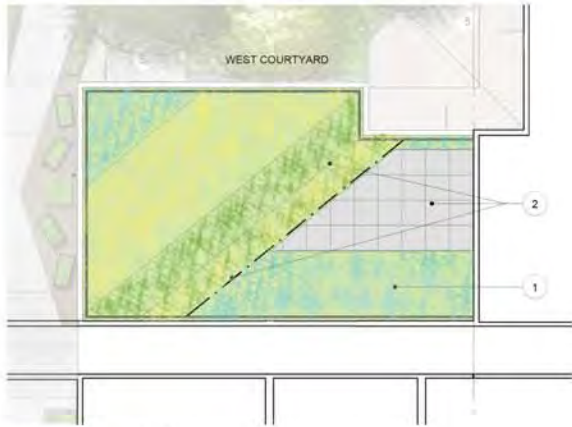




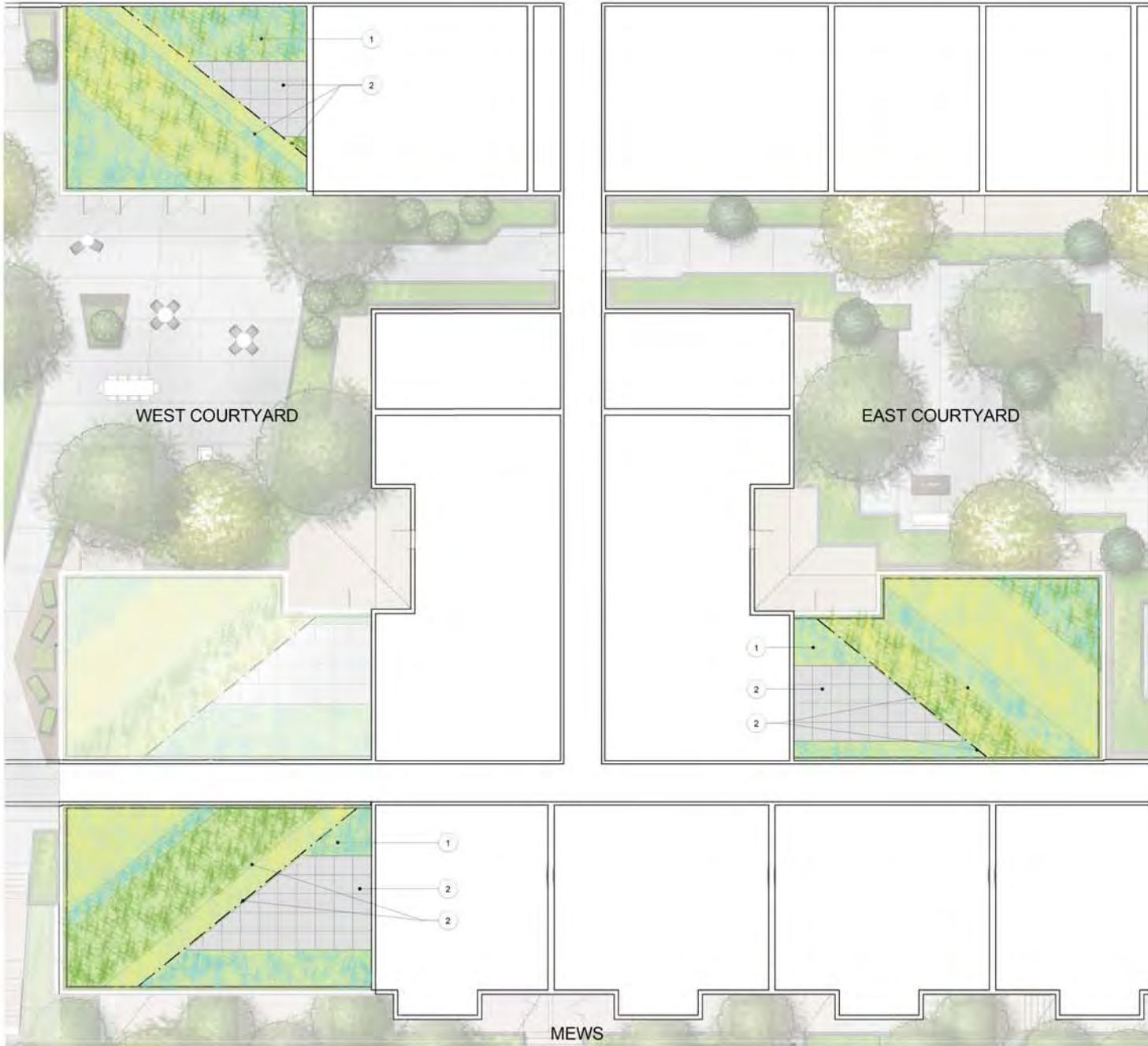
**EXTENSIVE GREEN ROOFS NOTES**

**BOTH 5TH FLOOR AND 6TH FLOORS:**

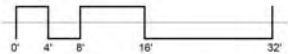
- 1 PLANTING:  
DROUGHT TOLERANT SUCCULENTS AND GRASSES IN SHALLOW SOIL PROFILE.
- 2 HARDSCAPE:  
PAVING ON TERRACES: PERMEABLE PAVERS  
PERIMETER GUARDRAIL: GLASS  
PRIVATE PATIO FENCING: CLEAR AND COLORED ACRYLIC  
ORNAMENTAL DIVIDERS IN PLANTING: COLORED ACRYLIC
- 3 LIGHTING:  
ARCHITECTURAL LIGHTING MOUNTED ON BUILDING  
DECORATIVE LIGHTING OF ORNAMENTAL DIVIDERS AND PRIVATE FENCING



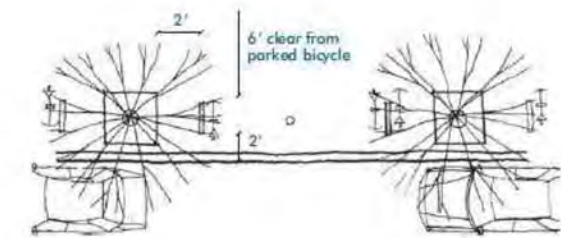
1 16th St Level 5 Extensive Roof Deck  
1/8" = 1'-0"



2 16th St Level 6 Extensive Roof Decks  
1/8" = 1'-0"

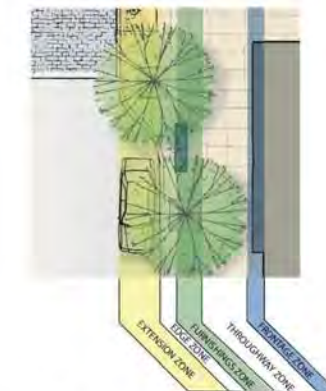






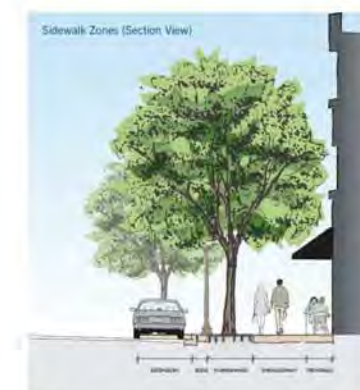
NOTES:  
1. TYPICAL BIKE RACK PLACEMENT FOR SIDEWALKS >12'  
2. SAN FRANCISCO BETTER STREETS PLAN  
3. P.221

2 Better Streets Sidewalk Bike Rack Plan Typ.  
1" = 1'-0"



NOTES:  
1. MIXED-USE TYPICAL SIDEWALK PLAN  
2. SAN FRANCISCO BETTER STREETS PLAN  
3. P.88

3 Better Streets Sidewalk Plan Zones Typ.  
1" = 1'-0"



NOTES:  
1. MIXED-USE TYPICAL SIDEWALK SECTION  
2. SAN FRANCISCO BETTER STREETS PLAN  
3. P.100

4 Better Streets Sidewalk Section Zones Typ.  
1" = 1'-0"





NOTES:  
1. COMMON NAME: DROOPING SHE OAK  
2. EVERGREEN

Casuarina stricta



NOTES:  
1. COMMON NAME: CERCIS CANADENSIS 'ALBA'  
2. DECIDUOUS

Cercis canadensis 'Alba'



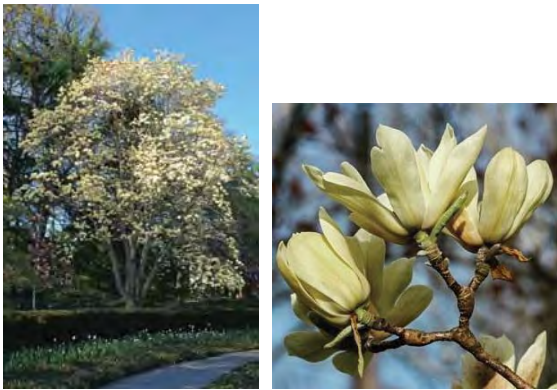
NOTES:  
1. COMMON NAME: MEYER LEMON  
2. EVERGREEN

Citrus 'Meyer Improved'



NOTES:  
1. COMMON NAME: ITALIAN CYPRESS  
2. EVERGREEN

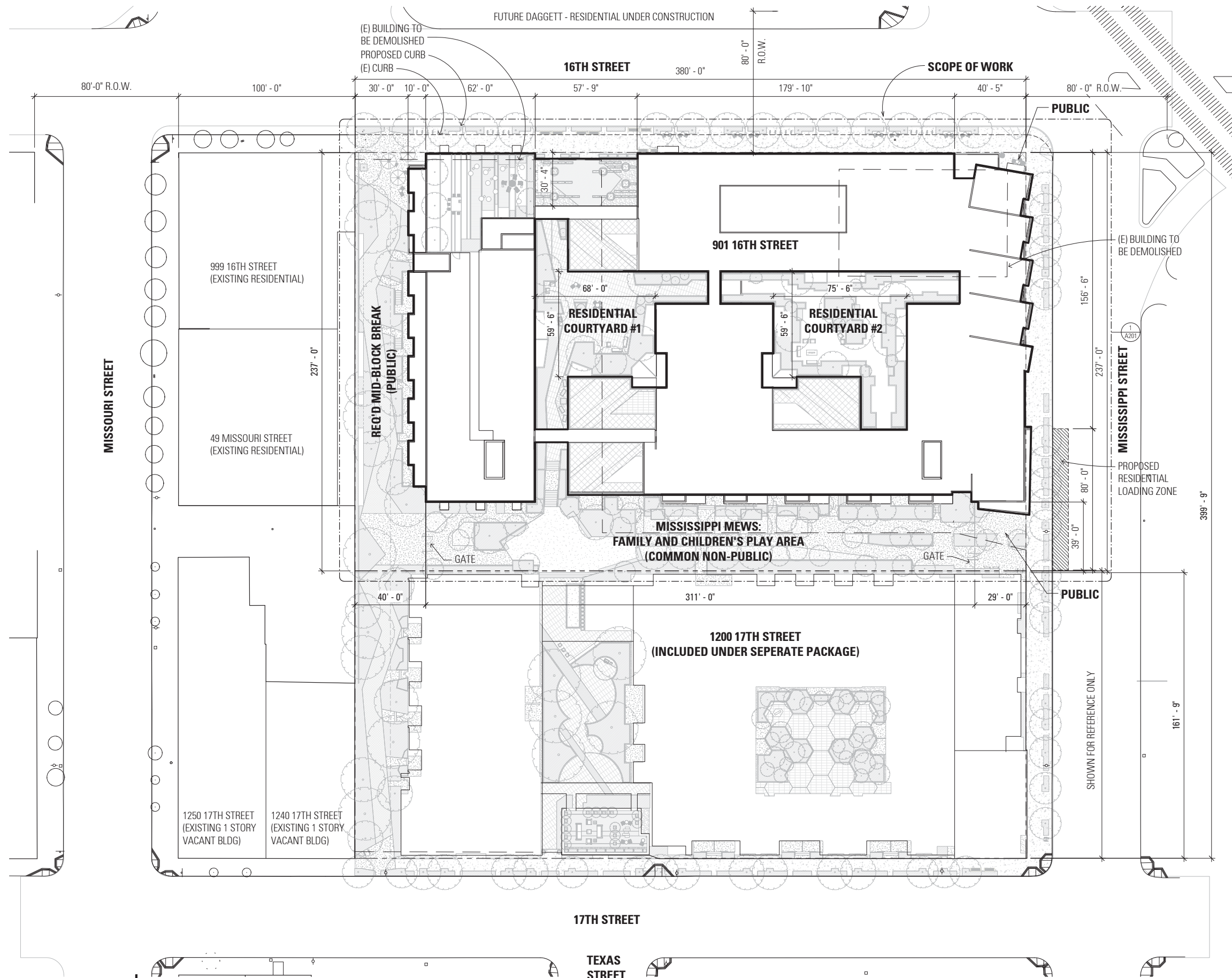
Cupressus sempervirens 'Stricta'



NOTES:  
1. COMMON NAME: ELIZABETH MAGNOLIA  
2. DECIDUOUS

Magnolia x 'Elizabeth'

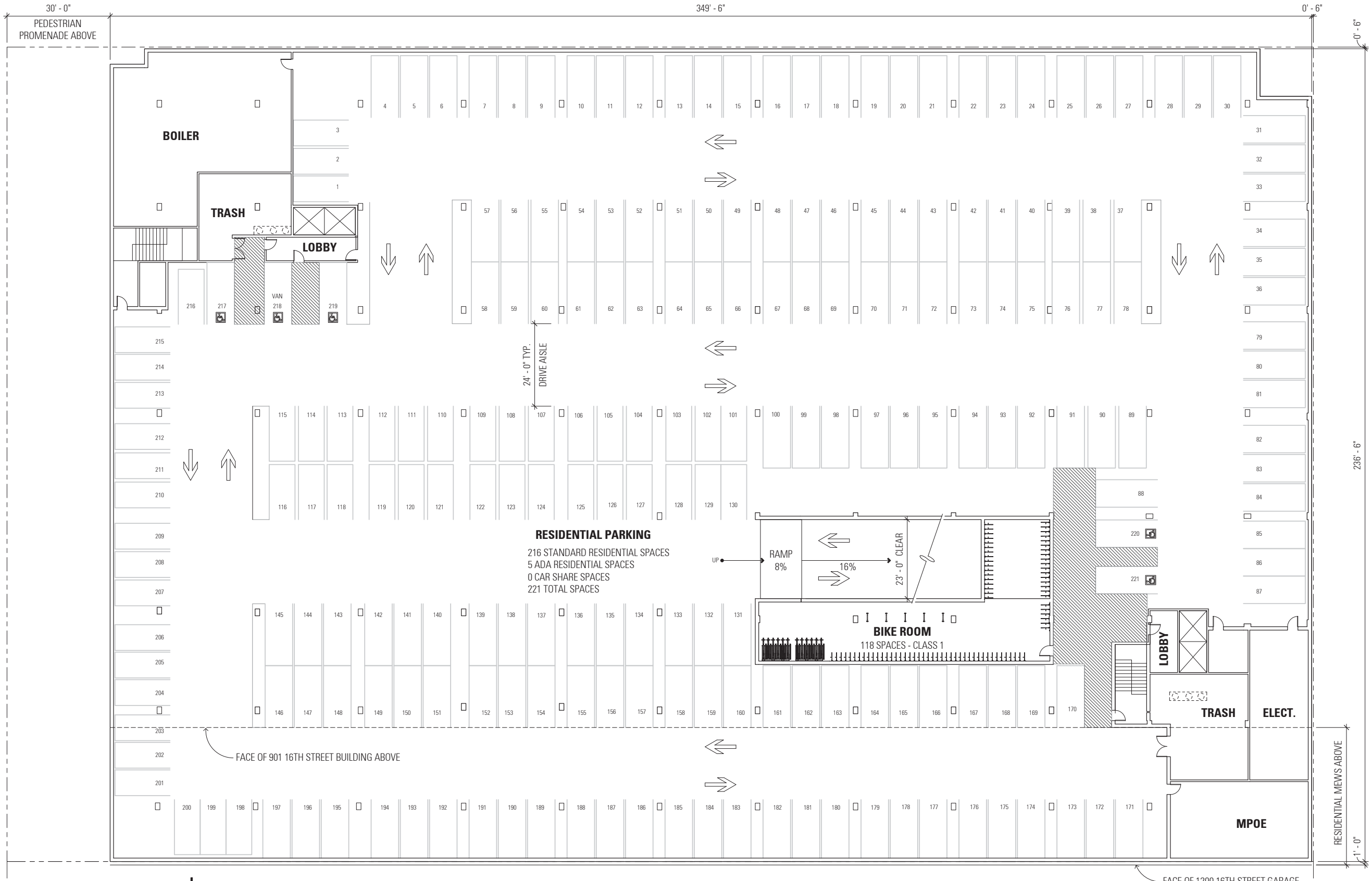




901 16th Street

SITE / ROOF PLAN

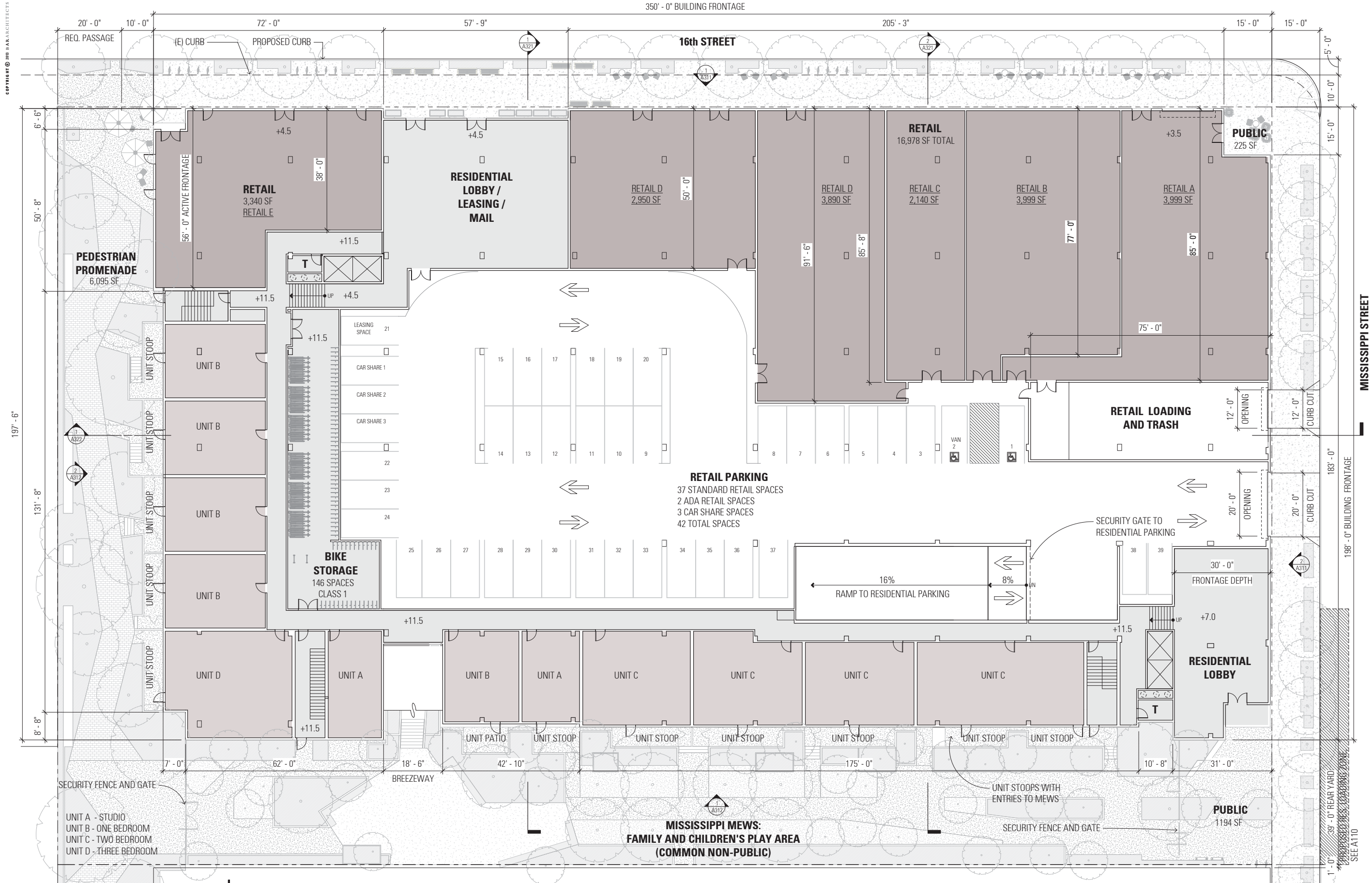




901 16th Street

PLAN BASEMENT LEVEL 1



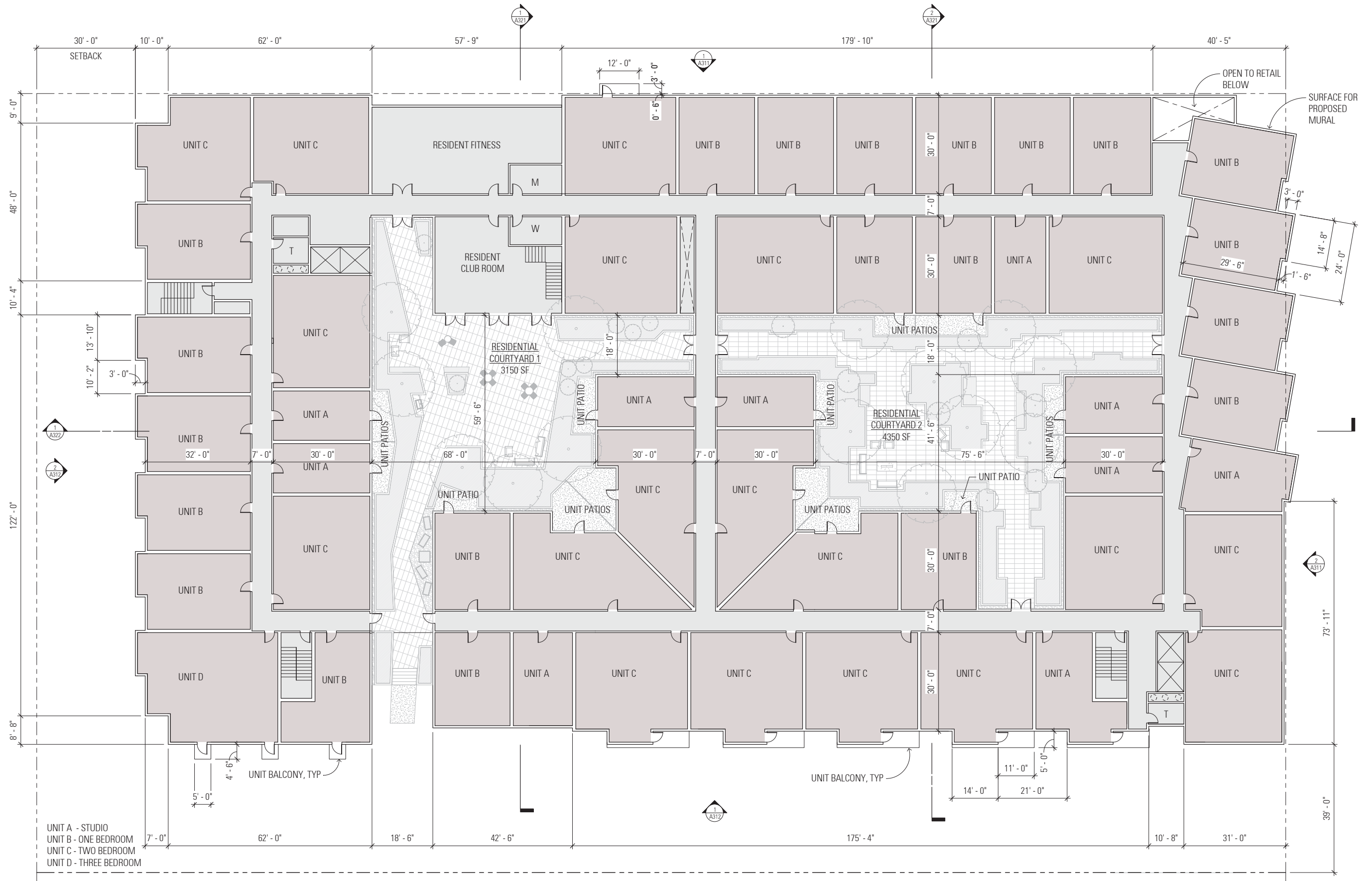


901 16th Street

GROUND FLOOR PLAN



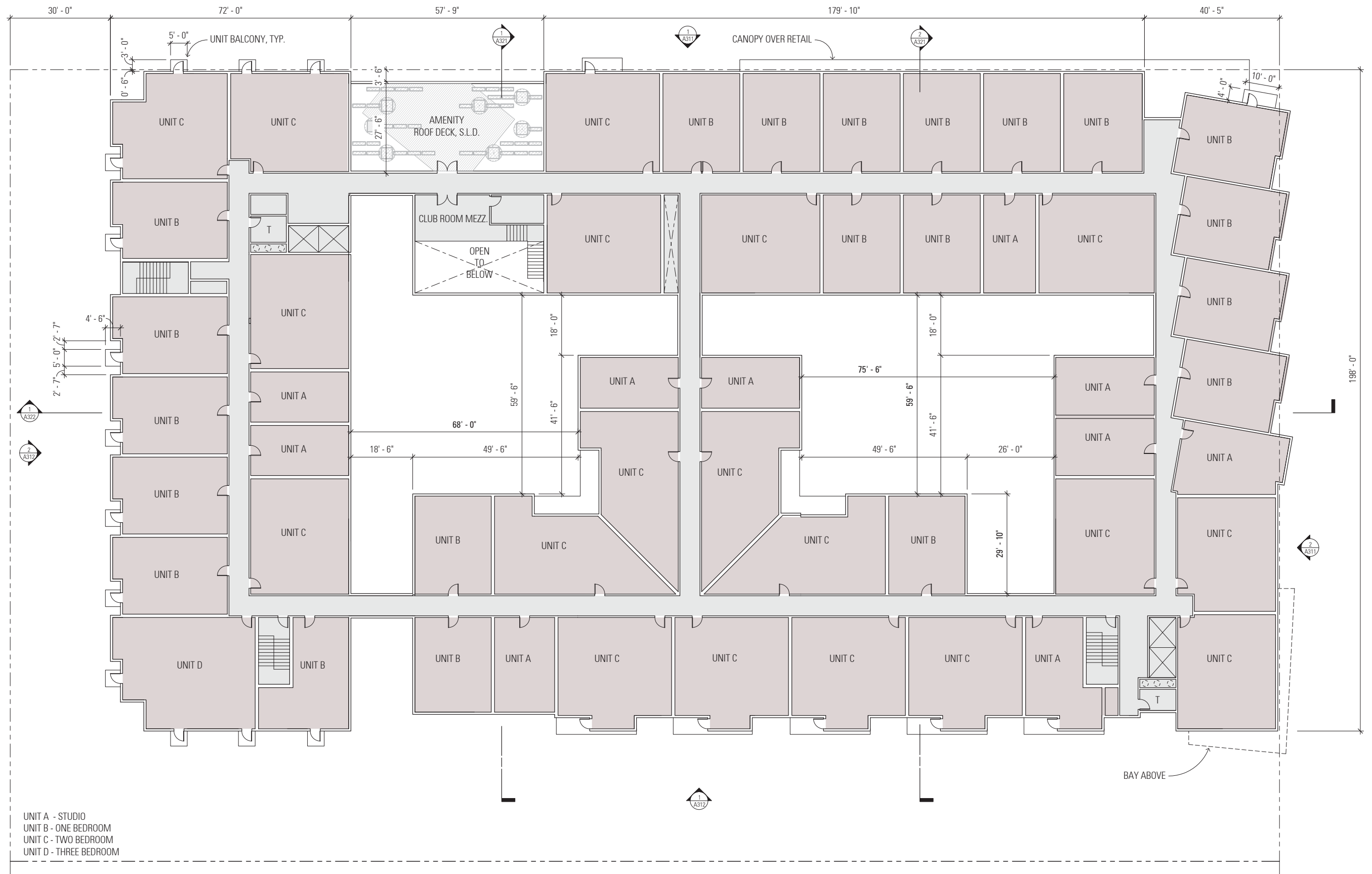




901 16th Street

PLAN LEVEL 2 (PODIUM)

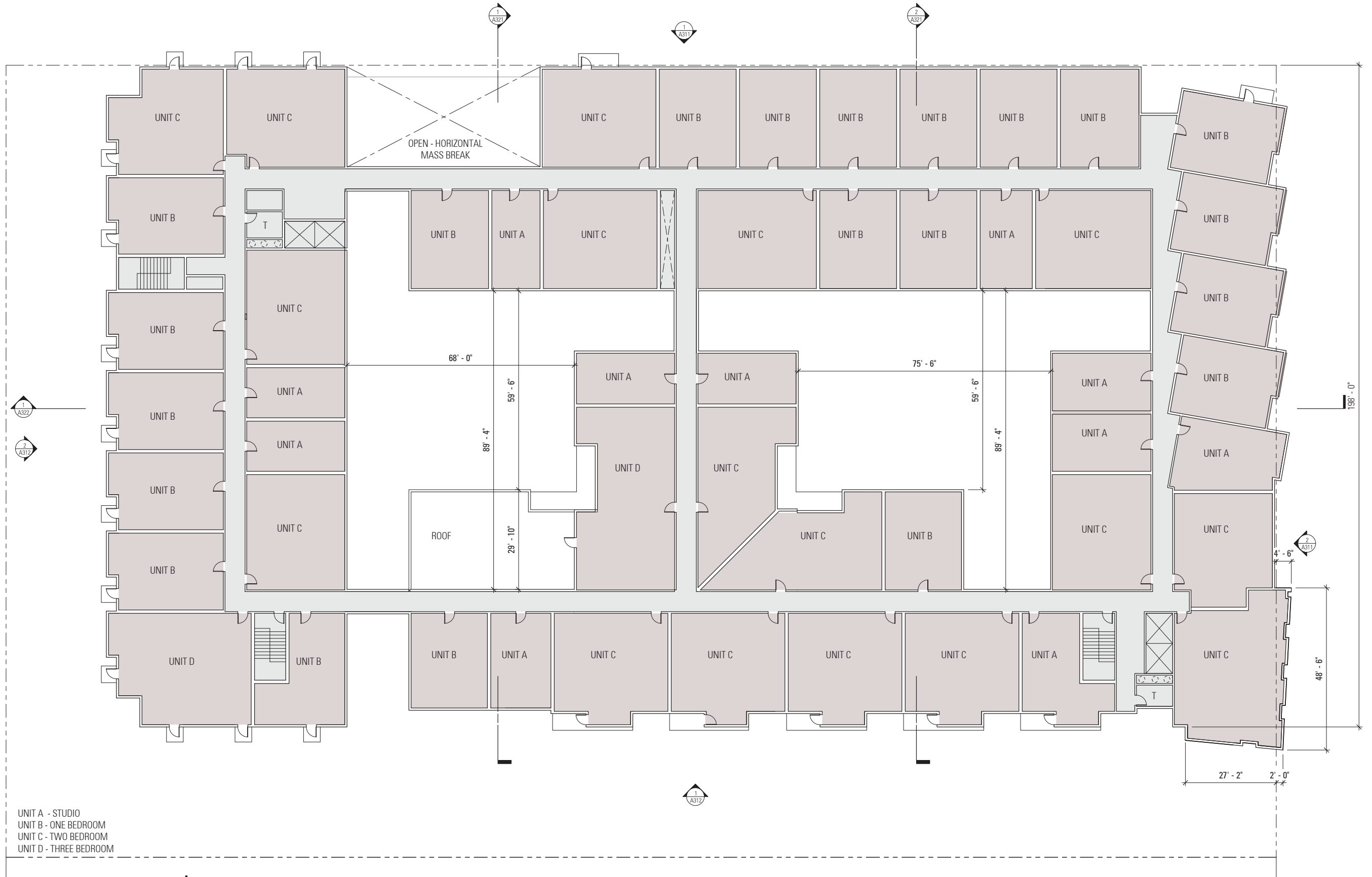




901 16th Street

PLAN LEVELS 3-4

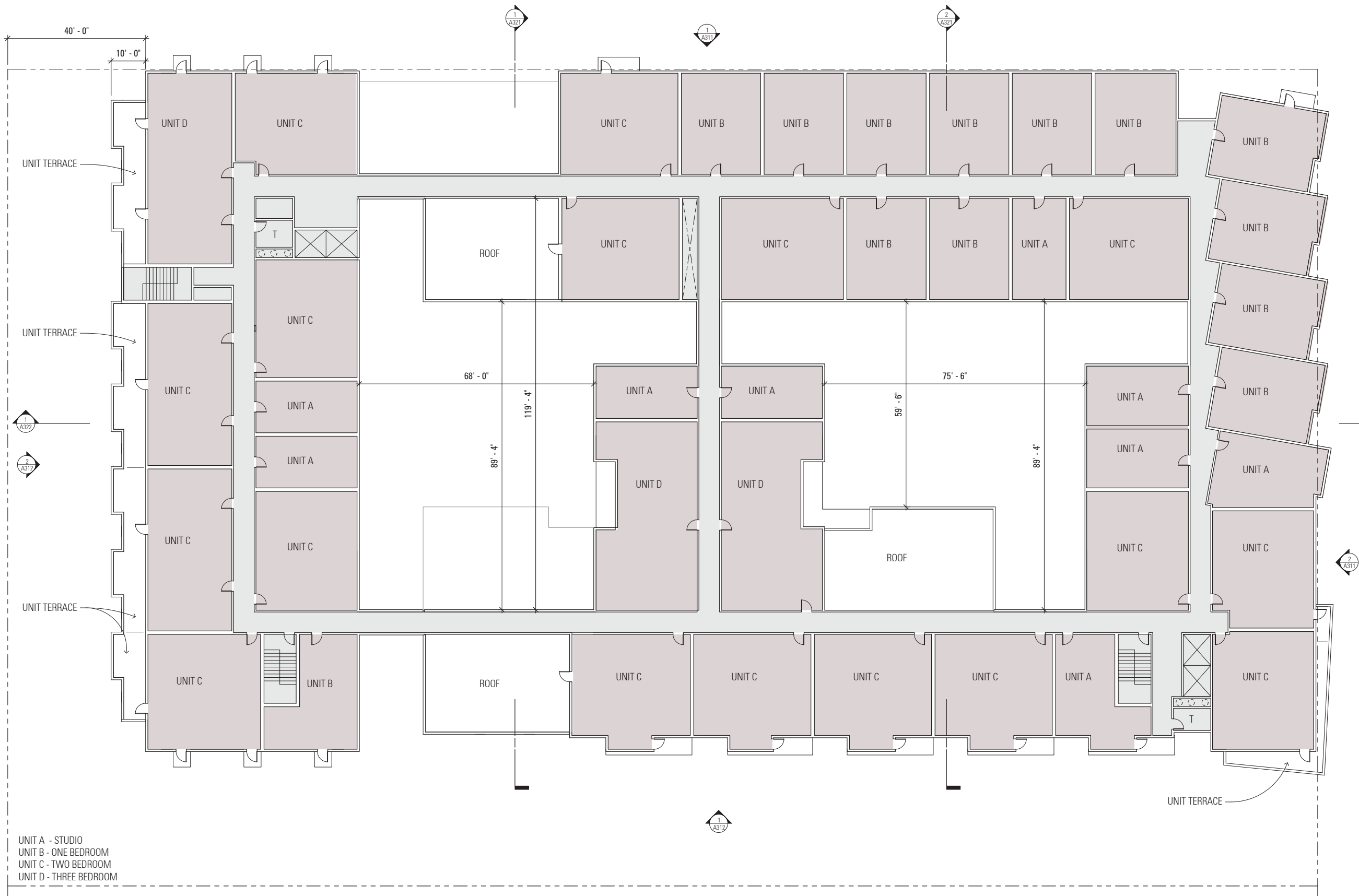




901 16th Street

PLAN LEVEL 5



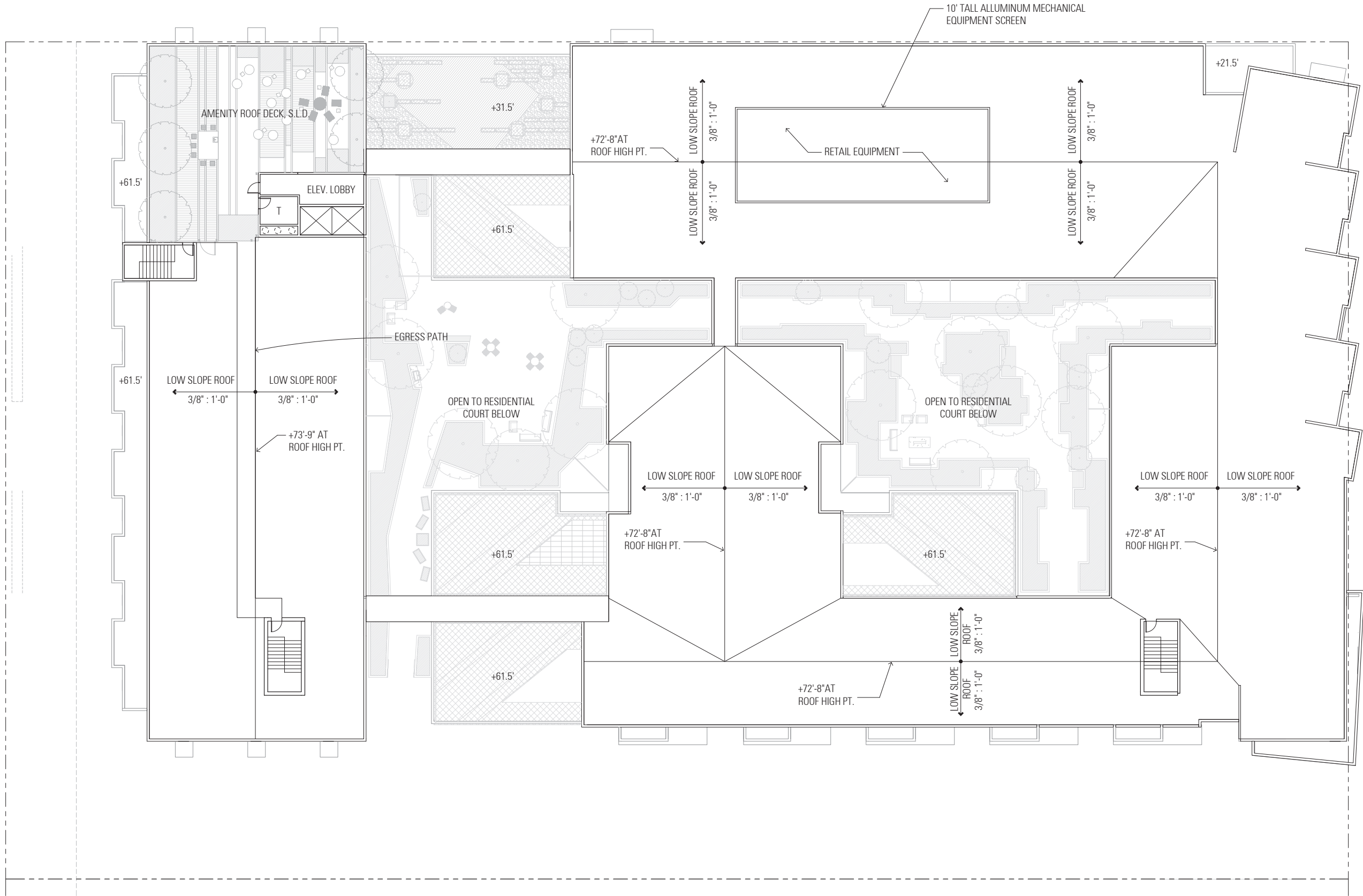


UNIT A - STUDIO  
UNIT B - ONE BEDROOM  
UNIT C - TWO BEDROOM  
UNIT D - THREE BEDROOM

901 16th Street

PLAN LEVEL 6





901 16th Street

ROOF PLAN



MATERIALS KEY

- |                       |                             |
|-----------------------|-----------------------------|
| 1. CEMENT PLASTER     | 7. STOREFRONT WINDOW SYSTEM |
| 2. METAL PANEL        | 8. TEMPERED GLASS GAUDDRAIL |
| 3. ALUMINUM EXTRUSION | 9. CABLE-RAIL GAUDDRAIL     |
| 4. BRICK              | 10. ALUMINUM SCREEN         |
| 5. CERAMIC TILE       | 11. ARCHITECTURAL CONCRETE  |
| 6. ALUMINUM WINDOW    |                             |



2 EAST ELEVATION - MISSISSIPPI  
3/32" = 1'-0"



1 NORTH ELEVATION - 16TH  
3/32" = 1'-0"

901 16th Street

EXTERIOR ELEVATIONS



MATERIALS KEY

- |                       |                             |
|-----------------------|-----------------------------|
| 1. CEMENT PLASTER     | 7. STOREFRONT WINDOW SYSTEM |
| 2. METAL PANEL        | 8. TEMPERED GLASS GAUDDRAIL |
| 3. ALUMINUM EXTRUSION | 9. CABLE-RAIL GAUDDRAIL     |
| 4. BRICK              | 10. ALUMINUM SCREEN         |
| 5. CERAMIC TILE       | 11. ARCHITECTURAL CONCRETE  |
| 6. ALUMINUM WINDOW    |                             |



2 WEST ELEVATION - PEDESTRIAN PROMENADE  
3/32" = 1'-0"



1 SOUTH ELEVATION - MEWS  
3/32" = 1'-0"

901 16th Street

EXTERIOR ELEVATIONS

**BAR**architects

901 Battery Street, Suite 300 | San Francisco, CA 94111 | 415 293 5700 | www.bararch.com

POTRERO PARTNERS LLC

PGA design

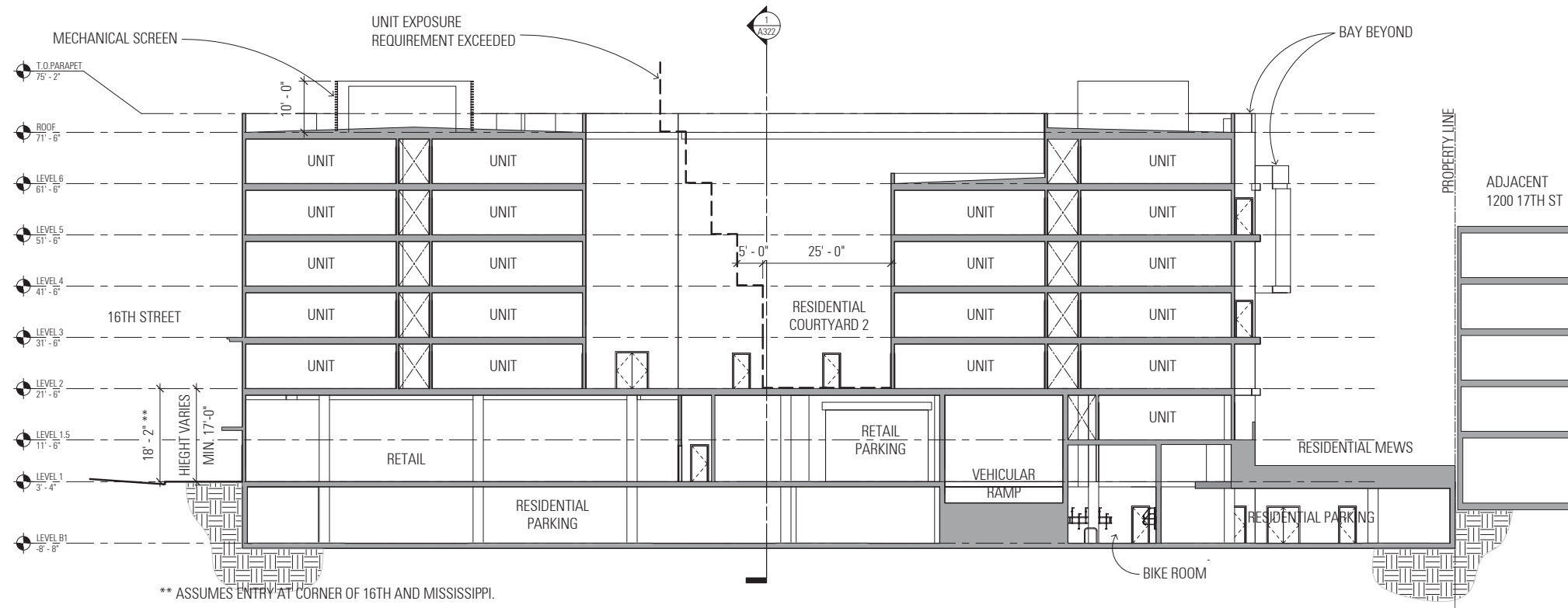
13051

5/12/2016

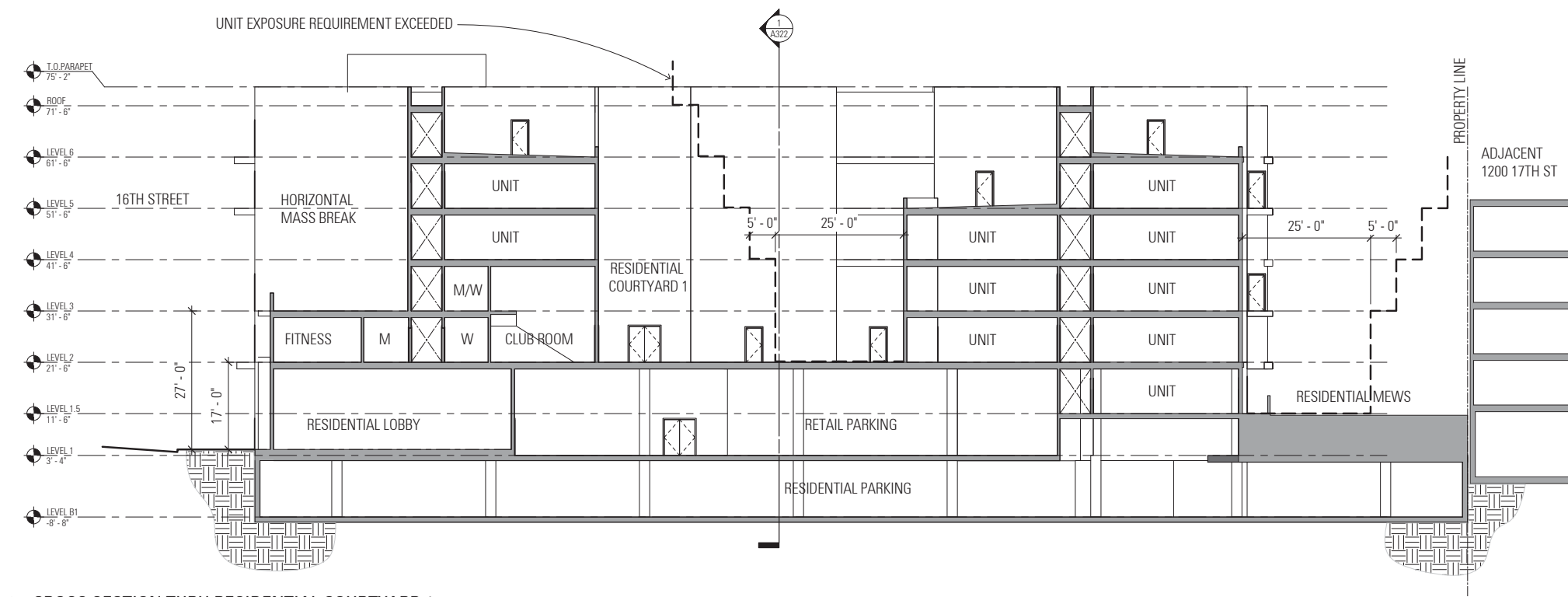
SCALE: 3/32" = 1'-0"

A312





2 CROSS SECTION THRU RESIDENTIAL COURTYARD 2  
A321 3/32" = 1'-0"

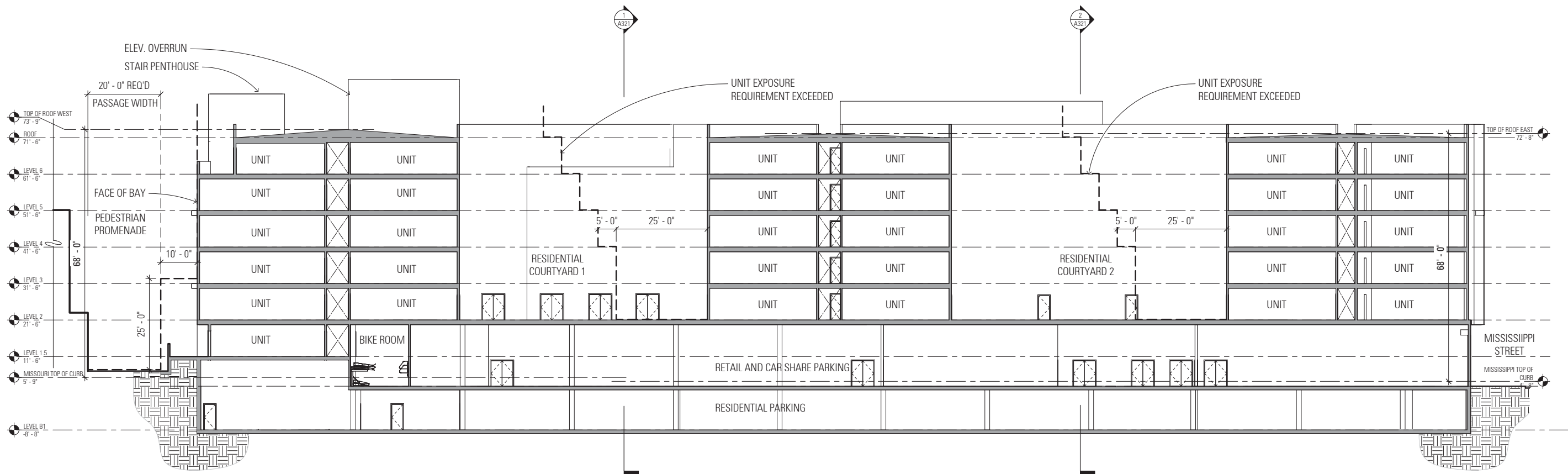


1 CROSS SECTION THRU RESIDENTIAL COURTYARD 1  
A321 3/32" = 1'-0"

901 16th Street

BUILDING SECTIONS



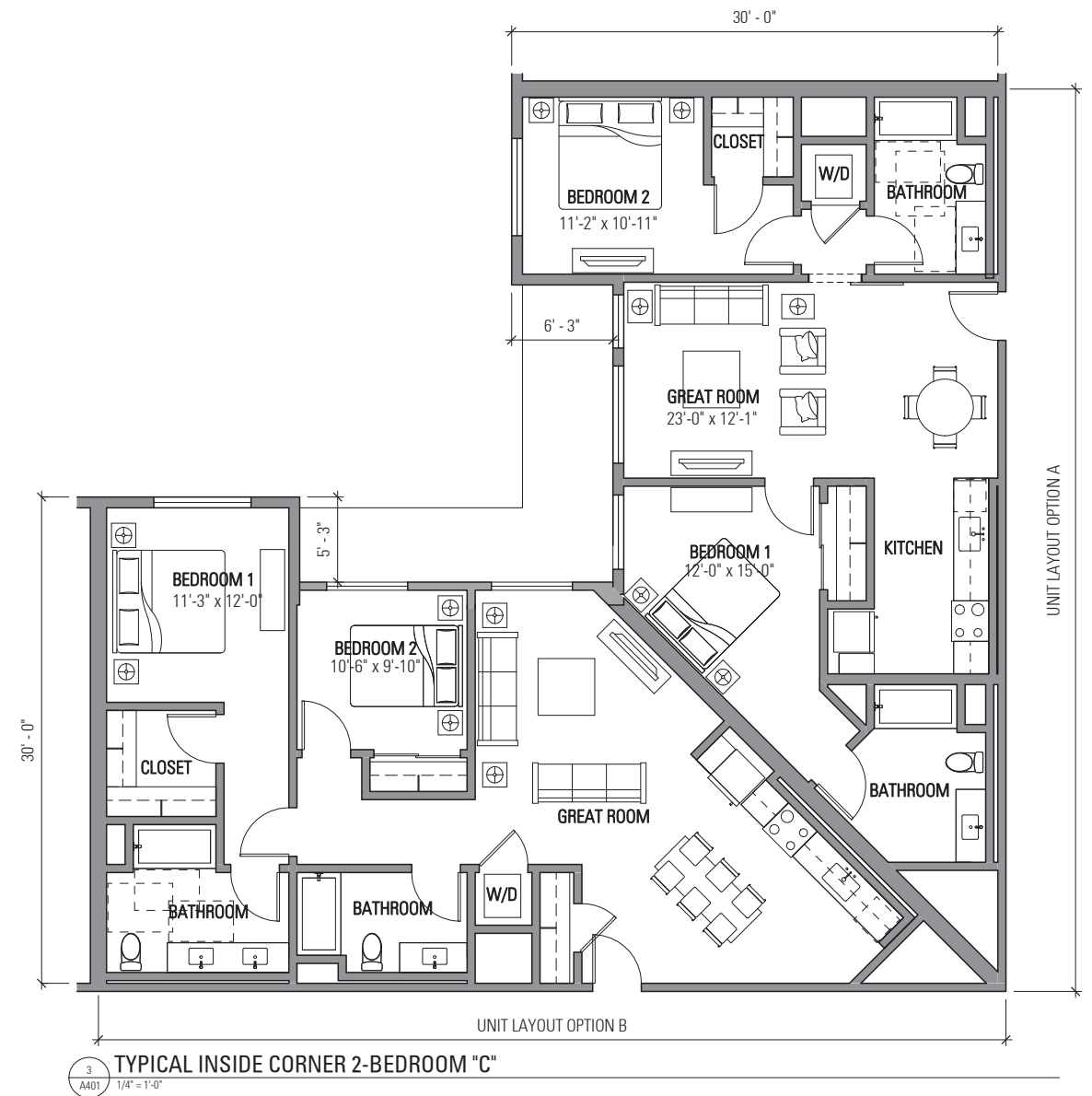
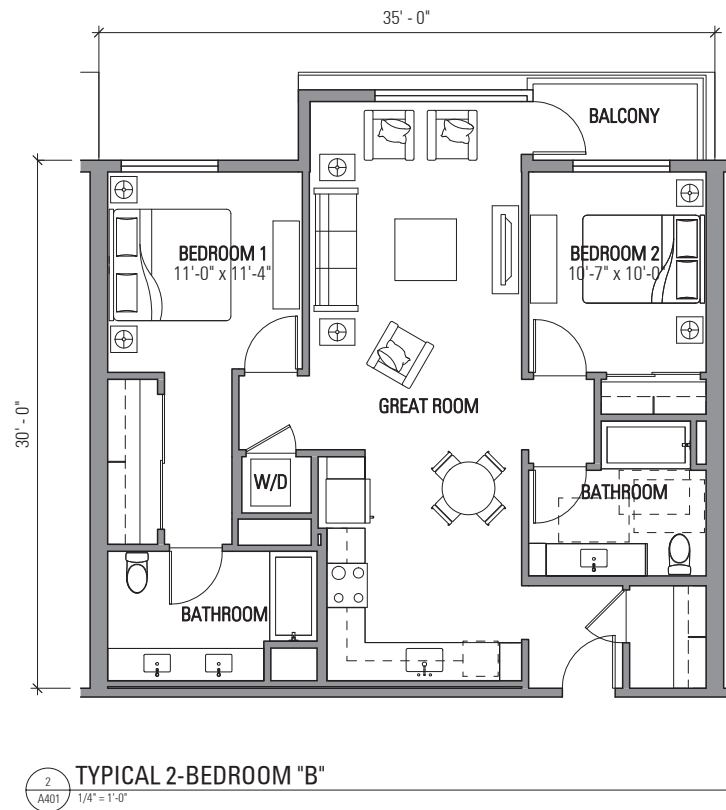
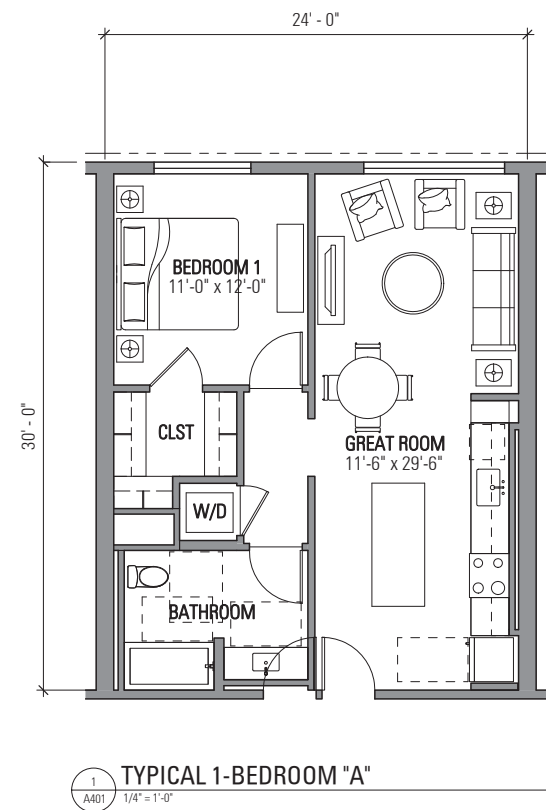
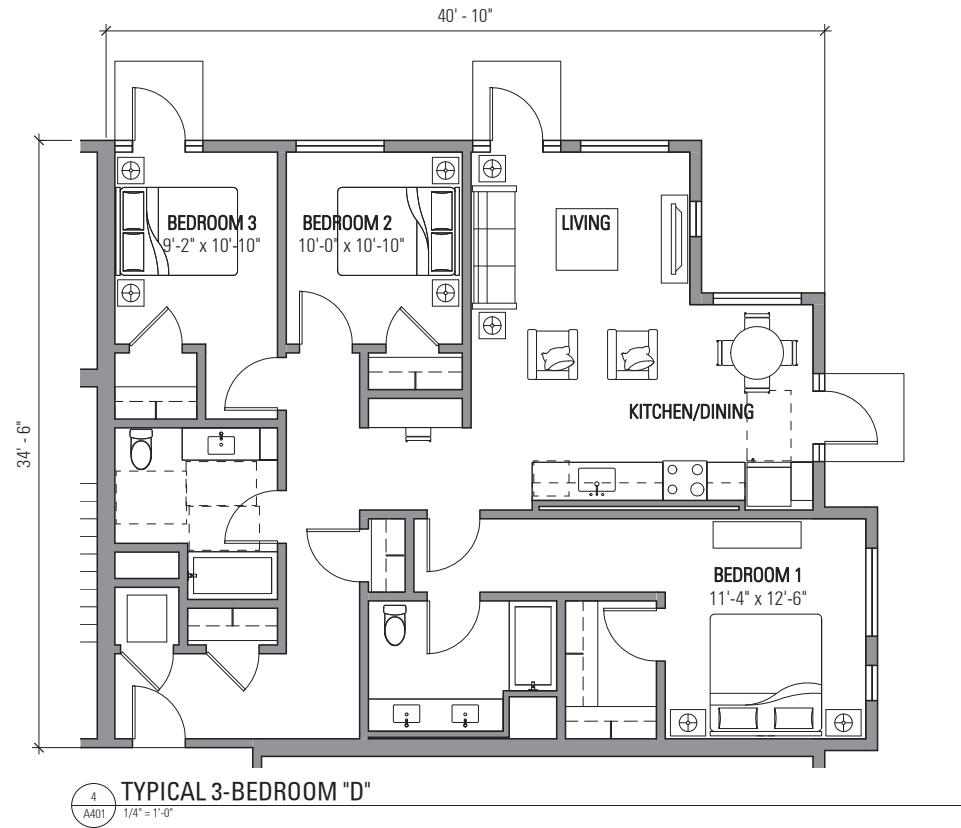


1 A322 LONGITUDINAL SECTION  
3/32" = 1'-0"

901 16th Street

BUILDING SECTIONS

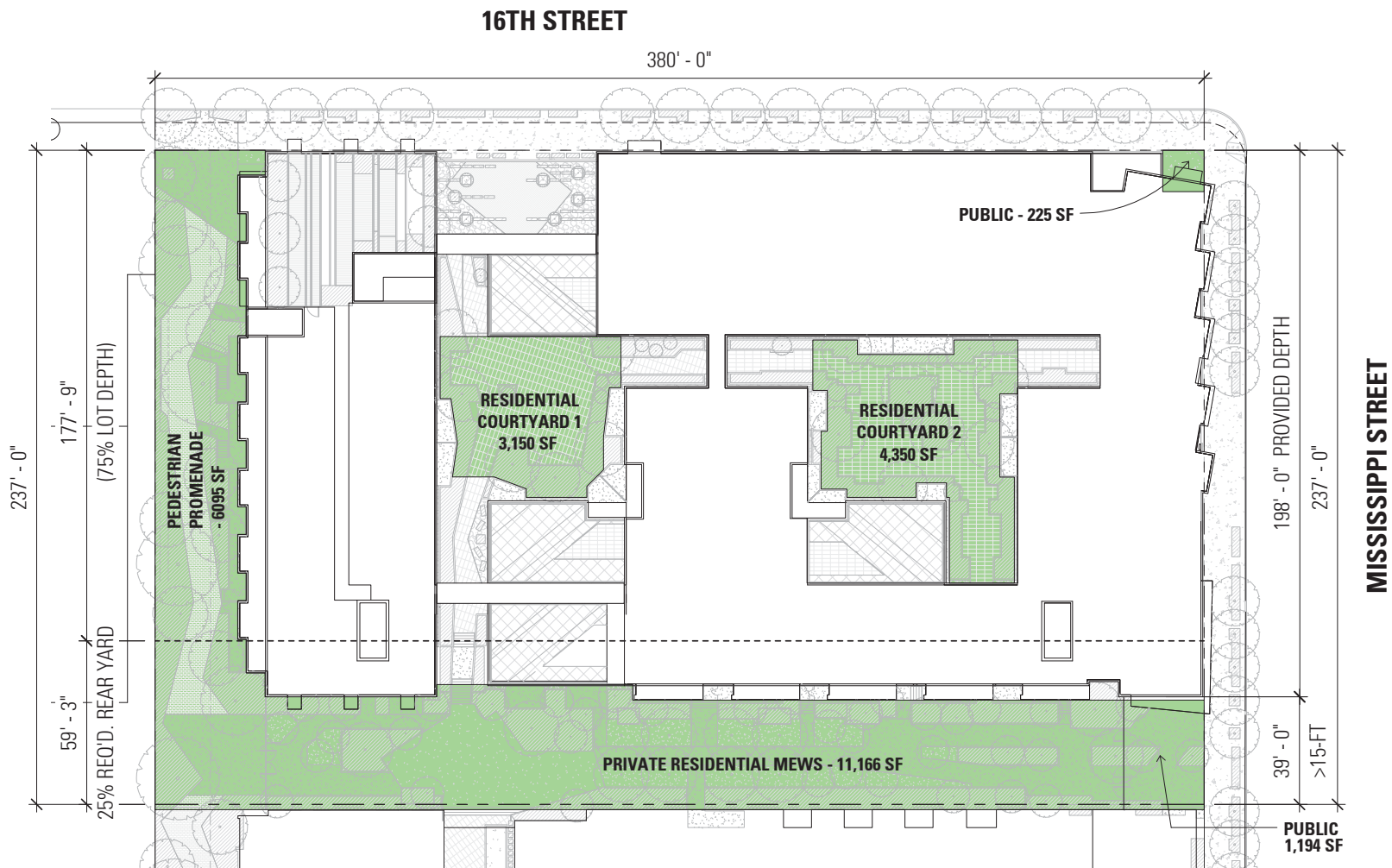




901 16th Street

TYPICAL UNIT PLANS





901 16TH STREET SITE PLAN  
SCALE: 1" = 60'

### MODIFICATION REQUEST 01 - REAR YARD

**Required:**

Per Section 134 (a)(1) UMU Districts are required to provide a rear yard depth of 25% total lot depth, but no less than 15-ft.

Comparable Rear Yard = .25 x 82,950 sf lot area = 20,737.5 sf

**Provided:**

39' provided to property line. Parcel 3949/002 provides an additional 1'-0" to face of building, allowing for a total clear dimension of 40'.

Comparable Rear Yard Areas Provided:

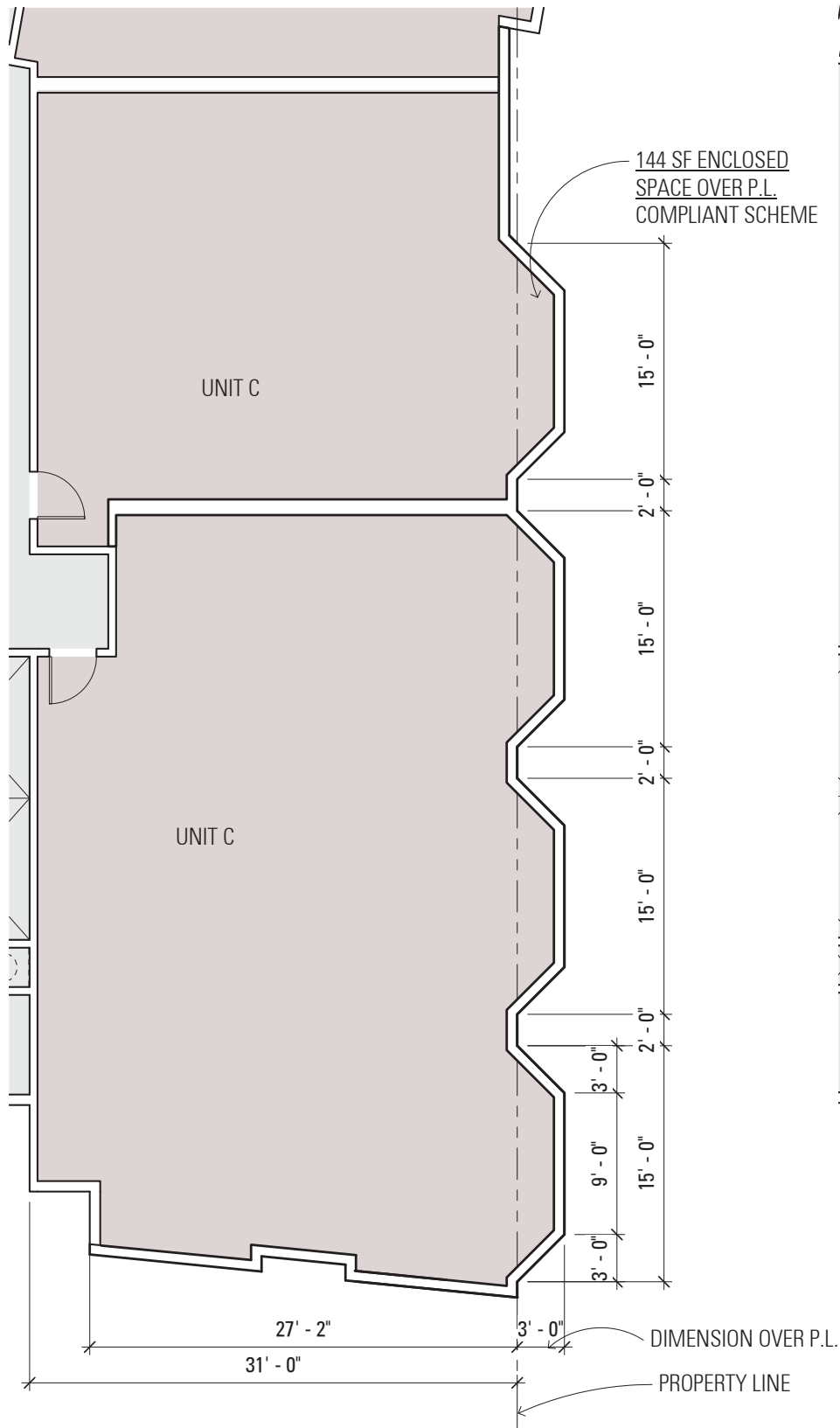
|                          |                         |
|--------------------------|-------------------------|
| Inner Courtyard 1        | 3,150 sf                |
| Inner Courtyard 2        | 4,350 sf                |
| Pedestrian Promenade     | 6,095 sf                |
| Private Residential Mews | 11,166 sf               |
| Public Use Areas         | 1,419 sf                |
| Total                    | 26,180 sf (Open to sky) |

\*\* Exceeds 25% rear yard equivalent by 5,442.5 sf

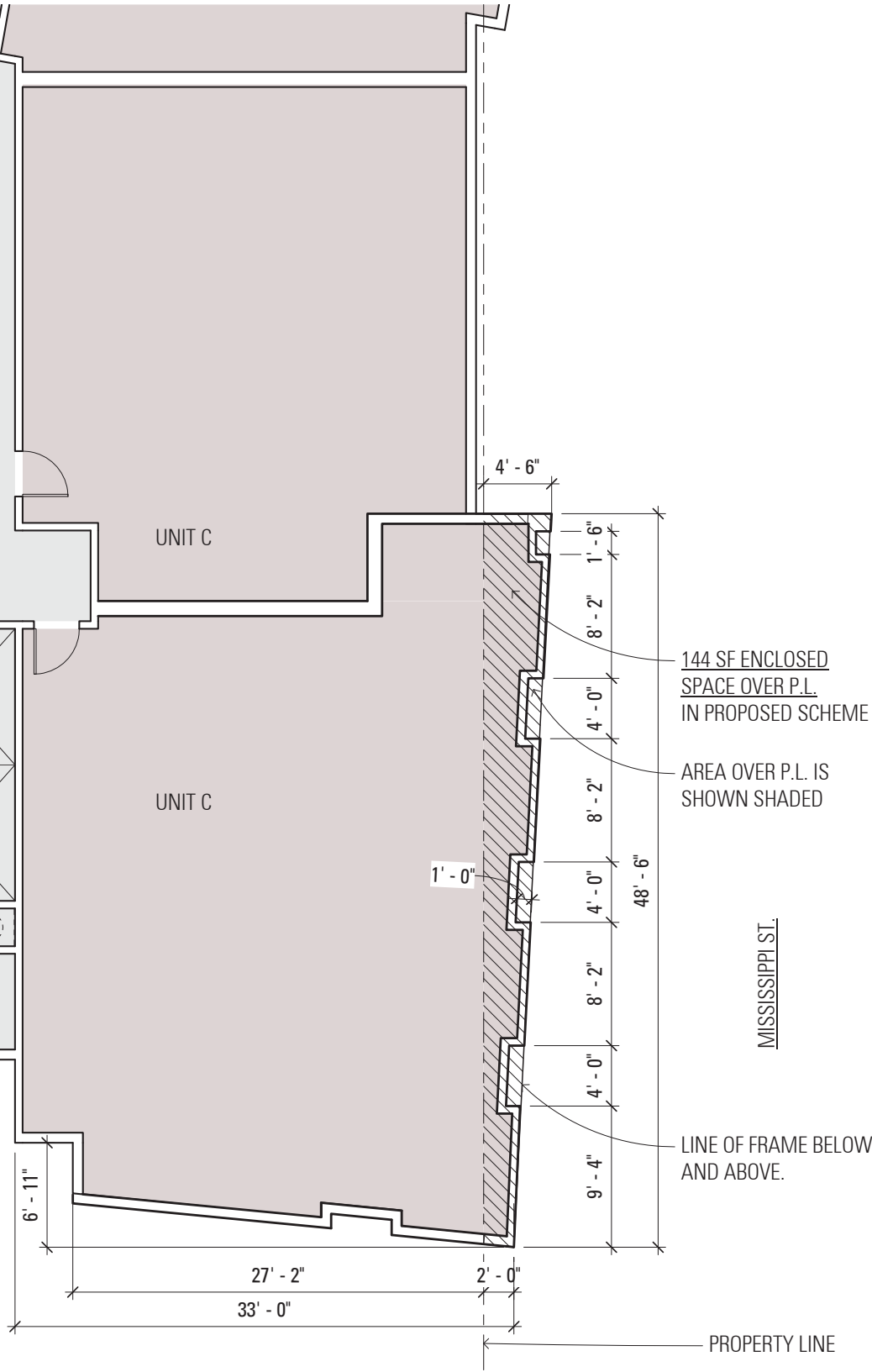
**Justification:**

Section 134(f) allows for a modification of the requirement through approval of the Planning Commission pursuant to Section 329. Project proposes providing a comparable amount of area elsewhere in the project. Project is providing an excess of the required Open Space, including 7,514 SF of publically accessible open space.





**COMPLIANT BAY WINDOW SCHEME - LEVEL 5**  
**SCALE: 3/16" = 1'-0"**



**PROPOSED BAY WINDOW PLAN - LEVEL 5**  
**SCALE: 3/16" = 1'-0"**

**MODIFICATION REQUEST: OBSTRUCTIONS OVER STREETS**

**Required:**  
Per Section 136 (c)(2) Bay (projecting) windows are governed by length and depth proportions established by this subsection.

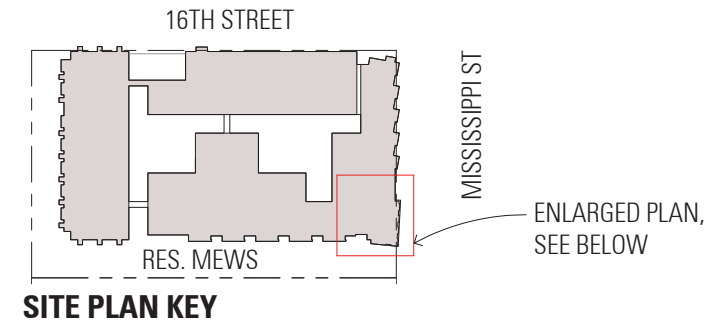
**Provided:**  
Bay windows throughout the project meet the requirements of Section 136 unless otherwise denoted.  
The bay window projection at the Southeast corner (Mississippi and Mews) of the project at Levels 4 and 5 requires a modification (see A205 for dimensions) due to the projection depth and length.

**Justification:**  
The intent of the projection is to architecturally bridge the transition between the differing design languages of the Mississippi and Mews elevations. In order to provide superior architectural design it is important that this element does not have the proportions of a standard bay outlined in Section 136. Rather it is an element with its own proportions different from that of a bay. Within the overall expression each bay component is expressed with a width which is Planning Code compliant. Due to the projection's height above grade, and it's location at the buildings corner, the projection does not negatively impact the streetscape experience.

COMPLIANT: 144 SF/FLR x 5 ALLOWABLE FLOORS = 720 SF TOTAL  
PROPOSED: 144 SF/FLR x 2 FLOORS = 288 SF TOTAL



**VIEW FROM MISSISSIPPI STREET AND MEWS**

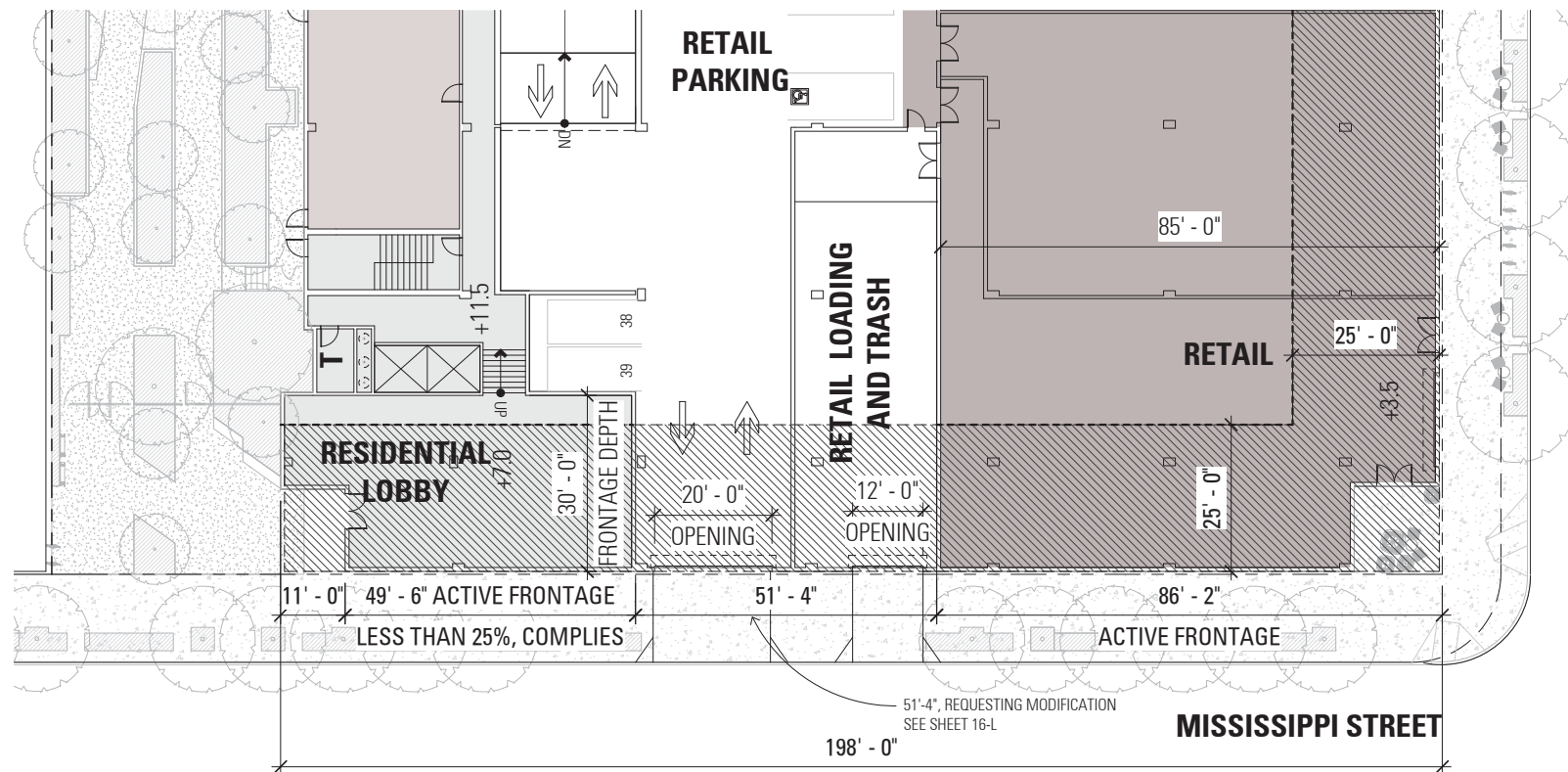


901 16th Street

**MODIFICATION REQUEST 02 - OBSTRUCTIONS - Sec.136**







**GROUND LEVEL PLAN - MISSISSIPPI STREET**  $(49.5+86.3) / 198 = 68\%$  ACTIVE USE, COMPLIES  
**SCALE: 1/32" = 1'-0"**

#### MODIFICATION REQUEST ACTIVE FRONTAGE AT MISSISSIPPI STREET

**Required:** Per Section 145.1  
Parking and loading entrances are limited to 20-ft per frontage

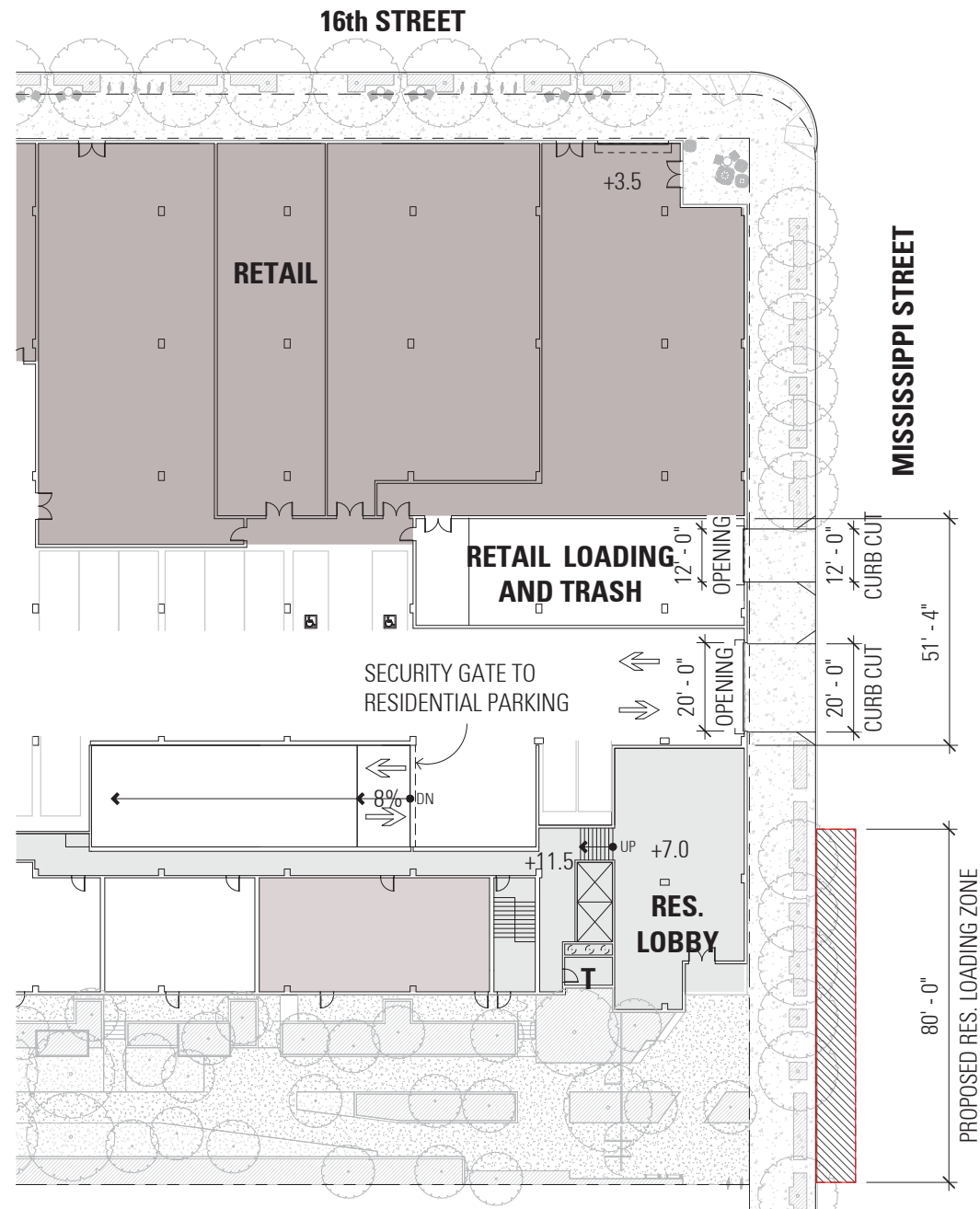
**Provided:**  
Project proposes two openings on Mississippi. One combined, retail and residential, parking entry with a 20-ft wide opening. One, 12-ft wide opening for retail loading.

**Justification:**  
No curb cuts are permitted along 16th Street, thus the project is limited to providing curb cuts along Mississippi, since no other street frontages exist. The project proposes limiting number of curb cuts by combing retail and residential parking to one entry. The retail loading and parking entries are being provided at minimum clear dimensions required for safety.

901 16th Street

## MODIFICATION REQUEST 03 - ACTIVE FRONTAGE - Sec.145.1





**SITE PLAN**  
**SCALE: 1" = 20'**

### MODIFICATION REQUEST : OFF STREET LOADING

**Required:** Per Table 152.1

Retail: 1 loading spaces required for retail uses 10,001 - 30,000 GSF

Residential: 2 loading space required for residential uses between 200,001-500,000 GSF

**Provided:**

One retail off-street loading zone provided along Mississippi - complies.

A 80-ft long painted loading zone on Mississippi near 16th is proposed

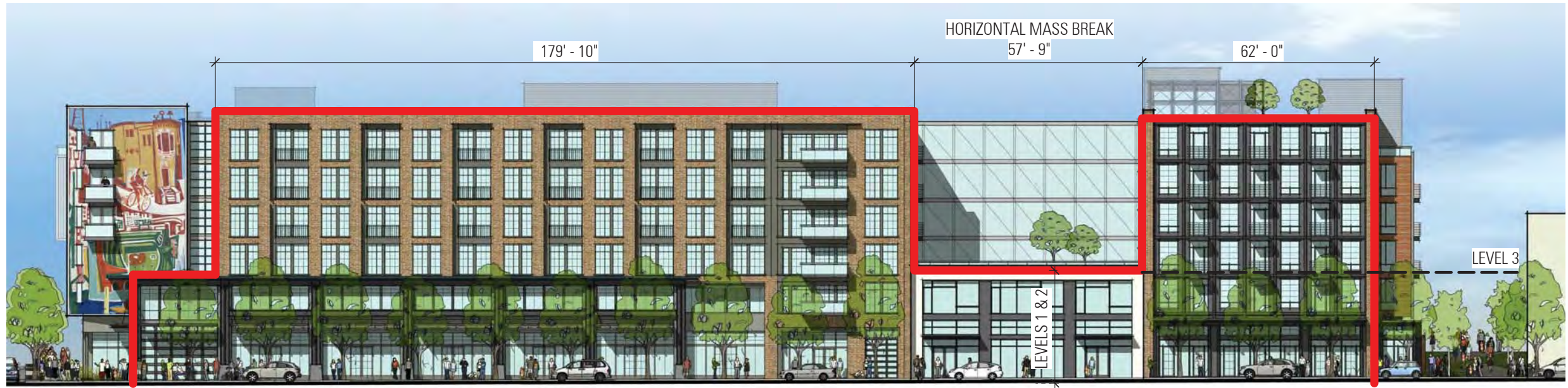
**Justification:**

No curb cuts are permitted along 16th Street, thus the project is limited to providing curb cuts along Mississippi, since no other street frontages exist. The project proposes two curb cuts on Mississippi for garage entry and retail loading. It is therefore proposed to provide residential loading on the street.

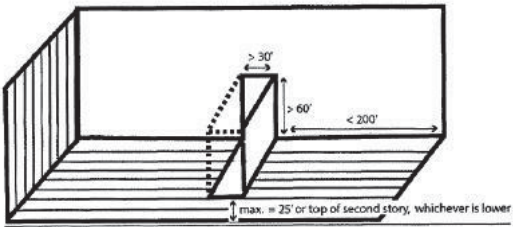
901 16th Street

## MODIFICATION REQUEST 05 - OFF STREET LOADING - Table 152.1





NORTH ELEVATION - 16TH STREET SHOWING THE HORIZONTAL MASS BREAK



**MODIFICATION REQUEST - HORIZONTAL MASS REDUCTION**

**Required:** Per Section 270.1  
Horizontal Mass Reduction break > 30' wide, >60' deep, from top of third story, and open to the sky, for length > 200 feet.

**Provided:**  
The 16th Street frontage is provided with a 57.75-ft wide by 30.3-ft deep void open to the sky. The reduction begins a 27-ft, providing a bulk reduction of 70,069 cubic feet. The reduction results in discrete frontages less than 200-ft.

**Justification:**  
Per Section 270.1(d) the Planning Commission may modify the requirement through the process set forth in Section 329. The following criteria for this modification has been met:  
- Section 270.1(d) Item 1: no more than 50% of the mass shall be reduced - while the proportions of the void have been modified, the area open to the sky remain the same as required.  
- Section 270.1(d) Item 2: the depth of any mass reduction breaks shall no be less than 15 feet in depth; 30.3-ft provided.  
- Section 270.1(d) Item 3: the proposed building envelope can be demonstrated to achieve the distinctly superior effect of reducing the apparent horizontal dimension of the building; the placement and width of the proposed break provide a clear reduction in horizontal length.  
- Section 270.1(d) Item 4: the proposed building achieves unique and superior architectural design; the proposed mass break occurs in a logical place in plan as it relates to the Texas Street axis and allows for a narrower street fronting mass adjacent to the Pedestrian Promenade.



VIEW OF MASS BREAK



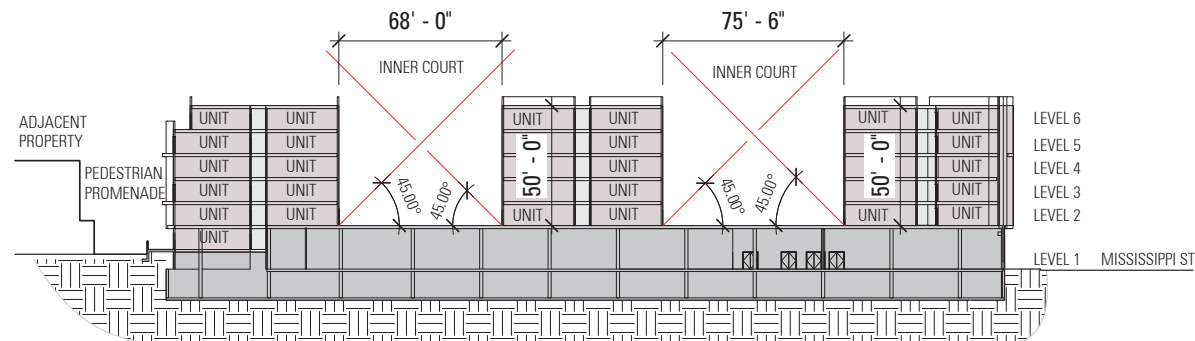
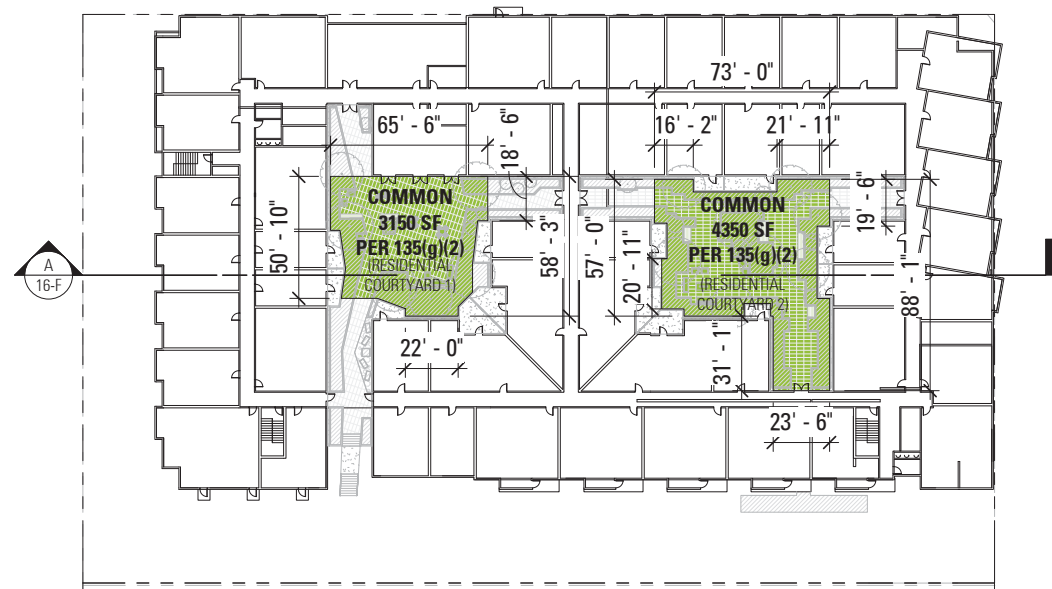
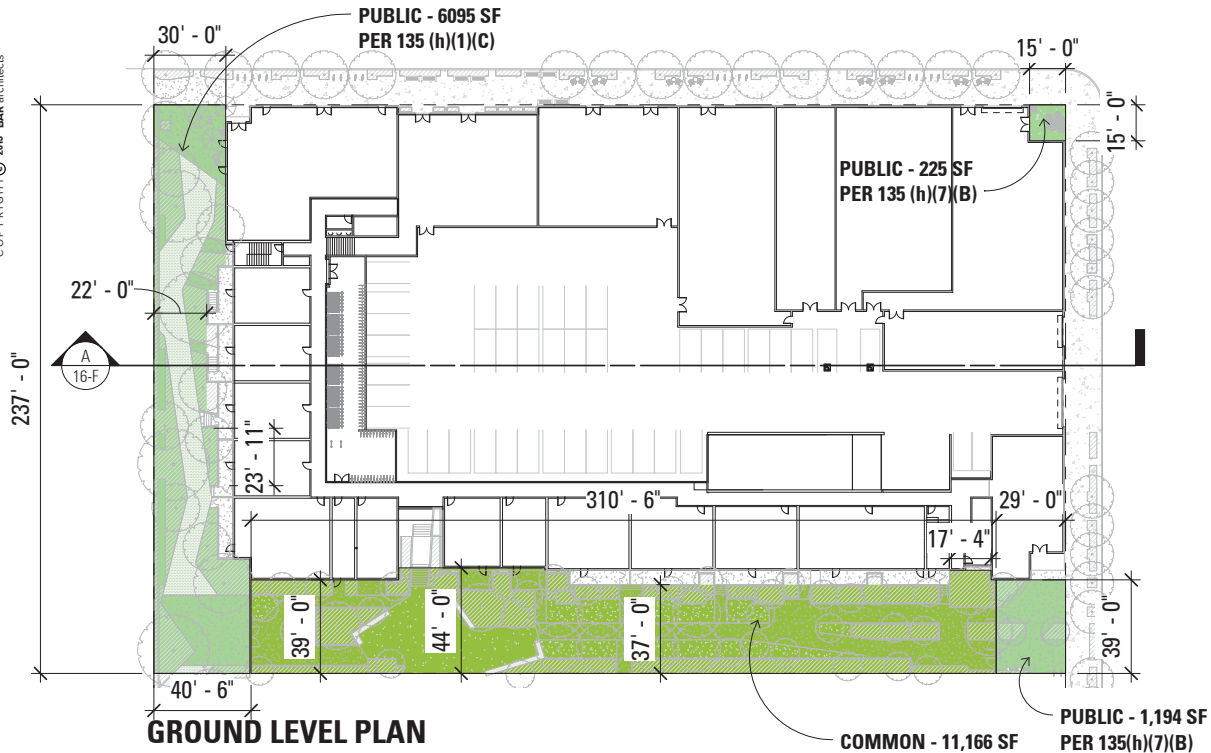
VIEW FROM DAGGETT PARK OF 16TH AND PEDESTRIAN PROMENADE

901 16th Street

**MODIFICATION REQUEST 06 - BULK LIMITATIONS - Sec. 270.1**



COPYRIGHT © 2013 BAR architects



**A. SECTION: INNER COURT DIMENSIONAL REQUIREMENTS FOR COMMON OPEN SPACE - Sec.135(g)(2)**

| REQUIRED     |           |                      |                |  | PROVIDED ON-SITE** |                     |        |               |
|--------------|-----------|----------------------|----------------|--|--------------------|---------------------|--------|---------------|
| Use          | Counts    | Req'd                | Total Required |  | Level              | Publicly Accessible | Common |               |
| Retail*      | 14,000 SF | 1 / 250 SF/SF Usable | 56 SF          |  | Podium Level       |                     | 7,500  | 7,500         |
| Residential  | 126 Units | x 80 SF/Unit         | 10,080 SF      |  | Ground Floor       | 7,514               | 11,166 | 18,680        |
|              | 134 Units | x 54 SF/Unit Public  | 7,236 SF       |  |                    |                     |        |               |
| <b>Total</b> |           |                      | <b>17,372</b>  |  |                    | 7,514               | 18,666 | <b>26,180</b> |
|              |           |                      |                |  |                    |                     |        | 26,180        |

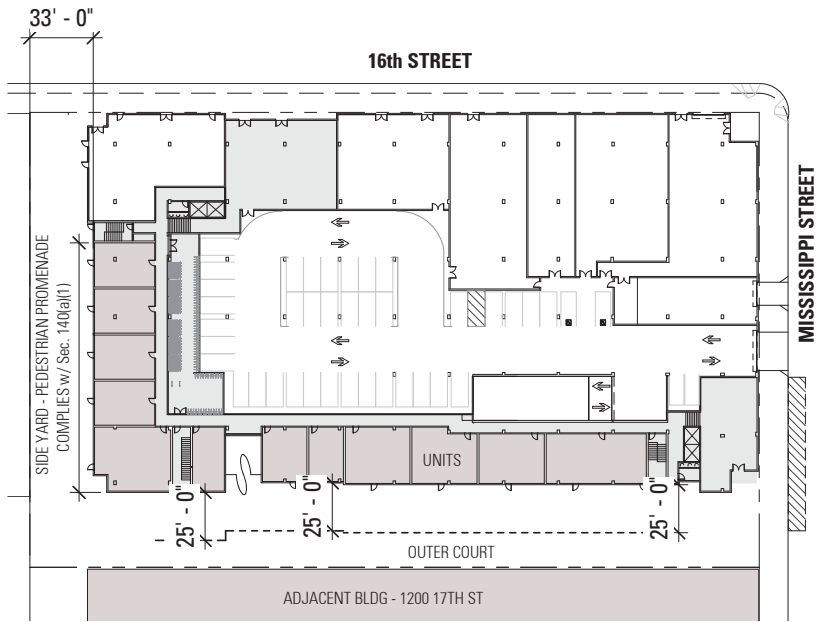
\* Retail SF shown as usable floor area and excludes back of house areas

\*\* Additional private and common open space provided at upper levels, but not shown as not required to meet code

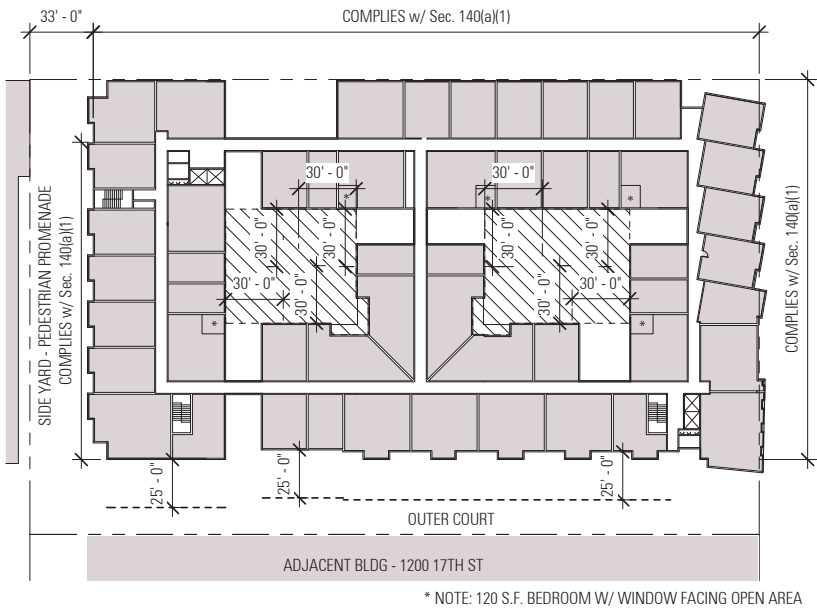
901 16th Street

COMPLIANCE - OPEN SPACE - Sec.135





**UNIT EXPOSURE - GROUND FLOOR**  
**SCALE: 1" = 100'**



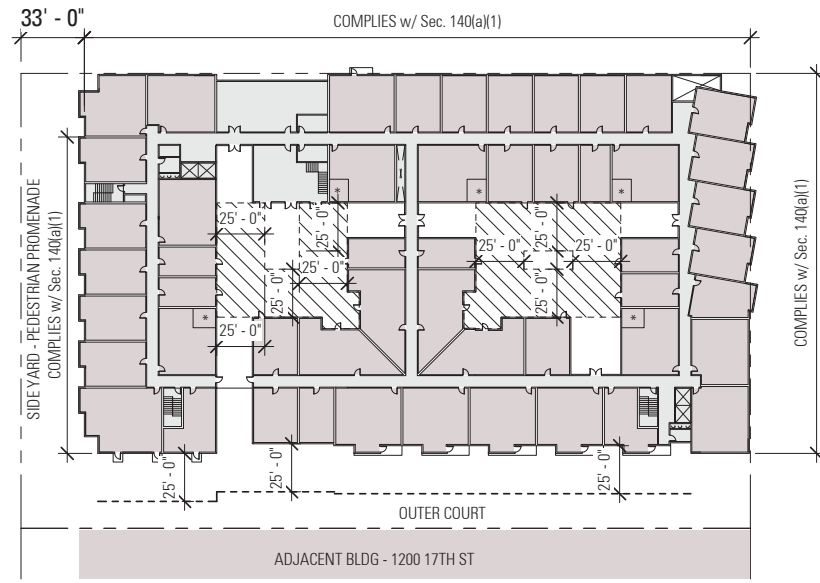
**UNIT EXPOSURE - LEVEL 4**  
**SCALE: 1" = 100'**

**UNIT EXPOSURE REQUIREMENT:**

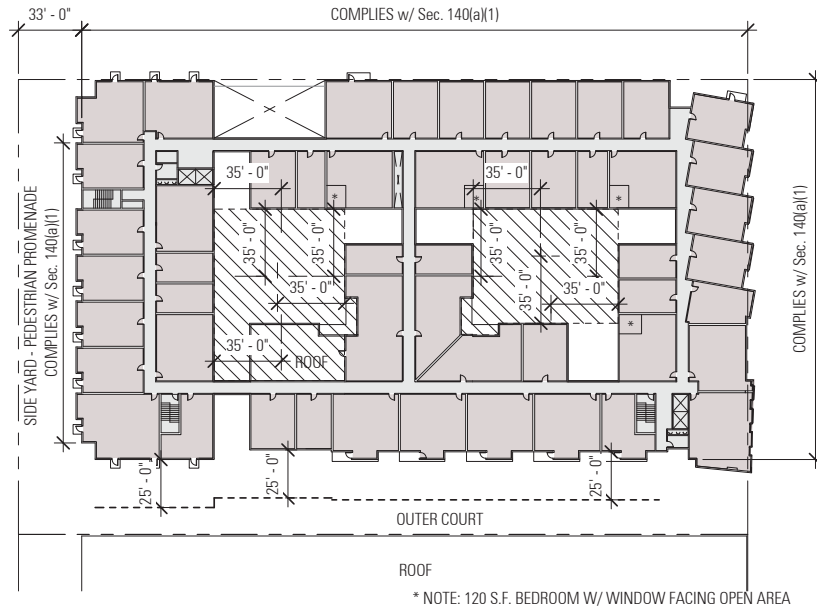
(a) ALL UNITS MUST HAVE A WINDOW IN AT LEAST ONE ROOM THAT MEETS THE 120-SF MINIMUM OF SUPERFICIAL FLOOR AREA REQUIREMENT OF SECTION 503 OF THE HOUSING CODE SHALL FACE DIRECTLY ON AN OPEN AREA OF ONE OF THE FOLLOWING TYPES:

(1) A public street, public alley at least 20 feet in width, side yard at least 25 feet in width, or rear yard meeting the requirements of this Code; provided, that if such windows are on an outer court whose width is less than 25 feet, the depth of such court shall be no greater than its width; or

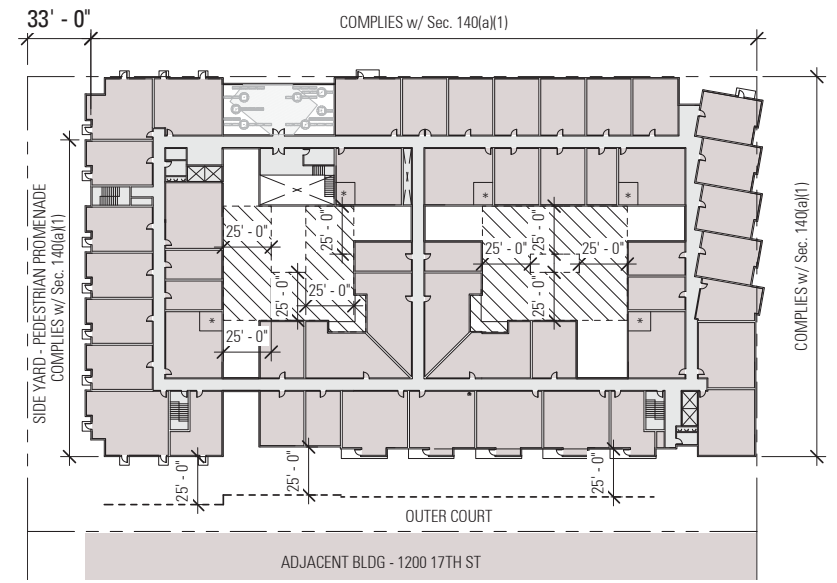
(2) An open area (whether an inner court or a space between separate buildings on the same lot) which is unobstructed and is no less than 25 feet in every horizontal dimension for the floor at which the Dwelling Unit in question is located and the floor immediately above it, with an increase of five feet in every horizontal dimension at each subsequent floor.



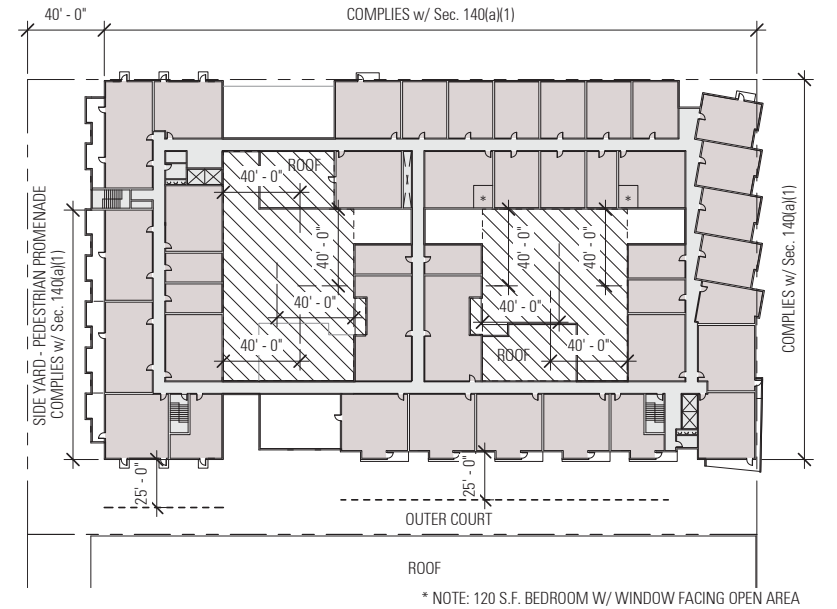
**UNIT EXPOSURE - LEVEL 2 PODIUM**  
**SCALE 1"=100'**



**UNIT EXPOSURE - LEVEL 5**  
**SCALE: 1" = 100'**



**UNIT EXPOSURE - LEVEL 3**  
**SCALE 1"=100'**



**UNIT EXPOSURE - LEVEL 6**  
**SCALE: 1" = 100'**

# 901 16th Street

## COMPLIANCE - UNIT EXPOSURE - Sec.140



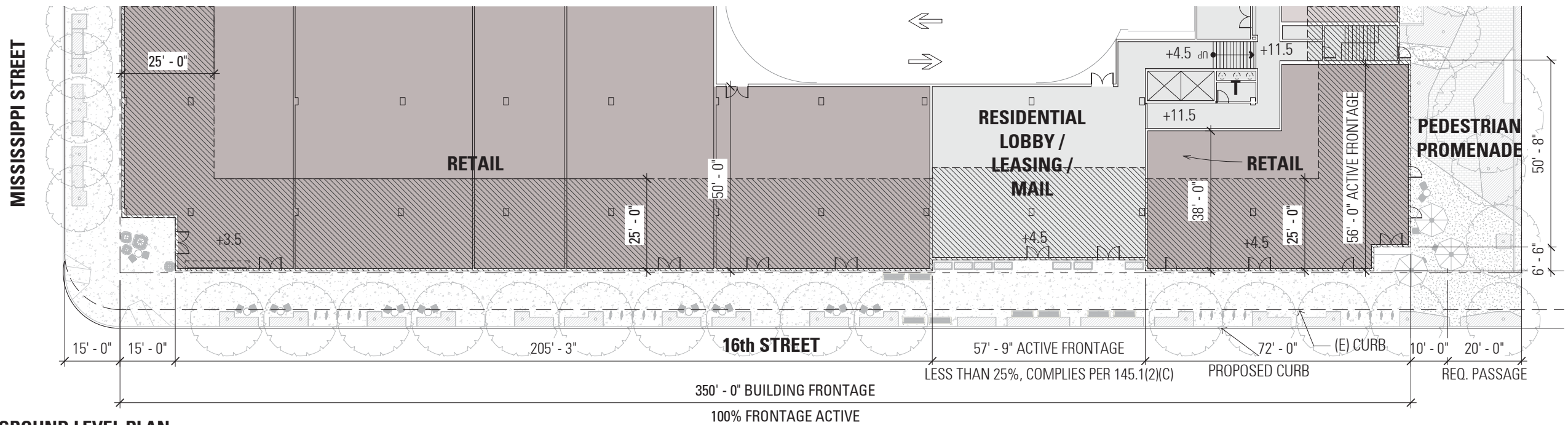


**GROUND FLOOR ACTIVE FRONTAGE**

Per 145.1(c)(3) With the exception of space allowed for parking and loading access, building egress, and access to mechanical systems, space for active uses as defined in Subsection (b)(2) shall be provided within the first 25 feet of building depth on the ground floor and 15 feet on floors above from any facade facing a street at least 30 feet in width.

**GROUND FLOOR NON-RESIDENTIAL USE CEILING HEIGHT**

Per 145.1(c)(4) Ground floor non-residential uses in UMU Districts shall have a minimum floor-to-floor height of 17 feet, as measured from grade.



**GROUND LEVEL PLAN**  
**SCALE: 1/32" = 1'-0"**

901 16th Street

**COMPLIANCE - 16TH ST ACTIVE FRONTAGE - Sec 145.1**



COPYRIGHT © 2013 BAR architects

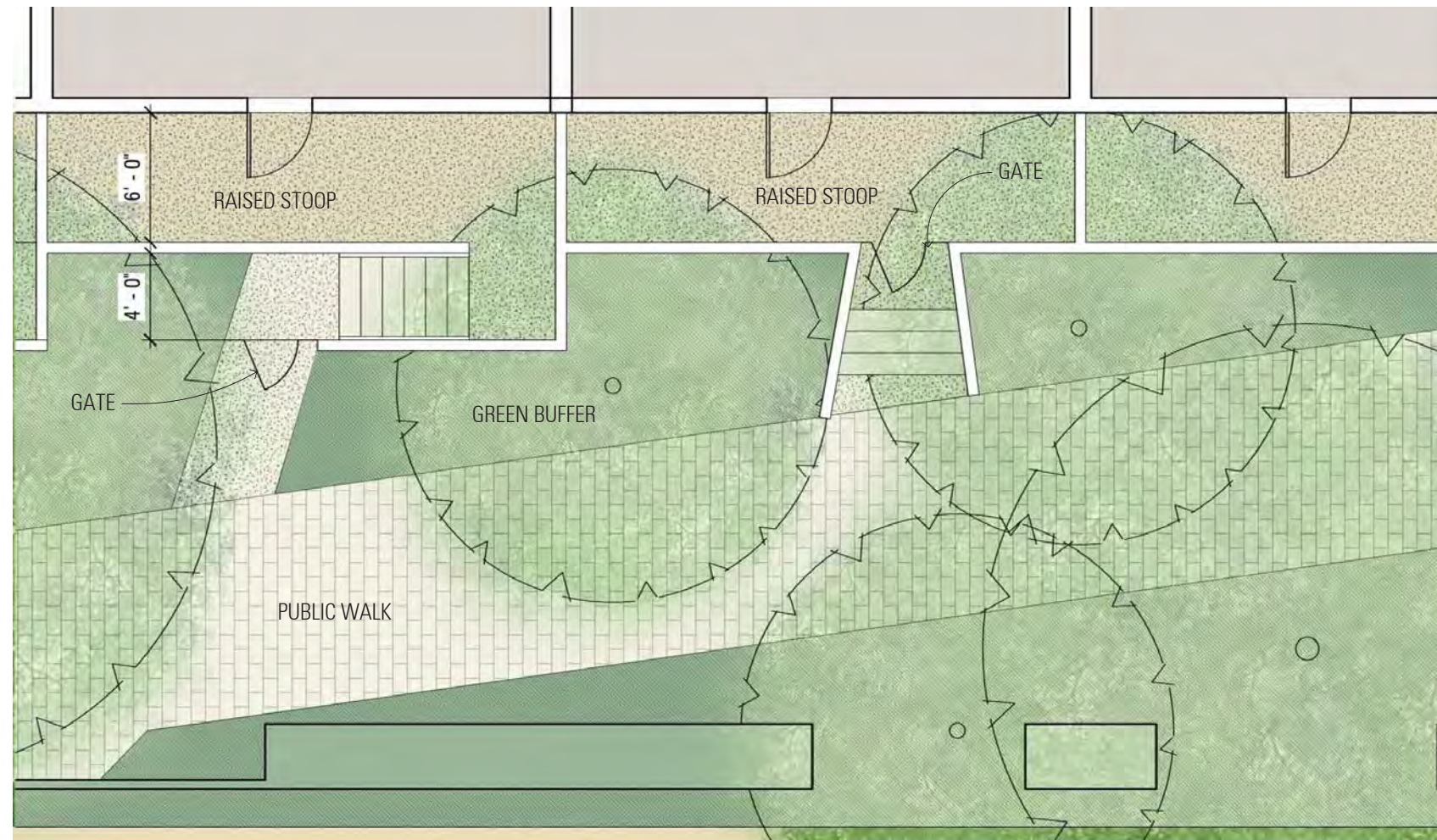
BAY WINDOW OVERHEAD  
PROVIDES COVER OVER  
FRONT DOOR

VISUALLY TRANSPARENT  
GLAZING, TYP.

METAL PANEL  
GATE



PEDESTRIAN PROMENADE ELEVATION - RAISED STOOPS



TYPICAL STOOP ENLARGED PLAN  
SCALE: 1/4" = 1'-0"

#### ACTIVE GROUND FLOOR:

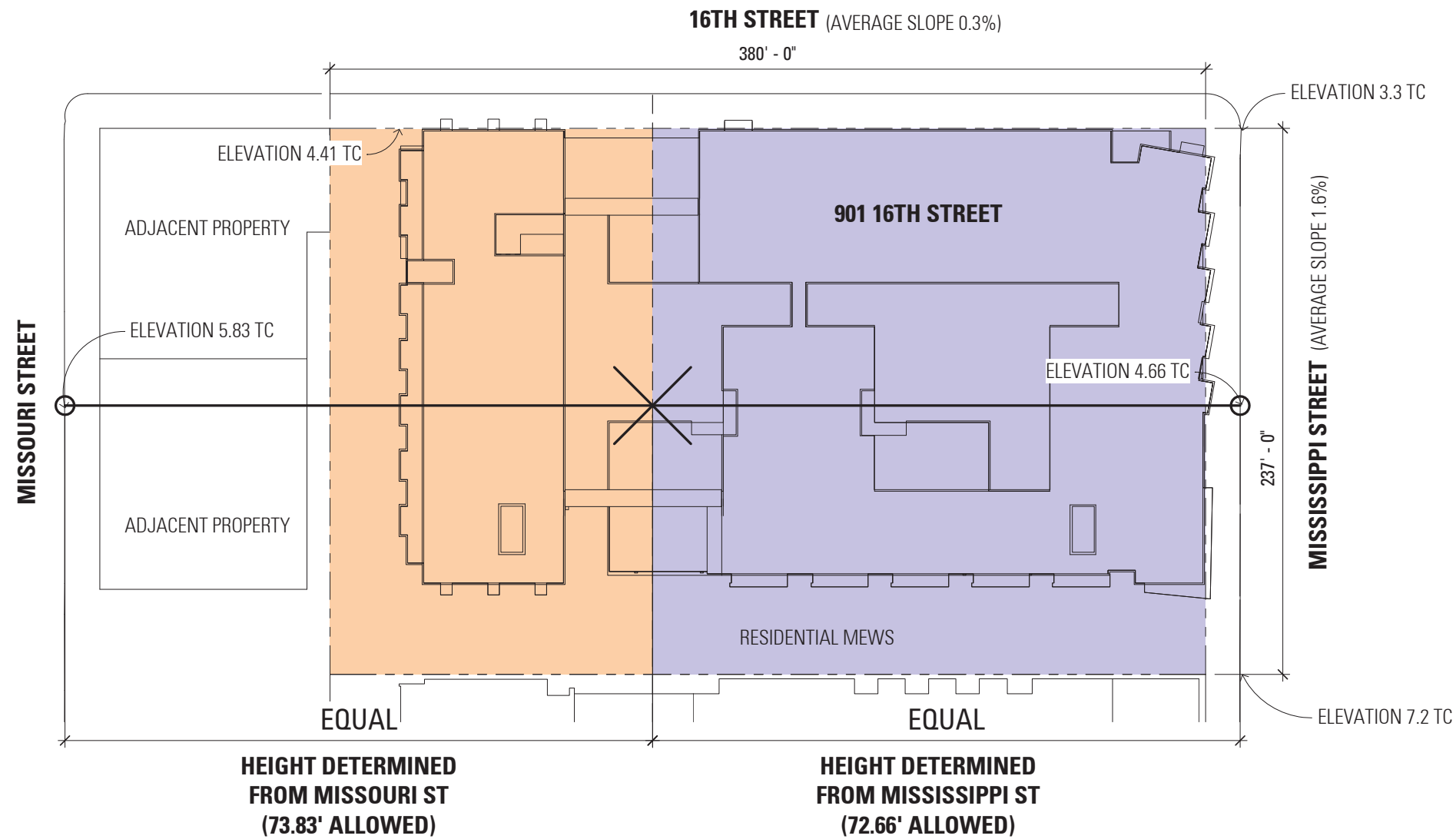
RESIDENTIAL UNITS EMPLOY THE GOALS AND STRATEGIES OF "GUIDELINES FOR GROUND FLOOR RESIDENTIAL DESIGN"

- A VARIED AND CHANGING PEDESTRIAN EXPERIENCE ALONG THE LENGTH OF THE BLOCK.
- AN EMPHASIS ON THE RECOGNIZABLE PRESENCE AND DELINEATION OF INDIVIDUAL DWELLING UNITS.
- ADEQUATE PRIVATE/PUBLIC TRANSITION SPACE FROM THE PUBLIC SIDEWALK TO THE GROUND FLOOR RESIDENTIAL UNITS
- FUNCTIONAL, INVITING AND SAFE STOOPS AND ENTRYWAYS.
- USABLE PRIVATE SPACE THAT ALSO ENCOURAGES PUBLIC INTERACTION AND SURVEILLANCE.
- A SOFTENING OF THE INTERFACE OF THE BUILDING AND SIDEWALK
- AN INCREASE IN GREENING AND THE AMOUNT OF PERMEABLE SURFACE IN THE PUBLIC REALM.

901 16th Street

COMPLIANCE - RESIDENTIAL STOOPS - Sec. 145.1





SITE PLAN

### BUILDING HEIGHT COMPLIANCE

#### SEC 260 (a)(1)(B):

Where the lot is level with or slopes downward from a street at the centerline of the building or building step, such point shall be taken at curb level on such a street. This point shall be used for height measurement only for a lot depth not extending beyond a line 100 feet from and parallel to such street, or beyond a line equidistant between such street and the street on the opposite side of the block, whichever depth is greater.

#### Required: SEC 260 (a)(1)(B):

68-X

Measured from the top of curb elevation at the midpoint of the building frontage, to the top of the low-slope roof. Where the lot has frontage on two or more streets, either street may be used for measurement. The height is to be measured to a line equidistant between such street and the street on the opposite side of the block.

- Parapets are permitted to extend 4-ft above height limit
- Elevator, stair penthouses, mechanical screens and penthouses, fire towers, and dormer windows may extend 16-ft above the allowable height provided that the height increase is limited to 20% of the roof area.

#### Provided:

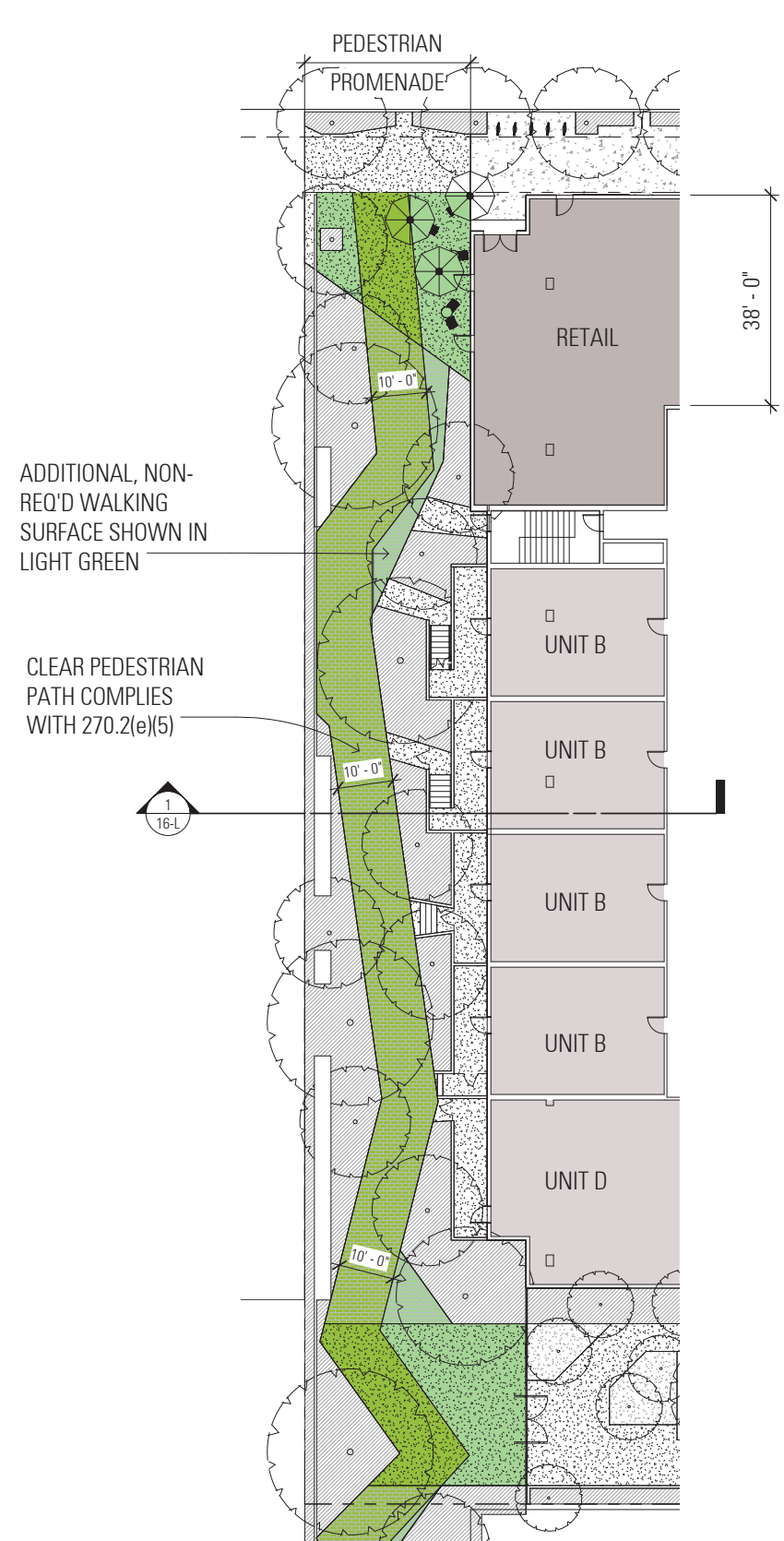
The project is measured at the midpoints of the Mississippi Street and Missouri Street frontages.

- Mississippi is measured from a datum of 4.66'
- Missouri is measured from a datum of 5.83'

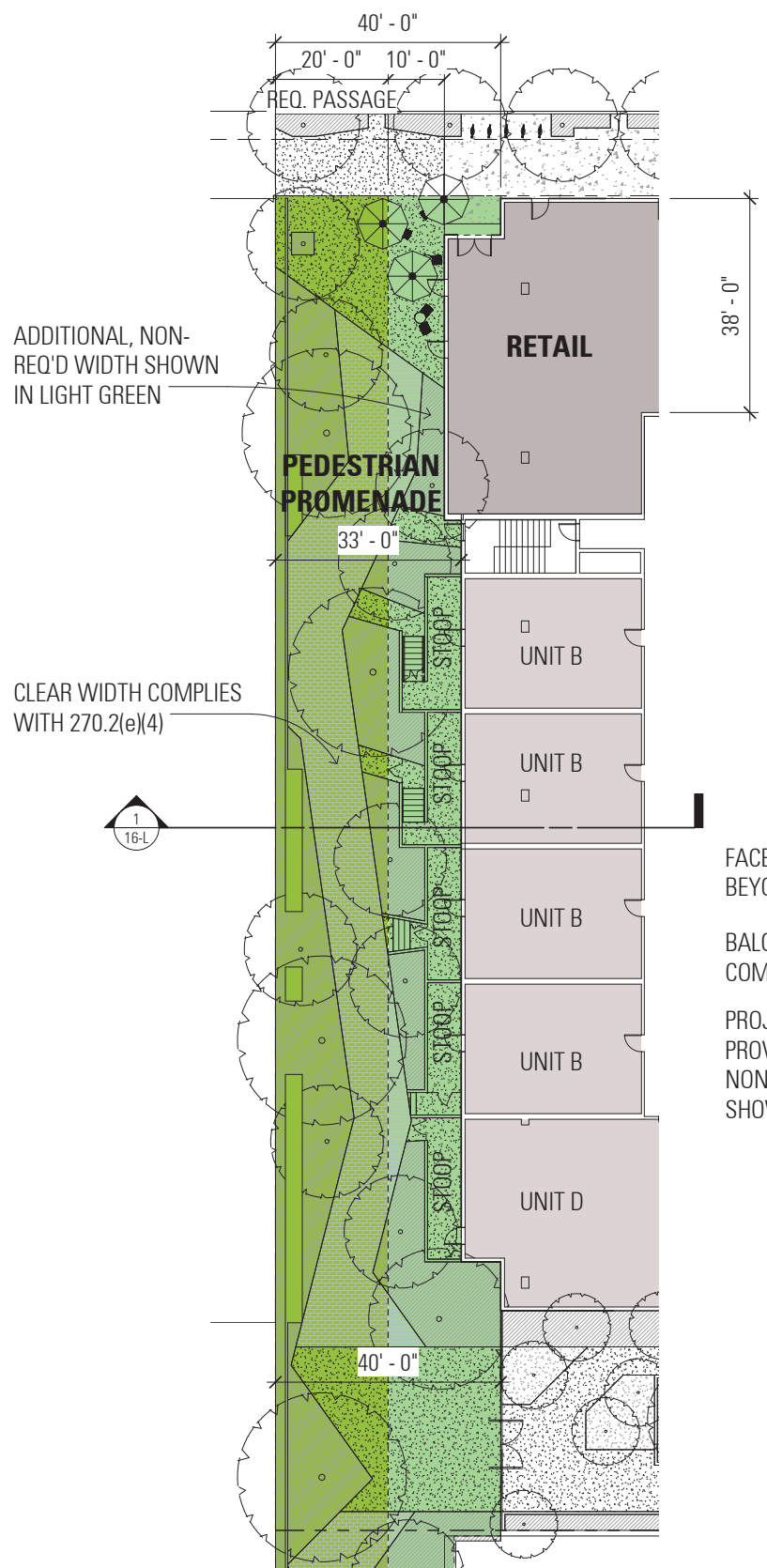
901 16th Street

COMPLIANCE - BUILDING HEIGHT DIAGRAM - Sec.260





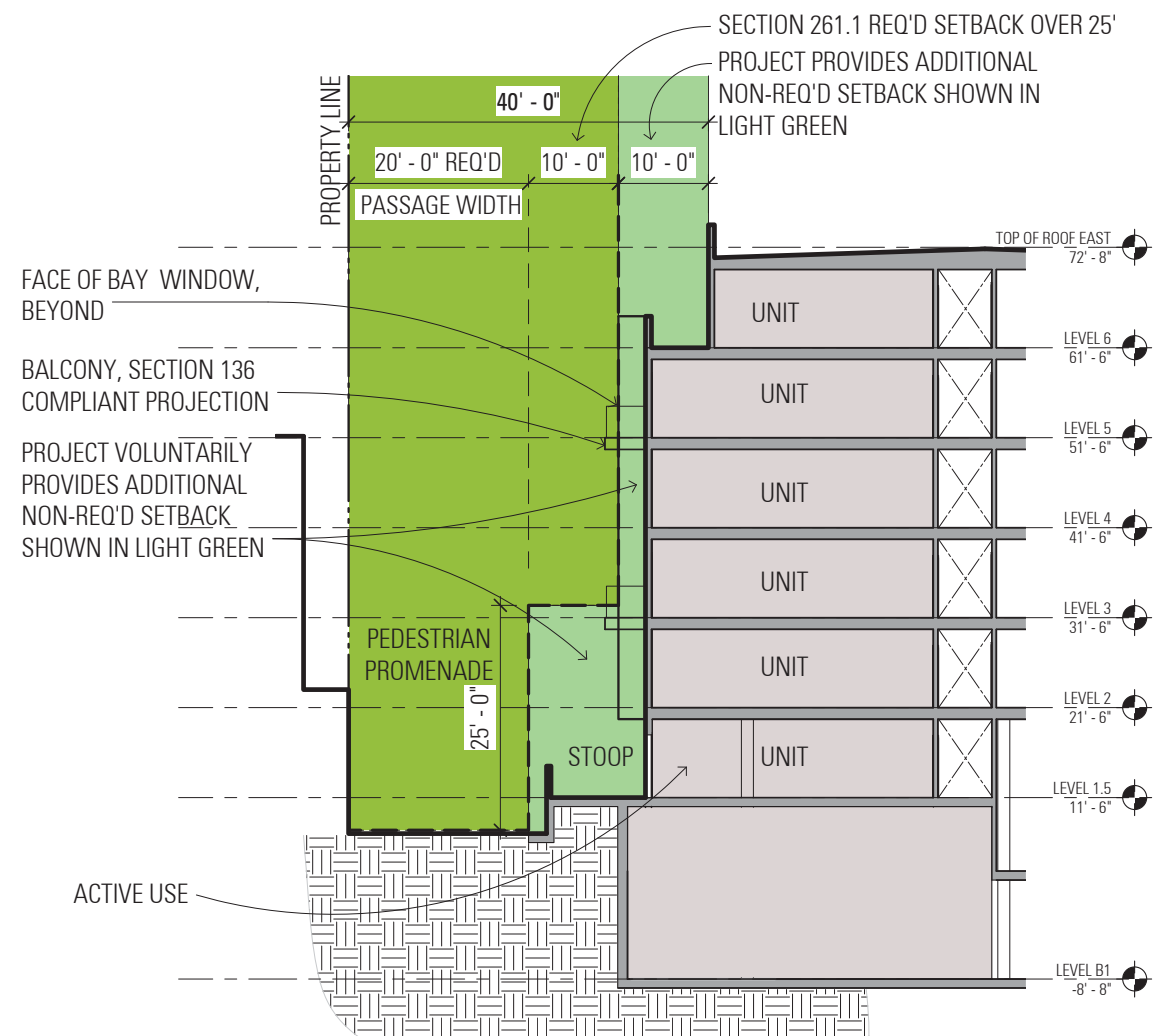
MID-BLOCK PASSAGE PLAN -  
REQ'D CLEAR WALKING SURFACE



MID-BLOCK PASSAGE PLAN -  
REQ'D WIDTH

**SPECIAL BUILT AND OPEN SPACE REQUIREMENT: MID-BLOCK BREAKS**

REQUIRED STANDARDS;  
270.2(e)(4): Minimum width of 20 feet from building face to building face and per  
261.1(d)(3)(A): 10-ft setback above 25-ft  
270.2(e)(5): Minimum clear walking width of 10 feet free of any obstructions.  
270.2(e)(13) Be fronted by active ground floor uses, as defined in Section 145.1, to the extent feasible.

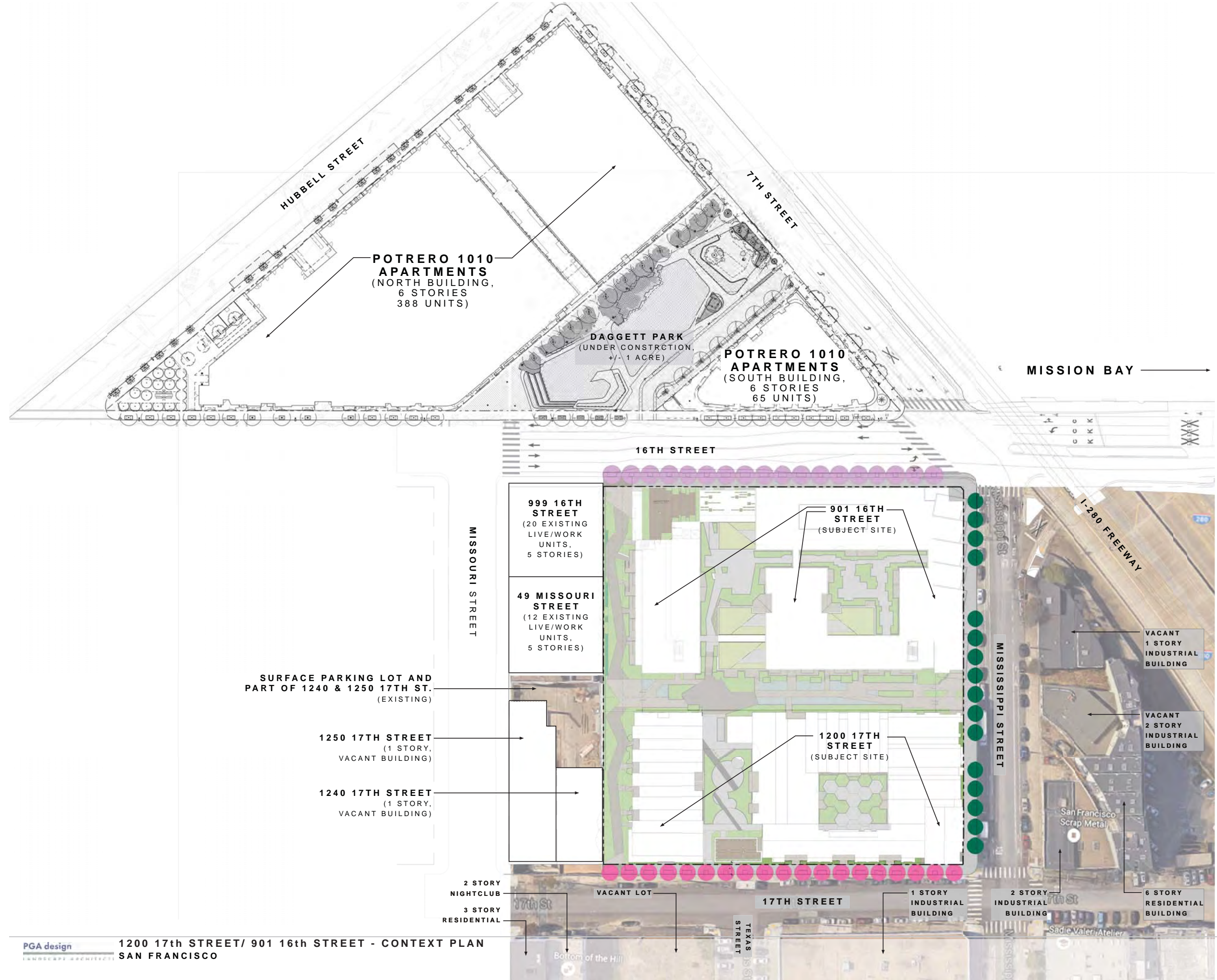


MID-BLOCK PASSAGE SECTION -  
REQ'D WIDTH

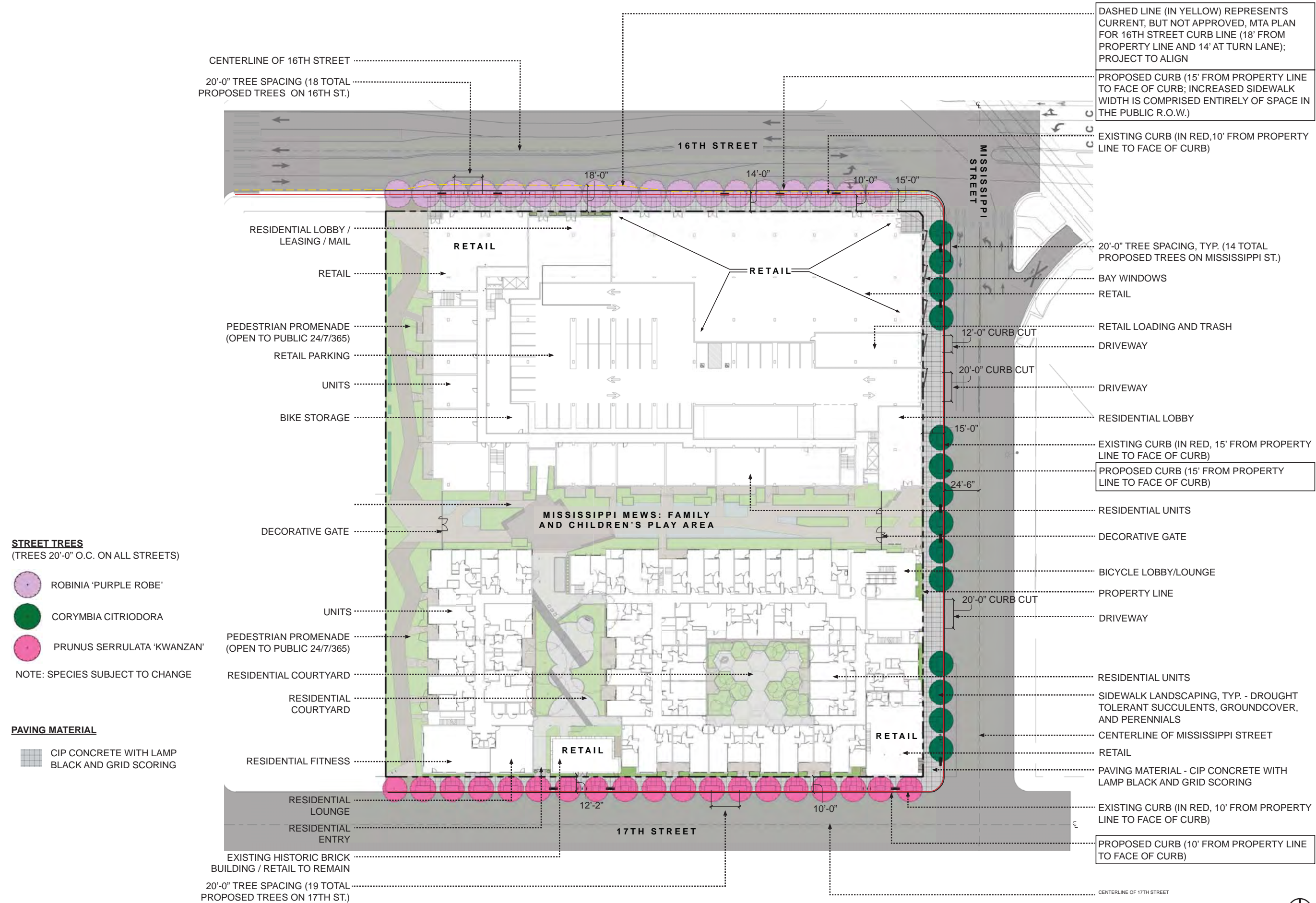
901 16th Street

**COMPLIANCE - MID BLOCK PASSAGE - Sec.270.2**









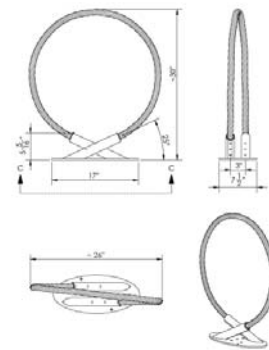


**SITE FURNISHINGS**

BIKE RACKS, TYP.

BENCH, TYP.

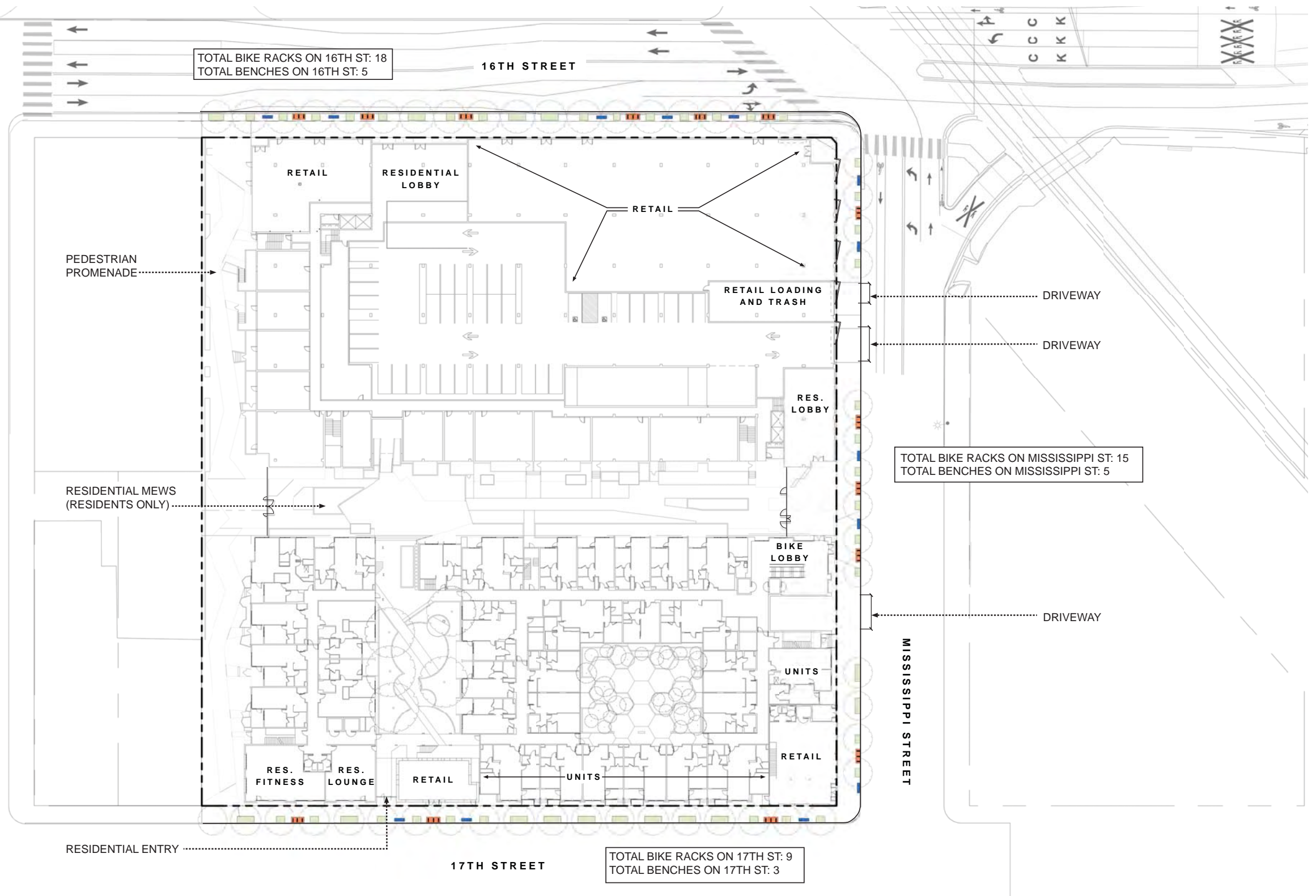
STREET TREE PLANTING AREA, TYP.



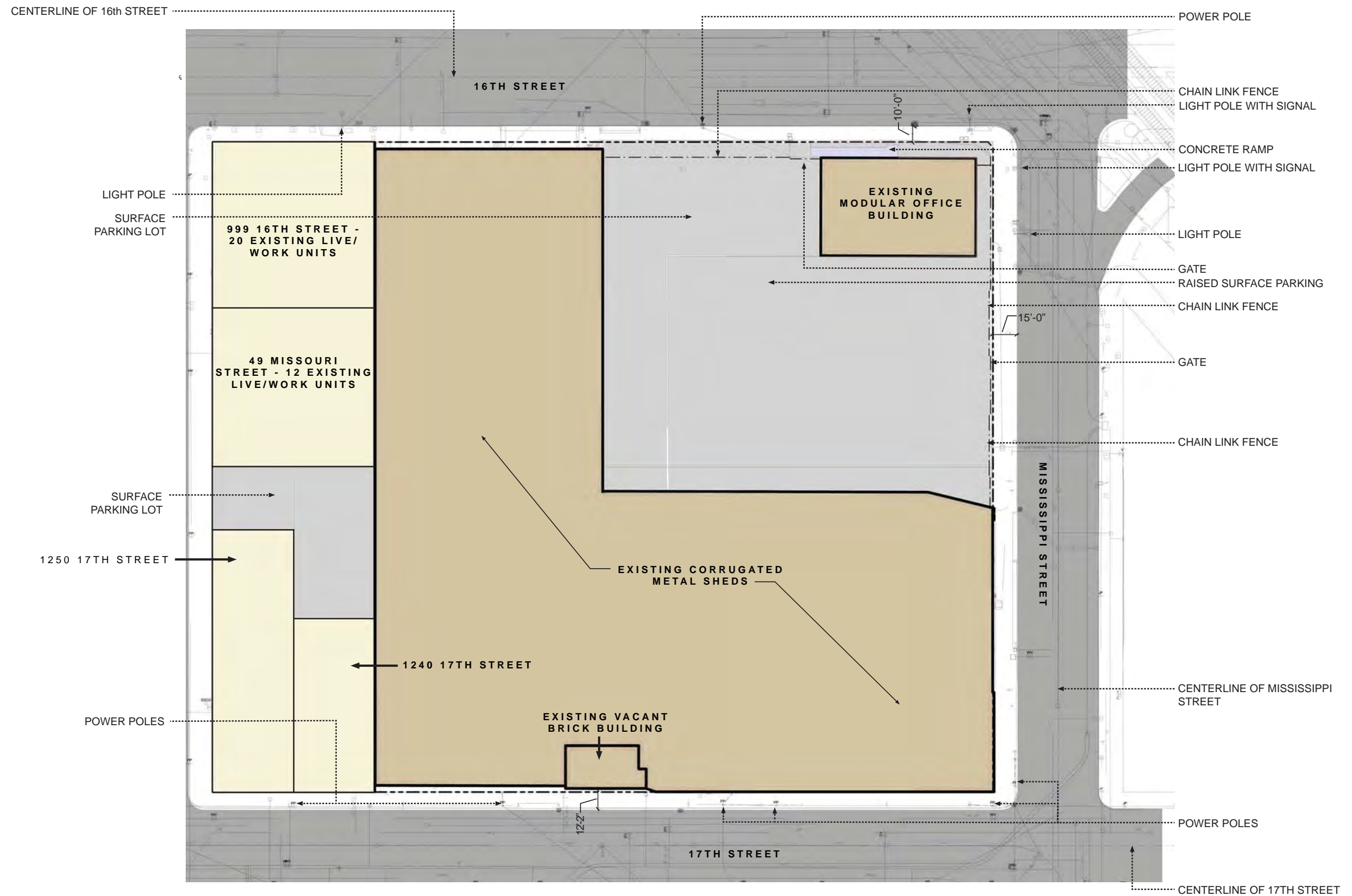
EXAMPLE OF BIKE RACK



EXAMPLE OF BENCH









# CONCEPTUAL DRAWINGS FOR PROPOSED PROJECT AT 901 16<sup>TH</sup> STREET & 1200 17<sup>TH</sup> STREET

MAY 12, 2016

ARCHITECT: OLLE LUNDBERG  
PROJECT MANAGER: MISA GRANNIS  
LUNDBERG DESIGN  
2620 3RD STREET  
SAN FRANCISCO, CA 94107



901 16th Street

ELEMENT REUSE CONCEPT - COVER SHEET





901 16th Street

ELEMENT REUSE CONCEPT - SITE PLAN





PUBLIC SEATING / FURNITURE  
ANCHORS TO TRUSS SYSTEM

REUSE CONCRETE FROM  
ORIGINAL CONSTRUCTION

MODELED TRUSS SIZES  
APPROXIMATE

CONCEPTUAL  
DRAWING

1 CONCEPTUAL EXAMPLE OF FURNITURE @ EXISTING TRUSS, LOOKING SOUTH THROUGH PEDESTRIAN PROMENADE

901 16th Street

ELEMENT REUSE CONCEPT - PEDESTRIAN PROMENADE





2 VIEW FROM 17TH STREET LOOKING NORTH THROUGH PEDESTRIAN PROMENADE  
WITH PROPOSED GATEWAY TRUSS ELEMENT



MODELED TRUSS SIZES  
APPROXIMATE

CONCEPTUAL  
DRAWING

901 16th Street

ELEMENT REUSE CONCEPT - PEDESTRIAN PROMENADE AT 17TH





MODELED TRUSS SIZES  
APPROXIMATE

CONCEPTUAL  
DRAWING



VIEW FROM 16TH STREET LOOKING SOUTH THROUGH PEDESTRIAN PROMENADE & CORNER RETAIL  
2 WITH PROPOSED GATEWAY TRUSS ELEMENT

901 16th Street

ELEMENT REUSE CONCEPT - PEDESTRIAN PROMENADE AT 16TH





MODELED  
TRUSS SIZE  
APPROXIMATE

3 6

VIEW FROM 16TH STREET LOOKING SOUTH AND WEST AT RETAIL ENTRY ON CORNER OF MISSISSIPPI  
AND 16TH STREETS WITH NEW TANK LIGHT MOUNTED ON REPURPOSED VERTICAL TRUSS ELEMENT

CONCEPTUAL  
DRAWING

901 16th Street

ELEMENT REUSE CONCEPT - RETAIL CORNER

BAR architects

901 Battery Street, Suite 300 | San Francisco, CA 94111 | 415 293 5700 | www.bararch.com

POTRERO PARTNERS LLC



13051

05/12/16

SCALE:

STUDY-5





MODELED TRUSS SIZE APPROXIMATE

3 6

VIEW FROM 16TH STREET LOOKING SOUTH AT RETAIL ENTRY ON CORNER OF MISSISSIPPI AND 16TH STREETS WITH NEW TANK LIGHT MOUNTED ON REPURPOSED VERTICAL TRUSS ELEMENT

901 16th Street

ELEMENT REUSE CONCEPT - RETAIL CORNER

**BAR**architects

901 Battery Street, Suite 300 | San Francisco, CA 94111 | 415 293 5700 | www.bararch.com

POTRERO PARTNERS LLC

PGA design INC  
LANDSCAPE ARCHITECTS

LD  
LUNDBERG DESIGN

13051

05/12/16

SCALE:

**STUDY-6**





3 6 VIEW FROM 16TH STREET LOOKING SOUTH AND WEST AT RETAIL ENTRY ON CORNER OF MISSISSIPPI AND 16TH STREETS WITH NEW TANK LIGHT MOUNTED ON REPURPOSED VERTICAL TRUSS ELEMENT

MODELED TRUSS SIZE APPROXIMATE

901 16th Street

ELEMENT REUSE CONCEPT - RETAIL CORNER





4

PROPOSED REUSE OF (E) TRUSSES AT EAST  
ENTRANCE TO FAMILY-FRIENDLY MEWS AREA



5

PROPOSED REUSE OF (E) TRUSSES AT WEST  
ENTRANCE TO FAMILY-FRIENDLY MEWS AREA

CONCEPTUAL  
DRAWINGS

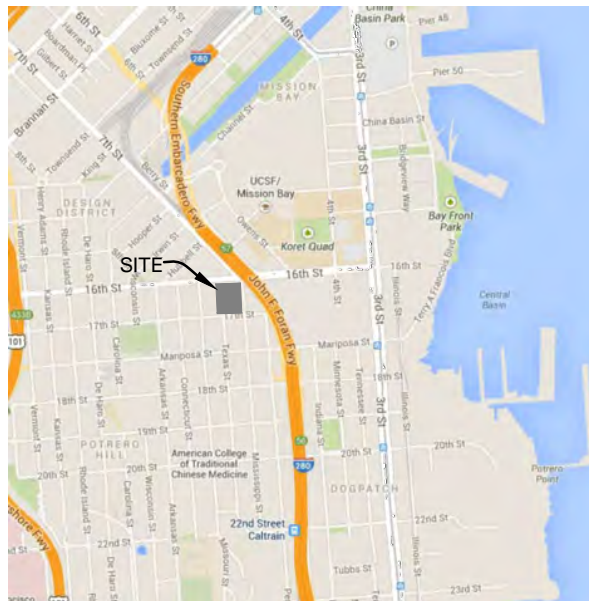
901 16th Street

ELEMENT REUSE CONCEPT - MISSISSIPPI MEWS ENTRY



# 1200 17TH STREET

# PLANNING COMMISSION HEARING - MAY 12, 2016



**SCOPE OF WORK:**

Request for Large Project Authorization (LPA) per Planning Code Sec. 329 and Adoption of CEQA Findings to demolish 105,000 sf of existing PDR warehouse and to construct:

**A North Building (901 16th St.)** - A 6-story, 68 foot tall, 260 dwelling unit, mixed-use building with 20,318 sf of retail and 263 parking spaces; and,  
**A South Building (1200 17th St.)** - A 4-story, 48 foot tall, 135 dwelling unit, mixed use building with 4,650 sf of retail and 125 parking spaces.

A pedestrian alley connection 16th Street and 17th Street would be constructed along the western property line.

**Under the LPA:** the project seeks modification to the following Planning Code Requirements:

- 1) Rear Yard (PC Sec 134);
- 2) Permitted Obstructions Over the Street (PC Sec 136);
- 3) Parking/Loading Entrance Width (PC Sec 145.1);
- 4) Off-Street Parking Maximum (PC Sec 151.1);
- 5) Off-Street Loading Minimum (PC Sec 152.1);
- 6) Horizontal Mass Reduction (PC Sec 270.1);
- 7) Accessory Units Provisions for Dwelling Units (PC Sec 329(d)(10)).

**COMBINED PROJECT DATA:**

| PARCEL              | 1200 17TH ST. | 901 16TH ST. | TOTALS     |
|---------------------|---------------|--------------|------------|
| Existing Warehouse  |               |              | 105,000 sf |
| Gross Site Area     | 61,940 sf     | 90,060 sf    | 152,000 sf |
| Gross Building Area | 213,009 sf    | 402,943 sf   | 615,952 sf |
| Retail Area         | 4,150 sf      | 20,318 sf    | 24,468 sf  |
| RESIDENTIAL UNITS:  |               |              |            |
| Studios             | 0             | 53           | 53 13%     |
| 1 Bedroom / 1 Bath  | 79            | 103          | 182 46%    |
| 2 Bedroom / 2 Bath  | 51            | 95           | 146 37%    |
| 3 Bedroom / 2 Bath  | 5             | 9            | 14 4%      |
| Total Units         | 135           | 260          | 395 100%   |
| PARKING:            |               |              |            |
| Residential Parking | 115           | 221          | 336 spaces |
| Retail Parking      | 8             | 39           | 47 spaces  |
| Car Share Spaces    | 2             | 3            | 5 spaces   |
| Total Parking       | 125           | 263          | 388 spaces |
| OPEN SPACE:         |               |              |            |
| Public Open Space   | 4,705 sf      | 7,514 sf     | 12,219 sf  |
| Common Open Space   | 8,602 sf      | 18,666 sf    | 27,268 sf  |
| Private Open Space  | 4,950 sf      | not included | 4,950 sf   |
| Total Open Space    | 18,257 sf     | 26,180 sf    | 44,437 sf  |

**CODE INFORMATION:**

|                   |   |
|-------------------|---|
| ZONING            | UMU - Urban Mixed Use<br>Eastern Neighborhoods Overlay District |
| HEIGHT DISTRICT   | 48 - X  |
| BLOCK/LOT         | 3949 / 002, 3950 / 001  |
| LOT SIZE          | 61,578 SF, 1.41 Acres   |
| OCCUPANCIES       | M - Retail<br>R2 - Residential<br>S2 - Parking Garage           |
| CONSTRUCTION TYPE | VA - 4 stories above grade<br>IA - 1 basement level below grade |

**1200 17TH STREET**  
**POTRERO PARTNERS, LLC**

## PROJECT DIRECTORY

OWNER / DEVELOPER:  
POTRERO PARTNERS, LLC  
Josh Smith  
445 Virginia Street  
San Mateo, CA 94402  
650.348.3232

ARCHITECT:  
CHRISTIANI JOHNSON ARCHITECTS  
Sherry Scott  
665 3rd Street, STE 350  
San Francisco, CA 94107  
415.243.9484

LANDSCAPE ARCHITECT

PGA DESIGN, INC.  
Cathy Garrett  
444 17th Street  
Oakland, CA 94612  
510.550.8852

CIVIL ENGINEER:

**BKF ENGINEERS**  
Eric Girod  
4670 Willow Road, STE 250  
Pleasanton, CA 94588  
925.396.7751

## DRAWING INDEX

|       |                                |
|-------|--------------------------------|
| G0.1  | COVER SHEET / PROJECT DATA     |
| G0.2  | PROJECT DATA                   |
| G0.3  | PLANNING CODE SUMMARY          |
| G0.4  | EXISTING SITE PHOTOS           |
| G1.1  | STREETVIEW - 17TH STREET       |
| G1.2  | STREETVIEW - MISSISSIPPI ST.   |
| G1.3  | STREETVIEW - RETAIL CORNER     |
| G1.4  | STREETVIEW - PROMENADE         |
| G1.5  | STREETVIEW - BRICK BLDG NW     |
| G1.6  | STREETVIEW - BRICK BLDG NE     |
| G1.7  | STREETVIEW - RESIDENTIAL ENTRY |
| G1.8  | MATERIALS                      |
| C1.0  | EXISTING SITE SURVEY           |
| L0.0  | OVERALL SITE PLAN              |
| L0.1  | PUBLIC / SHARED SPACES         |
| L0.2  | MISSISSIPPI MEWS               |
| L0.3  | PUBLIC / SHARED IMAGES         |
| L17.1 | 17TH ST. COURTYARD & ROOF DECK |
| L17.2 | 17TH ST. SECTIONS AND IMAGES   |

## PLANNING CODE DIAGRAM ADDENDUM

- 17A MODIFICATION 1 : REAR YARD
- 17B MODIFICATION 2 : OFF-STREET PARKING
- 17C MODIFICATION 3 : OFF-STREET LOADING
- 17D MODIFICATION 4 : FLEXIBLE UNITS
- 17E COMPLIANCE : OPEN SPACE
- 17F COMPLIANCE : UNIT EXPOSURE
- 17G COMPLIANCE : BUILDING HEIGHT
- 17H COMPLIANCE : HORIZONTAL MASS REDUCTION
- 17I COMPLIANCE : MID BLOCK PASSAGE
- 17J COMPLIANCE : ACTIVE FRONTAGE
- 17K COMPLIANCE : ACTIVE FRONTAGE
- 17L COMPLIANCE : STOOPS
- 17M COMPLIANCE : BRICK BUILDING PHOTOS
- 17N COMPLIANCE : BRICK BUILDING SETBACKS
- 17O COMPLIANCE : BRICK BUILDING SETBACKS PLAN

BETTER STREETS COMPLIANCE

- L1 CONTEXT PLAN
- L2 STREET SCAPE
- L3 SITE FURNISHINGS
- L4 EXISTING CONDITIONS

|        |                                    |
|--------|------------------------------------|
| A 1.00 | EXISTING SITE PLAN                 |
| A1.01  | ARCHITECTURAL SITE PLAN (PROPOSED) |
| A2.01  | FLOOR PLAN - BASEMENT LEVEL GARAGE |
| A2.02  | FLOOR PLAN - GROUND LEVEL 1        |
| A2.03  | FLOOR PLAN - LEVEL 2               |
| A2.04  | FLOOR PLAN - LEVELS 3-4            |
| A2.05  | ROOF PLAN                          |
| A3.01  | ELEVATIONS                         |
| A3.02  | ELEVATIONS                         |
| A3.03  | BUILDING SECTIONS                  |
| A4.01  | UNIT PLANS                         |
| A4.02  | UNIT PLANS                         |
| A4.03  | UNIT PLANS                         |

**CHRISTIANI JOHNSON ARCHITECTS, INC.**  
665 Third Street, Suite 350  
San Francisco, CA 94107  
415.243.9484  
[cjarchs.com](http://cjarchs.com)

|             |            |
|-------------|------------|
| Scale       | Date       |
| NTS         | 05.12.2016 |
| Sheet Title |            |
| COVER SHEET |            |

# G0.1



OFF STREET PARKING CALCULATIONS:

MAXIMUM RESIDENTIAL PARKING PERMITTED PER SECTION 151.1:

135 DWELLING UNITS X .75 = 101 SPACES PERMITTED

MAXIMUM RETAIL PARKING PERMITTED PER SECTION 151.1:

1 SPACE PER 500 SF RETAIL

4,150 SF / 500 SF = 8 SPACES

PARKING PROVIDED AS SHOWN ON A2.01:

|                                |            |
|--------------------------------|------------|
| RESIDENTIAL PARKING PROVIDED   | 115 SPACES |
| CAR SHARE SPACES PROVIDED      | 2 SPACES   |
| RETAIL PARKING SPACES PROVIDED | 8 SPACES   |
| TOTAL PARKING PROVIDED         | 125 SPACES |

| Unit Type | L1 | L2 | L3 | L4 | Totals |       |                                  |
|-----------|----|----|----|----|--------|-------|----------------------------------|
| 1A        | 7  | 7  | 3  | 3  | 20     | 79    | 59 % 1 bedroom / 1 bath          |
| 1A FLEX   | 4  |    |    |    | 4      |       |                                  |
| 1A.1      |    | 4  | 4  | 4  | 12     |       |                                  |
| 1A.2      |    |    | 4  | 4  | 8      |       |                                  |
| 1B        | 6  | 6  | 6  | 6  | 24     |       |                                  |
| 1B.1      |    | 1  | 1  | 1  | 3      |       |                                  |
| 1C        |    |    | 2  | 2  | 4      |       |                                  |
| 1D        | 1  | 1  | 1  | 1  | 4      |       |                                  |
| 2A.1      |    | 2  |    |    | 2      | 51    | 41% 2 bedroom / 2 bath or larger |
| 2A FLEX   | 2  |    |    |    | 2      |       |                                  |
| 2A.2      |    |    | 2  | 2  | 4      |       |                                  |
| 2A.3      | 3  | 3  | 3  | 3  | 12     |       |                                  |
| 2A.4      | 1  | 1  | 1  | 1  | 4      |       |                                  |
| 2B        | 2  | 2  | 2  | 2  | 8      |       |                                  |
| 2C        |    |    | 2  | 2  | 4      |       |                                  |
| 2D        | 2  | 2  | 2  | 2  | 8      |       |                                  |
| 2E        | 1  | 2  | 2  | 2  | 7      |       |                                  |
| 3A        | 1  | 1  | 1  | 1  | 4      | 5     |                                  |
| 3B -TH    | 1  |    |    |    | 1      |       |                                  |
|           | 31 | 32 | 36 | 36 | 135    | Units |                                  |

BICYCLE PARKING:

BICYCLE PARKING REQUIRED PER TABLE 155.2:

One Class I bicycle parking space is required for every dwelling unit up to 100 units and for every 4 units over 100.

100 UNITS = 100 spaces required

35 units / 4 = 9 spaces required

TOTAL CLASS I = 109 spaces required

191 CLASS 1 BICYCLE PARKING SPACES PROVIDED (1 per bedroom)

One Class 2 bicycle parking space is required for every 2,500 sf of retail space and one per 20 residential units.

4,150 sf retail / 2,500 sf = 2 spaces required

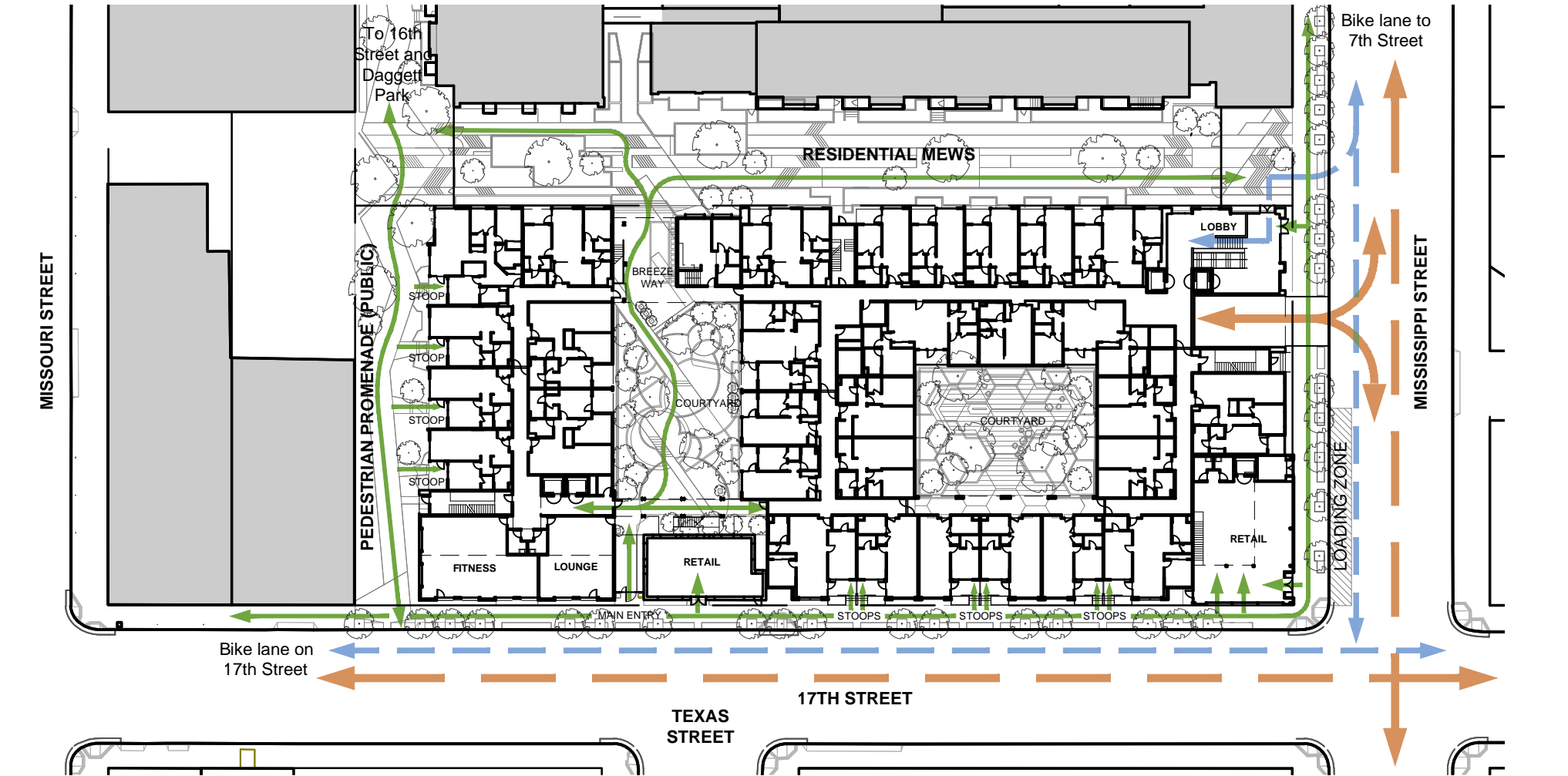
135 units / 20 = 7 spaces required

TOTAL CLASS 2 = 9 spaces required

12 CLASS 2 BICYCLE PARKING SPACES PROVIDED



GROUND LEVEL CIRCULATION DIAGRAM:



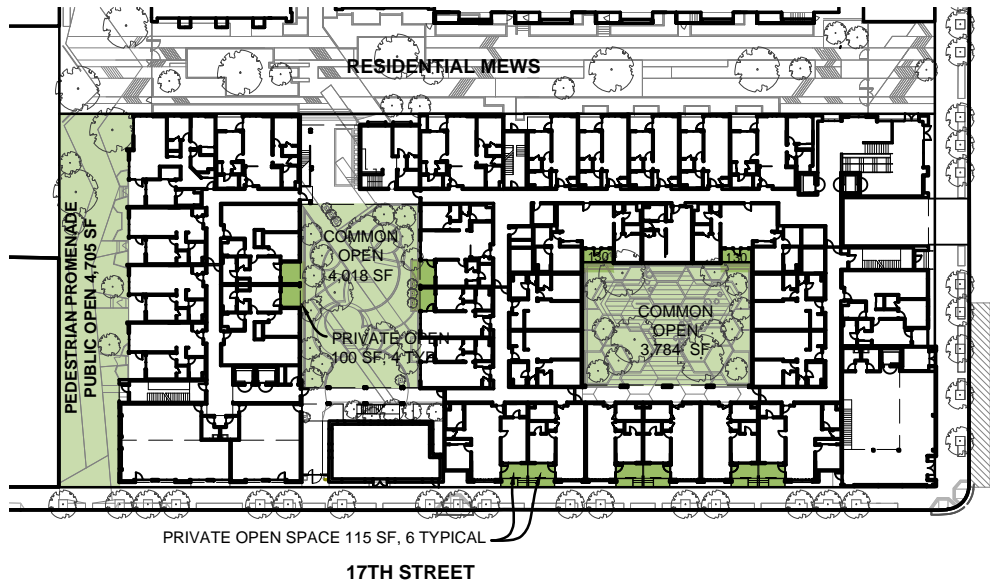
OPEN SPACE CALCULATIONS:

Open Space Required Per Table 843 80 sf per unit  
(54 sf if publicly accessible)  
Open Space Required = 135 units x 80 sf = 10,800sf  
18,257 sf provided / 10,800 sf required = **169% REQUIRED OPEN SPACE PROVIDED**

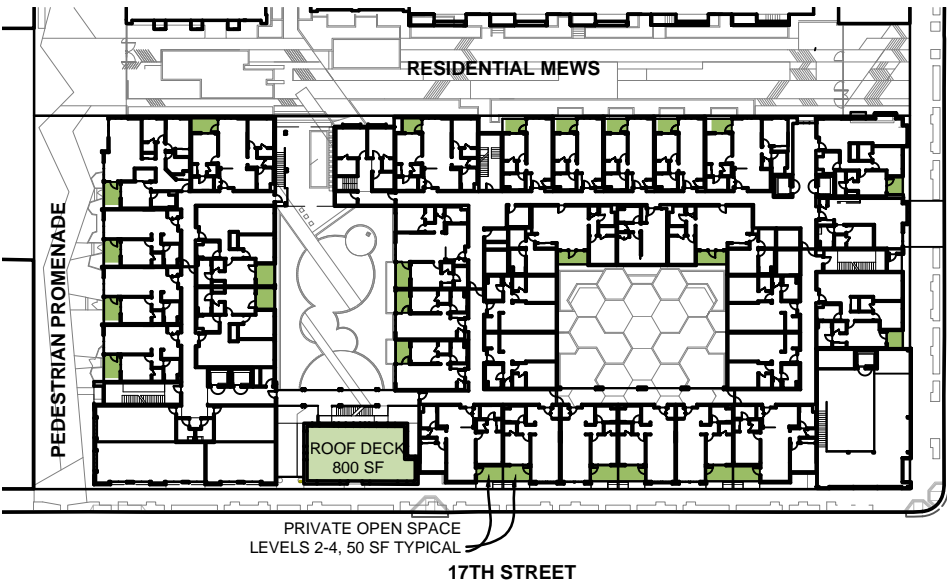
OPEN SPACE PROVIDED:

|   |           |
|---|-----------|
| Pedestrian Promenade: Public Open Space | 4,705 sf  |
| Private Patios @ Ground                 | 1,350 sf  |
| Private Decks Levels 2-4                | 3,600 sf  |
| East Courtyard                          | 3,784 sf  |
| West Courtyard                          | 4,018 sf  |
| Roof Deck Level 3                       | 800 sf    |
| Total Open Space Provided               | 18,257 sf |

- Pedestrian Circulation
- Bicycle Circulation
- Vehicle Circulation



OPEN SPACE DIAGRAM - GROUND LEVEL



OPEN SPACE DIAGRAM - LEVELS 2-4

REQUESTED LARGE PROJECT AUTHORIZATION MODIFICATIONS:

- REAR YARD MODIFICATION PURSUANT TO THE REQUIREMENTS OF SECTION 134 (f).  
The project requests a modification to the rear yard setback. A 25% rear yard on the 161'-9" deep lot would be 40'-5" with an area of 15,366 sf. Open space in excess of the rear yard requirement is provided in a combination of public open space along the pedestrian promenade, two residential courtyards, private decks and balconies, and a common roof deck. The project provides 169% of the open space required by Table 843.11.
- OFF STREET PARKING MODIFICATION PER SECTION 151.1.  
The project requests a modification to the off street parking requirements in section 151.1. The project proposes to provide 1 standard space for units with 2 bedrooms and larger and at least 1,000 sf in the underground garage. Several factors contribute to the desire 1 space for these units: Street parking will be eliminated on 16th Street to accommodate the BRT and parking on 17th street will be eliminated to accommodate a bicycle lane. In addition, the 2 and 3 bedroom units are designed with families in mind, and as such may require 1 car per unit. And finally, the configuration of the below-ground parking garage is within the building footprint and does not require additional excavation into landscaped areas. Parking stackers would require additional excavation depth and are not desired in this site with hazardous soil conditions.
- OFF STREET LOADING MODIFICATION PER SECTION 152.1.  
The project requests a modification to the off street loading requirements for 1 loading space in residential projects between 100,000 sf and 200,000 sf in size. We propose to accommodate loading on Mississippi Street with a yellow curb for loading and unloading. The project and streetscape along Mississippi will benefit by eliminating the additional curb-cut that an off street loading space would require and allows more of the building frontage to be dedicated to active retail, residential, and lobby uses.
- FLEXIBLE UNIT MODIFICATION PER SECTION 329(d)(10).  
The project requests a modification to provide 6 flexible units at the ground level along 17th Street. Flexible units will allow commercial and/or residential uses. These units will have accessible access through the residential building and will have direct entrances to 17th street to support either residential or commercial uses. 17th Street is a 66' right of way and exceeds the minimum 40' right of way width required for requesting this modification.

PLANNING CODE SUMMARY:

PROJECT COMPLIES WITH THE FOLLOWING PLANNING CODE PROVISIONS

|                   |   |
|-------------------|---|
| Zoning:           | UMU (Urban Mixed Use), Eastern Neighborhoods Overlay  |
| Height District   | 48-X  |
| Building Height:  | Section 102.12(b) Measured from the top of curb, center of building on 17th. The height of the reference point is +11.24' per Civil topo. Height is measured to the top of a flat roof or the average height of a sloped roof. Parapets may extend 4' above height limit. Building height is within limits, refer to elevations for more information.                                 |
| Bulk Limitations: | Section 270.1: a horizontal mass reduction of at least 30' in width and 60' in depth shall be provided for every building more than 200' long (resulting building facades should be less than 200'). Block break should begin not higher than 25' above grade and shall extend to the sky. The required break is provided at the existing brick building to remain along 17th Street. |
| Mid-Block Passage | Section 270.2: a mid-block passage is required to break down large blocks. Shall be min 20' wide with a 10' pedestrian walking surface. Must be ungated and be publicly assessable 24 hours. The passage is provided as the pedestrian promenade connecting 17th and 16th Streets to Daggett Park.  |
| Alley Setback:    | Section 261.1 #3 Mid Block Passages shall have a setback of 10' above 25' Our passage is required to be 20' so building height above 25' must be at least 30' wide. Refer to building sections for more information.  |
| Allowable Use:    | Table 843-Residential permitted, 40% must be 2 bed or larger and at least 1,000 sf. Retail permitted up to 25,000 per lot. Unit mix complies 41% 2 bedroom or larger provided and less than 25,000 sf of combined retail is provided.   |
| Parking:          | Per Section 151.1 parking is not required for any use. Maximum parking allowances are as follows: .75 cars for each dwelling unit, 1 car for every 2 bedroom 1,000 sf or larger. Retail parking is allowed 1 car for every 500 sf floor area up to 20,000 sf. Residential parking over .75 per DU requires an LPA modification.   |
| Car Share:        | 1 car share space minimum is required per Table 166. Car share spaces shall not count against the maximum allowed. 1 car share space is provided (2 spaces provided in alternate garage plan).  |
| Open Space:       | Per Table 843.11, 80 sf of open space is required per unit or 54 sf per unit if the open space is publicly accessible. One sf of open space is required per 250 sf of retail area. Table 135.3 describes size requirements for private and common open space. Project complies with open space requirements. Refer to adjacent open space diagrams for more information.              |





1 - NORTH SIDE OF BRICK BUILDING TO BE RESTORED



2 - SOUTH SIDE OF BRICK BUILDING TO BE RESTORED



3 - VIEW OF 17TH STREET LOOKING EAST



4 - VIEW OF MISSISSIPPI SIDEWALK LOOKING NORTH



5 - VIEW OF MISSISSIPPI STREET LOOKING NORTH



6 - VIEW OF 17TH STREET LOOKING WEST



CHRISTIANI JOHNSON ARCHITECTS, INC.  
665 Third Street, Suite 350  
San Francisco, CA 94107  
415.243.9484  
cjarchs.com

# 1200 17TH STREET

POTRERO PARTNERS, LLC

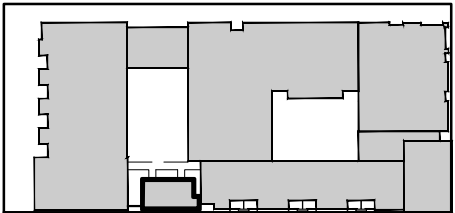
|             |            |
|-------------|------------|
| Scale       | Date       |
| NTS         | 05.12.2016 |
| Sheet Title |            |
| EXISTING    |            |
| SITE PHOTOS |            |

G0.4





KEY PLAN



17 STREET

MISSISSIPPI ST.



CHRISTIANI JOHNSON ARCHITECTS, INC.  
665 Third Street, Suite 350  
San Francisco, CA 94107  
415.243.9484  
cjarchs.com

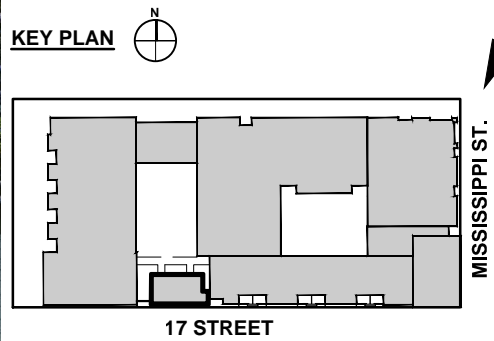
# 1200 17TH STREET

POTRERO PARTNERS, LLC

|                           |            |
|---------------------------|------------|
| Scale                     | Date       |
| NTS                       | 05.12.2016 |
| Sheet Title               |            |
| STREETVIEW<br>17TH STREET |            |

G1.1





# 1200 17TH STREET POTRERO PARTNERS, LLC

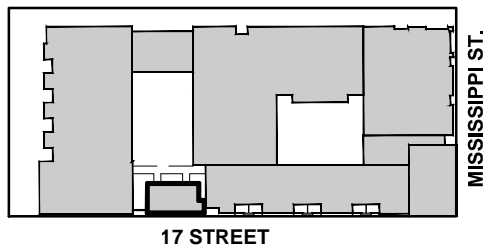
|             |                               |      |            |
|-------------|-------------------------------|------|------------|
| Scale       | NTS                           | Date | 05.12.2016 |
| Sheet Title | STREETVIEW<br>MISSISSIPPI ST. |      |            |

G1.2





KEY PLAN 



# 1200 17TH STREET

POTRERO PARTNERS, LLC

Scale NTS Date 05.12.2016  
Sheet Title STREETVIEW  
RETAIL @ 17TH ST.

G1.3

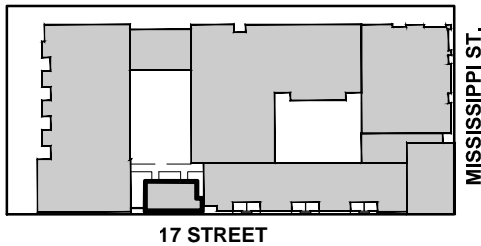
CHRISTIANI JOHNSON ARCHITECTS, INC.  
665 Third Street, Suite 350  
San Francisco, CA 94107  
415.243.9484  
cjarchs.com







KEY PLAN



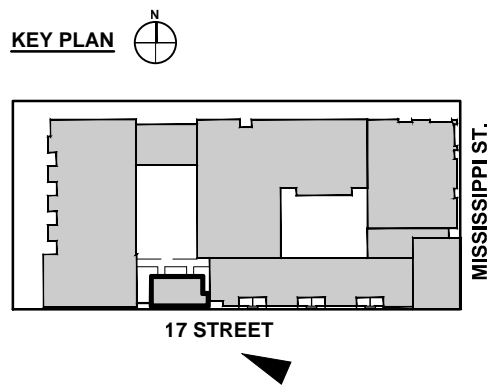
CHRISTIANI JOHNSON ARCHITECTS, INC.  
665 Third Street, Suite 350  
San Francisco, CA 94107  
415.243.9484  
cjarchs.com

1200 17TH STREET  
POTRERO PARTNERS, LLC

|                |            |
|----------------|------------|
| Scale          | Date       |
| NTS            | 05.12.2016 |
| Sheet Title    |            |
| STREETVIEW     |            |
| PED. PROMENADE |            |

G1.4





# 1200 17TH STREET

POTRERO PARTNERS, LLC

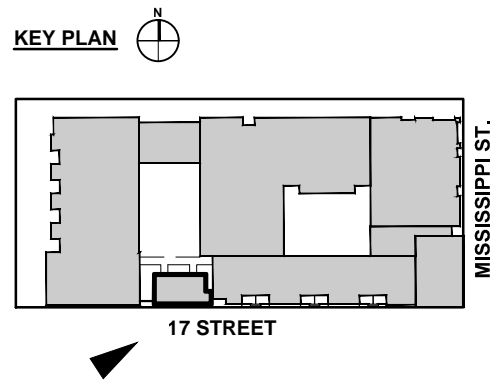
|                   |            |
|-------------------|------------|
| Scale             | Date       |
| NTS               | 05.12.2016 |
| Sheet Title       |            |
| STREETVIEW        |            |
| BRICK BUILDING NW |            |

G1.5

CHRISTIANI JOHNSON ARCHITECTS, INC.  
665 Third Street, Suite 350  
San Francisco, CA 94107  
415.243.9484  
cjarchs.com







CHRISTIANI JOHNSON ARCHITECTS, INC.  
 665 Third Street, Suite 350  
 San Francisco, CA 94107  
 415.243.9484  
 cjarchs.com

# 1200 17TH STREET

POTRERO PARTNERS, LLC

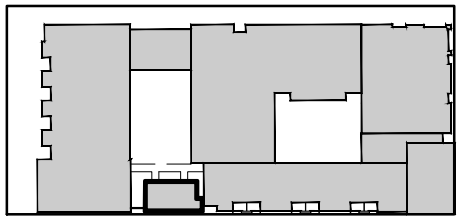
|                |            |
|----------------|------------|
| Scale          | Date       |
| NTS            | 05.12.2016 |
| Sheet Title    |            |
| STREETVIEW     |            |
| BRICK BLDG. NE |            |

G1.6





KEY PLAN 



# 1200 17TH STREET

POTRERO PARTNERS, LLC

|                   |            |
|-------------------|------------|
| Scale             | Date       |
| NTS               | 05.12.2016 |
| Sheet Title       |            |
| STREETVIEW        |            |
| RESIDENTIAL ENTRY |            |

G1.7

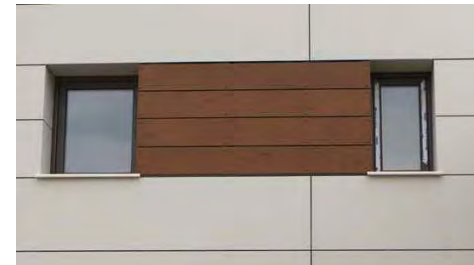




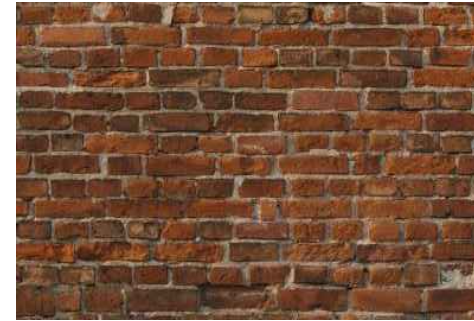
CORRUGATED METAL SIDING  
RETAIL BUILDING AT 17TH AND MISSISSIPPI STREETS



GREEN WALLS  
17TH ST. AMENITY SPACES



HORIZONTAL WINDOW GROUPING



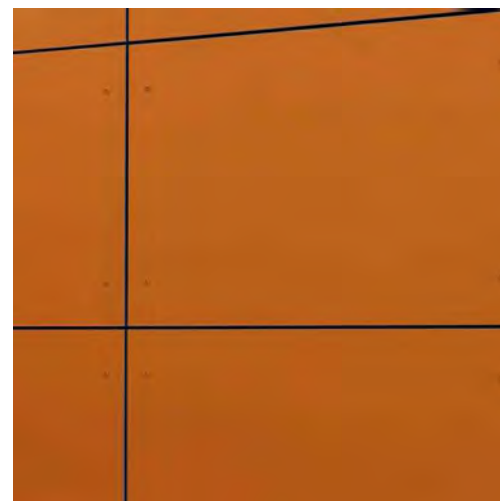
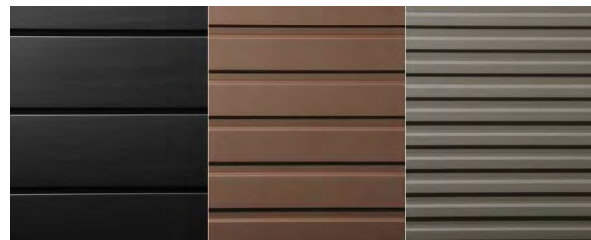
RESTORED BRICK AT HISTORIC RESOURCE



BOARD FORMED CONCRETE  
AT FIRST TWO LEVELS ALONG 17TH STREET



METAL RAILINGS AND STEPS  
17TH STREET AND PEDESTRIAN PROMENADE



FIBER CEMENT PANEL ACCENTS  
RESIDENCE ALONG MISSISSIPPI ST.



FIBER CEMENT PANEL



CORRUGATED CAST CONCRETE BASE ALONG 17TH ST.



FIBER CEMENT PANEL ACCENTS AT BAYS



CHRISTIANI JOHNSON ARCHITECTS, INC.  
665 Third Street, Suite 350  
San Francisco, CA 94107  
415.243.9484  
cjarchs.com

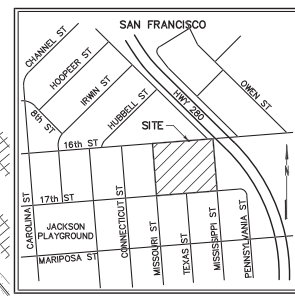
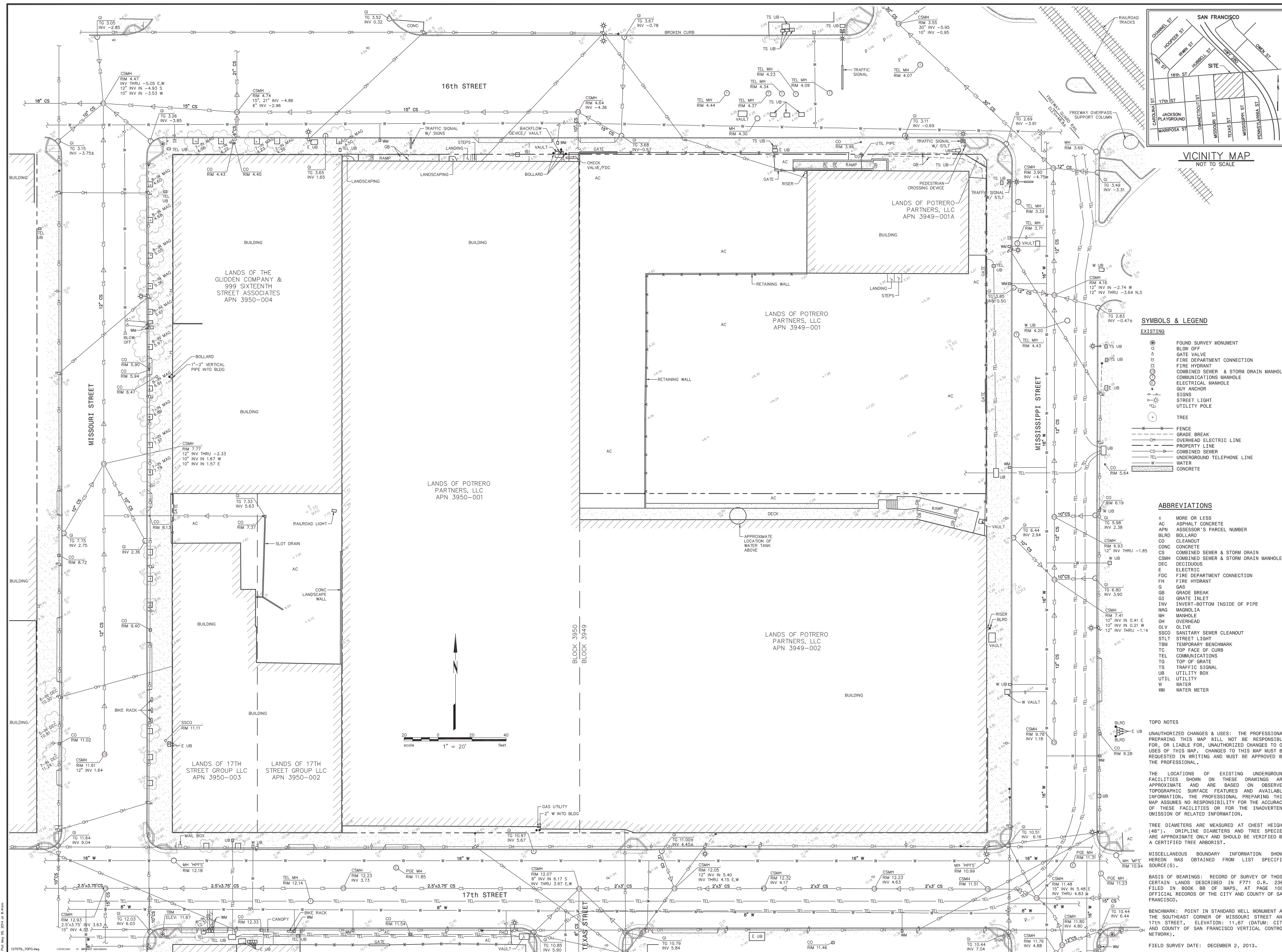
# 1200 17TH STREET

POTRERO PARTNERS, LLC

|             |            |
|-------------|------------|
| Scale       | Date       |
| NTS         | 05.12.2016 |
| Sheet Title |            |
| MATERIALS   |            |

G1.8





SYMBOLS & LEGEND

- EXISTING
- FOUND SURVEY MONUMENT
  - BLOW OFF
  - GATE VALVE
  - FIRE DEPARTMENT CONNECTION
  - FIRE HYDRANT
  - COMBINED SEWER & STORM DRAIN MANHOLE
  - COMMUNICATIONS MANHOLE
  - ELECTRICAL MANHOLE
  - GUY ANCHOR
  - SIGNS
  - STREET LIGHT
  - UTILITY POLE
  - TREE
  - FENCE
  - GRADE BREAK
  - OVERHEAD ELECTRIC LINE
  - PROPERTY LINE
  - COMBINED SEWER
  - UNDERGROUND TELEPHONE LINE
  - WATER
  - CONCRETE

ABBREVIATIONS

- ± MORE OR LESS
- AC ASPHALT CONCRETE
- APN ASSESSOR'S PARCEL NUMBER
- BLRD BOLLARD
- CO CLEANOUT
- CONC CONCRETE
- CS COMBINED SEWER & STORM DRAIN
- CSMH COMBINED SEWER & STORM DRAIN MANHOLE
- DEC DECIDUOUS
- E ELECTRIC
- FDC FIRE DEPARTMENT CONNECTION
- FH FIRE HYDRANT
- G GAS
- GB GRADE BREAK
- GI GRATE INLET
- INV INVERT-BOTTOM INSIDE OF PIPE
- MAG MAGNOLIA
- MH MANHOLE
- OH OVERHEAD
- OLV OLIVE
- SSCO SANITARY SEWER CLEANOUT
- STLT STREET LIGHT
- TBM TEMPORARY BENCHMARK
- TC TOP FACE OF CURB
- TEL TELECOMMUNICATIONS
- TG TOP OF GRATE
- TS TRAFFIC SIGNAL
- UB UTILITY BOX
- UTIL UTILITY
- W WATER
- WM WATER METER

TOPO NOTES

UNAUTHORIZED CHANGES & USES: THE PROFESSIONAL PREPARING THIS MAP WILL NOT BE RESPONSIBLE FOR, OR LIABLE FOR, UNAUTHORIZED CHANGES TO OR USES OF THIS MAP. CHANGES TO THIS MAP MUST BE REQUESTED IN WRITING AND MUST BE APPROVED BY THE PROFESSIONAL.

THE LOCATIONS OF EXISTING UNDERGROUND FACILITIES SHOWN ON THESE DRAWINGS ARE APPROXIMATE AND ARE BASED ON OBSERVED TOPOGRAPHIC SURFACE FEATURES AND AVAILABLE INFORMATION. THE PROFESSIONAL PREPARING THIS MAP ASSUMES NO RESPONSIBILITY FOR THE ACCURACY OF THESE FACILITIES OR FOR THE INADVERTENT OMISSION OF RELATED INFORMATION.

TREE DIAMETERS ARE MEASURED AT CHEST HEIGHT (48"). DIAPHRANE DIAMETERS AND TREE SPECIES ARE APPROXIMATE ONLY AND SHOULD BE VERIFIED BY A CERTIFIED TREE ARBORIST.

MISCELLANEOUS BOUNDARY INFORMATION SHOWN HEREON WAS OBTAINED FROM LIST SPECIFIC SOURCE(S).

BASIS OF BEARINGS: RECORD OF SURVEY OF THOSE CERTAIN LANDS DESCRIBED IN F771 O.R. 236, FILED IN BOOK 8B OF MAPS, AT PAGE 100, OFFICIAL RECORDS OF THE CITY AND COUNTY OF SAN FRANCISCO.

BENCHMARK: POINT IN STANDARD WELL MONUMENT AT THE SOUTHEAST CORNER OF MISSOURI STREET AND 17TH STREET. ELEVATION: 11.67 (DATUM: CITY AND COUNTY OF SAN FRANCISCO VERTICAL CONTROL NETWORK).

FIELD SURVEY DATE: DECEMBER 2, 2013.

PRELIMINARY  
NOT FOR CONSTRUCTION  
DATE: 05/09/2014  
RALPH E. THOMAS



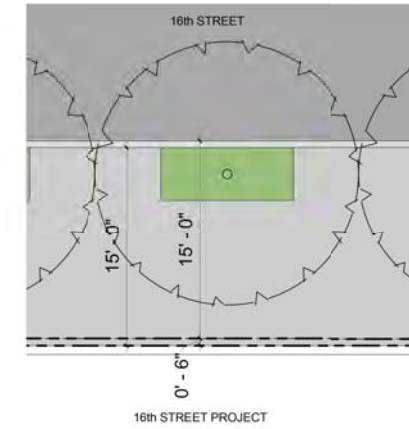
901-941 16th STREET  
APN LOT 1, BLK 3950, LOT 1, 2 & 14, BLK 3949  
SAN FRANCISCO, CALIFORNIA

TOPOGRAPHIC SURVEY

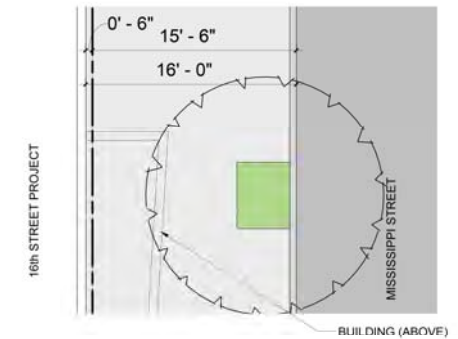
| Revisions | No. | Date | Issue | Drawn | Approved | Used |
|-----------|-----|------|-------|-------|----------|------|
|           |     |      |       |       |          |      |
|           |     |      |       |       |          |      |
|           |     |      |       |       |          |      |
|           |     |      |       |       |          |      |

Drawing Number:  
**C-1**

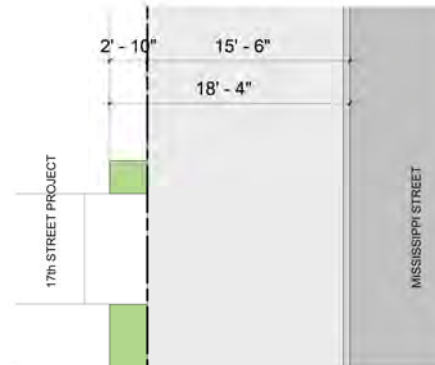




1 16th STREET SIDEWALK  
3/16" = 1'-0"



2 MISSISSIPPI SIDEWALK AT 16th ST PROJECT  
3/16" = 1'-0"



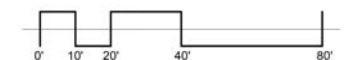
3 MISSISSIPPI STREET AT 17th ST PROJECT  
3/16" = 1'-0"



4 17th STREET SIDEWALK  
3/16" = 1'-0"

#### INDEX OF LANDSCAPE SHEETS

|        |                                |
|--------|--------------------------------|
| L-0.0  | OVERALL SITE PLAN              |
| L-0.1  | Public / Shared Spaces         |
| L-0.2  | Mississippi Mews               |
| L-0.3  | Public / Shared Images         |
| L-16.1 | 16th St Courtyards             |
| L-16.2 | 16th St Amenity Roof Decks     |
| L-16.3 | 16th St Extensive Roof Decks   |
| L-16.4 | 16th St Sections               |
| L-16.5 | 16th St Images                 |
| L-17.1 | 17th St Courtyards & Roof Deck |
| L-17.2 | 17th St Sections and Images    |



## OVERALL SITE PLAN L-0.0

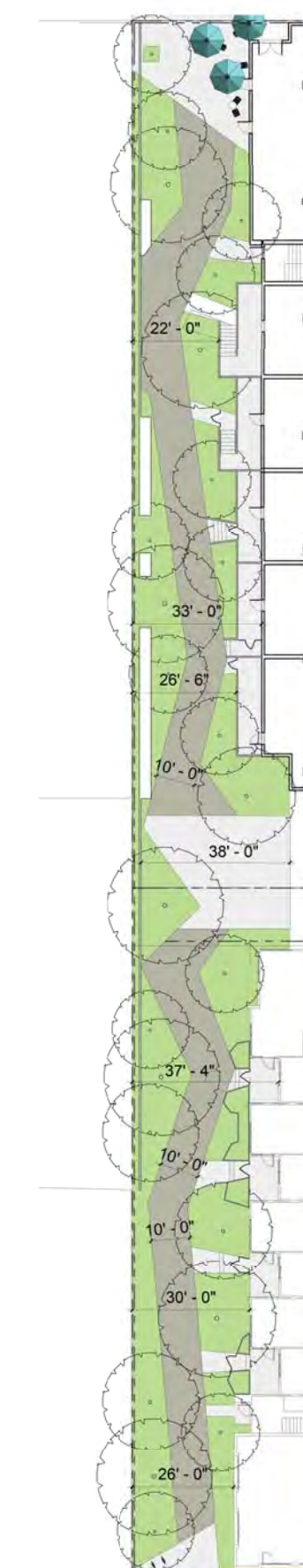
PGA design



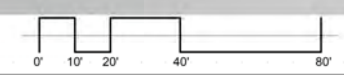
05/12/16

1200 17th St. / 901 16th St.  
San Francisco, California





1 Pedestrian Promenade  
1/16" = 1'-0"



**PUBLIC / SHARED SPACES NOTES**

**16TH STREET FRONTAGE:**

- 1 PLANTING:  
-STREET TREES: ROBINIA 'PURPLE ROBE'  
-DROUGHT TOLERANT SUCCULENTS, GROUNDCOVER, AND PERENNIALS
- 2 HARDSCAPE:  
-SIDEWALK: CIP CONCRETE WITH LAMP BLACK AND GRID SCORING  
AT PEDESTRIAN PROMENADE: CIP CONCRETE WITH LAMP BLACK, CUSTOM SCORING, AND SURFACE FINISH
- 3 FURNISHINGS:  
-RECLAIMED STEEL CABLE BIKE RACKS, RESISTANT TO CUTTING  
-BENCHES PLANTERS CAST CONCRETE AND GFRG
- 4 LIGHTING:  
-STREET LIGHTING  
-ARCHITECTURAL LIGHTING, BUILDING MOUNTED

**MISSISSIPPI STREET FRONTAGE:**

- 5 PLANTING:  
-STREET TREES:  
CORYMBIA CITRIODORA (AKA EUCALYPTUS CITRIODORA)  
PODOCARPUS GRACILIOR  
PRUNUS 'KWANZAN'  
-UNDERSTORY:  
DROUGHT TOLERANT SUCCULENTS, GROUNDCOVER, AND PERENNIALS
- 6 HARDSCAPE:  
-SIDEWALK: SIDEWALK: CIP CONCRETE WITH LAMP BLACK AND GRID SCORING
- 7 FURNISHINGS:  
-BIKE RACKS MADE FROM RECLAIMED STEEL CABLE, RESISTANT TO CUTTING  
-BENCHES
- 8 LIGHTING:  
-STREET LIGHTING  
-ARCHITECTURAL LIGHTING, BUILDING MOUNTED

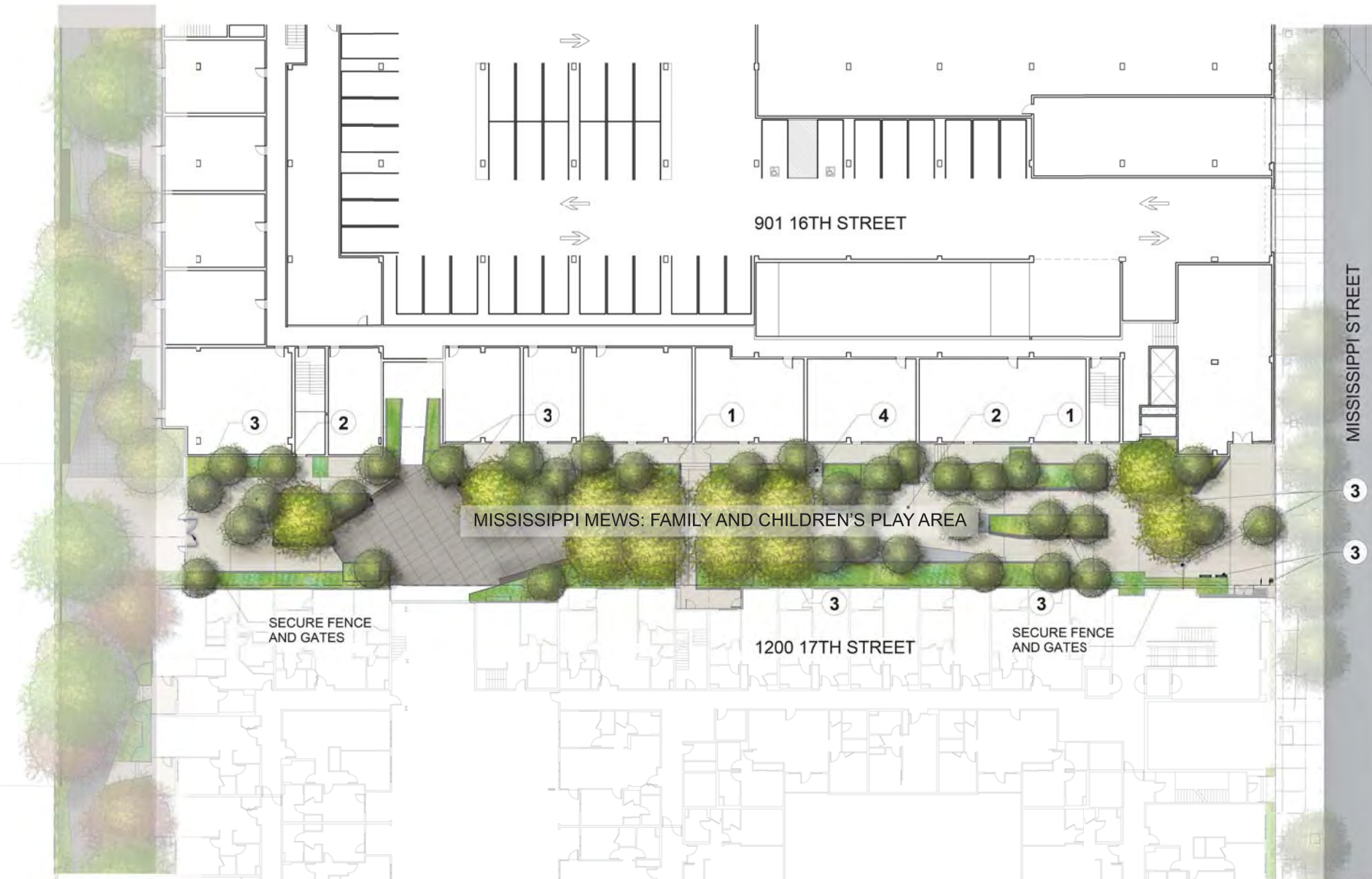
**17TH STREET FRONTAGE:**

- 9 PLANTING:  
-STREET TREES:  
ROBINIA 'PURPEL ROBE'  
CORYMBIA CITRIODORA  
PRUNUS SERRULATA  
-UNDERSTORY:  
DROUGHT TOLERANT SHRUBS, SUCCULENTS, GROUNDCOVER, AND PERENNIALS
- 10 HARDSCAPE:  
-SIDEWALK: CIP CONCRETE WITH LAMP BLACK AND GRID SCORING  
AT PEDESTRIAN PROMENADE: CIP CONCRETE WITH LAMP BLACK, CUSTOM SCORING, AND SURFACE FINISH
- 11 FURNISHINGS:  
-BIKE RACKS MADE FROM RECLAIMED STEEL CABLE, RESISTANT TO CUTTING  
-BENCHES  
-PLANTERS: RAISED PLANTERS AND POTS
- 12 LIGHTING:  
-STREET LIGHTING  
-STEP LIGHTING AT STOOPS  
-ARCHITECTURAL LIGHTING, BUILDING MOUNTED

**PEDESTRIAN PROMENADE:**

- 13 PLANTING:  
-TREES:  
GINKGO BILOBA  
GINKGO BILOBA 'PRINCETON SENTRY'  
ULMUS PARVIFOLIA  
DIOSPYROS KAKI  
-UNDERSTORY:  
DROUGHT TOLERANT SHRUBS, SUCCULENTS, GROUNDCOVER, AND PERENNIALS
- 14 HARDSCAPE:  
-AT SIDEWALKS: CIP CONCRETE WITH LAMP BLACK, CUSTOM SCORING, AND SURFACE FINISH  
-AT MEWS NODE: CIP CONCRETE WITH LAMP BLACK, CUSTOM SCORING, AND SURFACE FINISH  
-BETWEEN: ASPHALTIC PAVERS IN BLACK AND GREY
- 15 SCREENING:  
-SCREENING: TALL METAL FRAME WITH GREEN SCREEN AND VINES  
-SCREENING: SHORT SCREENING
- 16 FURNISHINGS:  
-RECLAIMED STEEL CABLE BIKE RACKS AT 17TH STREET NODE  
-CAFE SEATING AND TABLES AT 16TH ST CAFE  
-RAISED PLANTERS AT STOOPS WITH INDUSTRIAL CHARACTER
- 17 LIGHTING:  
-SCULPTURAL PATH LIGHTING (SAFETY LIGHTING)  
-STEP LIGHTING AT STOOPS
- 18 **STORMWATER: CISTERN IN GARAGE**



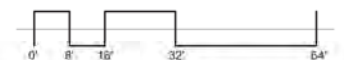


#### MEWS NOTES

##### MEWS MATERIALS:

- 1 PLANTING:  
TREES:  
GINKGO BILOBA  
GINKGO BILOBA 'PRINCETON SENTRY'  
MELALEUCA QUINQUENERVIA  
  
UNDERSTORY: DROUGHT TOLERANT SHRUBS, SUCCULENTS, GROUNDCOVER, AND PERENNIALS
- 2 HARDSCAPE:  
CONCRETE WITH SAWCUT SCORING  
SPECIAL CONCRETE WITH SAWCUT SCORING AND COLOR  
WALKABLE PEBBLE SURFACE
- 3 FURNISHINGS:  
BUILT-IN BENCHES  
BUILT-IN PLANTERS: BOARD-FORMED CONCRETE AND METAL STORAGE  
CONTAINER COMPONENTS  
PERCHING/SEATING RAILS  
RECLAIMED CABLE BIKE RACKS  
CUSTOM ORNAMENTAL SECURITY FENCING
- 4 LIGHTING:  
PATH LIGHTING  
STEP LIGHTING AT STOOPS  
ARCHITECTURAL LIGHTING, BUILDING MOUNTED

1 16th St level 1 Mews  
1/16" = 1'-0"



05/12/16

1200 17th St. / 901 16th St.  
San Francisco, California

Mississippi Mews - Family and Children's Play Area  
L-0.2





NOTES:  
1. COMMON NAME: LEMON SCENTED GUM  
2. EVERGREEN

Corymbia citriodora



NOTES:  
1. COMMON NAME: FUYU JAPANESE PERSIMMON  
2. DECIDUOUS

Diospyros kaki 'Fuyu'



NOTES:  
1. COMMON NAME: JACARANDA  
2. DECIDUOUS

Jacaranda mimosifolia



NOTES:  
1. COMMON NAME: GINKGO  
2. DECIDUOUS

Ginkgo biloba



NOTES:  
1. COMMON NAME: PRINCETON SENTRY GINKGO  
2. DECIDUOUS

Ginkgo biloba 'Princeton Sentry'



NOTES:  
1. COMMON NAME: PAPERBARK MELALEUCA  
2. EVERGREEN

Melaleuca quinquenervia



NOTES:  
1. COMMON NAME: PODOCARPUS  
2. EVERGREEN

Podocarpus gracilior



NOTES:  
1. COMMON NAME: KWANZAN CHERRY  
2. DECIDUOUS

Prunus 'Kwanzan'



NOTES:  
1. COMMON NAME: ARISTOCRAT FLOWERING PEAR  
2. DECIDUOUS

Pyrus calleryana 'Aristocrat'



NOTES:  
1. COMMON NAME: PURPLE ROBE ROBINIA  
2. DECIDUOUS

Robinia 'Purple Robe'



NOTES:  
1. COMMON NAME: LACEBARK ELM  
2. DECIDUOUS

Ulmus parvifolia



NOTES:  
1. ECOFLEX BIKE RACK  
2. LOCALLY SOURCED AND MANUFACTURED  
3. MADE FROM RECLAIMED, COATED STEEL CABLE: RESISTANT TO CUTTING

BIKE RACK





1 17th St Level 2 Podium West Courtyard  
1/8" = 1'-0"

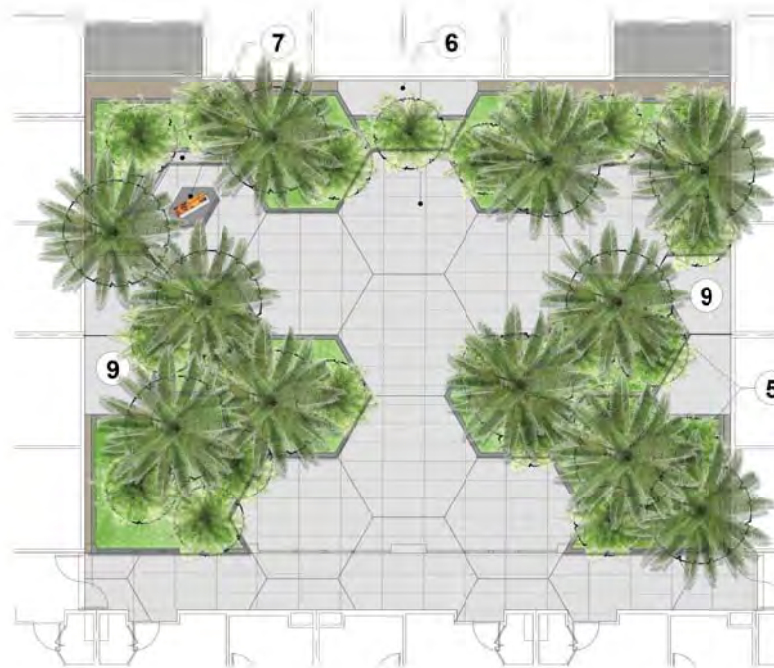
## 17TH ST. COURTYARD NOTES

### WEST COURTYARD:

- 1 PLANTING:  
TREES:  
OLEA EUROPAEA 'SWAN HILL'  
  
UNDERSTORY:  
DROUGHT TOLERANT SHRUBS, SUCCULENTS,  
PERENNIALS, GROUNDCOVERS, AND  
GRASSES  
MODERATE WATER USE PERENNIALS AND  
GROUNDCOVERS
- 2 HARDSCAPE:  
PAVING: PERMEABLE PAVERS  
PLANTER WALLS: CIP BOARD-FORMED CONCRETE  
DECORATIVE PEBBLE PERIMETER  
DECORATIVE PEBBLE AND STONE AREA AT BRICK  
BUILDING BASE
- 3 SITE FURNISHINGS:  
LOUNGE AND CAFE SEATING AND TABLES  
SUMMER KITCHEN  
BOCCE COURT  
PLANTERS CAST CONCRETE AND GFRC  
SEAT WALLS
- 4 LIGHTING:  
PATH LIGHTING  
TASK LIGHTING AT SUMMER KITCHEN  
ARCHITECTURAL LIGHTING ATTACHED TO BUILDING

### EAST COURTYARD:

- 5 PLANTING:  
TREES:  
CYATHEA COOPERI  
ARCHONTOPHOENIX CUNNINGHAMIANA  
  
UNDERSTORY:  
MODERATE WATER USE SHRUBS, PERENNIALS, AND  
GROUNDCOVERS
- 6 HARDSCAPE:  
PAVING: PERMEABLE PAVERS  
PLANTER WALLS: CIP BOARD-FORMED CONCRETE
- 7 SITE FURNISHINGS:  
LOUNGE AND CAFE SEATING AND TABLES  
SEAT WALLS, CIP CONCRETE  
FIREPIT, CIP CONCRETE  
PLANTERS, CAST CONCRETE WITH INTEGRAL COLOR
- 8 LIGHTING:  
PATH LIGHTING  
ARCHITECTURAL LIGHTING ATTACHED TO BUILDING
- 9 PRIVATE PATIOS  
PERMEABLE PAVERS  
SCREENING PANELS BETWEEN ADJACENT UNITS AND  
COURTYARD WHERE ADJACENT
- 10 STEPS TO OTHER LEVELS

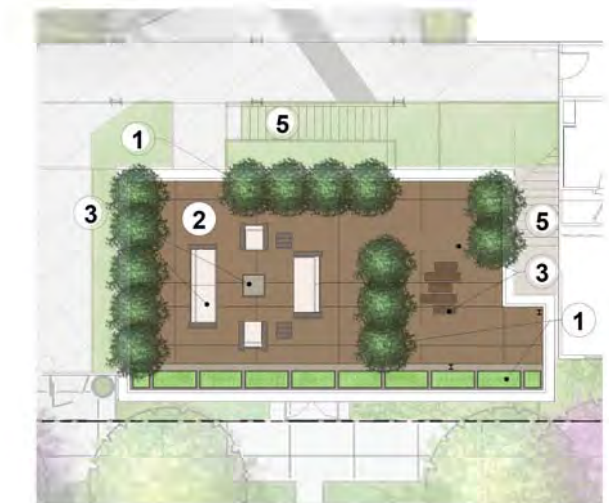


2 17th St Level 2 Podium East Courtyard  
1/8" = 1'-0"

## 17TH ST. AMENITY ROOF DECK NOTES

### ROOF OF EXISTING BRICK BUILDING:

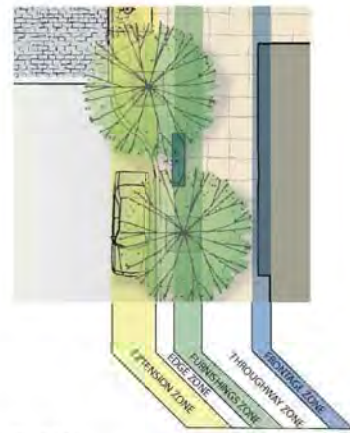
- 1 PLANTING:  
SMALL ORNAMENTAL TREES:  
PITTOSPORUM CRASSIFOLIUM
  - 2 UNDERSTORY:  
DROUGHT TOLERANT GROUNDCOVER, PERENNIALS, AND  
GRASSES
  - 3 HARDSCAPE:  
PAVING: IPE WOOD DECKING WITH HERRINGBONE  
PATTERN  
GLASS GUARDRAIL
  - 4 SITE FURNISHINGS:  
LOUNGE AND CAFE SEATING AND TABLES  
FIREPIT CAST CONCRETE  
PLANTERS: GFRC
  - 5 LIGHTING:  
STRING LIGHTS (NOT SHOWN)  
ARCHITECTURAL LIGHTING ATTACHED TO BUILDING
- STAIR TO OTHER LEVELS



3 17th St Level 3 Amenity Roof Deck  
1/8" = 1'-0"







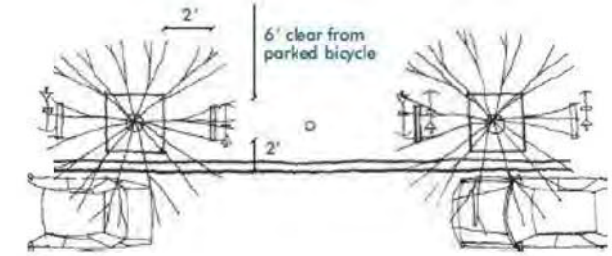
NOTES:  
1. MIXED-USE TYPICAL SIDEWALK PLAN  
2. SAN FRANCISCO BETTER STREETS PLAN  
3. P.98

① Better Streets Sidewalk Plan Zones  
1" = 1'-0"



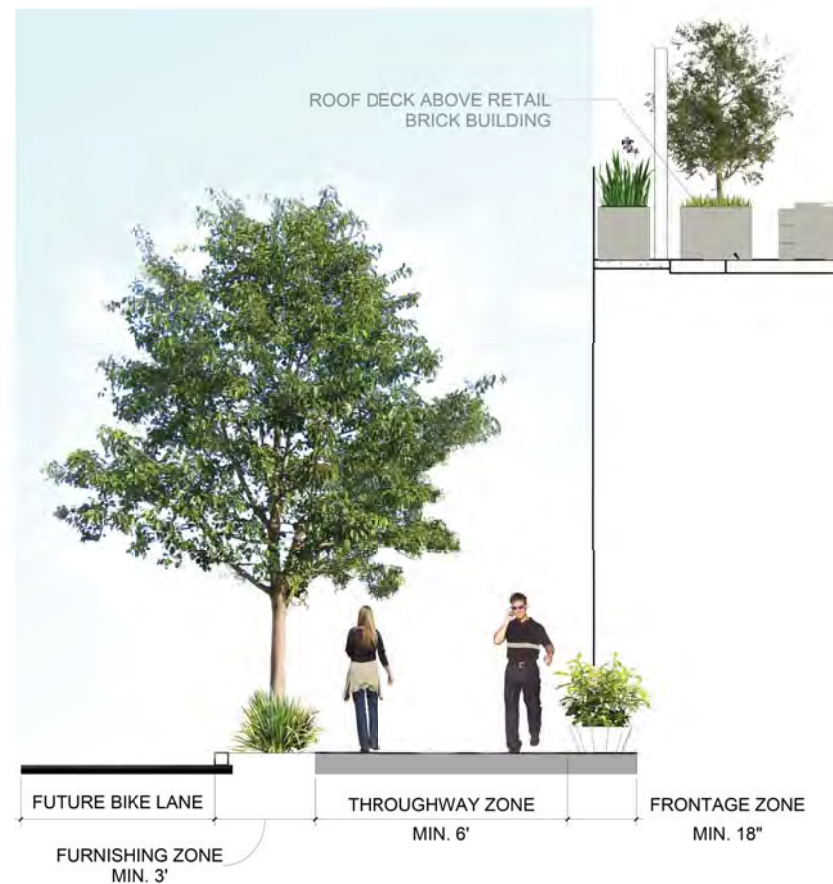
NOTES:  
1. MIXED-USE TYPICAL SIDEWALK SECTION  
2. SAN FRANCISCO BETTER STREETS PLAN  
3. P.100

② Better Streets Sidewalk Section Zones  
1" = 1'-0"



NOTES:  
1. TYPICAL BIKE RACK PLACEMENT FOR SIDEWALKS >12'  
2. SAN FRANCISCO BETTER STREETS PLAN  
3. P.221

③ Better Streets Sidewalk Bike Rack Plan  
1" = 1'-0"



④ Section at Existing Brick Building  
3/8" = 1'-0"



NOTES:  
1. KING PALM  
2. EVERGREEN  
ARCHONTOPHOENIX CUNNINGHAMIANA



NOTES:  
1. COMMON NAME: SWAN HILL OLIVE  
2. EVERGREEN  
OLEA EUROPEA 'SWAN HILL'

OLEA EUROPEA 'SWAN HILL'



NOTES:  
1. COMMON NAME: AUSTRALIAN TREE FERN  
2. EVERGREEN  
CYATHEA COOPERI

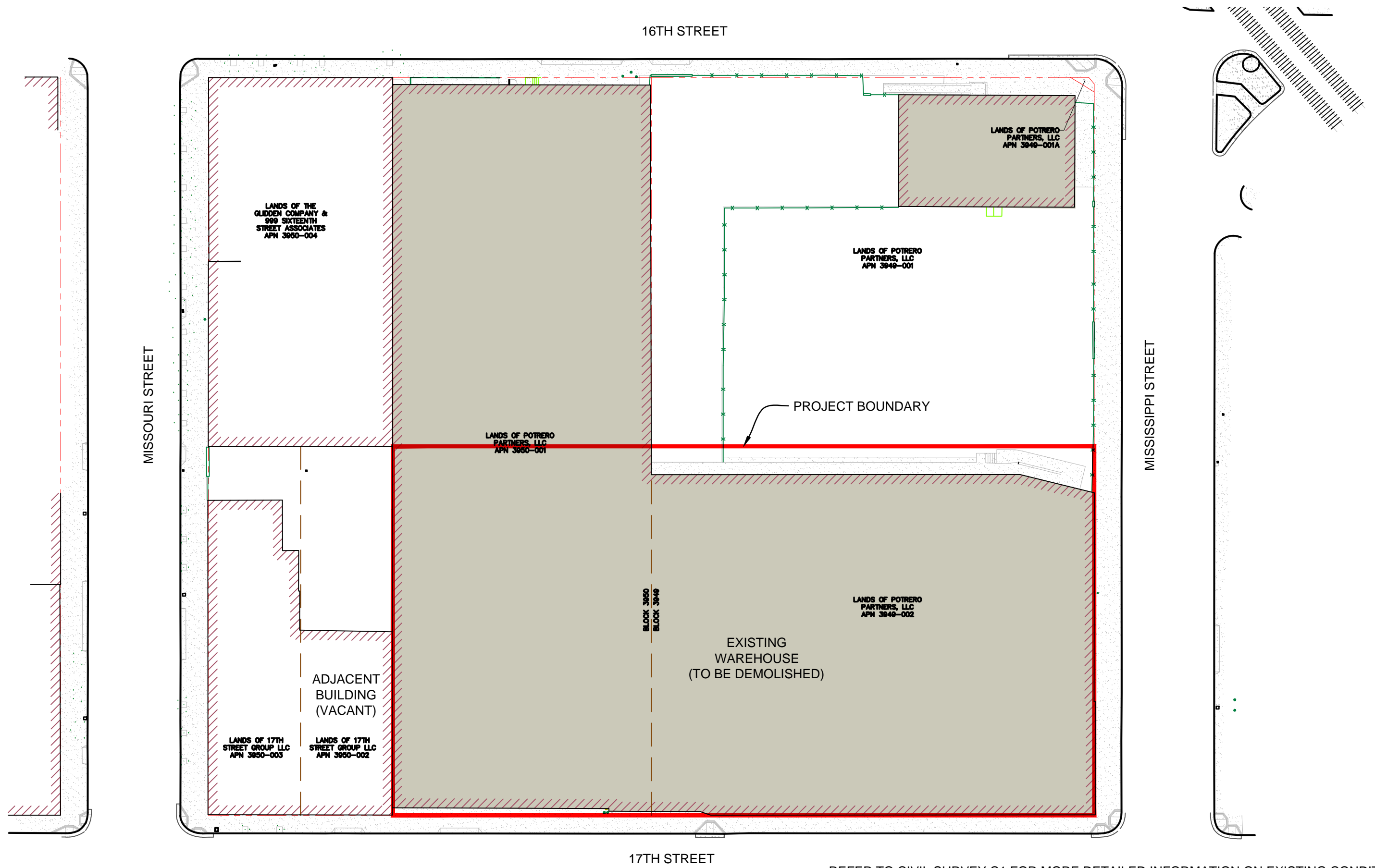
CYATHEA COOPERI



NOTES:  
1. COMMON NAME: KARO  
2. EVERGREEN  
PITTOSPORUM CRASSIFOLIUM

PITTOSPORUM CRASSIFOLIUM





CHRISTIANI JOHNSON ARCHITECTS, INC.  
665 Third Street, Suite 350  
San Francisco, CA 94107  
415.243.9484  
cjarchs.com

# 1200 17TH STREET

POTRERO PARTNERS, LLC



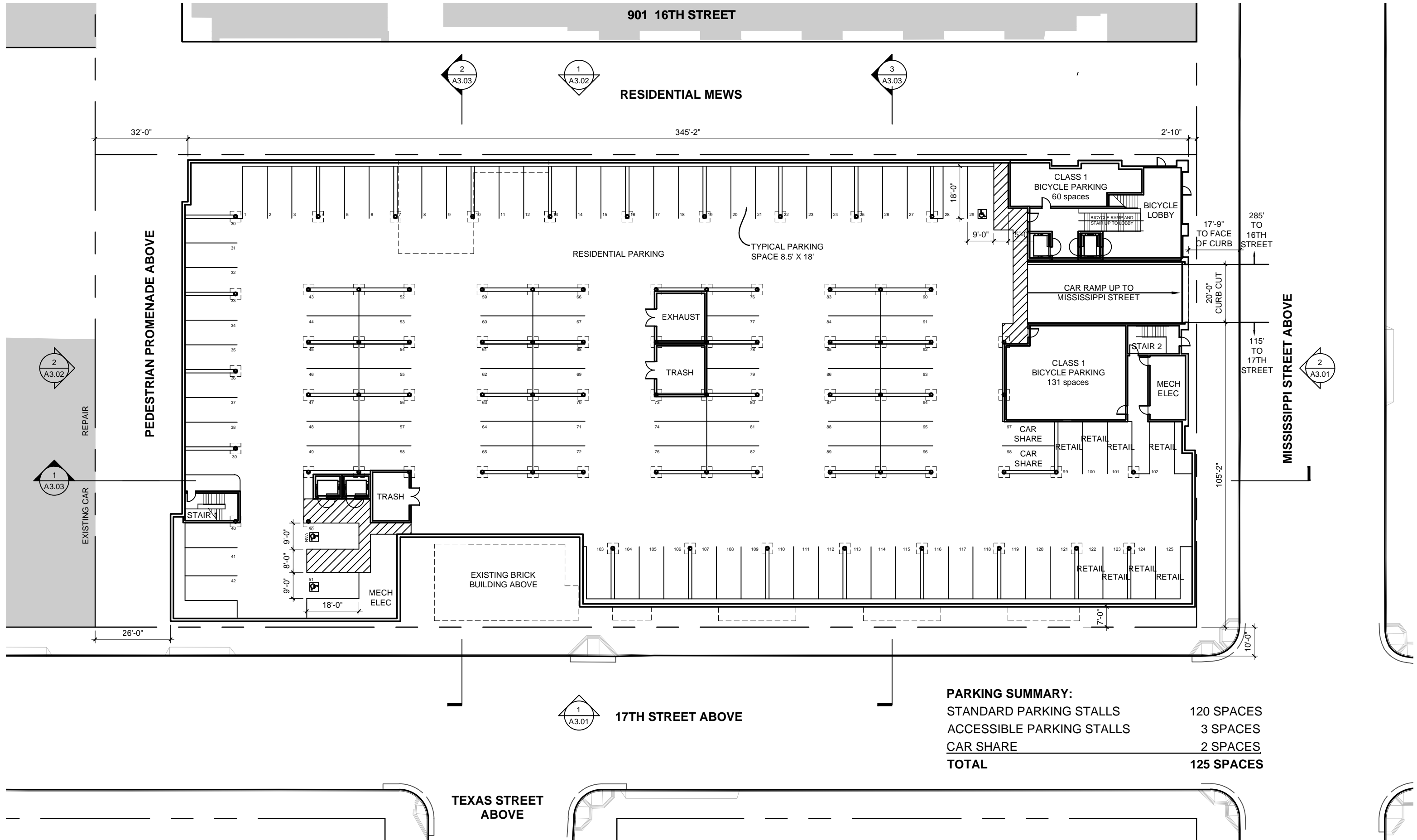
|                    |            |
|--------------------|------------|
| Scale              | Date       |
| 1"=50'-0"          | 05.12.2016 |
| Sheet Title        |            |
| SITE PLAN          |            |
| EXISTING CONDITION |            |

A1.00





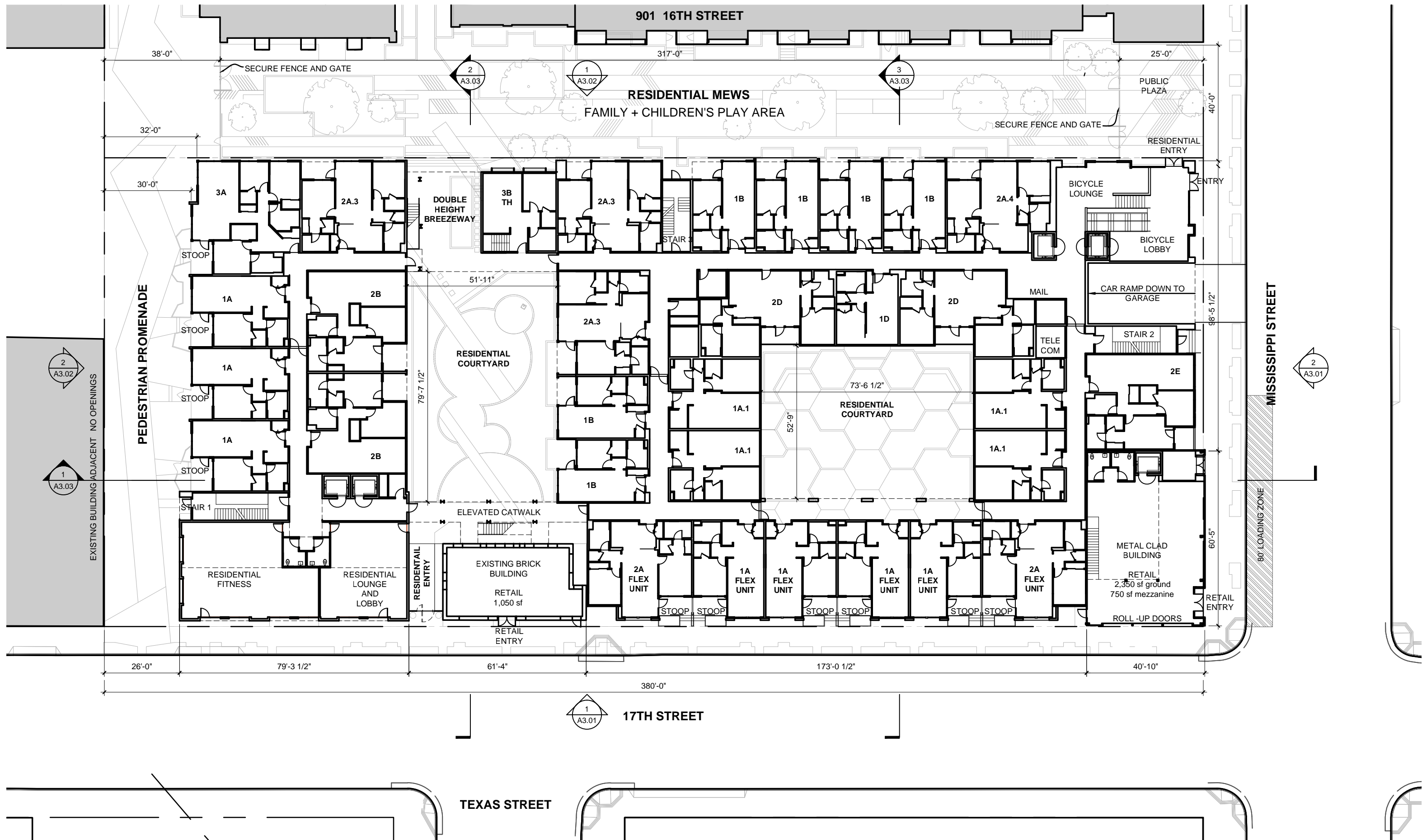




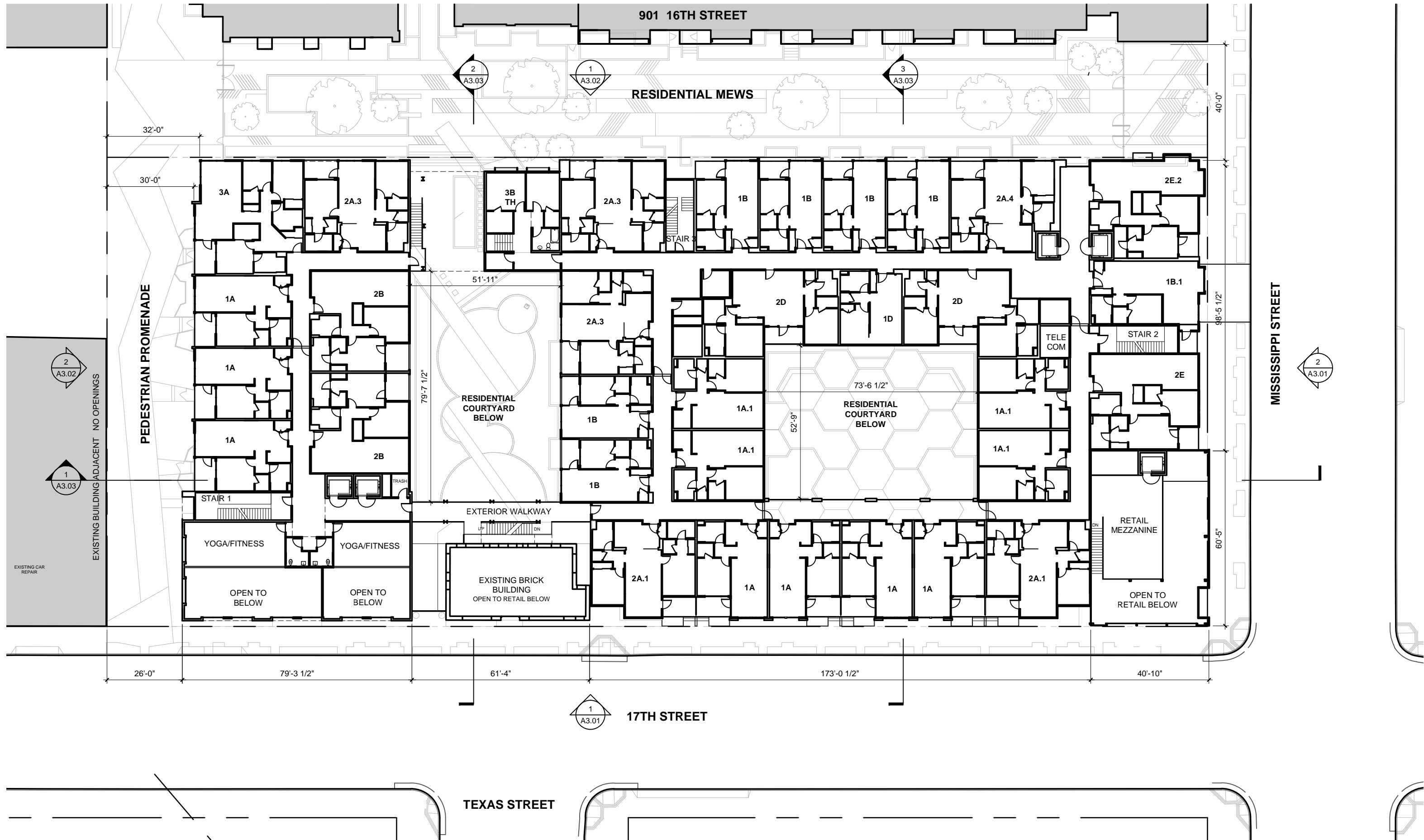
**PARKING SUMMARY:**

|                           |                   |
|---------------------------|-------------------|
| STANDARD PARKING STALLS   | 120 SPACES        |
| ACCESSIBLE PARKING STALLS | 3 SPACES          |
| CAR SHARE                 | 2 SPACES          |
| <b>TOTAL</b>              | <b>125 SPACES</b> |

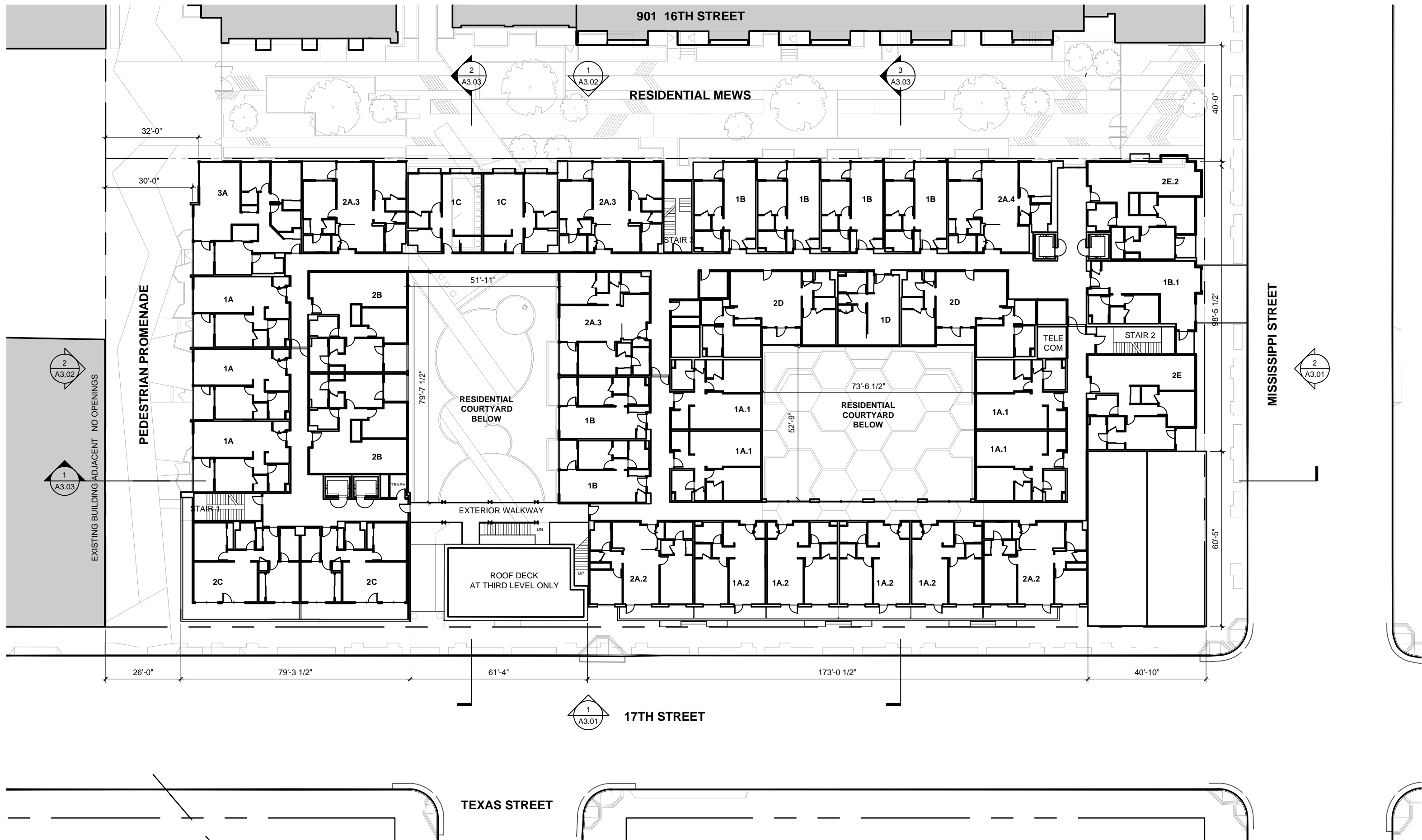




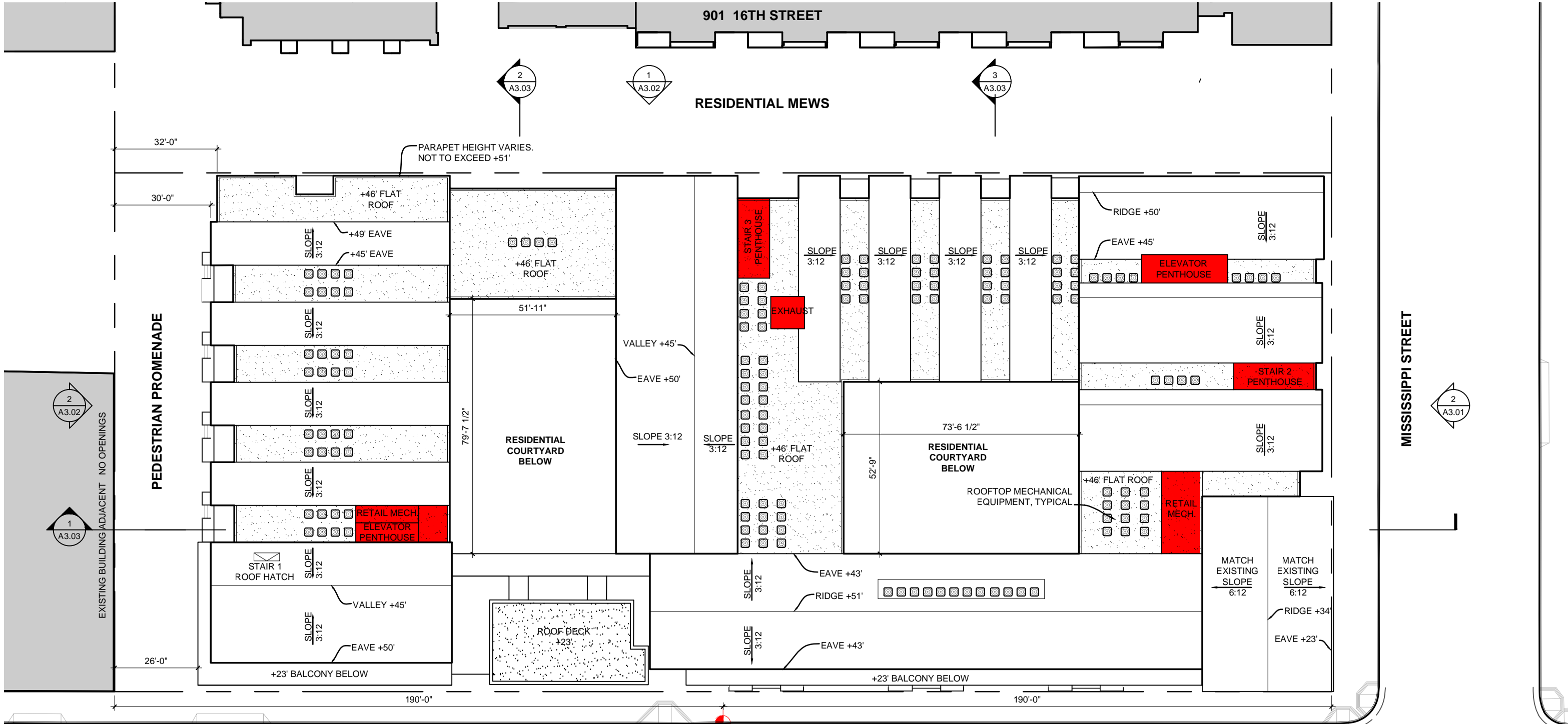












POINT OF BUILDING HEIGHT MEASUREMENT:  
MIDPOINT OF BUILDING ALONG 17TH STREET +0'  
(TOPO ELEVATION +11.24' REFER TO SURVEY C1)

SLOPED ROOFS ARE COMPLIANT WITH THE STANDARD HEIGHT MEASUREMENT PER SEC. 260.A.2  
TOTAL ROOF AREA = 40,073 SF  
TOTAL SLOPED ROOF AREA OVER 48' = 6,751 SF  
16.8% TOTAL ROOF AREA

ELEVATOR AND STAIR PENTHOUSES, AND RETAIL MECHANICAL AREAS ARE ALLOWED UP TO 10' ABOVE 48' HEIGHT LIMIT PER SEC. 261.B.1. (AREAS SHOWN SHADED RED)  
TOTAL ROOF AREA = 40,073 SF  
TOTAL PENTHOUSE AREA OVER 48' = 1,510 SF  
3.8% TOTAL ROOF AREA

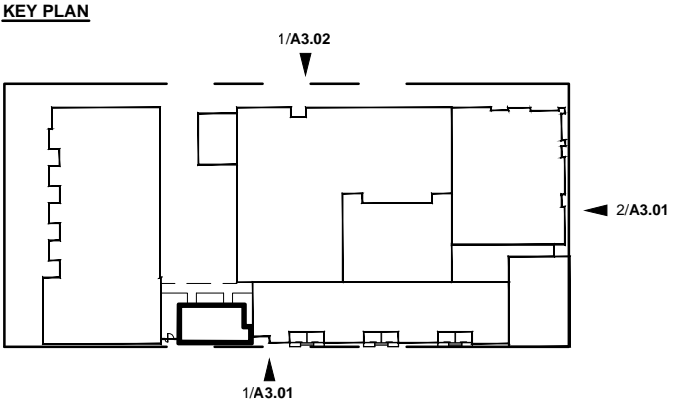
RESIDENTIAL EQUIPMENT WILL BE LOCATED ON FLAT ROOF AREAS SCREENED BY SLOPE ROOFS.





2 - MISSISSIPPI STREET (EAST) ELEVATION

- MATERIAL KEY**
- A BOARD FORMED CAST CONCRETE
  - B CORRUGATED CAST CONCRETE
  - C CORRUGATED METAL SIDING
  - D VERTICAL RIBBED METAL SIDING
  - E CEMENT BOARD PANEL SYSTEM
  - F HORIZONTAL SHIPLAP SIDING
  - G METAL ROOFING
  - H COMPOSITION SHINGLE ROOFING
  - I WINDOWS
  - J ALUMINUM PANELS
  - K PAINTED STEEL COLUMNS/BEAMS/RAILING
  - L ROLL-UP ALUMINUM DOOR



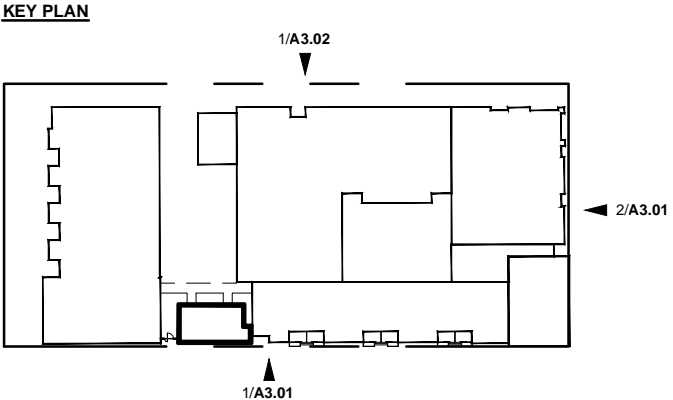
1 - 17TH STREET (SOUTH) ELEVATION





2 - PEDESTRIAN PROMENADE (WEST) ELEVATION

- MATERIAL KEY**
- A** BOARD FORMED CAST CONCRETE
  - B** CORRUGATED CAST CONCRETE
  - C** CORRUGATED METAL SIDING
  - D** VERTICAL RIBBED METAL SIDING
  - E** CEMENT BOARD PANEL SYSTEM
  - F** HORIZONTAL SHIPLAP SIDING
  - G** METAL ROOFING
  - H** COMPOSITION SHINGLE ROOFING
  - I** WINDOWS
  - J** ALUMINUM PANELS
  - K** PAINTED STEEL COLUMNS/BEAMS/RAILING
  - L** ROLL-UP ALUMINUM DOOR



1 - RESIDENTIAL MEWS (NORTH) ELEVATION



CHRISTIANI JOHNSON ARCHITECTS, INC.  
665 Third Street, Suite 350  
San Francisco, CA 94107  
415.243.9484  
cjarchs.com

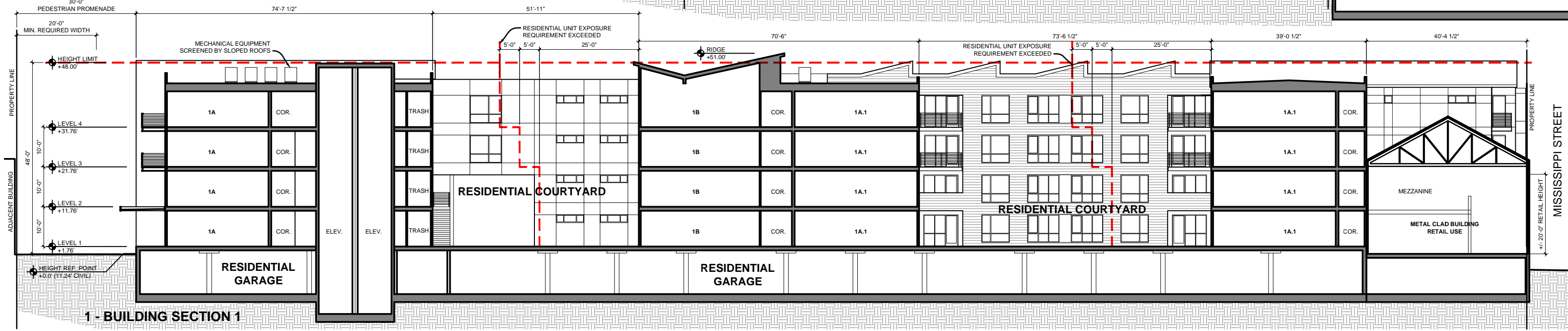
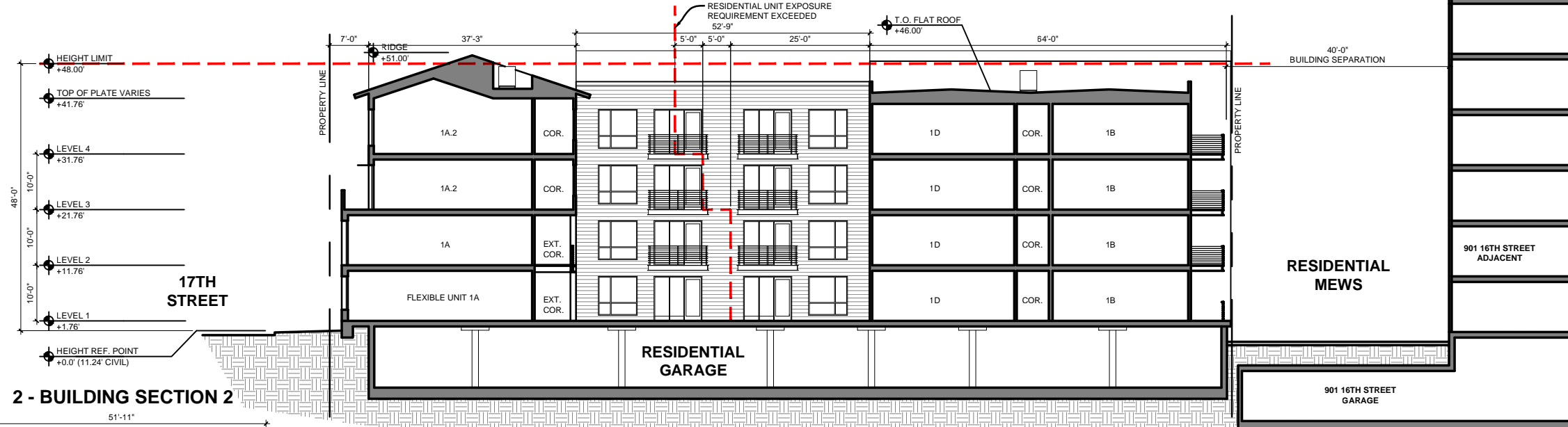
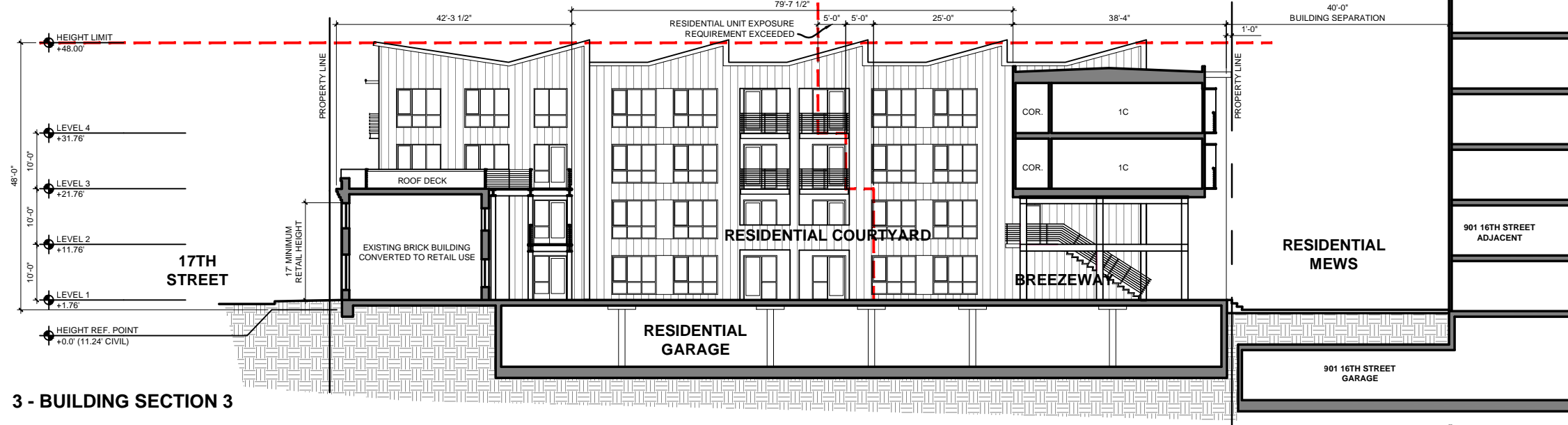
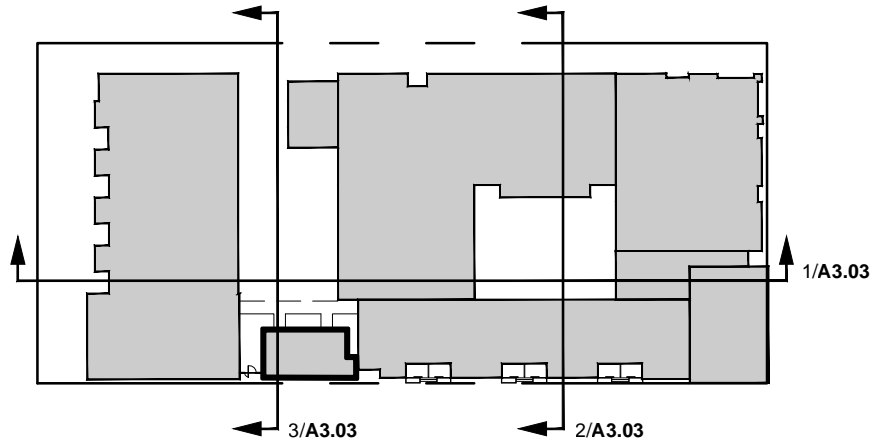
1200 17TH STREET  
POTRERO PARTNERS, LLC

Scale  
1"=20'-0"  
Date  
05.12.2016  
Sheet Title  
PROMENADE, MEWS  
ELEVATIONS

A3.02



# KEY PLAN



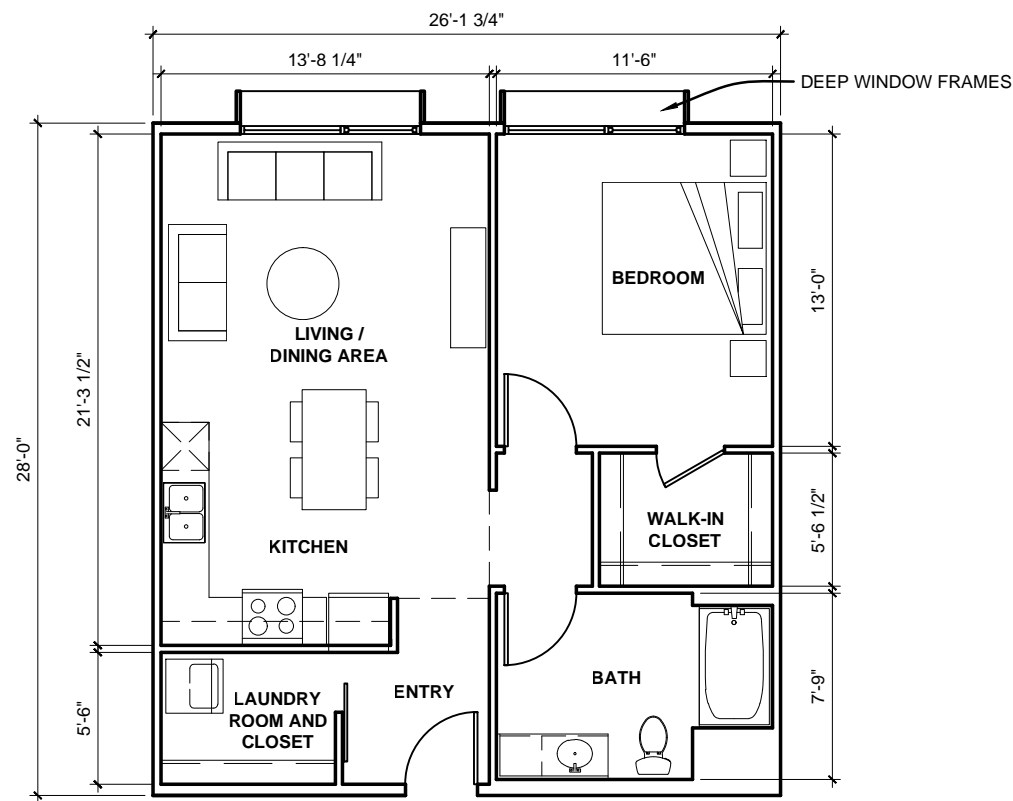
CHRISTIANI JOHNSON ARCHITECTS, INC.  
665 Third Street, Suite 350  
San Francisco, CA 94107  
415.243.9484  
cjarchs.com

## 1200 17TH STREET POTRERO PARTNERS, LLC

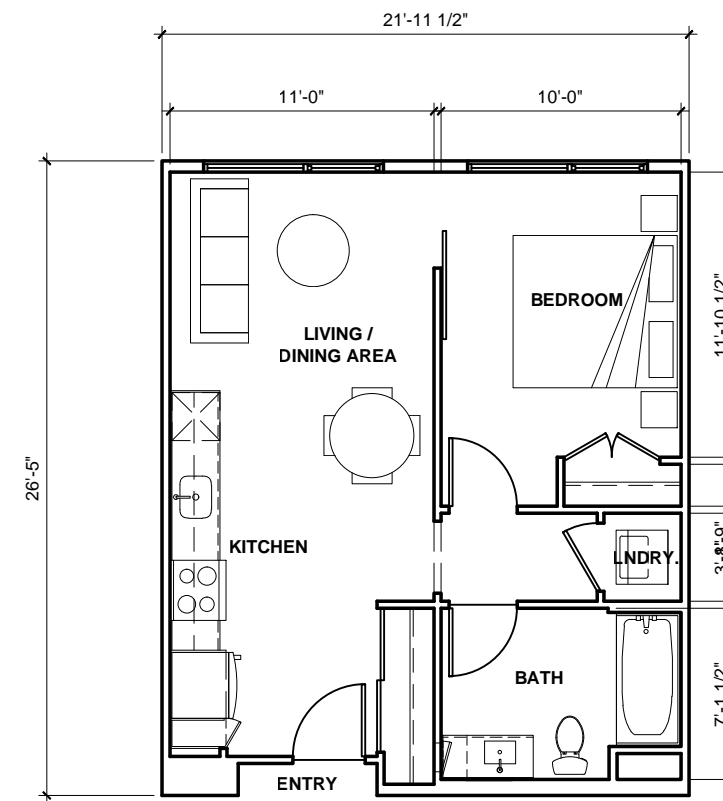
Scale 1"=20'-0" Date 05.12.2016  
Sheet Title BUILDING SECTIONS

A3.03

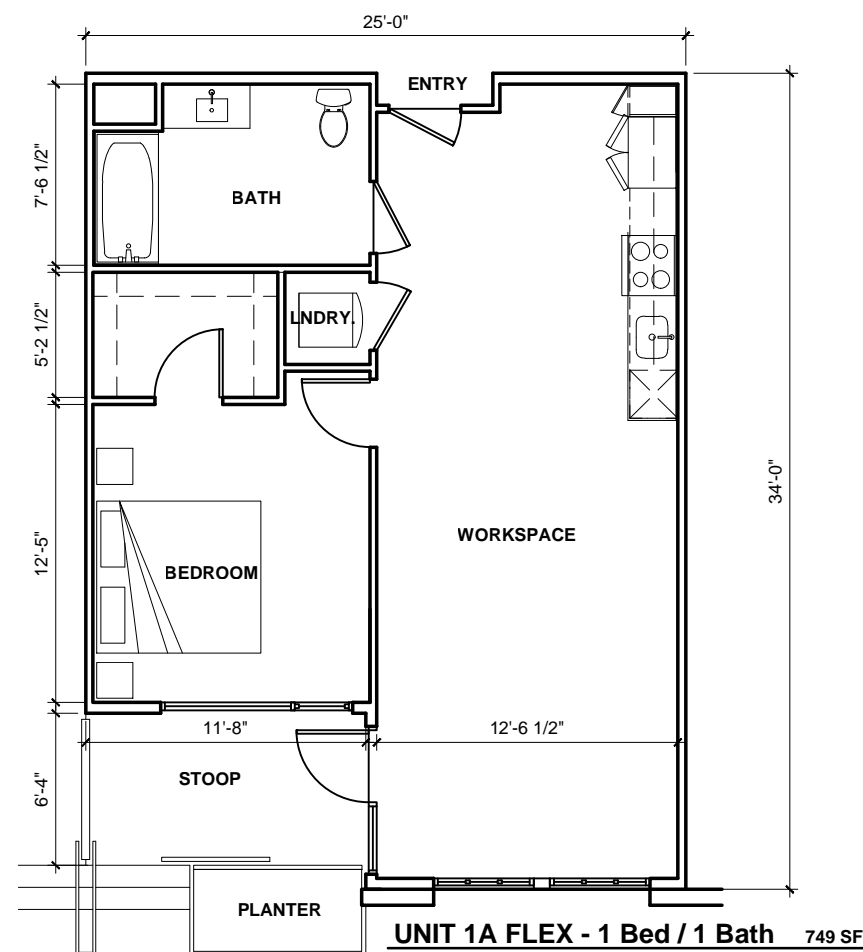




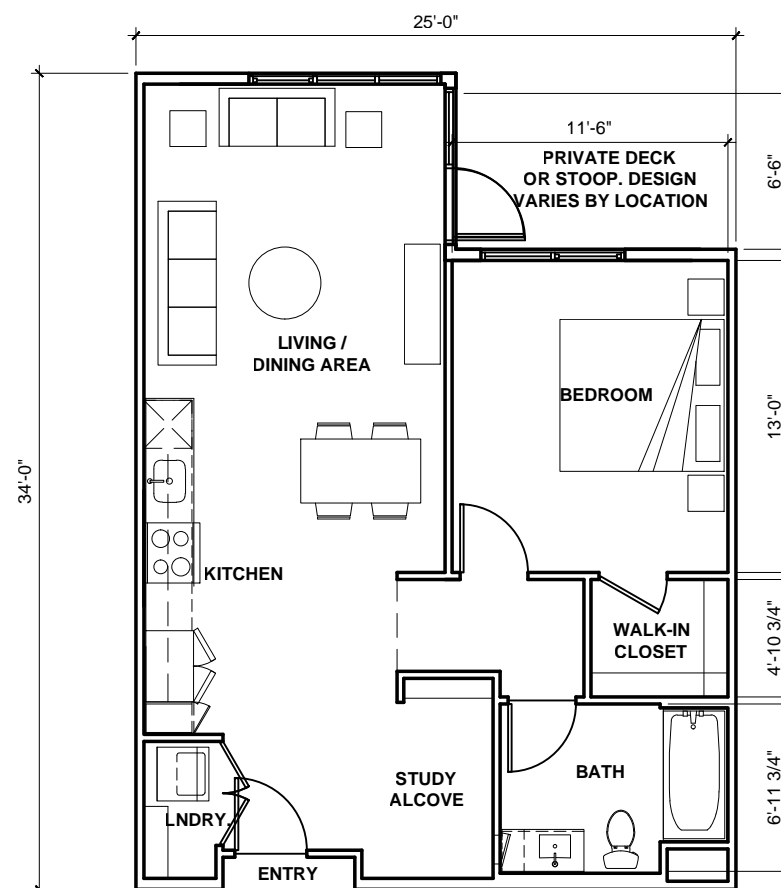
**UNIT 1C - 1 Bedroom / 1 Bath** 726 SF



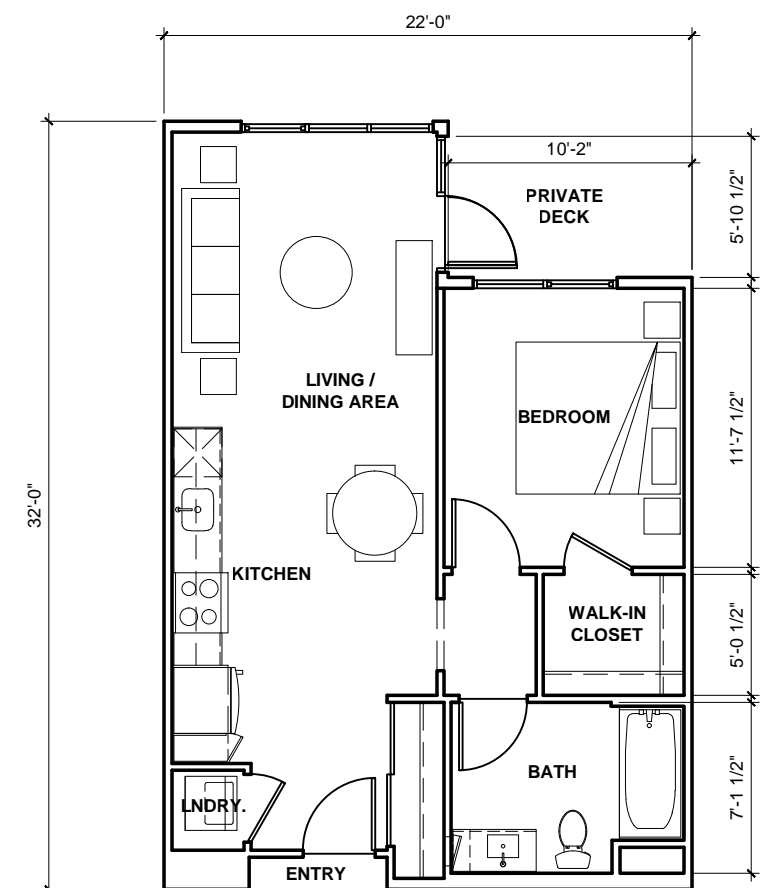
**UNIT 1D - 1 Bedroom / 1 Bath** 580 SF



**UNIT 1A FLEX - 1 Bed / 1 Bath** 749 SF

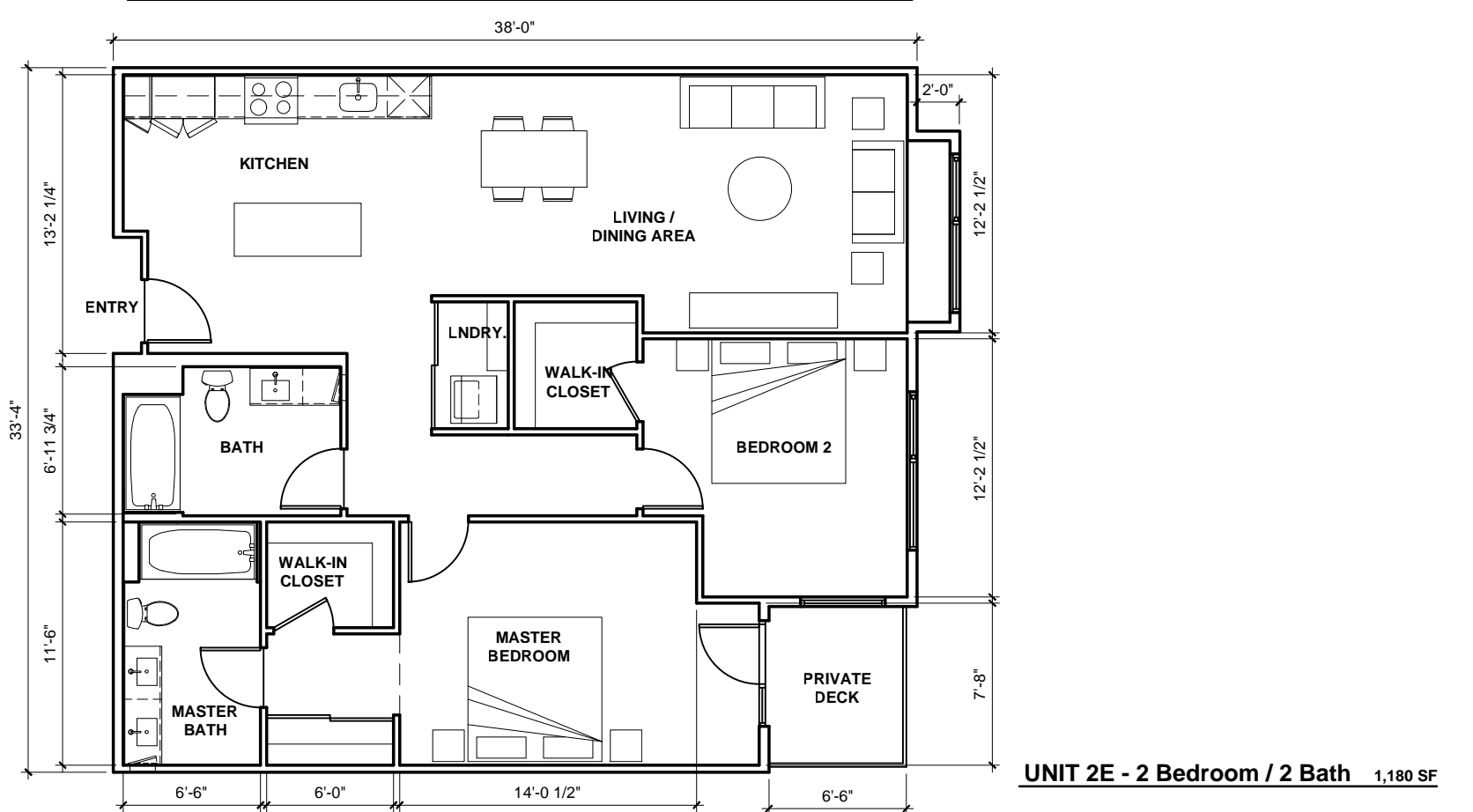
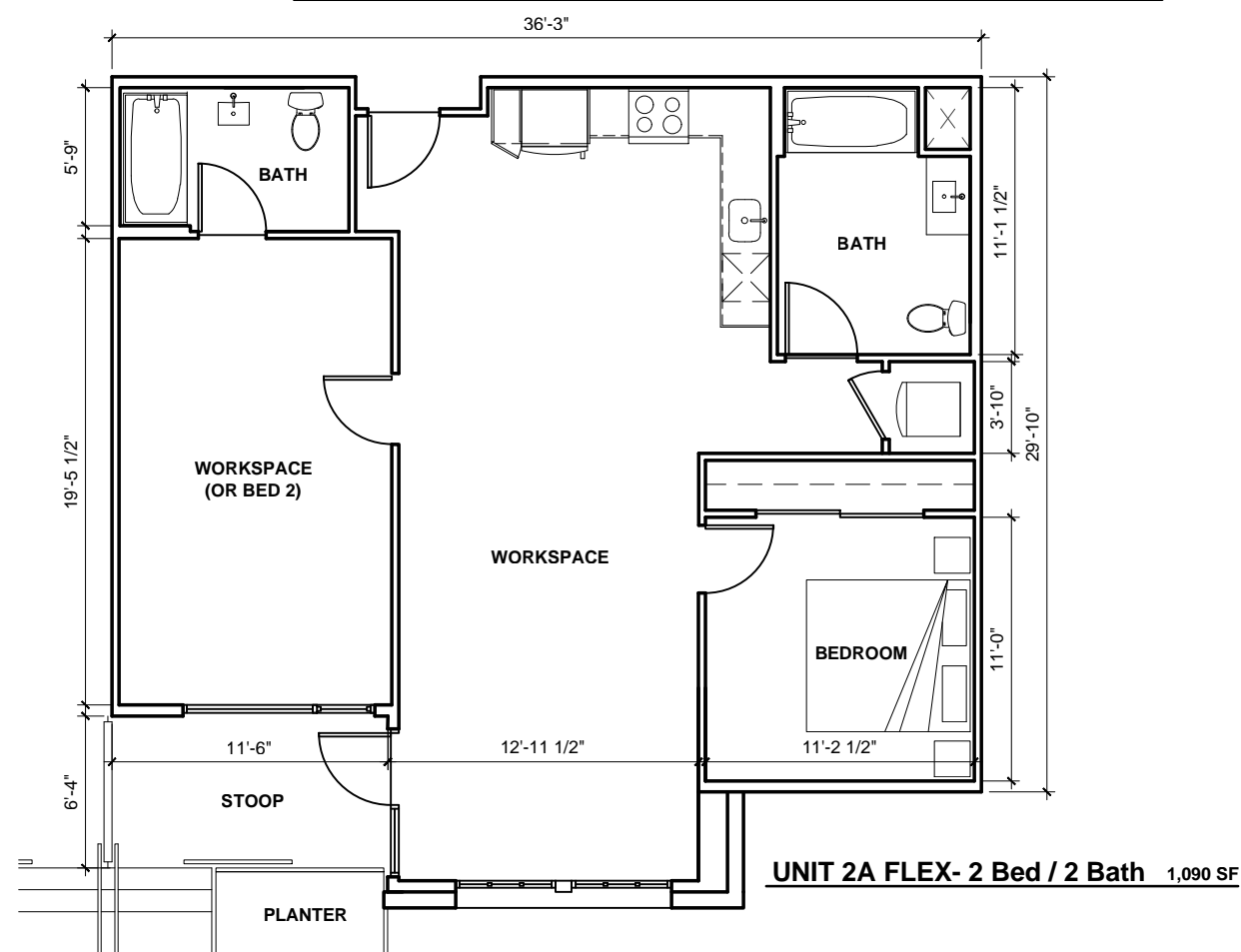
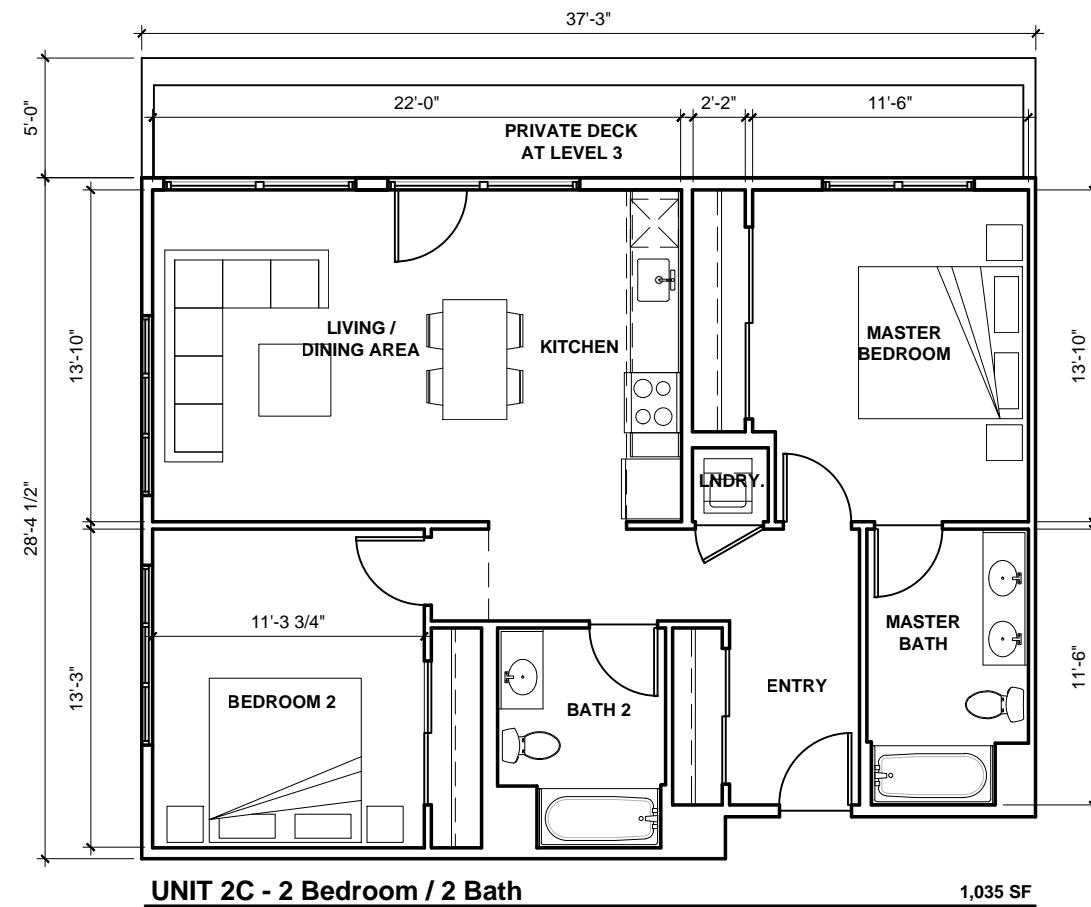
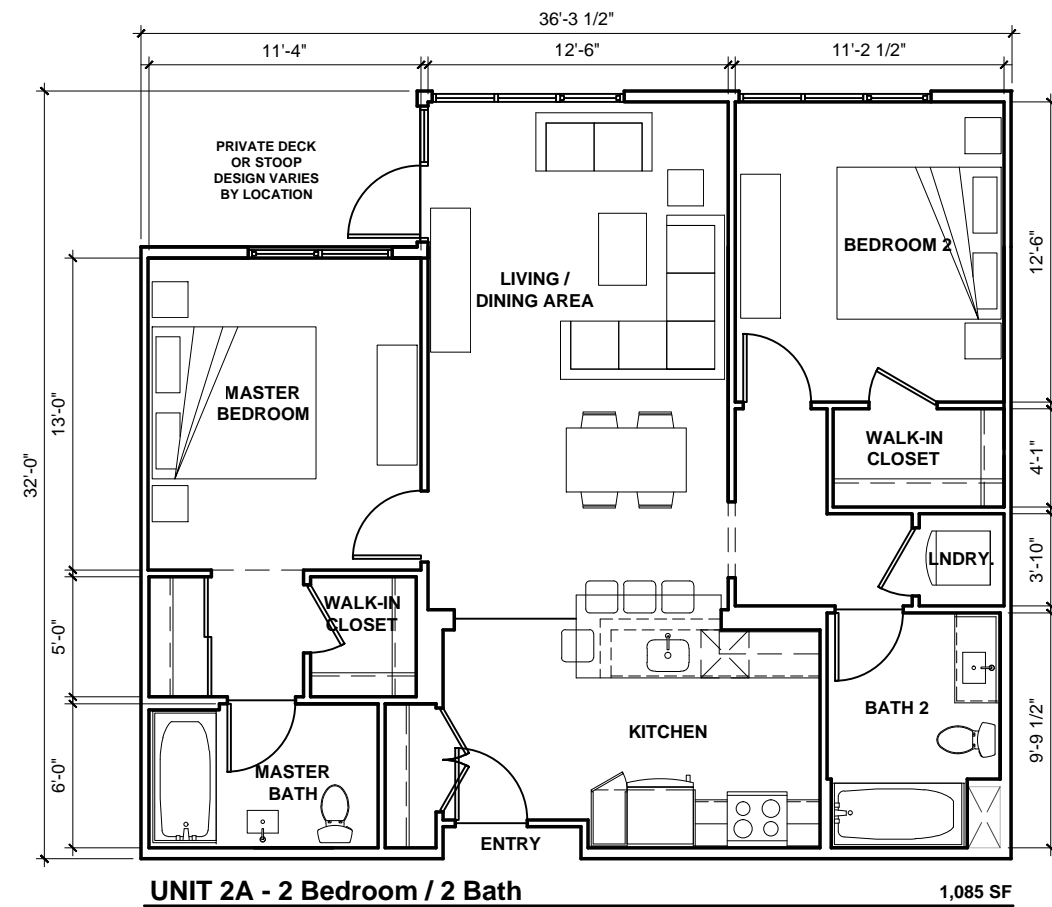


**UNIT 1A - 1 Bedroom / 1 Bath** 749 SF



**UNIT 1B - 1 Bedroom / 1 Bath** 625 SF





CHRISTIANI JOHNSON ARCHITECTS, INC.  
665 Third Street, Suite 350  
San Francisco, CA 94107  
415.243.9484  
cjarchs.com

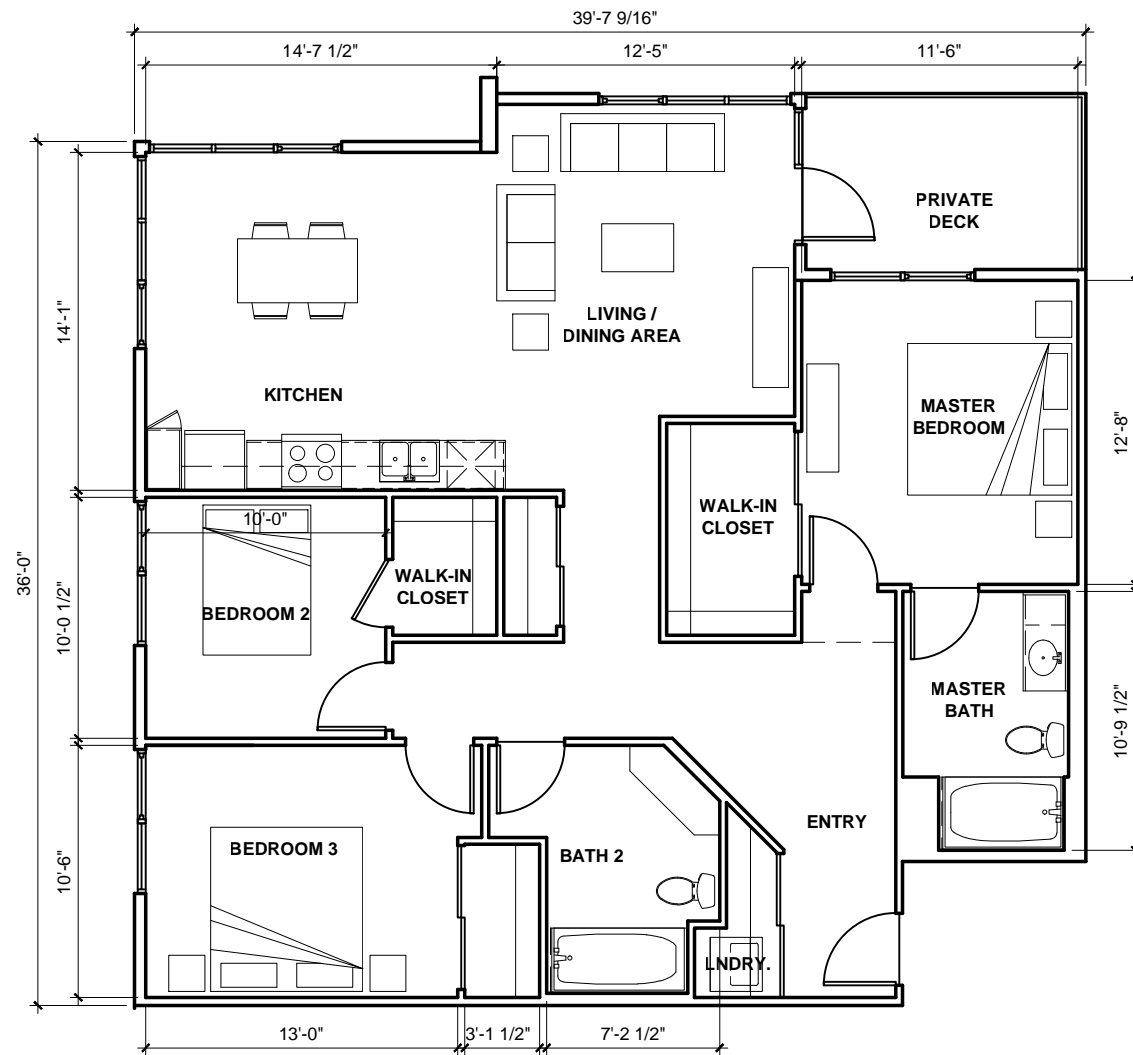
# 1200 17TH STREET

POTRERO PARTNERS, LLC

Scale 1/4" = 1'-0"  
Date 05.12.2016  
Sheet Title  
**UNIT PLANS**  
**2 BEDROOM + FLEX**

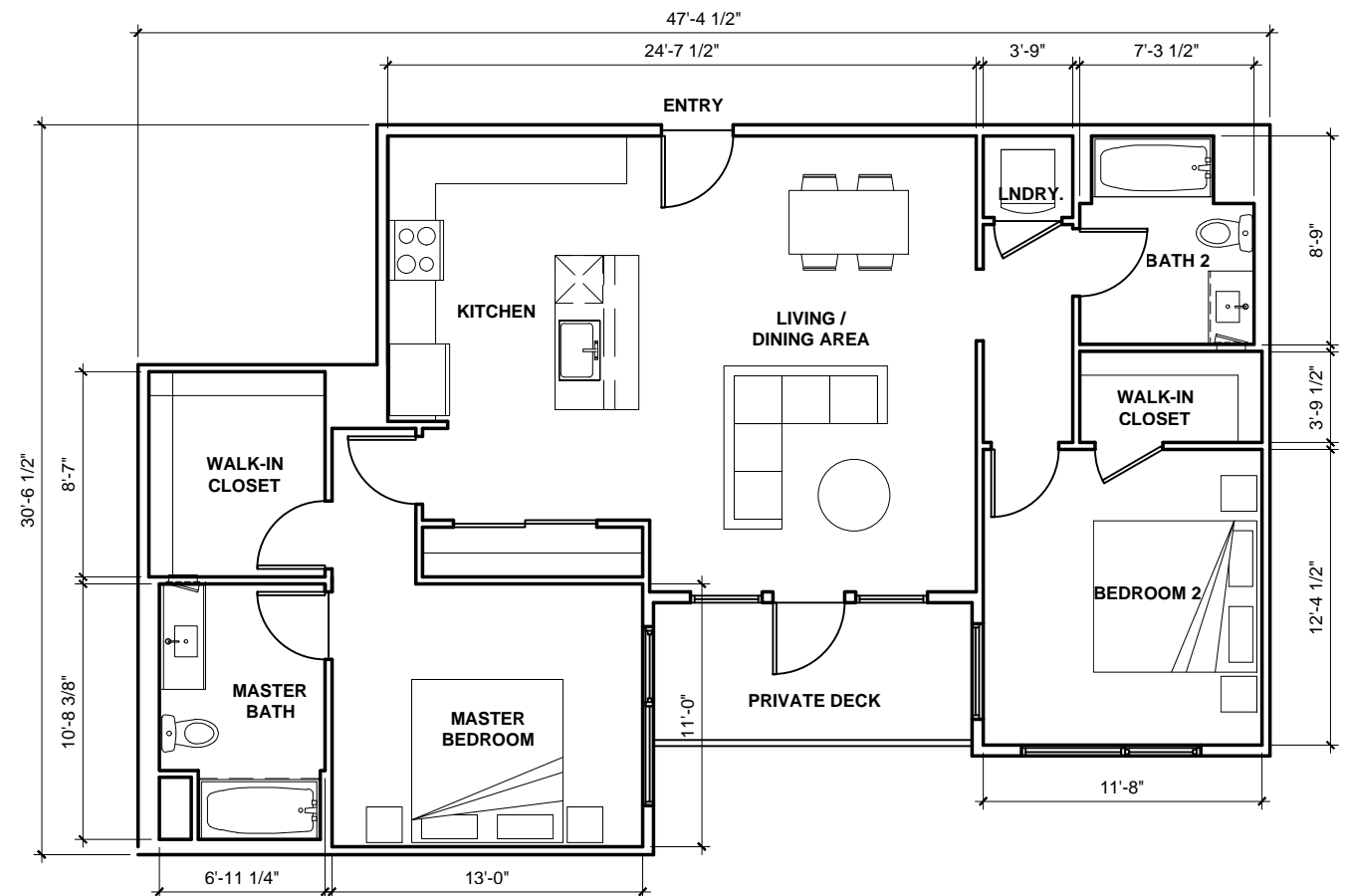
**A4.02**





**UNIT 3A - 3 Bedroom / 2 Bath**

1,310 SF



**UNIT 2D - 2 Bedroom / 2 Bath**

1,154 SF



CHRISTIANI JOHNSON ARCHITECTS, INC.  
665 Third Street, Suite 350  
San Francisco, CA 94107  
415.243.9484  
cjarchs.com

# 1200 17TH STREET

POTRERO PARTNERS, LLC

Scale  
1/4" = 1'-0"

Date  
05.12.2016

Sheet Title  
**UNIT PLANS**  
**2-3 BEDROOMS**

**A4.03**





REAR YARD MODIFICATION PURSUANT TO THE REQUIREMENTS OF SECTION 134 (F).

**REQUIRED:**

PER SECTION 134(A)(1) UMU DISTRICTS ARE REQUIRED TO PROVIDE A REAR YARD DEPTH OF 25% TOTAL LOT DEPTH BUT NO LESS THAN 15'

**PROVIDED:**

THE PROJECT REQUESTS AN EXCEPTION TO THE REAR YARD SETBACK. A 25% REAR YARD ON THE 161'-9" DEEP LOT WOULD BE 40'-5" WITH AN AREA OF 15,485 SF. OPEN SPACE IN EXCESS OF THE REAR YARD REQUIREMENT IS PROVIDED IN A COMBINATION OF PUBLIC OPEN SPACE ALONG THE PEDESTRIAN PROMENADE, TWO RESIDENTIAL COURTYARDS, PRIVATE DECKS AND BALCONIES, AND A COMMON ROOF DECK.

THE PROJECT PROVIDES 169% OF THE OPEN SPACE REQUIRED BY TABLE 843.11.

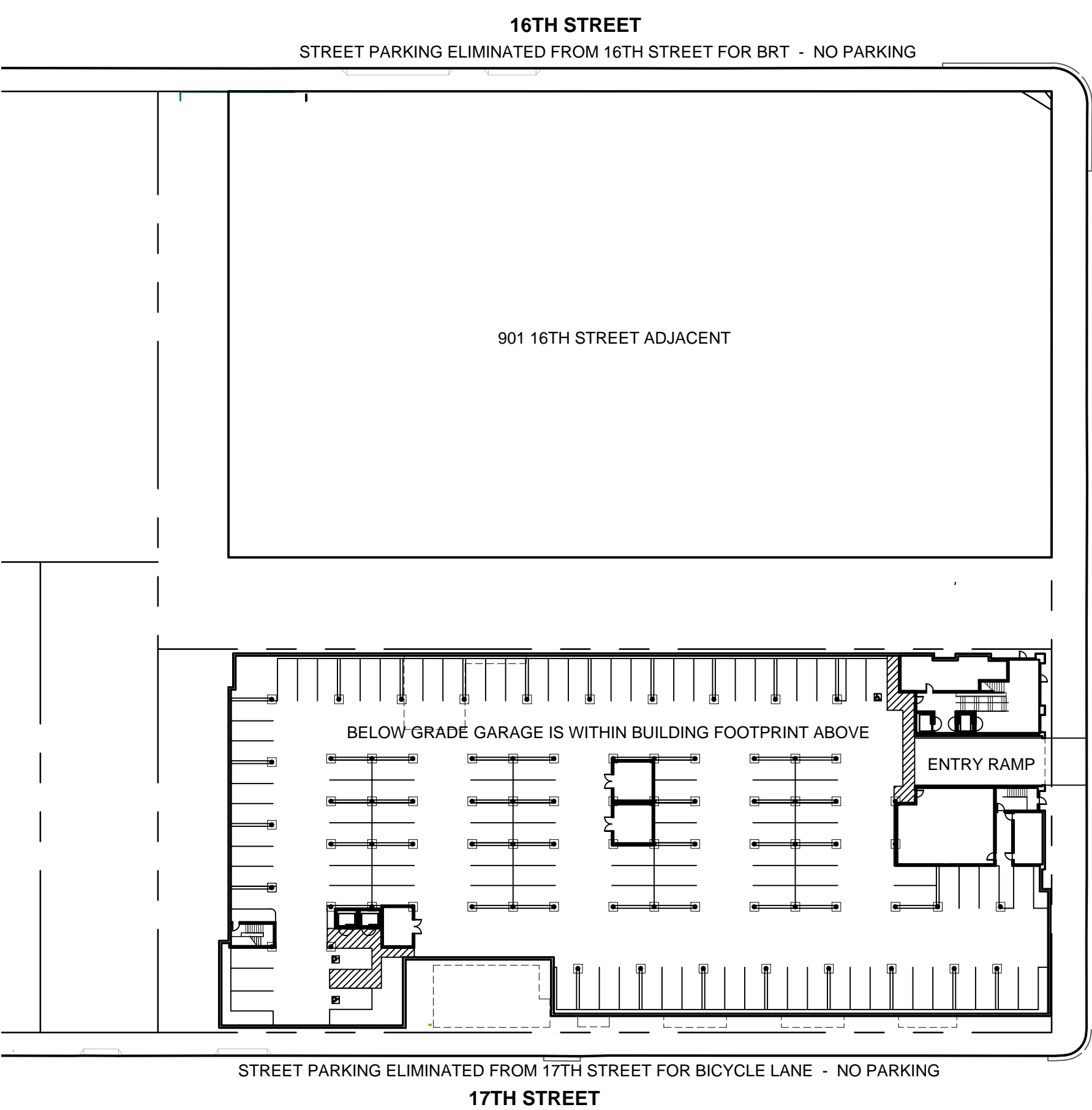
**TOTAL SITE AREA 61,940 SF**

**25% REAR YARD EQUIVALENT 15,485 SF**

**TOTAL OPEN SPACE PROVIDED 18,257 SF**

(REFER TO SHEET 17C FOR MORE DETAIL ON OPEN SPACE PROVIDED)





MODIFICATION REQUEST: OFF STREET PARKING

REQUIRED PER SECTION 151.1:  
Off street parking in UMU Districts is permitted up to .75 spaces per dwelling unit.

|                   |            |
|-------------------|------------|
| 135 units x .75 = | 101 spaces |
| car share =       | 2 spaces   |
| retail parking=   | 8 spaces   |
| Total             | 111 spaces |

PROVIDED:

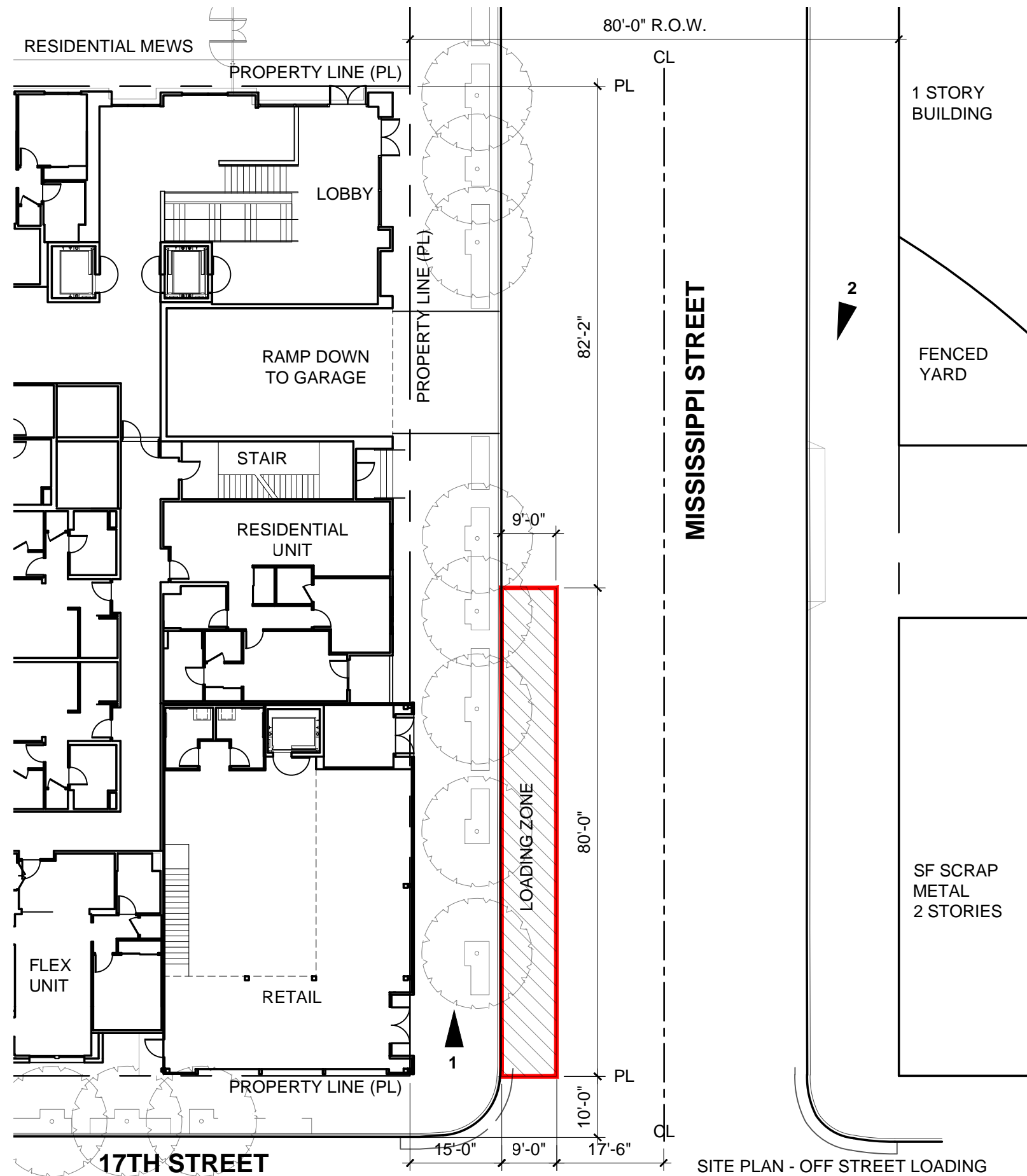
The project requests a modification to the off street parking requirements in section 151.1. The project proposes to provide .75 spaces per dwelling unit for all units with 2 bedrooms and larger and at least 1,000 sf in the underground garage.

1. Street parking will be eliminated on 16th Street to accommodate the BRT.
2. Street parking on 17th street will be eliminated to accommodate a bicycle lane.
3. The 2 and 3 bedroom units are designed with families in mind, and as such require 1 car per unit.
4. The Bottom of the Hill music venue at 17th street and Missouri depends on street parking for their patrons and has expressed concern that the demand for street parking for the new residential development will impact their business.
5. The configuration of the below-ground parking garage is within the building footprint and does not require additional excavation into adjacent landscaped areas to achieve the modified parking request. Parking stackers would require additional excavation depth and are not desired in this site with hazardous soil conditions.

|                            |            |
|----------------------------|------------|
| 79 1 bedroom units x .75 = | 59 spaces  |
| 56 2-3 bedroom units x 1 = | 56 spaces  |
| car share =                | 2 spaces   |
| retail parking =           | 8 spaces   |
| Total                      | 125 spaces |

SITE PLAN - OFF STREET PARKING





SITE PLAN - OFF STREET LOADING

## MODIFICATION REQUEST: OFF STREET LOADING

REQUIRED: PER TABLE 152.1

1 OFF STREET LOADING SPACE REQUIRED FOR RESIDENTIAL USES BETWEEN 100,000-200,000 GSF

THE PROJECT REQUESTS A MODIFICATION TO REQUIREMENT FOR 1 OFF STREET LOADING SPACE IN RESIDENTIAL PROJECTS BETWEEN 100,000 SF AND 200,000 GSF IN SIZE. LOADING WILL BE PROVIDED WITH AN 80' YELLOW CURB ON MISSISSIPPI STREET. THE PROJECT AND STREETScape ALONG MISSISSIPPI WILL BENEFIT BY ELIMINATING THE ADDITIONAL CURB-CUT THAT AN OFF STREET LOADING SPACE WOULD REQUIRE AND ALLOWS MORE OF THE BUILDING FRONTAGE TO BE DEDICATED TO ACTIVE RETAIL, RESIDENTIAL, AND LOBBY USES.

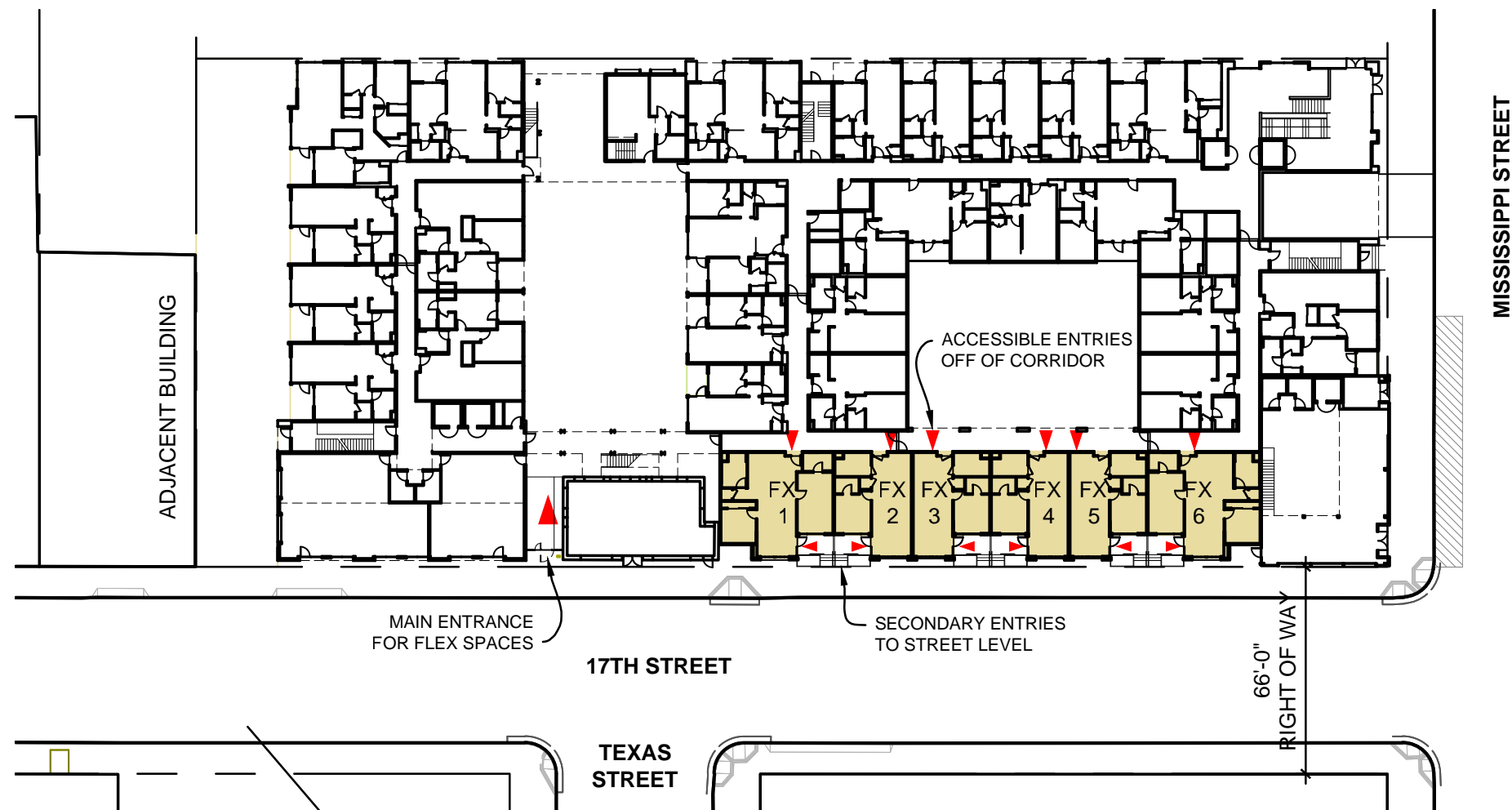
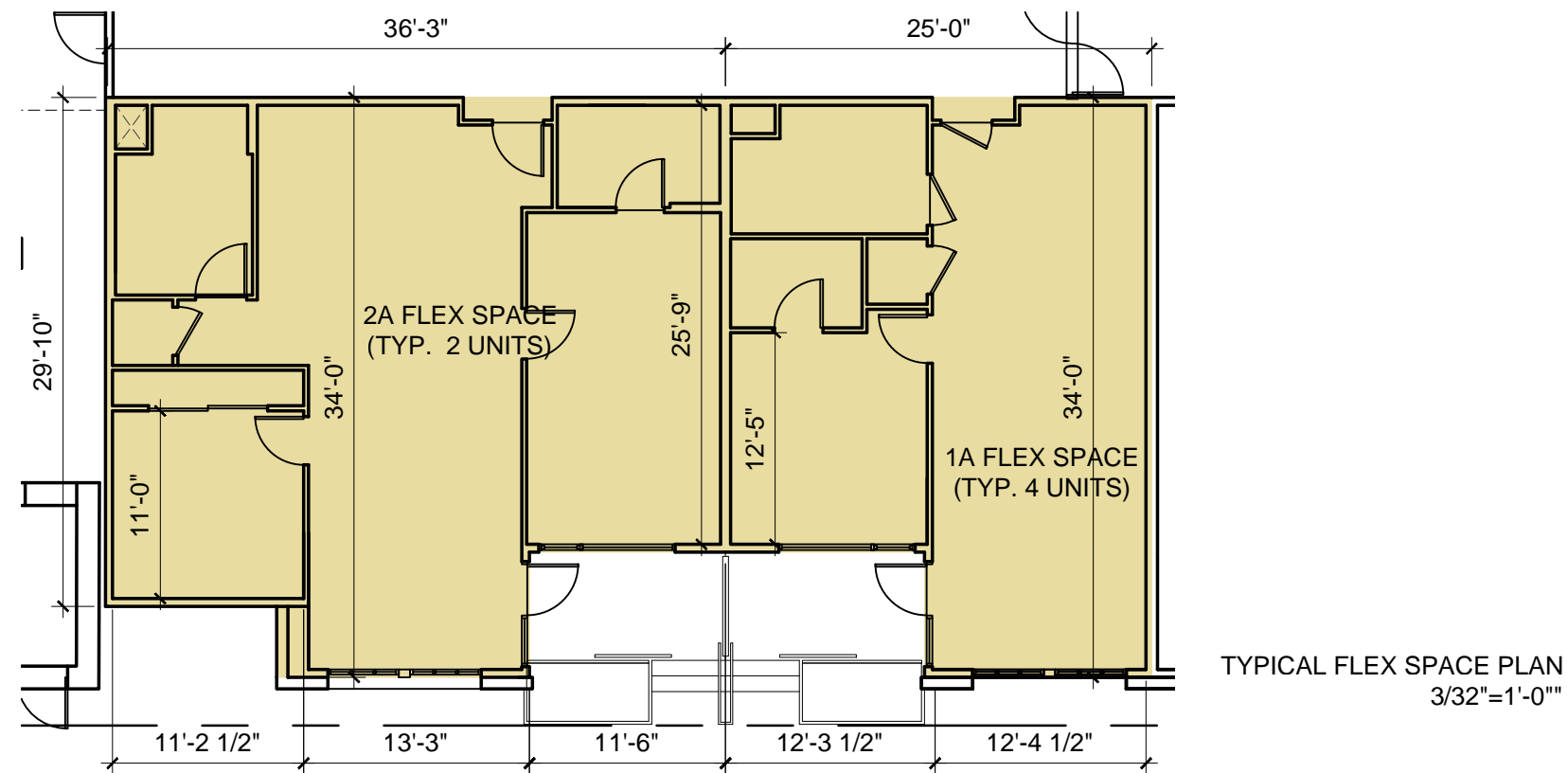


2- PHOTO OF EXISTING SIDEWALK AT PROPOSED LOADING ZONE



1- PHOTO OF EXISTING SIDEWALK AT PROPOSED LOADING ZONE





# FLEXIBLE UNITS MODIFICATION PER SECTION 329 (d) (10):

## **REQUIRED:**

(10) Flexible Units: Modification of the accessory use provisions of Section 803.3(b)(1)(c) for Dwelling Units. Dwelling Units modified under this Subsection shall continue to be considered Dwelling Units for the purposes of this Code and shall be subject to all such applicable controls and fees. Additionally, any building that receives a modification pursuant to this Subsection shall (i) have appropriately designed street frontages to accommodate both residential and modified accessory uses and (ii) obtain comment on the proposed modification from other relevant agencies prior to the Planning Commission hearing, including the Fire Department and Department of Building Inspection. Modifications are subject to the following:

- (A) A modification may only be granted for the ground floor portion of Dwelling Units that front on a street with a width equal to or greater than 40 feet.
- (B) The accessory use may only include those uses permitted as of right at the subject property. However, uses permitted in any unit obtaining an accessory use modification may be further limited by the Planning Commission.
- (C) The Planning Commission may grant exceptions to the size of the accessory use, type and number of employees, and signage restrictions of the applicable accessory use controls.

## **PROVIDED:**

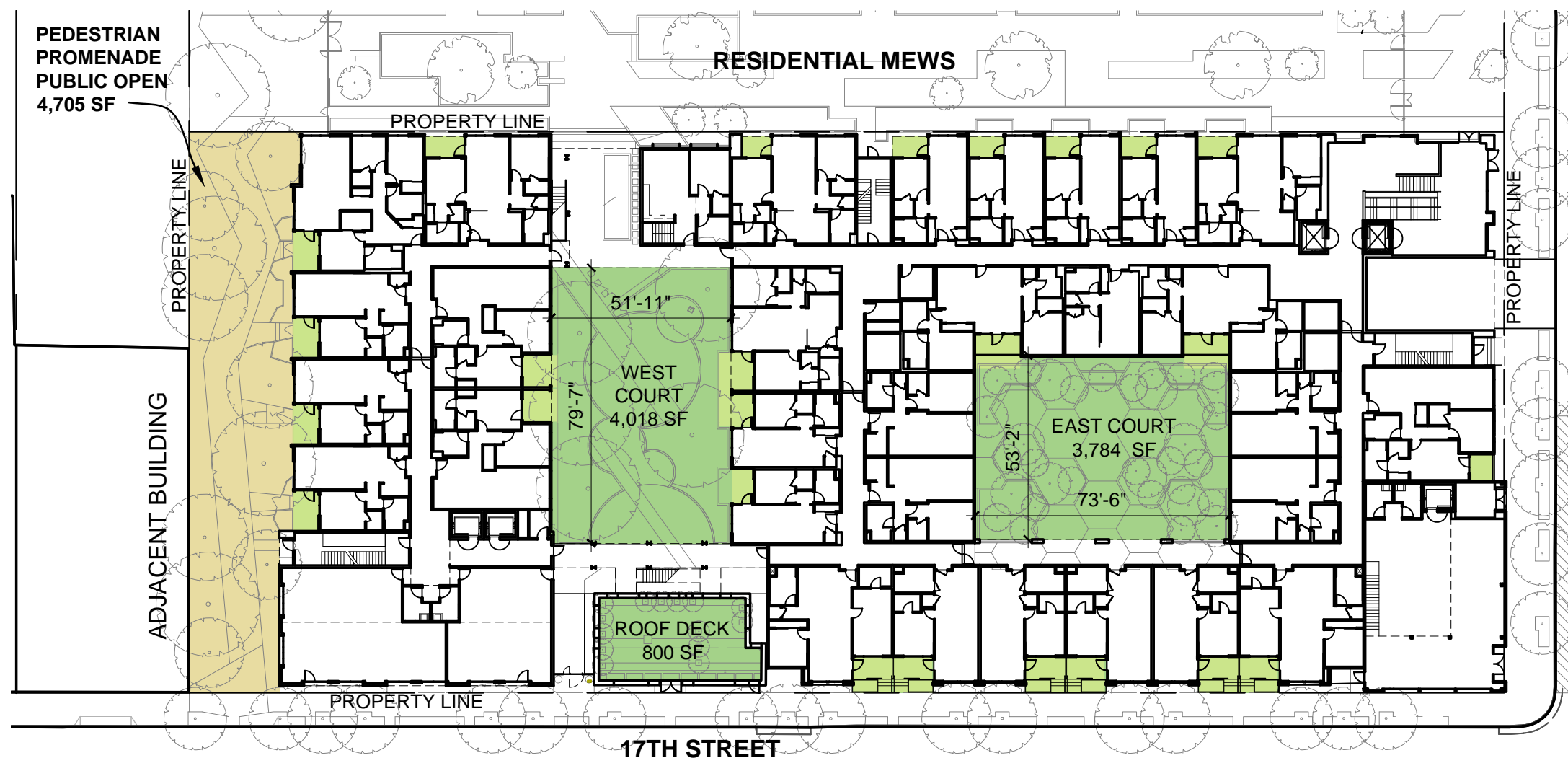
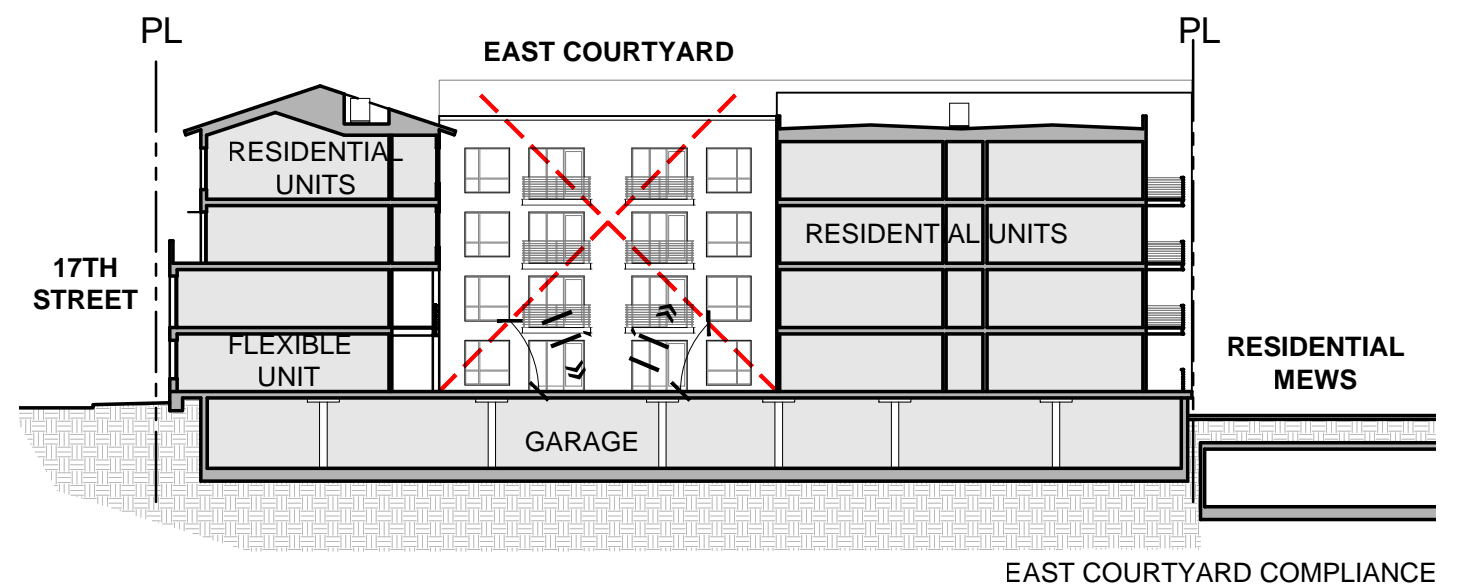
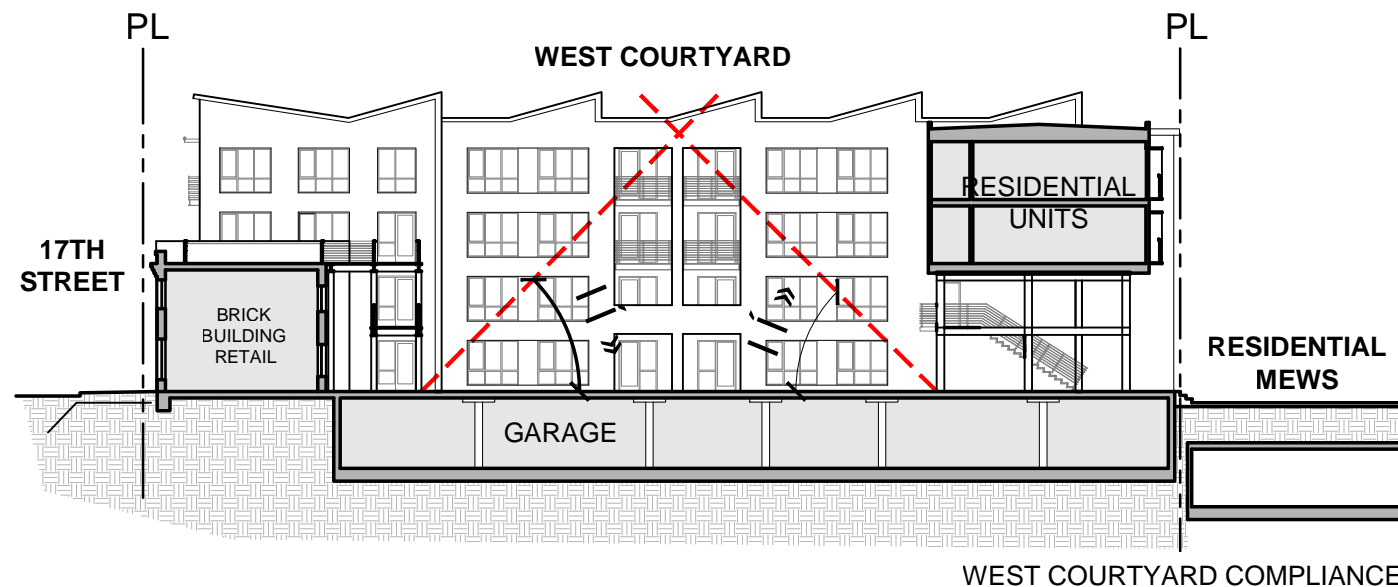
THE PROJECT IS PROVIDING 6 FLEX UNITS , AT GRADE, ALONG 17TH STREET. THE FLEX UNITS WILL PROVIDE OPEN FLOOR PLANS TO ACCOMMODATE COMMERCIAL USE IN CONJUNCTION WITH LIVING SPACE.

17TH STREET IS A 66' RIGHT OF WAY AND COMPLIES WITH SECTION 329 (d)(10).

ALL FLEX UNITS HAVE ACCESSIBLE ENTRIES AND SECONDARY STREET ACCESS.

LOCATION OF PROPOSED FLEX UNITS  
1"=50'-0"





#### OPEN SPACE CALCULATIONS:

PER TABLE 843.11, 80 SF OF OPEN SPACE IS REQUIRED PER UNIT OR 54 SF PER UNIT IF THE OPEN SPACE IS PUBLICLY ACCESSIBLE. ONE SF OF OPEN SPACE IS REQUIRED PER 250 SF OF RETAIL AREA. TABLE 135.3 DESCRIBES SIZE REQUIREMENTS FOR PRIVATE AND COMMON OPEN SPACE. PROJECT COMPLIES WITH OPEN SPACE REQUIREMENTS. REFER TO ADJACENT OPEN SPACE DIAGRAMS FOR MORE INFORMATION.

NOTE THAT THE RESIDENTIAL MEWS IS ADDITIONAL OPEN SPACE FOR TO RESIDENTS NOT INCLUDED IN THE CALCULATION FOR 1200 17TH STREET. THE MEWS IS INCLUDED AS PART OF THE 901 16TH STREET PROJECT.

18,257 sf provided / 10,800 sf required

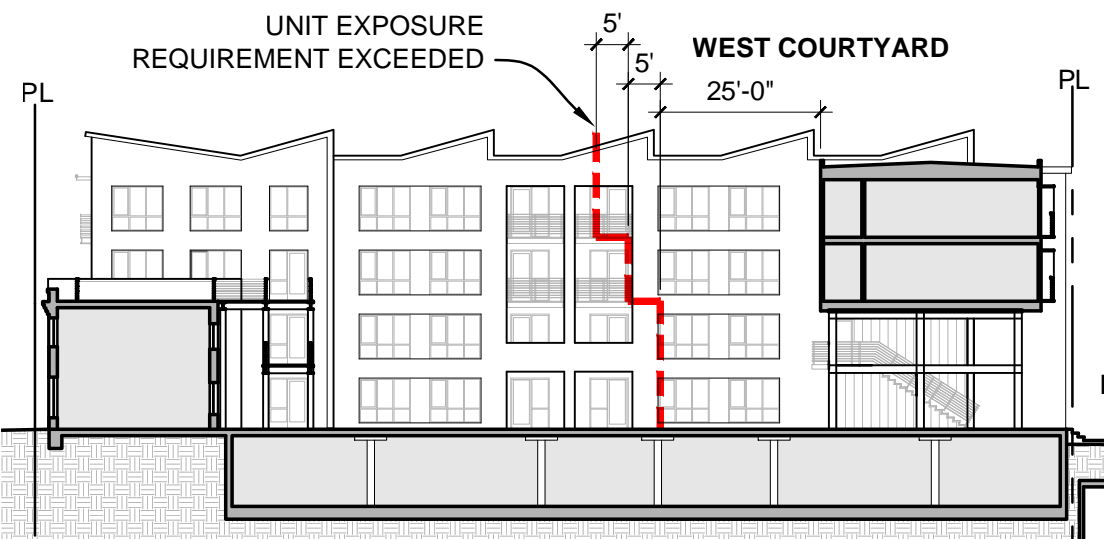
**169% REQUIRED OPEN SPACE PROVIDED**

OPEN SPACE PROVIDED:

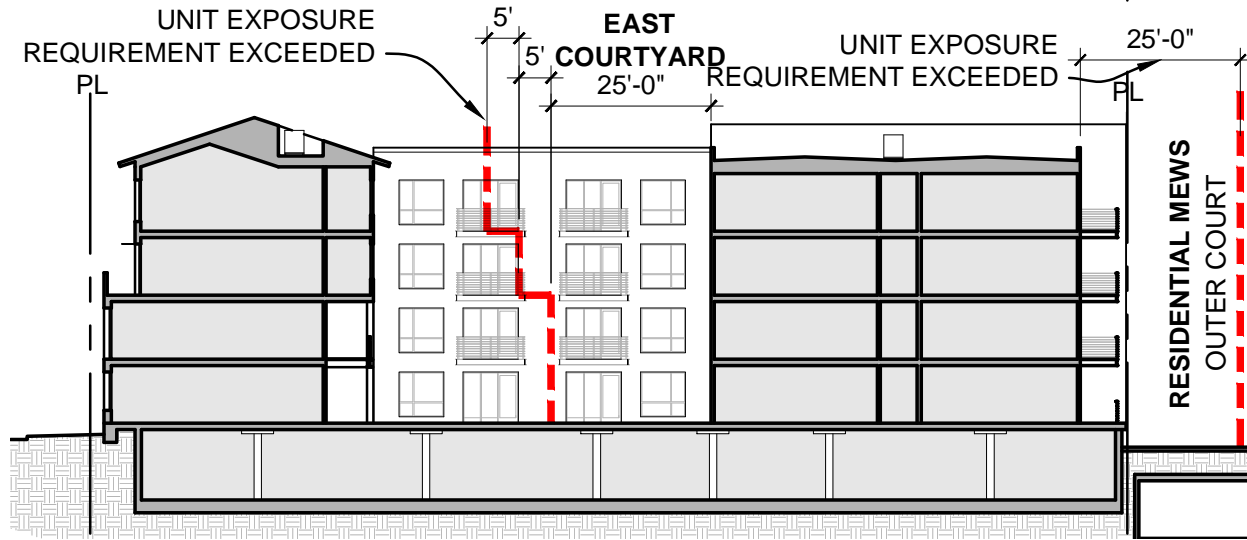
|   |                  |
|---|------------------|
| Pedestrian Promenade: Public Open Space | 4,705 sf         |
| Private Patios @ Ground                 | 1,350 sf         |
| Private Decks Levels 2-4                | 3,600 sf         |
| East Courtyard                          | 3,784 sf         |
| West Courtyard                          | 4,018 sf         |
| Roof Deck Level 3                       | 800 sf           |
| <b>Total Open Space Provided</b>        | <b>18,257 sf</b> |

|   |          |
|---|----------|
| <span style="display:inline-block; width:15px; height:15px; background-color:yellow; border:1px solid black;"></span> = PUBLIC OPEN SPACE       | 4,705 SF |
| <span style="display:inline-block; width:15px; height:15px; background-color:lightgreen; border:1px solid black;"></span> = COMMON OPEN SPACE   | 8,602 SF |
| <span style="display:inline-block; width:15px; height:15px; background-color:lightyellow; border:1px solid black;"></span> = PRIVATE OPEN SPACE | 4,950 SF |

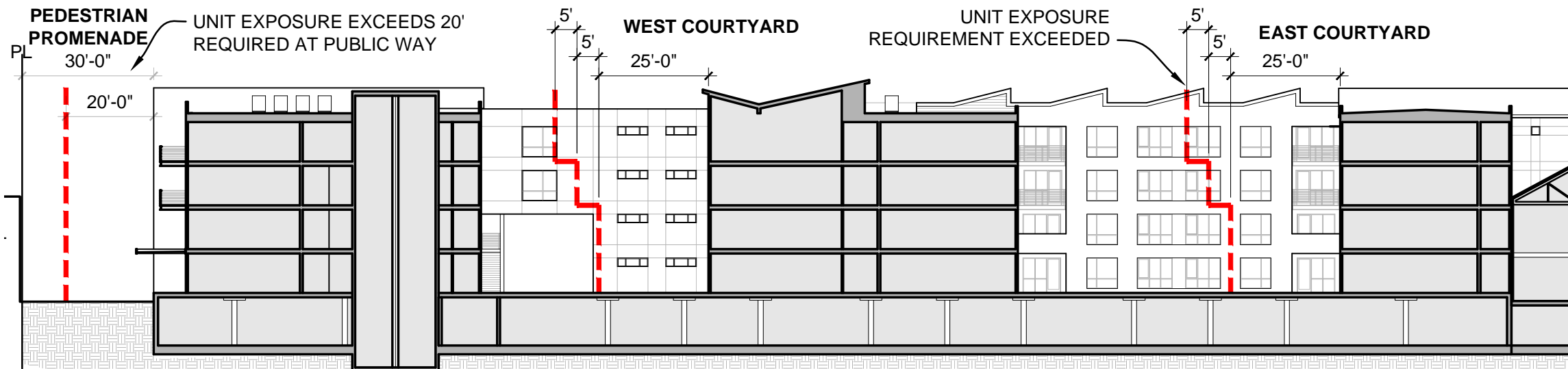




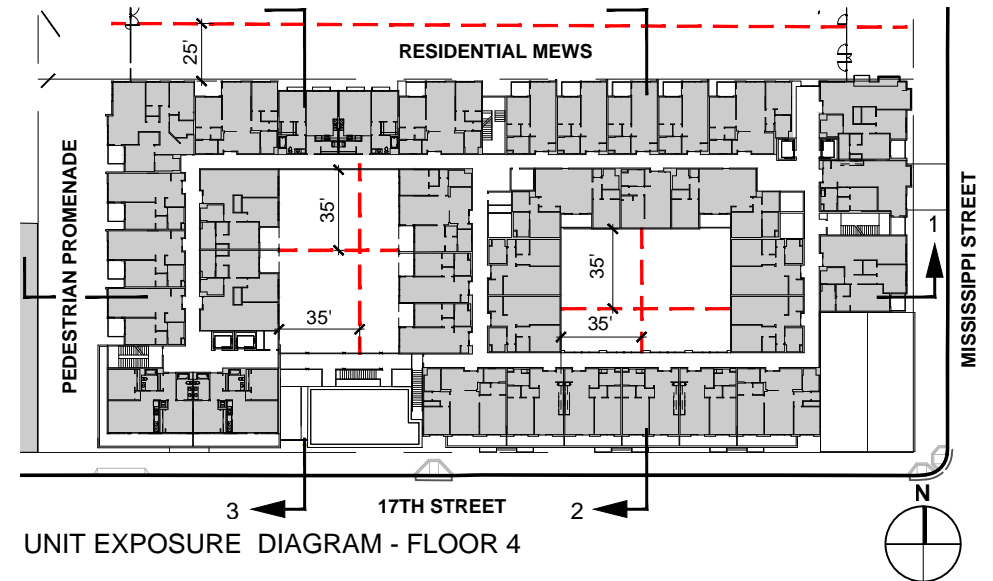
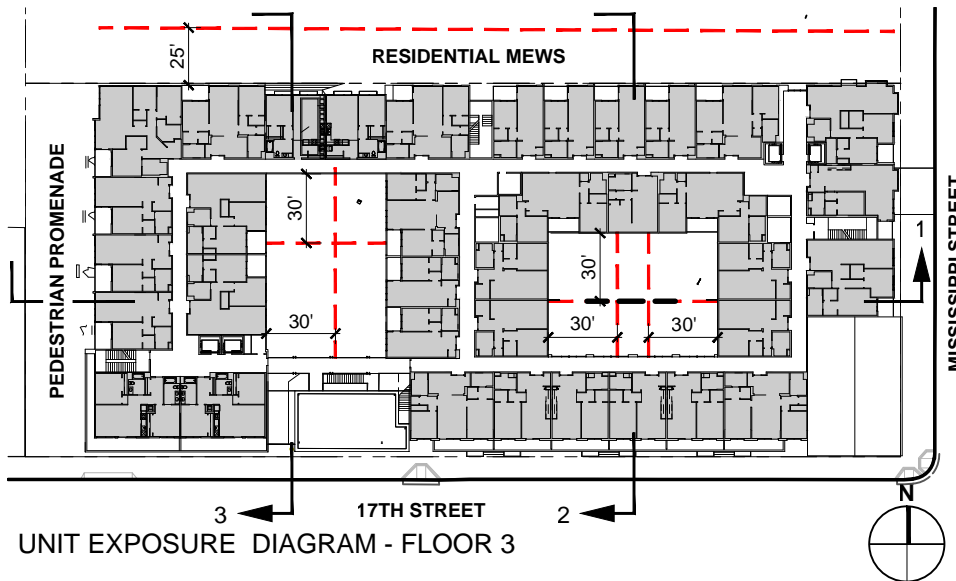
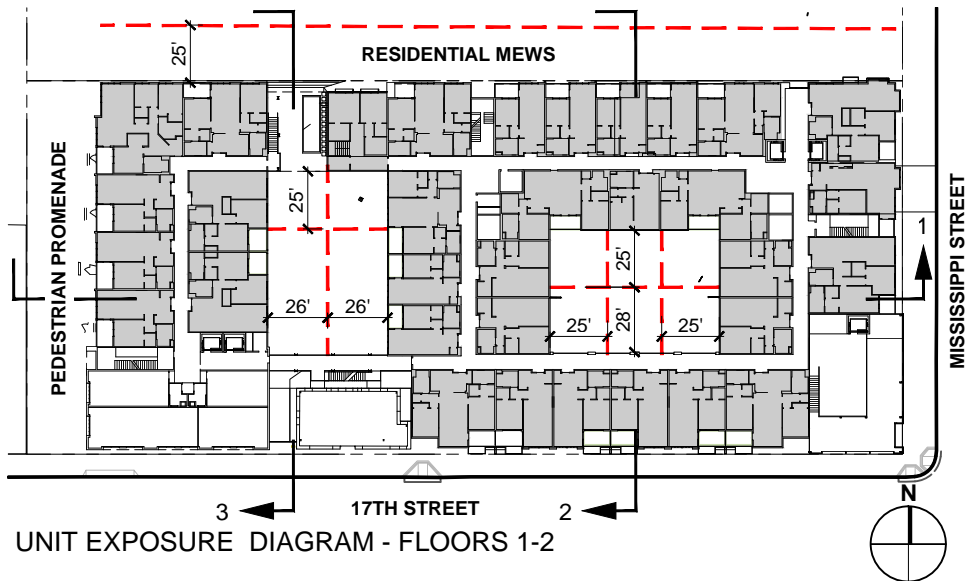
2- SITE SECTION - NORTH SOUTH COMPLIANCE, WEST COURTYARD



3- SITE SECTION - NORTH SOUTH COMPLIANCE, EAST COURTYARD



1- SITE SECTION - EAST WEST COMPLIANCE



# UNIT EXPOSURE REQUIREMENT:

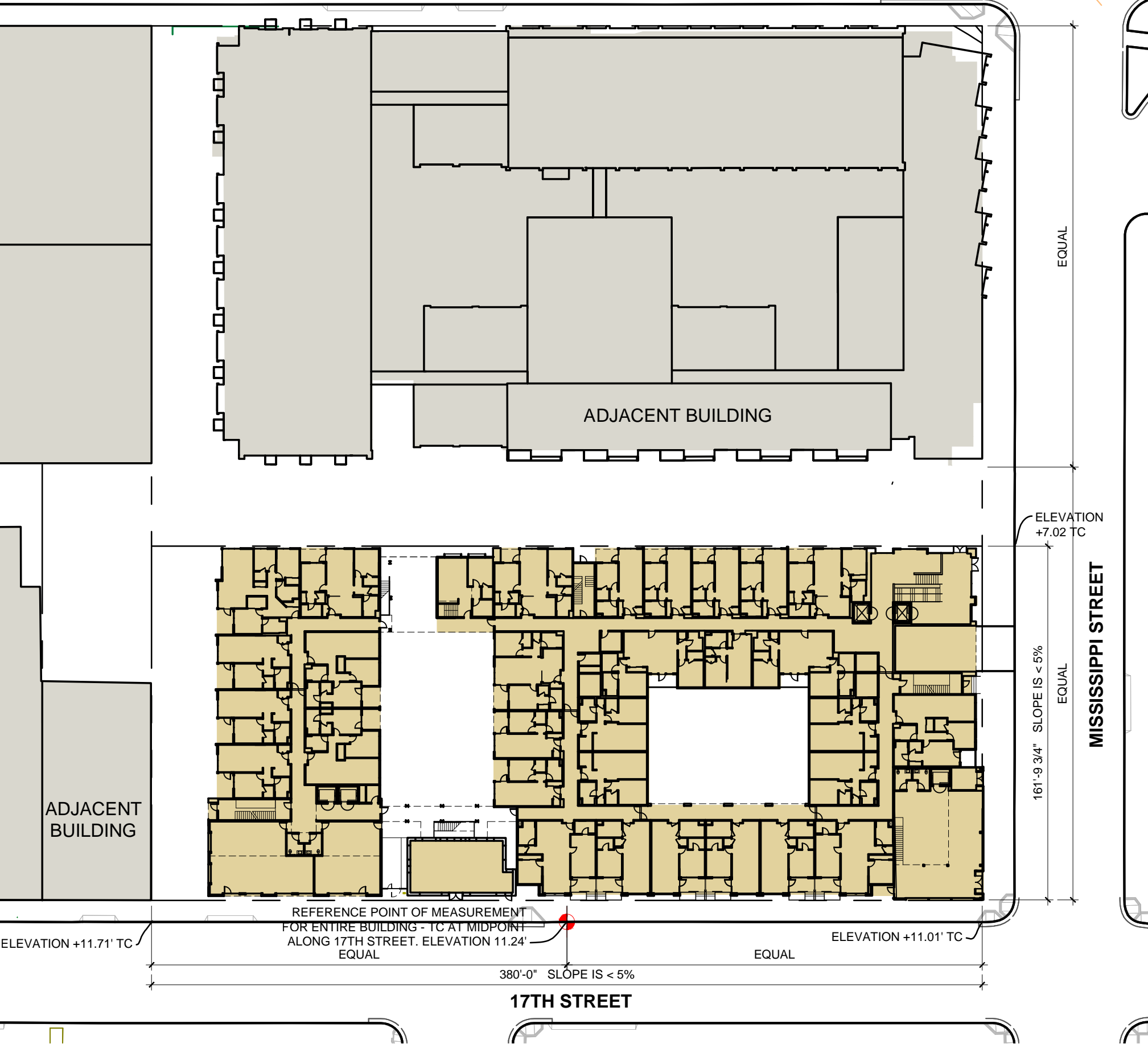
Per Table Section 140, ALL DWELLING UNITS IN ALL USE DISTRICTS TO FACE ON AN OPEN AREA.

(a) Windows (as defined by Section 504 of the San Francisco Housing Code) of at least one room that meets the 120-square-foot minimum superficial floor area requirement of Section 503 of the Housing Code shall face directly on an open area of one of the following types:

(1) Side yard at least 25 feet in width, or rear yard meeting the requirements of this Code; provided, that if such windows are on an outer court whose width is less than 25 feet, the depth of such court shall be no greater than its width; or

(2) Open area between separate buildings on the same lot (which is unobstructed and is no less than 25 feet in every horizontal dimension for the floor at which the Dwelling Unit in question is located and the floor immediately above it, with an increase of five feet in every horizontal dimension at each subsequent floor.





BUILDING HEIGHT COMPLIANCE

Required: SEC 260 (a)(1)(B):  
Where the lot is level with or slopes downward from a street at the centerline of the building or building step, such point shall be taken at curb level on such a street. This point shall be used for height measurement only for a lot depth not extending beyond a line 100 feet from and parallel to such street, or beyond a line equidistant between such street and the street on the opposite side of the block, whichever depth is greater.

SEC 260 (a)(3):  
In cases where the height limit is 65 feet or less and a street from which height measurements are made slopes laterally along the lot, or the ground slopes laterally on a lot that also slopes upward from the street, there shall be a maximum width for the portion of the building or structure that may be measured from a single point at curb or ground level, according to the definition of "height," as specified in the following table. These requirements shall not apply to any property to which the bulk limitations in Section 270 of this Code are applicable.

Table 260:  
For sites with a slope of 5% or less, there is no requirement for maximum portion of the building that may be measured from a single point.

- Provided:
- The height for the entire project at 1200 17th Street is measured from one datum point. The datum is at the top of existing curb at the midpoint of the 17th street
1. V@Á aa ~{ Á||, aa^A@a @A|A@a^A|Á
  2. Per Section 260.a.2, height is measured to the top of a flat roof or the average height of a sloped roof.
  3. Per Section 260 (b.1.A+B), Mechanical equipment, elevator and stair
  4. Ú^|Á^&q } Á qÁEDÚaa ^o Á æ Ácc} áÁ qaa| Á^@Á@a @Á æ
  5. All parts of the building as propose are lower than the maximum height permitted.

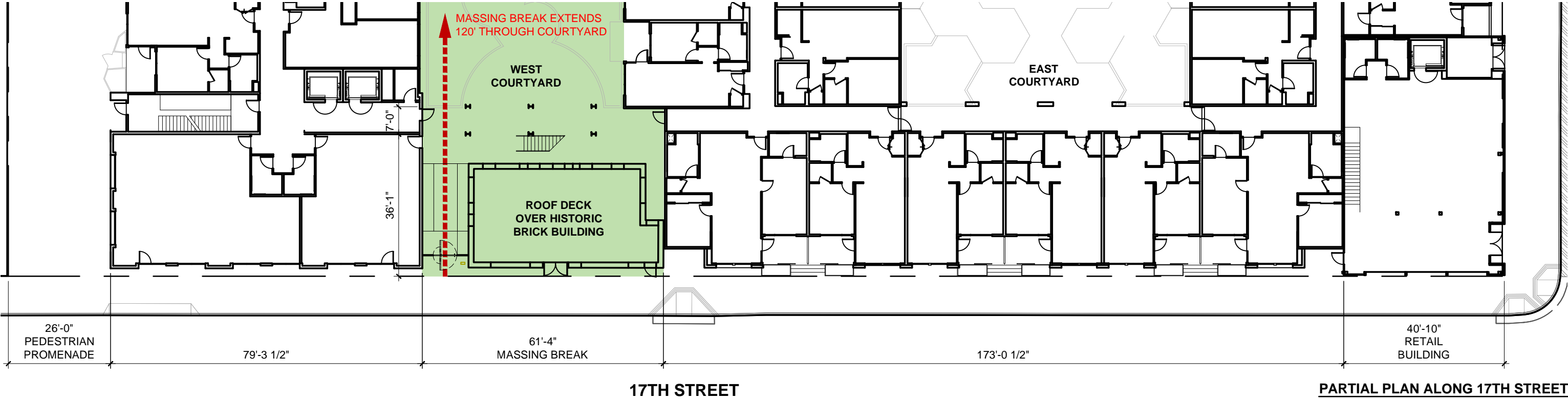
HEIGHT MEASUREMENT - 17TH STREET PLAN



THE REQUIRED BREAK IS PROVIDED AT THE EXISTING BRICK BUILDING TO REMAIN ALONG 17TH STREET.

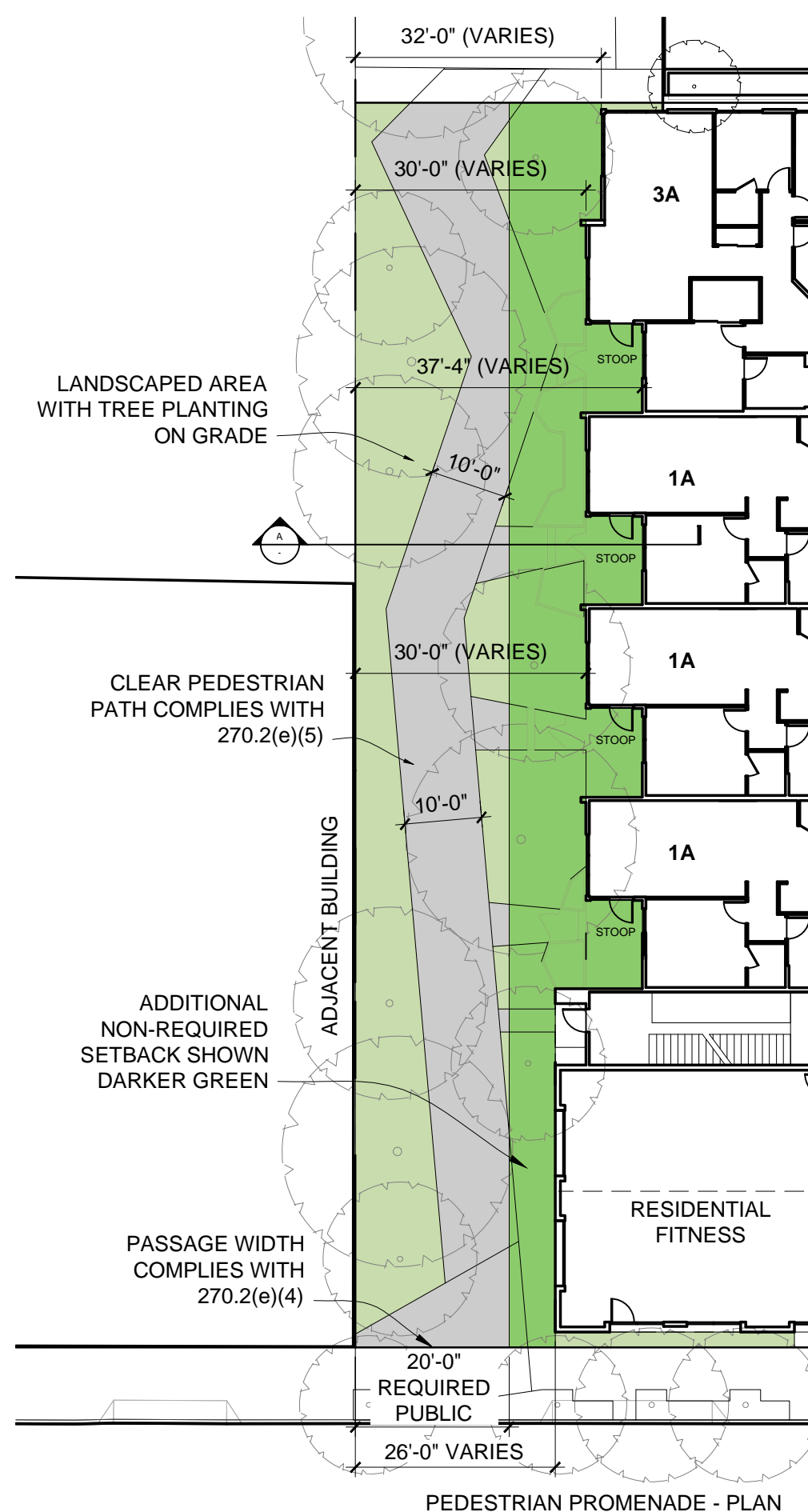
**BREAK PROVIDED IS 61'-4" WIDE AND 120' LONG AND EXCEEDS THE CODE REQUIREMENT**

SECTION 270.1: A HORIZONTAL MASS REDUCTION OF AT LEAST 30' IN WIDTH AND 60' IN DEPTH SHALL BE PROVIDED FOR EVERY BUILDING MORE THAN 200' LONG (RESULTING BUILDING FACADES SHOULD BE LESS THAN 200'). BLOCK BREAK SHOULD BEGIN NOT HIGHER THAN 25' ABOVE GRADE AND SHALL EXTEND TO THE SKY.

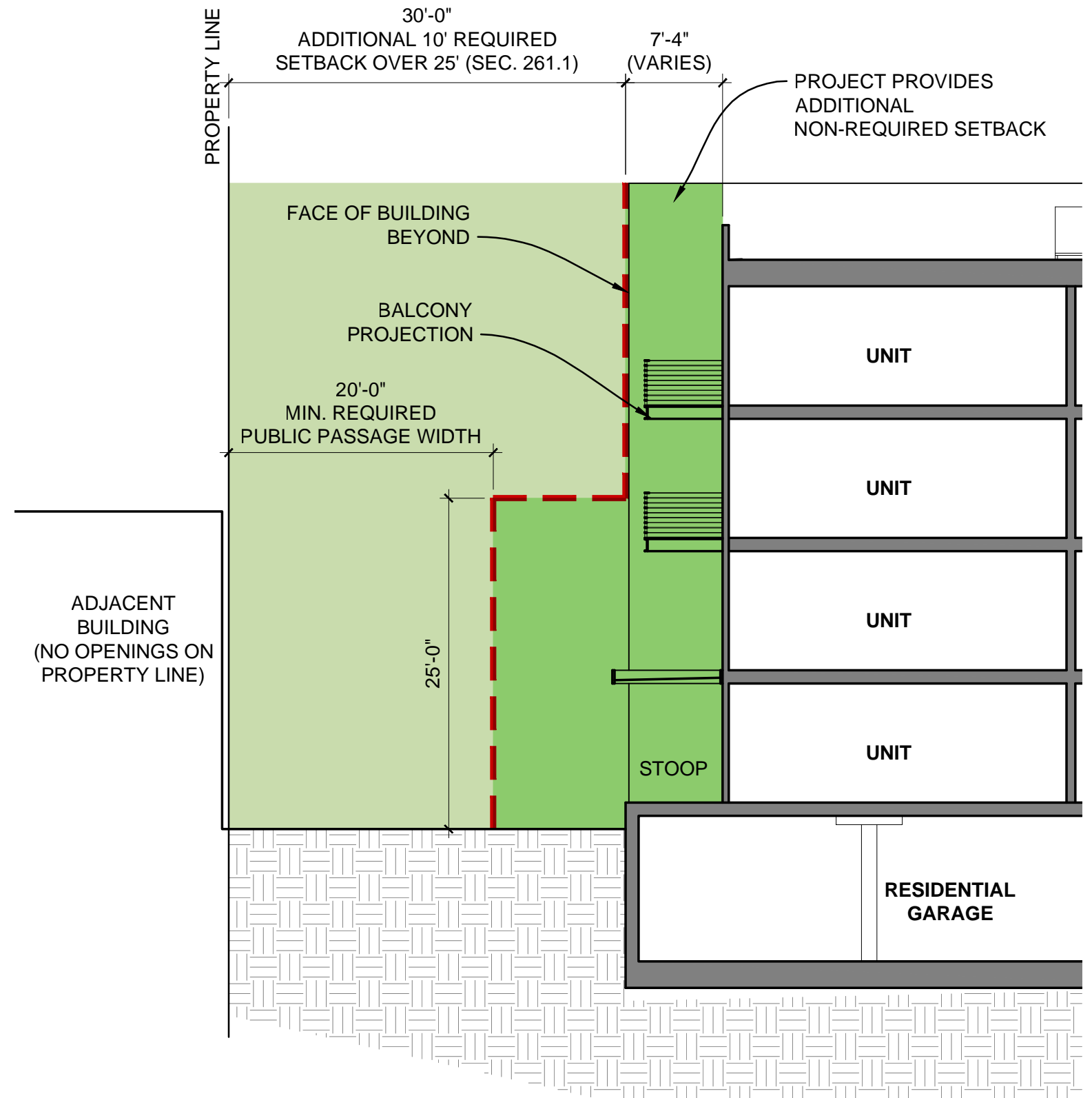


**1200 17TH STREET**  
**POTRERO PARTNERS, LLC**





PEDESTRIAN PROMENADE - PLAN



PEDESTRIAN PROMENADE - SECTION A



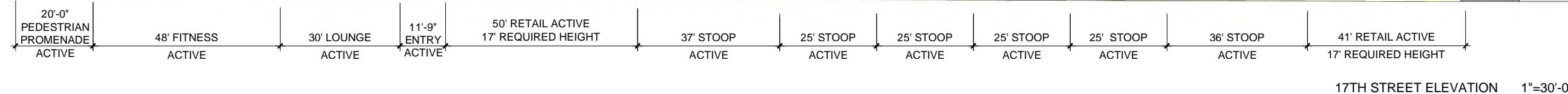
CHRISTIANI JOHNSON ARCHITECTS, INC.  
665 Third Street, Suite 350  
San Francisco, CA 94107  
415.243.9484  
cjarchs.com

# 1200 17TH STREET

POTRERO PARTNERS, LLC

Scale NTS Date 05.12.2016  
Sheet Title  
MID BLOCK PASSAGE COMPLIANCE

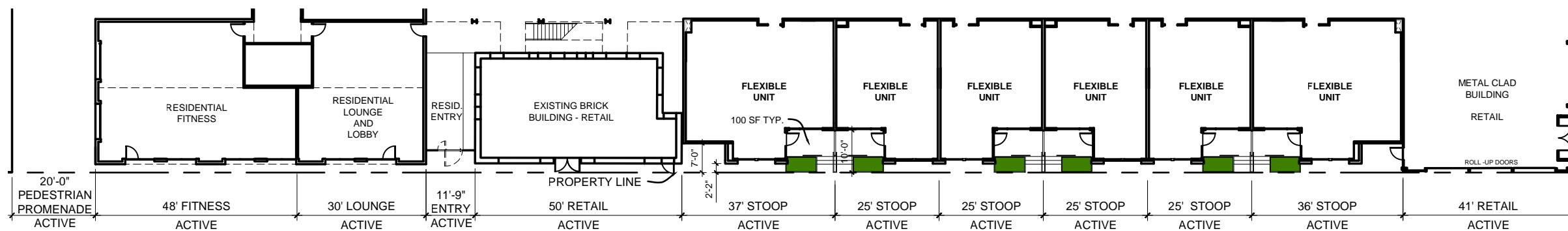




ALL ACTIVE USES PROVIDE DIRECT INDIVIDUAL PEDESTRIAN ACCESS TO A SIDEWALK PER CODE SEC 145.1(C)(3),

100% OF THE BUILDING FRONTAGE ON 17TH STREET CONTAINS AN ACTIVE USE.

GLAZING IS CLEAR, VISUALLY TRANSPARENT.



REFER TO SHEET 17J FOR MORE DETAIL ON STOOP DESIGN



RETAIL AT EXISTING BRICK BUILDING



RETAIL AT BRICK BUILDING AND RESIDENTIAL STOOPS



ACTIVE RETAIL AT 17TH AND MISSISSIPPI STREETS



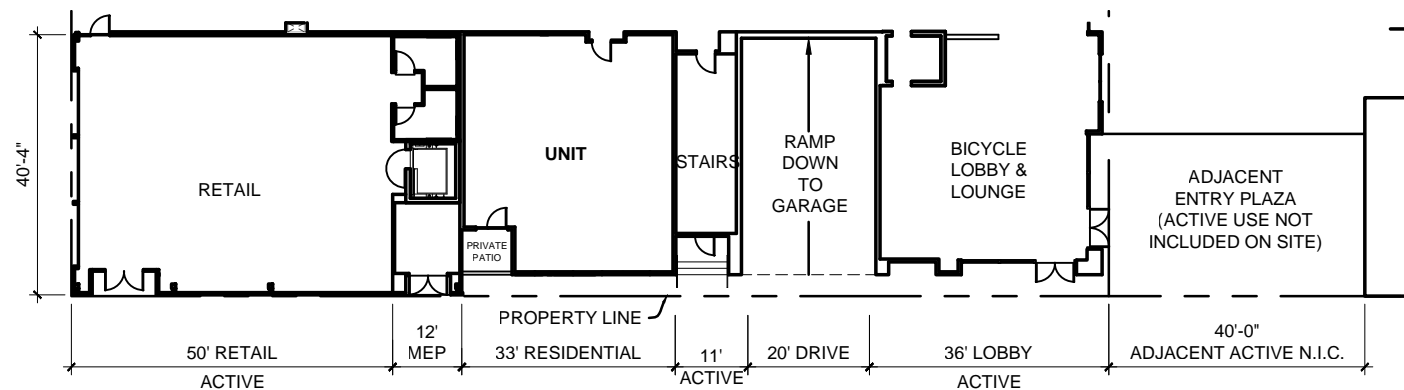


MISSISSIPPI STREET ELEVATION 1"=30'-0"

ALL ACTIVE USES PROVIDE DIRECT INDIVIDUAL PEDESTRIAN ACCESS TO A SIDEWALK PER CODE SEC 145.1(C)(3),

TOTAL FRONTAGE = 162'  
 ACTIVE 50'+11'+36'= 97'

60% ACTIVE STREET FRONTAGE AT MISSISSIPPI STREET



MISSISSIPPI GROUND LEVEL PLAN 1"=30'-0"



ACTIVE USES ALONG MISSISSIPPI STREET



CHRISTIANI JOHNSON ARCHITECTS, INC.  
 665 Third Street, Suite 350  
 San Francisco, CA 94107  
 415.243.9484  
 cjarch.com

**1200 17TH STREET**  
 POTRERO PARTNERS, LLC

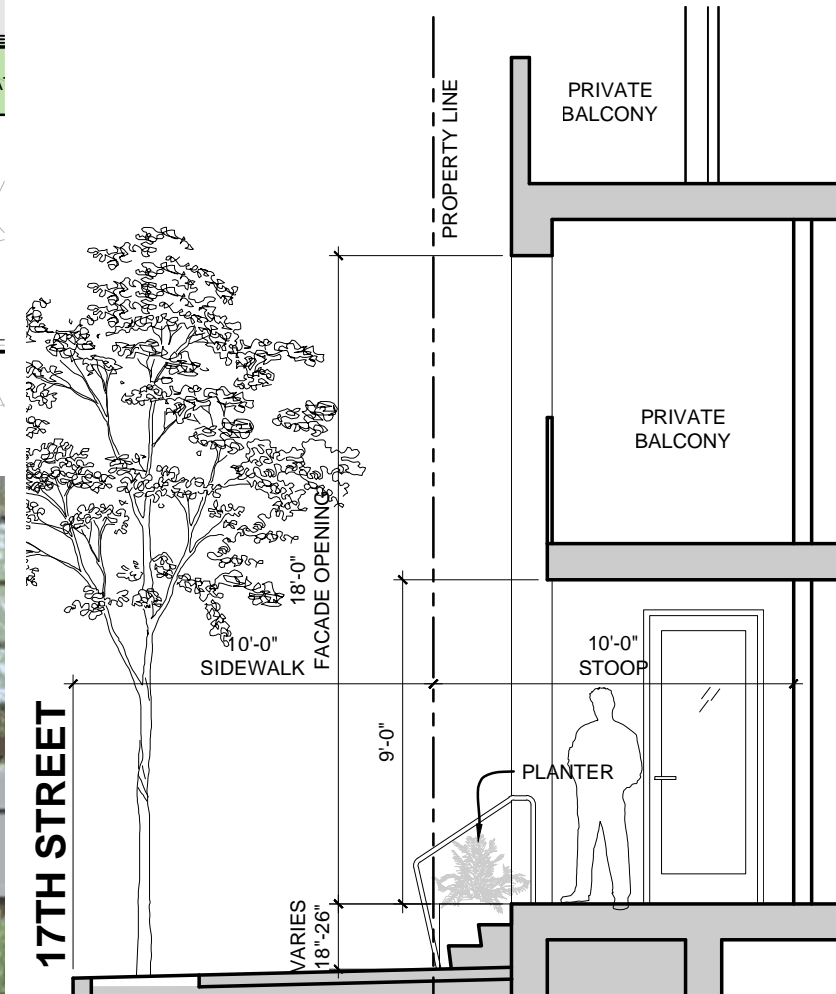
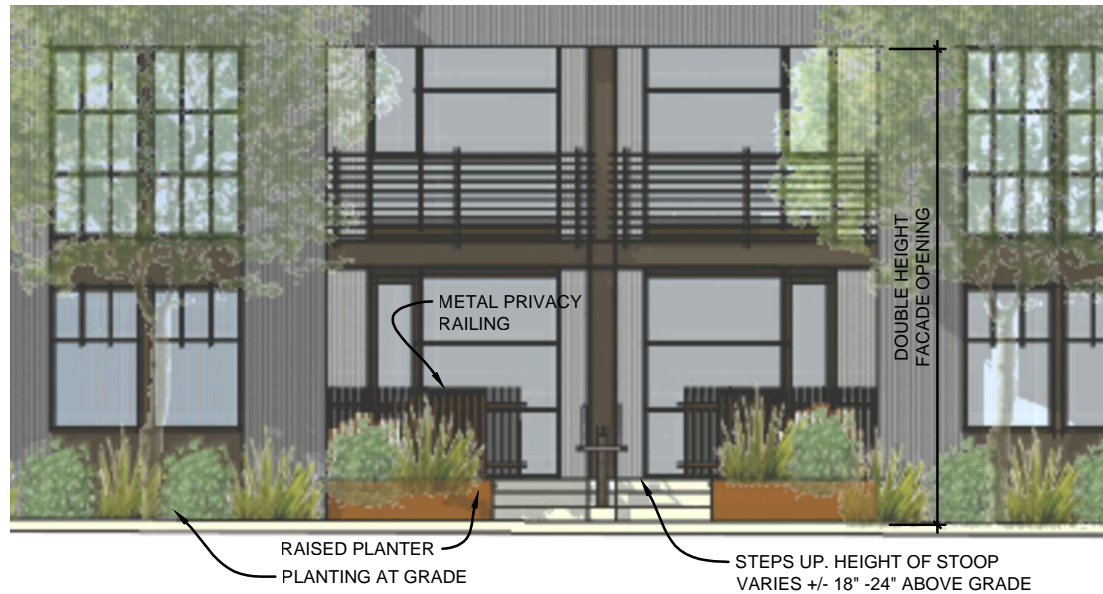
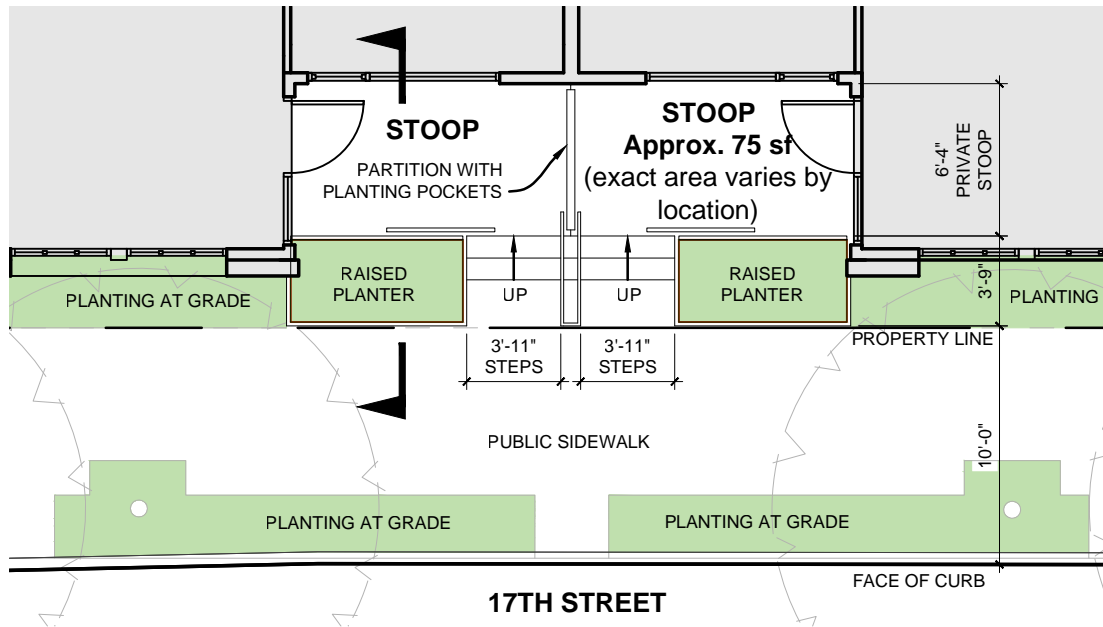
Scale AS NOTED Date 05.12.2016  
 Sheet Title  
**ACTIVE FRONTAGE**  
**MISSISSIPPI STREET**

**17K**





RESIDENTIAL BUILDING WITH STOOPS AT FLEX UNITS - DETAIL



TYPICAL STOOP SECTION : 1/8"=1'-0"



RESIDENTIAL BUILDING WITH STOOPS AT FLEX UNITS - 17TH STREET

ACTIVE GROUND FLOOR RESIDENTIAL EMPLOYS THE GOALS AND STRATEGIES OF "GUIDELINES FOR GROUND FLOOR RESIDENTIAL DESIGN":

- A VARIED AND CHANGING PEDESTRIAN EXPERIENCE ALONG THE LENGTH OF A BLOCK.
- AN EMPHASIS ON THE RECOGNIZABLE PRESENCE AND DELINEATION OF INDIVIDUAL RESIDENTIAL UNITS.
- ADEQUATE PRIVATE/PUBLIC TRANSITION SPACE FROM THE PUBLIC SIDEWALK TO THE GROUND FLOOR RESIDENTIAL UNITS.
- FUNCTIONAL, INVITING, AND SAFE STOOPS AND ENTRYWAYS.
- USABLE PRIVATE SPACE THAT ALSO ENCOURAGES PUBLIC INTERACTION AND SURVEILLANCE.
- A SOFTENING OF THE INTERFACE OF THE BUILDING AND SIDEWALK.
- AN INCREASE IN GREENING AND THE AMOUNT OF PERMEABLE SURFACE IN THE PUBLIC REALM.



CHRISTIANI JOHNSON ARCHITECTS, INC.  
665 Third Street, Suite 350  
San Francisco, CA 94107  
415.243.9484  
cjarchs.com

# 1200 17TH STREET

POTRERO PARTNERS, LLC

|                              |            |
|------------------------------|------------|
| Scale                        | Date       |
| NTS                          | 05.12.2016 |
| Sheet Title                  |            |
| RESIDENTIAL STOOP COMPLIANCE |            |

17L





1 BRICK BUILDING - VIEW NE



2 BRICK BUILDING - VIEW NW



3 BRICK BUILDING - VIEW NORTH



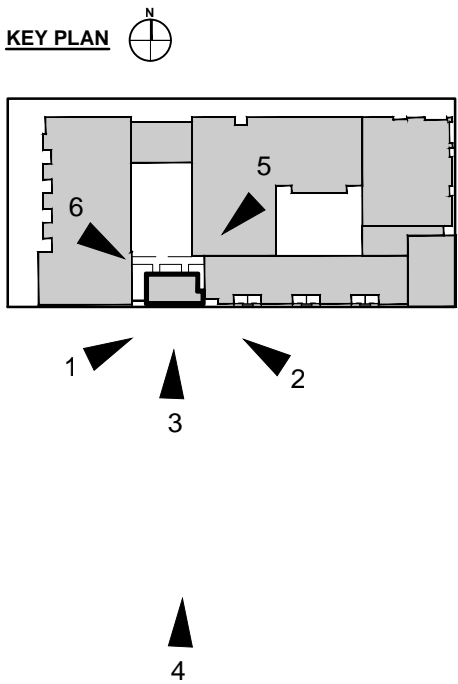
4 BRICK BUILDING - VIEW FROM TEXAS ST.



5 BRICK BUILDING - VIEW SW



6 BRICK BUILDING - VIEW SE







NEW RESIDENTIAL BUILDING  
ADJACENT

NEW BUILDING SETBACK  
4'-10" IN ORDER TO EXPOSE  
SOUTHEAST CORNER OF  
EXISTING BRICK BUILDING

STEEL CATWALK IS SET  
BACK 7'-6" FROM EXISTING  
BRICK BUILDING

NEW RESIDENTIAL  
LEASING / FITNESS  
/ LOUNGE

10'-5" WIDE SETBACK  
BETWEEN EXISTING  
BRICK BLDG & NEW  
BUILDING

NEW STEEL + GLASS  
ENTRY CANOPY

11'-9" WIDTH OF  
SETBACK BETWEEN  
BUILDINGS

17TH STREET

BRICK BUILDING - SETBACKS TO NEW CONSTRUCTION



CHRISTIANI JOHNSON ARCHITECTS, INC.  
665 Third Street, Suite 350  
San Francisco, CA 94107  
415.243.9484  
cjarchs.com

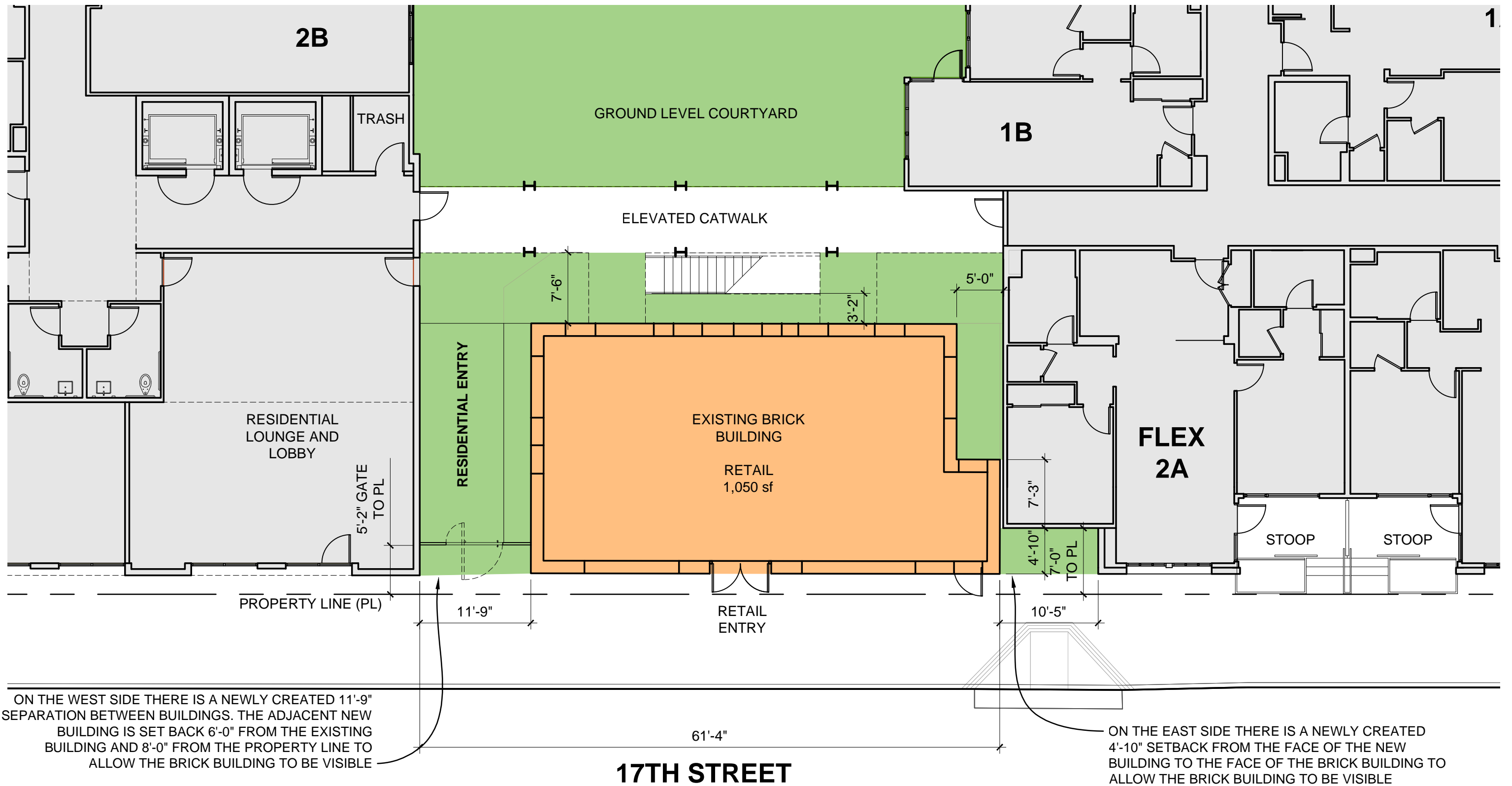
**1200 17TH STREET**  
POTRERO PARTNERS, LLC

Scale  
NTS  
Sheet Title  
**SETBACK DIAGRAM  
TO BRICK BUILDING**

Date  
05.12.2016

**17N**





SHADED AREA SHOWS EXTERIOR OPEN SPACE PROVIDED AROUND EXISTING BRICK BUILDING.

EXISTING BRICK BUILDING PERIMETER IS 152'-0". 144'-9" LINEAR FEET, OR 95% OF THE EXISTING BRICK BUILDING PERIMETER WILL BE VISIBLE.



CHRISTIANI JOHNSON ARCHITECTS, INC.  
665 Third Street, Suite 350  
San Francisco, CA 94107  
415.243.9484  
cjarchs.com

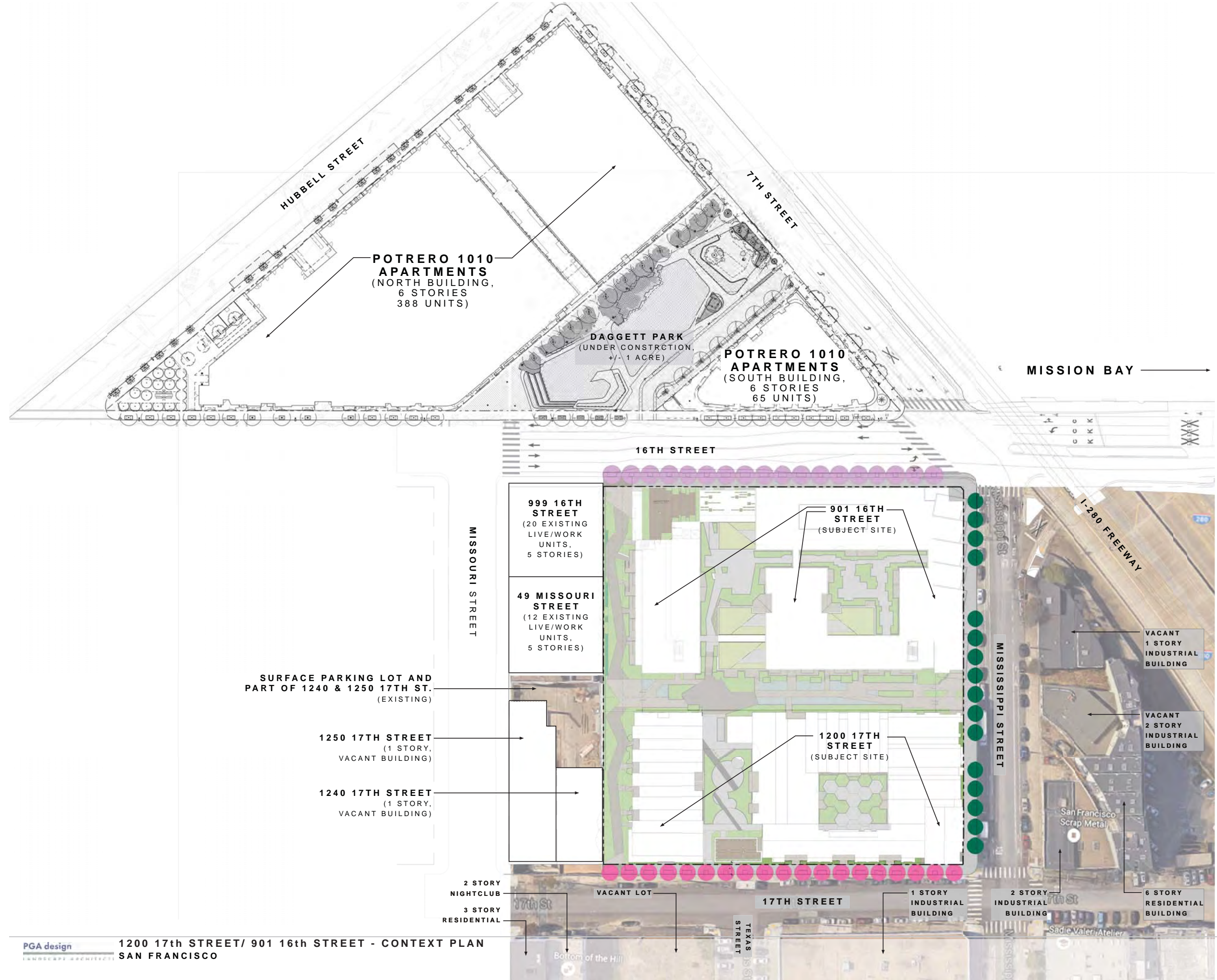
# 1200 17TH STREET

POTRERO PARTNERS, LLC

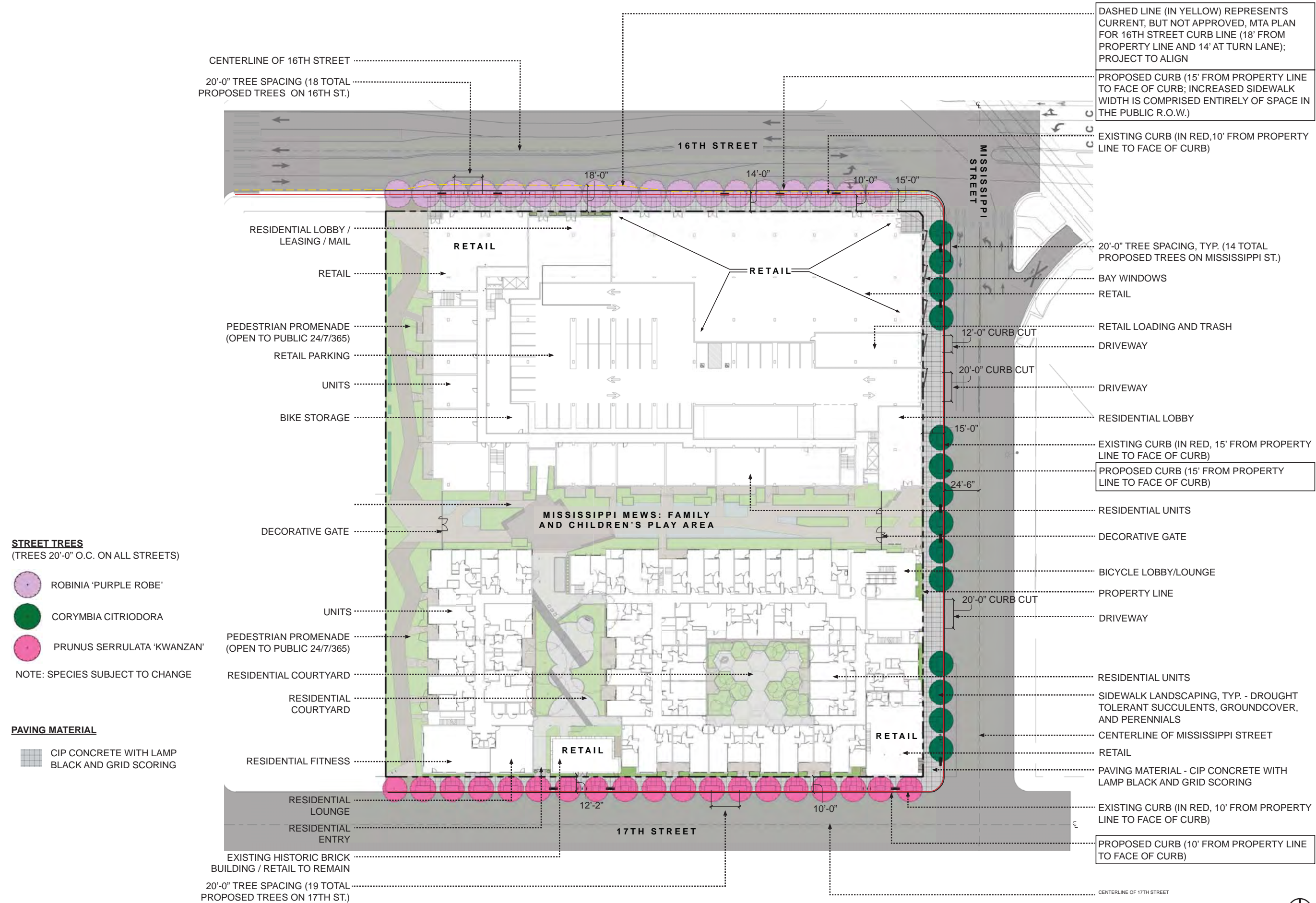


|                               |            |
|-------------------------------|------------|
| Scale                         | Date       |
| NTS                           | 05.12.2016 |
| Sheet Title                   |            |
| SETBACKS TO<br>BRICK BUILDING |            |









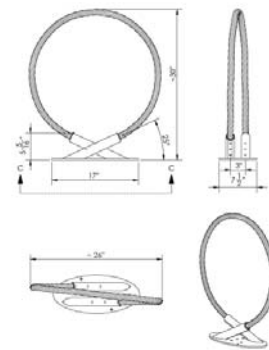


# **SITE FURNISHINGS**

BIKE RACKS, TYP.

BENCH, TYP.

STREET TREE PLANTING AREA, TYP.



EXAMPLE OF BIKE RACK



EXAMPLE OF BENCH

